

in GARY mode - Not Available for comment.
 comments submitted 11/19/12
 comments submitted 2/1/13
 comments to FIRM Elevation Quest 7/2/13

9/26/12

City of Portland

Development Review Application
 Planning Division Transmittal form

Application Number: 2012-601 **Application Date:** 9/19/2012 12:00:00
CBL: 444-A-1 AM
Project Name: Casco Bay Ferry Terminal Improvements
Address: 56 Commercial Street

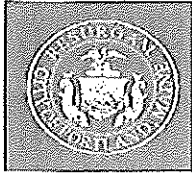
Project Description: Renovation and addition to the existing facility to accommodate operations associated with site and marine improvements.
Zoning: EWPZ

Other Reviews Required:
Review Type: Level II Site Plan

Distribution List:

<input type="checkbox"/> Planner <u>Nell</u>	Nell Donaldson	<input type="checkbox"/> Parking	John Peverada
<input checked="" type="checkbox"/> Zoning	Marge Schmuckal	<input type="checkbox"/> Design Review	Alex Jaegerman
<input type="checkbox"/> Traffic Engineer	Tom Errico	<input type="checkbox"/> Corporation Counsel	Danielle West-Chuhta
<input type="checkbox"/> Civil Engineer	David Sensus	<input type="checkbox"/> Sanitary Sewer	John Emerson
<input type="checkbox"/> Fire Department	Chris Pirone	<input type="checkbox"/> Inspections	Tammy Munson
<input type="checkbox"/> City Arborist	Jeff Tarling	<input type="checkbox"/> Historic Preservation	Deb Andrews
<input type="checkbox"/> Engineering	David Margolis-Pineo	<input type="checkbox"/> DRC Coordinator	Phil DiPierro
		<input type="checkbox"/> Outside Agency	

Comments needed by (7 days later): Wednesday, October 3, 2012



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Planning & Urban Development Department
Jeff Levine, AICP, Director

Planning Division
Alexander Jaegerman, Director

June 5, 2013

Lauren Swett
Woodard & Curran
41 Hutchins Drive
Portland, ME 04101

Project Name: Casco Bay Island Transit District
Address: 56 Commercial Street
Applicant: Casco Bay Island Transit District
Planner: Nell Donaldson

RECEIVED

JUN 10 2013

Dept. of Building Inspections
Hank Berry, Nick Matrone
City of Portland
Casco Bay Island Transit District
PO Box 4656
Portland, ME 04112

Project ID: 2012-601
CBL: 444-A-1

Dear Ms. Swett:

On June 5, 2013, the Planning Authority approved with conditions your Level II site plan for the improvements proposed at the Casco Bay Ferry Terminal at 56 Commercial Street. The decision is based upon the plan as submitted by Woodard & Curran (revisions dated May 7, 2013). The proposal was reviewed for conformance with the standards of the City of Portland's site plan ordinance (Section 14-526). The Level II site plan is approved with the following waivers and conditions:

A. WAIVERS

Site Lighting

Given the nature of the building as a transportation facility serving users at all times of day, the Planning Authority waives the technical standards related to exterior site lighting to allow maximum and average illumination levels that are higher than those stipulated in Sections 12.2.3 and 12.2.5 of the city's Technical Manual. As proposed, illumination levels meet the IESNA standards for transportation stations.

B. CONDITIONS OF APPROVAL

The Planning Authority found that the plan is in conformance with the site plan standards of the land use code subject to the following conditions of approval:

1. Prior to scheduling any pre-construction meeting, the applicant shall submit a construction management plan for review and approval by the city's Department of Public Services.
2. Prior to issuance of a building permit, the applicant shall submit an elevation certificate in compliance with floodplain regulations, verifying that the first floor elevation is at least 2 feet above the elevation of

9.6 NGVD as listed in the Flood Insurance Study promulgated by the Federal Emergency Management Agency for the City of Portland, or a minimum of 11.6 NGVD.

The approval is based on the May 7, 2013 site plan.

C. STANDARD CONDITIONS OF APPROVAL

Please note the following standard conditions of approval and requirements for all approved site plans:

1. **Develop Site According to Plan** The site shall be developed and maintained as depicted on the site plan and in the written submission of the applicant. Modification of any approved site plan or alteration of a parcel which was the subject of site plan approval after May 20, 1974, shall require the prior approval of a revised site plan by the Planning Board or Planning Authority pursuant to the terms of Chapter 14, Land Use, of the Portland City Code.
2. **Separate Building Permits Are Required** This approval does not constitute approval of building plans, which must be reviewed and approved by the City of Portland's Inspection Division and Fire Department. During the review of the building plans, the City of Portland reserves the right to require review by an independent third party at the applicant's expense. Please also note that separate permits are required for any new signage.
3. **Site Plan Expiration** The site plan approval will be deemed to have expired unless work has commenced within one (1) year of the approval or within a time period up to three (3) years from the approval date as agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the one (1) year expiration date.
4. **Inspection Fees** A minimum inspection fee payment of \$300 and seven (7) final sets of plans must be submitted to and approved by the Planning Division and Public Services Department prior to the release of a building permit, street opening permit or certificate of occupancy for site plans. If you need to make any modifications to the approved plans, you must submit a revised site plan application for staff review and approval.
5. **Preconstruction Meeting** Prior to the release of a building permit or site construction, a pre-construction meeting shall be held at the project site. This meeting will be held with the contractor, Development Review Coordinator, Public Service's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the Development Review Coordinator will confirm that the contractor is working from the approved site plan. The site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the pre-construction meeting.
6. **Department of Public Services Permits** If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)
7. **As-Built Final Plans** Final sets of as-built plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (*.dwg), release AutoCAD 2005 or greater.

The Development Review Coordinator must be notified five (5) working days prior to the date required for final site inspection. Phil DiPierro, Development Review Coordinator, can be reached at the Planning

Division at 874-8632. All site plan requirements must be completed and approved by the Development Review Coordinator prior to issuance of a Certificate of Occupancy. Please schedule any property closing with these requirements in mind.

If there are any questions, please contact Neil Donaldson at (207) 874-8723.

Sincerely,



Alex Jaegerman
Planning Division Director

CC: Jeff Levine, Director of Planning and Urban Development
Alexander Jaegerman, Planning Division Director
Barbara Barhydt, Development Review Services Manager
Phillip DiPierro, Development Review Coordinator, Planning
Margo Schmuckal, Zoning Administrator, Inspections Division
Tammy Munson, Inspection Division Director
Lannie Dobson, Administration, Inspections Division
Gayle Guertin, Administration, Inspections Division
Michael Bobinsky, Public Services Director
Katherine Earley, Engineering Services Manager, Public Services
Bill Clark, Project Engineer, Public Services
David Margolis-Pineo, Deputy City Engineer, Public Services
Doug Roncarati, Stormwater Coordinator, Public Services
Greg Vining, Associate Engineer, Public Services
Michelle Sweeney, Associate Engineer
John Low, Associate Engineer, Public Services
Matt Doughty, Field Inspection Coordinator, Public Services
Mike Farmer, Project Engineer, Public Services
Jano Ward, Administration, Public Services
Jeff Taring, City Arborist, Public Services
Captain Chris Pirone, Fire Department
Thomas Errico, P.E., T.Y. Lin Associates
David Senus, P.E., Woodard and Curran
Rick Blackburn, Assessor's Department
Approval Letter File

Marge Schmuckal - RE: CBITD Approval Letter

From: Lauren Swett <lswett@woodardcurran.com>
To: Helen Donaldson <HCD@portlandmaine.gov>
Date: 6/18/2013 12:54 PM
Subject: RE: CBITD Approval Letter
CC: Marge Schmuckal <MES@portlandmaine.gov>

Hi Marge,

Have you had a chance to check on the flood elevation reference?

Thanks,
Lauren

From: Helen Donaldson [mailto:HCD@portlandmaine.gov]
Sent: Tuesday, June 11, 2013 9:07 AM
To: Lauren Swett
Cc: Marge Schmuckal
Subject: RE: CBITD Approval Letter

Lauren,

I'll defer to Marge, as that comment originated with her.

Nell

>>> Lauren Swett <lswett@woodardcurran.com> 6/10/2013 2:12 PM >>>
Thank you Nell for sending this along last week!

I had a quick question for you with regards to the flood elevation listed on the second condition of approval. Attached is the FEMA FIRM for our area. FEMA lists a base flood elevation of 10 ft NGVD, while your approval letter references 9.6 ft. Regardless, at 12.33 feet, our finish floor elevation will be greater than 2' above the base flood elevation, but I wanted to check with you to see where the 9.6 value came from so that we are consistent when we submit the elevation certificate. I've cc'ed Marge here, as she may know too.

Thanks,
Lauren

From: Helen Donaldson [mailto:HCD@portlandmaine.gov]
Sent: Wednesday, June 05, 2013 2:33 PM
To: Lauren Swett
Cc: David Senus
Subject: Re: CBITD Approval Letter

Lauren and Dave,

Yes, you are finally there....See attached. Hard copies should also go in the mail.

It's a good project. I'm sorry it took so long to put it through the ringer here!

Best,
Nell

Nell Donaldson
City of Portland
389 Congress Street
Portland, Maine 04101
874-8723
hcd@portlandmaine.gov

>>> Lauren Swett <lswett@woodardcurran.com> 6/4/2013 5:41 PM >>>

Hi Nell,

It was good to talk to you today about the CBITD site plan approval. It sounds like we're finally there!

I am going to be out of the office for the rest of the week starting around noon tomorrow, but if there are any questions that come up my cell phone number is 207-219-3591.

When the approval letter is ready, please send it to Dave Senus so that he can forward it to the Architect, who will pass it along to the bidding contractors.

Thanks for all your help!

Lauren


Lauren Swett, P.E.

Woodard & Curran
41 Hutchins Drive
Portland, Maine 04102
Phone: (800)426-4262 ext. 3303
Fax: (207)774-6635
Email: lswett@woodardcurran.com

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Notice: Under Maine law, documents - including e-mails - in the possession of public officials or city employees about government business may be classified as public records. There are very few exceptions. As a result, please be advised that what is written in an e-mail could be released to the public and/or the media if requested.

Legend

-  1% annual chance (100-Year) Floodplain
-  1% annual chance (100-Year) Floodway
-  0.2% annual chance (500-Year) Floodplain



MAP SCALE 1" = 400'



NATIONAL FLOOD INSURANCE PROGRAM

FIRM FLOOD INSURANCE RATE MAP

CITY OF
PORTLAND, MAINE
CUMBERLAND COUNTY

PANEL 14 OF 17
(SEE MAP INDEX FOR PANELS NOT PRINTED)

