

327-B-3

2012-387

1884 Forest Ave

Convenience Store and gas Station

John Chau

add to Spreadsheet
~~project on~~
~~CD Drive~~

PETRUCELLI, MARTIN & HADDOW, LLP

Attorneys at Law
50 Monument Square
Post Office Box 17555
Portland, Maine 04112-8555

JISEL E. LOPEZ
jlopez@pmhlegal.com

Via Hand Delivery

January 23, 2012

Barbara Barhydt, Development Review Services Mgr
Planning & Urban Development Department
389 Congress Street, 4th Floor
Portland, Maine 04101-3509

RE: Cumberland Farms, Inc. – 801 Washington Avenue

Dear Barbara:

Enclosed please find the following documents and things for filing with your office on behalf of Cumberland Farms, Inc.:

1. Level II Site Plan Application with attachments;
2. Conditional Use Application with attachments;
3. City of Portland Wastewater Capacity Application;
4. Traffic Impact and Access Study;
5. Full-size copy of plans;
6. 11 x 17 copy of plans;
7. CD with copies of Items 1 through 7; and
7. Cumberland Farms' Check No. 262166 in the amount of \$700.

Attachment A

PETRUCCELLI, MARTIN & HADDOW, LLP

Attorneys at Law

50 Monument Square
Post Office Box 17555
Portland, Maine 04112-8555

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January 23, 2012

Barbara Barhydt, Development Review Services Mgr
Planning & Urban Development Department
389 Congress Street, 4th Floor
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RE: Cumberland Farms, Inc. – 801 Washington Avenue

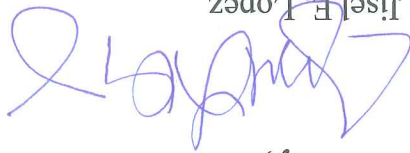
Dear Barbara:

Enclosed please find the following documents and things for filing with your office on behalf of Cumberland Farms, Inc.:

1. Level II Site Plan Application with attachments;
2. Conditional Use Application with attachments;
3. City of Portland Wastewater Capacity Application;
4. Traffic Impact and Access Study;
5. Full-size copy of plans;
6. 11 x 17 copy of plans;
7. CD with copies of Items 1 through 7; and
7. Cumberland Farms' Check No. 262166 in the amount of \$700.

We realize that the filing fee enclosed is in excess of the required filing fee for the enclosed applications, but ask that any excess payment be applied to future fees that will be incurred in this application process. Thank you for your assistance, and please do not hesitate to contact me with questions or concerns.

Sincerely,


Jisel E. Lopez

JEL/d

Enclosures

cc: Kathleen A. Sousa (w/out enclosures)



**Conditional Use Application
Development Review Application
Portland, Maine**

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Conditional Use Review. General information pertaining to the thresholds of review for site plans, public noticing procedures and the fee structure is contained in site plan applications and within the Developer's Packet.

(Please submit the Conditional Use Application in addition to an applicable site plan application.)

PROJECT NAME: Cumberland Farms

CHART/BLOCK/LOT: Map 163, Block B, Lots 3, 4, 5, 14, 15, 17

RIGHT, TITLE OR INTEREST: (Please identify the status of the applicant's right, title, or interest in the subject property.)

Applicant is the owner in fee simple of the property by virtue of deed dated 9/26/83 from Gulf Oil Corporation to VSH Realty, Inc., and recorded in the Cumberland County Registry of Deeds at Book 6310, Page 145. Cumberland Farms, Inc., is the successor in interest by merger with VSH Realty, Inc.

(Please provide documentary evidence, attached to this application, of the applicant's right, title, or interest in the property. For Example – a deed, option or contract to purchase or least the subject property.)

VICINITY MAP: (Please attach a map showing the subject parcel and abutting parcels, labeled as to ownership and/or current use.)

EXISTING USE: Describe the existing use of the subject property.

Retail establishment with gasoline sales (major/minor auto service station)

TYPE OF CONDITIONAL USE PROPOSED:

Major/Minor auto service station

SITE PLAN: Provide a site plan of the property, showing existing and proposed improvements, which meets the submission requirements of the applicable level of site plan review.

CONDITIONAL USE AUTHORIZED BY: SECTION 14- 183

Address any specific conditional use standards for the specific use contained in the zoning code in the written submission.

STANDARDS – CRITERIA FOR CONDITIONAL USE APPEAL – Section 14-474

Address the following criteria in your written application and any applicable conditional use standards contained in the zoning code for the specific use.

Upon showing that a proposed use is a conditional use under this article, a conditional use permit shall be granted unless the Board determines that:

- a. There are unique or distinctive characteristics or effects associated with the proposed conditional use;
- b. There will be an adverse impact upon the health, safety, or welfare of the public or the surrounding areas;
- c. Such impact differs substantially from the impact which would normally occur from such a use in that zone.

CONTACT INFORMATION:

<p>Applicant’s Contact for electronic plans</p> <p>Name: James B. Haddow, Esq. e-mail: jhaddow@pmhlegal.com work #:207-775-0200</p>
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<p>Applicant – must be owner, Lessee or Buyer</p> <p>Name: Cumberland Farms, Inc. Business Name, if applicable: Address: 100 Crossing Blvd. City/State : Framingham, MA Zip Code: 01702</p>	<p>Applicant Contact Information</p> <p>Work # 508-270-1466 Home# Cell # Fax# 781-459-0569 e-mail: ksousa@cumberlandgulf.com</p>
<p>Owner – (if different from Applicant)</p> <p>Name: Address: City/State : Zip Code:</p>	<p>Owner Contact Information</p> <p>Work # Home# Cell # Fax# e-mail:</p>
<p>Billing Information</p> <p>Name: Cumberland Farms, Inc. Address: 100 Crossing Blvd. City/State : Framingham, MA Zip Code: 01702</p>	<p>Billing Information</p> <p>Work # 508-270-1466 Cell # Fax# 781-459-0569 e-mail: ksousa@cumberlandgulf.com</p>

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521) and the Conditional Use Standards (Section 14-474). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Conditional Use review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: Cumberland Farms, Inc. By <i>Kathleen A. Sousa</i>	Date: <i>1/20/12</i>
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Kathleen A. Sousa, Land Planning Manager

The Portland Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division
Fourth Floor, City Hall
389 Congress Street
(207) 874-8721 or 874-8719

Office Hours
Monday thru Friday
8:00 a.m. – 4:30 p.m.

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

<p>Conditional Use</p> <p><input checked="" type="checkbox"/> Conditional Use \$100</p> <hr/> <p>The City invoices separately for the following:</p> <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) <p>Third party review is assessed separately.</p>	
<p>Site Plan Application</p> <p>Please submit a separate application for the applicable site plan review. Fees and charges are listed within the site plan application</p>	<p>Site Plan Application submitted and Fees Paid (office use) _____</p>

APPLICATION SUBMISSION

All site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

The application for a conditional use review must contain the following items:

- a. Conditional Use Application form that is completed and signed.
- b. Cover letter stating the nature of the project.
- c. Evidence of right, title and interest.
- d. Written Submittals that address the conditional use standards of Section 14-474 and any applicable standards of review contained in the zoning code for the specific use.
- e. One set of the paper plans and maps based upon the boundary survey at a scale not less than one (1) inch to fifty (50) feet and containing the information required for the applicable level of site plan review. The plan requirements and submission requirements are listed in the Site Plan Ordinance (Section 14 -527) of the Land Use Code. Refer to the application checklist for a detailed list of submittal requirements.
- f. One set of the plans at 11X17.
- g. The conditional use application fee and all other applicable fees as determined on the site plan application.

**Conditional Use Application Narrative
Cumberland Farms, Inc.
801 Washington Avenue, Portland, Maine**

Cumberland Farms, Inc. (“Cumberland”), is the current owner in fee simple of the above-referenced property (“the Property”) used as a retail establishment combined with gasoline sales. In 2010, the subject property was rezoned from Neighborhood Business (B-1) and Residential (R-3) to Community Business (B-2). While the purpose of the rezoning was to allow for the eventual expansion of the retail building, the rezoning also resulted in bringing the present use into conformity with the Portland Zoning Ordinance.¹ Major and minor auto service stations are conditional uses in the B-2 Community Business Zone pursuant to Section 14-183 of the Portland Zoning Ordinance.

Among the stated purposes of the B-2 Community Business Zone is “[t]o provide appropriate locations for the development and operation of community centers offering a mixture of commercial uses and services serving the adjoining neighborhoods and the larger community.” Cumberland has been operating a retail store and gasoline station at the above-referenced property for a number of years without adverse impacts to the area or neighboring properties. Cumberland now proposes to construct a 1,474 s.f. addition to its existing 2,400 s.f. convenience store for a total retail gross floor area of 3,874 s.f. The proposed additions and changes are intended to incorporate Cumberland’s new corporate branding, enhance the retail experience for Cumberland Farms customers, and address

¹ Section 14-47 of the Portland Zoning Ordinance provides that “retail establishments combined with gasoline, diesel or propane fuel sales shall be considered a single use for zoning purposes and shall constitute a major or minor auto service establishment.” Major or minor auto service stations are not permitted in the B-1 and R-3 districts but are authorized conditional uses in the B-2 Community Business Zone.

outstanding safety concerns. The exterior of the building will be renovated to reflect the new corporate colors and design themes, as shown on the business elevations provided with this application. Additional parking, including added handicapped spaces and bicycle racks, will be provided. Existing signage will be replaced with smaller signs, and LED lights where applicable, thereby reducing the potential impact on neighboring uses. Landscaping will meet all required buffers under the Zoning Ordinance and will in fact be enhanced to improve the appearance of the Property. Cumberland's proposed site improvements will enhance the physical appearance of the Property and allow it to continue to provide quality goods and services to the area. While auto service stations are an intensive use, the gasoline sales portion of the business will not be materially expanded and will therefore not cause any increased adverse impact upon the health, safety or welfare of the public or surrounding area.

In particular, the proposed store renovations will include:

- Renovation of existing dumpster on new concrete pad
- New concrete walk at the front and side of the store, including the proposed addition
- Installation of protective bollards (with sleeves) at parking spaces along the front and side of the store
- Relocation and expansion of parking spaces
- Addition of bicycle racks
- Landscaping to be enhanced (see Landscaping Plan)

- Addition of area lamps for enhanced safety of parking and dumpster areas
- Replacement of four existing 4' x 10' gas islands, concrete pad, and four fuel dispensers with new 4' x 10' islands, concrete pads, and four fuel dispensers in the same location as existing ("like for like" exchange). One dispenser will offer diesel fuel.
- Replacement of existing 24 x 96' gas canopy with new white ACM panel fascia with green stripe (same size and location). Note: *The existing canopy requires replacement due to safety issues.*
- Two existing Gulf Self Serve signs on the gas canopy, each of 16 s.f. to be removed and replaced with two Cumberland Farms ID signs each 12.38 s.f. with push-thru illuminated acrylic letters, resulting in a 7.24 s.f. reduction in canopy signage
- Reface existing store fascia with white ACM panel with green metal wall panel band incorporating the new addition (See Building Elevations).
- Two new wall signs on the store each of 15 s.f. The white background is non-illuminated with push-thru illuminated acrylic letters
- Re-image existing freestanding sign of 69 s.f., removing the Gulf disc and installing a new ID sign of 15 s.f. and a new LED price sign (regular and diesel) of 33 s.f. for a total of 48 s.f., an overall reduction of 21 s.f. of signage
- Fuel dispenser decals, each 1 s.f., to incorporate Cumberland's new logo and colors

- Replace 3-8,000 gal. single-wall fiberglass underground storage tanks with 2-20,000 gal. double-wall fiberglass underground dual compartment storage tanks providing for diesel fuel sales, upgrading the underground tanks and piping and reducing the number of fuel deliveries. One of the compartments will store 8,000 gallons of diesel fuel.

PROJECT NAME: Cumberland FarmsPROPOSED DEVELOPMENT ADDRESS: 801 Washington AvenuePROJECT DESCRIPTION: See attachedCHART/BLOCK/LOT: Map 163, Block B
Lots 3, 4, 5, 14, 15, 17PRELIMINARY PLAN _____ (date)
FINAL PLAN _____ (date)

CONTACT INFORMATION:

Applicant – must be owner, Lessee or Buyer Name: Cumberland Farms, Inc. Business Name, if applicable: Address: 100 Crossing Boulevard City/State : Framingham, MA Zip Code: 01702	Applicant Contact Information Work # 508-270-1466 Home# Cell # Fax# 781-459-0569 e-mail: ksousa@cumberlandgulf.com
Owner – (if different from Applicant) Name: n/a Address: City/State : Zip Code:	Owner Contact Information Work # Home# Cell # Fax# e-mail: .
Agent/ Representative <i>Jisel Lopez</i> Name James Haddow, Esq, Petrucelli Martin & Haddow LLP Address: 50 Monument Square City/State : Portland, ME Zip Code: 04112-8555	Agent/Representative Contact information Work # 207-775-0200 Cell # Fax# 207-775-2360 e-mail: jhaddow&pmhlegal.com <i>jlopez@pmhlegal.com</i>
Billing Information Name: Cumberland Farms, Inc. Address: 100 Crossing Boulevard City/State : Framingham, MA Zip Code: 01702	Billing Information Work # 508-270-1466 Cell # Fax# 781-459-0569 e-mail: ksousa@cumberlandgulf.com

Engineer Name: MHF Design Consultants Inc. Address: 44 Stiles Rd., Suite 1 City/State : Salem, NH Zip Code: 03079	Engineer Contact Information Work # 603-893-0720 Cell # Fax# 603-893-0733 e-mail:
Surveyor Name: Maine Survey Consultants Address: P.O. Box 485 City/State : Portland, ME Zip Code:	Surveyor Contact Information Work # 207-583-6159 Cell # Fax# 207-583-2334 e-mail:
Architect Name: Ahronian & Assoc. Address: 310 George Washington Hwy City/State : Smithfield, RI Zip Code: 02828	Architect Contact Information Work # 401-232-5010 Cell # Fax# 401-232-5080 e-mail:
Attorney Name: James Haddow, Esq. Address: 50 Monument Sq. City/State : Portland, ME Zip Code: 04112	Attorney Contact Information Work # 207-775-0200 Cell # Fax# 207-775-2360 e-mail: jhaddow@pmhlegal.com

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

Level II Development (check applicable reviews) <input checked="" type="checkbox"/> Less than 10,000 sq. ft. (\$400.00) <input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)	Fees Paid (office use) _____ _____	Other Reviews (check applicable reviews) <input type="checkbox"/> Traffic Movement (\$1,000) <input type="checkbox"/> Stormwater Quality (\$250) <input type="checkbox"/> Section 14-403 Review (\$400 + \$25/lot) <input type="checkbox"/> # of Lots ____ x \$25/lot = _____ <input type="checkbox"/> Other _____ <input type="checkbox"/> Change of Use <input type="checkbox"/> Flood Plain <input type="checkbox"/> Shoreland <input type="checkbox"/> Design Review <input type="checkbox"/> Housing Replacement <input type="checkbox"/> Historic Preservation	Fees Paid (office use) _____ _____ _____
The City invoices separately for the following: <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) Third party review is assessed separately.			
Plan Amendments (check applicable reviews) <input type="checkbox"/> Planning Staff Review (\$250) <input type="checkbox"/> Planning Board Review (\$500)	Fees Paid (office use) _____ _____		

PROJECT DATA

The following information is required where applicable, in order complete the application

Total Site Area	31,972 sq. ft.
Proposed Total Disturbed Area of the Site	sq. ft.
(If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)	
<u>IMPERVIOUS SURFACE AREA</u>	
•Proposed Total Paved Area	25,364 sq. ft.
•Existing Total Impervious Area	22,056 sq. ft.
•Proposed Total Impervious Area	25,364 sq. ft.
•Proposed Total Impervious Area	
•Proposed Impervious Net Change	3308 sq. ft.
<u>BUILDING AREA</u>	
•Proposed Building Footprint	3,874 sq. ft.
•Proposed Building Footprint Net change	1474 sq. ft.
•Existing Total Building Floor Area	2,400 sq. ft.
•Proposed Total Building Floor Area	3,874 sq. ft.
•Proposed Building Floor Area Net Change	1474 sq. ft.
•New Building	(addition) (yes or no)
<u>ZONING</u>	
•Existing	B-2
•Proposed, if applicable	n/a
<u>LAND USE</u>	
•Existing	Retail store with gasoline sales
•Proposed	same
<u>RESIDENTIAL, IF APPLICABLE</u>	
•Proposed Number of Affordable Housing Units	n/a
•Proposed Number of Residential Units to be Demolished	
•Existing Number of Residential Units	
•Proposed Number of Residential Units	
•Subdivision, Proposed Number of Lots	
<u>PARKING SPACES</u>	
•Existing Number of Parking Spaces	22 (including 8 at pumps)
•Proposed Number of Parking Spaces	32 (including 8 at pumps)
•Number of Handicapped Parking Spaces	2
•Proposed Total Parking Spaces	33
<u>BICYCLE PARKING SPACES</u>	
•Existing Number of Bicycle Parking Spaces	0
•Existing Number of Bicycle Parking Spaces	0
•Proposed Number of Bicycle Parking Spaces	10
•Total Bicycle Parking Spaces	10
<u>ESTIMATED COST OF PROJECT</u>	
	\$979,250

APPLICATION SUBMISSION

As of December 1, 2010, all site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Until December 1, 2010, submissions shall include seven (7) packets with folded plans containing the following materials:

1. **Seven (7) full size site plans** that must be **folded**.
2. Seven (7) copies of all written materials or as follows, unless otherwise noted:
 - a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
5. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 100 feet.
6. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
7. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
8. One (1) set of plans reduced to 11 x 17.

Refer to the application checklist for a detailed list of submittal requirements.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

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Signature of Applicant: Cumberland Farms, Inc. By: 	Date:
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Kathleen A. Sousa, Land Planning Mgr.

**Level II Site Plan Application
Cumberland Farms, Inc.
801 Washington Avenue, Portland, Maine**

Project Description:

Cumberland Farms, Inc. (“Cumberland”), is the current owner in fee simple of the above-referenced property (“the Property”) used as a retail establishment combined with gasoline sales. Cumberland now proposes to construct a 1,474 s.f. addition to its existing 2,400 s.f. convenience store for a total retail gross floor area of 3,874 s.f. The exterior of the building will be renovated to reflect the new corporate colors and design themes, as shown on the business elevations provided with this application. The proposed store renovations have prompted additional site improvements, including the following:

- Renovation of existing dumpster on new concrete pad
- New concrete walk at the front and side of the store, including the proposed addition
- Installation of protective bollards (with sleeves) at parking spaces along the front and side of the store
- Relocation and expansion of parking spaces
- Addition of bicycle racks
- Landscaping to be enhanced (see Landscaping Plan)
- Addition of area lamps for enhanced safety of parking and dumpster areas
- Replacement of four existing 4’ x 10’ gas islands, concrete pad, and four fuel dispensers with new 4’ x 10’ islands, concrete pads, and four fuel dispensers in the

same location as existing (“like for like” exchange). One dispenser will offer diesel fuel.

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- Replace 3-8,000 gal. single-wall fiberglass underground storage tanks with 2-20,000 gal. double-wall fiberglass underground dual compartment storage tanks providing for diesel fuel sales, upgrading the underground tanks and piping and

reducing the number of fuel deliveries. One of the compartments will store 8,000 gallons of diesel fuel.

Written Assessment of Zoning:

In 2010, the subject property was rezoned from Neighborhood Business (B-1) and Residential (R-3) to Community Business (B-2). Prior to its rezoning, the use of the property (major/minor auto service establishment) was a grandfathered non-conforming use in the B-1 and R-3 zones. Major/minor auto service establishments are an authorized conditional use in the B-2 Community Business Zone.

Requests for Waivers:

Cumberland is not currently requesting any waivers from the Portland site plan/technical standards.

Summary of Significant Natural Features:

There are no significant natural features on the Property. The proposed additions and site changes will not reduce existing landscaping buffers and will primarily implicate areas that are already developed on the site.

Easements and Burdens:

There are no known easements or other burdens on the Property and the proposed Project will not result in any new easements or burdens.

Consistency with Master Plan:

One of the stated Development Goals in the City's "Community Commercial Policies and Land Use Plan" (Comprehensive Plan, page 28) is to "Promote preservation and revitalization of its existing commercial centers and maintain a scale within them that

is compatible and integrated with other land use.” Among the City’s Capital Improvement Goals (page 29) is to “Strengthen and upgrade existing neighborhoods by providing physical improvements, which enhance and preserve the neighborhood character and environment.” The proposed project is consistent with these goals in that it aims to revitalize and improve the functionality and physical appearance of an established commercial enterprise. While Cumberland has operated a retail store and gasoline station at the Property for many years, and has thus proven itself to be compatible with surrounding uses in the area, the proposed site changes will result in greater compatibility and harmony with both neighboring residential and commercial uses. The increased retail space will also allow for increased economic activity and may allow for increased employment opportunities.

APPLICATION OF CUMBERLAND FARMS, INC.

801 WASHINGTON STREET

EVIDENCE OF RIGHT, TITLE AND INTEREST

V1079

QUITCLAIM DEED

Book 6310
Page 145

39906

KNOW ALL MEN BY THESE PRESENTS:

THAT GULF OIL CORPORATION, a Pennsylvania corporation, having an office at 355 Lyon Avenue, East Providence, Rhode Island 02914, GRANTOR, in consideration of ONE DOLLAR (\$1.00) and other valuable consideration paid by V.S.H. REALTY, INC., having a place of business at 777 Dedham Street, Canton, Massachusetts 02021, GRANTEE, the receipt whereof it does hereby acknowledge, does hereby remise, release, bargain, sell and convey and forever quitclaim unto the said GRANTEE, its successors and assigns forever, three certain lots or parcels of land, together with the buildings and other improvements thereon, situated in the City of Portland, County of Cumberland and State of Maine, said land being more particularly bounded and described as follows:

PARCEL ONE

Beginning at the point of intersection of the northeasterly side line of Washington Avenue with the northwesterly side line of Ocean Avenue;

Thence running northeasterly by said Ocean Avenue fifty (50) feet to a point and to land now or formerly of Phillippe's, Inc.;

Thence running northwesterly and parallel with said northeasterly side line of Washington Avenue one hundred (100) feet to a point and land now or formerly of Joseph Phillippe;

Thence running southwesterly and parallel with said northwesterly side line of Ocean Avenue and by said land now or formerly of Joseph Phillippe, fifty (50) feet to said Washington Avenue;

Thence running southeasterly by said northeasterly side line of Washington Avenue one hundred (100) feet to the point of beginning.

Together with all the right, title and interest of the Grantor in and to all land lying in all streets, highways, rights of way and gores abutting on or appurtenant to said premises.

BEING the same premises conveyed to Gulf Oil Corporation, the Grantor herein, by deed of Minnie A. Green dated March 12, 1948 and recorded with the Cumberland County Registry of Deeds, State of Maine, on March 12, 1948 in Book 1907, Page 154.

PARCEL TWO

A certain lot or parcel of land at Lunt's Corner, in said Portland, on the Northwesterly side of Ocean Avenue, bounded and described as follows:

Beginning at the most easterly corner of land formerly of George Green, which corner is distant fifty (50) feet from the intersection of the northwesterly side line of Ocean Avenue with the northeasterly side line of Washington Avenue;

Together with all the right, title and interest of the Grantor in and to all land lying in all streets, highways, rights of way and gores abutting on or appurtenant to said premises.

BEING the same premises conveyed to Gulf Oil Corporation, the Grantor herein, by deed of Phillippe's, Inc. dated November 7, 1966 and recorded with the Cumberland County Registry of Deeds, State of Maine on November 7, 1966 in Book 2979, Page 377.

All of said premises are subject to the Following:

Any state of facts and conditions that an accurate survey and personal inspection of the premises would disclose; easements, conditions, restrictions and reservations of record or which may have been imposed thereon; existing tenancies, if any, zoning ordinances, if any; and taxes and assessments both general and special, if any, which shall fall due and payable following the date of closing.

TO HAVE AND TO HOLD the same, together with all privileges and appurtenances thereunto belonging, unto the said V.S.H. REALTY, INC., its successors and assigns forever.

IN WITNESS WHEREOF, the said GULF OIL CORPORATION has caused this instrument to be sealed with its corporate seal and signed in its corporate name by R. P. Greisler, its Attorney-in-Fact, thereunto duly authorized, this 26th day of September in the year of our Lord one thousand nine hundred and eighty-three.

Signed, Sealed and Delivered in the Presence of:

Mauri Salento

GULF OIL CORPORATION

BY: R. P. Greisler
R. P. Greisler
Attorney-in-Fact

R. D. Hess
R. D. HESS

ATTEST:
BY: J. Ronald Sandberg
J. RONALD SANDBERG
Assistant Secretary of
Gulf Oil Corporation

SEAL

STATE OF RHODE ISLAND X
X SS.
COUNTY OF PROVIDENCE X

DATE: September 26, 1983

Then personally appeared the above named R. P. Greisler, an Attorney-in-Fact of Gulf Oil Corporation, and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

George T. Balian
George T. Balian
Notary Public

SEAL

27 1983

OF DEEDS CUMBERLAND COUNTY, MAINE
at 2 H 1983 M, and recorded in

310 PAGE 145

James J. Walsh

Register My Commission Expires June 30, 1986

105

QUINCLAIN DEED

GULF OIL CORPORATION

U.S. PATENT OFFICE

APPLICATION OF CUMBERLAND FARMS, INC.

801 WASHINGTON STREET

COPIES OF STATE AND/OR FEDERAL PERMITS



STATE OF MAINE

Date of Certificate:

03/11/2011

DEPARTMENT OF ENVIRONMENTAL PROTECTION

FACILITY REGISTRATION CERTIFICATE FOR
Underground Storage Tank

Please display this certificate in a visible location at the registered facility.

Facility:

CUMBERLAND FARMS INC 1839
801 WASHINGTON ST
PORTLAND

Facility Registration Number: 9071
Date of Registration: October 23, 1986
Facility Phone: 207-780-8032

Operator: ECLIPSE DIVISION

CUMBERLAND FARMS INC
588 SILVER ST
AGAWAM
MA 01001
508-270-8300

Sensitive Area Status:

Facility Use:

Retail Oil

Owner: ECLIPSE DIVISION

CUMBERLAND FARMS INC
588 SILVER ST
AGAWAM
MA 01001
508-270-8300

Underground Storage Tank

Number of Active Aboveground Tanks: 0

Number of Active Underground Tanks: 3

If the information on this form is accurate and complete, please retain for your records.

The Maine Department of Environmental Protection must be notified of any errors or changes in the information on this form. To accomplish this, please draw a line through the incorrect or outdated information, insert the correct information, and return this form to:

Department of Environmental Protection Bureau of Remediation and Waste Management
State House Station # 17 Augusta, ME 04333

Attn: Underground Tanks Program

If you have any questions concerning this process, please call (207)287-2651 and ask for the administrator of the Underground Storage Tanks program.

INDIVIDUAL TANK DATA FOR SITE NUMBER: 9071

Tank	Tank Under/ Above ground	Tank Type	Tank Size	Tank Monitoring	Date Tank Installed	Tank Status	Tank Status Date
1	Underground	F/glass single-walled	8000	Automatic Tank Gauge	01/01/1985	Active	01/01/1985

Chamber	Chamber Size	Product Stored	Pipe Under/ Above ground	Date Installed	Pipe Monitoring	Piping Type	Overfill Protection
1	8000	Premium Unleaded	Underground	11/28/1996	Secondary Containment / Cont Elec Mon	Flexible double-walled	Vent Ball

INDIVIDUAL TANK DATA FOR SITE NUMBER: 9071

Tank	Tank Under/ Above ground	Tank Type	Tank Size	Tank Monitoring	Date Tank Installed	Tank Status	Tank Status Date
2	Underground	F/glass single-walled	8000	Automatic Tank Gauge	01/01/1985	Active	01/01/1985

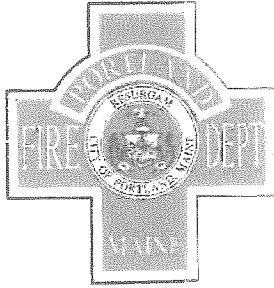
Chamber	Chamber Size	Product Stored	Pipe Under/ Above ground	Date Installed	Pipe Monitoring	Piping Type	Overfill Protection
1	8000	Unleaded Gasoline	Underground	11/28/1996	Secondary Containment / Cont Elec Mon	Flexible double-walled	Vent Ball

INDIVIDUAL TANK DATA FOR SITE NUMBER: 9071

Tank	Tank Under/ Above ground	Tank Type	Tank Size	Tank Monitoring	Date Tank Installed	Tank Status	Tank Status Date
3	Underground	F/glass single-walled	8000	Automatic Tank Gauge	01/01/1985	Active	01/01/1985

Chamber	Chamber Size	Product Stored	Pipe Under/ Above ground	Date Installed	Pipe Monitoring	Piping Type	Overfill Protection
1	8000	Unleaded Gasoline	Underground	11/28/1996	Secondary Containment / Cont Elec Mon	Flexible double-walled	Vent Ball

Issue Date: 6/13/2011



Fire Prevention Permit
from the
City of Portland Fire Department
Fiscal Year 2012
License Number: 3542

To all whom these presents may concern:

This is to certify that the City of Portland Fire Department has granted a permit to:

CUMBERLAND FARMS #5604/#1839

Address: 801 WASHINGTON AVE

Permit For:

- 1) FLAMMABLE/COMBUSTIBLE LQD
- 2) RENEWAL APPLICATION FEE
- 3) SPECIAL TYPE DISPENSING

This permit is granted subject to strict observance of all laws, ordinances, and regulation enacted for the protection of the City so far as they may apply, and is to continue in force until 6/30/2012, unless sooner revoked.

Approved By:

Gregory E. Cass, Fire Prevention Officer

Issued By:

Frederick J. LaMontagne, Fire Chief

This permit is not transferable by owner, corporation, address, or type.

APPLICATION OF CUMBERLAND FARMS, INC.

801 WASHINGTON STREET

EVIDENCE OF FINANCIAL CAPACITY



September 15, 2011

Department of Planning & Urban Development
Division of Planning
Portland City Hall
389 Congress Street
Portland, ME 04101

RE: Evidence of Financial Capacity

Dear Department of Planning & Urban Development:

Cumberland Farms, Inc. is proposing a building addition and other site improvements at 801 Washington Avenue. The purpose of this letter is to satisfy your requirement of evidence of financial capacity. This project is expected to cost \$979,250 and the company has sums well in excess of that amount available to fund the project. We look forward to working with you.

Please feel free to call me with any questions you may have in regards to this letter.

Sincerely,

A handwritten signature in black ink, appearing to read "H. Rosenstein".

Howard S. Rosenstein
Chief Financial Officer
Cumberland Farms, Inc.

Phone: 508-270-8339

Cumberland Farms, Inc.
100 Crossing Boulevard, Framingham, MA 01702
508-270-1400
www.cumberlandfarms.com

APPLICATION OF CUMBERLAND FARMS, INC.

801 WASHINGTON STREET

EVIDENCE OF UTILITY CAPACITY

Kathleen Sousa

From: Dominic A. Taverna
Sent: Monday, October 17, 2011 8:33 AM
To: Kathleen Sousa
Cc: 'brent@dlpoulin.com'; John Marth; Michael Durant
Subject: FW: Cumberland Farms Store- 801 Washington Ave Portland, ME 441-1065485-012

Categories: Red Category

Kathie

I received a voice mail on Friday from Jamie at Central Maine Power. I returned his VM and left my contact information on Friday. Please see the attached E-mail from Jamie regarding power at this location. Let me know if you need anything else on this.

Dom

Dominic A. Taverna
Senior Construction Project Manager
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

Office 508-270-4407
Cell 508-380-2678
Email dtaverna@cumberlandgulf.com

This electronic message contains information from Cumberland Gulf Group of Companies, which may be privileged. The information is intended to be for the use of the addressee only. If you are not the addressee, note that any disclosure, copy, distribution or use of the contents of this message is prohibited.

From: Cough, Jamie [<mailto:Jamie.Cough@cmpco.com>]
Sent: Monday, October 17, 2011 6:32 AM
To: Dominic A. Taverna
Subject: Cumberland Farms Store- 801 Washington Ave Portland, ME 441-1065485-012

Dominick:

This store currently has a peak load of 58 kw (approximately 64 kva), and is served by a 3x50 kva bank (150 kva total). This 600A service can be supported by the existing transformer and service drop. Should the actual demands approach the transformer rating of 150 kva, we would be looking at installing an additional run of 336 to the service drop. Given the diversification of your loads, it is not likely.

If you have any additional questions, please contact me.

Regards,

Jamie

Jamie Cough
Energy Services Advisor
Central Maine Power Company
162 Canco Road
Portland, ME 04103
207-842-2367 office
207-458-0382 cell
207-626-4082 fax

APPLICATION OF CUMBERLAND FARMS, INC.

801 WASHINGTON STREET

MANAGEMENT OF SOLID WASTE GENERATED ON-SITE

WASTE HAULERS SERVICES AGREEMENT

This Services Agreement ("Agreement"), dated March 1, 2011, is between Cumberland Farms, Inc., of 100 Crossing Blvd., Framingham, MA 01702 ("Cumberland Farms"), and Troiano Waste Service, Inc., P.O. Box 3541, Portland, ME 04104 ("Provider").

In consideration of the covenants herein contained, Cumberland Farms and Provider hereby agree as follows:

- 1. Services:** Provider shall perform the waste hauling services listed on Exhibit A. Cumberland Farms will not be responsible for any other work ("out of scope" or "other work") performed without its prior consent. Out of scope services include but are not limited extra pickups and increasing container sizes.
- 2. Price & Locations:** The services will be performed per the pricing and locations listed in Exhibit B. Cumberland Farms will not be responsible for any surcharges, and/or penalties assessed by the Provider as a result of any blocked dumpsters (not caused by Cumberland Farms or its agents or employees), or any additional fees including but not limited to: fuel surcharges, environmental permit fees or taxes (including transportation fees assessed by any government agency). All fees related to the services are included in the base prices and will not be added to the costs of service during the term of Agreement.
- 3. Term:** The term of this Agreement is March 1, 2011 through February 28, 2013, unless earlier terminated as provided below. The Agreement may be renewed at Cumberland Farms' option for unlimited two year terms, upon at least 30 days prior written notice to Provider. Pricing will remain the same for each additional term.
- 4. Billing:** Provider must invoice Cumberland Farms within 60 days of completion of services or out of scope work for each location. Payment terms for work completed are net 30 days from the date of receipt of invoice. Out of scope work must be billed separately or itemized on the invoices for services. Cumberland Farms may set off against any invoiced amount, monies owed to Cumberland Farms by Provider. Cumberland Farms will not be responsible for any bills submitted after 60 days of completion of services or out of scope work. Cumberland Farms may, at its discretion, charge a \$50 processing fee for invoices submitted more than 60 days from completion date.
- 5. Insurance:** Provider represents that it now carries, and agrees it will continue during the term of this Agreement to carry, as a minimum, insurance in amounts set forth on attached Exhibit C or such other amounts or other coverage as Cumberland Farms may from time to time establish in its sole discretion. Cumberland Farms acknowledges that the services performed under this Agreement may be performed by technicians who are independent distributors for Provider who may not be required, under applicable law to carry workers' compensation coverage on themselves.

6. **Indemnification:** Provider shall indemnify, defend and hold harmless Cumberland Farms and its affiliates, directors, officers, employees, shareholders, members, managers, agents and representatives (the "Cumberland Parties") from any and all liabilities, damages, losses, claims, causes of action, costs and expenses, of any kind or nature (including without limitation, reasonable attorneys' fees and technical consultants' fees and expenses), as they come due and amounts paid in judgment or settlement incurred or sustained by or asserted against the Cumberland Parties in any manner directly or indirectly arising out of or relating to the services provided by Provider or its agents, subcontractors, or employees; or any breach by Provider, its employees, agents or subcontractors of any covenant or other agreement set forth in this Agreement, except for damages related to Cumberland Farms' sole negligence. The indemnification rights of the Cumberland Farms entities will survive the termination of this Agreement and shall not in any way be altered, diminished or limited without agreement by Cumberland Farms in writing prior to a claim being made.

7. **Warranty:** Provider warrants that all services will be performed in a professional and workmanlike manner, and will be free from any defects in workmanship and material and will conform to the requirements of this Agreement.

8. **Ownership of Documents:** Cumberland Farms shall have complete and unrestricted right to use all documents prepared by Provider in connection with its performance of the services described or referred to herein. Said documents are to be the property of Cumberland Farms and are not to be used on other projects except by agreement of Cumberland Farms in writing.

9. **Termination:** Notwithstanding the term of this Agreement set forth above:

- A. Either party may terminate this Agreement in whole or in part upon at least 30-days prior written notice to the other party without cause or if the other party: (i) becomes insolvent or seeks protection, voluntarily or involuntarily, under any bankruptcy, insolvency, reorganization, or similar law; (ii) makes a general assignment for the benefit of creditors; or (iii) fails to perform any of its obligations set forth in this Agreement.
- B. Upon receipt of any termination notice by Cumberland Farms, Provider shall immediately discontinue services on the date and to the extent specified in the notice. Within 30 days from the date of any final invoice, Cumberland Farms shall pay Provider for services performed prior to the time specified in said notice, to the extent not previously reimbursed, at the Rates set forth herein. Provided, however, Cumberland Farms may withhold money due for any services not performed in accordance with the requirements of this Agreement. Provider is not entitled to, and shall not seek, any other reimbursement or payment, claims, or damages for lost profits or incidental or consequential damages.

10. **Contractor Relationship:** In furnishing services hereunder, Provider shall be acting as an independent contractor in relation to Cumberland Farms. Neither party shall have the right to act in any manner to obligate or bind the other in any manner whatsoever and nothing contained herein shall give, or is intended to give, any rights of any kind to any third person or party. Neither Provider nor any personnel employed by or affiliated with Provider for the benefit of Cumberland Farms shall be considered as having employee status at Cumberland Farms. The parties acknowledge that they are not entering a joint venture or partnership with each other, and neither party shall represent to a third party that it has entered into such a joint venture or partnership.
11. **Cumberland Farms Employees:** No employee(s) or former employee(s) of Cumberland Farms may be employed by Provider at Cumberland Farms facilities to perform services pursuant to this Agreement without the prior written consent of Cumberland Farms. Cumberland Farms will promptly comply with any written request from Cumberland Farms to replace or substitute any employee performing services under this Agreement.
12. **Default/Self Help:** In the event Provider fails to perform the services as set forth in this Agreement, Cumberland Farms may provide notice verbally, by email, or facsimile to Provider of such failure and Provider shall cure such failure within three business days from the date of the notice. If Provider does not cure such failure within the required period of time, Cumberland Farms may invoke a self-help privilege by hiring a third party contractor to remedy the failure. In that event, Provider will pay Cumberland Farms on demand the difference between its pricing and the third party contractor pricing.
13. **Taxes:** Provider shall, and shall cause its subcontractors to, pay all federal, state and local income taxes, employment taxes, and sales, consumer, excise, use, and other similar taxes incurred by either party as a result of the services (other than taxes solely attributed to Cumberland Farms' income). Taxes that are levied upon the provider, by a government agency during the term of this agreement, and are related to the services, but are not in effect at the signing of this agreement, may be added to the base fee upon approval by Cumberland Farms at its sole discretion.
14. **Compliance with Laws:** Provider shall comply with all federal, state and local laws, regulations, ordinances or codes applicable to it and the performance of its obligations pursuant to this Agreement, including federal statutes commonly known as CERCLA, RCRA, OSHA, ADA, ADEA, FDCA, USA PATRIOT Act, and Title VII of the Civil Rights Act of 1964, as each of the same may be amended from time to time, and similar state or local counterparts.
15. (A) **Hazardous Materials:** Provider will assume full responsibility for any release, spill, leak, discharge, disposal, pumping, pouring, emission, emptying, injecting, leaching, dumping or escaping ("Release") or threat of Release of any hazardous substance into the environment at any location at which services are performed, if caused by Provider, its agents, servants, invitees, employees or subcontractors.. Provider will take all necessary precautions to avoid any such Releases or threats of Release. Provider will assume full responsibility for the handling and disposal of hazardous materials in accordance with applicable state, federal and local laws.

(B) **Emergency Conditions:** In the event of emergency conditions, including the discovery of a Release or threat of Release of hazardous substances into the environment, Provider shall comply with all applicable laws and immediately notify Cumberland Farms of such emergency conditions.

16. **Disputes/Governing Law:** Any dispute concerning any question of fact or law arising under this Agreement which is not disposed of by agreement between Provider and Cumberland Farms, shall be submitted to arbitration by a single arbitrator under the rules of the American Arbitration Association as then in effect and the award of the arbitrators shall be enforceable in any court of competent jurisdiction. The arbitration is to be conducted in Boston, Massachusetts. This Agreement shall be interpreted under the laws of the Commonwealth of Massachusetts, notwithstanding any choice of law rules.

17. **Proprietary Information:** Any proprietary information concerning Cumberland Farms, including but not limited to its business, operating, financial or marketing information, which is obtained by Provider incident to the performance of this Agreement shall remain the property of Cumberland Farms, is considered confidential, and no rights or licenses for its use is granted to Provider. Provider shall keep all confidential information in strict confidence and not disclose it without the prior written consent of Cumberland Farms. The provisions of this section will survive the termination of this Agreement.

18. **Publicity:** Provider shall not issue any publicity releases (including news releases, articles and advertising) relating to this Agreement and the services without the prior written approval of Cumberland Farms.

19. **Modifications:** This Agreement may be modified or rescinded only in writing between the parties' duly authorized agents.

20. **Waiver:** The failure of either party to exercise any rights or insist in any instance upon strict performance by the other party of any provision in this Agreement will not constitute a waiver of that provision, any other provision, or any future obligation to comply with that provision or any other provision of this Agreement.

21. **Assignment:** Provider shall not assign any part of this Agreement nor delegate any duties herein without Cumberland Farms' prior written consent. Any purported assignment or delegation made without Cumberland Farms' consent will be null and void.

22. **Severability:** In case any one or more of the provisions of this Agreement should be invalid, illegal, or unenforceable in any respect, the validity, legality, and enforceability of the remaining provisions of this Agreement will not in any way be affected or impaired thereby.

23. Official Notices and Points of Contacts: Unless otherwise provided herein, any notice required or permitted to be given hereunder by one party to the other shall be in writing and given in person, email with acknowledgement of receipt by a duly authorized representative, facsimile with verification of receipt, commercial courier with proof of delivery, or certified mail, returned receipt requested. The effective date will be the date received except for certified mail, for which delivery will be effective upon receipt, rejection or two (2) delivery attempts. The address of Cumberland Farms and Provider for all notices hereunder shall be:

If to: Troiano Waste Service, Inc.
PO Box 3541
Portland, ME 04104

Attn: Nelson Libby

If to Cumberland Farms:
Cumberland Farms, Inc.
100 Crossing Boulevard
Framingham, MA 01702
Attention: Tom Cacciola

With a copy to:
Cumberland Farms, Inc
100 Crossing Boulevard
Framingham, MA 01702
Attention: Eric Hemmalin

Either party may designate another address for all purposes of this Agreement by giving to the other party written notice of such address change.

24. Entire Agreement: This Agreement, together with all of the attached exhibits, constitutes the entire agreement between Cumberland Farms and Provider. It supersedes all prior contemporaneous communications, representations, understandings or agreements whether oral or written with respect to the subject matter thereof and has been induced by no representations, statements, or agreements other than those herein expressed.

25. Other Payment Arrangements: Upon Provider's request, Cumberland Farms may apply monies owed under this Agreement for payment directly to Provider's subcontractors, suppliers and materialmen and deduct the same amount from monies owed to Provider, provided that Provider has confirmed in writing that the subcontractor's, supplier's, and/or materialmen's work or product is not disputed by Provider. Provider will release Cumberland Farms from any liability associated with any Contractor's dispute with any subcontractor's suppliers and/or materialmen.

26. Subcontractors: Cumberland Farms reserves the right to approve all subcontractors for work at its locations.

27. Liens: To the extent enforceable under controlling law, Provider shall not cause, suffer or permit any mechanic's lien or materialmen's liens to be filed against any Cumberland Farms' property by reason of any work, labor, services or materials performed or furnished by or on behalf of Provider. If any such lien is filed, Provider shall cause such lien to be released of record by payment, bond, or order of a court of competent jurisdiction within 15 business days after the receipt of notice of the filing.

28. **No Exclusive Remedy:** No right or remedy conferred upon or reserved by either party in this Agreement is intended to be exclusive of any other right or remedy herein or by law provided, but each will be cumulative and in addition to every other right or remedy given herein or now or hereafter existing at law or in equity.


29. **No Third-Party Beneficiaries:** Nothing in this Agreement, whether express or implied, is intended to confer any rights or remedies under or by reason of this Agreement on any person, other than the parties and their respective permitted successors and assigns, nor is anything in this Agreement intended to relieve or discharge the obligation or liability of any third person to any party, nor give any third person any right of subrogation or action against any party.

30. **Successors:** This Agreement and all of the provisions hereof will be binding upon and inure to the benefit of the Parties hereto and their respective successors and permitted assigns.

Provider and Cumberland Farms acknowledge that they have read this Agreement, understand it, and agree to be bound by its terms. Each representative signing below warrants that he or she is duly authorized to bind his or her respective party.

PROVIDER

CUMBERLAND FARMS, INC.

By: 
_____ Duly Authorized

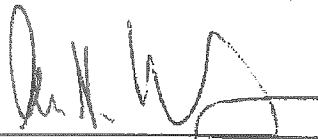

_____ Duly Authorized

EXHIBIT A
Cumberland Farms, Inc.
Solid Waste Disposal Services
SCOPE OF WORK

Cumberland Farms Inc. (CFI) requires Provider to furnish equipment and services to dispose of and/or recycle waste materials (hereinafter "services"). For the purpose of this scope of work "waste materials" means all non-hazardous solid waste and/or recyclable materials generated by CFI's service addresses. Recyclable materials shall include any type of material that can be recycled or recovered whether separated or not separated prior to waste materials collection.

- All materials deposited in the containers for disposal and/or recycling will be normal refuse or "solid waste" as that term is commonly referred to or defined in applicable environmental law and regulations, and will not include "hazardous waste" or other such material as defined in applicable environmental laws and regulations.
- Provider shall assume full responsibility for the handling, processing, transportation, shipment, transshipment and disposal and/or recycling of all solid wastes in accordance with applicable environmental laws and regulations and at approved solid waste and/or recycling facilities or landfills, and shall take all necessary precautions to avoid any releases of hazardous waste into the environment.
- In the event provider discovers any hazardous materials in any shipment of solid waste, or any release of any hazardous material into the environment, it shall promptly comply with all applicable laws and notify Cumberland Farms immediately.
- Provider shall be familiar and comply with all regulations and or ordinances including those governing fire lane accessibility, overnight noise, and "time of service" restrictions.
- All equipment furnished by provider shall remain the property of provider and Cumberland Farms shall bear no responsibility or liability for any loss or damage to the equipment or for its contents at any Cumberland Farms location.
- Provider's equipment will be maintained in good condition at all times and when necessary will be replaced at Cumberland Farms discretion free of charge.
- Provider will ensure to close all dumpster enclosures after service at each facility (if applicable) and will be responsible for replacing all missing gate locks.
- Provider is responsible for damages to persons or property as a result of any gates left open by Provider or its subcontractors.
- Provider shall provide all services as designated by the Client including open boxes, temporary equipment, and other extra services as required.

EXHIBIT B

VENDOR	STORE	ADDRESS	CITY	ST	TRASH	FREQ	PRICE	C/B	FREQ
TROIANO WASTE	5604	801 Washington Avenue	Portland	ME	10 YARD	1 X WEEK	\$ 255	8 YARD	1 X WEEK
TROIANO WASTE	5607	137 Main Street	Gorham	ME	6 YARD	1 X WEEK	\$ 168	6 YARD	1 X WEEK
TROIANO WASTE	5608	287 Ocean House Road	Cape Elizabeth	ME	6 YARD	Every Other Week	\$ 90	6 YARD	Every Other Week
TROIANO WASTE	5613	512 Woodsford Street	Portland	ME	10 YARD	1 X WEEK	\$ 255	6 YARD	1 X WEEK
TROIANO WASTE	5614	145 Sabattus Road	Sabattus	ME	6 YARD	1 X WEEK	\$ 159	10 YARD	1 X WEEK
TROIANO WASTE	5616	564 Alfred Street	Biddeford	ME	6 YARD	1 X WEEK	\$166.00	6 YARD	1 X WEEK
TROIANO WASTE	5617	441 Payne Road	Scarborough	ME	8 YARD	1 X WEEK	\$ 184	8 YARD	1 X WEEK
TROIANO WASTE	5619	939 US Rte 1	Yarmouth	ME	10 YARD	1 X WEEK	\$ 200	8 YARD	1 X WEEK

EXHIBIT C**CUMBERLAND FARMS, INC.
WASTE HAULER INSURANCE REQUIREMENTS**

The insurance coverages must be written by carriers rated by A.M. Best as A-, VII or better.
 The insurance coverages are in all cases Primary for Services Contractor and Non-Contributory for Cumberland Farms or Gulf Oil. Any deductibles or retentions shall be the sole responsibility of Services Contractor.
 The insurance certificate must show the complete insurance carrier name as it appears in the A.M. Best Guide.
 The insurance certificate must be an original document signed by the producer; copies are not acceptable.
 The insurance certificate must be completed in its entirety and contain policy numbers; binders are not acceptable.
 The insurance certificate must show 30 days notice of cancellation.
 The insurance certificate must show Cumberland Farms, Inc. and Gulf Oil, LP as an Additional Insured on all liability policies.

COMMERCIAL GENERAL LIABILITY

Must be an occurrence-based policy which covers all aspects of insured's operations including blanket contractual liability, products and completed operations coverage, bodily injury liability and broad form property damage, with limits of coverage not less than the following:

\$1,000,000 each occurrence
 \$2,000,000 aggregate

BUSINESS AUTOMOBILE LIABILITY

Must include coverage for ANY AUTO with limits of coverage not less than the following:

\$1,000,000 combined single limit

EXCESS LIABILITY

Must be excess coverage over all noted liability policies, including Employer's Liability and covering all aspects of insured's operations with limits of coverage not less than the following:

\$2,000,000
 each occurrence

\$4,000,000 aggregate

WORKERS' COMPENSATION

Must include coverage for Occupational Disease, for states where work is being conducted, operations are performed and where exposures may exist, in accordance with state and federal laws and regulations with limits of coverage not less than the following:

WORKERS' COMPENSATION

Statutory Limits

EMPLOYERS' LIABILITY

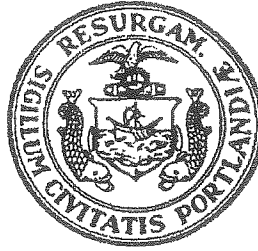
\$500,000 each accident
 \$500,000 disease each employee

NOTE: Please mail original insurance information only to:

Cumberland Gulf Group of Companies
 Maintenance Dept. Services Contractor
 P.O. Box 12010-CF
 Hemet, CA 92546-8010

CITY OF PORTLAND WASTEWATER CAPACITY APPLICATION

Department of Public Services,
55 Portland Street,
Portland, Maine 04101-2991



Mr. Frank J. Brancely,
Senior Engineering Technician,
Phone #: (207) 874-8832,
Fax #: (207) 874-8852,
E-mail: fjb@portlandmaine.gov

Date: 9/19/11

1. Please, Submit Utility, Site, and Locus Plans.

Site Address: 801 Washington Ave
(Regarding addressing, please contact Leslie Kaynor, either at 756-8346, or at LMK@portlandmaine.gov)
Proposed Use: Retail Motor Fuel Outlet
Previous Use: Retail Motor Fuel Outlet
Existing Sanitary Flows: 462 (Monthly Average) GPD
Existing Process Flows: N/A GPD
Description and location of City sewer, at proposed building sewer lateral connection:
Maintain existing connection to City sewer system.

Chart Block Lot Number: Map 63, Block B
Lots 3, 4, 5, 14, 15 & 17

Site Category	Commercial	<u>X</u>
	Industrial <i>(complete part 4 below)</i>	<u> </u>
	Governmental	<u> </u>
	Residential	<u> </u>
	Other <i>(specify)</i>	<u> </u>

Clearly, indicate the proposed connection, on the submitted plans.

2. Please, Submit Domestic Wastewater Design Flow Calculations.

Estimated Domestic Wastewater Flow Generated: 1,260 (See Attached Spreadsheet) GPD
Peaking Factor/ Peak Times: Peak Time of Business = 7:00 - 9:00 AM & 4:00 - 6:00 PM
Specify the source of design guidelines: *(i.e. "Handbook of Subsurface Wastewater Disposal in Maine," "Plumbers and Pipe Fitters Calculation Manual," __ Portland Water District Records, __ Other (specify)*
Handbook of Subsurface Wastewater Disposal in Maine

Note: Please submit calculations showing the derivation of your design flows, either on the following page, in the space provided, or attached, as a separate sheet.

3. Please, Submit Contact Information.

Owner/Developer Name: Dawn Johnson c/o Cumberland Farms, Inc.
Owner/Developer Address: 100 Crossing Boulevard, Framingham, MA
Phone: (508) 271-1431 Fax: (781) 459-1082 E-mail: djohnson@cumberlandgulf.com
Engineering Consultant Name: Frank C. Monteiro, PE c/o MHF Design Consultants, Inc.
Engineering Consultant Address: 44 Stiles Road, Suite One, Salem, NH 03079
Phone: (603) 893-0720 Fax: (603) 893-0733 E-mail: fcm@mhfdesign.com
City Planner's Name: Barbara Barhydt Phone: (207) 874-8699

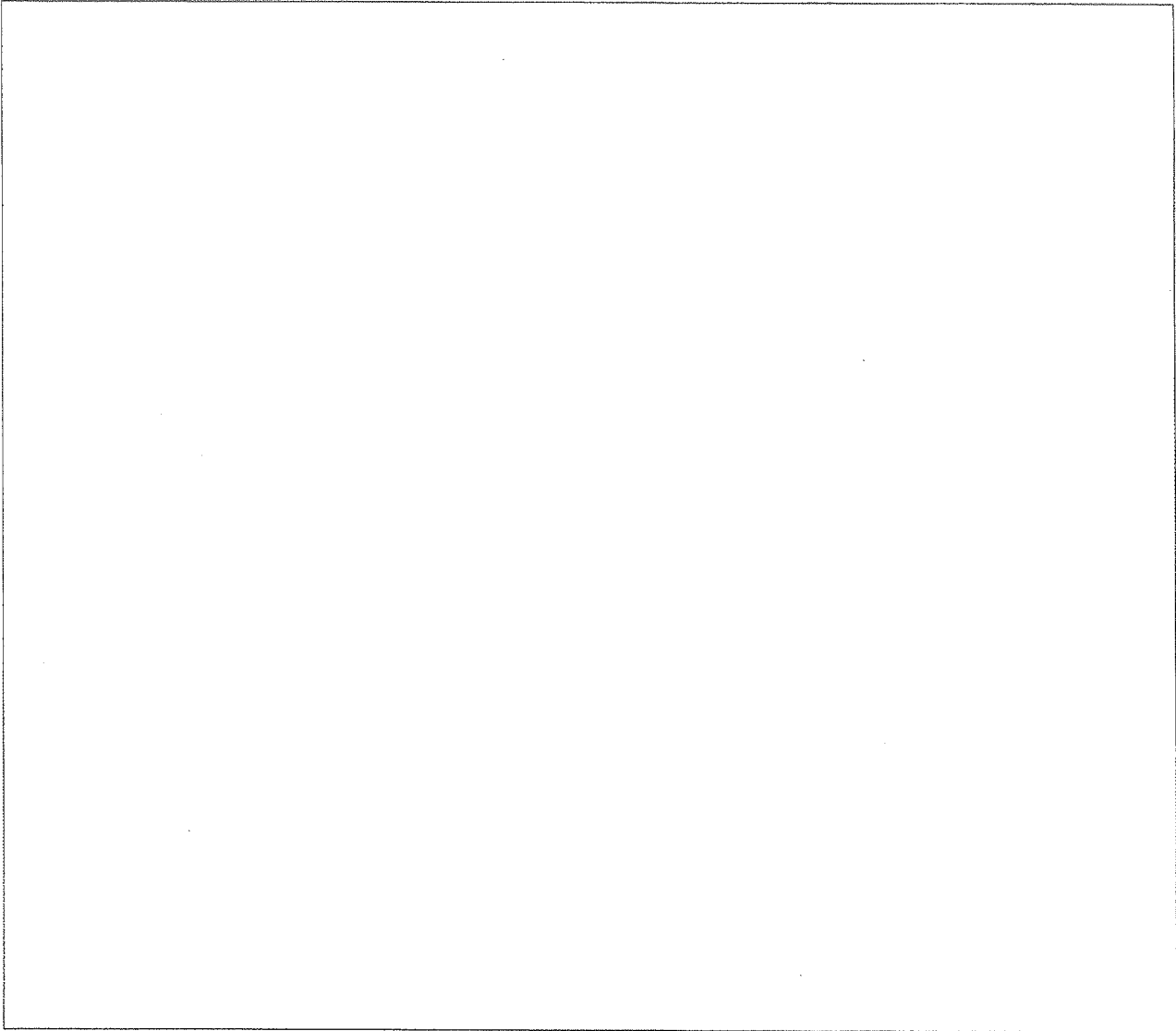
Note: Consultants and Developers should allow +/- 15 days, for capacity status, prior to Planning Board Review.

4. Please, Submit Industrial Process Wastewater Flow Calculations

Estimated Industrial Process Wastewater Flows Generated: N/A GPD
Do you currently hold Federal or State discharge permits? Yes No
Is the process wastewater termed categorical under CFR 40? Yes No
OSHA Standard Industrial Code (SIC): (<http://www.osha.gov/oshstats/sicser.html>)
Peaking Factor/Peak Process Times:

Note: On the submitted plans, please show the locations, where the building's sanitary, and process water sewer laterals, exit the facility, where they enter the city's sewer, the location of any control manholes, wet wells, or other access points, and the locations of any filters, strainers, or grease traps.

Notes, Comments, or Calculations:

A large, empty rectangular box with a thin black border, intended for providing notes, comments, or calculations related to the submitted plans.

Comparison of Actual Water Usage vs Design Flow

Water Meter Readings

Date	# Days	Reading 100 CF	Usage Cubic Feet	Average CF/Day	Usage Gallons	Average GPD
10/26/2009		392				
11/25/2009	30	408	1,600	53.33	11,968	398.93
12/29/2009	34	427	1,900	55.88	14,212	418.00
1/28/2010	30	443	1,600	53.33	11,968	398.93
3/2/2010	33	461	1,800	54.55	13,464	408.00
3/31/2010	29	476	1,500	51.72	11,220	386.90
4/29/2010	29	492	1,600	55.17	11,968	412.69
5/28/2010	29	510	1,800	62.07	13,464	464.28
6/30/2010	33	533	2,300	69.70	17,204	521.33
7/29/2010	29	554	2,100	72.41	15,708	541.66
8/30/2010	32	576	2,200	68.75	16,456	514.25
9/30/2010	31	598	2,200	70.97	16,456	530.84
10/28/2010	28	618	2,000	71.43	14,960	534.29
12/1/2010	34	641	2,300	67.65	17,204	506.00
1/3/2011	33	660	1,900	57.58	14,212	430.67
Total	434		26,800	61.75	200,464	461.90

Design Flow Based on Maine Subsurface Disposal System Rules

Service Station:

100 gpd per fuel pump cabinet plus 15 gpd per employee (within 24-hr period)

Shopping Centers or stores with public restrooms and showers:

400 gpd per water closet plus 20 gpd per shower plus 15 gpd per employee (within 24 hr period)

Cumberland Farms, Inc.

4 Fuel Dispensers x 100 gpd	400
2 Water Closets x 400 gpd =	800
4 Employees x 15 gpd =	60

Total Design Flow: 1,260

To determine peak flow from water meter readings: multiply monthly average by 1.9

Peak Flow: 877.61

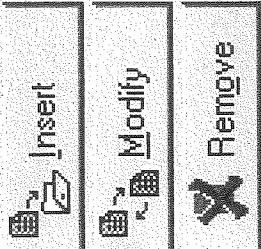
Utility Account



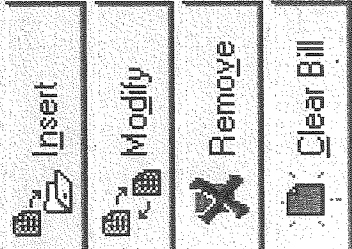
Account # **120570-02** Customer **CUMBERLAND FARMS V1079**

Address **801 WASHINGTON AVE PORTLAND ME 04103-**

Service	Service From	Service To	Exchange Date	Status	Status Date	Service Class
SEWER	07/10/1992	08/17/2007	08/17/2007	OPEN	07/10/1992	SMONTH
SEWER	08/17/2007			OPEN	08/17/2007	SMONTH
WATER	07/10/1992	08/17/2007	08/17/2007	OPEN	07/10/1992	W/MEMBER
WATER	08/17/2007			OPEN	08/17/2007	W/MEMBER



Reading Date	Read By	Source	Reason	Reading	Usage	Billable	Rdr
01/03/2011 15:12	MC#2	R		660.00	19.00	19.00	
12/01/2010 13:44	MC#2	R		641.00	23.00	23.00	
10/28/2010 14:58	MC#2	R		618.00	20.00	20.00	
09/30/2010 14:58	MC#2	R		598.00	22.00	22.00	
08/30/2010 14:13	MC#2	R		576.00	22.00	22.00	
07/29/2010 16:13	MC#2	R		554.00	21.00	21.00	
06/30/2010 14:14	MC#2	R		533.00	23.00	23.00	
05/28/2010 14:06	MC#2	R		510.00	18.00	18.00	
04/29/2010 15:05	MC#2	R		492.00	16.00	16.00	
03/31/2010 13:34	MC#2	R		476.00	15.00	15.00	
03/02/2010 14:01	MC#2	R		461.00	18.00	18.00	
01/28/2010 14:07	MC#2	R		443.00	16.00	16.00	
12/29/2009 14:16	MC#2	R		427.00	19.00	19.00	
11/25/2009 15:27	MC#2	R		408.00	16.00	16.00	
10/26/2009 14:15	MC#2	R		392.00	20.00	20.00	



TRAFFIC IMPACT AND ACCESS STUDY

**PROPOSED CUMBERLAND FARMS EXPANSION
PORTLAND, MAINE**

GPI

**105 CENTRAL STREET, SUITE 4100
STONEHAM, MASSACHUSETTS 02180
(781) 279-5500**

PREPARED FOR:

**CUMBERLAND GULF GROUP OF COMPANIES
100 CROSSING BOULEVARD
FRAMINGHAM, MASSACHUSETTS 01702**

SEPTEMBER 2011

GPI

***Traffic Impact and Access Study
Proposed Cumberland Farms Expansion
Portland, Maine
September 2011***

TECHNICAL MEMORANDUM

REF: NHX-2009626

DATE: September 6, 2011

TO: Ms. Kathleen A. Sousa
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

FROM: Mr. Jason R. Plourde, P.E., Assistant Vice President
Ms. Heather L. Monticup, Project Manager

RE: Traffic Impact and Access Study
Proposed Cumberland Farms Expansion
Portland, Maine

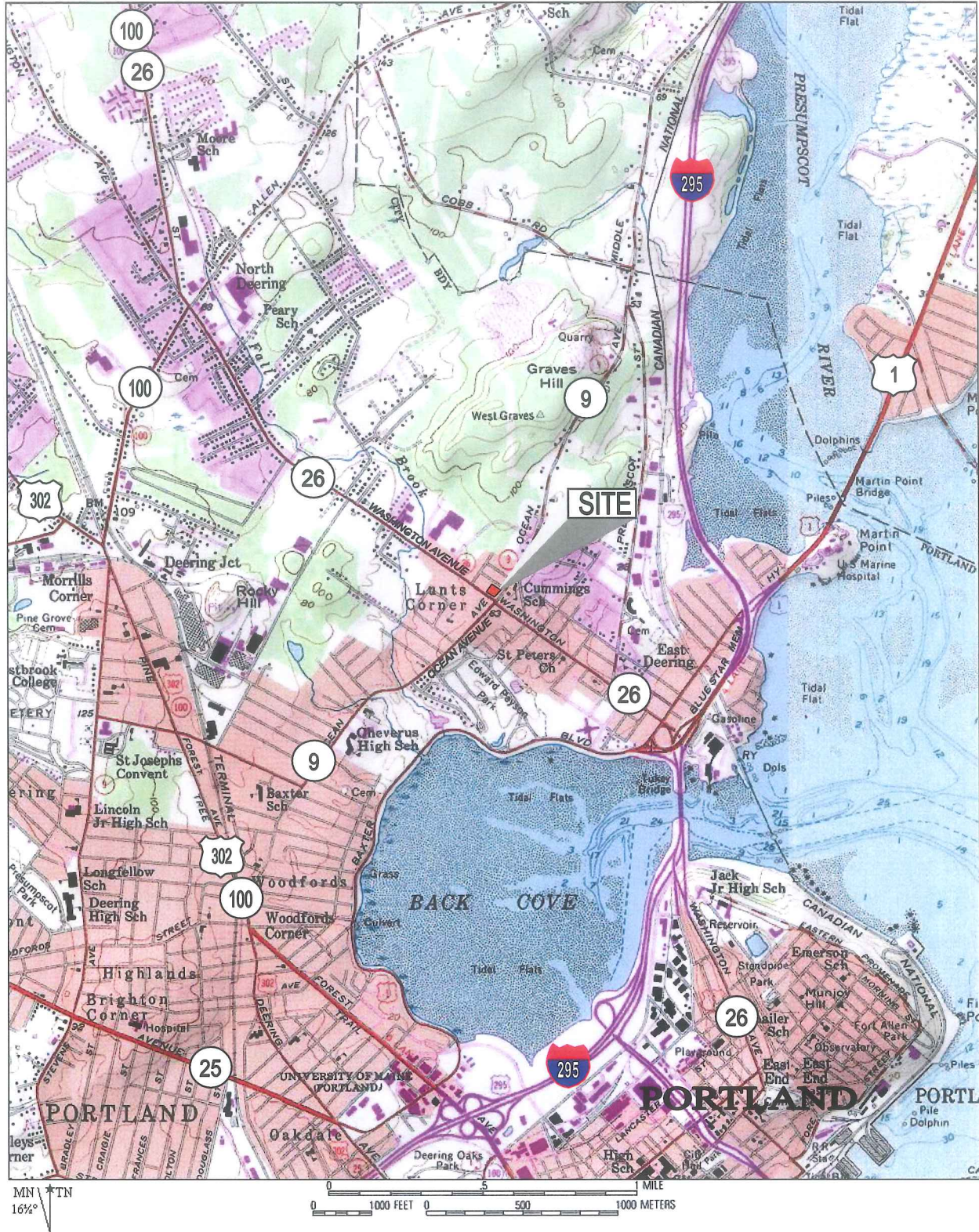
INTRODUCTION

Greenman-Pedersen, Inc. (GPI) has prepared this *Traffic Impact and Access Study* for a proposed expansion to the Cumberland Farms facility located at 801 Washington Avenue in Portland, Maine. The Cumberland Farms facility currently consists of a ±2,400 square foot convenience store with 4 fuel dispensers (8 vehicle fueling positions [vfps]). The proposed project consists of maintaining the existing fueling pumps and expanding the existing convenience store by 1,474 square feet for a total of 3,874 square feet. This traffic study has been prepared to evaluate the traffic impacts and access/egress requirements for the proposed expansion project.

The site is located on the northeast corner of the Washington Avenue (Route 26) and Ocean Avenue (Route 9) intersection. Access to the site is currently provided via two driveways on the east side of Washington Avenue and one driveway on the north side of Ocean Avenue. As proposed, the three driveways will be maintained. The site is bounded by the East Deering Fire Department to the east, Washington Avenue to the west, the Law Office of Patricia A. Nelson-Reade, R.N. to the north, and Ocean Avenue to the south. Figure 1 shows the site in relation to the surrounding roadways.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine



● ● ● Greenman-Pedersen, Inc.
Engineers, Architects, Planners, Construction Engineers & Inspectors

Figure 1
Site Location Map

Since the expansion project is expected to generate less than 100 additional passenger car equivalents (PCE) trips during the peak hours of traffic generation and the expansion project does not propose a change in use from the existing site, the project is not expected to require a *Traffic Movement Permit* from the Maine Department of Transportation (MaineDOT).

EXISTING CONDITIONS

Evaluation of the traffic impacts associated with the proposed expansion project requires an evaluation of existing and projected traffic volumes on the adjacent streets, the volume of traffic expected to be generated by the project, and the impact that this traffic will have on the adjacent streets and nearby intersections. In preparing this study for the proposed expansion project, the intersection of Washington Street and Ocean Avenue and the existing site driveway intersections were evaluated in accordance with current local and state guidelines.

Geometrics and Traffic Control

Washington Avenue and Ocean Avenue

Washington Avenue and Ocean Avenue intersect to form a four-way, signalized intersection under actuated traffic signal control. The Washington Avenue northbound and southbound approaches consist of an exclusive left-turn lane and a shared through/right-turn lane. The Ocean Avenue eastbound approach consists of an exclusive left-turn lane and a shared through/right-turn lane. The Ocean Avenue westbound approach consists of a single general-purpose travel lane. Based on field observations, however, vehicles were observed to utilize the wide approach as an exclusive left-turn lane and a shared through/right-turn lane. Directional travel along Washington Avenue and Ocean Avenue is separated by double yellow centerlines. The posted speed limit along Washington Avenue and Ocean Avenue is 30 miles per hour (mph). When school children are present, the posted speed limit along Ocean Avenue east of Washington Avenue is 15 mph. Crosswalks are striped across all legs of the intersection and sidewalks are provided along both sides of Washington Avenue and Ocean Avenue in the vicinity of the intersection.

Washington Avenue at Cumberland Farms Driveways

Washington Avenue intersects the Cumberland Farms north and south site driveways to form two unsignalized intersections, with the Cumberland Farms driveways approaches under STOP control. Washington Avenue adjacent to the site consists of a single general-purpose travel lane in the northbound direction. Washington Avenue southbound consists of two travel lanes with

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

the outside lane as a shared through/right-turn lane and the inside lane as an extension of the exclusive left-turn lane at the Ocean Avenue signalized intersection. Directional flow along Washington Avenue is separated by a double yellow centerline. The Cumberland Farms driveways allow both entering and exiting traffic.

Ocean Avenue at Cumberland Farms Driveways

Ocean Avenue intersects the Cumberland Farms site driveway to form an unsignalized intersection, with the Cumberland Farms driveway approach under STOP control. Ocean Avenue adjacent to the site consists of a single general-purpose travel lane in each direction separated by a double yellow centerline. The Cumberland Farms driveway allows both entering and exiting traffic.

Traffic Volumes

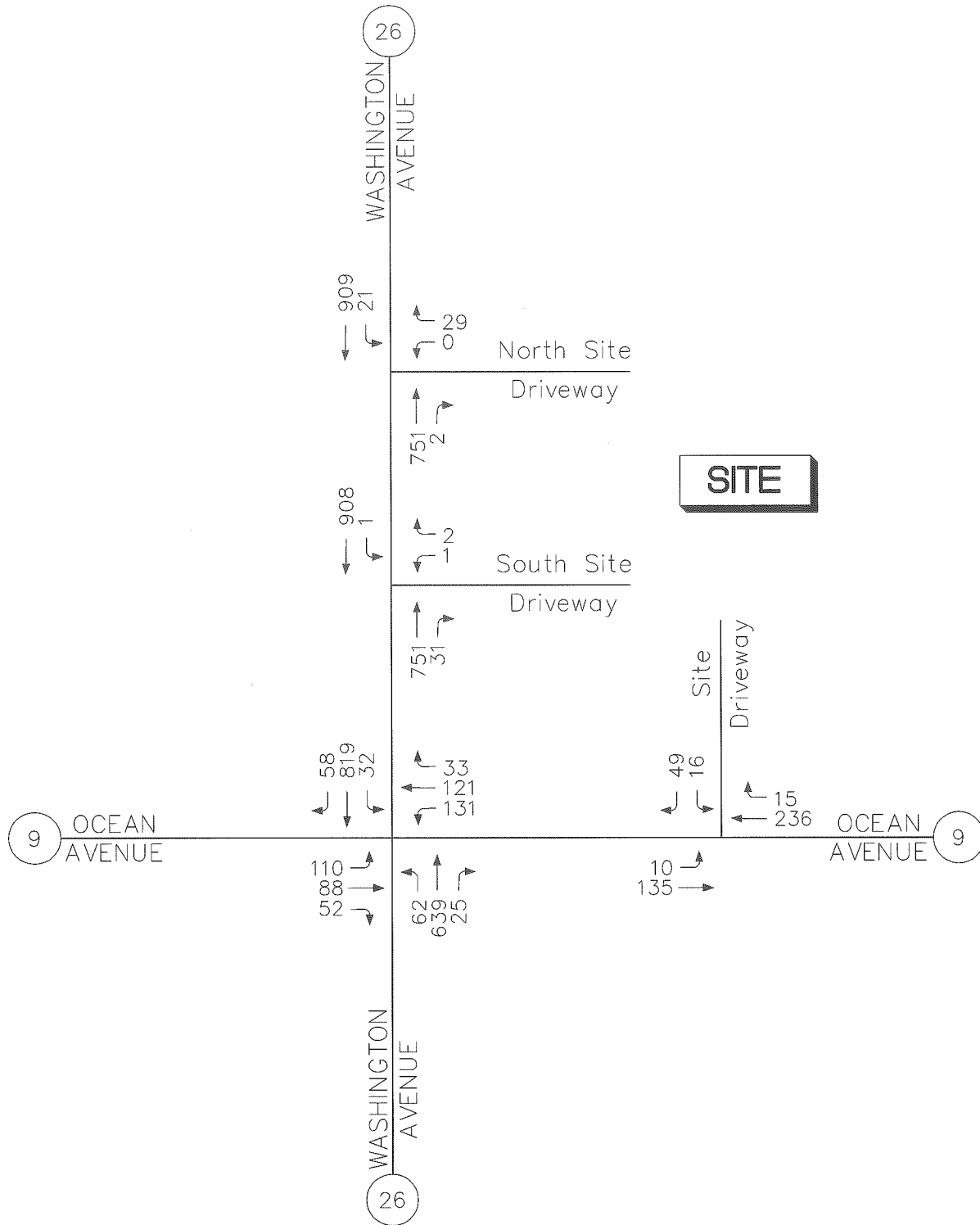
Base traffic conditions within the study area were developed by conducting manual turning movement counts (TMCs), vehicle classification counts, and automatic traffic recorder (ATR) counts in November 2009. The TMCs and vehicle classification counts were performed on November 18, 19, and 21, 2009 during the weekday AM peak period (7:00 to 9:00 AM), the weekday PM peak period (4:00 to 6:00 PM), and the Saturday midday peak period (11:00 AM to 2:00 PM) at the study area intersections. The ATRs were used to collect weekday daily traffic volumes along Washington Avenue and along Ocean Avenue adjacent to the site in November 2009. All traffic-count data are provided in the Appendix.

Traffic on a given roadway typically fluctuates throughout the year depending on the area and the type of roadway. To determine if the traffic-count data needed to be adjusted to account for this fluctuation, traffic-volume data from MaineDOT were researched.¹ Based on these weekly traffic-volume data, traffic volumes during the week that the traffic counts were conducted were found to be 9.2 percent below 30th highest hour traffic-volume conditions. Therefore, the November traffic counts were upwardly adjusted to provide the 30th highest hour traffic-volume conditions. The MaineDOT traffic data are provided in the Appendix. Table 1 summarizes the existing daily and peak-hour traffic volumes on Washington Avenue and on Ocean Avenue adjacent to the site. The weekday AM, weekday PM, and Saturday midday peak-hour traffic-flow networks are shown graphically on Figures 2 through 4, respectively.

¹ *Traffic Volume Counts – 2009 Annual Report*; Maine Department of Transportation; Urban Group 1, Weekly Group Mean Factors; 2011.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



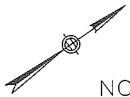
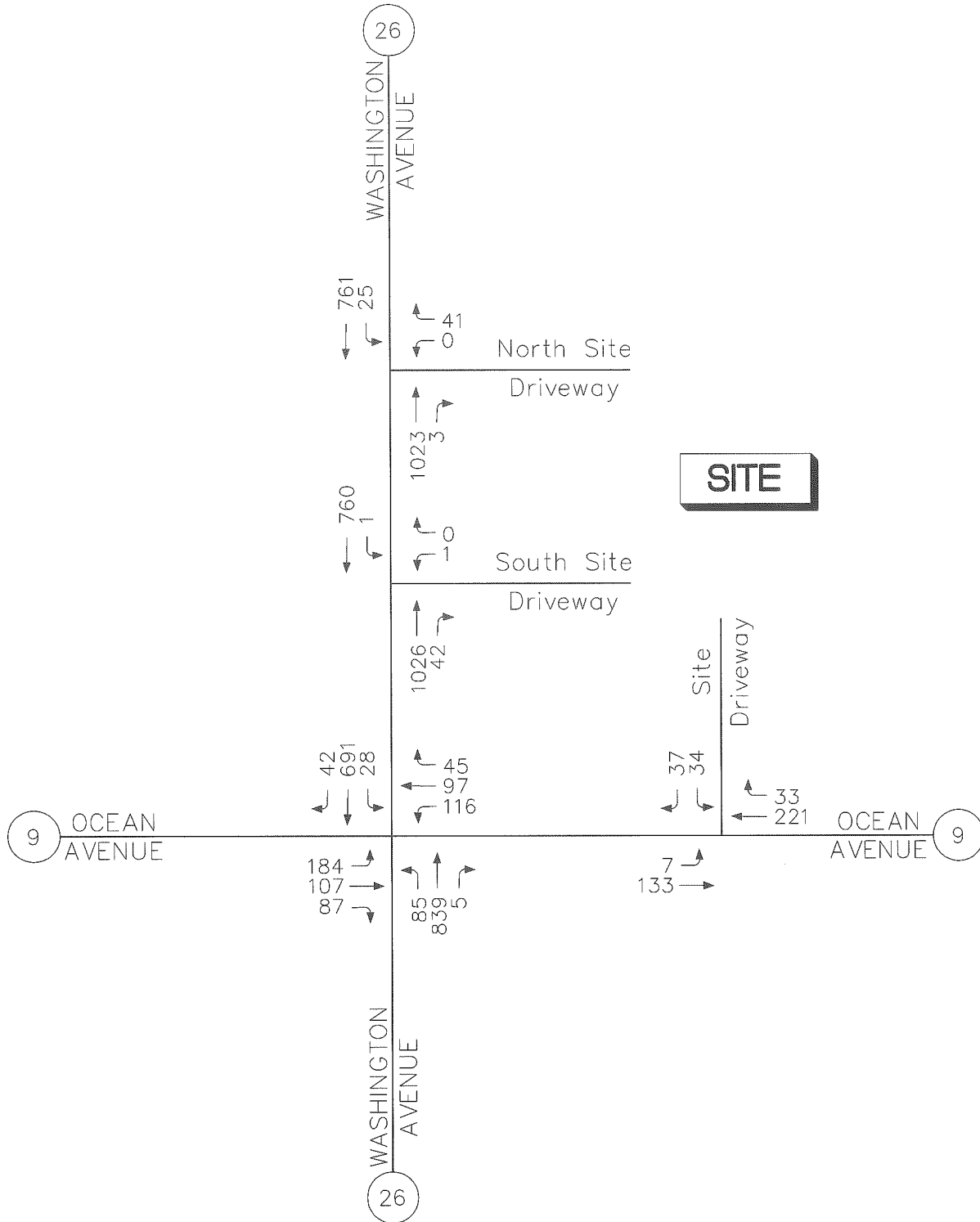
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Figure 2

2009 Existing
Weekday AM
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



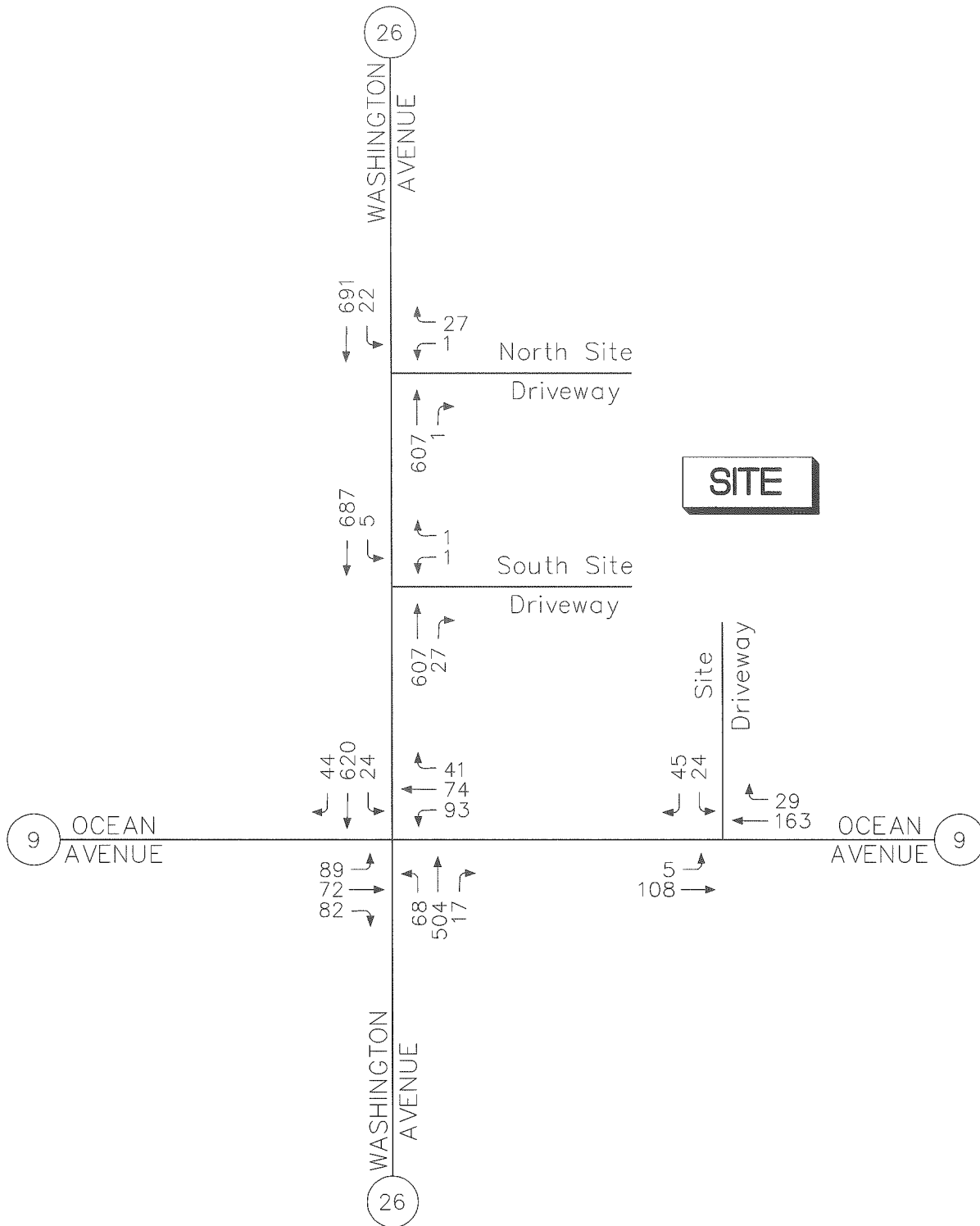
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Figure 3

2009 Existing
Weekday PM
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



NOT TO SCALE

Figure 4

2009 Existing
Saturday Midday
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

Table 1
EXISTING TRAFFIC-VOLUME SUMMARY

Location/Time Period	Daily Volume (vpd) ^a	Peak-Hour Volume (vph) ^b	K-Factor (%) ^c	Directional Distribution ^d
Washington Ave. west of Ocean Ave.:				
<i>Weekday Daily</i>	19,930			
<i>Weekday AM Peak Hour</i>		1,691	8.5	54% SB
<i>Weekday PM Peak Hour</i>		1,829	9.2	58% NB
<i>Saturday Midday Peak Hour</i>		1,322	6.6	52% SB
Ocean Ave. north of Washington Ave.:				
<i>Weekday Daily</i>	4,940			
<i>Weekday AM Peak Hour</i>		430	8.7	66% WB
<i>Weekday PM Peak Hour</i>		398	8.1	65% WB
<i>Saturday Midday Peak Hour</i>		321	6.5	65% WB

^a In vehicles per day. Traffic counts upwardly adjusted to reflect 30th highest hour.

^b In vehicles per hour. Traffic counts upwardly adjusted to reflect 30th highest hour.

^c Percentage of daily traffic occurring during the peak hour.

^d SB = southbound, NB = northbound, WB = westbound.

Traffic Collision History

Traffic collision data for the study area intersections were researched from the MaineDOT Crash Records Section between 2007 and 2009. The historical traffic collision information that has been collected for the study area locations is summarized in Table 2.

As shown in Table 2, the Washington Avenue intersection with Ocean Avenue and the roadway segment along Washington Avenue between Ocean Avenue and Byfield Road have reported 10 or more collisions during the three-year period. The remaining intersections and roadway segments were reported to have experienced one accident or less during the three-year time period. No fatalities were reported at any of the study area intersections or roadway segments.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

**Table 2
TRAFFIC COLLISION HISTORY SUMMARY**

Location	Number of Accidents		Severity ^a				Accident Type ^b						Percent During		
	Total	Average per Year	PD	PI	Possible Injury	F	CM	RE	HO	FO	SS	Ped	Other	Commuter Peak ^c	Wet/Icy Conditions
Washington Ave. at Cumberland Farms Driveways	0	0.0	0	0	0	0	0	0	0	0	0	0	0	0%	0%
Washington Ave. at Ocean Ave.	30	10.0	19	2	9	0	7	22	0	0	1	0	0	37%	27%
Washington Ave. between Ocean Ave. and Byfield Rd.	10	3.33	6	1	3	0	4	4	0	0	0	1	1	50%	50%
Ocean Ave. at Cumberland Farms Driveway	1	0.33	1	0	0	0	1	0	0	0	0	0	0	0%	0%
Ocean Ave. between Washington Ave. and Slemons Rd.	0	0	0	0	0	0	0	0	0	0	0	0	0	0%	0%

Source: MaineDOT Crash Records Section (2007 through 2009).

^aPD = property damage only; PI = personal injury; F = fatality.

^bCM = cross movement/angle; RE = rear end; HO = head on; FO = fixed object; SS = sideswipe; Ped = pedestrian.

^cPercent of vehicle incidents that occurred during the weekday AM and weekday PM commuter peak periods.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

The Washington Avenue and Ocean Avenue intersection has experienced 30 reported collisions over the three-year analysis period with 37 percent (11 of 30) involving personal injury or possible injury. Of the 30 crashes, 73 percent (22 of 30) were identified as rear-end-type collisions, 27 percent (8 of 30) were identified as angle-type or crossing movement-type collisions, and 1 was identified as a sideswipe-type collision. The majority of the reported rear-end collisions (68 percent, 15 of 22) occurred on the Washington Avenue approaches to the signalized intersection with Ocean Avenue (9 on the southbound approach and 6 on the northbound approach). Most of the rear-end collisions were attributed to driver inattention and vehicles following too closely. One incident involved a motorist driving under the influence; seven collisions occurred when the pavement was wet, snowy, or icy; and two crashes occurred when the traffic signal was operating under flash mode. These reported collisions do not indicate a particular collision pattern correctable by engineering measures. A collision diagram provided by MaineDOT for this location is included in the Appendix.

The Washington Avenue roadway segment between Ocean Avenue and Byfield Road experienced 10 reported collisions over the three-year analysis period with 40 percent (4 of 10) involving personal injury or possible injury. Of the 10 crashes, 40 percent (4 of 10) were identified as rear-end-type collisions, 40 percent (4 of 10) were identified as angle-type or crossing movement-type collisions, 1 was identified as an incident with a pedestrian violation, and 1 involved a vehicle backing out of a commercial driveway into oncoming traffic. Most of the reported collisions were attributed to driver inattention and failure to yield. One incident involved a motorist driving under the influence; and half of the collisions (5 of 10) occurred when the pavement was wet, snowy, or icy. Due to the number of curb cuts along this segment of Washington Avenue and the types of incidents reported, there does not appear to be a particular collision pattern correctable by engineering measures. A collision diagram provided by MaineDOT for this roadway segment is included in the Appendix.