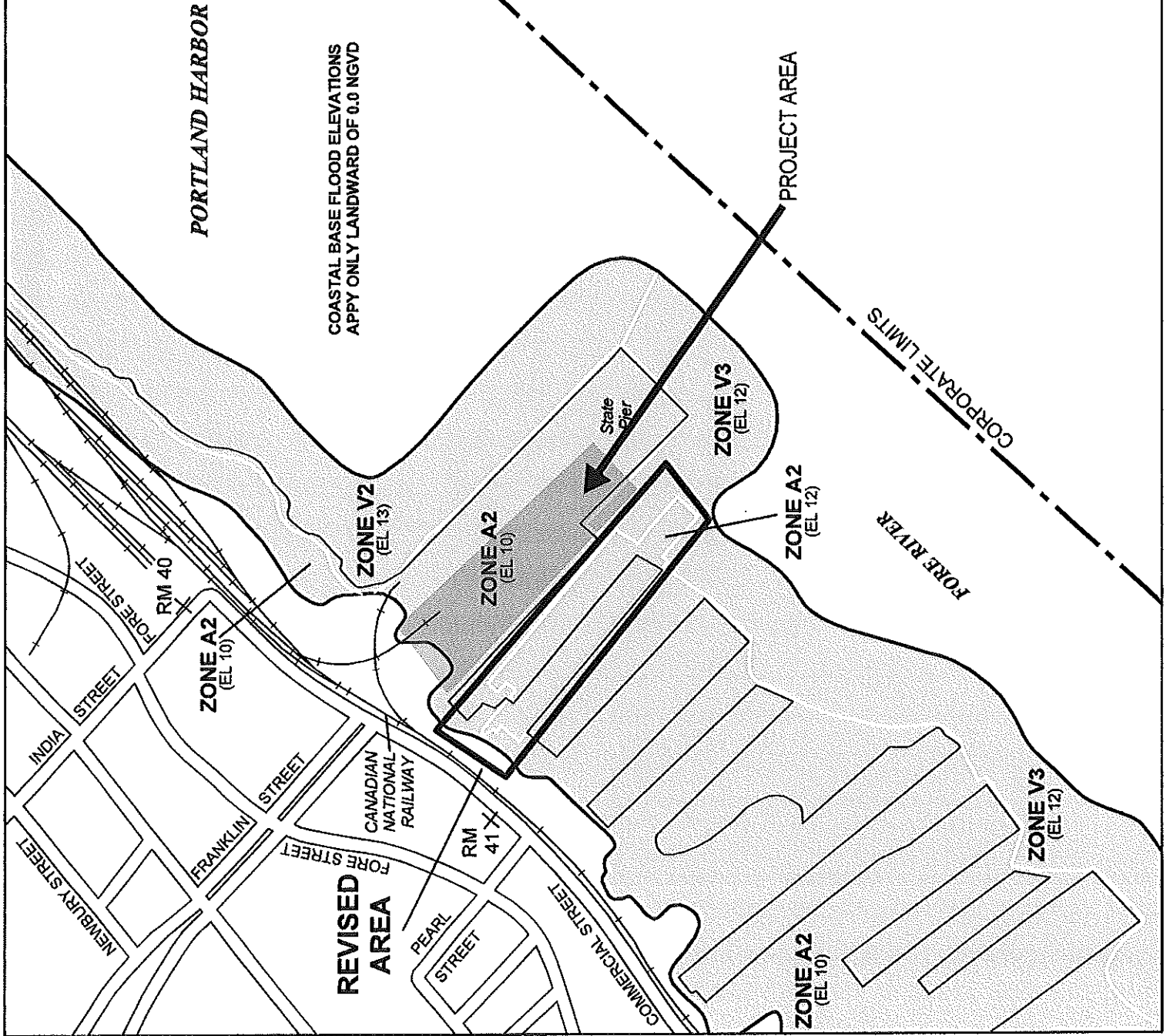
REVISED TO
REFLECT LOMR
EFFECTIVE:
SEPTEMBER 27, 2011

COMMUNITY-PANEL NUMBER
230951 0014 B

EFFECTIVE DATE:
JULY 17, 1986



Federal Emergency Management Agency



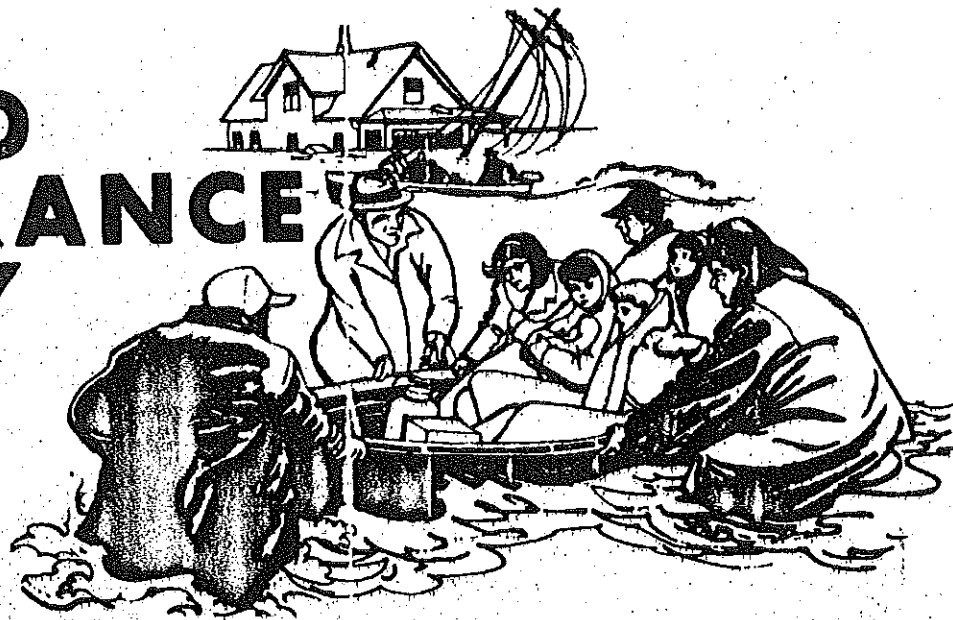
Marge Schmuckal - CBITD - FIRMS

From: Marge Schmuckal
To: lswett@woodardcurran.com
Date: 7/2/2013 4:45 PM
Subject: CBITD - FIRMS
CC: Helen Donaldson

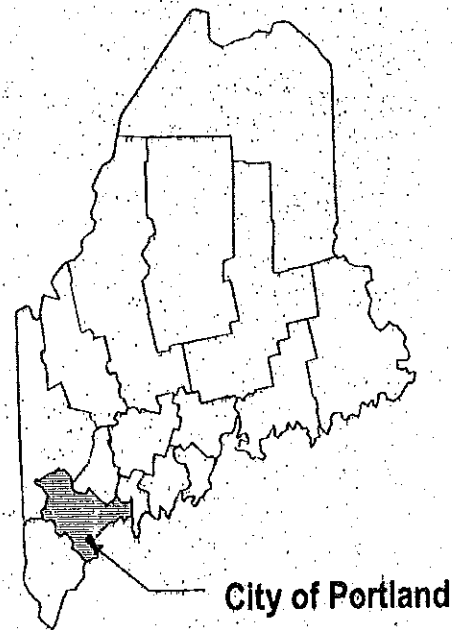
Hi Lauren,

It is my understanding that the most correct way is to determine elevation is to use the Flood Insurance Study that was issued by FEMA when we first received our maps. The study more accurately pinpoints the elevations, instead of the rounding up or down on the maps. I was going to send you a copy of it, but my "G" drive with the information is down. If you are at tomorrow's meeting I can bring you what I am talking about. Or I may shut down my computer to night and see if rebooting corrects my problem. There is a method behind my madness.
Marge

FLOOD INSURANCE STUDY



**CITY OF
PORTLAND
AND TOWN OF
LONG ISLAND,
MAINE
CUMBERLAND COUNTY**



REVISED:
DECEMBER 8, 1998



Federal Emergency Management Agency

COMMUNITY NUMBERS
230051-City of Portland
231035-Town of Long Island

Congress Street Bridge. The 100-year flood elevations for Back Cove range from 9.4 feet at the mouth to 9.8 feet in both the northwest and southwest quadrants.

The stillwater elevations for the 10-, 50-, 100-, and 500-year floods have been determined for Casco Bay, the Fore River, and Back Cove and are shown in Table 3, "Summary of Stillwater Elevations."

TABLE 3 - SUMMARY OF STILLWATER ELEVATIONS

<u>FLOODING SOURCE AND LOCATION</u>	<u>ELEVATION (feet NGVD)</u>			
	<u>10-YEAR</u>	<u>50-YEAR</u>	<u>100-YEAR</u>	<u>500-YEAR</u>
CASCO BAY				
Martin Point	8.6	9.2	9.5	10.0
Fish Point	8.7	9.3	9.5	10.1
At Pumpkin Nob on Peaks Island	8.6	9.2	9.4	10.0
At City Point on Peaks Island	8.6	9.1	9.4	9.9
Eastern side of Peaks Island	8.6	9.1	9.4	9.9
Western side of Cushings Island	8.5	9.1	9.3	9.9
Eastern side of Cushings Island	8.6	9.1	9.4	9.9
Eastern side of House Island	8.5	9.1	9.3	9.8
Western side of House Island	8.6	9.2	9.4	10.0
Great Diamond Island	8.6	9.2	9.4	10.0
Western side of Little Diamond Island	8.6	9.2	9.4	9.9
Eastern side of Little Diamond Island	8.6	9.2	9.4	10.0
Northern side of Long Island	8.5	9.1	9.3	9.9
Southern side of Long Island	8.4	9.0	9.2	9.7
Western side of Long Island	8.5	9.1	9.4	9.9
Northeastern side of Cliff Island	8.5	9.1	9.3	9.9
Southeastern side of Cliff Island	8.5	9.0	9.3	9.8
Western side of Cliff Island	8.5	9.1	9.4	9.9
Jewell Island	8.5	9.0	9.3	9.8
FORE RIVER				
At its mouth	8.7	9.3	9.5	10.1
State Pier	8.7	9.4	9.6	10.1
Coast Guard Base (South Portland)	8.8	9.5	9.7	10.3
Portland Bridge	8.9	9.6	9.8	10.4
Veterans Memorial Bridge	8.9	9.6	9.8	10.4
Thompson Point	9.1	9.9	10.2	10.9
Congress Street Bridge	9.1	9.8	10.1	10.8
BACK COVE				
Casco Bay outlet and north shore	8.6	9.2	9.4	10.0
West, south, and east shores	8.9	9.5	9.8	10.4

MEMORANDUM

To: FILE

From:

Subject: Application ID: 2012-601

Date: 2/1/2013

Comments Submitted by: Marge Schmuckal/Zoning on 2/1/2013

On 1/17/2013 the Zoning Board of Appeals reviewed the request for a variance for the setback of 22' instead of the currently required 25' in the EWPZ. The Board first voted in agreement with my memo that the roof line because of its height would not have to meet the 25' setback to the pier edge. The Board did not vote on the other portion of the appeal. One Board member stated that he did not think this was even an appeal. He thought it would be legally nonconforming. The appeal was tabled.

On 1/24/2013 I met with Austin Smith of Simon Architects and Peter Plumb of Murray Plumb and Murray. Austin had the original plans of the building when it was first built. He showed me that the supports that held up the existing building are the same supports that will be holding up the new building from the piers under the decking to upward and supporting the roof. They are the same in support then and now. They were able to get me an overlay of the proposed building on the original building plans. They match. There are no changes to the location of those supports. Therefore, I have concluded that the current structure is legally nonconforming for the setback. And that there are rights to rebuild within that legal nonconforming setback if done within one year. And further more I have determined that the proposed structure and project is in compliance with the Land Use Zoning Ordinance.

It is noted that DEP was notified of the appeal because it is within the Shoreland Zone. DEP gave tacit approval by not negatively responding prior to the appeal date.

The project will still need to supply an Elevation Certificate in compliance with Floodplain regulations at the time of permit application. They must verify that the first floor elevation is at least 2 feet above the elevation of 9.6 NGVD as listed in the Flood Insurance Study promulgated by the Federal Emergency Management Agency for the City of Portland, or a minimum of 11.6 NGVD.

My review indicates that the rest of the EWPZ requirements are being met. Separate permits shall be required for any new signage.

Marge Schmuckal

Marge Schmuckal - Casco Bay Ferry Terminal Improvements

From: Marge Schmuckal
To: Helen Donaldson
Date: 11/19/2012 4:07 PM
Subject: Casco Bay Ferry Terminal Improvements

56 Commercial Street - 444-A-001
#2012-601 - Easter Waterfront Port Zone (EWPZ)
11/19/2012

Project in several phases. Phase one removes a portion of the building and replaces it with a 3,006 sq. ft new terminal waiting room, new ticket office, and new restrooms for passengers and employees. The entire building is located within the EWPZ zone. The use is a domestic ferry use and is a listed permitted use in the EWPZ zone. The only required setback is a minimum of 25' from the edge of any pier, wharf or working edge. The submission shows an approximate 22' from the pier edge to the side of the new addition. This violation requires a variance to the Zoning Board of Appeals. The applicant acknowledged this requirement in the submission. As of this date, an appeal application has not been received.

The project is also located in an A-2 Flood Zone with an elevation of 10 feet as shown on FEMA panel 14. Under the City of Portland requirements, the minimum floor area for the addition shall be no less than 2 feet above the given map elevation of 10., or at least 12'. The applicant is showing 12.33 feet. The applicant will be required to fill out the paper work and a certificate of elevation for the new addition when appropriate.

Marge Schmuckal
Zoning Administrator

ZBA

Applicant: CASCO Bay Lines

Date: 9/23/12 & 10/19/12

Address: 56 Commercial St

C-B-L: 444-A-001

CHECK-LIST AGAINST ZONING ORDINANCE

Date - Existing Dev.

Zone Location - WPA EWPZ

Interior or corner lot -

Proposed Use/Work - CASCO Bay Lines TO Demo & Enlarge The Area
OK - domestic ferry use W/adj

Seepage Disposal -

Lot Street Frontage - No min

Front Yard -

Rear Yard -

Side Yard -

20A
regular
NO decision
remanded back

no req except for 25' setback from the edge of any pier, wharf or working edge
~ 22' shown

Projections -

Width of Lot -

Height - max bldg height 45' - less than 22' ~ 21' shown

Lot Area - no min. - 106,100 sq ft

Lot Coverage/Impervious Surface - 100% allowed

Area per Family - N/A

Off-street Parking - # 14 - 332.1(j)

Adjacent to parking garage - showing over 17 parking spaces including ramp
? Public Building 1 per 400 sq ft
New 13,500 ÷ 400 = 17
The number of parking spaces required at 50% of uses as otherwise provided in Division 20 -

Loading Bays -

Site Plan - level II

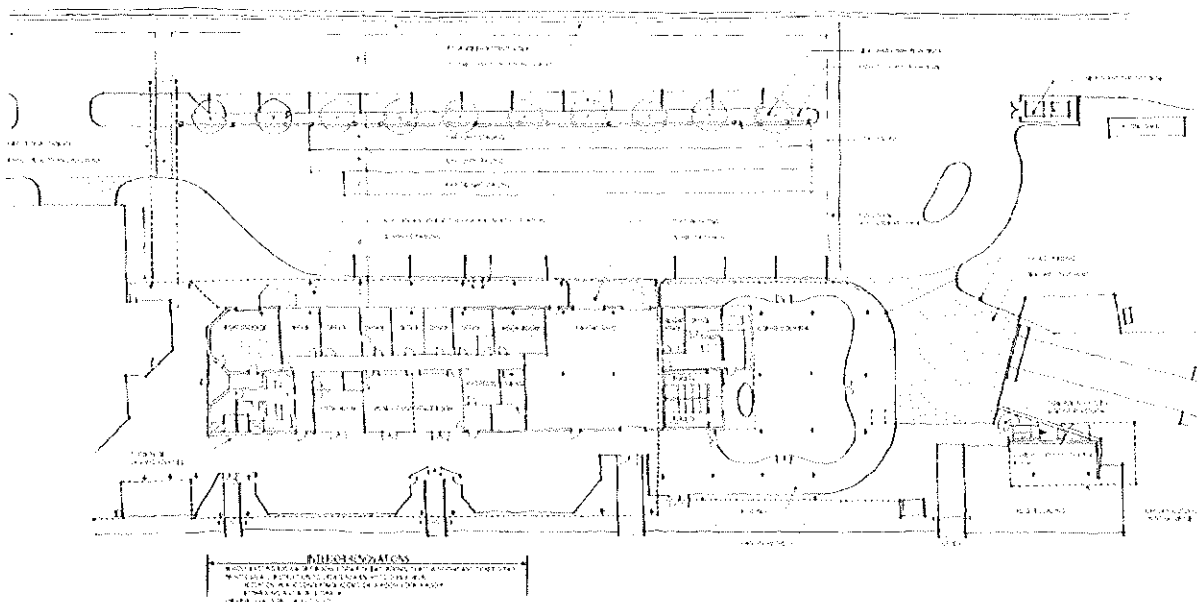
Shoreland Zoning/Stream Protection - nothing

Flood Plains - AB zone - el 10 - panel 17 - Showing New AS (2.33') will need to provide a Certificate of Elevation

PROJECT DATA

The following information is required where applicable, in order to complete the application

Total Site Area	108,100	sq. ft.
Proposed Total Disturbed Area of the Site	22,000	sq. ft.
(If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)		
IMPERVIOUS SURFACE AREA		
• Proposed Total Paved Area	0 (no new)	sq. ft.
• Existing Total Impervious Area	103,960	sq. ft.
• Proposed Total Impervious Area	103,960	sq. ft.
• Proposed Impervious Net Change	0	sq. ft.
BUILDING AREA		
• Proposed Building Footprint	13,580	sq. ft.
• Proposed Building Footprint Net change	3,007	sq. ft.
• Existing Total Building Floor Area	10,573	sq. ft.
• Proposed Total Building Floor Area	13,580	sq. ft.
• Proposed Building Floor Area Net Change	3,007	sq. ft.
• New Building	No	(yes or no)
ZONING		
• Existing	EWPZ & Shoreland Overlay Zone	
• Proposed, if applicable	N/A	
LAND USE		
• Existing	Ferry Terminal	
• Proposed	Ferry Terminal	
RESIDENTIAL, IF APPLICABLE		
• Proposed Number of Affordable Housing Units	N/A	
• Proposed Number of Residential Units to be Demolished		
• Existing Number of Residential Units		
• Proposed Number of Residential Units		
• Subdivision, Proposed Number of Lots		
PARKING SPACES		
• Existing Number of Parking Spaces	N/A	
• Proposed Number of Parking Spaces		
• Number of Handicapped Parking Spaces		
• Proposed Total Parking Spaces		
BICYCLE PARKING SPACES		
• Existing Number of Bicycle Parking Spaces	N/A	
• Existing Number of Bicycle Parking Spaces		
• Proposed Number of Bicycle Parking Spaces		
• Total Bicycle Parking Spaces		
ESTIMATED COST OF PROJECT		



CBITD Terminal Master Plan: Proposed Site and Floor Plan

2.2 MASTER PLAN PHASE 1

Phase 1 of the CBITD Terminal Master Plan includes building expansion and renovation and marine improvements. The intent of the project will be to modify the existing facility to improve the level of service provided to the nearly one million people who currently use the facility's ferry services. There will be no change of use and existing services will be relocated within the building to more effectively use the facility's space. The improvements are not proposed to add additional capacity to the facility, but instead to enhance the experience for customers using the facility. We do not anticipate a change in the level of traffic to the facility.

2.2.1 Marine Improvements

The marine work includes the replacement of fender piles and portions of the wale and chock on the fixed fender system along Gates 1, 2, 3, and 4; the repair of the transfer bridge for the car ferry, and removal, repair, and repainting of the pontoon that supports the transfer bridge; and the replacement of breasting and alignment dolphins for the car ferry slip. It is our understanding that Site Plan Review is not required for these marine improvements, because the work is specifically associated with the maintenance of existing marine infrastructure. A building permit will be acquired for the marine construction work.

2.2.2 Building Improvements

This Site Plan Application is being submitted for the CBFT building expansion and renovation work. The extent of this work includes the following components:

- Construction of a building expansion (3,006 square feet) to contain a new terminal waiting room, new ticket office, new freight office, and new restrooms for passengers and employees;

-
- Site improvements around the perimeter of the building expansion;
 - Renovation of portions of the existing facility with partitions, doors, and shower stalls;
 - Relocation of existing mechanical and electrical systems;
 - Replacement of a failing exterior concrete slab at the entrance to the adjacent garage;
 - Repair of structural trusses at Gates 1, 2, and 3; and
 - Correction of water infiltration problems along the existing building's eastern wall.

This work will result in a significant improvement to the CBFT, providing improved service to the facility's many customers.

2.3 LEVEL II SITE PLAN APPLICATION

Due to the size of the proposed building expansion (3,006 square feet expansion < 10,000 square feet), the project requires review under a Level II Site Plan. The following report is presented in conformance with the requirements of a Final Level II Site Plan Applications. Attachments are included throughout the report in support of various sections. Civil and architectural plan sheets are attached showing the proposed design of the project. A lighting plan has not been completed at the time of submission, and will be submitted under separate cover.

6. ASSESSMENT OF ZONING

The property is located in the Eastern Waterfront Port Zone (EWPZ) and Shoreland Overlay Zone, and will be designed to comply with the standards and intent of Division 17.5 and Division 26 of the land use regulations, respectively. The proposed maintenance activities will not result in any changes to the site's existing use; the proposed renovations will help to better accommodate the facility's operations.

6.1 EASTERN WATERFRONT PORT ZONE (LAND USE CODE DIVISION 17.5)

The project consists of improvements to an existing ferry terminal building. In accordance with Division 17.5 of the Land Use Code, intermodal marine passenger facilities are a permitted use within the EWPZ.

6.1.1 Dimensional Standards (Land Use Code Section 14-303)

There are no requirements for minimum lot size, minimum frontage, or front, side, and rear setbacks. The maximum impervious surface allowed within the zone is 100%. The maximum building height is forty five feet. The proposed terminal building improvements will meet these dimensional standards.

The minimum setback from the pier line is 25 feet from the edge of the pier. This setback area may be utilized for water-dependent uses and public uses and activities. The proposed building expansion currently proposes a setback of 22 feet. The proposed building layout utilizes existing structural piles that currently support the existing building's overhang. The installation of additional piles to achieve the three additional feet of setback would be cost prohibitive, and would result in a greater environmental impact than using these existing piles. The Applicant is submitting a Practical Difficulty Variance with the Zoning Board of Appeals concurrently with this application for Site Plan review. We understand that Site Plan approval will not be granted until the Zoning Board of Appeals has made a decision on the variance.

fill Variance ultimately not needed

6.1.2 Performance Standards (Land Use Code Section 14-304)

The proposed terminal building improvements will not result in any changes to:

- The outdoor storage of materials (14-304 (a));
- Noise (14-304 (b));
- Vibration (14-304 (c));
- Discharge (14-304 (e));
- Storage of vehicles (14-304 (f));
- Off-street parking (14-304 (h));
- Off-street loading (14-304 (i)); and
- Functional utility of the pier and access to the water's edge (14-304 (o));

In addition:

- The project will comply with all federal and state environmental regulations (14-304 (d)). See Section 4 of this report for more information on the other permits being filed for this project.
- The project will not require any filling of docking and berthing areas (14-304 (g)).
- The project will comply with all shoreland and flood plain management regulations (14-304 (j)). See the following subsection of this report for more information on compliance with these regulations.
- All lighting will be shielded to prevent interference with vessels in the harbor and will be compliant with the Site Lighting Standards of the City of Portland Technical Manual (14-304 (k)). The lighting plan will be submitted under separate cover.
- Any new signs shall be permitted in accordance with Division 22 of the Land Use Code (14-304 (l)).
- The facility is an approved marine use, and the project will not have any impact on, or involve any non-marine uses (14-304 (n)).

The project will be in compliance with all performance standards of the EWPZ.

6.2 SHORELAND REGULATIONS (LAND USE CODE DIVISION 26)

The shoreland regulations are applicable to any structure built on, over, or abutting a dock, wharf or pier (14-447). The existing terminal building and its proposed expansion are located on the Maine State Pier.

6.2.1 Land Use Standards (Land Use Code Section 14-449)

6.2.1.1 Principal and Accessory Structures (Land Use Code Section 14-449(a))

For principal and accessory structures, no shoreland setback requirements exist for projects located in the EWPZ and the Shoreland Overlay Zone (14-449(a)1.)

Flood plain

The lowest floor elevation and openings of the building will be elevated at least one foot above the elevation of the 100-year flood plain (14-449(a)3.). The project is located within the A2 zone, which has a 100-year flood elevation of 10 feet (based on NGVD 1929). Using the same datum, the proposed building will have a floor elevation of 12.33 feet, 2.33 feet higher than the 100-year flood elevation. A copy of the Federal Emergency Management Authority (FEMA) Flood Insurance Rate Map (FIRM) showing the 100-year flood elevation is attached.

6.2.1.2 Structures Extending Over the Normal High Water Line (Land Use Code Section 14-449(b))

The project will meet the following standards for structures extending over the Normal High Water Line:

- The project will not alter or create new access to the terminal building from the shore.
- The project location will not interfere with existing beach areas.
- The project will not impact existing fisheries.

- The building expansion has been sized appropriately to better handle the existing passenger load experienced at the terminal building. The building use will not change.
- Permits will be received from the Department of Environmental protection and the Army Corps of Engineers for this project. See Section 4 of this report for more information.
- The project will be built on an existing pier. This action is allowed in the EWPZ.
- No residential dwellings will be created as part of this project.

6.2.1.3 Other General Standards (Land Use Code Sections 14-449(c)-(o))

No clearing is proposed as part of this project (14-449(c)). The only vegetation removal will be the elimination of a concrete planter which currently contains a tree. Appropriate erosion and sediment control measures will be utilized during construction (14-449(d)). Details of the proposed erosion and sediment control are shown on the attached drawings.

The project is not anticipated to have an impact on the following:

- Soils (14-449(e)) – the building expansion will be founded on existing earthen fill and piles.
- Water quality (14-449(f)) – no new impervious area will be created as part of this project, and the proposed building is considered to have a lower pollutant load than the existing pavement.
- Archaeological sites (14-449(g)) – the project is located on an existing developed pier.
- Installation of public utility service (14-449(h)) – no new public utility service will be installed as part of this project.
- Essential service (14-449(i)) – no new essential services will be installed as part of this project.
- Roads and driveways (14-449(j)) – no new roads or driveways will be installed as part of this project.
- Parking areas (14-449(k)) – no new parking areas will be installed as part of this project.
- Septic waste disposal (14-449(l)) – the project will utilize an existing sewer service connecting to the City of Portland sewer system.
- Stormwater runoff (14-449(m)) – the project will create no new impervious area as part of this project. Stormwater management is further addressed in Section 9.
- Agriculture (14-449(n)) – the project will not include any agricultural activities.

The project's general site plan features (14-449(o)) shall meet the following standards:

- The project will maintain safe and healthful conditions;
- The project will not result in water pollution, erosion, or sedimentation to surface waters;

- The project will adequately provide for disposal of all wastewater;
- The project will not have an adverse impact on the spawning grounds, fish, aquatic life, bird or other wildlife habitat;
- The project will not impact shore cover or visual as well as actual points of access to inland and coastal waters;
- The project will not impact archaeological or historic resources;
- The project will not adversely impact existing commercial fishing or maritime activities;
- The project will avoid problems associated with flood plain development and use.

The project will be in conformance with all shoreland regulations.

6.3 FLOOD PLAIN MANAGEMENT REGULATIONS (LAND USE CODE DIVISION 26.5)

The project is required to comply with the flood plain management regulations because the proposed work will be taking place within an area of special flood hazard (14-450.3). Areas of special flood hazard are defined as “the land in the flood plain having a one (1) percent or greater chance of flooding in any given year as specifically identified in the Flood Insurance Study” (14-450.5). The attached FEMA FIRM map shows that the project area is located within the A2 Zone, which has a 100-year flood (equivalent to flood with one percent chance of occurring in any year) elevation of 10 feet (vertical datum NGVD).