Vehicle Speeds

Speed measurements were conducted along Washington Avenue and along Ocean Avenue adjacent to the site by measuring the elapsed time for vehicles traveling a short, pre-measured distance between two checkpoints. The travel time was recorded using ATRs and the speed was derived by dividing the elapsed time into the measured distance between checkpoints. The results of the speed measurements are summarized in Table 3.

**Table 3
OBSERVED TRAVEL SPEEDS ^a**

Location/Direction	Posted Speed Limit	Average Speed	85 th Percentile Speed ^b
Washington Avenue adjacent to the Site:			
<i>Northbound</i>	30	31	37
<i>Southbound</i>	30	25	34
Ocean Avenue adjacent to the Site:			
<i>Eastbound</i>	30	22	29
<i>Westbound</i>	30	19	27

^a In miles per hour (mph).

^b Speed at, or below which, 85 percent of all observed vehicles travel.

As shown in Table 3, the average speeds along Washington Avenue adjacent to the site were found to be between 25 and 31 miles per hour (mph) for northbound and southbound travel, with the 85th percentile speeds between 34 and 37 mph. The average speeds along Ocean Avenue adjacent to the site were found to be between 19 and 22 mph for eastbound and westbound travel, with the 85th percentile speeds between 27 and 29 mph. The 85th percentile speeds along Washington Avenue are slightly higher than the posted speed limit of 30 mph and the observed travel speeds along Ocean Avenue were found to be generally consistent with the posted speed limit of 30 mph when school children are not present.

Sight Distance

To identify potential safety concerns associated with site access and egress, sight distances have been evaluated at the site driveway locations to determine if the available sight distances for vehicles exiting the site meet or exceed the minimum distances required for approaching vehicles to safely stop. The available sight distances were compared with minimum requirements, as established by the American Association of State Highway and Transportation Officials (AASHTO)². AASHTO is the national standard by which vehicle sight distance is calculated, measured, and reported. In addition, the available sight distances were compared with City of Portland’s and MaineDOT’s sight distance requirements.

² *A Policy on Geometric Design of Highways and Streets*; American Association of State Highway and Transportation Officials (AASHTO); 2004.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

Sight distance is the length of roadway ahead that is visible to the driver. The Stopping Sight Distance (SSD) is the minimum distance required for a vehicle traveling at a certain speed to safely stop before reaching a stationary object in its path. The values are based on a driver perception and reaction time of 2.5 seconds and a braking distance calculated for wet, level pavements. When the roadway is either on an upgrade or downgrade, grade correction factors are applied. The SSD is measured from an eye height of 3.5 feet to an object height of 2 feet above street level, equivalent to the taillight height of a passenger car. The SSD is measured along the centerline of the traveled way of the major road.

The Intersection Sight Distance (ISD) is provided on minor street approaches to allow the drivers of stopped vehicles a sufficient view of the major roadway to decide when to enter the major roadway. By definition, the ISD is the minimum distance required for a motorist exiting a minor street to turn onto the major street, without being overtaken by an approaching vehicle reducing its speed from the design speed to 70 percent of the design speed. Based on City of Portland's and MaineDOT's regulations, the ISD is measured from an eye height of 3.5 feet to an object height of 4.25 feet above street level, 10 feet back from the edge of the travel way.

The SSD is generally more important as it represents the minimum distance required for safe stopping while the ISD is based only upon acceptable speed reductions to the approaching traffic stream. The ISD, however, must be equal to or greater than the minimum required SSD in order to provide safe operations at the intersection. In accordance with the AASHTO manual, *"If the available sight distance for an entering or crossing vehicle is at least equal to the appropriate stopping sight distance for the major road, then drivers have sufficient sight distance to anticipate and avoid collisions. However, in some cases, this may require a major-road vehicle to stop or slow to accommodate the maneuver by a minor-road vehicle. To enhance traffic operations, intersection sight distances that exceed stopping sight distances are desirable along the major road."* Accordingly, the ISD should be at least equal to the distance required to allow a driver approaching the minor road to safely stop.

The available SSD and ISD at the site driveway locations were measured and compared to minimum requirements, as established by AASHTO, City of Portland, and MaineDOT. Since the distance required to stop a vehicle is dependent on the speed of that vehicle, speed studies were conducted as presented in the previous section. Based on both the posted speed limit and the observed speeds, the SSD and ISD requirements at these intersections were calculated. The required minimum sight distances for each speed are compared to the available distances, as shown in Table 4.

**Table 4
SIGHT DISTANCE SUMMARY**

Location/Direction	Stopping Sight Distance (feet)		Intersection Sight Distance (feet)		
	Measured	Minimum Required ^a	Measured	MaineDOT Required ^b	Desirable ^c
Washington Ave. at West Site Driveways:					
<i>East of intersection</i>	500+	200	500+	250	412
<i>West of intersection</i>	500+	200	500+	250	379
Washington Avenue at East Site Driveway:					
<i>East of intersection</i>	500+	200	500+	250	412
<i>West of intersection</i>	500+	200	500+	250	379
Ocean Avenue at Site Driveway:					
<i>North of intersection</i>	500+	200	500+	250	324
<i>South of intersection</i>	500+	200	500+	250	302

^a Values based on AASHTO requirements for posted speed limit of 30 mph.

^b Values based on City of Portland’s and MaineDOT’s ISD requirements for posted speed limit of 30 mph.

^c Values based on AASHTO requirements for vehicles driving at 85th percentile speeds; Washington Avenue westbound (east of intersection) = 37 mph, Washington Avenue eastbound (west of intersection) = 34 mph, Ocean Avenue southbound (north of intersection) = 29 mph, Ocean Avenue northbound (south of intersection) = 27 mph.

As indicated in Table 4, available sight distances at the site driveway intersections with Washington Street and with Ocean Avenue exceed the AASHTO, City of Portland, and MaineDOT SSD and ISD requirements for safe operation based on the posted and observed speeds. To ensure the safe and efficient flow of traffic to and from the site, it is recommended that any proposed plantings, vegetation, landscaping, and signing along the site frontage be kept low to the ground (no more than 3.0 feet above street level) or set back sufficiently from the edge of Washington Avenue, Ocean Avenue, and the site driveways so as not to inhibit the available sight lines.

FUTURE CONDITIONS

To estimate the impact of site-generated traffic within the study area, existing traffic volumes were projected to the expected opening year (2012) of the development. This design horizon was chosen to be consistent with City of Portland and MaineDOT guidelines for the preparation of a traffic study. Traffic volumes on the roadway network at this time will include existing traffic, new traffic due to normal traffic growth, and traffic related to any significant development by others expected to be completed in the area within the 2012 design horizon. Consideration of these factors resulted in the development of 2012 No-Build traffic volumes, which assume that the proposed development is not built. The incremental impacts of the proposed project may then be determined by adding site-generated traffic volumes (Build conditions) and making comparisons to the No-Build conditions.

Traffic Growth

To develop the 2012 No-Build forecast volumes, two components of traffic growth were considered. First, an annual average traffic-growth percentage was determined. Based on historical traffic-volume data researched from MaineDOT, traffic volumes have been increasing at an average rate of 1.15 percent per year since 2000.³ Based on this information and as confirmed with City of Portland officials, a compounded annual traffic growth rate of 1.5 percent was assumed to account for general population growth and traffic generated by smaller area developments. The MaineDOT historical traffic-volume data are provided in the Appendix.

Second, any planned or approved specific developments in the area that would generate a significant volume of traffic on study area roadways by the 2012 design year were included. Based on discussions with officials from City of Portland and MaineDOT, there are no developments planned to be constructed within the design horizon that would affect traffic volumes within the study area. A traffic study was prepared for the Cheverus High School Master Plan that projected an expansion to the school's enrollment from 544 students in 2008 to 700 students by 2013.⁴ Based on discussions with Cheverus High School administrators, the current enrollment in the 2010-2011 school year is 499 students and there are no current plans to increase enrollment much further by 2012. The original Master Plan anticipated changing from an all-male enrollment to a co-educational student body, but school administrators indicated that this change is not expected to occur within the design horizon and may not be feasible at the current location.

³ *Traffic Volume Counts – 2009 Annual Report*; Maine Department of Transportation.

⁴ *Traffic Impact Study, Proposed Cheverus High School Master Plan*; Portland, Maine; Eaton Traffic Engineering; August 22, 2008.

The 2012 No-Build peak-hour traffic volumes were accordingly developed by applying a 1.5 percent compounded annual traffic-growth rate to the 2009 Existing traffic volumes. The 2012 No-Build weekday AM, weekday PM, and Saturday midday peak-hour traffic volumes are shown graphically on Figures 5 through 7, respectively.

Planned Roadway Improvements

Officials from the City of Portland and MaineDOT were contacted to determine if any roadway improvements are planned within the study area. At this time, there are no roadway improvement projects planned within the study area that would be implemented within the design horizon.

Trip Generation

The site currently contains a Cumberland Farms facility that includes a ±2,400 square foot convenience store with 4 fuel dispensers (8 vfps). As proposed, the project consists of maintaining the existing fueling pumps and expanding the existing convenience store by 1,474 square feet for a total of 3,874 square feet. Traffic to be generated by the proposed expansion project was forecast using the trip rates contained in the Institute of Transportation Engineers (ITE) *Trip Generation* report.⁵

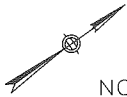
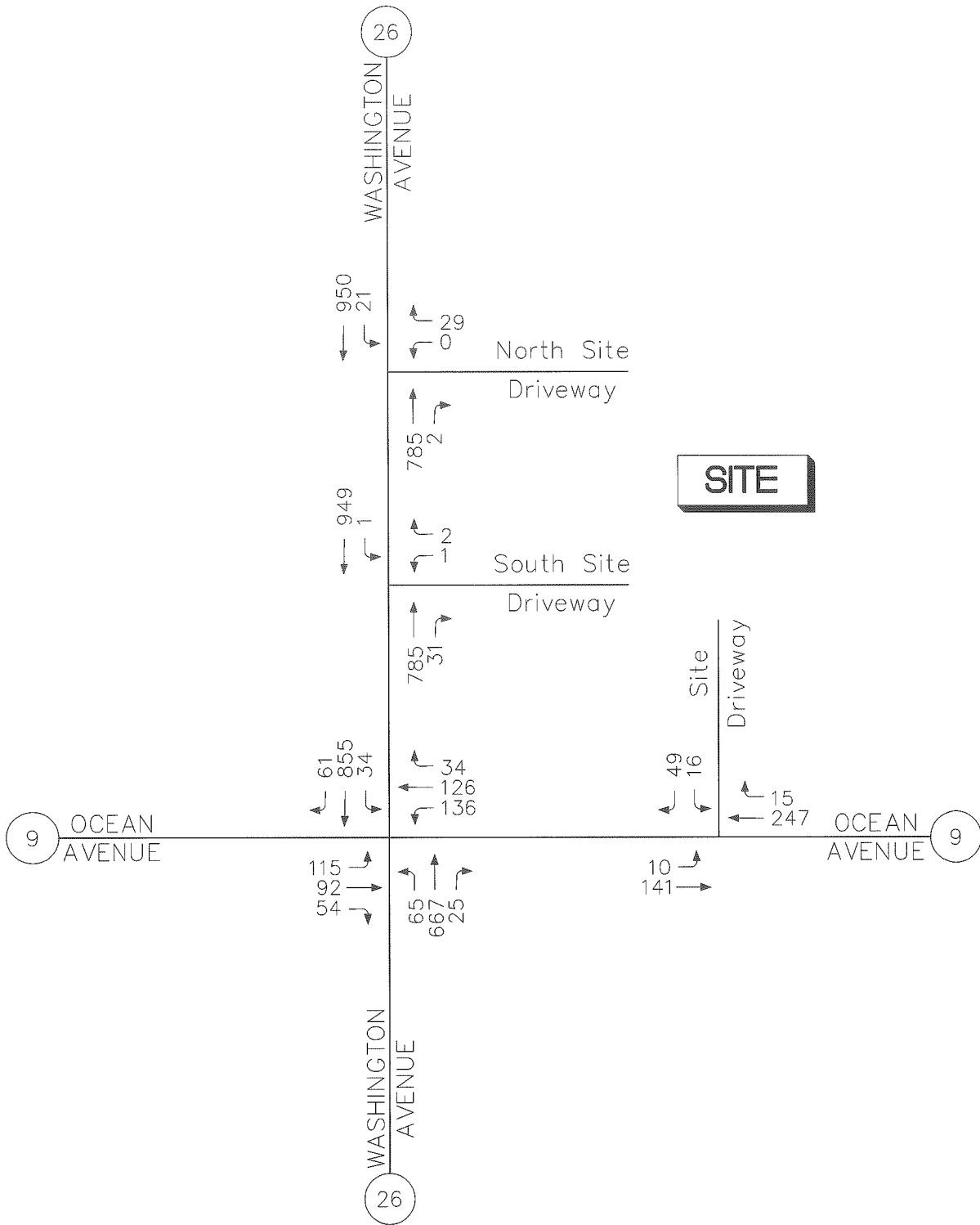
Not all of the vehicle trips expected to be generated by the proposed expansion project represent *new* trips on the study area roadway system. Studies have shown that for developments such as the one proposed, a substantial portion of the site-generated vehicle trips are already present in the adjacent passing stream of traffic or are diverted from another route to the proposed site. Based on information published in the ITE *Trip Generation Handbook*, the average *pass-by* trip percentage is 63 percent during the weekday AM peak hour and 66 percent during the weekday PM peak hour for Convenience Markets with Gasoline Pumps (Land Use Code 853).⁶ Table 5 provides a trip-generation summary for the additional traffic anticipated to be generated by the proposed expansion project. All trip-generation data are provided in the Appendix.

⁵ *Trip Generation, 8th Edition: an ITE Informational Report*. Washington, DC: Institute of Transportation Engineers, 2008.

⁶ Hooper, Kevin G. *Trip Generation Handbook: an ITE Recommended Practice*. Washington, D.C.: Institute of Transportation Engineers, 2004.

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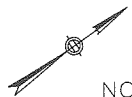
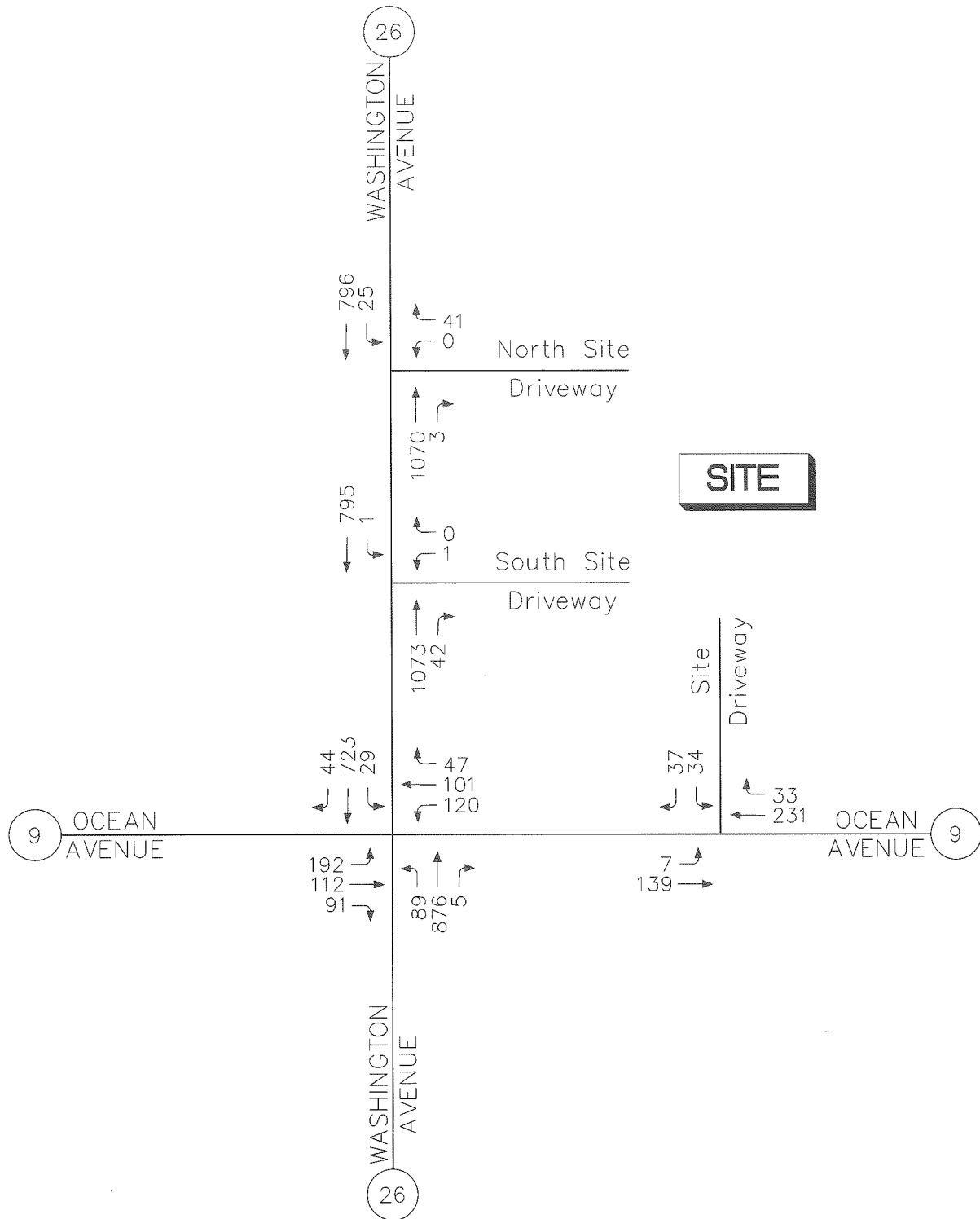
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Figure 5

2012 No-Build
Weekday AM
Peak Hour Traffic Volumes

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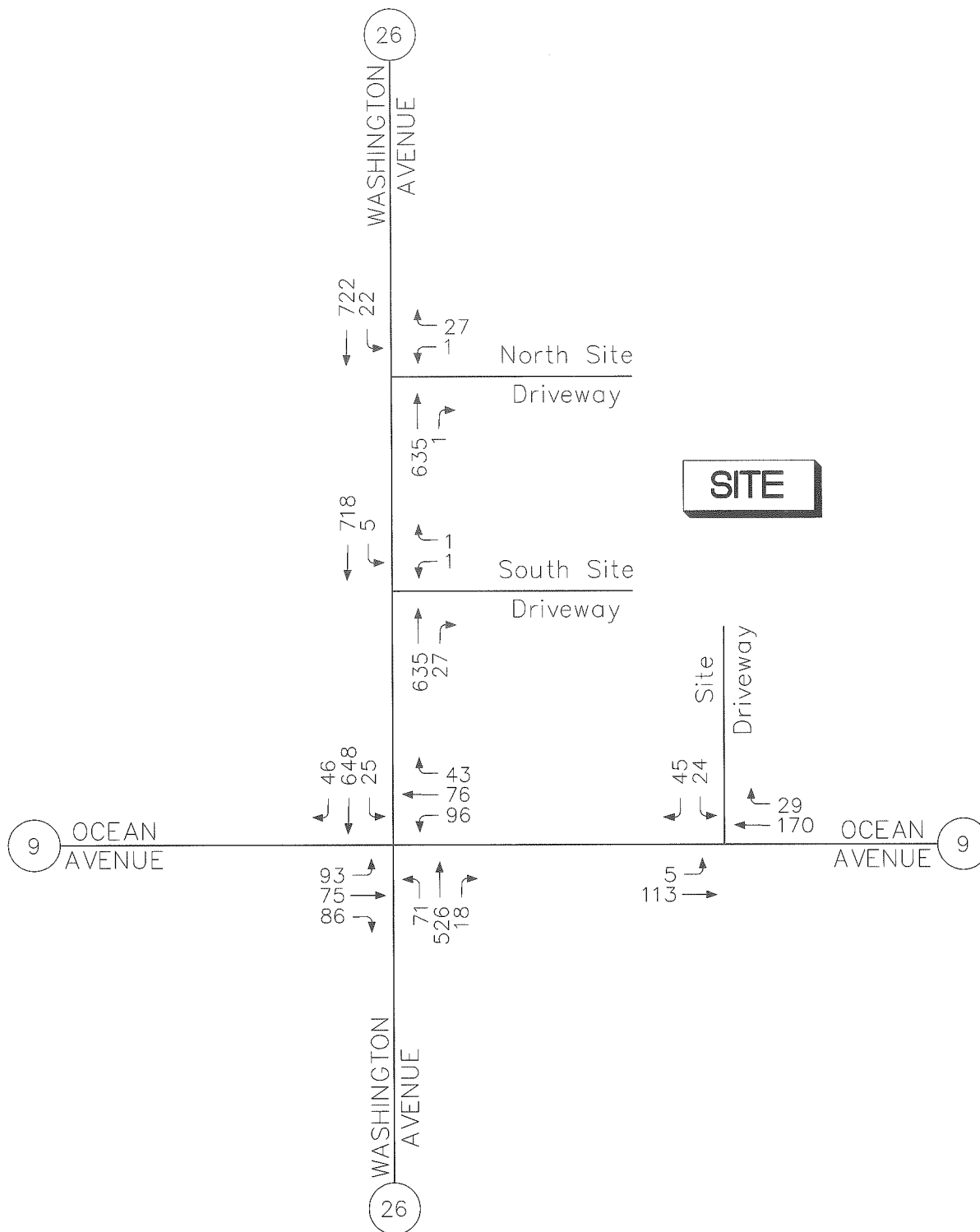
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Figure 6

2012 No-Build
Weekday PM
Peak Hour Traffic Volumes

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NOT TO SCALE

Figure 7

2012 No-Build
Saturday Midday
Peak Hour Traffic Volumes

Table 5
TRIP-GENERATION SUMMARY – Additional Trips

Time Period/Direction	Total Additional Trips ^a	Pass-By Trips ^b	New Trips ^c
Weekday AM Peak Hour:			
<i>In</i>	33	20	13
<i>Out</i>	<u>32</u>	<u>20</u>	<u>12</u>
<i>Total</i>	65	40	25
Weekday PM Peak Hour:			
<i>In</i>	44	29	15
<i>Out</i>	<u>44</u>	<u>29</u>	<u>15</u>
<i>Total</i>	88	58	30
Saturday Midday Peak Hour:			
<i>In</i>	35	21	14
<i>Out</i>	<u>33</u>	<u>21</u>	<u>12</u>
<i>Total</i>	68	42	26

^a ITE Land Use Code 853 (Convenience Market with Gasoline Pumps) for ±1,480 sf – rounded up for conservative (worse-case) scenario.

^b 63% of Total Additional Trips during the weekday AM and Saturday midday peak hours, and 66% during the weekday PM peak hour.

^c Total Additional Trips minus Pass-By Trips.

As shown, the proposed Cumberland Farms expansion is expected to generate 25 additional *new* vehicle trips (13 entering and 12 exiting) during the weekday AM peak hour, 30 additional *new* vehicle trips (15 entering and 15 exiting) during the weekday PM peak hour, and 226 additional *new* vehicle trips (14 entering and 12 exiting) during the Saturday midday peak hour.

To determine the number of PCE trips for the expected trip-generation change associated with the expansion project, data were provided by Cumberland Gulf Group of Companies. Currently, there are two truck deliveries that occur at the facility and they take place on Mondays and Thursdays between 1 and 2 PM. At this time, there are not expected to be additional truck deliveries associated with the proposed expansion project. If additional deliveries are required, the trucks would be scheduled to arrive outside of the peak periods (i.e., not between 7-9 AM or 4-6 PM on a weekday and not between 11 AM -2 PM on a Saturday). Therefore, the projected trips reflected in Table 5 also reflect the total additional PCE trips that can be expected as part of the proposed Cumberland Farms expansion project. Since the proposed expansion project is expected to generate less than 100 additional PCE trips during the peak hours of traffic generation and there will not be a change in use from the existing site, the project is not expected to require a *Traffic Movement Permit* from MaineDOT.

Trip Distribution

Having estimated project-generated vehicle trips, the next step was to determine the distribution of project traffic and assign these trips to the local roadway network. Since the project consists of expanding the existing facility with the same uses, the distribution of proposed additional site traffic on the area roadways was based on existing travel patterns at the site driveways. Accordingly, approximately 30 percent of the additional site traffic is expected to and from the north on Washington Avenue, 30 percent is expected to and from the south on Washington Avenue, 30 percent is expected to and from the east on Ocean Avenue, and 10 percent is expected to and from the west on Ocean Avenue. The distribution of *pass-by* traffic is expected to follow the directional distribution of adjacent street traffic observed during the various peak hours on Washington Avenue and on Ocean Avenue.

Build Conditions

Based on the traffic-generation and distribution estimates for this project, the traffic volumes associated with the proposed Cumberland Farms expansion were assigned to the roadway network as shown on Figures 8 through 10. The site-generated traffic volumes were then added to the No-Build traffic volumes to develop the Build traffic volumes. The 2012 Build weekday AM, weekday PM, and Saturday midday peak-hour traffic-flow networks are graphically depicted on Figures 11 through 13, respectively.

Traffic Increases

The proposed expansion project will result in increases in traffic on the roadways leading beyond the study area. As shown on Figures 8 through 10, peak-hour traffic-volume increases beyond the study area are expected to be in the range of 3 to 9 vehicles per hour. These increases represent approximately 1 additional vehicle every 6.5 to 20 minutes during the peak hours.

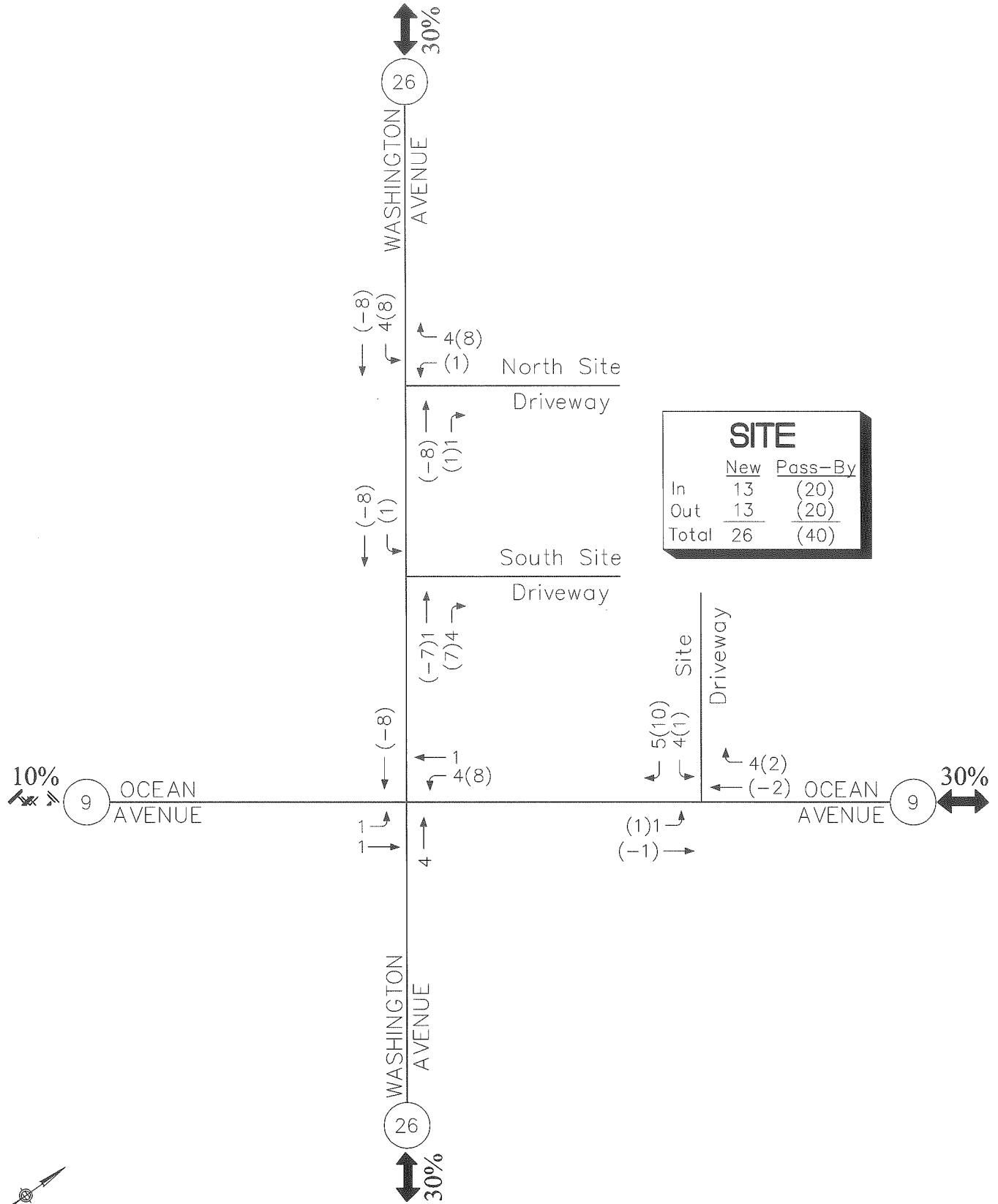
CAPACITY AND QUEUE ANALYSIS

Methodology

Level-of-service analyses were conducted at the study-area intersections under 2009 Existing, 2012 No-Build, and 2012 Build conditions during the weekday AM, weekday PM, and Saturday

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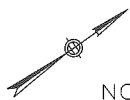
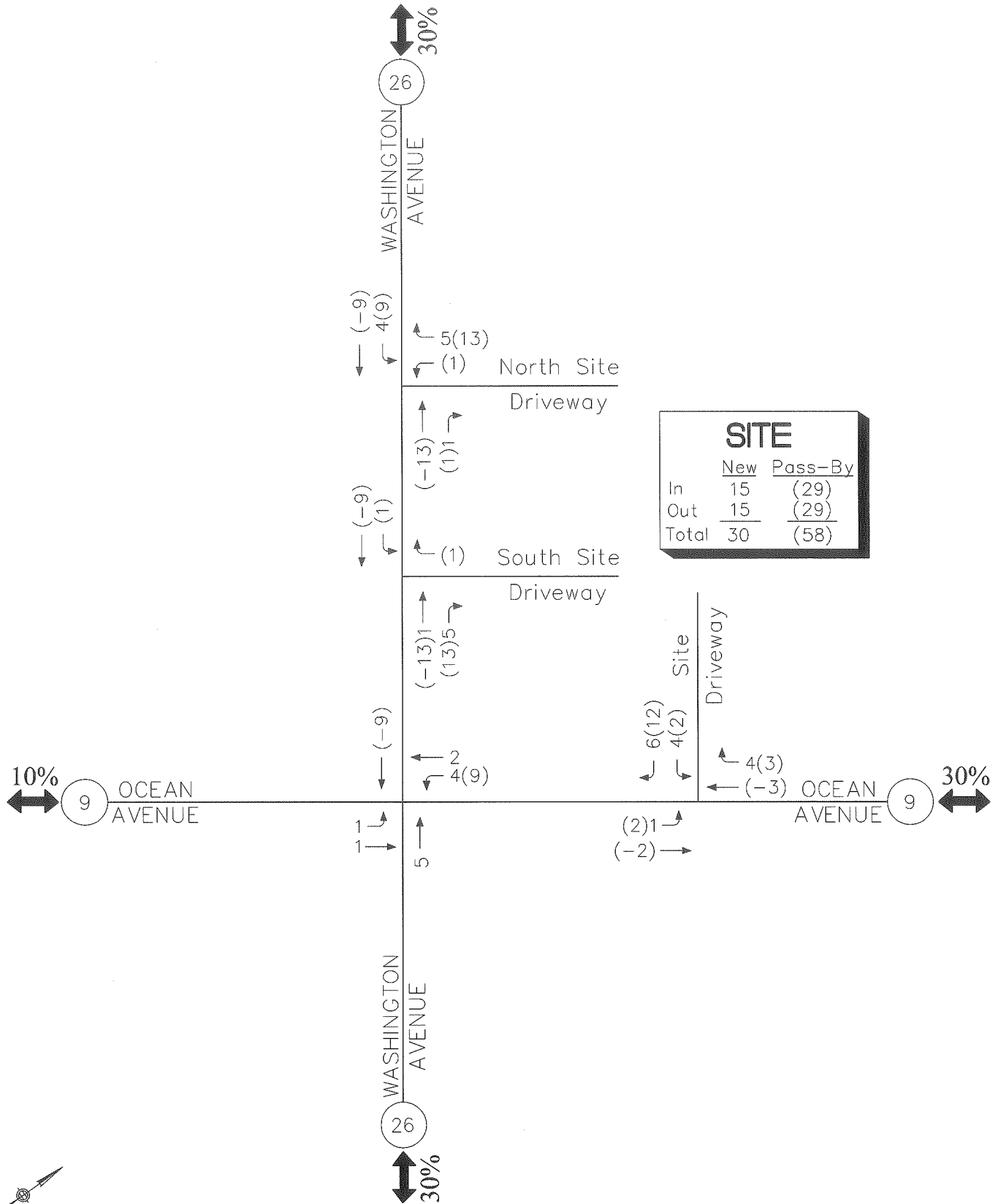
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Figure 8

Additional Site Generated Trips
Weekday AM
Peak Hour Traffic Volumes

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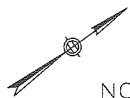
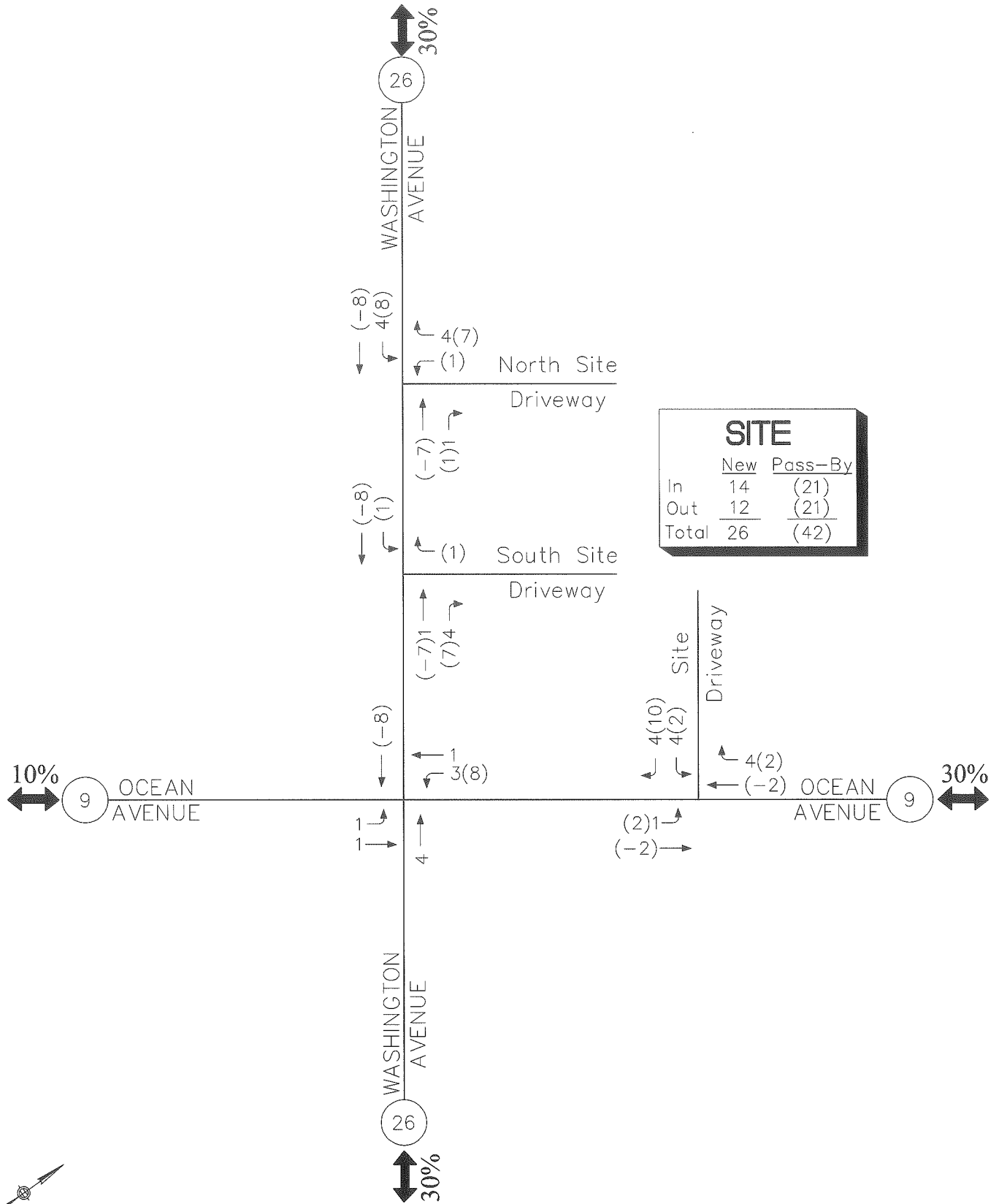
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Figure 9

Additional Site Generated Trips
Weekday PM
Peak Hour Traffic Volumes

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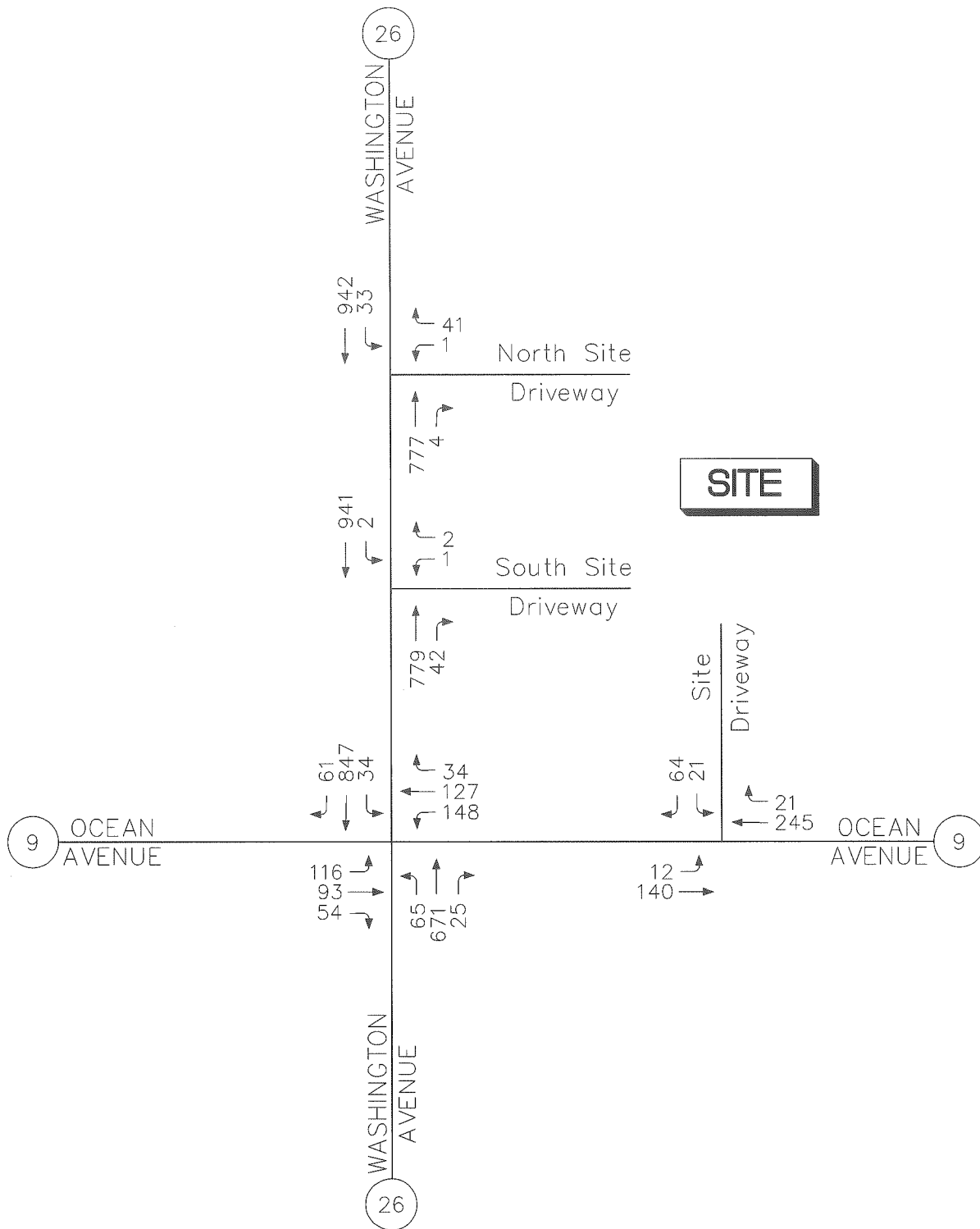
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Figure 10

Additional Site Generated Trips
Saturday Midday
Peak Hour Traffic Volumes

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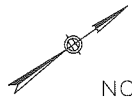
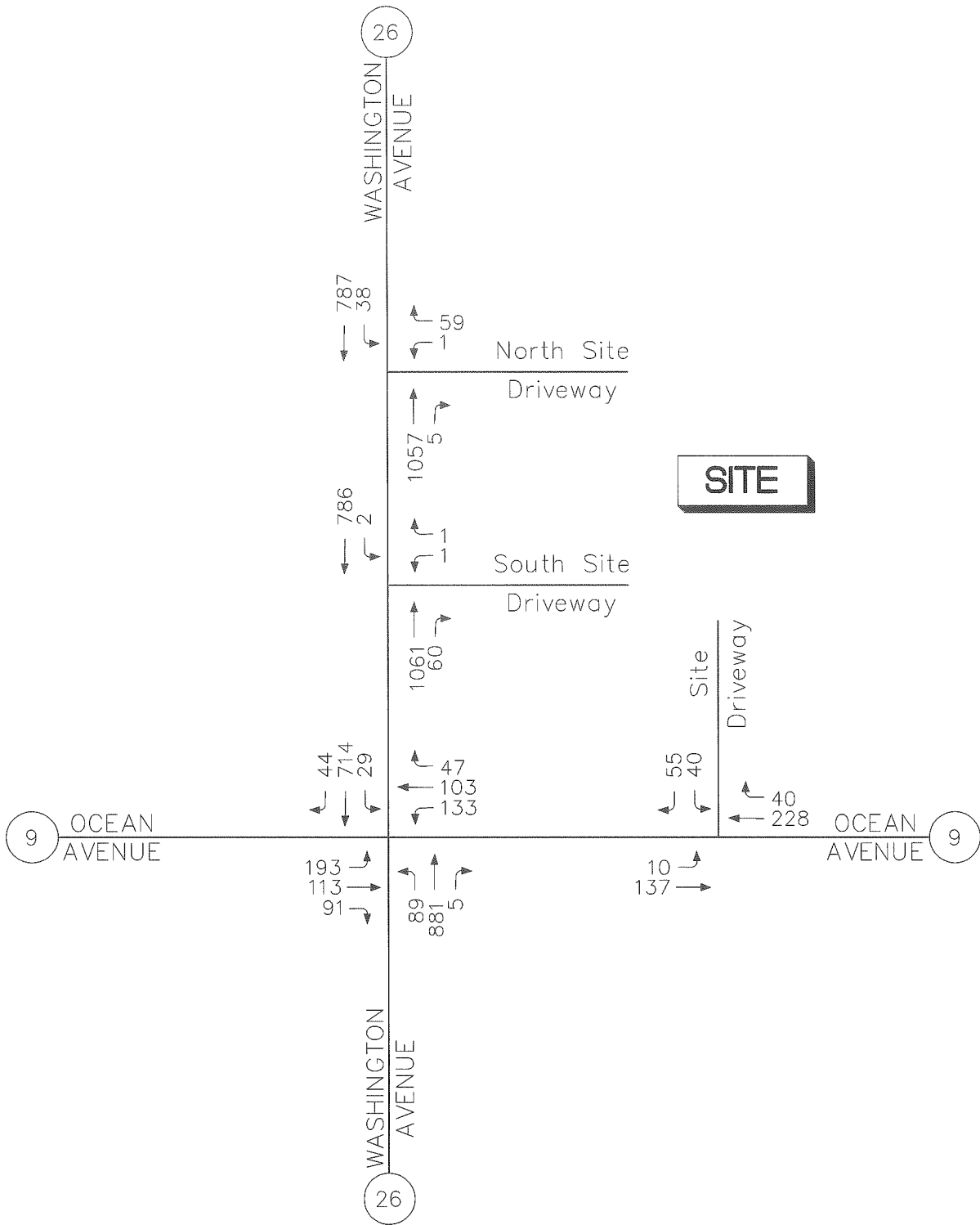
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Figure 11

2012 Build
Weekday AM
Peak Hour Traffic Volumes

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Proposed Cumberland Farms Expansion - Portland, Maine



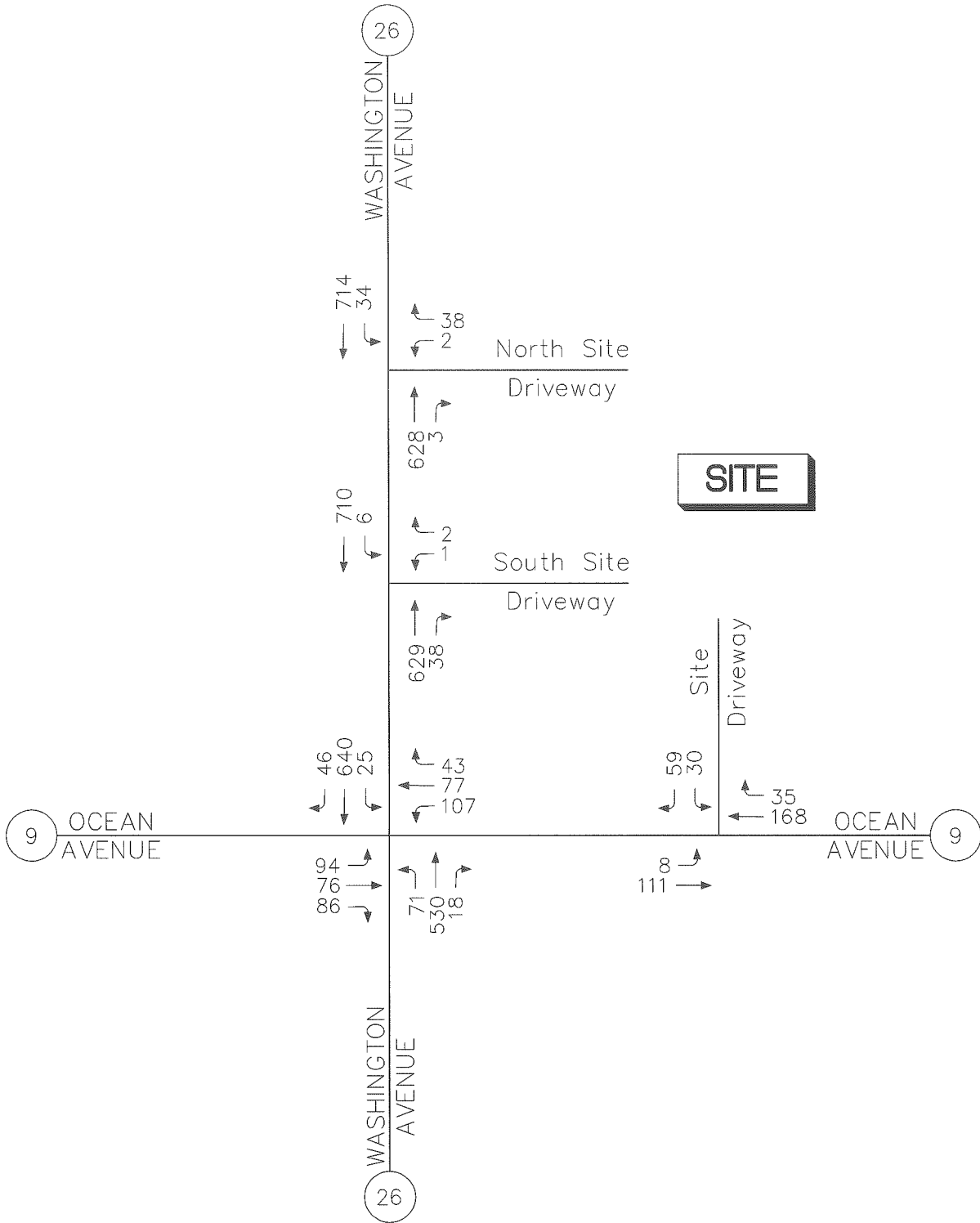
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Figure 12

2012 Build
Weekday PM
Peak Hour Traffic Volumes

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Proposed Cumberland Farms Expansion - Portland, Maine



NOT TO SCALE

Figure 13

2012 Build
Saturday Midday
Peak Hour Traffic Volumes

midday peak hours. The capacity and queue analysis methodology is based on the concepts and procedures in the *Highway Capacity Manual* (HCM) and is described in the Appendix.⁷

The queue analysis methodology for signalized and unsignalized intersections is based on the concepts and procedures described in the HCM. The maximum back of queue during a 95th percentile signal cycle were calculated for each critical lane group during the peak periods studied. The back of queue is the length of a backup of vehicles from the STOP-line of a signalized intersection to the last vehicle in the queue that is required to stop, regardless of the signal indication. The length of this queue depends on a number of factors including signal timing, vehicle arrival patterns, and the saturation flow rate.

For unsignalized intersections, the 95th percentile queue represents the length of queue of the critical minor-street movement that is not expected to be exceeded 95 percent of the time during the analysis period (typically one hour). In this case, the queue length is a function of the capacity of the movement and the movement's degree of saturation.

Analysis Results

The results of the level-of-service and queue analyses are shown in Table 6 for the study area intersections. All analysis worksheets are provided in the Appendix.

Washington Avenue and Ocean Avenue

Under existing and future traffic-volume conditions, the Washington Avenue and Ocean Avenue signalized intersection is expected to operate at overall acceptable levels (LOS D or better) during the weekday AM, weekday PM, and Saturday midday peak hours. The proposed expansion project is expected to have minimal impact on the signalized intersection with an increase in overall delay of 1.2 seconds or less during the peak hours and increases in queue lengths of less than 1 vehicle on any lane group. Traffic volume increases at this location are expected to be approximately 1 additional vehicle every 4.5 to 6 minutes during the peak hours.

Washington Avenue at North and South Site Driveways

The Washington Avenue approaches to the intersections with the Cumberland Farms north and south site driveways are expected to operate at optimal levels (LOS A) under existing and future traffic-volume conditions during the weekday AM, weekday PM, and Saturday midday peak hours. All delays are expected to occur on the site and not affect traffic flow along Washington Avenue. Volume-to-capacity ratios on the Cumberland Farms driveways are expected to be well

⁷*Highway Capacity Manual 2000*; Transportation Research Board; Washington, D.C.; 2000.

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below 1.00 and queues are not anticipated to exceed 2 vehicles during the peak hours, indicating that ample capacity will remain after the proposed expansion project is constructed and occupied.

Ocean Avenue and Site Driveway

The Ocean Avenue approaches to the intersections with the Cumberland Farms site driveway are expected to operate at optimal levels (LOS A) under existing and future traffic-volume conditions during the weekday AM, weekday PM, and Saturday midday peak hours. The Cumberland Farms site driveway is expected to operate at desirable levels (LOS B) under existing and future traffic-volume conditions during the peak hours. Volume-to-capacity ratios on the Cumberland Farms driveway are expected to be well below 1.00 and queues are not anticipated to exceed 1 vehicle during the peak hours, indicating that ample capacity will remain after the proposed expansion project is constructed and occupied.

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**Table 6
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY**

Intersection/Peak Hour/Lane Group	2009 Existing				2012 No-Build			2012 Build				
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
Washington Avenue and Ocean Avenue												
<i>Weekday AM:</i>												
Ocean Ave. EB left-turns	0.56	37.2	D	76/137	0.58	38.0	D	64/138	0.61	38.5	D	73/140
Ocean Ave. EB through/right-turns	0.63	43.4	D	87/158	0.65	44.9	D	102/172	0.72	50.2	D	95/167
Ocean Ave. WB left-turns	0.61	33.9	C	27/78	0.65	35.5	D	42/94	0.74	44.0	D	48/95
Ocean Ave. WB through/right-turns	0.61	39.4	D	78/99	0.63	40.1	D	81/107	0.73	48.8	D	84/110
Washington Ave. NB left-turns	0.44	21.3	C	39/74	0.47	22.0	C	34/85	0.47	21.8	C	45/84
Washington Ave. NB through/right-turns	0.76	20.7	C	178/243	0.80	22.3	C	186/237	0.79	21.4	C	183/245
Washington Ave. SB left-turns	0.15	11.9	B	18/48	0.17	12.6	B	19/46	0.16	12.1	B	18/47
Washington Ave. SB through/right-turns	0.99	46.9	D	48/74	1.03	59.7	E	44/77	1.01	53.1	D	49/79
Overall Intersection	0.88	35.6	D	--	0.93	41.6	D	--	0.87	40.1	D	--
<i>Weekday PM:</i>												
Ocean Ave. EB left-turns	0.96	84.3	F	128/183	1.03	105.6	F	136/182	1.04	108.8	F	135/181
Ocean Ave. EB through/right-turns	0.96	90.0	F	138/205	1.04	117.0	F	136/208	1.04	117.0	F	135/207
Ocean Ave. WB left-turns	0.67	41.8	D	61/77	0.71	46.1	D	60/80	0.81	57.4	E	62/80
Ocean Ave. WB through/right-turns	0.83	66.3	E	84/110	0.90	82.9	F	87/115	0.92	88.1	F	91/123
Washington Ave. NB left-turns	0.39	13.0	B	46/86	0.44	14.1	B	46/84	0.42	13.6	B	48/85
Washington Ave. NB through/right-turns	0.91	29.7	C	194/233	0.94	32.9	C	195/231	0.94	33.2	C	197/233
Washington Ave. SB left-turns	0.20	15.1	B	17/41	0.22	16.3	B	17/41	0.23	16.5	B	19/42
Washington Ave. SB through/right-turns	0.79	20.3	C	35/53	0.81	21.2	C	36/57	0.80	20.4	C	33/51
Overall Intersection	0.79	38.2	D	--	0.83	44.8	D	--	0.83	46.0	D	--

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic; unsignalized intersection queues analyzed using Synchro 7.

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**Table 6 (continued)
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY**

Intersection/Peak Hour/Lane Group	2009 Existing			2012 No-Build			2012 Build					
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
Washington Avenue and Ocean Avenue												
<i>Saturday Midday:</i>												
Ocean Ave. EB left-turns	0.47	26.8	C	51/100	0.50	27.5	C	57/107	0.50	27.5	C	51/100
Ocean Ave. EB through/right-turns	0.67	39.3	D	70/129	0.73	44.4	D	81/144	0.74	45.8	D	83/143
Ocean Ave. WB left-turns	0.51	27.5	C	43/85	0.53	28.5	C	34/81	0.59	30.7	C	44/85
Ocean Ave. WB through/right-turns	0.61	35.4	D	67/96	0.65	38.5	D	70/102	0.66	39.3	D	64/98
Washington Ave. NB left-turns	0.30	10.4	B	38/72	0.34	11.1	B	37/70	0.33	10.9	B	39/68
Washington Ave. NB through/right-turns	0.62	13.4	B	132/219	0.64	13.9	B	133/227	0.65	14.0	B	138/223
Washington Ave. SB left-turns	0.08	8.7	A	16/46	0.09	8.7	A	19/50	0.09	8.8	A	16/46
Washington Ave. SB through/right-turns	0.81	21.1	C	37/58	0.84	22.6	C	36/57	0.83	22.2	C	38/60
Overall Intersection	0.73	21.4	C	--	0.76	22.9	C	--	0.76	23.1	C	--

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic, unsignalized intersection queues analyzed using Synchro 7.

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Proposed Cumberland Farms Expansion – Portland, Maine

**Table 6 (continued)
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY**

Intersection/Peak Hour/Lane Group	2009 Existing				2012 No-Build				2012 Build			
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
Washington Avenue and North Site Driveway												
<i>Weekday AM:</i>												
Site Driveway WB left-turns/right-turns	0.08	15.4	C	--/7	0.09	16.1	C	--/7	0.14	17.6	C	--/12
Washington Ave. NB through/right-turns	0.48	0.0	A	--/0	0.50	0.0	A	--/0	0.50	0.0	A	--/0
Washington Ave. SB left-turns/through	0.03	1.1	A	--/3	0.03	1.1	A	--/3	0.05	1.7	A	--/4
Washington Ave. SB through	0.39	0.0	A	--/0	0.40	0.0	A	--/0	0.40	0.0	A	--/0
<i>Weekday PM:</i>												
Site Driveway WB left-turns/right-turns	0.23	29.0	D	--/21	0.27	34.6	D	--/26	0.42	44.1	E	--/47
Washington Ave. NB through/right-turns	0.66	0.0	A	--/0	0.69	0.0	A	--/0	0.68	0.0	A	--/0
Washington Ave. SB left-turns/through	0.06	2.3	A	--/5	0.07	2.6	A	--/6	0.11	3.8	A	--/9
Washington Ave. SB through	0.32	0.0	A	--/0	0.34	0.0	A	--/0	0.34	0.0	A	--/0
<i>Saturday Midday:</i>												
Site Driveway WB left-turns/right-turns	0.07	13.6	B	--/5	0.07	14.1	B	--/6	0.10	14.5	B	--/9
Washington Ave. NB through/right-turns	0.39	0.0	A	--/0	0.41	0.0	A	--/0	0.40	0.0	A	--/0
Washington Ave. SB left-turns/through	0.03	1.1	A	--/2	0.03	1.1	A	--/2	0.04	1.6	A	--/3
Washington Ave. SB through	0.29	0.0	A	--/0	0.31	0.0	A	--/0	0.30	0.0	A	--/0

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic, unsignalized intersection queues analyzed using Synchro 7.

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Proposed Cumberland Farms Expansion – Portland, Maine

**Table 6 (continued)
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY**

Intersection/Peak Hour/Lane Group	2009 Existing			2012 No-Build			2012 Build					
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
Washington Avenue and South Site Driveway												
<i>Weekday AM:</i>												
Site Driveway WB left-turns/right-turns	0.02	23.1	C	--/1	0.02	25.8	D	--/1	0.02	25.7	D	--/1
Washington Ave. NB through/right-turns	0.50	0.0	A	--/0	0.52	0.0	A	--/0	0.52	0.0	A	--/0
Washington Ave. SB left-turns/through	0.00	0.1	A	--/0	0.00	0.1	A	--/0	0.00	0.1	A	--/0
Washington Ave. SB through	0.39	0.0	A	--/0	0.40	0.0	A	--/0	0.40	0.0	A	--/0
<i>Weekday PM:</i>												
Site Driveway WB left-turns/right-turns	0.03	98.2	F	--/2	0.04	143.5	F	--/3	0.05	87.3	F	--/4
Washington Ave. NB through/right-turns	0.68	0.0	A	--/0	0.71	0.0	A	--/0	0.72	0.0	A	--/0
Washington Ave. SB left-turns/through	0.00	0.1	A	--/0	0.00	0.1	A	--/0	0.01	0.2	A	--/1
Washington Ave. SB through	0.32	0.0	A	--/0	0.34	0.0	A	--/0	0.34	0.0	A	--/0
<i>Saturday Midday:</i>												
Site Driveway WB left-turns/right-turns	0.01	17.7	C	--/1	0.01	18.8	C	--/1	0.01	16.9	C	--/1
Washington Ave. NB through/right-turns	0.41	0.0	A	--/0	0.42	0.0	A	--/0	0.43	0.0	A	--/0
Washington Ave. SB left-turns/through	0.01	0.3	A	--/0	0.01	0.3	A	--/0	0.01	0.3	A	--/1
Washington Ave. SB through	0.29	0.0	A	--/0	0.31	0.0	A	--/0	0.30	0.0	A	--/0

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic, unsignalized intersection queues analyzed using Synchro 7.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

**Table 6 (continued)
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY**

Intersection/Peak Hour/Lane Group	2009 Existing				2012 No-Build				2012 Build			
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
Ocean Avenue and Site Driveway												
<i>Weekday AM:</i>												
Ocean Ave. EB left-turns/through	0.01	0.6	A	--/1	0.01	0.6	A	--/1	0.01	0.7	A	--/1
Ocean Ave. WB through/right-turns	0.16	0.0	A	--/0	0.17	0.0	A	--/0	0.17	0.0	A	--/0
Site Driveway SB left-turns/right-turns	0.10	10.6	B	--/8	0.10	10.7	B	--/8	0.13	10.9	B	--/11
<i>Weekday PM:</i>												
Ocean Ave. EB left-turns/through	0.01	0.4	A	--/0	0.01	0.4	A	--/0	0.01	0.6	A	--/1
Ocean Ave. WB through/right-turns	0.16	0.0	A	--/0	0.17	0.0	A	--/0	0.17	0.0	A	--/0
Site Driveway SB left-turns/right-turns	0.11	10.9	B	--/9	0.11	11.0	B	--/10	0.15	11.2	B	--/13
<i>Saturday Midday:</i>												
Ocean Ave. EB left-turns/through	0.00	0.4	A	--/0	0.00	0.4	A	--/0	0.01	0.6	A	--/0
Ocean Ave. WB through/right-turns	0.12	0.0	A	--/0	0.13	0.0	A	--/0	0.13	0.0	A	--/0
Site Driveway SB left-turns/right-turns	0.10	10.2	B	--/8	0.10	10.2	B	--/8	0.13	10.4	B	--/11

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic, unsignalized intersection queues analyzed using Synchro 7.

CONCLUSIONS

Existing and future conditions in the study area have been described, analyzed, and evaluated with respect to traffic operations and the impact of the proposed Cumberland Farms expansion project. Conclusions of this effort are presented below.

- The Cumberland Farms facility currently consists of a ±2,400 square foot convenience store with 8 vfps. The proposed project consists of maintaining the existing fueling pumps and expanding the existing convenience store by 1,474 square feet for a total of 3,874 square feet.
- Access to the site is currently provided via two driveways on the east side of Washington Avenue and one driveway on the north side of Ocean Avenue. As proposed, the three driveways will be maintained.
- Available sight distances at the site driveways exceed the minimum SSD and ISD requirements for safe operation based on the posted and observed speeds. To ensure the safe and efficient flow of traffic to and from the site, it is recommended that any proposed plantings, vegetation, landscaping, and signing along the site frontage be kept low to the ground (no more than 3.0 feet above street level) or set back sufficiently from the edge of Washington Avenue, Ocean Avenue, and the site driveways so as not to inhibit the available sight lines.
- There are two truck deliveries that occur at the facility and they take place on weekdays outside of the commuter peak periods. At this time, there are not expected to be additional truck deliveries associated with the proposed expansion project. Therefore, the total additional PCE trips that can be expected as part of the proposed Cumberland Farms expansion project are 65 total additional PCE trips during the weekday AM peak hour, 88 total additional PCE trips during the weekday PM peak hour, and 68 total additional PCE trips during the Saturday midday peak hour. Since the proposed expansion project is expected to generate less than 100 total additional PCE trips during the peak hours of traffic generation and there will not be a change in use from the existing site, the project is not expected to require a *Traffic Movement Permit* from MaineDOT.
- Increases in traffic volumes on roadways leading beyond the study area as a result of the project are expected in the range of 3 to 9 vehicles per hour during the weekday AM, weekday PM, and Saturday midday peak hours. These increases represent approximately 1 additional vehicle every 6.5 to 20 minutes during the peak hours.
- The proposed expansion project is expected to have a minimal impact at the Washington Avenue and Ocean Avenue signalized intersection with an increase in overall intersection delay of 1.2 seconds or less during the peak hours (LOS D or better) and increases in queue

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

length of less than 1 vehicle on any lane group. Traffic volume increases at this location are expected to be approximately 1 additional vehicle every 4.5 to 6 minutes during the peak hours.

- The Washington Avenue approaches to the intersections with the Cumberland Farms north and south site driveways are expected to operate at optimal levels (LOS A) under existing and future traffic-volume conditions. All delays are expected to occur on the site and not affect traffic flow along Washington Avenue. Volume-to-capacity ratios on the Cumberland Farms driveways are expected to be well below 1.00 and queues are not anticipated to exceed 2 vehicles, indicating that ample capacity will remain after the proposed expansion project is constructed and occupied.
- The Ocean Avenue approaches to the intersections with the Cumberland Farms site driveway are expected to operate at optimal levels (LOS A) under existing and future traffic-volume conditions. The Cumberland Farms site driveway is expected to operate at desirable levels (LOS B) under existing and future traffic-volume conditions. Volume-to-capacity ratios on the Cumberland Farms driveway are expected to be well below 1.00 and queues are not anticipated to exceed 1 vehicle, indicating that ample capacity will remain after the proposed expansion project is constructed and occupied.

PETRUCCELLI, MARTIN & HADDOW, LLP

Attorneys at Law

Two Monument Square, Suite 900
Post Office Box 17555
Portland, Maine 04112-8555

JISEL E. LOPEZ
jlopez@pmhlegal.com

April 2, 2012

Via Hand Delivery

Ms. Shukria Wiar, Planner
Planning & Urban Development Dept.
389 Congress Street, 4th Floor
Portland, ME 04101-3509

Re: Cumberland Farms, Inc.—801 Washington Avenue

Dear Shukria:

As you know, Cumberland Farms filed its application for Level II Site Plan and conditional use review in connection with the proposed expansion of its store at 801 Washington Avenue on January 23, 2012. In response to the comments and concerns of the planning staff and the Planning Board after the recent workshop, Cumberland Farms has made the following revisions to its application:

- Parking space dimensions have been revised to meet City standards (9' x 18'). Two spaces will remain unchanged from the initial proposal (9' x 19') due to the need to accommodate the Red Box kiosk and sidewalk extension; a formal waiver request has been submitted with respect to these two spaces (see below).
- A 6' white vinyl fence has been added along the rear of the property and along the side of the property abutting residential uses.
- Plans have been revised to include the requested information regarding the concrete walkway, bituminous pavement section and stabilized construction exit.
- Planning staff requested additional information regarding the drainage off the property and the southerly property coordinates and the plans have been revised to address these areas.
- Snow storage is shown on the site plans.
- Lighting has been revised to meet City standards and address the spillover on adjacent lots. Catalogue cut sheets are enclosed.

Ms. Shukria Wiar, Planner

April 2, 2012

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- In her review of the application, Ms. Schmuckal indicated that the proposed front yard setback exceeded the maximum setback in the B-2 zone but that the Planning Board could approve a further setback upon request as set forth in Section 14-526(a)(27)(j). While the plans have not been revised to reflect this, we do want to make it clear that we are seeking this approval from the Board.
- The Department of Public Services has recommended the construction of an esplanade along Washington Avenue. Cumberland Farms will not be installing an esplanade since this represents an added expense and is not required under the Ordinance. Cumberland Farms is, however, willing to revisit this issue if the Board considers the esplanade to be essential to approval of the project.
- As requested by the Planning staff, Cumberland Farms will work with the Maine DEP and the City of Portland Fire Department to ensure that the new underground tanks are properly registered and that the old tanks are safely removed. Cumberland Farms will obtain the necessary permits from both agencies prior to the start of any work on the project.
- The existing HVAC units will remain in the store and the site plan has been updated to reflect this.
- Cumberland Farms is not proposing any new traffic or wayfinding signs and will therefore not be submitting a separate site plan. Existing signs have been marked on the submitted plans.
- As requested by the abutters to the rear of the property, Cumberland Farms will be restricting the dumpster pickup times. All pickups will now be restricted to between the hours of 7am and 7pm. Cumberland Farms has also added a 6' fence along the rear of the property (see above) to address the abutters' safety concerns.
- Both the Planning Board and the abutters had raised concerns about the side of the building being used as a "public bathroom." To address this issue, Cumberland Farms has added a fence along the side of the property to prevent easy access (see above). Additional lighting has not been included in an effort to avoid increased spillover to the residential abutters.
- There will be no Blue Rhino propane cylinder exchange at the site.
- Since rooftop signs are prohibited in the City, any such signs have been moved to below the roofline and the plans have been revised to reflect this. All signs are expected to meet the criteria in Chapter 22 of the City ordinance and all necessary permits will be requested when appropriate.

Additionally, enclosed please find the following narratives and waiver requests in support of the filed application:

PETRUCELLI, MARTIN & HADDOW, LLP

Ms. Shukria Wiar, Planner

April 2, 2012

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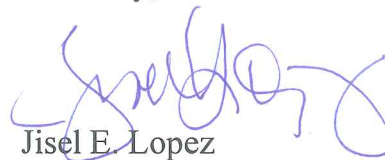
- Waiver with supporting documentation regarding the driveway on Washington Avenue which does not meet Section 14-183 (b) (ingress-egress).
- Waiver with supporting documentation for the two non-conforming parking spaces.
- Narrative regarding the project's compliance with the B-2 design guidelines.
- Letter from Jason Plourde, P.E., of Greenman Pedersen, Inc., regarding the traffic comments. A waiver request with respect to the non-conforming site driveways has been attached.
- A stormwater management plan was requested as the parcel was found to be within the Urban Impaired Stream watershed by the City's consultant. Cumberland Farms has researched this issue and is of the opinion that such a plan is not required under Chapter 500 of the Maine DEP stormwater management rules. A narrative summarizing Cumberland Farm's position is enclosed.

Please note that we attempted to obtain a utility capacity letter from Central Maine Power but were unsuccessful. We have, however, included an email response from CMP regarding this request and their capacity to serve the proposed addition; given that this site is already functioning as a convenience store/gas station and that the proposed addition will not substantially change business operations, we hope this email is sufficient to meet the City's site plan requirements. If you do require a formal letter from CMP, please let us know and we will again attempt to obtain one.

In addition to the full-sized plans and above-referenced submissions, we have enclosed a CD containing all final application materials. If you have any questions regarding the enclosed, please do not hesitate to contact me.

Thank you for your attention to this matter.

Sincerely,



Jisel E. Lopez

JEL/jp

cc: Kathleen Sousa



Engineering and Construction Services

REF.: NIX-2009626

March 30, 2012

Ms. Kathleen A. Sousa
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

SUBJECT: Response to Traffic Review Comments
Proposed Cumberland Farms Expansion
Portland, Maine

Dear Ms. Sousa:

Greenman-Pedersen, Inc. (GPI) has prepared this Response to Comments (RTC) memorandum to respond to the traffic-related comments made on the *Traffic Impact and Access Study*¹ prepared for a proposed expansion to the existing Cumberland Farms facility located at 801 Washington Avenue in Portland, Maine. An evaluation of the *Traffic Impact and Access Study* was conducted by the City of Portland's traffic peer review consultant, Mr. Thomas A. Errico, P.E. of T.Y. Lin International, and his comments were provided in a February 15, 2012 letter² prepared by Ms. Shukria Wiar, City of Portland Planner. These comments are also reflected in a March 13, 2012 memorandum prepared by Ms. Shukria Wiar to the Portland Planning Board. GPI has reviewed the comments made and has prepared this letter to address any outstanding concerns. A copy of the February 15, 2012 comment letter is attached to this letter.

Comment 1: *I concur with the Traffic Impact Study that the project does not require a Traffic Movement Permit.*

Response 1: Comment acknowledged.

Comment 2: *While the Washington Avenue/Ocean Avenue intersection operates at an acceptable level of service, certain turn movements are projected to operate at poor levels following project completion (It should be noted that the proposed project does not create the deficiency, but exacerbates an existing problem). The applicant should identify strategies for implementation that will eliminate the deficient conditions.*

¹ Greenman-Pedersen, Inc. *Traffic Impact and Access Study – Proposed Cumberland Farms Expansion, Portland, Maine*; September 6, 2011.

² Wiar, Shukria. "Staff Review Comments for Level II Site Plan and Conditional Use – Planning Board Review." Letter to Jisel Lopez. 15 Feb. 2012. Portland, Maine.

Response 2: GPI concurs that long delays (LOS E/F) currently occur along the Ocean Avenue eastbound and westbound approaches that will be exacerbated with the addition of future traffic growth independent of the proposed expansion project. Based on documentation provided by the Institute of Transportation Engineers (ITE)³ and based on observations conducted by GPI at similar facilities containing a convenience store and gasoline fueling positions, there is poor correlation between the size of a convenience store and the number of trips generated by the facility. The most valid independent variables for predicting trip-generation rates for a facility containing a convenience store and gasoline fueling positions have been found to be based on the number of vehicle fueling positions and the volume of adjacent street traffic. Although the number of vehicle fueling positions and the adjacent street traffic are not proposed to be altered as part of the proposed expansion project, the change in size of the convenience store was selected to demonstrate an overestimated increase in site trips. Since the proposed expansion project will provide additional amenities to existing patrons and the expected increase in site trips is expected to be negligible per independent variable documentation, increases to traffic and delays at the Washington Avenue and Ocean Avenue signalized intersection are expected to be insignificant as a result of the proposed expansion project.

North of the signalized intersection with Ocean Avenue, Washington Avenue southbound consists of two travel lanes that transition to an exclusive left-turn lane and a shared through/right-turn lane at the Ocean Avenue intersection, thereby creating a left-turn trap for vehicles traveling in the inside lane. Subsequent to the collection of traffic counts and the safety evaluation conducted as part of the *Traffic Impact and Access Study*, a lane designation sign was posted on the Washington Avenue southbound approach to Ocean Avenue with the intent of alleviating driver confusion and vehicular incidents. Similarly, Washington Avenue northbound south of the signalized intersection with Ocean Avenue consists of two travel lanes that transition to an exclusive left-turn lane and a shared through/right-turn lane at the Ocean Avenue intersection, thereby creating a left-turn trap for vehicles traveling in the inside lane. Based on field observations and inventory, no lane designation signs are present on the Washington Avenue northbound approach or on the Ocean Avenue eastbound approach. Independent of the proposed expansion project since the expected increase in site trips is expected to be negligible, consideration should be given to posting lane designation signs on the Washington Avenue northbound approach and the Ocean Avenue eastbound approach for vehicular safety.

Comment 3: *Crash numbers are significant at the Washington Avenue/Ocean Avenue intersection. I need to review the crash patterns in more detail.*

³ Johnson, Kevin L., and Matthew I. Hammond. "Trip-Generation Characteristics for Convenience Stores." *ITE Journal* 71.8 (August 2001): 26-30; Ghezawi, Ramzi S., Frederick J. Wegmann, and Arun Chatterjee. "Convenience Store Trip Generation." *ITE Journal* 68.5 (May 1998).

Ms. Kathleen A. Sousa
March 30, 2012
Page 3 of 4

Response 3: Comment acknowledged.

Comment 4: *Typically, traffic volume data should not be older than two years. The counts were conducted in 2009. However, I find the data to be acceptable given the likelihood that little traffic changes have occurred in the study area.*

Response 4: Comment acknowledged.

Comment 5: *The parking space dimensions do not meet City standards. The plans should be revised for spaces that are 9'x18' or a formal waiver from the City's Technical standards should be requested with supporting documentation.*

Response 5: Based on coordination efforts with the Site Engineer, MHF Design Consultants, Inc., the parking spaces have been redesigned to meet the City of Portland's guidelines for standard parking spaces (9 feet wide x 18 feet long).

Comment 6: *The applicant needs to provide an analysis of the existing driveways on both Washington Avenue and Ocean Avenue and whether conditions meet City standards. Specifically, driveway width, driveway separation and corner clearance standards should be reviewed. I would note that I am particularly concerned about the Washington Avenue driveway located nearest the intersection. If the driveways do not meet City standards, a formal waiver request will need to be provided with supporting documentation.*

Response 6: As detailed within *Response 2*, the proposed expansion project will provide additional amenities to existing patrons and traffic-volume increases on the existing site driveways are expected to be insignificant. The layout of the site and site driveways is conducive for on-site circulation and provides the opportunity for vehicles to enter and exit the site at different locations so as not to impact through traffic along adjacent roadways. Per the City of Portland's *Transportation Systems and Street Design Standards*, the existing Washington Avenue and Ocean Avenue driveways connect perpendicular to the respective roadways and meet the minimum width standard (20 feet), with the Washington Avenue north driveway (± 28 feet) and Ocean Avenue driveway (± 40 feet) exceeding the maximum width standard (24 feet) to accommodate heavy vehicles. The distance between the Washington Avenue driveways (± 120 feet) is slightly below the minimum driveway spacing standard (125 feet for a posted speed limit of 30 miles per hour). The corner clearance distance for the Washington Avenue northern driveway (± 150 feet) meets the minimum standard for arterial and collector streets (150 feet), with the Washington Avenue driveway (± 30 feet) and the Ocean Avenue driveway (± 95 feet) not meeting this standard. Although the expected increase in site trips is expected to be negligible (see *Response 2*) and the design and location of the existing driveways have



Ms. Kathleen A. Sousa
March 30, 2012
Page 4 of 4

been previously approved by the City of Portland, the proponent will submit a formal waiver request to the City of Portland for the driveway standards that are not satisfied.

Comment 7: *The site should provide facilities that allow pedestrians to safely access from abutting sidewalks.*

Response 7: Based on coordination efforts with the Site Engineer, MHF Design Consultants, Inc., the sidewalk located on Washington Avenue along the site frontage between the two existing site driveways is proposed to remain. At the Ocean Avenue site driveway, a crosswalk is proposed to be striped from the existing sidewalk perpendicular to Ocean Avenue and provide for pedestrian connectivity to the convenience store.

Comment 8: *Sidewalk and curb conditions abutting the property and ADA Ramp provisions at the corner will need to be reviewed by DPS.*

Response 8: Comment acknowledged.

Should you have any questions, or require additional information, please contact me.

Sincerely,

GREENMAN – PEDERSEN, INC.

Jason R. Plourde, P.E.
Assistant Vice President

enclosure(s)

cc. Ms. Jisel Lopez
Mr. Christopher Tymula

RESPONSE TO LOCAL COMMENTS

Proposed Cumberland Farms Expansion - Portland, Maine

ATTACHMENT

**STAFF REVIEW COMMENTS FOR LEVEL II SITE PLAN AND CONDITIONAL USE
- PLANNING BOARD REVIEW LETTER**

Cumberland Farms, Inc.
801 Washington Avenue
Waiver Request: Driveway on Washington Avenue

Section 14-183 provides that certain conditional uses, including major and minor auto service stations, are permitted in the B-2 zone if they meet certain requirements such as:

b. Circulation. No ingress or egress driveways shall be located within thirty (30) feet from an intersection. No entrance or exit for vehicles shall be in such proximity to a playground, school, church, other places of public assembly, or any residential zone that the nearness poses a threat or potential danger to the safety of the public.

As has been pointed out by the Planning staff, the project has three driveway cuts from Ocean Avenue and Washington Avenue; one of the driveways servicing Washington Avenue is located less than thirty feet from an intersection. Cumberland Farms is requesting that the Board waive the above requirement with respect to the non-conforming driveway as this driveway has been existing on the site for over 20 years and Cumberland Farms does not intend to make any changes to the driveway as part of its currently proposed expansion. As the attached letter shows, the subject parcel has been approved for operation as an auto service station since 1988. When Cumberland Farms purchased the property and sought approval from the Planning Division for the construction of the existing building, its application materials noted that “[e]xisting curb-cut openings on Washington Avenue will remain.” (See attached). These curb cuts therefore precede Cumberland Farms’ use of the property and Cumberland Farms is requesting that the Board accept them as a grandfathered use.

Cumberland Farms, Inc.
801 Washington Avenue
Waiver Request: driveways and corner clearance distances

As set forth in Response 6 in the March 30, 2012 letter from Jason Plourde, P.E., the site existing driveways do not meet current City standards. In particular, the distance between the Washington Avenue driveways is slightly below the minimum driveway spacing standard. One of the Washington Avenue driveways and the Ocean Avenue driveway also do not meet the minimum standard for arterial and collector streets. Cumberland Farms is seeking a waiver of these requirements because it is not proposing any changes to the existing uses on the property and the design and location of these driveways were previously approved by the City of Portland (see attached documentation submitted in connection with other driveway waiver request). As shown on the traffic study submitted with the initial site plan application materials and as further explained in Mr. Plourde's letter, the existing layout of the site, including the driveways, is conducive for on-site circulation in a busy traffic area. While the proposed project will increase the retail space, this change is not expected to materially increase the number of site trips to the site. Furthermore, because the number of gasoline dispensers will remain unchanged, increases in traffic and delays at the Washington Avenue and Ocean Avenue intersection are expected to be insignificant as a result of this project.

Cumberland Farms, Inc.
801 Washington Avenue
Waiver Request: Parking Space Dimensions

The City of Portland's Technical standards provide that all standard parking spaces shall measure 9 feet wide by 18 feet long. Cumberland Farms is proposing to add an additional ten parking spaces for a total of 32 parking spaces on the site (including eight at pumps). As originally proposed, these parking spaces measured 9 feet by 19 inches. Cumberland Farms has revised its site plan to conform to the City's technical requirements for parking space dimensions for the majority of the parking spaces on the site but is requesting a waiver of these standards for two parking spaces. As shown on the attached site plan, these spaces are located at the front of the retail store, in front of the new proposed Red Box video kiosk. Due to the fact that Cumberland Farms is proposing to extend the sidewalk directly abutting these spaces, a reduction in the length of these spaces is not feasible. Since the size of the property easily accommodates these larger parking spaces and, in Cumberland Farms' experience, customers generally prefer larger spaces, Cumberland Farms is requesting that the Planning Board approve the construction of two 9' x 18' spaces on the property.

**Cumberland Farms, Inc.
801 Washington Avenue
Response to Stormwater Management Comments**

Cumberland Farms submits the following response to the comments of the City's consultant, David Senus, regarding the application of the stormwater management rules:

Comment 3.b.1- The City of Portland Code of Ordinances Section 14-526(b).3.b refers to the City Technical Manual for stormwater standards. The City Technical Manual refers to the Maine DEP Chapter 500 Stormwater Management Rules . The Chapter 500 Basic Standards (4.A.1) and General Standards (4.B.1) only apply when the project disturbs over 1 acre. The Flooding Standard (4.D.1) only applies when the project results in 3 acres or more of impervious area. Therefore, it is our opinion that the project is not subject to any of the Maine DEP standards.

Comment 3.b.2- We agree that the project falls within the Fall Brook watershed which is classified as an Urban Impaired Stream. However, since the project does not impact more than 1acre, then no stormwater permits are required according to the Flowchart for Chapter 500 Standards.

Also, the DEP Chapter 500 Urban Impaired Stream Standard (4.D.1) states that the standard must be met when the project results in 3 acres or more of impervious area or 20 acres or more of developable area. Furthermore, Standard (4.D.3) also states that a project can receive an exception when there is a redevelopment of existing pavement which is not likely to increase stormwater impacts beyond the levels already present in the runoff from the existing impervious area. Since a majority of the new impervious

area for this project is resulting from the larger convenience store roof, the discharge from this roof would be considered clean run off. Also, the additional site pavement runoff is de minimus relative to the existing paved area runoff.

Kathleen Sousa

From: Dominic A. Taverna
Sent: Monday, October 17, 2011 8:33 AM
To: Kathleen Sousa
Cc: 'brent@dlpoulin.com'; John Marth; Michael Durant
Subject: FW: Cumberland Farms Store- 801 Washington Ave Portland, ME 441-1065485-012

Categories: Red Category

Kathleen

I received a voice mail on Friday from Jamie at Central Maine Power.
I returned his VM and left my contact information on Friday.
Please see the attached E-mail from Jamie regarding power at this location.
Let me know if you need anything else on this.

Dom

Dominic A. Taverna
Senior Construction Project Manager
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

Office 508-270-4407
Cell 508-350-2578
Email dtaverna@cumberlandgulf.com

This electronic message contains information from Cumberland Gulf Group of Companies, which may be privileged. The information is intended to be for the use of the addressee only. If you are not the addressee, note that any disclosure, copy, distribution or use of the contents of this message is prohibited.

From: Cough, Jamie [<mailto:Jamie.Cough@cmccco.com>]
Sent: Monday, October 17, 2011 6:32 AM
To: Dominic A. Taverna
Subject: Cumberland Farms Store- 801 Washington Ave Portland, ME 441-1065485-012

Dominick:

This store currently has a peak load of 58 kw (approximately 64 kva), and is served by a 3x50 kva bank (150 kva total). This 600A service can be supported by the existing transformer and service drop. Should the actual demands approach the transformer rating of 150 kva, we would be looking at installing an additional run of 336 to the service drop. Given the diversification of your loads, it is not likely.

If you have any additional questions, please contact me.

Regards,

Jamie

Jamie Cough
Energy Services Advisor
Central Maine Power Company
162 Canco Road
Portland, ME 04103
207-842-2367 office
207-458-0382 cell
207-626-4082 fax

CITY OF PORTLAND, MAINE

389 CONGRESS STREET
PORTLAND, MAINE 04101
(207) 775-5451



DEPARTMENT OF PLANNING & URBAN DEVELOPMENT

P. SAMUEL HOFFSES, CHIEF
INSPECTION SERVICES DIVISION

June 14, 1988

Cumberland Farms
777 Dedham Street
Canton, MA 02021

Re: 801 Washington Ave, Portland, ME

Dear Sir:

Your application to demolish existing building and construct a 2400 sq ft building has been reviewed and a permit is herewith issued subject to the following requirements:

Site Plan Review Requirements

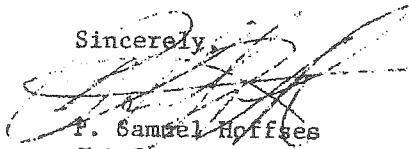
Inspection Services--Approved W.J. Turner 6/8/88
Planning Division--Approved Ms. Maureen O'Meara 6/6/88
Public Works--Approved Mr. R.Roy 3/10/88
Fire Dept.--Approved F.L. J.R. Doblowski

Building Code Requirements

1. The demolition of this structure shall be done as per the building code.
2. All lot lines shall be clearly marked before calling for foundation inspection.
3. Roof shall be designed for a minimum of 50 psf live load. Also for canopy over gas pumps.
4. Gas storage tank shall be installed as per D.E.P. regulation.

If you have any questions regarding these requirements, please do not hesitate to contact this office.

Sincerely,


P. Samuel Hoffses
Chief, Inspection Services

PSH/jmv

1987 Traffic Study



BACKGROUND AND EXISTING CONDITIONS

Cumberland Farms, Inc. retained T. Y. Lin International/Hunter-Ballew Associates (TYLI/HBA) to prepare a traffic impact study in conjunction with the proposed revision/expansion of the existing Cumberland Farms Convenience Store/Gasoline Station located at the intersection of Washington Avenue/Ocean Avenue in Portland, Maine. Figure 1 presents the location of the proposed project.

Present plans indicate the proposed development will consist of the removal of the existing 1920 square foot building and construction of a new 2400 square foot convenience store and removal of the existing 2 gasoline islands and construction of 4 new gasoline islands. Existing curb-cut openings on Washington Avenue will remain, while the curb-cut openings on Ocean Avenue will be moved approximately 35 feet to the east.

The purpose of this study is to determine and evaluate the impact of traffic associated with the proposed revision/expansion on the operation of the Washington Avenue/Ocean Avenue intersection, and evaluate the proposed site access/circulation plan.

A manual turning movement count was conducted at the Washington Avenue/Ocean Avenue intersection on Thursday, November 19, 1987 from 7:00 - 9:00 AM and 3:30 - 5:30 PM. The results of these counts indicate the peak hours occur from 7:30 - 8:30 AM and 4:30 - 5:30 PM. Comparison of these volumes to historical volumes taken in 1982 (by the FACTS Traffic Signal Management Study) indicates volumes on the Ocean Avenue approaches have increased by approximately 52 percent per year during the AM peak hour and 27 percent per year during the PM peak hour. These large growth patterns probably are not associated with typical development growth or background growth but with major adjustments in travel patterns. All indicators seem to point toward the Tukey's Bridge reconstruction project and its associated delays and congestion as the underlying reason for the altered travel patterns. To provide a more realistic estimation of traffic volumes at the Washington Avenue/Ocean Avenue intersection following the completion of the Tukey's Bridge project, 1987 Ocean Avenue volumes were estimated by increasing the 1982 FACTS volumes by 28 percent or 5.6 percent per year. Following this adjustment, the peak hour traffic volumes were factored to estimate the 30th highest hour volumes (typically used for impact analysis) and daily traffic volumes adjusted to represent the Average Annual Daily Traffic volumes utilizing MDOT weekly group mean factors. Figure 2 presents the estimated 1987 30th highest hour volumes during the AM and PM peak hours and the 1987 Average Annual Daily Traffic volumes.