The requirements of a flood hazard area development permit shall be met as required for this project (14-450.6). The project will conform to the following standards of the flood plain management regulations (14-450.8):

- The project shall be designed to resist the impacts of flooding, and will be constructed above the 100-year flood elevation (14-450.8(a)).
- The project will utilize an existing public water supply (14-450.8(b)).
- The project will utilize existing public sanitary sewer services (14-450.8(c)).
- On-site waste disposal systems will be located to avoid the impacts of flooding (14-450.8(d)).
- No watercourses will be altered as part of this project (14-450.8(e)).
- The project includes no residential construction (14-450.8(f)).
- The project includes the construction of a nonresidential structure. The proposed building expansion is located in the A2 Zone. The zone has a base flood elevation of 10 feet (NGVD). The proposed building expansion will have a finish floor elevation of 12.33 feet (NGVD), higher than the required two feet above the flood elevation (14-450.8(g)).
- The project will not include any manufactured homes (14-450.8(h)).

ok

-
- The project will not include any recreational vehicles (14-450.8(i)).
 - The project will not include any accessory structures (14-450.8(j)).
 - The project will be located on an existing developed pier, and will not be located within a floodway (14-450.8(k)).
 - No enclosed areas will be constructed below the proposed building's floor (14-450.8(l)).
 - No new bridges will be constructed as part of the project (14-450.8(m)).
 - No new containment walls will be constructed as part of the project (14-450.8(n)).
 - No new wharves, piers, or docks will be constructed as part of the project (14-450.8(o)).
 - The project is located within Zone A2, and will conform to all applicable local, state, and federal regulations (14-450.8(p)). Information on the project's other permit applications is included in Section 4.

The project will comply with all flood plain management regulations.

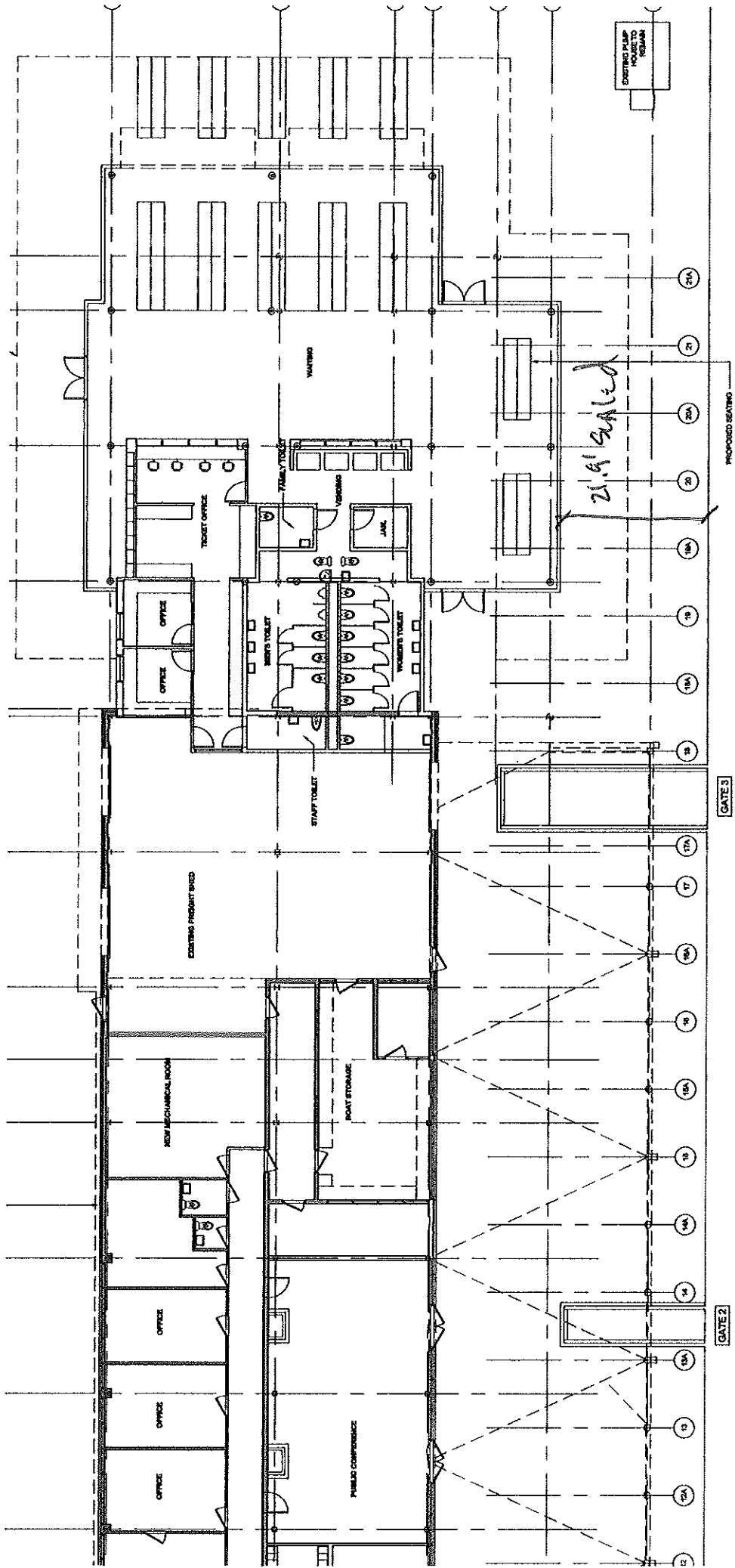
4. EVIDENCE OF STATE AND/OR FEDERAL APPROVALS

This project is in the process of submitting permit applications for various state and federal approvals:

- **NEPA Categorical Exclusion** – The project is being funded by the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA). It is anticipated that the project will qualify for a National Environmental Policy Act Categorical Exclusion. An application for Categorical Exclusion will be submitted to the FTA for review.
- **NRPA PBR** – The project qualifies for a Natural Resource Protection Act Permit By Rule under the Maine Department of Environmental Protection's (DEP) Chapter 305 regulations. Section 11 of this chapter applies to state transportation facilities. The Maine DEP has determined that due to the project's funding oversight from the Maine Department of Transportation, the project qualifies for a permit by rule under this section. An NRPA Permit By Rule will be submitted to Maine DEP.
- **SLOD Minor Revision** – A Site Location of Development Act permit exists for the project site. Due to the minimal impact of the proposed work, the DEP has indicated that only a SLOD Minor Revision is necessary. A SLOD minor revision will be submitted to Maine DEP.
- **Army Corps** – The project is required to submit a notification form for Category 1 of the Maine General Permit from the U.S. Army Corps of Engineers. The project qualifies for Category 1 as it is considered a "repair and maintenance" project and will comply with the Army Corps' pile driving criteria for Category 1 projects. A Category 1 notification will be submitted to the Army Corps of Engineers.

In addition, to the permits listed above, the marine repair work being completed in conjunction with the CBFT building work requires a marine construction permit from the Board of Harbor Commissioners.

All permits are in the process of being submitted. Copies of permits or notification forms will be provided to the City under separate cover as they become available.



**CASCO BAY FERRY
TERMINAL**

RENOVATIONS & ADDITIONS
IN COMMERCIAL DISTRICT
PORTLAND, MAINE

DATE: _____

PROJECT NO. _____

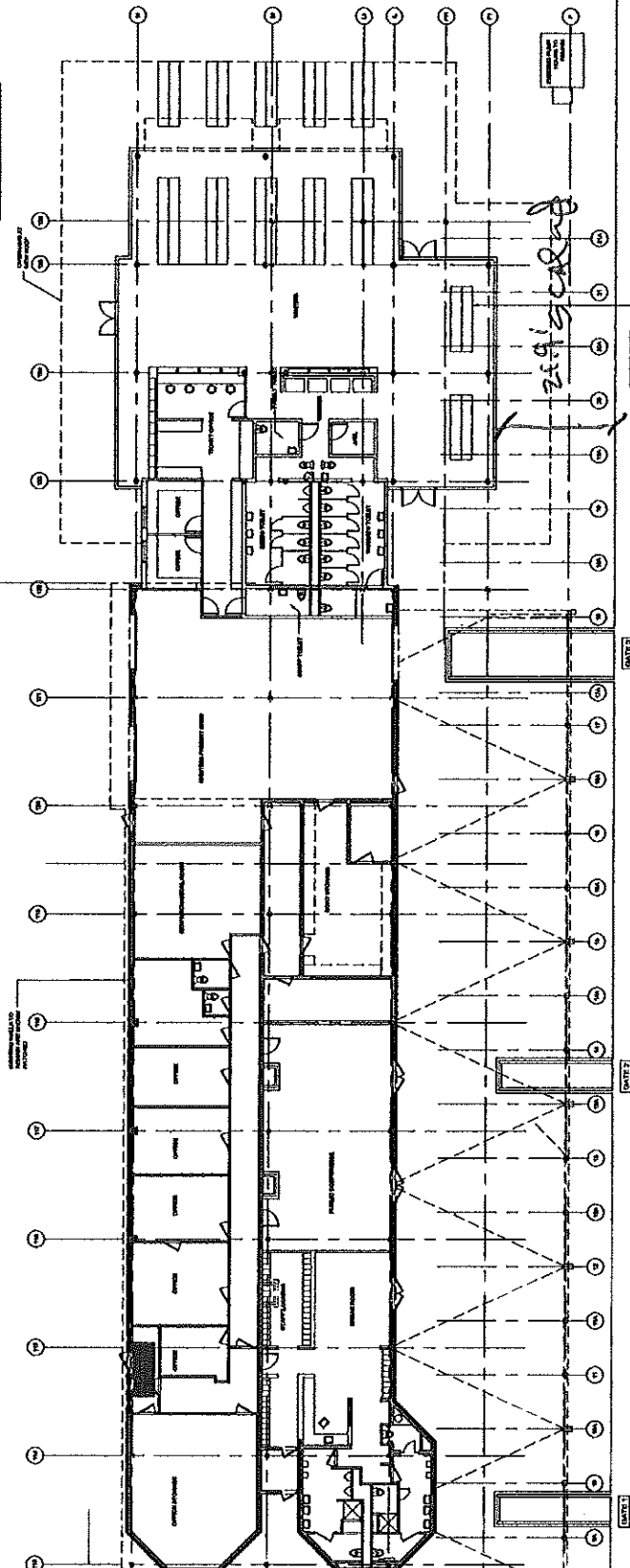
DATE: _____

PROJECT NO. _____

DATE: _____

CONTINUED FROM SHEET 1 OF 2
NEW CONSTRUCTION
TOTAL: 1,500 SF

RENOVATIONS & ADDITIONS
IN COMMERCIAL DISTRICT
PORTLAND, MAINE

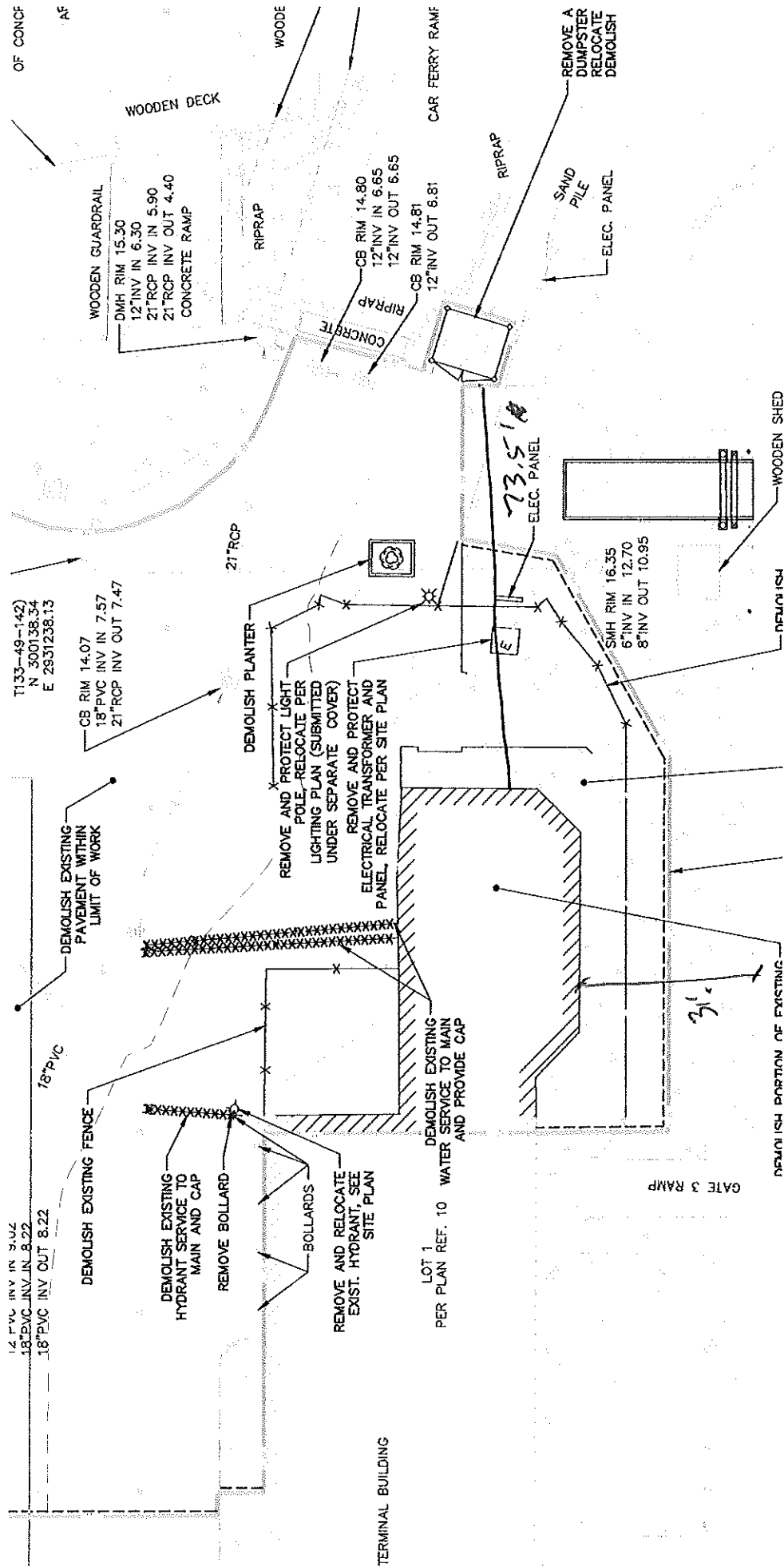


DATE: _____

DATE: _____

DATE: _____

DATE: _____



EXIST

GATE 2 RAMP

12\"/>

14\"/>

18\"/>

WOODEN DECK

WOODEN GUARDRAIL
DMH RIM 15.30
12\"/>

CONCRETE RAMP
21\"/>

CONCRETE

DEMOLISH PLANTER
21\"/>

REMOVE AND PROTECT LIGHT
POLE - RELOCATE PER
LIGHTING PLAN (SUBMITTED
UNDER SEPARATE COVER)