LED AREA LIGHT - MEDIUM (XAM)



U.S. and international patents pending

EXPECTED LIFE - Minimum 60,000 hours to 100,000 hours depending upon the ambient temperature of the installation location. See LSI web site for specific guidance.

LEDS - Select high-brightness LEDs. 5750°K color temperature (nominal).

DISTRIBUTION/PERFORMANCE - Types 2, 3, FT*, and 5. Exceptional uniformity with full cutoff creates bright environment at lower light levels. Improved backlight cutoff minimizes light trespass.

HOUSING - Housing is die-formed aluminum. Weather-tight housing contains factory prewired driver and field connections. Wiring access door is located underneath and utilizes tool-less thumbscrew fastener.

OPTICAL UNIT - Lens is clear, flat tempered glass, sealed to aluminum optics housing to create an IP67 rated, sealed optical unit.

MOUNTING - Tapered rear design allows fixtures to be mounted in a quad pattern without the need for extension arms. Use with Crossover steel round and steel square poles. See separate data sheets.

ELECTRICAL - Universal voltage power supply 120-277 VAC, (50/60 Hz) input. Also available in 347 volt and 480 volt. Supply voltage must be specified for use with optional button-type photocells. PCI is available in 120, 208, 240, 277 or 347 volt. Optional twistlock photocell receptacle is also available. Photocell must be ordered separately. See accessories.

DRIVER - State-of-the-art driver technology designed specifically for LSI LED light sources provides unsurpassed system efficiency. Components are fully encased in potting material for moisture resistance. Driver complies with IEC and FCC standards.

OPERATING TEMPERATURE - -40°C to +50°C (-40°F to +122°F).

FINISH - Fixtures are finished with LSI's DuraGrip® polyester powder coat finishing process. The DuraGrip finish withstands extreme weather changes without cracking or peeling, and is guaranteed for five full years. Standard colors include bronze, black, platinum plus, buff, graphite, satin verde green, metallic silver and white.

WARRANTY - LSI LED fixtures carry a limited 5-year warranty.

PHOTOMETRICS - Application layouts are available upon request. Contact LSI Applications Group at lighting.apps@lsi-industries.com

SHIPPING WEIGHT - 27.0 lbs (12.7 kg)

LISTING - ETL listed to U.S. and International safety standards. Suitable for wet locations.

* FT distribution is classified as semi-cutoff.

Crossover® LED Lighting

lighting facts^{CM}

A Program of the U.S. DOE

Light Output (Lumens)	9996
Watts	139.5
Lumens per Watt (Efficacy)	71

Color Accuracy	70
Color Rendering Index (CRI)	

Light Color
Correlated Color Temperature (CCT)

5750 (Daylight)

2700K 3000K 4500K 6500K

All results are according to IESNA LM-79-2008: *Approved Method for the Electrical and Photometric Testing of Solid-State Lighting*. The U.S. Department of Energy (DOE) verifies product test data and results. Products qualified under the DOE ENERGY STAR® program have the ENERGY STAR mark on this label.

Visit www.lightingfacts.com for the *Label Reference Guide*.

Registration Number: KGGN-FAQHKB
Model Number: XAM-2-LED-119-CW-UE
Type: Outdoor pole/arm-mounted area and roadway fixtures

This "Lighting Facts Label" represents LSI's commitment, through participation in the Department of Energy's SSL Quality Advocates program, to accurately report performance criteria of our LED fixtures. **This label represents a single test of a single fixture. Input power below represents rated values for the family.** Color temperature and lumen output for each variation of this Crossover® fixture can be found in the ordering information below as well as at www.lightingfacts.com.

LIGHT OUTPUT		
Fixture	Distribution	Lumens (Nominal)
XAM	Type 2	9900
	Type 3	9400
	Type FT	10,200
	Type 5	9000
	Input Power of 137 watts	

IP67



ROHS
COMPLIANT



Intertek
Suitable for wet locations



LED AREA LIGHT - MEDIUM (XAM)



LUMINAIRE ORDERING INFORMATION

TYPICAL ORDER EXAMPLE: **XAM 2 LED 119 CW UE WHT PCI120**

Prefix	Distribution	Light Source	# of LEDs	LED Color Temperature	Line Voltage	Luminaire Finish	Options
XAM - LED Area Medium	2 - Type 2 3 - Type 3 FT - Forward Throw 5 - Type 5	LED	119	CW - Cool White (5750°K nom.)	UE - Universal Voltage (120-277) 347 480	BLK - Black BRZ - Bronze BUF - Buff GPT - Graphite MSV - Metallic Silver PLP - Platinum Plus SVG - Satin Verde Green WHT - White	Button Type Photocells PCI 120 - 120v PCI 208 - 208v PCI 240 - 240v PCI 277 - 277v PCI 347 - 347v PCR - Twistlock Photoelectric Control Receptacle TB - Terminal Block

LUMINAIRE EPA CHART² - XAM

	Single	1.2
	D180°	2.4
	D90°	2.0
	T90°	3.4
	TN120°	3.4
	Q90°	4.0

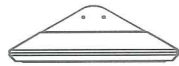
ACCESSORY ORDERING INFORMATION (Accessories are field installed)

Description	Order Number
BKA-XBO-EC-6-CLR 6" Extension Arm	382136CLR ¹
BKS-XBO-WM-*"-CLR Wall Mounting Bracket	382132CLR ¹
DVS - Direct View Shield (rear mount only) ²	390687CLR
X4RPP Round Pole Plate for 4" Poles	379967CLR ¹
X5RPP Round Pole Plate for 5" Poles	379968CLR ¹
PC 120 - Photocell for use w/ PCR option (120V)	122514
PC 208-277 - Photocell for use w/ PCR option (208V, 240V or 277V)	122515
PC 347 - Photocell for use w/ PCR option (347V)	159516
PC 480 - Photocell for use w/ PCR option (480V)	1225180

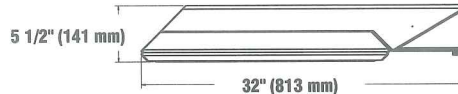
NOTES:

- 1- Replace "CLR" with desired color (ex. 382136BRZ)
- 2- Direct View Shield adds to fixture EPA. Consult factory.

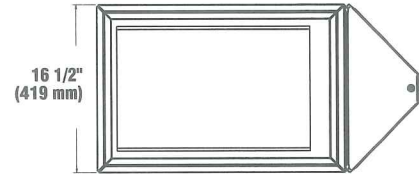
DIMENSIONS



Front View

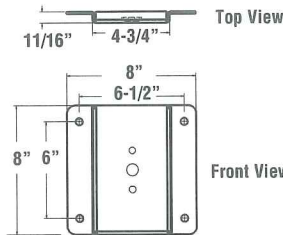


Side View

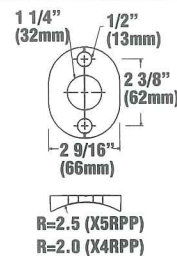


Bottom View

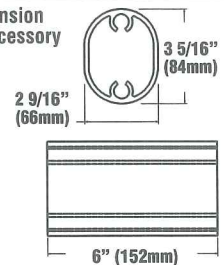
Wall Bracket



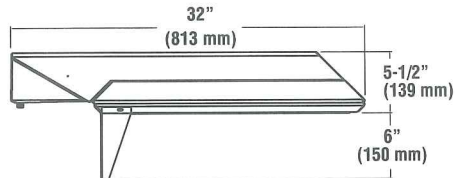
Round Pole Plate Accessory



6" Extension Arm Accessory



DVS - Direct View Shield (390687CLR)



06/07/10

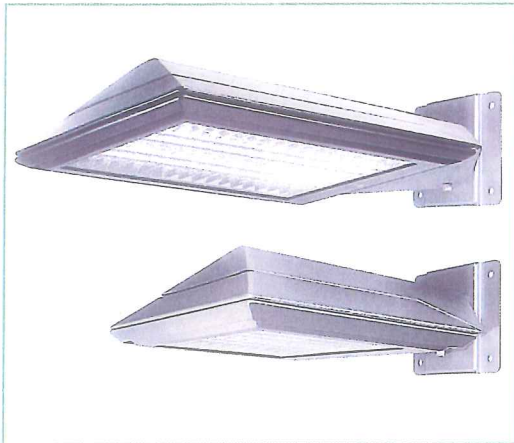
Project Name _____ Fixture Type _____

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LSI INDUSTRIES INC.

Catalog # _____



LED WALL LIGHT - SMALL (XAWS) AND MEDIUM (XAWM)



U.S. and international patents pending

EXPECTED LIFE - Minimum 60,000 hours to 100,000 hours depending upon the ambient temperature of the installation location. See LSI web site for specific guidance.

LEDS - Select high-brightness LEDs. 5750°K color temperature (nominal).

DISTRIBUTION/PERFORMANCE - Types 2, 3, and FT*. Exceptional uniformity with full cutoff creates bright environment at lower light levels.

HOUSING - Housing is die-formed aluminum. Weather-tight housing contains factory prewired driver and field connections. Wiring access door is located underneath and utilizes tool-less thumbscrew fastener.

OPTICAL UNIT - Lens is clear, flat tempered glass, sealed to aluminum optics housing to create an IP65 rated, sealed optical unit.

MOUNTING - Tapered rear design allows fixtures to be easily mounted in a junction box using a simple wall plate (supplied).

ELECTRICAL - Universal voltage power supply 120-277 VAC, (50/60 Hz) input. Also available in 347V and 480V. Supply voltage must be specified for use with optional button-type photocells. PCI is available in 120, 208, 240, 277, or 347 volt.

DRIVER - State-of-the-art driver technology designed specifically for LSI LED light sources provides unsurpassed system efficiency. Components are fully encased in potting material for moisture resistance.

OPERATING TEMPERATURE - -40°C to +50°C (-40°F to +122°F).

FINISH - Fixtures are finished with LSI's DuraGrip® polyester powder coat finishing process. The DuraGrip finish withstands extreme weather changes without cracking or peeling, and is guaranteed for five full years. Standard colors include bronze, black, platinum plus, buff, graphite, satin verde green, metallic silver and white.

WARRANTY - LSI LED fixtures carry a limited 5-year warranty.

PHOTOMETRICS - Application layouts are available upon request. Contact LSI Applications Group at lighting.apps@lsi-industries.com

SHIPPING WEIGHT - 21.0 lbs (9.5 kg) for small and 27.0 lbs (12.7 kg) for medium

LISTING - Listed to U.S. and Canadian safety standards. Suitable for wet locations.

* FT distribution is classified as semi-cutoff.

LIGHT OUTPUT		
Fixture	Distribution	Lumens (Nominal)
XAWS	Type 2	5103
	Type 3	4355
	Type FT	5350
Input Power of 70 watts		
XAWM	Type 2	9900
	Type 3	9400
	Type FT	10,200
Input Power of 137 watts		

LED WALL LIGHT - SMALL (XAWS) AND MEDIUM (XAWM)

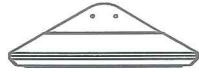


LUMINAIRE ORDERING INFORMATION

TYPICAL ORDER EXAMPLE: **XAWS 3 LED 63 CW UE MSV PCI277**

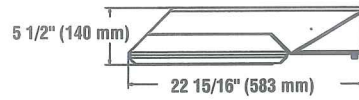
Prefix	Distribution	Light Source	# of LEDs	LED Color Temperature	Line Voltage	Luminaire Finish	Options
XAWS - LED Wall Mount Small	2 - Type 2 3 - Type 3 FT - Forward Throw	LED	63	CW - Cool White (5750°K nom.)	UE - Universal Voltage (120-277) 347 480	BLK - Black BRZ - Bronze BUF - Buff GPT - Graphite MSV - Metallic Silver PLP - Platinum Plus SVG - Satin Verde Green WHT - White	Button Type Photocells PCI 120 - 120v PCI 208 - 208v PCI 240 - 240v PCI 277 - 277v PCI 347 - 347v
XAWM - LED Wall Mount Medium	2 - Type 2 3 - Type 3 FT - Forward Throw		119				

DIMENSIONS

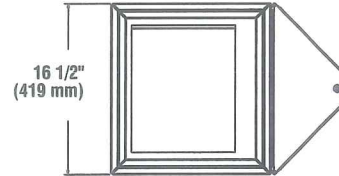


Front View

SMALL

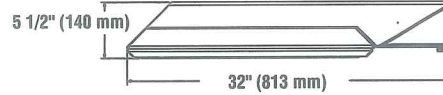


Side View

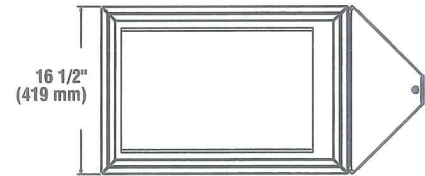


Bottom View

MEDIUM



Side View

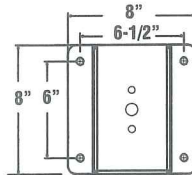


Bottom View

WALL BRACKET



Top View



Front View

04/21/10

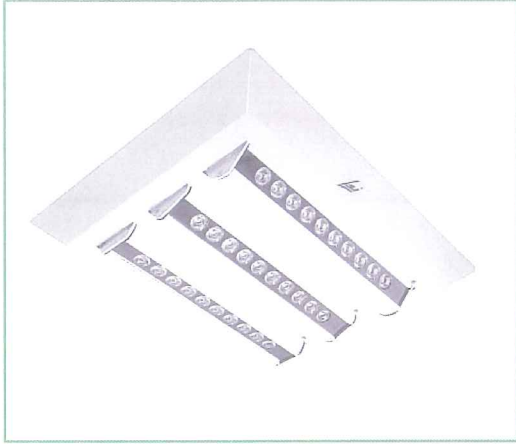
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Project Name _____ Fixture Type _____

Catalog # _____



GENERATION 3 LED FOCUS CANOPY LIGHT (CRO3)



US patents D590100 & D574995 & 7828456 and US & Int'l. patents pending

LEDS - Select high-brightness LEDs. 5300°K color temperature, 70 CRI (nominal).

OPTICS / DISTRIBUTIONS – Ultra-High efficiency optics provide precise beam placement for optimal retail surface illumination. Each bank of LEDs in a light cartridge is independently adjustable between +/- 45° allowing targeted zone illumination. Regardless of light cartridge position luminaire provides cutoff.

OPTICAL UNIT – Featuring a slim 2" profile luminaire. Housing is die-formed aluminum with independently adjustable extruded aluminum light cartridges. Each light cartridge is provided with a gasketed clear tempered glass lens providing a water-resistant seal.

DRIVER – State-of-the-art driver technology designed specifically for LSI LED light sources provides unsurpassed system efficiency. Input power is 50 watts. Components are fully encased in potting for moisture resistance. Driver complies with IEC and FCC standards.

DRIVER HOUSING – Weather-tight aluminum driver/electrical enclosure is elevated above canopy deck to ensure no water entry and providing "knock-out" entry for primary wiring.

FINISH – Standard color is white. Fixture is finished with LSI's DuraGrip[®] polyester powder coat process. DuraGrip withstands extreme weather changes without cracking or peeling.

OPERATING TEMPERATURE – -40°C to +50°C (-40°F to +122°F)

ELECTRICAL - Universal voltage power supply 120-277V VAC, (50/60 Hz) input. Two-stage surge protection (including separate surge protection built into electronic driver) meets IEEE C62.41.2-2002, Scenario 1, Location Category C

INSTALLATION - Installs in a 12" or 16" deck pan. Deck penetration consists of 5 drilled holes simplifying installation and water sealing. Unit is designed to retrofit into existing Scottsdale[®] (4") hole as well as openings for Encore[®] and Encore[®] Top Access and to reconnect wiring from the SC/ECTA without having to relocate conduit. Retrofit panels are available for existing Scottsdales and Encores (see back page) as well as kits for recessed and 2x2 installations (see separate spec sheets).

EXPECTED LIFE - Minimum 60,000 hours to 100,000 hours depending upon the ambient temperature of the installation location. See LSI web site for specific guidance.

WARRANTY - Limited 5-year warranty

LISTING - ETL listed to UL1598, UL8750 and other U.S. and International safety standards. Suitable for wet locations.

PHOTOMETRICS – Application layouts are available upon request. Contact LSI Petroleum Lighting or petroleum.apps@lsi-industries.com

LIGHT OUTPUT - CRO3

Distribution	Lumens (Nominal)
Type FO	1000 (for each of 3 banks - total 3000)
Input Power of 40 watts	



IP67

RoHS



GENERATION 3 LED FOCUS CANOPY LIGHT (CRO3)



LUMINAIRE ORDERING INFORMATION

TYPICAL ORDER EXAMPLE: **CRO3 FO LED 30 350 CW UE WHT**

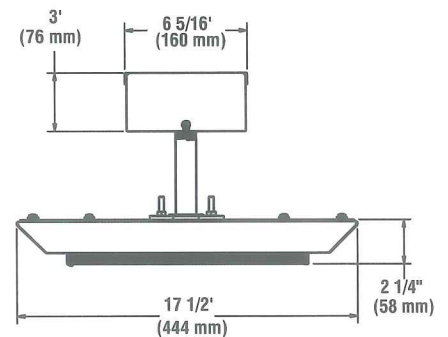
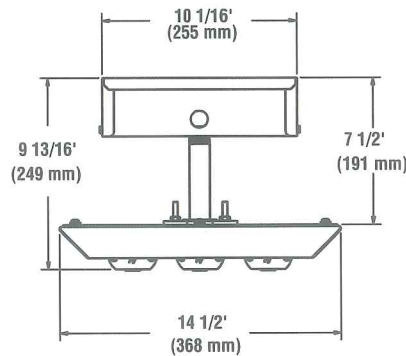
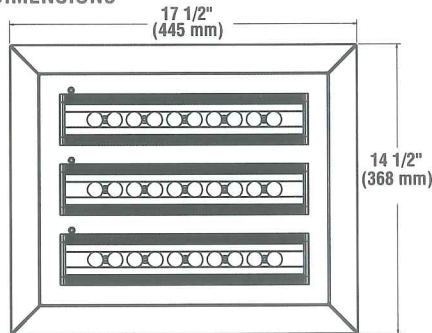
Prefix	Distribution	Light Source	# of LEDs	Drive Current	Color Temperature	Input Voltage	Finish
CRO3	FO - Focus	LED	30	350 - 350 mA	CW - Cool White (5000° K nom)	UE - Universal Voltage (120-277V AC)	WHT - White

ACCESSORY ORDERING INFORMATION (Accessories are field installed)

Description	Order Number	Description	Order Number
Retrofit Panel - SC to CRO3, for 16" Deck Panel	430951	Retrofit 2x2 Cover Panel Blank (no holes)	357282
Retrofit Panel - EC / ECTA / SCF to CRO3, for 16" Deck Panel	430765	Retrofit RIC Cover Panel Blank (no holes)	354702
Retrofit Panel - SC to CRO3, for 12" Deck Panel	430797	Kit - Hole Plugs and Silicone (enough for 25 retrofits) ¹	1320540
Retrofit Panel - ECTA / SCF to CRO3, for 12" Deck Panel	430759	¹ Consists of (25) 7/8" hole plugs and (1) 10.3 oz tube of RTV	
Retrofit 2x2 Cover Panel (w/ centered hole pattern for CRO3)	430966		

Superkits® are available to retrofit CRO3 Ambient and Focus fixtures into a wide variety of existing 2x2 and recessed housings. See separate spec sheets.

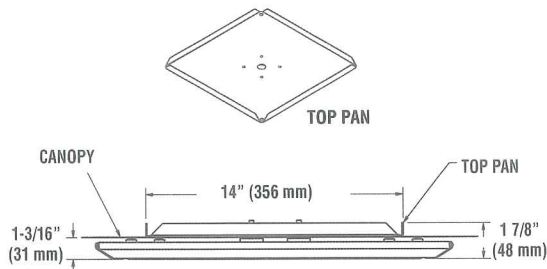
DIMENSIONS



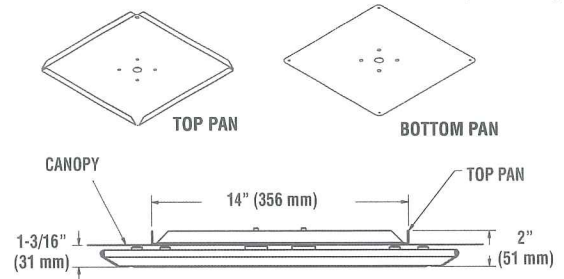
SHIPPING WEIGHTS

12.2 lbs.	1 fixture
53.3 lbs.	4 fixtures

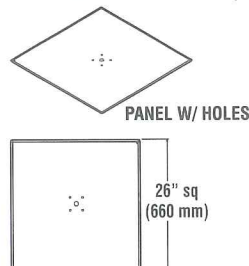
16" DECK RETROFIT PANEL - SC (#430951)



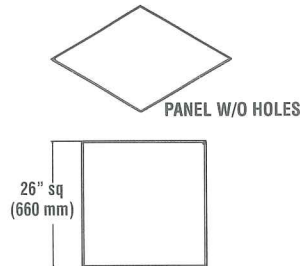
16" DECK RETROFIT PANELS - EC/ECTA/SCF (#430765)



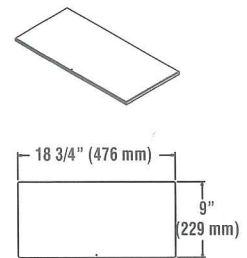
2X2 COVER PANEL W/ HOLES (430966)



2X2 COVER PANEL BLANK (357282)



RIC COVER PANEL (354702)



07/28/11

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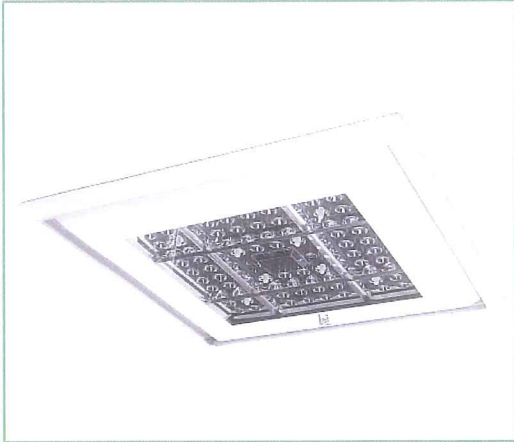
Project Name _____ Fixture Type _____
Catalog # _____



GENERATION 3 LED AMBIENT CANOPY LIGHT (CRO3)



US patent D574994 & 7828456 and US & Int'l. patents pending



SMARTTEC™ ENERGY SAVING FEATURES:

THERMAL CONTROL - Sensors in both optical unit and driver enclosure reduce drive current when ambient temperatures exceed 50°C. Current is lowered in imperceptible 5% increments every 5 minutes until recommended operating temp is reached.

DAYLIGHT SENSING - Eliminates day-burners (optional) by sensing daylight levels (not affected by the light coming from the LEDs). Turns fixture off at light levels approximately equal to sunrise and on at sunset.

OCCUPANCY SENSING - Small, built-in flush-mounted sensor (optional) detects movement in a 20-foot circle. Unit dims to 30% power when no presence is detected. Smart dim slowly lowers light level over a 15-second period.

LEDS - Four LED array choices; 64 and 84 (19.4" square optical unit) and 104 and 128 (21.7" square). Each feature select high-brightness LEDs; 5300°K color temperature, 70 CRI (nominal).

DRIVE CURRENT OPTIONS - Choose from three drive current levels; 350 mA (most energy-efficient), 450 mA or 550 mA (best first-cost value).

OPTICS / DISTRIBUTIONS - Ultra-high efficiency reflectors provide three distributions to meet your lighting needs. Choose from symmetric, asymmetric (for directing light toward or away from selected areas), or enhanced symmetric (for increased brightness and attraction from the road). Luminaire is rated full cutoff.

OPTICAL UNIT - Featuring an ultra-slim 1" profile, housing is die-formed aluminum with a clear tempered glass lens. Unit is water-resistant, sealed to an **IP67** rating. Patented integral single blade heat sink does not trap dirt and grime, ensuring cool running performance over the life of the fixture.

THE INDUSTRY'S ONLY BREATHABLE SEAL - Teflon® breather allows super-tight **IP67** protection while preventing temperature cycling from building up internal pressures and vacuums that can stress optical unit seals and components.

DRIVER - Proprietary, state-of-the-art SmartTec™ driver technology provides unsurpassed system efficiency, control and protection. Components are fully encased in potting for **IP67** moisture resistance. Complies with IEC and FCC standards.

DRIVER HOUSING - Wet location rated driver/electrical enclosure is elevated above canopy deck to help prevent water entry and to provide easy "knock-out" connection of primary wiring.

FINISH - Standard color is white. Finished with LSI's DuraGrip® polyester powder coat process. DuraGrip withstands extreme weather changes without cracking or peeling.

OPERATING TEMPERATURE - -40°C to +50°C (-40°F to +122°F)

ELECTRICAL - Universal voltage power supply, 120-277 VAC, 50/60 Hz input. Two-stage surge protection (including separate surge protection built into electronic driver) meets IEEE C62.41.2-2002, Scenario 1, Location Category C

INSTALLATION - Installs in a 12" or 16" deck pan. Deck penetration consists of 5 drilled holes simplifying installation and water sealing. Unit is designed to retrofit into existing Scottsdale® (4") hole as well as openings for Encore® and Encore® Top Access and to reconnect wiring from the SC/ECTA without having to relocate conduit. Retrofit panels are available for existing Scottsdales and Encores (see back page) as well as kits for recessed and 2x2 installations (see separate spec sheets).

EXPECTED LIFE - Minimum 60,000 hours to 100,000 hours depending upon the ambient temperature of the installation location. See LSI web site for specific guidance.

WARRANTY - Limited 5-year warranty.

LISTING - ETL listed to UL1598, UL8750 and other U.S. and International safety standards. Suitable for wet locations.

PHOTOMETRICS - Application layouts are available on request. Contact LSI Petroleum Lighting or petroleum.apps@lsi-industries.com

Crossover LED Lighting

lighting facts^{CM}

A Program of the U.S. DOE

Light Output (Lumens)	11950
Watts	146.5
Lumens per Watt (Efficacy)	81

Color Accuracy

Color Rendering Index (CRI) **67**

Light Color

Correlated Color Temperature (CCT) **5176 (Daylight)**

2700K 3000K 4500K 6500K

Warm White Bright White Daylight

All results are according to IESNA LM-79-2008: Approved Method for the Electrical and Photometric Testing of Solid-State Lighting. The U.S. Department of Energy (DOE) verifies product test data and results.

Visit www.lightingfacts.com for the Label Reference Guide.

Registration Number: KGGN-80LD1Y
 Model Number: CRO3 S LED128 350 CW UE
 Type: Canopy light

LIGHT OUTPUT - CRO3					
	# of LEDs	Distribution / Lumens			Watts
		S	A	ES	
350 mA	64	7087	6872	6272	75
	84	8996	8899	7996	99
	104	11243	10951	10318	123
	128	13599	13237	12416	150
450 mA	64	8616	8304	7440	99
	84	10753	10639	9567	131
	104	13302	12981	12346	162
	128	16031	15687	14722	199
550 mA	64	9767	9435	8386	128
	84	12158	11939	10796	163



IP67

RoHS



Suitable for wet locations



07/28/11

Project Name _____ Fixture Type _____

Catalog # _____

GENERATION 3 LED AMBIENT CANOPY LIGHT (CRO3)



LUMINAIRE ORDERING INFORMATION

TYPICAL ORDER EXAMPLE: **CRO3 S LED 128 450 CW UE WHT IMS**

Prefix	Distribution	Light Source	# of LEDs	Drive Current	Color Temperature	Input Voltage	Finish	Options*
CRO3	S - Symmetric A - Asymmetric ES - Enhanced Symmetric	LED	64	350 - 350mA	CW - Cool White	UE - Universal Voltage (120-277 AC)	WHT - White	IPC - Integral Photocell IMS - Integral Motion Sensor
			84	450 - 450mA				
			104	350 - 350mA				
			128	450 - 450mA				

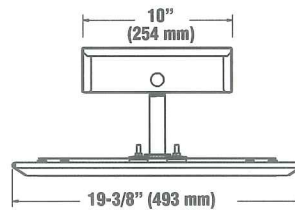
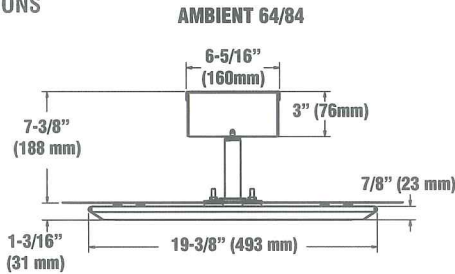
*Consult factory for availability

ACCESSORY ORDERING INFORMATION (Accessories are field installed)

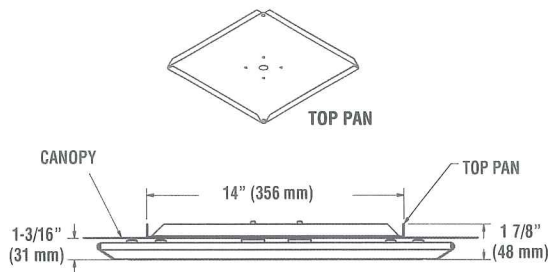
Description	Order Number	Description	Order Number
Retrofit Panel - SC to CRO3, for 16" Deck Panel	430951	Retrofit 2x2 Cover Panel Blank (no holes)	357282
Retrofit Panel - EC / ECTA / SCF to CRO3, for 16" Deck Panel	430765	Retrofit RIC Cover Panel Blank (no holes)	354702
Retrofit Panel - SC to CRO3, for 12" Deck Panel	430797	Kit - Hole Plugs and Silicone (enough for 25 retrofits) ¹	1320540
Retrofit Panel - ECTA / SCF to CRO3, for 12" Deck Panel	430759	¹ Consists of (25) 7/8" hole plugs and (1) 10.3 oz tube of RTV	
Retrofit 2x2 Cover Panel (w/ centered hole pattern for CRO3)	430966		

Superkits® are available to retrofit CRO3 Ambient and Focus fixtures into a wide variety of existing 2x2 and recessed housings. See separate spec sheets.

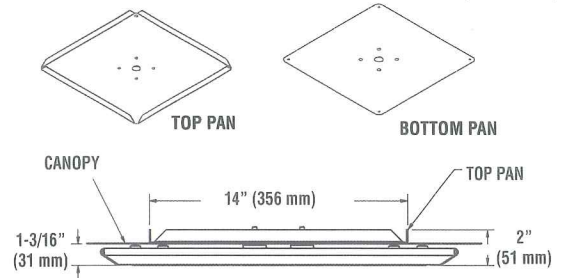
DIMENSIONS



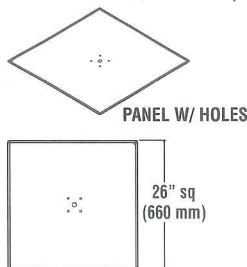
16" DECK RETROFIT PANEL - SC (#430951)



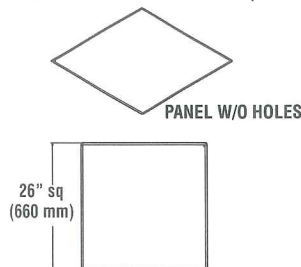
16" DECK RETROFIT PANELS - EC/ECTA/SCF (#430765)



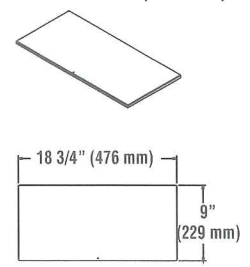
2X2 COVER PANEL W/ HOLES (430966)



2X2 COVER PANEL BLANK (357282)



RIC COVER PANEL (354702)



07/28/11

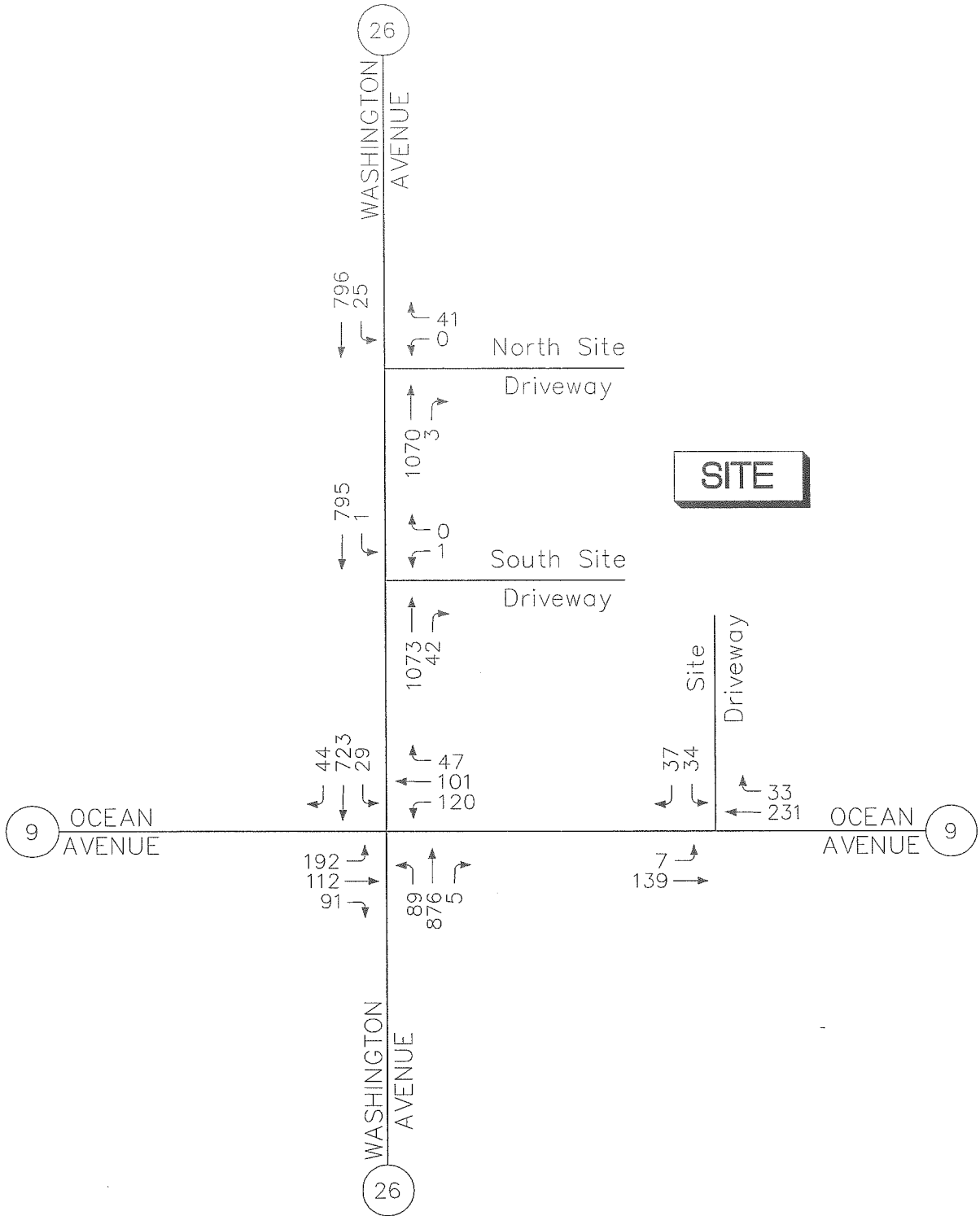
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LSI INDUSTRIES INC.

Project Name _____ Fixture Type _____
Catalog # _____

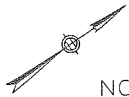


TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



SITE



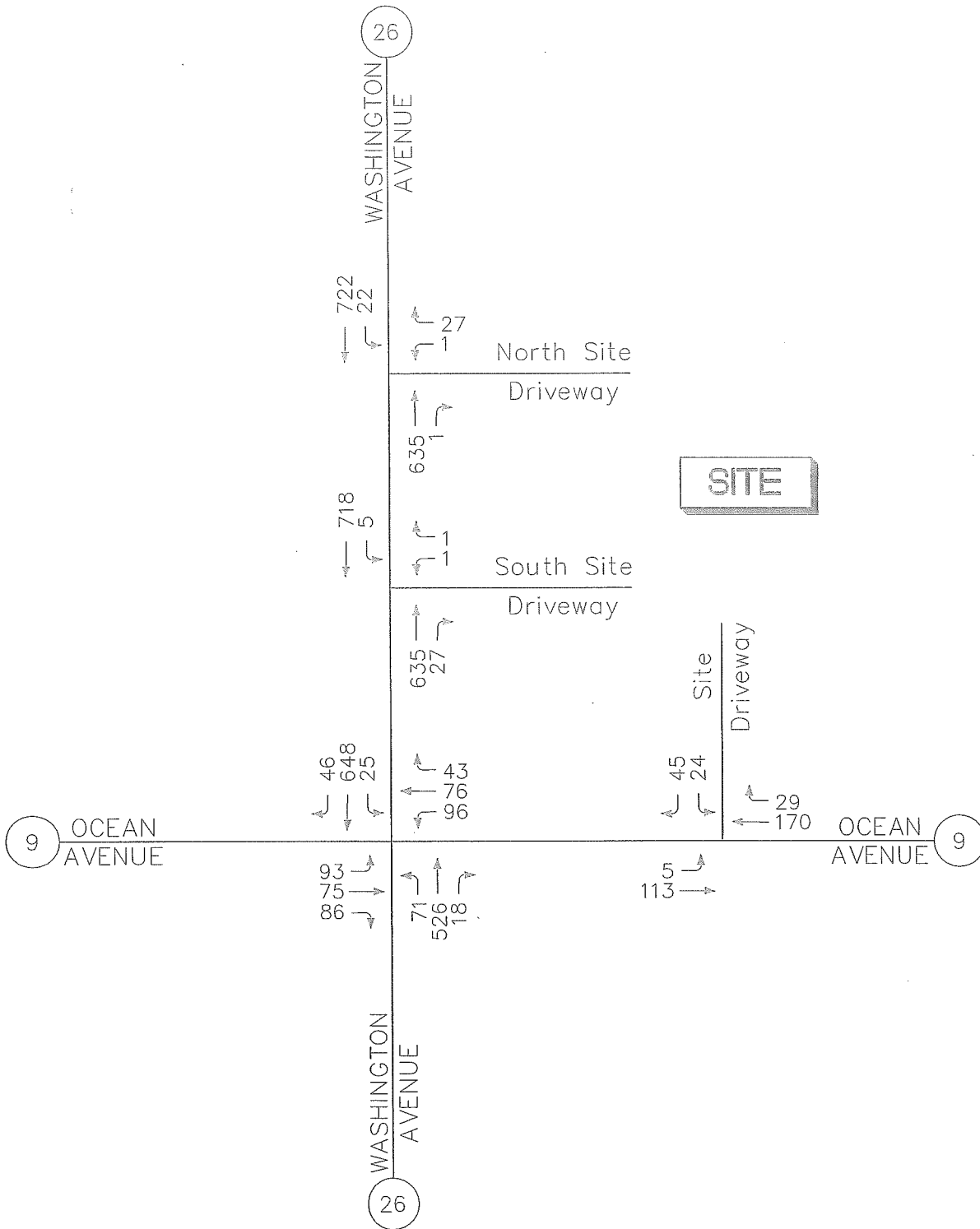
NOT TO SCALE

Figure 6

2012 No-Build
Weekday PM
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



NOT TO SCALE

Figure 7

2012 No-Build
 Saturday Midday
 Peak Hour Traffic Volumes

The 2012 No-Build peak-hour traffic volumes were accordingly developed by applying a 1.5 percent compounded annual traffic-growth rate to the 2009 Existing traffic volumes. The 2012 No-Build weekday AM, weekday PM, and Saturday midday peak-hour traffic volumes are shown graphically on Figures 5 through 7, respectively.

Planned Roadway Improvements

Officials from the City of Portland and MaineDOT were contacted to determine if any roadway improvements are planned within the study area. At this time, there are no roadway improvement projects planned within the study area that would be implemented within the design horizon.

Trip Generation

The site currently contains a Cumberland Farms facility that includes a ±2,400 square foot convenience store with 4 fuel dispensers (8 vfps). As proposed, the project consists of maintaining the existing fueling pumps and expanding the existing convenience store by 1,474 square feet for a total of 3,874 square feet. Traffic to be generated by the proposed expansion project was forecast using the trip rates contained in the Institute of Transportation Engineers (ITE) *Trip Generation* report.⁵

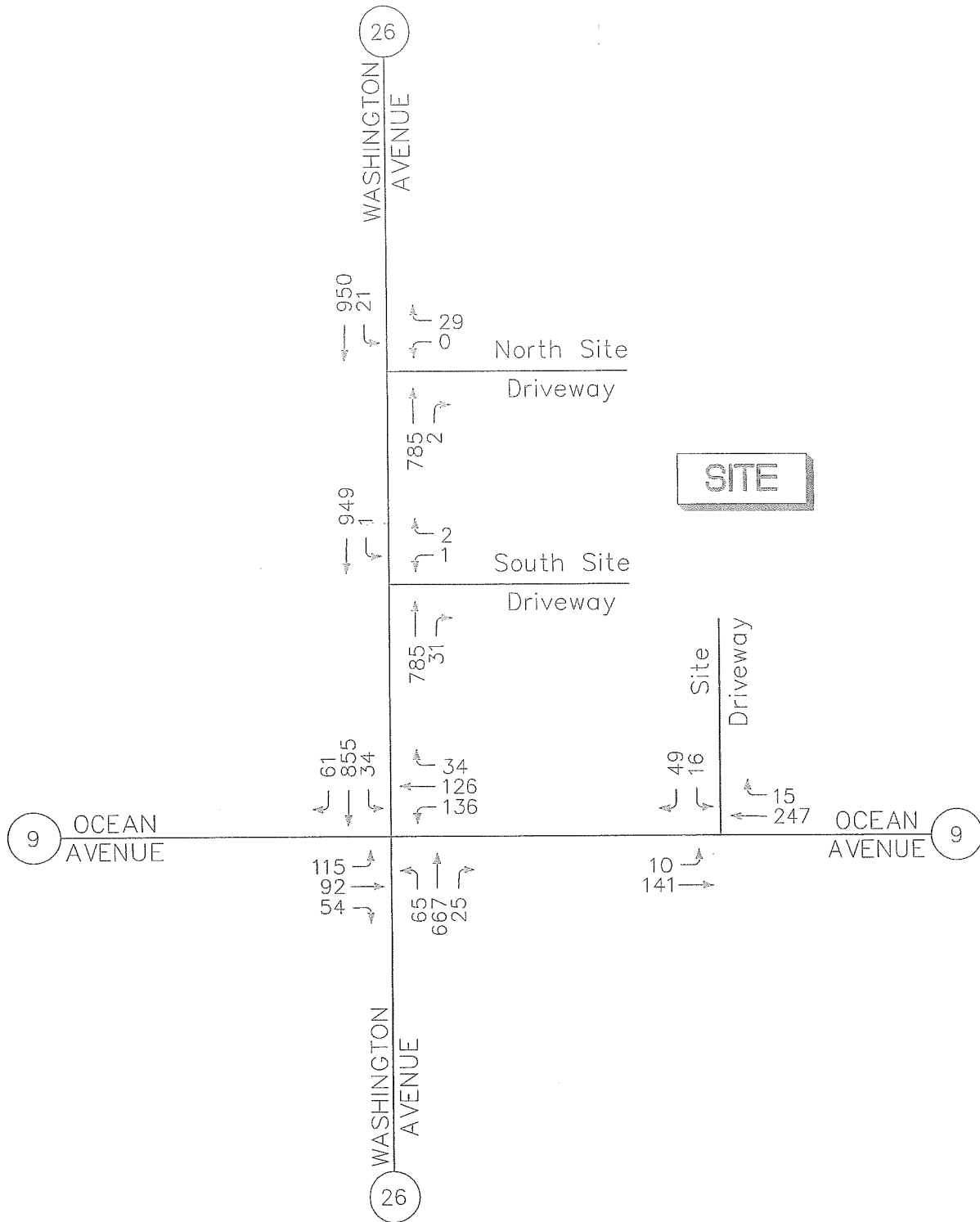
Not all of the vehicle trips expected to be generated by the proposed expansion project represent *new* trips on the study area roadway system. Studies have shown that for developments such as the one proposed, a substantial portion of the site-generated vehicle trips are already present in the adjacent passing stream of traffic or are diverted from another route to the proposed site. Based on information published in the ITE *Trip Generation Handbook*, the average *pass-by* trip percentage is 63 percent during the weekday AM peak hour and 66 percent during the weekday PM peak hour for Convenience Markets with Gasoline Pumps (Land Use Code 853).⁶ Table 5 provides a trip-generation summary for the additional traffic anticipated to be generated by the proposed expansion project. All trip-generation data are provided in the Appendix.