REMOVE AND PROTECT
ELECTRICAL TRANSFORMER AND
PANEL, RELOCATE PER SITE PLAN

DEMOLISH EXISTING
PAVEMENT WITHIN
LIMIT OF WORK

DEMOLISH EXISTING FENCE

DEMOLISH EXISTING
HYDRANT SERVICE TO
MAIN AND CAP

WOODDE

CB RIM 14.80
12\"/>

CS RIM 14.81
12\"/>

73510
ELEC. PANEL

SMH RIM 16.35
6\"/>

WOODEN SHED

DEMOLISH PORTION OF EXISTING

LOT 1
PER PLAN REF. 10 WATER
SERVICE TO MAIN
AND PROVIDE CAP

REMOVE BOLLARD

BOLLARDS
REMOVE AND RELOCATE
EXIST. HYDRANT, SEE
SITE PLAN

CAR FERRY RAMP

RIPRAP

SAND PILE

ELEC. PANEL

DEMOLISH EXISTING
PAVEMENT WITHIN
LIMIT OF WORK

DEMOLISH EXISTING
FENCE

DEMOLISH EXISTING
HYDRANT SERVICE TO
MAIN AND PROVIDE CAP

REMOVE BOLLARD

BOLLARDS
REMOVE AND RELOCATE
EXIST. HYDRANT, SEE
SITE PLAN

REMOVE A
DUMPSTER
RELOCATE
DEMOLISH

GATE 3 RAMP

31'

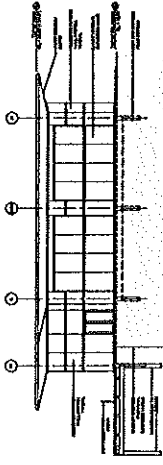
TERMINAL BUILDING

new plus
beyond

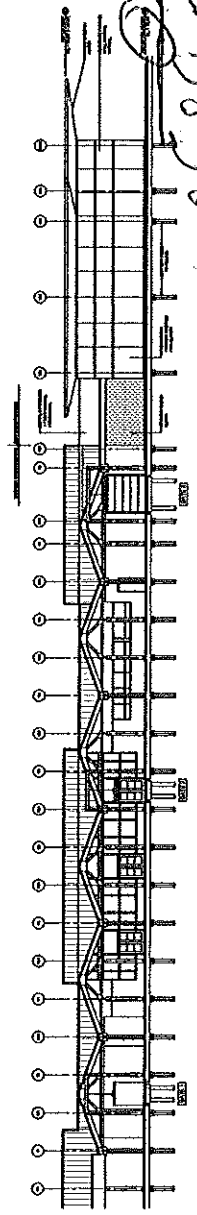
JASNO BAY FERRY
TERMINAL

BUILDING
ELEVATIONS

A201



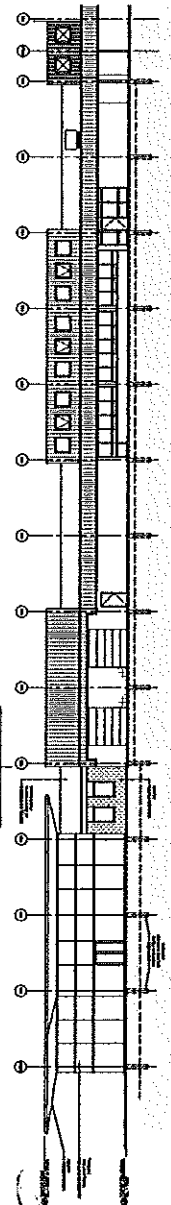
1 SOUTH BUILDING ELEVATION



2 WEST BUILDING ELEVATION

new concrete → existing

top of full floor
+12.33



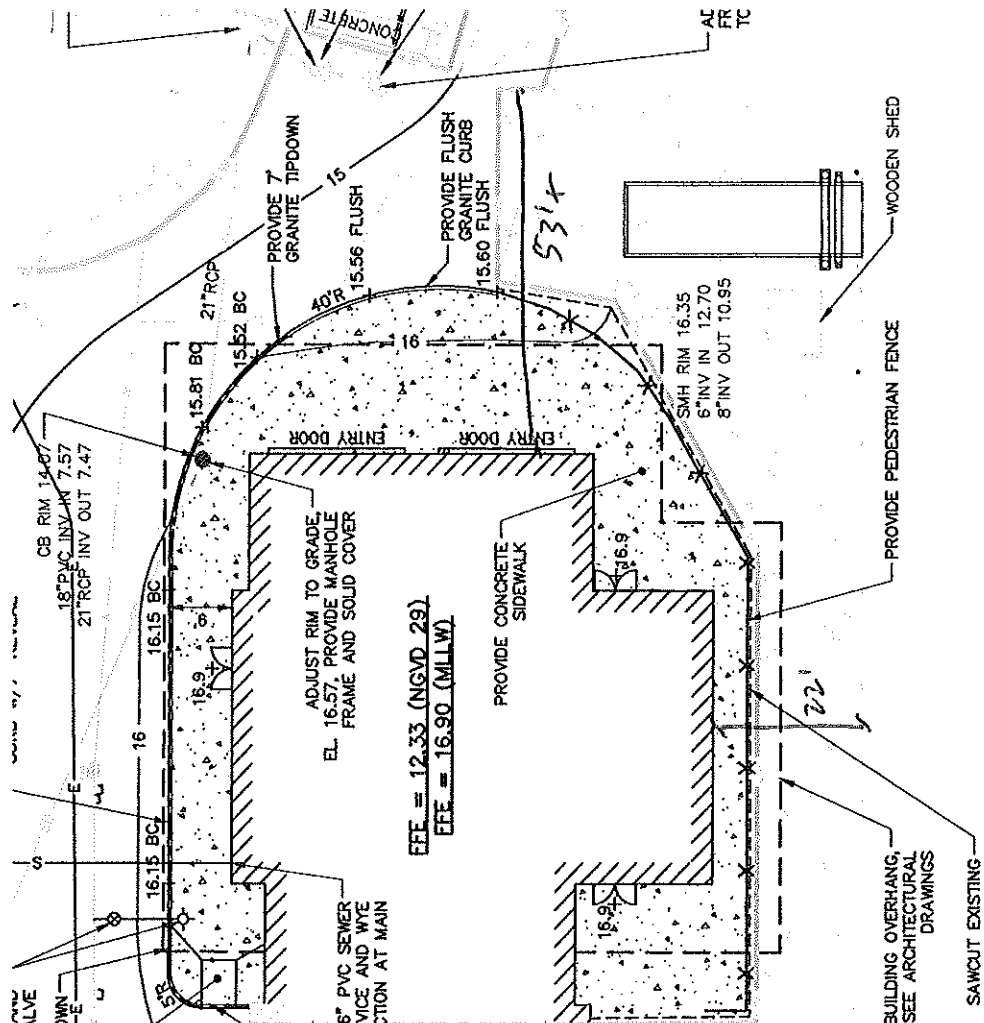
3 EAST BUILDING ELEVATION

top of full

EL 33.33

top of finished

floor = 12.33



PROVIDE GATE VALVE
 PROVIDE CURB STOP
 6" DI FIRE PROTECTION WATER SERVICE
 2" COPPER DOMESTIC WATER SERVICE
 PROVIDE NEW GATE VALVE
 PROVIDE 7" GRANITE TIPDOWN
 PROVIDE ADA ACCESSIBLE CURB RAMP
 BOLLARDS
 PROVIDE 6" PVC SEWER SERVICE AND WYE CONNECTION AT MAIN
 ADJUST RIM TO GRADE EL. 16.57 PROVIDE MANHOLE FRAME AND SOLID COVER
 EEE = 12.33 (NGVD 29)
 EEE = 16.90 (MLLW)
 PROVIDE CONCRETE SIDEWALK
 SAWCUT EXISTING
 BUILDING OVERHANG SEE ARCHITECTURAL DRAWINGS
 PROVIDE PEDESTRIAN FENCE
 WOODEN SHED
 PROVIDE 7" GRANITE TIPDOWN
 PROVIDE FLUSH GRANITE CURB
 PROVIDE FLUSH GRANITE CURB
 15.60 FLUSH
 15.56 FLUSH
 40'R
 15.52 BC
 21" RCP
 15.81 BC
 16.15 BC
 16.15 BC
 16.9
 16.9
 16.9
 22'
 22'
 16.35
 6" INV IN 12.70
 8" INV OUT 10.95
 5314
 AL FR TC
 PAY PHONE
 LOT 1
 PER PLAN REF. 10
 CONCRETE
 WALK REPAIR, SEE ARCHITECTURAL DRAWINGS
 NEED

Legend

- 1% annual chance (100-Year) Floodplain
- 1% annual chance (100-Year) Floodway
- 0.2% annual chance (500-Year) Floodplain



MAP SCALE 1" = 400'



NATIONAL FLOOD INSURANCE PROGRAM

FIRM FLOOD INSURANCE RATE MAP

CITY OF PORTLAND, MAINE
CUMBERLAND COUNTY

PANEL 14 OF 17
(SEE MAP INDEX FOR PANELS NOT PRINTED)

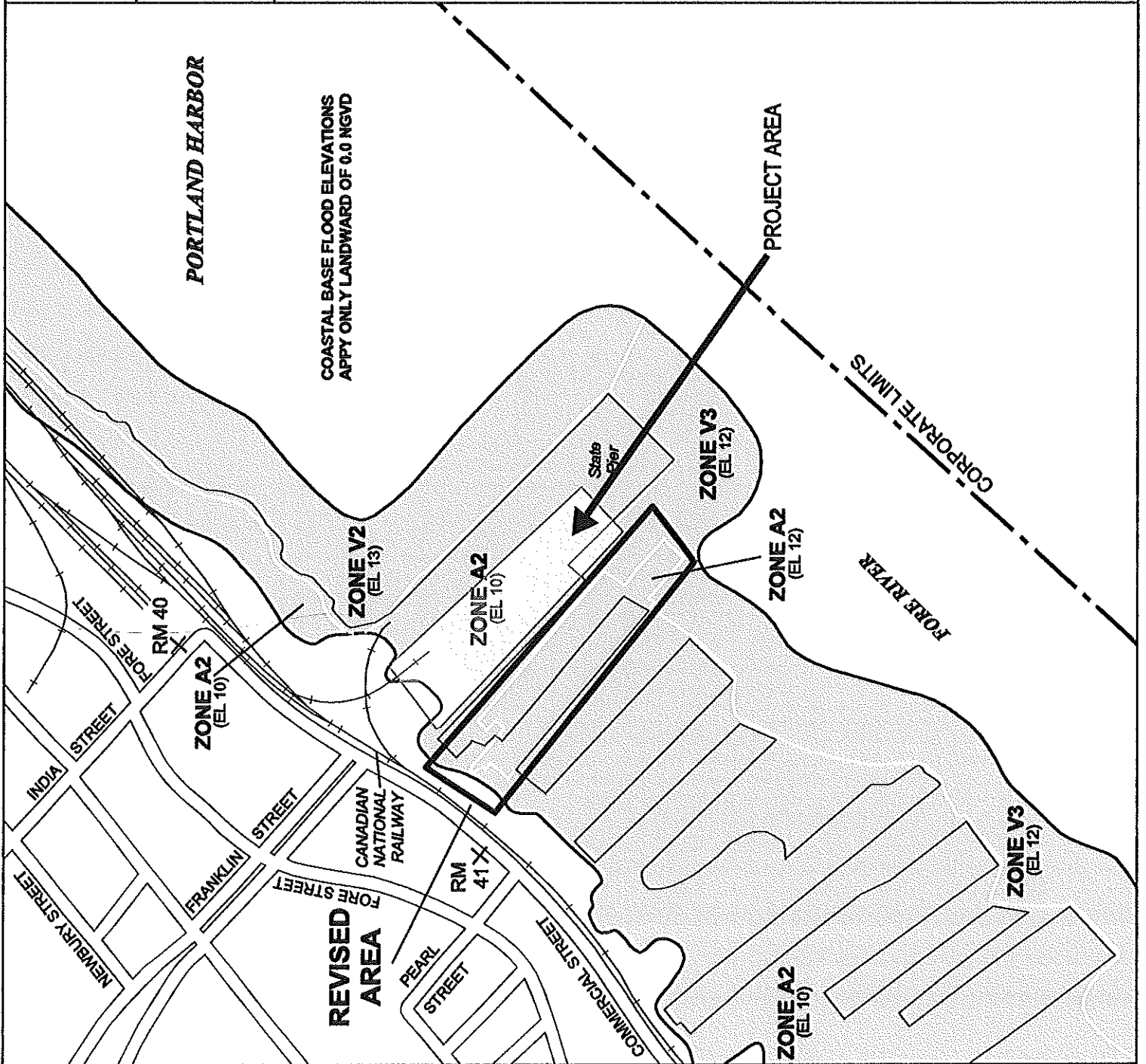
REVISED TO REFLECT LOMR
EFFECTIVE: SEPTEMBER 27, 2011

COMMUNITY-PANEL NUMBER
230051 0014 B

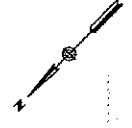
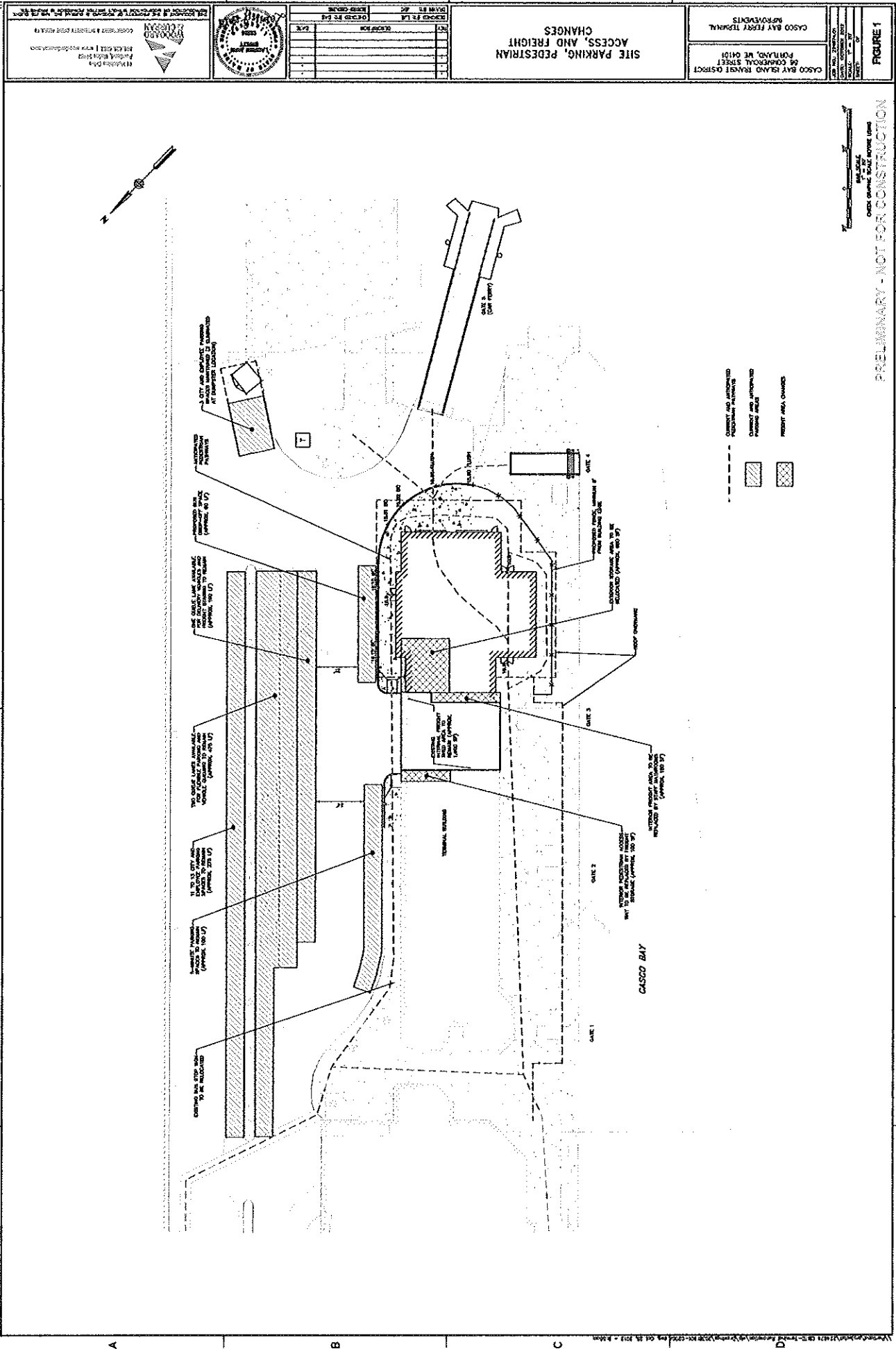
EFFECTIVE DATE:
JULY 17, 1986



Federal Emergency Management Agency



1 2 3 4 5 6

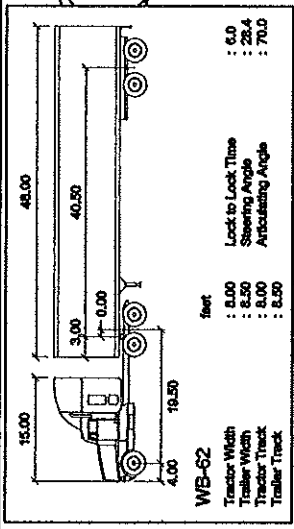
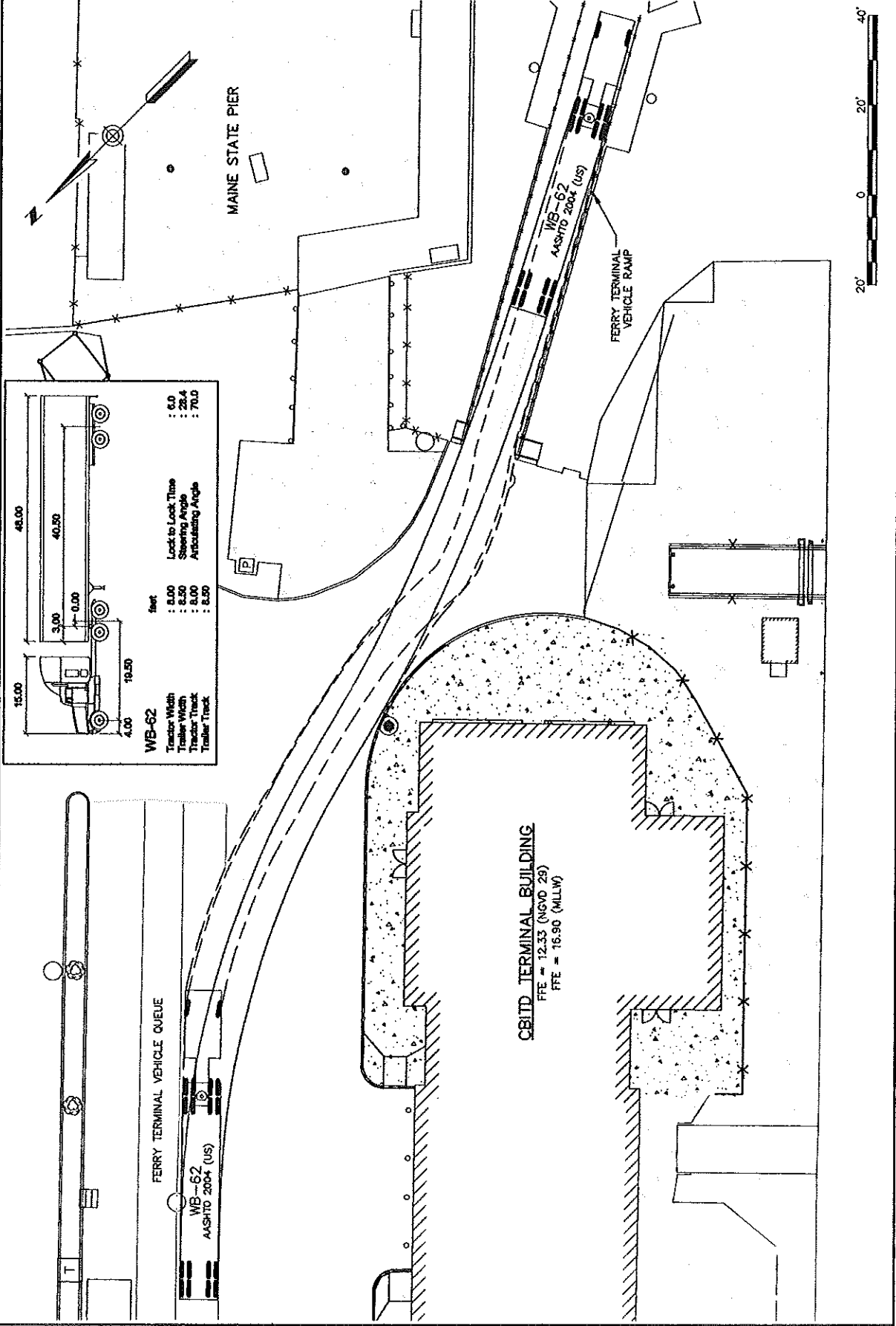


<p>PORTLAND, OREGON DEPARTMENT OF TRANSPORTATION 1000 NE Oregon Street, Portland, OR 97232</p>		<p>PROJECT NO. 2012-001</p> <p>DATE: 10/15/12</p> <p>SCALE: 1" = 100'</p>	<p>FIGURE 1</p>
		<p>CASCO BAY FREIGHT TERMINAL</p> <p>CASCO BAY FREIGHT TERMINAL</p> <p>BY CONTRACT STREET</p> <p>PORTLAND, OR 97201</p>	
<p>SITE PARKING, PEDESTRIAN ACCESS, AND FREIGHT CHANGES</p>		<p>DESIGNED BY: [Firm Name]</p> <p>DATE: 10/15/12</p> <p>PROJECT NO. 2012-001</p>	

PRELIMINARY - NOT FOR CONSTRUCTION

DATE: 10/15/12

- PROPOSED ACCESSIBLE PARKING SPACES
- PROPOSED AND EXISTING DRIVEWAYS
- EXISTING AREA CHANGES





Scott Simons Architects

76 York Street
Portland, Maine 04101
phone 207 772 4656
fax 207 828 4656
www.simonsarchitects.com

MEMORANDUM---Architecture and Streetscape Narrative

date: Friday, 09.05.12
project: CASCO BAY FERRY TERMINAL 2012-0270
to: City of Portland, Planning Division

from: Austin K Smith Scott Simons Architects
cc: Hank Berg Casco Bay Island Transit District
Nick Mavodones Casco Bay Island Transit District
David Sensus Woodard & Curran
Lauren Swett Woodard & Curran

subject: Architecture & Streetscape Narrative: Eastern Waterfront
City of Portland Level II Site Plan Application

(1) STANDARD. Eastern Waterfront design standards: All major and minor development reviewed under the provisions of the eastern waterfront zones shall be designed to support the development of this urban neighborhood as a dense, mixed-use, pedestrian friendly neighborhood.