⁵ *Trip Generation, 8th Edition: an ITE Informational Report*. Washington, DC: Institute of Transportation Engineers, 2008.

⁶ Hooper, Kevin G. *Trip Generation Handbook: an ITE Recommended Practice*. Washington, D.C.: Institute of Transportation Engineers, 2004.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



NOT TO SCALE

Figure 5

2012 No-Build
Weekday AM
Peak Hour Traffic Volumes

**Table 4
SIGHT DISTANCE SUMMARY**

Location/Direction	Stopping Sight Distance (feet)		Intersection Sight Distance (feet)		
	Measured	Minimum Required ^a	Measured	MaineDOT Required ^b	Desirable ^c
Washington Ave. at West Site Driveways:					
<i>East of intersection</i>	500+	200	500+	250	412
<i>West of intersection</i>	500+	200	500+	250	379
Washington Avenue at East Site Driveway:					
<i>East of intersection</i>	500+	200	500+	250	412
<i>West of intersection</i>	500+	200	500+	250	379
Ocean Avenue at Site Driveway:					
<i>North of intersection</i>	500+	200	500+	250	324
<i>South of intersection</i>	500+	200	500+	250	302

^a Values based on AASHTO requirements for posted speed limit of 30 mph.

^b Values based on City of Portland's and MaineDOT's ISD requirements for posted speed limit of 30 mph.

^c Values based on AASHTO requirements for vehicles driving at 85th percentile speeds; Washington Avenue westbound (east of intersection) = 37 mph, Washington Avenue eastbound (west of intersection) = 34 mph, Ocean Avenue southbound (north of intersection) = 29 mph, Ocean Avenue northbound (south of intersection) = 27 mph.

As indicated in Table 4, available sight distances at the site driveway intersections with Washington Street and with Ocean Avenue exceed the AASHTO, City of Portland, and MaineDOT SSD and ISD requirements for safe operation based on the posted and observed speeds. To ensure the safe and efficient flow of traffic to and from the site, it is recommended that any proposed plantings, vegetation, landscaping, and signing along the site frontage be kept low to the ground (no more than 3.0 feet above street level) or set back sufficiently from the edge of Washington Avenue, Ocean Avenue, and the site driveways so as not to inhibit the available sight lines.

FUTURE CONDITIONS

To estimate the impact of site-generated traffic within the study area, existing traffic volumes were projected to the expected opening year (2012) of the development. This design horizon was chosen to be consistent with City of Portland and MaineDOT guidelines for the preparation of a traffic study. Traffic volumes on the roadway network at this time will include existing traffic, new traffic due to normal traffic growth, and traffic related to any significant development by others expected to be completed in the area within the 2012 design horizon. Consideration of these factors resulted in the development of 2012 No-Build traffic volumes, which assume that the proposed development is not built. The incremental impacts of the proposed project may then be determined by adding site-generated traffic volumes (Build conditions) and making comparisons to the No-Build conditions.

Traffic Growth

To develop the 2012 No-Build forecast volumes, two components of traffic growth were considered. First, an annual average traffic-growth percentage was determined. Based on historical traffic-volume data researched from MaineDOT, traffic volumes have been increasing at an average rate of 1.15 percent per year since 2000.³ Based on this information and as confirmed with City of Portland officials, a compounded annual traffic growth rate of 1.5 percent was assumed to account for general population growth and traffic generated by smaller area developments. The MaineDOT historical traffic-volume data are provided in the Appendix.

Second, any planned or approved specific developments in the area that would generate a significant volume of traffic on study area roadways by the 2012 design year were included. Based on discussions with officials from City of Portland and MaineDOT, there are no developments planned to be constructed within the design horizon that would affect traffic volumes within the study area. A traffic study was prepared for the Cheverus High School Master Plan that projected an expansion to the school's enrollment from 544 students in 2008 to 700 students by 2013.⁴ Based on discussions with Cheverus High School administrators, the current enrollment in the 2010-2011 school year is 499 students and there are no current plans to increase enrollment much further by 2012. The original Master Plan anticipated changing from an all-male enrollment to a co-educational student body, but school administrators indicated that this change is not expected to occur within the design horizon and may not be feasible at the current location.

³ *Traffic Volume Counts – 2009 Annual Report*; Maine Department of Transportation.

⁴ *Traffic Impact Study, Proposed Cheverus High School Master Plan*; Portland, Maine; Eaton Traffic Engineering; August 22, 2008.

**Table 3
OBSERVED TRAVEL SPEEDS ^a**

Location/Direction	Posted Speed Limit	Average Speed	85 th Percentile Speed ^b
Washington Avenue adjacent to the Site:			
<i>Northbound</i>	30	31	37
<i>Southbound</i>	30	25	34
Ocean Avenue adjacent to the Site:			
<i>Eastbound</i>	30	22	29
<i>Westbound</i>	30	19	27

^a In miles per hour (mph).

^b Speed at, or below which, 85 percent of all observed vehicles travel.

As shown in Table 3, the average speeds along Washington Avenue adjacent to the site were found to be between 25 and 31 miles per hour (mph) for northbound and southbound travel, with the 85th percentile speeds between 34 and 37 mph. The average speeds along Ocean Avenue adjacent to the site were found to be between 19 and 22 mph for eastbound and westbound travel, with the 85th percentile speeds between 27 and 29 mph. The 85th percentile speeds along Washington Avenue are slightly higher than the posted speed limit of 30 mph and the observed travel speeds along Ocean Avenue were found to be generally consistent with the posted speed limit of 30 mph when school children are not present.

Sight Distance

To identify potential safety concerns associated with site access and egress, sight distances have been evaluated at the site driveway locations to determine if the available sight distances for vehicles exiting the site meet or exceed the minimum distances required for approaching vehicles to safely stop. The available sight distances were compared with minimum requirements, as established by the American Association of State Highway and Transportation Officials (AASHTO)². AASHTO is the national standard by which vehicle sight distance is calculated, measured, and reported. In addition, the available sight distances were compared with City of Portland’s and MaineDOT’s sight distance requirements.

² *A Policy on Geometric Design of Highways and Streets*; American Association of State Highway and Transportation Officials (AASHTO); 2004.

Sight distance is the length of roadway ahead that is visible to the driver. The Stopping Sight Distance (SSD) is the minimum distance required for a vehicle traveling at a certain speed to safely stop before reaching a stationary object in its path. The values are based on a driver perception and reaction time of 2.5 seconds and a braking distance calculated for wet, level pavements. When the roadway is either on an upgrade or downgrade, grade correction factors are applied. The SSD is measured from an eye height of 3.5 feet to an object height of 2 feet above street level, equivalent to the taillight height of a passenger car. The SSD is measured along the centerline of the traveled way of the major road.

The Intersection Sight Distance (ISD) is provided on minor street approaches to allow the drivers of stopped vehicles a sufficient view of the major roadway to decide when to enter the major roadway. By definition, the ISD is the minimum distance required for a motorist exiting a minor street to turn onto the major street, without being overtaken by an approaching vehicle reducing its speed to turn from the design speed to 70 percent of the design speed. Based on City of Portland's and MaineDOT's regulations, the ISD is measured from an eye height of 3.5 feet to an object height of 4.25 feet above street level, 10 feet back from the edge of the travel way.

The SSD is generally more important as it represents the minimum distance required for safe stopping while the ISD is based only upon acceptable speed reductions to the approaching traffic stream. The ISD, however, must be equal to or greater than the minimum required SSD in order to provide safe operations at the intersection. In accordance with the AASHTO manual, *"If the available sight distance for an entering or crossing vehicle is at least equal to the appropriate stopping sight distance for the major road, then drivers have sufficient sight distance to anticipate and avoid collisions. However, in some cases, this may require a major-road vehicle to stop or slow to accommodate the maneuver by a minor-road vehicle. To enhance traffic operations, intersection sight distances that exceed stopping sight distances are desirable along the major road."* Accordingly, the ISD should be at least equal to the distance required to allow a driver approaching the minor road to safely stop.

The available SSD and ISD at the site driveway locations were measured and compared to minimum requirements, as established by AASHTO, City of Portland, and MaineDOT. Since the distance required to stop a vehicle is dependent on the speed of that vehicle, speed studies were conducted as presented in the previous section. Based on both the posted speed limit and the observed speeds, the SSD and ISD requirements at these intersections were calculated. The required minimum sight distances for each speed are compared to the available distances, as shown in Table 4.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

**Table 2
TRAFFIC COLLISION HISTORY SUMMARY**

Location	Number of Accidents		Severity ^a				Accident Type ^b						Percent During		
	Total	Average per Year	PD	PI	Possible Injury	F	CM	RE	HO	FO	SS	Ped	Other	Commuter Peak ^c	Wet/Icy Conditions
Washington Ave. at Cumberland Farms Driveways	0	0.0	0	0	0	0	0	0	0	0	0	0	0	0%	0%
Washington Ave. at Ocean Ave.	30	10.0	19	2	9	0	7	22	0	0	1	0	0	37%	27%
Washington Ave. between Ocean Ave. and Byfield Rd.	10	3.33	6	1	3	0	4	4	0	0	0	1	1	50%	50%
Ocean Ave. at Cumberland Farms Driveway	1	0.33	1	0	0	0	1	0	0	0	0	0	0	0%	0%
Ocean Ave. between Washington Ave. and Slemons Rd.	0	0	0	0	0	0	0	0	0	0	0	0	0	0%	0%

Source: MaineDOT Crash Records Section (2007 through 2009).

^a PD = property damage only; PI = personal injury; F = fatality.

^b CM = cross movement/angle; RE = rear end; HO = head on; FO = fixed object; SS = sideswipe; Ped = pedestrian.

^c Percent of vehicle incidents that occurred during the weekday AM and weekday PM commuter peak periods.

The Washington Avenue and Ocean Avenue intersection has experienced 30 reported collisions over the three-year analysis period with 37 percent (11 of 30) involving personal injury or possible injury. Of the 30 crashes, 73 percent (22 of 30) were identified as rear-end-type collisions, 27 percent (8 of 30) were identified as angle-type or crossing movement-type collisions, and 1 was identified as a sideswipe-type collision. The majority of the reported rear-end collisions (68 percent, 15 of 22) occurred on the Washington Avenue approaches to the signalized intersection with Ocean Avenue (9 on the southbound approach and 6 on the northbound approach). Most of the rear-end collisions were attributed to driver inattention and vehicles following too closely. One incident involved a motorist driving under the influence; seven collisions occurred when the pavement was wet, snowy, or icy; and two crashes occurred when the traffic signal was operating under flash mode. These reported collisions do not indicate a particular collision pattern correctable by engineering measures. A collision diagram provided by MaineDOT for this location is included in the Appendix.

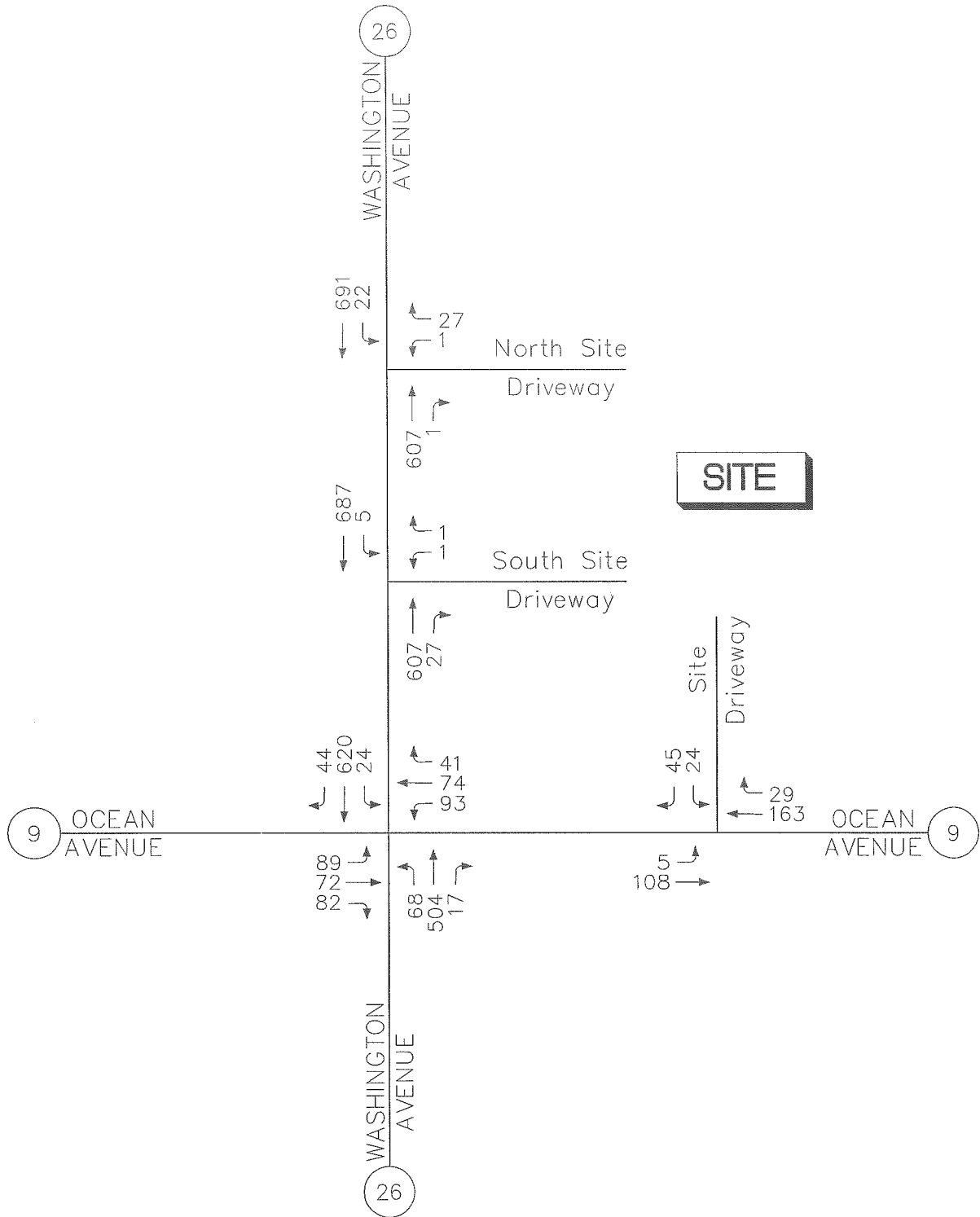
The Washington Avenue roadway segment between Ocean Avenue and Byfield Road experienced 10 reported collisions over the three-year analysis period with 40 percent (4 of 10) involving personal injury or possible injury. Of the 10 crashes, 40 percent (4 of 10) were identified as rear-end-type collisions, 40 percent (4 of 10) were identified as angle-type or crossing movement-type collisions, 1 was identified as an incident with a pedestrian violation, and 1 involved a vehicle backing out of a commercial driveway into oncoming traffic. Most of the reported collisions were attributed to driver inattention and failure to yield. One incident involved a motorist driving under the influence; and half of the collisions (5 of 10) occurred when the pavement was wet, snowy, or icy. Due to the number of curb cuts along this segment of Washington Avenue and the types of incidents reported, there does not appear to be a particular collision pattern correctable by engineering measures. A collision diagram provided by MaineDOT for this roadway segment is included in the Appendix.

Vehicle Speeds

Speed measurements were conducted along Washington Avenue and along Ocean Avenue adjacent to the site by measuring the elapsed time for vehicles traveling a short, pre-measured distance between two checkpoints. The travel time was recorded using ATRs and the speed was derived by dividing the elapsed time into the measured distance between checkpoints. The results of the speed measurements are summarized in Table 3.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



SITE



NOT TO SCALE

Figure 4

2009 Existing
Saturday Midday
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

Table 1
EXISTING TRAFFIC-VOLUME SUMMARY

Location/Time Period	Daily Volume (vpd) ^a	Peak-Hour Volume (vph) ^b	K-Factor (%) ^c	Directional Distribution ^d
Washington Ave. west of Ocean Ave.:				
<i>Weekday Daily</i>	19,930			
<i>Weekday AM Peak Hour</i>		1,691	8.5	54% SB
<i>Weekday PM Peak Hour</i>		1,829	9.2	58% NB
<i>Saturday Midday Peak Hour</i>		1,322	6.6	52% SB
Ocean Ave. north of Washington Ave.:				
<i>Weekday Daily</i>	4,940			
<i>Weekday AM Peak Hour</i>		430	8.7	66% WB
<i>Weekday PM Peak Hour</i>		398	8.1	65% WB
<i>Saturday Midday Peak Hour</i>		321	6.5	65% WB

^a In vehicles per day. Traffic counts upwardly adjusted to reflect 30th highest hour.

^b In vehicles per hour. Traffic counts upwardly adjusted to reflect 30th highest hour.

^c Percentage of daily traffic occurring during the peak hour.

^d SB = southbound, NB = northbound, WB = westbound.

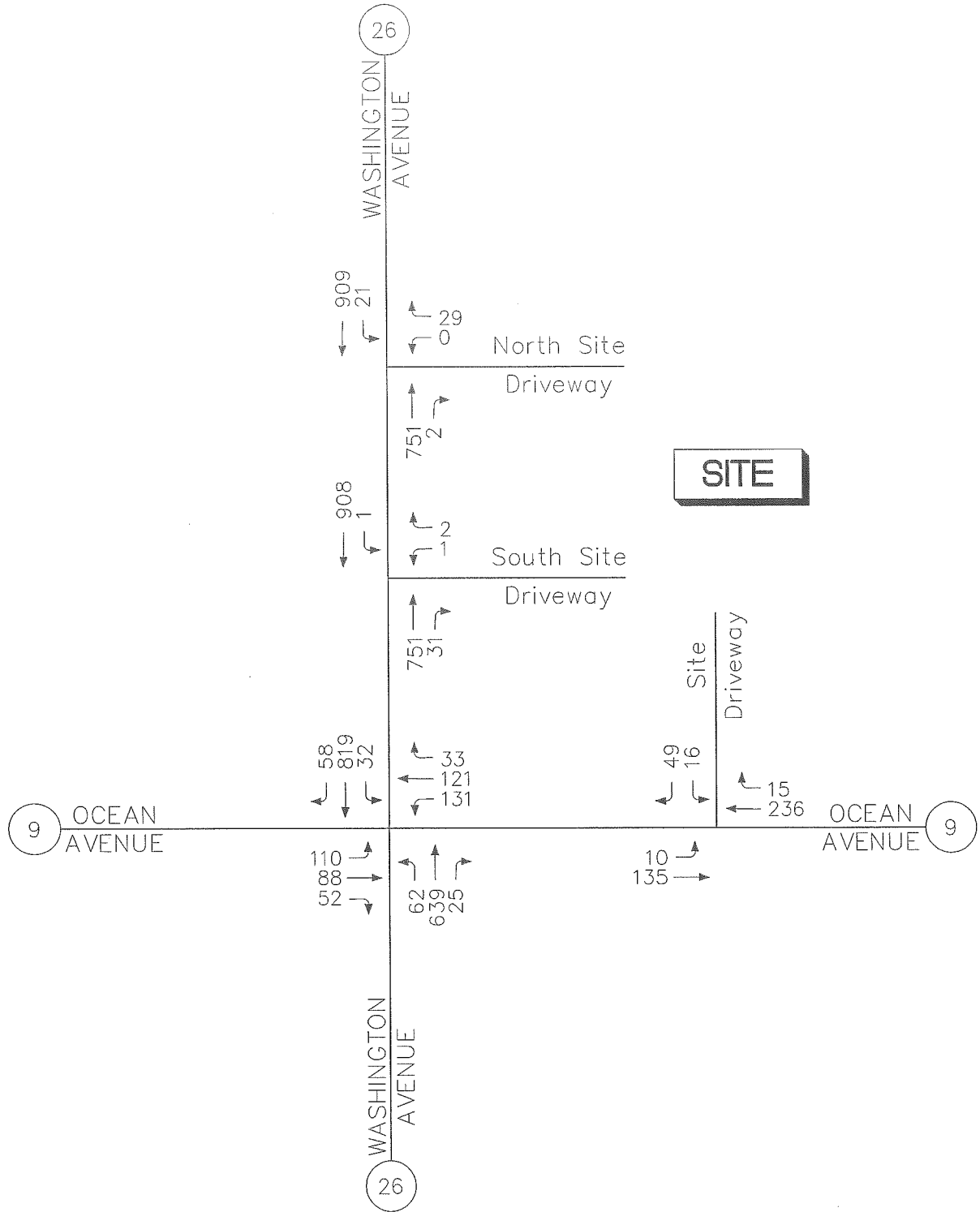
Traffic Collision History

Traffic collision data for the study area intersections were researched from the MaineDOT Crash Records Section between 2007 and 2009. The historical traffic collision information that has been collected for the study area locations is summarized in Table 2.

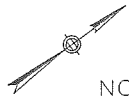
As shown in Table 2, the Washington Avenue intersection with Ocean Avenue and the roadway segment along Washington Avenue between Ocean Avenue and Byfield Road have reported 10 or more collisions during the three-year period. The remaining intersections and roadway segments were reported to have experienced one accident or less during the three-year time period. No fatalities were reported at any of the study area intersections or roadway segments.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



SITE



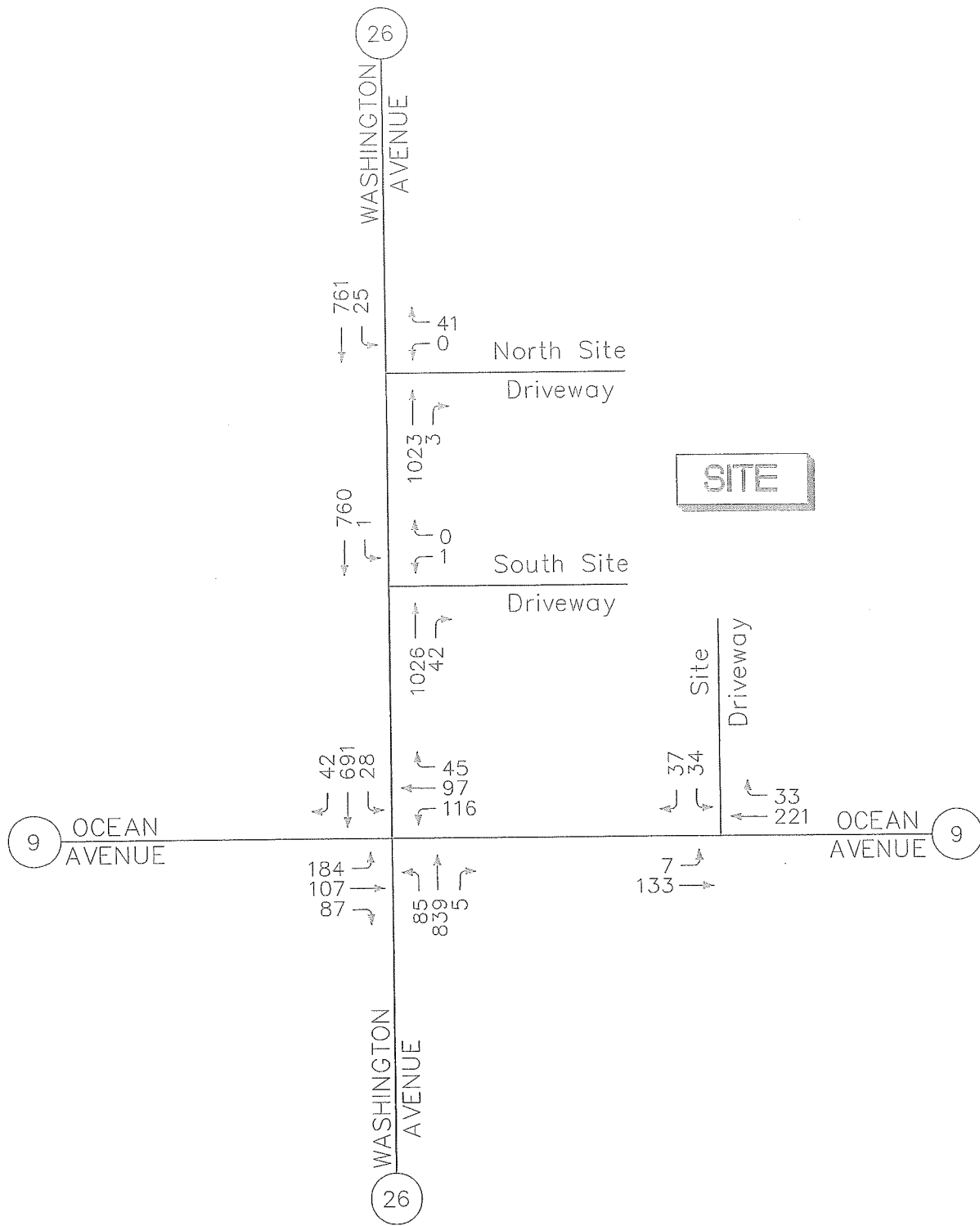
NOT TO SCALE

Figure 2

2009 Existing
Weekday AM
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



NOT TO SCALE

Figure 3

2009 Existing
Weekday PM
Peak Hour Traffic Volumes

Since the expansion project is expected to generate less than 100 additional passenger car equivalents (PCE) trips during the peak hours of traffic generation and the expansion project does not propose a change in use from the existing site, the project is not expected to require a *Traffic Movement Permit* from the Maine Department of Transportation (MaineDOT).

EXISTING CONDITIONS

Evaluation of the traffic impacts associated with the proposed expansion project requires an evaluation of existing and projected traffic volumes on the adjacent streets, the volume of traffic expected to be generated by the project, and the impact that this traffic will have on the adjacent streets and nearby intersections. In preparing this study for the proposed expansion project, the intersection of Washington Street and Ocean Avenue and the existing site driveway intersections were evaluated in accordance with current local and state guidelines.

Geometrics and Traffic Control

Washington Avenue and Ocean Avenue

Washington Avenue and Ocean Avenue intersect to form a four-way, signalized intersection under actuated traffic signal control. The Washington Avenue northbound and southbound approaches consist of an exclusive left-turn lane and a shared through/right-turn lane. The Ocean Avenue eastbound approach consists of an exclusive left-turn lane and a shared through/right-turn lane. The Ocean Avenue westbound approach consists of a single general-purpose travel lane. Based on field observations, however, vehicles were observed to utilize the wide approach as an exclusive left-turn lane and a shared through/right-turn lane. Directional travel along Washington Avenue and Ocean Avenue is separated by double yellow centerlines. The posted speed limit along Washington Avenue and Ocean Avenue is 30 miles per hour (mph). When school children are present, the posted speed limit along Ocean Avenue east of Washington Avenue is 15 mph. Crosswalks are striped across all legs of the intersection and sidewalks are provided along both sides of Washington Avenue and Ocean Avenue in the vicinity of the intersection.

Washington Avenue at Cumberland Farms Driveways

Washington Avenue intersects the Cumberland Farms north and south site driveways to form two unsignalized intersections, with the Cumberland Farms driveways approaches under STOP control. Washington Avenue adjacent to the site consists of a single general-purpose travel lane in the northbound direction. Washington Avenue southbound consists of two travel lanes with

the outside lane as a shared through/right-turn lane and the inside lane as an extension of the exclusive left-turn lane at the Ocean Avenue signalized intersection. Directional flow along Washington Avenue is separated by a double yellow centerline. The Cumberland Farms driveways allow both entering and exiting traffic.

Ocean Avenue at Cumberland Farms Driveways

Ocean Avenue intersects the Cumberland Farms site driveway to form an unsignalized intersection, with the Cumberland Farms driveway approach under STOP control. Ocean Avenue adjacent to the site consists of a single general-purpose travel lane in each direction separated by a double yellow centerline. The Cumberland Farms driveway allows both entering and exiting traffic.

Traffic Volumes

Base traffic conditions within the study area were developed by conducting manual turning movement counts (TMCs), vehicle classification counts, and automatic traffic recorder (ATR) counts in November 2009. The TMCs and vehicle classification counts were performed on November 18, 19, and 21, 2009 during the weekday AM peak period (7:00 to 9:00 AM), the weekday PM peak period (4:00 to 6:00 PM), and the Saturday midday peak period (11:00 AM to 2:00 PM) at the study area intersections. The ATRs were used to collect weekday daily traffic volumes along Washington Avenue and along Ocean Avenue adjacent to the site in November 2009. All traffic-count data are provided in the Appendix.

Traffic on a given roadway typically fluctuates throughout the year depending on the area and the type of roadway. To determine if the traffic-count data needed to be adjusted to account for this fluctuation, traffic-volume data from MaineDOT were researched.¹ Based on these weekly traffic-volume data, traffic volumes during the week that the traffic counts were conducted were found to be 9.2 percent below 30th highest hour traffic-volume conditions. Therefore, the November traffic counts were upwardly adjusted to provide the 30th highest hour traffic-volume conditions. The MaineDOT traffic data are provided in the Appendix. Table 1 summarizes the existing daily and peak-hour traffic volumes on Washington Avenue and on Ocean Avenue adjacent to the site. The weekday AM, weekday PM, and Saturday midday peak-hour traffic-flow networks are shown graphically on Figures 2 through 4, respectively.

¹ *Traffic Volume Counts – 2009 Annual Report*; Maine Department of Transportation; Urban Group 1, Weekly Group Mean Factors; 2011.

TECHNICAL MEMORANDUM

REF: NHX-2009626

DATE: September 6, 2011

TO: Ms. Kathleen A. Sousa
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

FROM: Mr. Jason R. Plourde, P.E., Assistant Vice President
Ms. Heather L. Monticup, Project Manager

RE: Traffic Impact and Access Study
Proposed Cumberland Farms Expansion
Portland, Maine

INTRODUCTION

Greenman-Pedersen, Inc. (GPI) has prepared this *Traffic Impact and Access Study* for a proposed expansion to the Cumberland Farms facility located at 801 Washington Avenue in Portland, Maine. The Cumberland Farms facility currently consists of a $\pm 2,400$ square foot convenience store with 4 fuel dispensers (8 vehicle fueling positions [vfps]). The proposed project consists of maintaining the existing fueling pumps and expanding the existing convenience store by 1,474 square feet for a total of 3,874 square feet. This traffic study has been prepared to evaluate the traffic impacts and access/egress requirements for the proposed expansion project.

The site is located on the northeast corner of the Washington Avenue (Route 26) and Ocean Avenue (Route 9) intersection. Access to the site is currently provided via two driveways on the east side of Washington Avenue and one driveway on the north side of Ocean Avenue. As proposed, the three driveways will be maintained. The site is bounded by the East Deering Fire Department to the east, Washington Avenue to the west, the Law Office of Patricia A. Nelson-Reade, R.N. to the north, and Ocean Avenue to the south. Figure 1 shows the site in relation to the surrounding roadways.

GPI Greenman-Pedersen, Inc.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

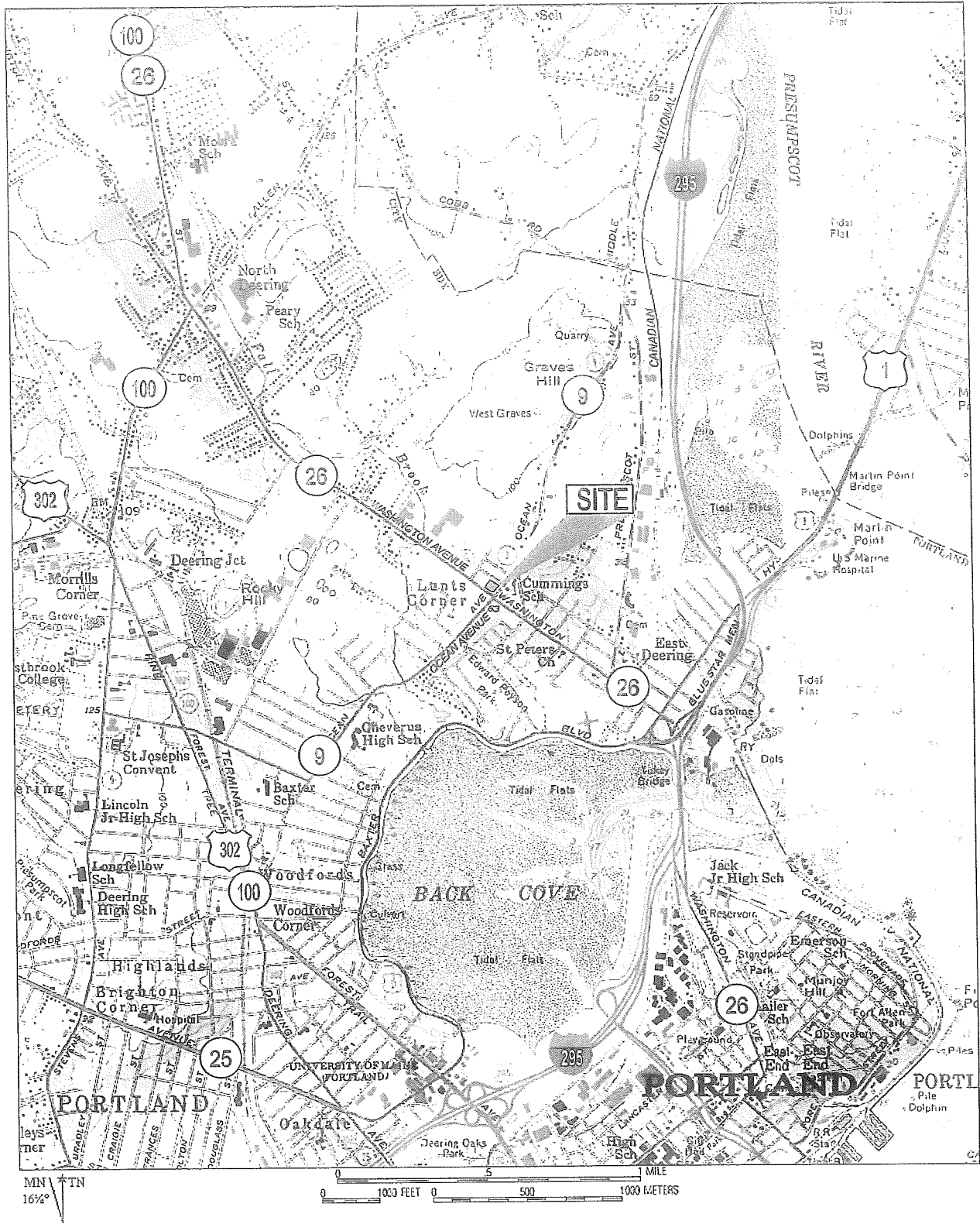





Figure 1
Site Location Map




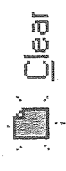
Account # **120570-02** Customer **CUMBERLAND FARMS V1079**

Address **801 WASHINGTON AVE PORTLAND ME 04103-**

Service	Service From	Service To	Exchange Date	Status	Status Date	Service Class
SEWER	07/10/1992	08/17/2007	08/17/2007	OPEN	07/10/1992	SMONTH
SEWER	08/17/2007			OPEN	08/17/2007	SMONTH
WATER	07/10/1992	08/17/2007	08/17/2007	OPEN	07/10/1992	WMEMBR
WATER	08/17/2007			OPEN	08/17/2007	WMEMBR

 Insert
 Modify
 Remove

Reading Date	Read By	Source	Reason	Reading	Usage	Billable	Rdr
01/03/2011 15:12	MC#2	R		660.00	19.00	19.00	
12/01/2010 13:44	MC#2	R		641.00	23.00	23.00	
10/28/2010 14:58	MC#2	R		618.00	20.00	20.00	
09/30/2010 14:58	MC#2	R		598.00	22.00	22.00	
08/30/2010 14:13	MC#2	R		576.00	22.00	22.00	
07/29/2010 16:13	MC#2	R		554.00	21.00	21.00	
06/30/2010 14:14	MC#2	R		533.00	23.00	23.00	
05/28/2010 14:06	MC#2	R		510.00	18.00	18.00	
04/29/2010 15:05	MC#2	R		492.00	16.00	16.00	
03/31/2010 13:34	MC#2	R		476.00	15.00	15.00	
03/02/2010 14:01	MC#2	R		461.00	18.00	18.00	
01/28/2010 14:07	MC#2	R		443.00	16.00	16.00	
12/29/2009 14:16	MC#2	R		427.00	19.00	19.00	
11/25/2009 15:27	MC#2	R		408.00	16.00	16.00	
10/26/2009 14:15	MC#2	R		392.00	20.00	20.00	

 Insert
 Modify
 Remove
 Clear Bill

TRAFFIC IMPACT AND ACCESS STUDY

PROPOSED CUMBERLAND FARMS EXPANSION
PORTLAND, MAINE

GPI

105 CENTRAL STREET, SUITE 4100
STONEHAM, MASSACHUSETTS 02180
(781) 279-5500

PREPARED FOR:

CUMBERLAND GULF GROUP OF COMPANIES
100 CROSSING BOULEVARD
FRAMINGHAM, MASSACHUSETTS 01702

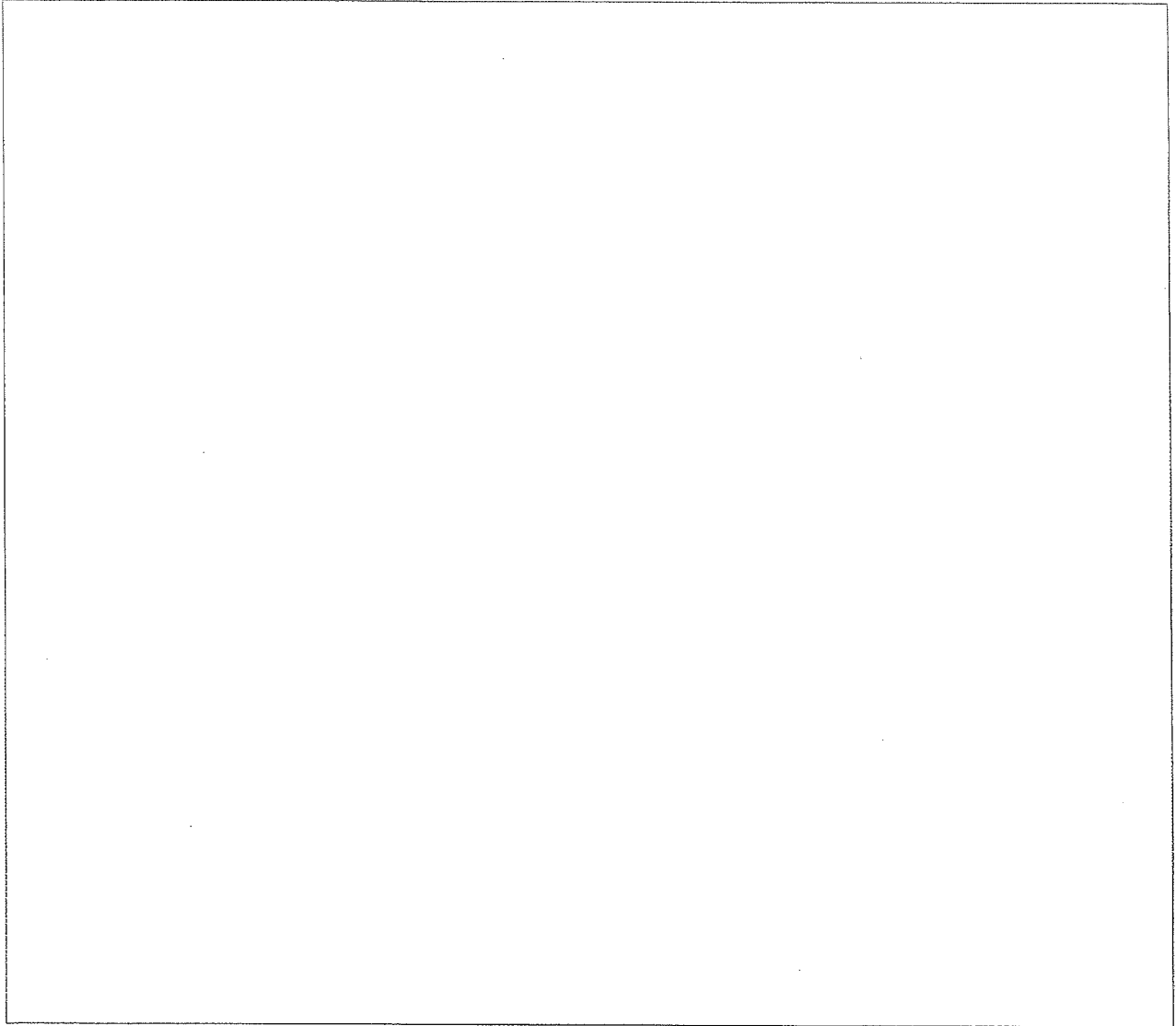
SEPTEMBER 2011

GPI

*Traffic Impact and Access Study
Proposed Cumberland Farms Expansion
Portland, Maine
September 2011*

Note: On the submitted plans, please show the locations, where the building's sanitary, and process water sewer laterals, exit the facility, where they enter the city's sewer, the location of any control manholes, wet wells, or other access points, and the locations of any filters, strainers, or grease traps.

Notes, Comments, or Calculations:



Comparison of Actual Water Usage vs Design Flow

Water Meter Readings

Date	# Days	Reading 100 CF	Usage Cubic Feet	Average CF/Day	Usage Gallons	Average GPD
10/26/2009		392				
11/25/2009	30	408	1,600	53.33	11,968	398.93
12/29/2009	34	427	1,900	55.88	14,212	418.00
1/28/2010	30	443	1,600	53.33	11,968	398.93
3/2/2010	33	461	1,800	54.55	13,464	408.00
3/31/2010	29	476	1,500	51.72	11,220	386.90
4/29/2010	29	492	1,600	55.17	11,968	412.69
5/28/2010	29	510	1,800	62.07	13,464	464.28
6/30/2010	33	533	2,300	69.70	17,204	521.33
7/29/2010	29	554	2,100	72.41	15,708	541.66
8/30/2010	32	576	2,200	68.75	16,456	514.25
9/30/2010	31	598	2,200	70.97	16,456	530.84
10/28/2010	28	618	2,000	71.43	14,960	534.29
12/1/2010	34	641	2,300	67.65	17,204	506.00
1/3/2011	33	660	1,900	57.58	14,212	430.67
Total	434		26,800	61.75	200,464	461.90

Design Flow Based on Maine Subsurface Disposal System Rules

Service Station:

100 gpd per fuel pump cabinet plus 15 gpd per employee (within 24-hr period)

Shopping Centers or stores with public restrooms and showers:

400 gpd per water closet plus 20 gpd per shower plus 15 gpd per employee (within 24 hr period)

Cumberland Farms, Inc.

4 Fuel Dispensers x 100 gpd	400
2 Water Closets x 400 gpd =	800
4 Employees x 15 gpd =	60

Total Design Flow: 1,260

To determine peak flow from water meter readings: multiply monthly average by 1.9

Peak Flow: 877.61

EXHIBIT C

**CUMBERLAND FARMS, INC.
WASTE HAULER INSURANCE REQUIREMENTS**

The insurance coverages must be written by carriers rated by A.M. Best as A-, VII or better.
The insurance coverages are in all cases Primary for Services Contractor and Non-Contributory for Cumberland Farms or Gulf Oil. Any deductibles or retentions shall be the sole responsibility of Services Contractor.
The insurance certificate must show the complete insurance carrier name as it appears in the A.M. Best Guide.
The insurance certificate must be an original document signed by the producer; copies are not acceptable.
The insurance certificate must be completed in its entirety and contain policy numbers; binders are not acceptable.
The insurance certificate must show 30 days notice of cancellation.
The insurance certificate must show Cumberland Farms, Inc. and Gulf Oil, LP as an Additional Insured on all liability policies.