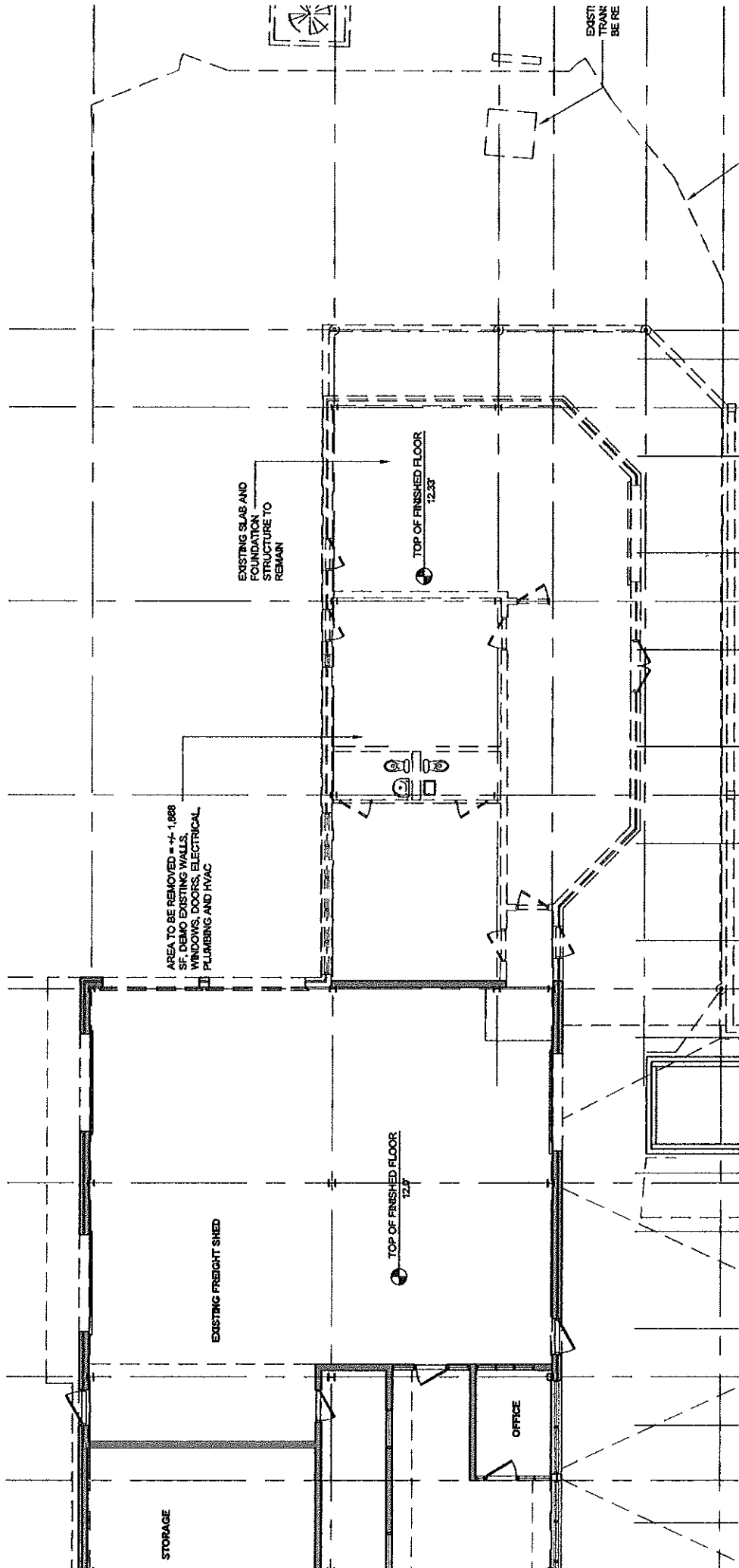
The work within this proposal includes the relocation and expansion of the ticketing and waiting area for the Casco Bay Ferry Terminal. In addition to the expansion, the waiting room and ticketing areas are being relocated to the most heavily used gates, Four & Five. The overall increase in area is 3,006 square feet. This expansion accommodates a doubling of ridership from the original facility built in 1988.

The new proposed waiting area will have a significant presence due to tall glass curtainwalls on three elevations. The ceiling and roof structure cantilevers over the curtainwall to shade the glass and to cover waiting pedestrians. The use of the curtainwall system provides transparency to the ferries, gates and waterfront in general. For the first time visitor the high ceiling and large expanse of glass gives a visual clue to the major civic space.

In the design of the terminal expansion all types of transportation were taken into account. The facility supports Metro Bus arrivals and departures, school bus boarding for island students, bicycle users, and primarily pedestrians filtering through the terminal to the streets of Portland.

project: Casco Bay Ferry Terminal-2012-0270
Outstanding Documents Status.doc

date: 09/05/12



STORAGE

EXISTING FREIGHT SHED

OFFICE

AREA TO BE REMOVED - +/- 1,888 SF. DEMO EXISTING WALLS, WINDOWS, DOORS, ELECTRICAL, PLUMBING AND HVAC

EXISTING SLAB AND FOUNDATION STRUCTURE TO REMAIN

TOP OF FINISHED FLOOR
12.37

TOP OF FINISHED FLOOR
12.33

EXIST. TRUNK
BE RE

Marge Schmuckal - Casco Bay Ferry Terminal documents

From: Austin Smith <austin@simonsarchitects.com>
To: Marge Schmuckal <MES@portlandmaine.gov>
Date: 1/25/2013 9:49 AM
Subject: Casco Bay Ferry Terminal documents
CC: "Peter S. Plumb" <PSP@mpmlaw.com>, Henry Berg <HankB@cascobaylines.com>, ...
Attachments: 2013.01.25 CBFT-F1 & S1.pdf

Marge:

Thanks very much for your time yesterday.

I've attached electronic copies of the documents we discussed. (Drawings F1 & S1, dated 11.19.86)
We have also added a layer showing our proposed construction in red, as you requested.

In addition, we will put full size, hard copies in today's mail.

Have a nice weekend, Austin Smith

Austin K. Smith, AIA RLA LEED AP

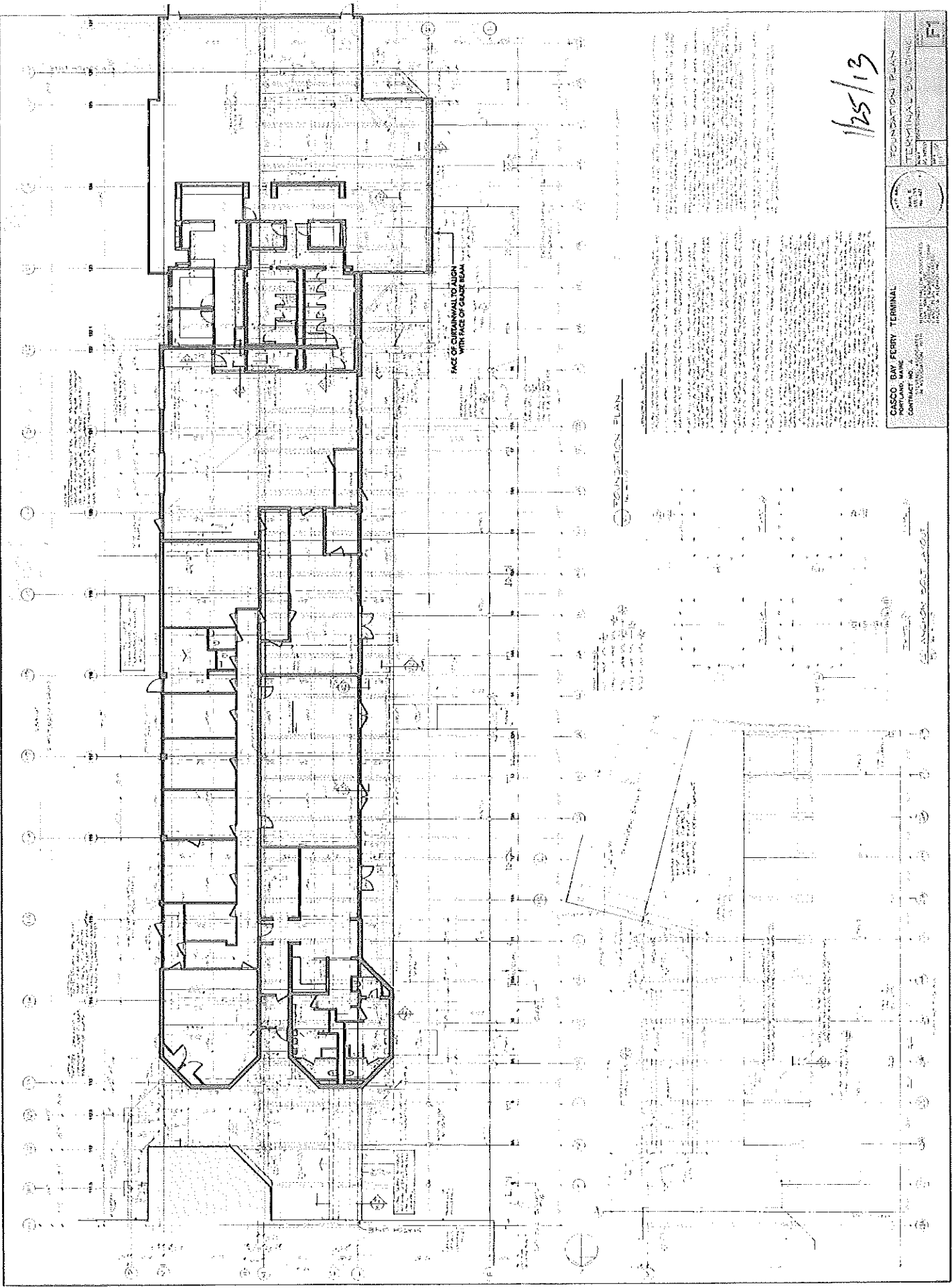
Maine Licensed Architect
phone : 207 772 4656 x103

Scott Simons Architects 75 York Street Portland, Maine 04101 simonsarchitects.com

1/25/13

CASCO BAY FERRY TERMINAL
PORTLAND, MAINE
CONTRACT NO. 4
IN ACCORDANCE WITH
MEDES 105-106

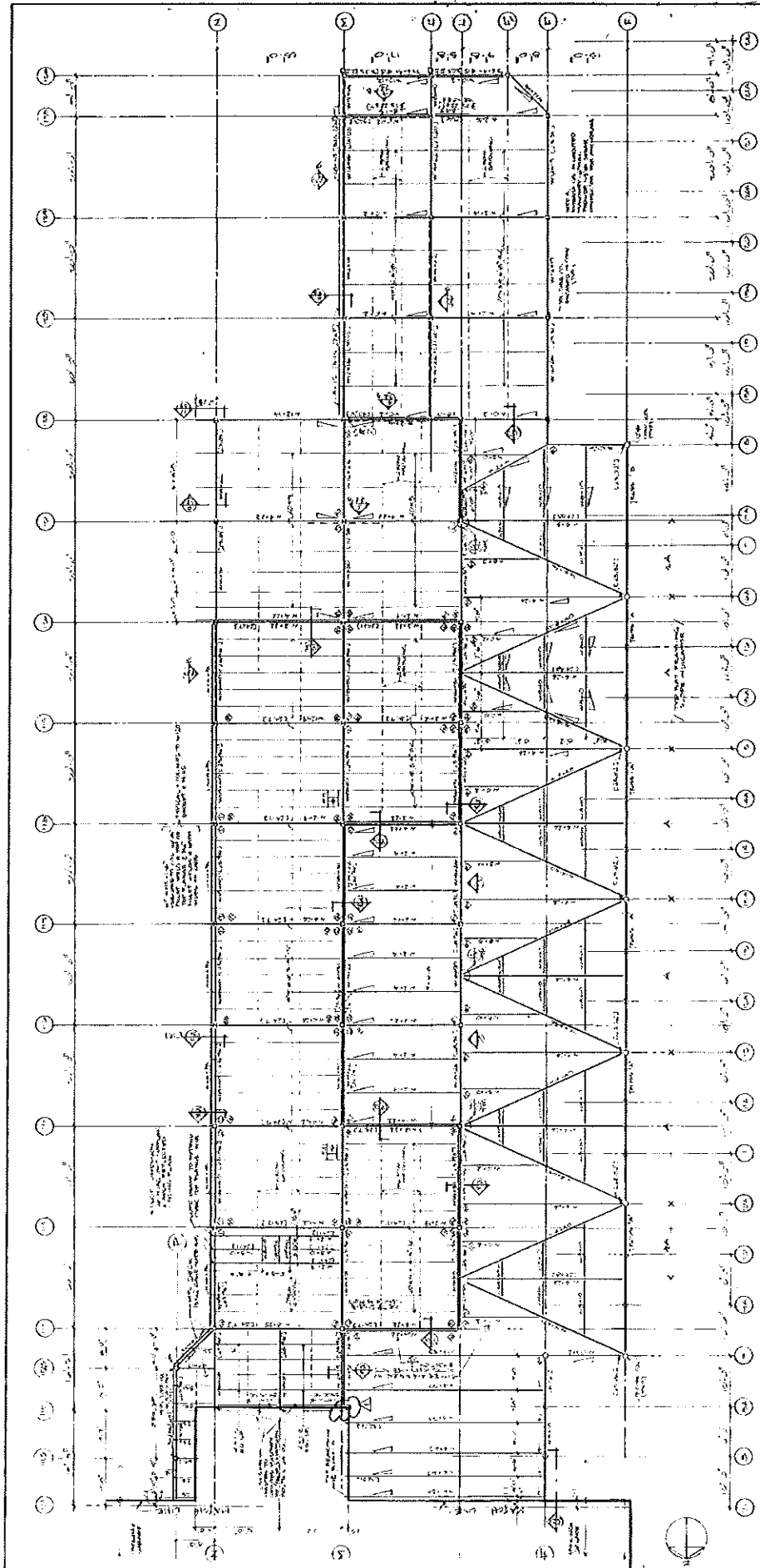
FOUNDATION PLAN
TERMINAL BUILDING



FOUNDATION PLAN

DATE: 1/25/13
DRAWN BY: [Signature]
CHECKED BY: [Signature]

1/25/13



DECK TYPE LEGEND

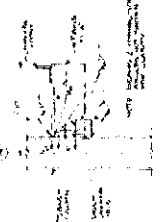
- AREA UNDER DECK
- AREA FOR FUTURE AND FLOOR
- EXISTING DECK
- EXISTING DECK

LEGEND

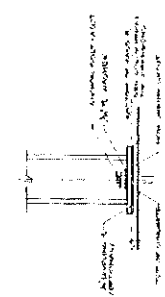
- RAILWAY
- DRAINAGE
- DRAINAGE

1. ALL DIMENSIONS ARE IN METERS UNLESS OTHERWISE SPECIFIED.
 2. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE SPECIFIED.
 3. ALL DIMENSIONS ARE TO CENTER UNLESS OTHERWISE SPECIFIED.
 4. ALL DIMENSIONS ARE TO EDGE UNLESS OTHERWISE SPECIFIED.
 5. ALL DIMENSIONS ARE TO CENTER UNLESS OTHERWISE SPECIFIED.
 6. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE SPECIFIED.
 7. ALL DIMENSIONS ARE TO CENTER UNLESS OTHERWISE SPECIFIED.
 8. ALL DIMENSIONS ARE TO EDGE UNLESS OTHERWISE SPECIFIED.
 9. ALL DIMENSIONS ARE TO CENTER UNLESS OTHERWISE SPECIFIED.
 10. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE SPECIFIED.