COMMERCIAL GENERAL LIABILITY

Must be an occurrence-based policy which covers all aspects of insured's operations including blanket contractual liability, products and completed operations coverage, bodily injury liability and broad form property damage, with limits of coverage not less than the following:

\$1,000,000 each occurrence
\$2,000,000 aggregate

BUSINESS AUTOMOBILE LIABILITY

Must include coverage for **ANY AUTO** with limits of coverage not less than the following:

\$1,000,000 combined single limit

EXCESS LIABILITY

Must be excess coverage over all noted liability policies, including Employer's Liability and covering all aspects of insured's operations with limits of coverage not less than the following: each occurrence

\$2,000,000

\$4,000,000 aggregate

WORKERS' COMPENSATION

Must include coverage for Occupational Disease, for states where work is being conducted, operations are performed and where exposures may exist, in accordance with state and federal laws and regulations with limits of coverage not less than the following:

WORKERS' COMPENSATION

Statutory Limits

EMPLOYERS' LIABILITY

\$500,000 each accident
\$500,000 disease each employee

NOTE: Please mail original insurance information only to:

Cumberland Gulf Group of Companies
Maintenance Dept. Services Contractor
P.O. Box 12010-CF
Hemet, CA 92546-8010

CITY OF PORTLAND WASTEWATER CAPACITY APPLICATION

Department of Public Services,
55 Portland Street,
Portland, Maine 04101-2991



Mr. Frank J. Brancely,
Senior Engineering Technician,
Phone #: (207) 874-8832,
Fax #: (207) 874-8852,
E-mail: fjb@portlandmaine.gov

Date: 9/19/11

1. Please, Submit Utility, Site, and Locus Plans.

Site Address: 801 Washington Ave
(Regarding addressing, please contact Leslie Kaynor, either at 756-8346, or at LMK@portlandmaine.gov)
Proposed Use: Retail Motor Fuel Outlet
Previous Use: Retail Motor Fuel Outlet
Existing Sanitary Flows: 462 (Monthly Average) GPD
Existing Process Flows: N/A GPD
Description and location of City sewer, at proposed building sewer lateral connection:
Maintain existing connection to City sewer system.

Chart Block Lot Number: Map 63, Block B
Lots 3, 4, 5, 14, 15 & 17

Site Category	Commercial	<u>X</u>
	Industrial (complete part 4 below)	_____
	Governmental	_____
	Residential	_____
	Other (specify)	_____

Clearly, indicate the proposed connection, on the submitted plans.

2. Please, Submit Domestic Wastewater Design Flow Calculations.

Estimated Domestic Wastewater Flow Generated: 1,260 (See Attached Spreadsheet) GPD
Peaking Factor/ Peak Times: Peak Time of Business = 7:00 - 9:00 AM & 4:00 - 6:00 PM
Specify the source of design guidelines: (i.e. "Handbook of Subsurface Wastewater Disposal in Maine,"
"Plumbers and Pipe Fitters Calculation Manual," Portland Water District Records, Other (specify)
Handbook of Subsurface Wastewater Disposal in Maine

Note: Please submit calculations showing the derivation of your design flows, either on the following page, in the space provided, or attached, as a separate sheet.

3. Please, Submit Contact Information.

Owner/Developer Name: Dawn Johnson c/o Cumberland Farms, Inc.
Owner/Developer Address: 100 Crossing Boulevard, Framingham, MA
Phone: (508) 271-1431 Fax: (781) 459-1082 E-mail: djohnson@cumberlandgulf.com
Engineering Consultant Name: Frank C. Monteiro, PE c/o MHF Design Consultants, Inc.
Engineering Consultant Address: 44 Stiles Road, Suite One, Salem, NH 03079
Phone: (603) 893-0720 Fax: (603) 893-0733 E-mail: fcm@mhfdesign.com
City Planner's Name: Barbara Barhydt Phone: (207) 874-8699

Note: Consultants and Developers should allow +/- 15 days, for capacity status, prior to Planning Board Review.

4. Please, Submit Industrial Process Wastewater Flow Calculations. N/A.

Estimated Industrial Process Wastewater Flows Generated: _____ GPD
Do you currently hold Federal or State discharge permits? Yes _____ No _____
Is the process wastewater termed categorical under CFR 40? Yes _____ No _____
OSHA Standard Industrial Code (SIC): _____
(<http://www.osha.gov/oshstats/sicser.html>)
Peaking Factor/Peak Process Times: _____

EXHIBIT A
Cumberland Farms, Inc.
Solid Waste Disposal Services
SCOPE OF WORK

Cumberland Farms Inc. (CFI) requires Provider to furnish equipment and services to dispose of and/or recycle waste materials (hereinafter "services"). For the purpose of this scope of work "waste materials" means all non-hazardous solid waste and/or recyclable materials generated by CFI's service addresses. Recyclable materials shall include any type of material that can be recycled or recovered whether separated or not separated prior to waste materials collection.

- All materials deposited in the containers for disposal and/or recycling will be normal refuse or "solid waste" as that term is commonly referred to or defined in applicable environmental law and regulations, and will not include "hazardous waste" or other such material as defined in applicable environmental laws and regulations.
- Provider shall assume full responsibility for the handling, processing, transportation, shipment, transshipment and disposal and/or recycling of all solid wastes in accordance with applicable environmental laws and regulations and at approved solid waste and/or recycling facilities or landfills, and shall take all necessary precautions to avoid any releases of hazardous waste into the environment.
- In the event provider discovers any hazardous materials in any shipment of solid waste, or any release of any hazardous material into the environment, it shall promptly comply with all applicable laws and notify Cumberland Farms immediately.
- Provider shall be familiar and comply with all regulations and or ordinances including those governing fire lane accessibility, overnight noise, and "time of service" restrictions.
- All equipment furnished by provider shall remain the property of provider and Cumberland Farms shall bear no responsibility or liability for any loss or damage to the equipment or for its contents at any Cumberland Farms location.
- Provider's equipment will be maintained in good condition at all times and when necessary will be replaced at Cumberland Farms discretion free of charge.
- Provider will ensure to close all dumpster enclosures after service at each facility (if applicable) and will be responsible for replacing all missing gate locks.
- Provider is responsible for damages to persons or property as a result of any gates left open by Provider or its subcontractors.
- Provider shall provide all services as designated by the Client including open boxes, temporary equipment, and other extra services as required.

EXHIBIT B

MEMBER	STORE	ADDRESS	CITY	ST	TRASH	FREQ	PRICE	D/E	FREQ
TROIANO WASTE	5604	801 Washington Avenue	Portland	ME	10 YARD	1X WEEK	\$ 255	8 YARD	1X WEEK
TROIANO WASTE	5607	197 Main Street	Gorham	ME	6 YARD	1X WEEK	\$ 168	6 YARD	1X WEEK
TROIANO WASTE	5609	287 Ocean House Road	Cape Elizabeth	ME	6 YARD	Every Other Week	\$ 90	6 YARD	Every Other Week
TROIANO WASTE	5613	512 Woodford Street	Portland	ME	10 YARD	1X WEEK	\$ 235	6 YARD	1X WEEK
TROIANO WASTE	5614	245 Sebastian Road	Sebaste	ME	6 YARD	1X WEEK	\$ 150	10 YARD	1X WEEK
TROIANO WASTE	5616	564 Alfred Street	Bridgford	ME	6 YARD	1X WEEK	\$166.00	6 YARD	1X WEEK
TROIANO WASTE	5617	441 Payne Road	Scarborough	ME	8 YARD	1X WEEK	\$ 184	8 YARD	1X WEEK
TROIANO WASTE	5619	939 US Rte 1	Yarmouth	ME	10 YARD	1X WEEK	\$ 200	8 YARD	1X WEEK

23. **Official Notices and Points of Contacts:** Unless otherwise provided herein, any notice required or permitted to be given hereunder by one party to the other shall be in writing and given in person, email with acknowledgement of receipt by a duly authorized representative, facsimile with verification of receipt, commercial courier with proof of delivery, or certified mail, returned receipt requested. The effective date will be the date received except for certified mail, for which delivery will be effective upon receipt, rejection or two (2) delivery attempts. The address of Cumberland Farms and Provider for all notices hereunder shall be:

If to: Troiano Waste Service, Inc.
PO Box 3541
Portland, ME 04104

Attn: Nelson Libby

If to Cumberland Farms:
Cumberland Farms, Inc.
100 Crossing Boulevard
Framingham, MA 01702
Attention: Tom Cacciola

With a copy to:
Cumberland Farms, Inc
100 Crossing Boulevard
Framingham, MA 01702
Attention: Eric Hemmalin

Either party may designate another address for all purposes of this Agreement by giving to the other party written notice of such address change.

24. **Entire Agreement:** This Agreement, together with all of the attached exhibits, constitutes the entire agreement between Cumberland Farms and Provider. It supersedes all prior contemporaneous communications, representations, understandings or agreements whether oral or written with respect to the subject matter thereof and has been induced by no representations, statements, or agreements other than those herein expressed.

25. **Other Payment Arrangements:** Upon Provider's request, Cumberland Farms may apply monies owed under this Agreement for payment directly to Provider's subcontractors, suppliers and materialmen and deduct the same amount from monies owed to Provider, provided that Provider has confirmed in writing that the subcontractor's, supplier's, and/or materialmen's work or product is not disputed by Provider. Provider will release Cumberland Farms from any liability associated with any Contractor's dispute with any subcontractor's suppliers and/or materialmen.

26. **Subcontractors:** Cumberland Farms reserves the right to approve all subcontractors for work at its locations.

27. **Liens:** To the extent enforceable under controlling law, Provider shall not cause, suffer or permit any mechanic's lien or materialmen's liens to be filed against any Cumberland Farms' property by reason of any work, labor, services or materials performed or furnished by or on behalf of Provider. If any such lien is filed, Provider shall cause such lien to be released of record by payment, bond, or order of a court of competent jurisdiction within 15 business days after the receipt of notice of the filing.

28. **No Exclusive Remedy:** No right or remedy conferred upon or reserved by either party in this Agreement is intended to be exclusive of any other right or remedy herein or by law provided, but each will be cumulative and in addition to every other right or remedy given herein or now or hereafter existing at law or in equity.

29. **No Third-Party Beneficiaries:** Nothing in this Agreement, whether express or implied, is intended to confer any rights or remedies under or by reason of this Agreement on any person, other than the parties and their respective permitted successors and assigns, nor is anything in this Agreement intended to relieve or discharge the obligation or liability of any third person to any party, nor give any third person any right of subrogation or action against any party.

30. **Successors:** This Agreement and all of the provisions hereof will be binding upon and inure to the benefit of the Parties hereto and their respective successors and permitted assigns.


Provider and Cumberland Farms acknowledge that they have read this Agreement, understand it, and agree to be bound by its terms. Each representative signing below warrants that he or she is duly authorized to bind his or her respective party.

PROVIDER

CUMBERLAND FARMS, INC.

By: 

Duly Authorized



Duly Authorized

10. **Contractor Relationship:** In furnishing services hereunder, Provider shall be acting as an independent contractor in relation to Cumberland Farms. Neither party shall have the right to act in any manner to obligate or bind the other in any manner whatsoever and nothing contained herein shall give, or is intended to give, any rights of any kind to any third person or party. Neither Provider nor any personnel employed by or affiliated with Provider for the benefit of Cumberland Farms shall be considered as having employee status at Cumberland Farms. The parties acknowledge that they are not entering a joint venture or partnership with each other, and neither party shall represent to a third party that it has entered into such a joint venture or partnership.
11. **Cumberland Farms Employees:** No employee(s) or former employee(s) of Cumberland Farms may be employed by Provider at Cumberland Farms facilities to perform services pursuant to this Agreement without the prior written consent of Cumberland Farms. Cumberland Farms will promptly comply with any written request from Cumberland Farms to replace or substitute any employee performing services under this Agreement.
12. **Default/Self Help:** In the event Provider fails to perform the services as set forth in this Agreement, Cumberland Farms may provide notice verbally, by email, or facsimile to Provider of such failure and Provider shall cure such failure within three business days from the date of the notice. If Provider does not cure such failure within the required period of time, Cumberland Farms may invoke a self-help privilege by hiring a third party contractor to remedy the failure. In that event, Provider will pay Cumberland Farms on demand the difference between its pricing and the third party contractor pricing.
13. **Taxes:** Provider shall, and shall cause its subcontractors to, pay all federal, state and local income taxes, employment taxes, and sales, consumer, excise, use, and other similar taxes incurred by either party as a result of the services (other than taxes solely attributed to Cumberland Farms' income). Taxes that are levied upon the provider, by a government agency during the term of this agreement, and are related to the services, but are not in effect at the signing of this agreement, may be added to the base fee upon approval by Cumberland Farms at its sole discretion.
14. **Compliance with Laws:** Provider shall comply with all federal, state and local laws, regulations, ordinances or codes applicable to it and the performance of its obligations pursuant to this Agreement, including federal statutes commonly known as CERCLA, RCRA, OSHA, ADA, ADEA, FDCA, USA PATRIOT Act, and Title VII of the Civil Rights Act of 1964, as each of the same may be amended from time to time, and similar state or local counterparts.
15. (A) **Hazardous Materials:** Provider will assume full responsibility for any release, spill, leak, discharge, disposal, pumping, pouring, emission, emptying, injecting, leaching, dumping or escaping ("Release") or threat of Release of any hazardous substance into the environment at any location at which services are performed, if caused by Provider, its agents, servants, invitees, employees or subcontractors.. Provider will take all necessary precautions to avoid any such Releases or threats of Release. Provider will assume full responsibility for the handling and disposal of hazardous materials in accordance with applicable state, federal and local laws.

(B) **Emergency Conditions:** In the event of emergency conditions, including the discovery of a Release or threat of Release of hazardous substances into the environment, Provider shall comply with all applicable laws and immediately notify Cumberland Farms of such emergency conditions.

16. **Disputes/Governing Law:** Any dispute concerning any question of fact or law arising under this Agreement which is not disposed of by agreement between Provider and Cumberland Farms, shall be submitted to arbitration by a single arbitrator under the rules of the American Arbitration Association as then in effect and the award of the arbitrator shall be enforceable in any court of competent jurisdiction. The arbitration is to be conducted in Boston, Massachusetts. This Agreement shall be interpreted under the laws of the Commonwealth of Massachusetts, notwithstanding any choice of law rules.

17. **Proprietary Information:** Any proprietary information concerning Cumberland Farms, including but not limited to its business, operating, financial or marketing information, which is obtained by Provider incident to the performance of this Agreement shall remain the property of Cumberland Farms, is considered confidential, and no rights or licenses for its use is granted to Provider. Provider shall keep all confidential information in strict confidence and not disclose it without the prior written consent of Cumberland Farms. The provisions of this section will survive the termination of this Agreement.

18. **Publicity:** Provider shall not issue any publicity releases (including news releases, articles and advertising) relating to this Agreement and the services without the prior written approval of Cumberland Farms.

19. **Modifications:** This Agreement may be modified or rescinded only in writing between the parties' duly authorized agents.

20. **Waiver:** The failure of either party to exercise any rights or insist in any instance upon strict performance by the other party of any provision in this Agreement will not constitute a waiver of that provision, any other provision, or any future obligation to comply with that provision or any other provision of this Agreement.

21. **Assignment:** Provider shall not assign any part of this Agreement nor delegate any duties herein without Cumberland Farms' prior written consent. Any purported assignment or delegation made without Cumberland Farms' consent will be null and void.

22. **Severability:** In case any one or more of the provisions of this Agreement should be invalid, illegal, or unenforceable in any respect, the validity, legality, and enforceability of the remaining provisions of this Agreement will not in any way be affected or impaired thereby.

WASTE HAULERS SERVICES AGREEMENT

This Services Agreement ("Agreement"), dated March 1, 2011, is between Cumberland Farms, Inc., of 100 Crossing Blvd., Framingham, MA 01702 ("Cumberland Farms"), and Troiano Waste Service, Inc., P.O. Box 3541, Portland, ME 04104 ("Provider").

In consideration of the covenants herein contained, Cumberland Farms and Provider hereby agree as follows:

1. **Services:** Provider shall perform the waste hauling services listed on Exhibit A. Cumberland Farms will not be responsible for any other work ("out of scope" or "other work") performed without its prior consent. Out of scope services include but are not limited extra pickups and increasing container sizes.
2. **Price & Locations:** The services will be performed per the pricing and locations listed in Exhibit B. Cumberland Farms will not be responsible for any surcharges, and/or penalties assessed by the Provider as a result of any blocked dumpsters (not caused by Cumberland Farms or its agents or employees), or any additional fees including but not limited to: fuel surcharges, environmental permit fees or taxes (including transportation fees assessed by any government agency). All fees related to the services are included in the base prices and will not be added to the costs of service during the term of Agreement.
3. **Term:** The term of this Agreement is March 1, 2011 through February 28, 2013, unless earlier terminated as provided below. The Agreement may be renewed at Cumberland Farms' option for unlimited two year terms, upon at least 30 days prior written notice to Provider. Pricing will remain the same for each additional term.
4. **Billing:** Provider must invoice Cumberland Farms within 60 days of completion of services or out of scope work for each location. Payment terms for work completed are net 30 days from the date of receipt of invoice. Out of scope work must be billed separately or itemized on the invoices for services. Cumberland Farms may set off against any invoiced amount, monies owed to Cumberland Farms by Provider. Cumberland Farms will not be responsible for any bills submitted after 60 days of completion of services or out of scope work. Cumberland Farms may, at its discretion, charge a \$50 processing fee for invoices submitted more than 60 days from completion date.
5. **Insurance:** Provider represents that it now carries, and agrees it will continue during the term of this Agreement to carry, as a minimum, insurance in amounts set forth on attached Exhibit C or such other amounts or other coverage as Cumberland Farms may from time to time establish in its sole discretion. Cumberland Farms acknowledges that the services performed under this Agreement may be performed by technicians who are independent distributors for Provider who may not be required, under applicable law to carry workers' compensation coverage on themselves.

6. **Indemnification:** Provider shall indemnify, defend and hold harmless Cumberland Farms and its affiliates, directors, officers, employees, shareholders, members, managers, agents and representatives (the "Cumberland Parties") from any and all liabilities, damages, losses, claims, causes of action, costs and expenses, of any kind or nature (including without limitation, reasonable attorneys' fees and technical consultants' fees and expenses), as they come due and amounts paid in judgment or settlement incurred or sustained by or asserted against the Cumberland Parties in any manner directly or indirectly arising out of or relating to the services provided by Provider or its agents, subcontractors, or employees; or any breach by Provider, its employees, agents or subcontractors of any covenant or other agreement set forth in this Agreement, except for damages related to Cumberland Farms' sole negligence. The indemnification rights of the Cumberland Farms entities will survive the termination of this Agreement and shall not in any way be altered, diminished or limited without agreement by Cumberland Farms in writing prior to a claim being made.

7. **Warranty:** Provider warrants that all services will be performed in a professional and workmanlike manner, and will be free from any defects in workmanship and material and will conform to the requirements of this Agreement.

8. **Ownership of Documents:** Cumberland Farms shall have complete and unrestricted right to use all documents prepared by Provider in connection with its performance of the services described or referred to herein. Said documents are to be the property of Cumberland Farms and are not to be used on other projects except by agreement of Cumberland Farms in writing.

9. **Termination:** Notwithstanding the term of this Agreement set forth above:

- A. Either party may terminate this Agreement in whole or in part upon at least 30-days prior written notice to the other party without cause or if the other party: (i) becomes insolvent or seeks protection, voluntarily or involuntarily, under any bankruptcy, insolvency, reorganization, or similar law; (ii) makes a general assignment for the benefit of creditors; or (iii) fails to perform any of its obligations set forth in this Agreement.
- B. Upon receipt of any termination notice by Cumberland Farms, Provider shall immediately discontinue services on the date and to the extent specified in the notice. Within 30 days from the date of any final invoice, Cumberland Farms shall pay Provider for services performed prior to the time specified in said notice, to the extent not previously reimbursed, at the Rates set forth herein. Provided, however, Cumberland Farms may withhold money due for any services not performed in accordance with the requirements of this Agreement. Provider is not entitled to, and shall not seek, any other reimbursement or payment, claims, or damages for lost profits or incidental or consequential damages.

Jamie Cough
Energy Services Advisor
Central Maine Power Company
162 Canco Road
Portland, ME 04103
207-842-2367 office
207-458-0382 cell
207-626-4082 fax

APPLICATION OF CUMBERLAND FARMS, INC.

801 WASHINGTON STREET

MANAGEMENT OF SOLID WASTE GENERATED ON-SITE

APPLICATION OF CUMBERLAND FARMS, INC.

801 WASHINGTON STREET

EVIDENCE OF UTILITY CAPACITY

Kathleen Sousa

From: Dominic A. Taverna
Sent: Monday, October 17, 2011 8:33 AM
To: Kathleen Sousa
Cc: 'brant@dipoulin.com'; John Martin; Michael Durant
Subject: FW: Cumberland Farms Store- 801 Washington Ave Portland, ME 441-1065485-012

Categories: Red Category

Kathleen

I received a voice mail on Friday from Jamie at Central Maine Power. I returned his VM and left my contact information on Friday. Please see the attached E-mail from Jamie regarding power at this location. Let me know if you need anything else on this.

Dom

Dominic A. Taverna
Senior Construction Project Manager
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

Office 508-270-4407
Cell 508-880-2678
Email: dtaverna@cumberlandgulf.com

This electronic message contains information from Cumberland Gulf Group of Companies, which may be privileged. The information is intended to be for the use of the addressee only. If you are not the addressee, note that any disclosure, copy, distribution or use of the contents of this message is prohibited.

From: Cough, Jamie [<mailto:Jamie.Cough@cmnco.com>]
Sent: Monday, October 17, 2011 6:32 AM
To: Dominic A. Taverna
Subject: Cumberland Farms Store- 801 Washington Ave Portland, ME 441-1065485-012

Dominick:

This store currently has a peak load of 58 kw (approximately 64 kva), and is served by a 3x50 kva bank (150 kva total). This 600A service can be supported by the existing transformer and service drop. Should the actual demands approach the transformer rating of 150 kva, we would be looking at installing an additional run of 336 to the service drop. Given the diversification of your loads, it is not likely.

If you have any additional questions, please contact me.

Regards,

Jamie



September 15, 2011

Department of Planning & Urban Development
Division of Planning
Portland City Hall
389 Congress Street
Portland, ME 04101

RE: Evidence of Financial Capacity

Dear Department of Planning & Urban Development:

Cumberland Farms, Inc. is proposing a building addition and other site improvements at 801 Washington Avenue. The purpose of this letter is to satisfy your requirement of evidence of financial capacity. This project is expected to cost \$979,250 and the company has sums well in excess of that amount available to fund the project. We look forward to working with you.

Please feel free to call me with any questions you may have in regards to this letter.

Sincerely,

A handwritten signature in black ink, appearing to read "H. Rosenstein".

Howard S. Rosenstein
Chief Financial Officer
Cumberland Farms, Inc.

Phone: 508-270-8339

INDIVIDUAL TANK DATA FOR SITE NUMBER: 9071

Tank	Tank Under/ Above ground	Tank Type	Tank Size	Tank Monitoring	Date Tank Installed	Tank Status	Tank Status Date
1	Underground	F/glass single-walled	8000	Automatic Tank Gauge	01/01/1985	Active	01/01/1985

Chamber	Chamber Size	Product Stored	Pipe Under/ Above ground	Date Installed	Pipe Monitoring	Piping Type	Overfill Protection
1	8000	Premium Unleaded	Underground	11/28/1996	Secondary Containment / Cont Elec Mon	Flexible double-walled	Vent Ball

INDIVIDUAL TANK DATA FOR SITE NUMBER: 9071

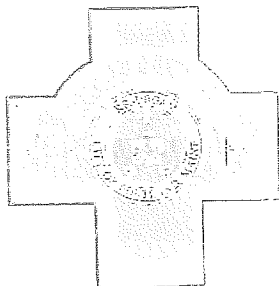
Tank	Tank Under/ Above ground	Tank Type	Tank Size	Tank Monitoring	Date Tank Installed	Tank Status	Tank Status Date
2	Underground	F/glass single-walled	8000	Automatic Tank Gauge	01/01/1985	Active	01/01/1985

Chamber	Chamber Size	Product Stored	Pipe Under/ Above ground	Date Installed	Pipe Monitoring	Piping Type	Overfill Protection
1	8000	Unleaded Gasoline	Underground	11/28/1996	Secondary Containment / Cont Elec Mon	Flexible double-walled	Vent Ball

INDIVIDUAL TANK DATA FOR SITE NUMBER: 9071

Tank	Tank Under/ Above ground	Tank Type	Tank Size	Tank Monitoring	Date Tank Installed	Tank Status	Tank Status Date
3	Underground	F/glass single-walled	8000	Automatic Tank Gauge	01/01/1985	Active	01/01/1985

Chamber	Chamber Size	Product Stored	Pipe Under/ Above ground	Date Installed	Pipe Monitoring	Piping Type	Overfill Protection
1	8000	Unleaded Gasoline	Underground	11/28/1996	Secondary Containment / Cont Elec Mon	Flexible double-walled	Vent Ball



Fire Prevention Permit
from the
City of Portland Fire Department
Fiscal Year 2012
License Number: 3542

To all whom these presents may concern:

This is to certify that the City of Portland Fire Department has granted a permit for

CUMBERLAND FARMS #5604/#1839

Address: 801 WASHINGTON AVE

- Permit For:
- 1) FLAMMABLE/COMBUSTIBLE LIQ
 - 2) RENEWAL APPLICATION FEE
 - 3) SPECIAL TYPE DISPENSING

This permit is granted subject to strict observance of all laws, ordinances, and regulation enacted for the protection of the City so far as they may apply, and is to continue in force until 6/30/2012, unless sooner revoked.

Approved By:

Gregory E. Cass, Fire Prevention Officer

Issued By:

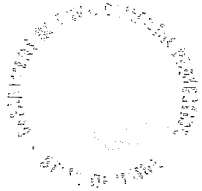
Frederick J. LaMontagne, Fire Chief

This permit is not transferable by owner, corporation, address, or type.

APPLICATION OF CUMBERLAND FARMS, INC.

801 WASHINGTON STREET

COPIES OF STATE AND/OR FEDERAL PERMITS



STATE OF MAINE

Date of Certificate:

03/14/2011

DEPARTMENT OF ENVIRONMENTAL PROTECTION

FACILITY REGISTRATION CERTIFICATE FOR
Underground Storage Tank

Please display this certificate in a visible location at the registered facility.

Facility:

CUMBERLAND FARMS INC 1629
801 WASHINGTON ST
PORTLAND

Facility Registration Number:

9076

Date of Registration:

October 23, 1986

Facility Phone:

207-780-9032

Operator: ECLIPSE DIVISION

CUMBERLAND FARMS INC
588 SILVER ST
AGAWAM
MA 01001

508-270-8300

Sensitive Area Status:

Facility Use:

Retail Oil

Owner: ECLIPSE DIVISION

CUMBERLAND FARMS INC
588 SILVER ST
AGAWAM
MA 01001

508-270-8300

Underground Storage Tank

Number of Active Aboveground Tanks: 0

Number of Active Underground Tanks: 3

If the information on this form is accurate and complete, please retain for your records.

The Maine Department of Environmental Protection must be notified of any errors or changes in the information on this form. To accomplish this, please draw a line through the incorrect or outdated information, insert the correct information, and return this form to:

Department of Environmental Protection Bureau of Remediation and Waste Management
State House Station # 17 Augusta, ME 04333

Attn: Underground Tanks Program

If you have any questions concerning this process, please call (207)287-2651 and ask for the administrator of the Underground Storage Tanks program.

Together with all the right, title and interest of the Grantor in and to all land lying in all streets, highways, rights of way and gores abutting on or appurtenant to said premises.

BEING the same premises conveyed to Gulf Oil Corporation, the Grantor herein, by deed of Phillippe's, Inc. dated November 7, 1966 and recorded with the Cumberland County Registry of Deeds, State of Maine on November 7, 1966 in Book 2979, Page 377.

All of said premises are subject to the Following:

Any state of facts and conditions that an accurate survey and personal inspection of the premises would disclose; easements, conditions, restrictions and reservations of record or which may have been imposed thereon; existing tenancies, if any, zoning ordinances, if any; and taxes and assessments both general and special, if any, which shall fall due and payable following the date of closing.

TO HAVE AND TO HOLD the same, together with all privileges and appurtenances thereunto belonging, unto the said V.S.H. REALTY, INC., its successors and assigns forever.

IN WITNESS WHEREOF, the said GULF OIL CORPORATION has caused this instrument to be sealed with its corporate seal and signed in its corporate name by R. P. Greisler, its Attorney-in-Fact, thereunto duly authorized, this 26th day of September in the year of our Lord one thousand nine hundred and eighty-three.

Signed, Sealed and Delivered in the Presence of:

Maui Salento

GULF OIL CORPORATION

BY: R. P. Greisler
R. P. Greisler
Attorney-in-Fact

ATTEST:

R. D. Hess
R. D. HESS

BY: J. Ronald Sandberg
J. RONALD SANDBERG
Assistant Secretary of
Gulf Oil Corporation

SEAL

STATE OF RHODE ISLAND X
X SS.
COUNTY OF PROVIDENCE X

DATE: September 26, 1983

Then personally appeared the above named R. P. Greisler, an Attorney-in-Fact of Gulf Oil Corporation, and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

George T. Balian
George T. Balian
Notary Public

SEAL

27 1983

OF DEEDS CUMBERLAND COUNTY, MAINE
at 2 H 19M, and recorded in
310 PAGE 145

James J. Walsh

Register My Commission Expires June 30, 1988

QUINCY, ILL. DEPT.

CHRYSLER CORPORATION

V1079

QUITCLAIM DEED

Book 6310
Page 145

39906

KNOW ALL MEN BY THESE PRESENTS:

THAT GULF OIL CORPORATION, a Pennsylvania corporation, having an office at 355 Lyon Avenue, East Providence, Rhode Island 02914, GRANTOR, in consideration of ONE DOLLAR (\$1.00) and other valuable consideration paid by V.S.H. REALTY, INC., having a place of business at 777 Dedham Street, Canton, Massachusetts 02021, GRANTEE, the receipt whereof it does hereby acknowledge, does hereby remise, release, bargain, sell and convey and forever quitclaim unto the said GRANTEE, its successors and assigns forever, three certain lots or parcels of land, together with the buildings and other improvements thereon, situated in the City of Portland, County of Cumberland and State of Maine, said land being more particularly bounded and described as follows:

PARCEL ONE

Beginning at the point of intersection of the northeasterly side line of Washington Avenue with the northwesterly side line of Ocean Avenue;

Thence running northeasterly by said Ocean Avenue fifty (50) feet to a point and to land now or formerly of Phillippe's, Inc.;

Thence running northwesterly and parallel with said northeasterly side line of Washington Avenue one hundred (100) feet to a point and land now or formerly of Joseph Phillippe;

Thence running southwesterly and parallel with said northwesterly side line of Ocean Avenue and by said land now or formerly of Joseph Phillippe, fifty (50) feet to said Washington Avenue;

Thence running southeasterly by said northeasterly side line of Washington Avenue one hundred (100) feet to the point of beginning.

Together with all the right, title and interest of the Grantor in and to all land lying in all streets, highways, rights of way and gores abutting on or appurtenant to said premises.

BEING the same premises conveyed to Gulf Oil Corporation, the Grantor herein, by deed of Minnie A. Green dated March 12, 1948 and recorded with the Cumberland County Registry of Deeds, State of Maine, on March 12, 1948 in Book 1907, Page 154.

PARCEL TWO

A certain lot or parcel of land at Lunt's Corner, in said Portland, on the Northwesterly side of Ocean Avenue, bounded and described as follows:

Beginning at the most easterly corner of land formerly of George Green, which corner is distant fifty (50) feet from the intersection of the northwesterly side line of Ocean Avenue with the northeasterly side line of Washington Avenue;

V1079

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35906

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PARCEL ONE

Beginning at the point of intersection of the northeasterly side line of Washington Avenue with the northwesterly side line of Ocean Avenue;

Thence running northeasterly by said Ocean Avenue fifty (50) feet to a point and to land now or formerly of Phillippe's, Inc.;

Thence running northwesterly and parallel with said northeasterly side line of Washington Avenue one hundred (100) feet to a point and land now or formerly of Joseph Phillippe;

Thence running southwesterly and parallel with said northwesterly side line of Ocean Avenue and by said land now or formerly of Joseph Phillippe, fifty (50) feet to said Washington Avenue;

Thence running southeasterly by said northeasterly side line of Washington Avenue one hundred (100) feet to the point of beginning.

Together with all the right, title and interest of the Grantor in and to all land lying in all streets, highways, rights of way and gores abutting on or appurtenant to said premises.

BEING the same premises conveyed to Gulf Oil Corporation, the Grantor herein, by deed of Minnie A. Green dated March 12, 1948 and recorded with the Cumberland County Registry of Deeds, State of Maine, on March 12, 1948 in Book 1907, Page 154.

PARCEL TWO

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Beginning at the most easterly corner of land formerly of George Green, which corner is distant fifty (50) feet from the intersection of the northwesterly side line of Ocean Avenue with the northeasterly side line of Washington Avenue;

reducing the number of fuel deliveries. One of the compartments will store 8,000 gallons of diesel fuel.

Written Assessment of Zoning:

In 2010, the subject property was rezoned from Neighborhood Business (B-1) and Residential (R-3) to Community Business (B-2). Prior to its rezoning, the use of the property (major/minor auto service establishment) was a grandfathered non-conforming use in the B-1 and R-3 zones. Major/minor auto service establishments are an authorized conditional use in the B-2 Community Business Zone.

Requests for Waivers:

Cumberland is not currently requesting any waivers from the Portland site plan/technical standards.

Summary of Significant Natural Features:

There are no significant natural features on the Property. The proposed additions and site changes will not reduce existing landscaping buffers and will primarily implicate areas that are already developed on the site.

Easements and Burdens:

There are no known easements or other burdens on the Property and the proposed Project will not result in any new easements or burdens.

Consistency with Master Plan:

One of the stated Development Goals in the City's "Community Commercial Policies and Land Use Plan" (Comprehensive Plan, page 28) is to "Promote preservation and revitalization of its existing commercial centers and maintain a scale within them that

is compatible and integrated with other land use." Among the City's Capital Improvement Goals (page 29) is to "Strengthen and upgrade existing neighborhoods by providing physical improvements, which enhance and preserve the neighborhood character and environment." The proposed project is consistent with these goals in that it aims to revitalize and improve the functionality and physical appearance of an established commercial enterprise. While Cumberland has operated a retail store and gasoline station at the Property for many years, and has thus proven itself to be compatible with surrounding uses in the area, the proposed site changes will result in greater compatibility and harmony with both neighboring residential and commercial uses. The increased retail space will also allow for increased economic activity and may allow for increased employment opportunities.

Level II Site Plan Application
Cumberland Farms, Inc.
801 Washington Avenue, Portland, Maine

Project Description:

Cumberland Farms, Inc. ("Cumberland"), is the current owner in fee simple of the above-referenced property ("the Property") used as a retail establishment combined with gasoline sales. Cumberland now proposes to construct a 1,474 s.f. addition to its existing 2,400 s.f. convenience store for a total retail gross floor area of 3,874 s.f. The exterior of the building will be renovated to reflect the new corporate colors and design themes, as shown on the business elevations provided with this application. The proposed store renovations have prompted additional site improvements, including the following:

- Renovation of existing dumpster on new concrete pad
- New concrete walk at the front and side of the store, including the proposed addition
- Installation of protective bollards (with sleeves) at parking spaces along the front and side of the store
- Relocation and expansion of parking spaces
- Addition of bicycle racks
- Landscaping to be enhanced (see Landscaping Plan)
- Addition of area lamps for enhanced safety of parking and dumpster areas
- Replacement of four existing 4' x 10' gas islands, concrete pad, and four fuel dispensers with new 4' x 10' islands, concrete pads, and four fuel dispensers in the

same location as existing (“like for like” exchange). One dispenser will offer diesel fuel.

- Replacement of existing 24 x 96’ gas canopy with new white ACM panel fascia with green stripe (same size and location). Note: *The existing canopy requires replacement due to safety issues.*
- Two existing Gulf Self Serve signs on the gas canopy, each of 16 s.f. to be removed and replaced with two Cumberland Farms ID signs each 12.38 s.f. with push-thru illuminated acrylic letters, resulting in a 7.24 s.f. reduction in canopy signage
- Reface existing store fascia with white ACM panel with green metal wall panel band incorporating the new addition (See Building Elevations).
- Two new wall signs on the store each of 15 s.f. The white background is non-illuminated with push-thru illuminated acrylic letters
- Re-image existing freestanding sign of 69 s.f., removing the Gulf disc and installing a new ID sign of 15 s.f. and a new LED price sign (regular and diesel) of 33 s.f. for a total of 48 s.f., an overall reduction of 21 s.f. of signage
- Fuel dispenser decals, each 1 s.f., to incorporate Cumberland’s new logo and colors
- Replace 3-8,000 gal. single-wall fiberglass underground storage tanks with 2-20,000 gal. double-wall fiberglass underground dual compartment storage tanks providing for diesel fuel sales, upgrading the underground tanks and piping and

PROJECT DATA

The following information is required where applicable, in order complete the application

Total Site Area	31,972 sq. ft.
Proposed Total Disturbed Area of the Site	sq. ft.
(If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)	
IMPERVIOUS SURFACE AREA	
•Proposed Total Paved Area	25,364 sq. ft.
•Existing Total Impervious Area	22,056 sq. ft.
•Proposed Total Impervious Area	25,364 sq. ft.
•Proposed Impervious Net Change	3308 sq. ft.
BUILDING AREA	
•Proposed Building Footprint	3,874 sq. ft.
•Proposed Building Footprint Net change	1474 sq. ft.
•Existing Total Building Floor Area	2,400 sq. ft.
•Proposed Total Building Floor Area	3,874 sq. ft.
•Proposed Building Floor Area Net Change	1474 sq. ft.
•New Building	(addition) (yes or no)
ZONING	
•Existing	B-2
•Proposed, if applicable	n/a
LAND USE	
•Existing	Retail store with gasoline sales
•Proposed	same
RESIDENTIAL, IF APPLICABLE	
•Proposed Number of Affordable Housing Units	n/a
•Proposed Number of Residential Units to be Demolished	
•Existing Number of Residential Units	
•Proposed Number of Residential Units	
•Subdivision, Proposed Number of Lots	
PARKING SPACES	
•Existing Number of Parking Spaces	22 (including 8 at pumps)
•Proposed Number of Parking Spaces	32 (including 8 at pumps)
•Number of Handicapped Parking Spaces	2
•Proposed Total Parking Spaces	33
BICYCLE PARKING SPACES	
•Existing Number of Bicycle Parking Spaces	0
•Existing Number of Bicycle Parking Spaces	0
•Proposed Number of Bicycle Parking Spaces	10
•Total Bicycle Parking Spaces	10
ESTIMATED COST OF PROJECT	\$979,250

APPLICATION SUBMISSION

As of December 1, 2010, all site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Until December 1, 2010, submissions shall include seven (7) packets with folded plans containing the following materials:

1. Seven (7) full size site plans that must be folded.
2. Seven (7) copies of all written materials or as follows, unless otherwise noted:
 - a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
5. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 100 feet.
6. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
7. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
8. One (1) set of plans reduced to 11 x 17.

Refer to the application checklist for a detailed list of submittal requirements.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level II Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: Cumberland Farms, Inc. By: <i>Kathleen A. Sousa</i>	Date:
---	-------

Kathleen A. Sousa, Land Planning Mgr.

PROJECT NAME: Cumberland Farms

PROPOSED DEVELOPMENT ADDRESS: 801 Washington Avenue

PROJECT DESCRIPTION: See attached

CHART/BLOCK/LOT: Map 163, Block B
Lots 3, 4, 5, 14, 15, 17

PRELIMINARY PLAN _____ (date)
 FINAL PLAN _____ (date)

CONTACT INFORMATION:

<p>Applicant – must be owner, Lessee or Buyer</p> <p>Name: Cumberland Farms, Inc.</p> <p>Business Name, if applicable:</p> <p>Address: 100 Crossing Boulevard</p> <p>City/State : Framingham, MA Zip Code: 01702</p>	<p>Applicant Contact Information</p> <p>Work # 508-270-1466</p> <p>Home#</p> <p>Cell # Fax# 781-459-0569</p> <p>e-mail: ksousa@cumberlandgulf.com</p>
<p>Owner – (if different from Applicant)</p> <p>Name: n/a</p> <p>Address:</p> <p>City/State : Zip Code:</p>	<p>Owner Contact Information</p> <p>Work #</p> <p>Home#</p> <p>Cell # Fax# e-mail: .</p>
<p>Agent/ Representative <i>Jisel Lopez</i></p> <p>Name James Haddow, Esq, Petrucelli Martin & Haddow LLP</p> <p>Address: 50 Monument Square</p> <p>City/State : Portland, ME Zip Code: 04112-8555</p>	<p>Agent/Representative Contact information</p> <p>Work # 207-775-0200</p> <p>Cell # Fax# 207-775-2360</p> <p>e-mail: jhaddow&pmhlegal.com <i>jlopez@pmhlegal.com</i></p>
<p>Billing Information</p> <p>Name: Cumberland Farms, Inc.</p> <p>Address: 100 Crossing Boulevard</p> <p>City/State : Framingham, MA Zip Code: 01702</p>	<p>Billing Information</p> <p>Work # 508-270-1466</p> <p>Cell # Fax# 781-459-0569</p> <p>e-mail: ksousa@cumberlandgulf.com</p>

Engineer Name: MHF Design Consultants Inc. Address: 44 Stiles Rd., Suite 1 City/State : Salem, NH Zip Code: 03079	Engineer Contact Information Work # 603-893-0720 Cell # Fax# 603-893-0733 e-mail:
Surveyor Name: Maine Survey Consultants Address: P.O. Box 485 City/State : Portland, ME Zip Code:	Surveyor Contact Information Work # 207-583-6159 Cell # Fax# 207-583-2334 e-mail:
Architect Name: Ahronian & Assoc. Address: 310 George Washington Hwy City/State : Smithfield, RI Zip Code: 02828	Architect Contact Information Work # 401-232-5010 Cell # Fax# 401-232-5080 e-mail:
Attorney Name: James Haddow, Esq. Address: 50 Monument Sq. City/State : Portland, ME Zip Code: 04112	Attorney Contact Information Work # 207-775-0200 Cell # Fax# 207-775-2360 e-mail: jhaddow@pmhlegal.com

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

Level II Development (check applicable reviews) <input checked="" type="checkbox"/> Less than 10,000 sq. ft. (\$400.00) <input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)	Fees Paid (office use) _____ _____	Other Reviews (check applicable reviews) <input type="checkbox"/> Traffic Movement (\$1,000) <input type="checkbox"/> Stormwater Quality (\$250) <input type="checkbox"/> Section 14-403 Review (\$400 + \$25/lot) <input type="checkbox"/> # of Lots ____ x \$25/lot = _____ <input type="checkbox"/> Other _____ <input type="checkbox"/> Change of Use <input type="checkbox"/> Flood Plain <input type="checkbox"/> Shoreland <input type="checkbox"/> Design Review <input type="checkbox"/> Housing Replacement <input type="checkbox"/> Historic Preservation	Fees Paid (office use) _____ _____ _____
<hr/> The City invoices separately for the following: <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) Third party review is assessed separately.			
Plan Amendments (check applicable reviews) <input type="checkbox"/> Planning Staff Review (\$250) <input type="checkbox"/> Planning Board Review (\$500)	Fees Paid (office use) _____ _____		

- Addition of area lamps for enhanced safety of parking and dumpster areas
- Replacement of four existing 4' x 10' gas islands, concrete pad, and four fuel dispensers with new 4' x 10' islands, concrete pads, and four fuel dispensers in the same location as existing ("like for like" exchange). One dispenser will offer diesel fuel.
- Replacement of existing 24 x 96' gas canopy with new white ACM panel fascia with green stripe (same size and location). Note: *The existing canopy requires replacement due to safety issues.*
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- Replace 3-3,000 gal. single-wall fiberglass underground storage tanks with 2-20,000 gal. double-wall fiberglass underground dual compartment storage tanks providing for diesel fuel sales, upgrading the underground tanks and piping and reducing the number of fuel deliveries. One of the compartments will store 8,000 gallons of diesel fuel.