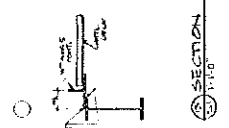
SOFT FRAMING PLAN



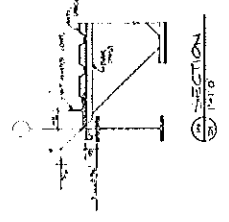
SECTION 1



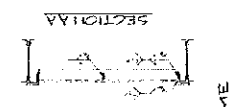
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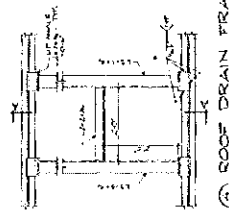
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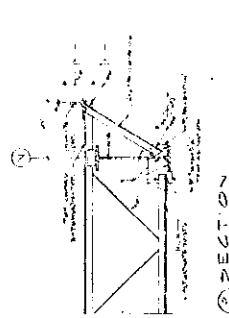
SECTION 4



SECTION 5



SECTION 6



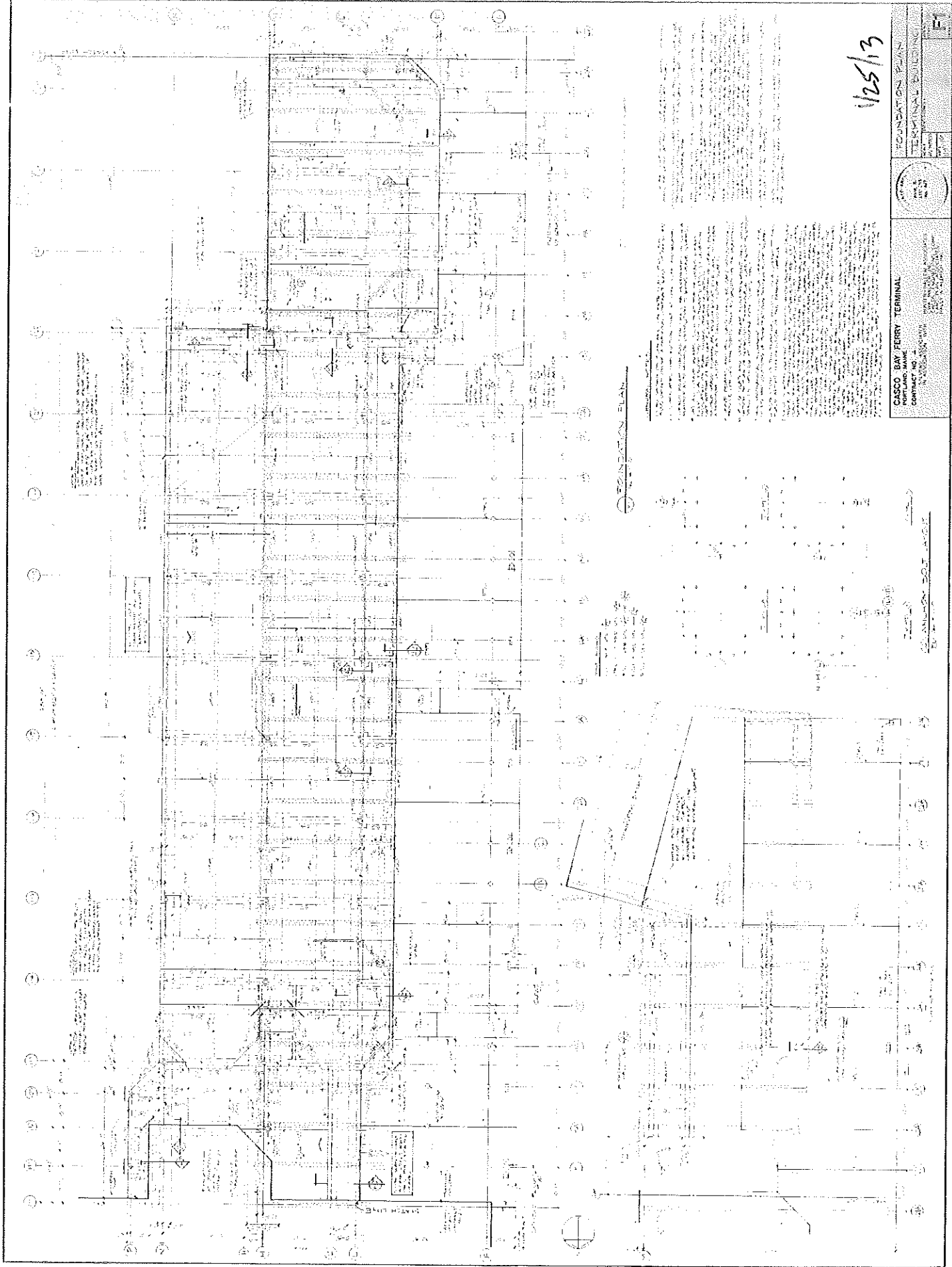
SECTION 7

125/13

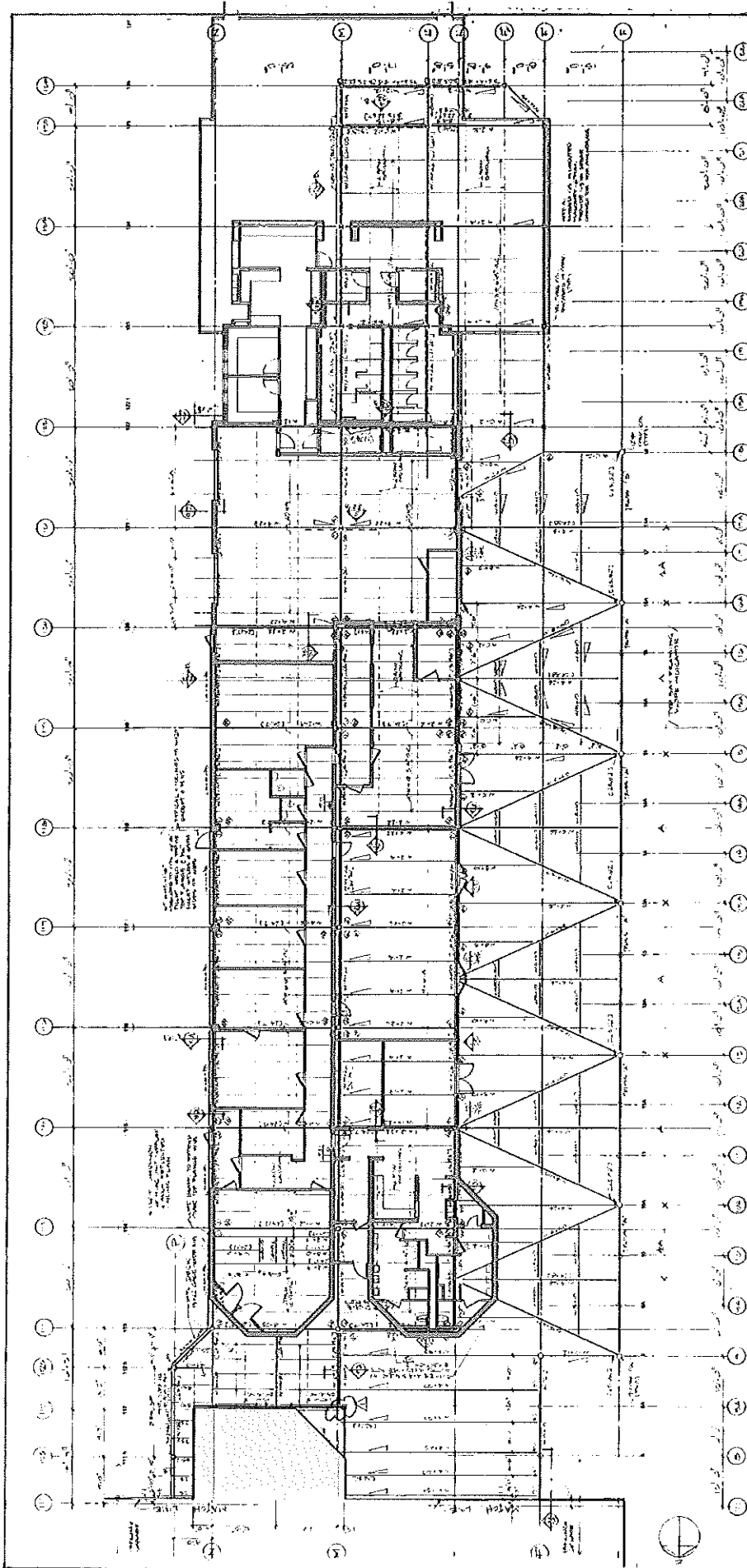
CASCO BAY FERRY TERMINAL
 CONTRACT NO. 1
 1955

FOUNDATION
 TERMINAL BUILDING

1



1/25/13



DECK TYPE SEGMENT

- DECK TYPE SEGMENT WITH 4" EPS INSULATION
- AREA FOR FUTURE 2ND FLOOR
- 2" MINIMUM THICKNESS

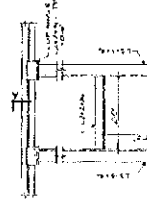
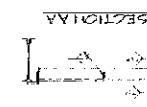
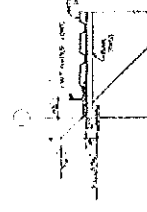
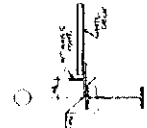
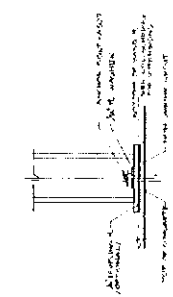
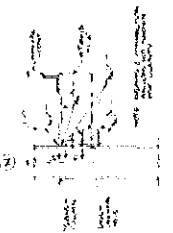
LEGEND

- RADIATION
- DRAINAGE DIRECTION

GENERAL NOTES:

1. REFER TO THE OTHER DRAWINGS FOR ALL DIMENSIONS AND FINISHES.
2. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE AIAA, ASPEC, AND IBC.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.
4. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL EXISTING UTILITIES AND STRUCTURES.
5. THE CONTRACTOR SHALL PROTECT ALL EXISTING STRUCTURES AND UTILITIES.
6. THE CONTRACTOR SHALL MAINTAIN PROPER DRAINAGE AND WATER MANAGEMENT.
7. THE CONTRACTOR SHALL MAINTAIN PROPER VENTILATION AND AIR FLOW.
8. THE CONTRACTOR SHALL MAINTAIN PROPER SOUND ATTENUATION.
9. THE CONTRACTOR SHALL MAINTAIN PROPER LIGHTING AND ELECTRICAL SYSTEMS.
10. THE CONTRACTOR SHALL MAINTAIN PROPER HEATING AND COOLING SYSTEMS.
11. THE CONTRACTOR SHALL MAINTAIN PROPER FIRE PROTECTION SYSTEMS.
12. THE CONTRACTOR SHALL MAINTAIN PROPER SAFETY AND SECURITY SYSTEMS.
13. THE CONTRACTOR SHALL MAINTAIN PROPER RECORDS AND DOCUMENTATION.
14. THE CONTRACTOR SHALL MAINTAIN PROPER COMMUNICATION AND COORDINATION.
15. THE CONTRACTOR SHALL MAINTAIN PROPER SCHEDULING AND TIMELINE.
16. THE CONTRACTOR SHALL MAINTAIN PROPER BUDGET AND COST CONTROL.

ROOF FRAMING PLAN



SECTION A-A

SECTION B-B

SECTION C-C

ROOF DRAIN FRAME

SECTION D-D

Marge Schmuckal - Casco Bay Ferry Terminal

From: Barbara Barhydt
To: Bourke, Jeanie; Munson, Tammy; Schmuckal, Marge
Date: 2/21/2013 11:07 AM
Subject: Casco Bay Ferry Terminal
CC: Donaldson, Helen; Levine, Jeff

Good morning:

This email serves to confirm that the building permits can be issued for the proposed marine repairs and replacements associated with the terminal expansion at 56 Commercial Street, prior to site plan approval for the building expansion. These marine repairs and replacements were not deemed subject to site plan review. They are limited to those outlined in your Level II site plan application, including:

1. the replacement of fender piles and portions of the whale and chock along Gates 1-4
2. repairs to the transfer bridge for the car ferry
3. replacement of breasting and alignment dolphins for the car ferry slip

Thank you.

Barbara

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