Trip Distribution

Having estimated project-generated vehicle trips, the next step was to determine the distribution of project traffic and assign these trips to the local roadway network. Since the project consists of expanding the existing facility with the same uses, the distribution of proposed additional site traffic on the area roadways was based on existing travel patterns at the site driveways. Accordingly, approximately 30 percent of the additional site traffic is expected to and from the north on Washington Avenue, 30 percent is expected to and from the south on Washington Avenue, 30 percent is expected to and from the east on Ocean Avenue, and 10 percent is expected to and from the west on Ocean Avenue. The distribution of *pass-by* traffic is expected to follow the directional distribution of adjacent street traffic observed during the various peak hours on Washington Avenue and on Ocean Avenue.

Build Conditions

Based on the traffic-generation and distribution estimates for this project, the traffic volumes associated with the proposed Cumberland Farms expansion were assigned to the roadway network as shown on Figures 8 through 10. The site-generated traffic volumes were then added to the No-Build traffic volumes to develop the Build traffic volumes. The 2012 Build weekday AM, weekday PM, and Saturday midday peak-hour traffic-flow networks are graphically depicted on Figures 11 through 13, respectively.

Traffic Increases

The proposed expansion project will result in increases in traffic on the roadways leading beyond the study area. As shown on Figures 8 through 10, peak-hour traffic-volume increases beyond the study area are expected to be in the range of 3 to 9 vehicles per hour. These increases represent approximately 1 additional vehicle every 6.5 to 20 minutes during the peak hours.

CAPACITY AND QUEUE ANALYSIS

Methodology

Level-of-service analyses were conducted at the study-area intersections under 2009 Existing, 2012 No-Build, and 2012 Build conditions during the weekday AM, weekday PM, and Saturday

Table 5
TRIP-GENERATION SUMMARY – Additional Trips

Time Period/Direction	Total Additional Trips ^a	Pass-By Trips ^b	New Trips ^c
Weekday AM Peak Hour:			
<i>In</i>	33	20	13
<i>Out</i>	<u>32</u>	<u>20</u>	<u>12</u>
<i>Total</i>	65	40	25
Weekday PM Peak Hour:			
<i>In</i>	44	29	15
<i>Out</i>	<u>44</u>	<u>29</u>	<u>15</u>
<i>Total</i>	88	58	30
Saturday Midday Peak Hour:			
<i>In</i>	35	21	14
<i>Out</i>	<u>33</u>	<u>21</u>	<u>12</u>
<i>Total</i>	68	42	26

^a ITE Land Use Code 853 (Convenience Market with Gasoline Pumps) for ±1,480 sf – rounded up for conservative (worse-case) scenario.

^b 63% of Total Additional Trips during the weekday AM and Saturday midday peak hours, and 66% during the weekday PM peak hour.

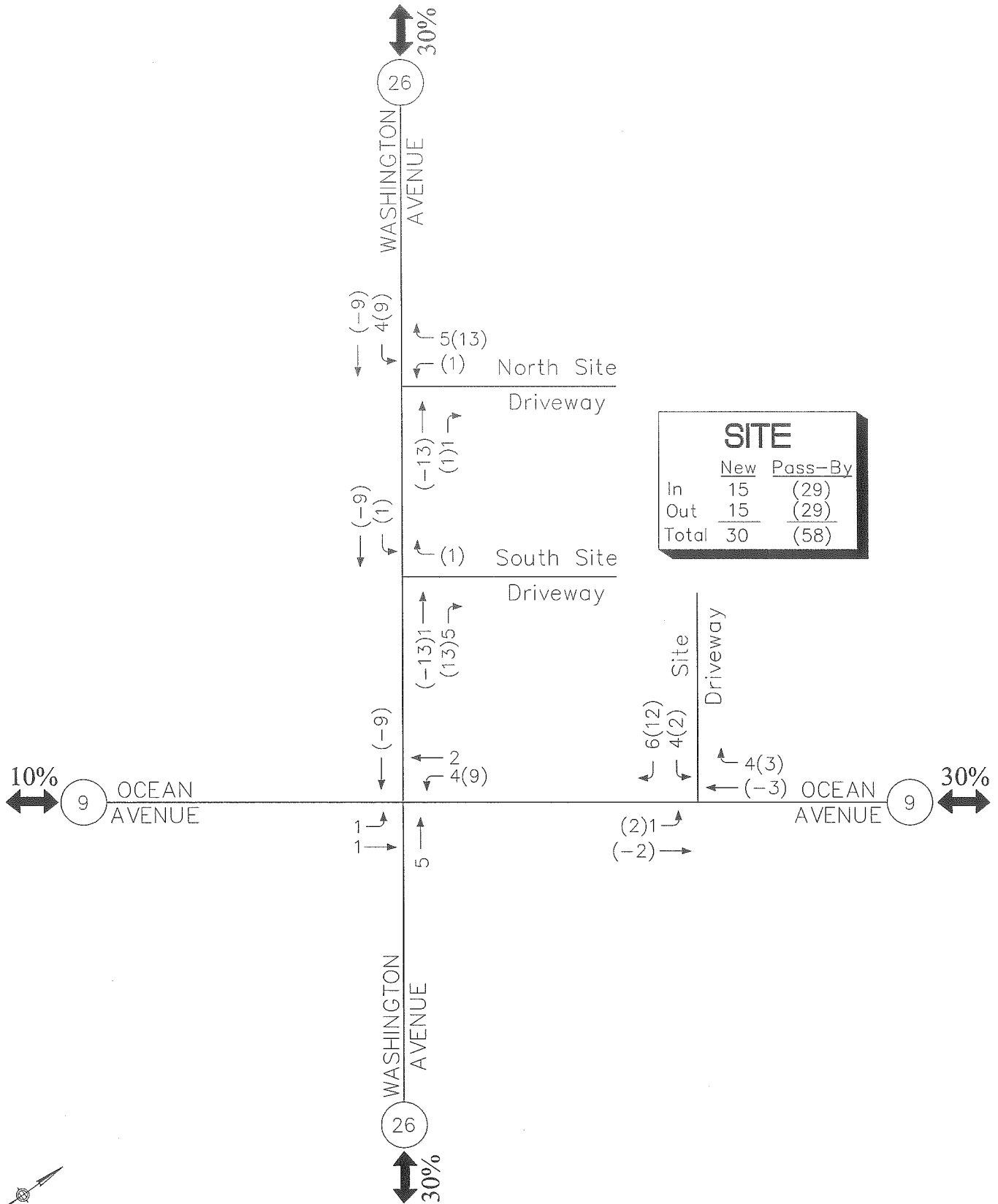
^c Total Additional Trips minus Pass-By Trips.

As shown, the proposed Cumberland Farms expansion is expected to generate 25 additional *new* vehicle trips (13 entering and 12 exiting) during the weekday AM peak hour, 30 additional *new* vehicle trips (15 entering and 15 exiting) during the weekday PM peak hour, and 226 additional *new* vehicle trips (14 entering and 12 exiting) during the Saturday midday peak hour.

To determine the number of PCE trips for the expected trip-generation change associated with the expansion project, data were provided by Cumberland Gulf Group of Companies. Currently, there are two truck deliveries that occur at the facility and they take place on Mondays and Thursdays between 1 and 2 PM. At this time, there are not expected to be additional truck deliveries associated with the proposed expansion project. If additional deliveries are required, the trucks would be scheduled to arrive outside of the peak periods (i.e., not between 7-9 AM or 4-6 PM on a weekday and not between 11 AM -2 PM on a Saturday). Therefore, the projected trips reflected in Table 5 also reflect the total additional PCE trips that can be expected as part of the proposed Cumberland Farms expansion project. Since the proposed expansion project is expected to generate less than 100 additional PCE trips during the peak hours of traffic generation and there will not be a change in use from the existing site, the project is not expected to require a *Traffic Movement Permit* from MaineDOT.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine

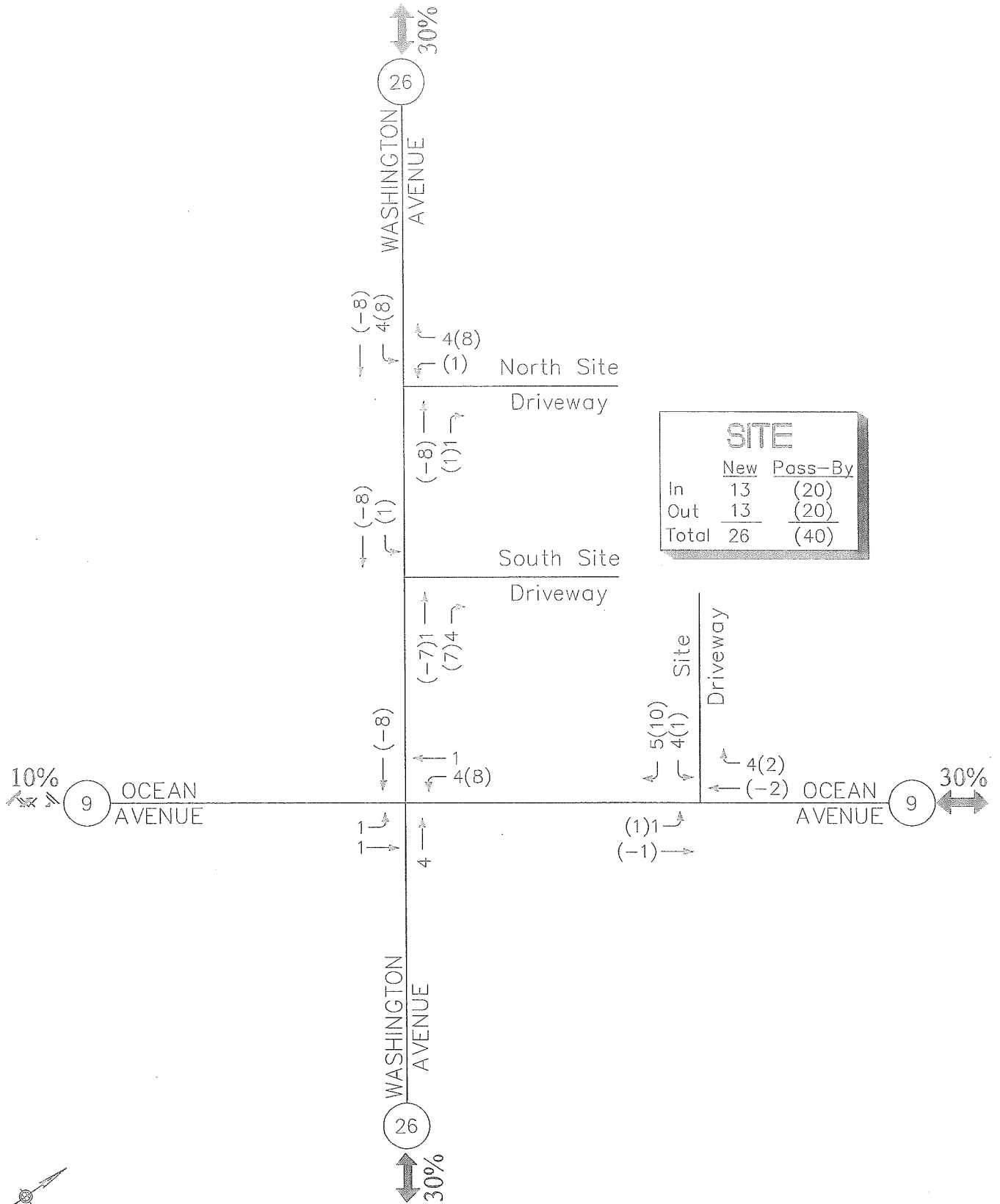


NOT TO SCALE

Figure 9
Additional Site Generated Trips
Weekday PM
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



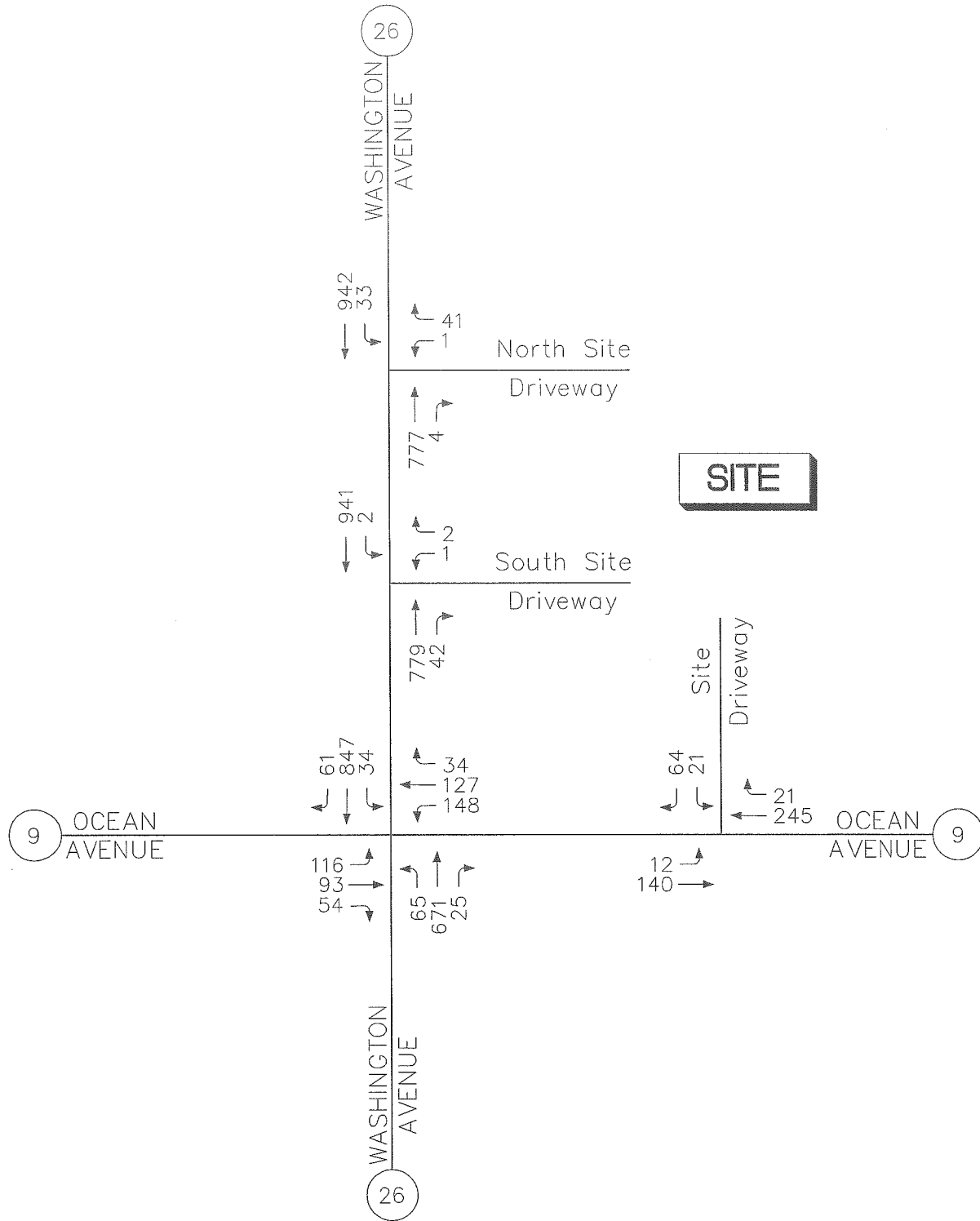
NOT TO SCALE

Figure 8

Additional Site Generated Trips
Weekday A.M.
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



NOT TO SCALE

Figure 11

2012 Build
Weekday AM
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine

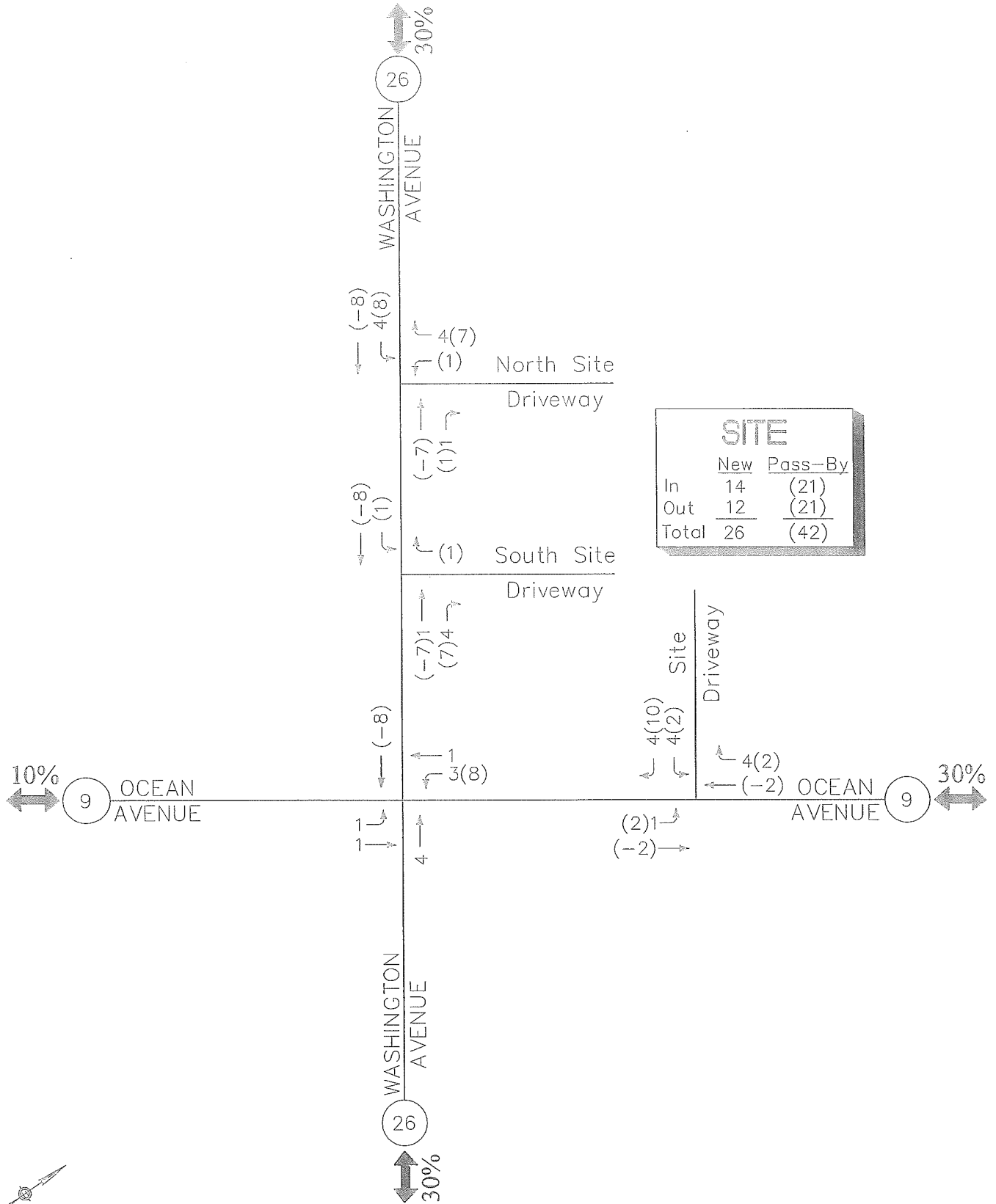
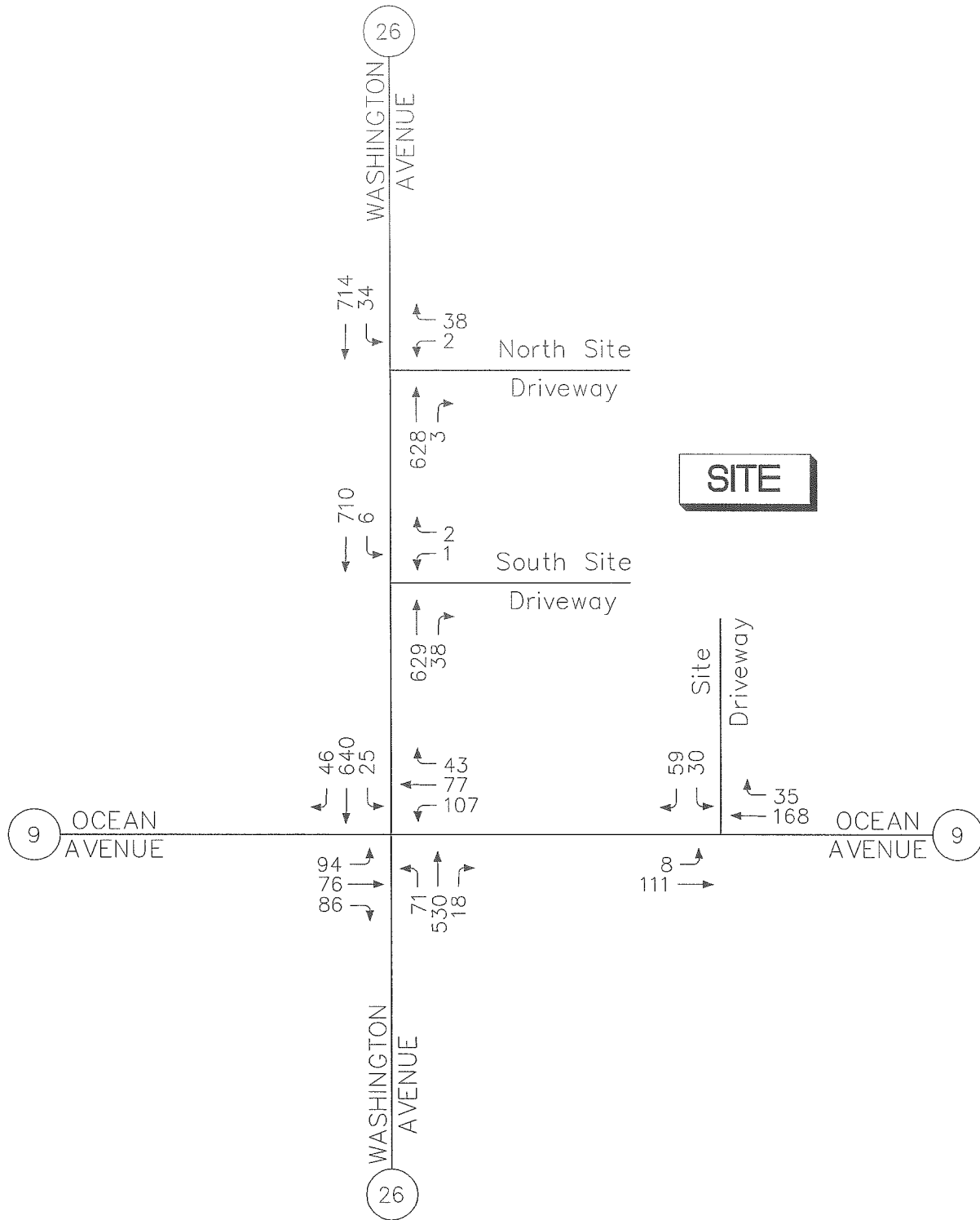


Figure 10

Additional Site Generated Trips
Saturday Midday
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



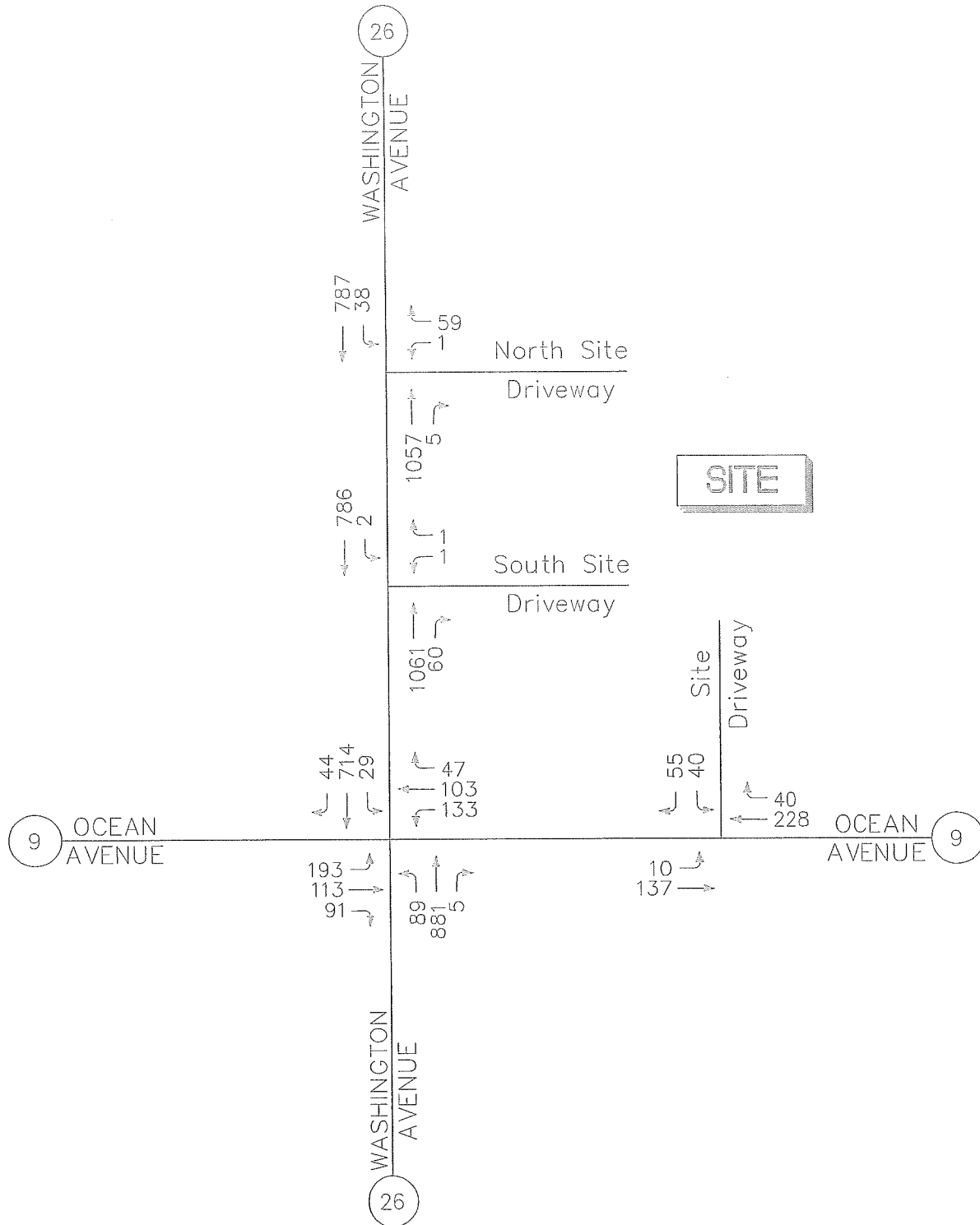
NOT TO SCALE

Figure 13

2012 Build
Saturday Midday
Peak Hour Traffic Volumes

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion - Portland, Maine



NOT TO SCALE

Figure 12

2012 Build
Weekday PM
Peak Hour Traffic Volumes

below 1.00 and queues are not anticipated to exceed 2 vehicles during the peak hours, indicating that ample capacity will remain after the proposed expansion project is constructed and occupied.

Ocean Avenue and Site Driveway

The Ocean Avenue approaches to the intersections with the Cumberland Farms site driveway are expected to operate at optimal levels (LOS A) under existing and future traffic-volume conditions during the weekday AM, weekday PM, and Saturday midday peak hours. The Cumberland Farms site driveway is expected to operate at desirable levels (LOS B) under existing and future traffic-volume conditions during the peak hours. Volume-to-capacity ratios on the Cumberland Farms driveway are expected to be well below 1.00 and queues are not anticipated to exceed 1 vehicle during the peak hours, indicating that ample capacity will remain after the proposed expansion project is constructed and occupied.

midday peak hours. The capacity and queue analysis methodology is based on the concepts and procedures in the *Highway Capacity Manual* (HCM) and is described in the Appendix.⁷

The queue analysis methodology for signalized and unsignalized intersections is based on the concepts and procedures described in the HCM. The maximum back of queue during a 95th percentile signal cycle were calculated for each critical lane group during the peak periods studied. The back of queue is the length of a backup of vehicles from the STOP-line of a signalized intersection to the last vehicle in the queue that is required to stop, regardless of the signal indication. The length of this queue depends on a number of factors including signal timing, vehicle arrival patterns, and the saturation flow rate.

For unsignalized intersections, the 95th percentile queue represents the length of queue of the critical minor-street movement that is not expected to be exceeded 95 percent of the time during the analysis period (typically one hour). In this case, the queue length is a function of the capacity of the movement and the movement's degree of saturation.

Analysis Results

The results of the level-of-service and queue analyses are shown in Table 6 for the study area intersections. All analysis worksheets are provided in the Appendix.

Washington Avenue and Ocean Avenue

Under existing and future traffic-volume conditions, the Washington Avenue and Ocean Avenue signalized intersection is expected to operate at overall acceptable levels (LOS D or better) during the weekday AM, weekday PM, and Saturday midday peak hours. The proposed expansion project is expected to have minimal impact on the signalized intersection with an increase in overall delay of 1.2 seconds or less during the peak hours and increases in queue lengths of less than 1 vehicle on any lane group. Traffic volume increases at this location are expected to be approximately 1 additional vehicle every 4.5 to 6 minutes during the peak hours.

Washington Avenue at North and South Site Driveways

The Washington Avenue approaches to the intersections with the Cumberland Farms north and south site driveways are expected to operate at optimal levels (LOS A) under existing and future traffic-volume conditions during the weekday AM, weekday PM, and Saturday midday peak hours. All delays are expected to occur on the site and not affect traffic flow along Washington Avenue. Volume-to-capacity ratios on the Cumberland Farms driveways are expected to be well

⁷*Highway Capacity Manual 2000*; Transportation Research Board; Washington, D.C.; 2000.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

**Table 6 (continued)
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY**

Intersection/Peak Hour/Lane Group	2009 Existing				2012 No-Build				2012 Build			
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
Washington Avenue and Ocean Avenue												
<i>Saturday Midday:</i>												
Ocean Ave. EB left-turns	0.47	26.8	C	51/100	0.50	27.5	C	57/107	0.50	27.5	C	51/100
Ocean Ave. EB through/right-turns	0.67	39.3	D	70/129	0.73	44.4	D	81/144	0.74	45.8	D	83/143
Ocean Ave. WB left-turns	0.51	27.5	C	43/85	0.53	28.5	C	34/81	0.59	30.7	C	44/85
Ocean Ave. WB through/right-turns	0.61	35.4	D	67/96	0.65	38.5	D	70/102	0.66	39.3	D	64/98
Washington Ave. NB left-turns	0.30	10.4	B	38/72	0.34	11.1	B	37/70	0.33	10.9	B	39/68
Washington Ave. NB through/right-turns	0.62	13.4	B	132/219	0.64	13.9	B	133/227	0.65	14.0	B	138/223
Washington Ave. SB left-turns	0.08	8.7	A	16/46	0.09	8.7	A	19/50	0.09	8.8	A	16/46
Washington Ave. SB through/right-turns	0.81	21.1	C	37/58	0.84	22.6	C	36/57	0.83	22.2	C	38/60
Overall Intersection	0.73	21.4	C	--	0.76	22.9	C	--	0.76	23.1	C	--

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic, unsignalized intersection queues analyzed using Synchro 7.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

Table 6
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY

Intersection/Peak Hour/Lane Group	2009 Existing				2012 No-Build				2012 Build			
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
<i>Washington Avenue and Ocean Avenue</i>												
<i>Weekday AM:</i>												
Ocean Ave. EB left-turns	0.56	37.2	D	76/137	0.58	38.0	D	64/138	0.61	38.5	D	73/140
Ocean Ave. EB through/right-turns	0.63	43.4	D	87/158	0.65	44.9	D	102/172	0.72	50.2	D	95/167
Ocean Ave. WB left-turns	0.61	33.9	C	27/78	0.65	35.5	D	42/94	0.74	44.0	D	48/95
Ocean Ave. WB through/right-turns	0.61	39.4	D	78/99	0.63	40.1	D	81/107	0.73	48.8	D	84/110
Washington Ave. NB left-turns	0.44	21.3	C	39/74	0.47	22.0	C	34/85	0.47	21.8	C	45/84
Washington Ave. NB through/right-turns	0.76	20.7	C	178/243	0.80	22.3	C	186/237	0.79	21.4	C	183/245
Washington Ave. SB left-turns	0.15	11.9	B	18/48	0.17	12.6	B	19/46	0.16	12.1	B	18/47
Washington Ave. SB through/right-turns	0.99	46.9	D	48/74	1.03	59.7	E	44/77	1.01	53.1	D	49/79
Overall Intersection	0.88	35.6	D	---	0.93	41.6	D	---	0.87	40.1	D	---
<i>Weekday PM:</i>												
Ocean Ave. EB left-turns	0.96	84.3	F	128/183	1.03	105.6	F	136/182	1.04	108.8	F	135/181
Ocean Ave. EB through/right-turns	0.96	90.0	F	138/205	1.04	117.0	F	136/208	1.04	117.0	F	135/207
Ocean Ave. WB left-turns	0.67	41.8	D	61/77	0.71	46.1	D	60/80	0.81	57.4	E	62/80
Ocean Ave. WB through/right-turns	0.83	66.3	E	84/110	0.90	82.9	F	87/115	0.92	88.1	F	91/123
Washington Ave. NB left-turns	0.39	13.0	B	46/86	0.44	14.1	B	46/84	0.42	13.6	B	48/85
Washington Ave. NB through/right-turns	0.91	29.7	C	194/233	0.94	32.9	C	195/231	0.94	33.2	C	197/233
Washington Ave. SB left-turns	0.20	15.1	B	17/41	0.22	16.3	B	17/41	0.23	16.5	B	19/42
Washington Ave. SB through/right-turns	0.79	20.3	C	35/53	0.81	21.2	C	36/57	0.80	20.4	C	33/51
Overall Intersection	0.79	38.2	D	---	0.83	44.8	D	---	0.83	46.0	D	---

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic, unsignalized intersection queues analyzed using Synchro 7.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

**Table 6 (continued)
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY**

Intersection/Peak Hour/Lane Group	2009 Existing				2012 No-Build				2012 Build			
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
Washington Avenue and South Site Driveway												
<i>Weekday AM:</i>												
Site Driveway WB left-turns/right-turns	0.02	23.1	C	--/1	0.02	25.8	D	--/1	0.02	25.7	D	--/1
Washington Ave. NB through/right-turns	0.50	0.0	A	--/0	0.52	0.0	A	--/0	0.52	0.0	A	--/0
Washington Ave. SB left-turns/through	0.00	0.1	A	--/0	0.00	0.1	A	--/0	0.00	0.1	A	--/0
Washington Ave. SB through	0.39	0.0	A	--/0	0.40	0.0	A	--/0	0.40	0.0	A	--/0
<i>Weekday PM:</i>												
Site Driveway WB left-turns/right-turns	0.03	98.2	F	--/2	0.04	143.5	F	--/3	0.05	87.3	F	--/4
Washington Ave. NB through/right-turns	0.68	0.0	A	--/0	0.71	0.0	A	--/0	0.72	0.0	A	--/0
Washington Ave. SB left-turns/through	0.00	0.1	A	--/0	0.00	0.1	A	--/0	0.01	0.2	A	--/1
Washington Ave. SB through	0.32	0.0	A	--/0	0.34	0.0	A	--/0	0.34	0.0	A	--/0
<i>Saturday Midday:</i>												
Site Driveway WB left-turns/right-turns	0.01	17.7	C	--/1	0.01	18.8	C	--/1	0.01	16.9	C	--/1
Washington Ave. NB through/right-turns	0.41	0.0	A	--/0	0.42	0.0	A	--/0	0.43	0.0	A	--/0
Washington Ave. SB left-turns/through	0.01	0.3	A	--/0	0.01	0.3	A	--/0	0.01	0.3	A	--/1
Washington Ave. SB through	0.29	0.0	A	--/0	0.31	0.0	A	--/0	0.30	0.0	A	--/0

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic, unsignalized intersection queues analyzed using Synchro 7.

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

Table 6 (continued)
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY

Intersection/Peak Hour/Lane Group	2009 Existing				2012 No-Build				2012 Build			
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
<i>Washington Avenue and North Site Driveway</i>												
<i>Weekday AM:</i>												
Site Driveway WB left-turns/right-turns	0.08	15.4	C	--/7	0.09	16.1	C	--/7	0.14	17.6	C	--/12
Washington Ave. NB through/right-turns	0.48	0.0	A	--/0	0.50	0.0	A	--/0	0.50	0.0	A	--/0
Washington Ave. SB left-turns/through	0.03	1.1	A	--/3	0.03	1.1	A	--/3	0.05	1.7	A	--/4
Washington Ave. SB through	0.39	0.0	A	--/0	0.40	0.0	A	--/0	0.40	0.0	A	--/0
<i>Weekday PM:</i>												
Site Driveway WB left-turns/right-turns	0.23	29.0	D	--/21	0.27	34.6	D	--/26	0.42	44.1	E	--/47
Washington Ave. NB through/right-turns	0.66	0.0	A	--/0	0.69	0.0	A	--/0	0.68	0.0	A	--/0
Washington Ave. SB left-turns/through	0.06	2.3	A	--/5	0.07	2.6	A	--/6	0.11	3.8	A	--/9
Washington Ave. SB through	0.32	0.0	A	--/0	0.34	0.0	A	--/0	0.34	0.0	A	--/0
<i>Saturday Midday:</i>												
Site Driveway WB left-turns/right-turns	0.07	13.6	B	--/5	0.07	14.1	B	--/6	0.10	14.5	B	--/9
Washington Ave. NB through/right-turns	0.39	0.0	A	--/0	0.41	0.0	A	--/0	0.40	0.0	A	--/0
Washington Ave. SB left-turns/through	0.03	1.1	A	--/2	0.03	1.1	A	--/2	0.04	1.6	A	--/3
Washington Ave. SB through	0.29	0.0	A	--/0	0.31	0.0	A	--/0	0.30	0.0	A	--/0

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic, unsignalized intersection queues analyzed using Synchro 7.

CONCLUSIONS

Existing and future conditions in the study area have been described, analyzed, and evaluated with respect to traffic operations and the impact of the proposed Cumberland Farms expansion project. Conclusions of this effort are presented below.

- The Cumberland Farms facility currently consists of a ±2,400 square foot convenience store with 8 vfps. The proposed project consists of maintaining the existing fueling pumps and expanding the existing convenience store by 1,474 square feet for a total of 3,874 square feet.
- Access to the site is currently provided via two driveways on the east side of Washington Avenue and one driveway on the north side of Ocean Avenue. As proposed, the three driveways will be maintained.
- Available sight distances at the site driveways exceed the minimum SSD and ISD requirements for safe operation based on the posted and observed speeds. To ensure the safe and efficient flow of traffic to and from the site, it is recommended that any proposed plantings, vegetation, landscaping, and signing along the site frontage be kept low to the ground (no more than 3.0 feet above street level) or set back sufficiently from the edge of Washington Avenue, Ocean Avenue, and the site driveways so as not to inhibit the available sight lines.
- There are two truck deliveries that occur at the facility and they take place on weekdays outside of the commuter peak periods. At this time, there are not expected to be additional truck deliveries associated with the proposed expansion project. Therefore, the total additional PCE trips that can be expected as part of the proposed Cumberland Farms expansion project are 65 total additional PCE trips during the weekday AM peak hour, 88 total additional PCE trips during the weekday PM peak hour, and 68 total additional PCE trips during the Saturday midday peak hour. Since the proposed expansion project is expected to generate less than 100 total additional PCE trips during the peak hours of traffic generation and there will not be a change in use from the existing site, the project is not expected to require a *Traffic Movement Permit* from MaineDOT.
- Increases in traffic volumes on roadways leading beyond the study area as a result of the project are expected in the range of 3 to 9 vehicles per hour during the weekday AM, weekday PM, and Saturday midday peak hours. These increases represent approximately 1 additional vehicle every 6.5 to 20 minutes during the peak hours.
- The proposed expansion project is expected to have a minimal impact at the Washington Avenue and Ocean Avenue signalized intersection with an increase in overall intersection delay of 1.2 seconds or less during the peak hours (LOS D or better) and increases in queue

TRAFFIC IMPACT AND ACCESS STUDY

Proposed Cumberland Farms Expansion – Portland, Maine

Table 6 (continued)
LEVEL-OF-SERVICE AND QUEUE ANALYSIS SUMMARY

Intersection/Peak Hour/Lane Group	2009 Existing				2012 No-Build				2012 Build			
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
<i>Ocean Avenue and Site Driveway</i>												
<i>Weekday AM:</i>												
Ocean Ave. EB left-turns/through	0.01	0.6	A	--/1	0.01	0.6	A	--/1	0.01	0.7	A	--/1
Ocean Ave. WB through/right-turns	0.16	0.0	A	--/0	0.17	0.0	A	--/0	0.17	0.0	A	--/0
Site Driveway SB left-turns/right-turns	0.10	10.6	B	--/8	0.10	10.7	B	--/8	0.13	10.9	B	--/11
<i>Weekday PM:</i>												
Ocean Ave. EB left-turns/through	0.01	0.4	A	--/0	0.01	0.4	A	--/0	0.01	0.6	A	--/1
Ocean Ave. WB through/right-turns	0.16	0.0	A	--/0	0.17	0.0	A	--/0	0.17	0.0	A	--/0
Site Driveway SB left-turns/right-turns	0.11	10.9	B	--/9	0.11	11.0	B	--/10	0.15	11.2	B	--/13
<i>Saturday Midday:</i>												
Ocean Ave. EB left-turns/through	0.00	0.4	A	--/0	0.00	0.4	A	--/0	0.01	0.6	A	--/0
Ocean Ave. WB through/right-turns	0.12	0.0	A	--/0	0.13	0.0	A	--/0	0.13	0.0	A	--/0
Site Driveway SB left-turns/right-turns	0.10	10.2	B	--/8	0.10	10.2	B	--/8	0.13	10.4	B	--/11

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Maximum queue length in feet per lane during an average/95th percentile cycle (assuming 25 feet per vehicle). Signalized intersection queues analyzed using SimTraffic, unsignalized intersection queues analyzed using Synchro 7.

PETRUCELLI, MARTIN & HADDOW, LLP

Attorneys at Law

Two Monument Square, Suite 900
Post Office Box 17555
Portland, Maine 04112-8555

JISEL E. LOPEZ
jlopez@pmhlegal.com

April 2, 2012

Via Hand Delivery

Ms. Shukria Wiar, Planner
Planning & Urban Development Dept.
389 Congress Street, 4th Floor
Portland, ME 04101-3509

Re: Cumberland Farms, Inc.—801 Washington Avenue

Dear Shukria:

As you know, Cumberland Farms filed its application for Level II Site Plan and conditional use review in connection with the proposed expansion of its store at 801 Washington Avenue on January 23, 2012. In response to the comments and concerns of the planning staff and the Planning Board after the recent workshop, Cumberland Farms has made the following revisions to its application:

- Parking space dimensions have been revised to meet City standards (9' x 18'). Two spaces will remain unchanged from the initial proposal (9' x 19') due to the need to accommodate the Red Box kiosk and sidewalk extension; a formal waiver request has been submitted with respect to these two spaces (see below).
- A 6' white vinyl fence has been added along the rear of the property and along the side of the property abutting residential uses.
- Plans have been revised to include the requested information regarding the concrete walkway, bituminous pavement section and stabilized construction exit.
- Planning staff requested additional information regarding the drainage off the property and the southerly property coordinates and the plans have been revised to address these areas.
- Snow storage is shown on the site plans.
- Lighting has been revised to meet City standards and address the spillover on adjacent lots. Catalogue cut sheets are enclosed.

length of less than 1 vehicle on any lane group. Traffic volume increases at this location are expected to be approximately 1 additional vehicle every 4.5 to 6 minutes during the peak hours.

- The Washington Avenue approaches to the intersections with the Cumberland Farms north and south site driveways are expected to operate at optimal levels (LOS A) under existing and future traffic-volume conditions. All delays are expected to occur on the site and not affect traffic flow along Washington Avenue. Volume-to-capacity ratios on the Cumberland Farms driveways are expected to be well below 1.00 and queues are not anticipated to exceed 2 vehicles, indicating that ample capacity will remain after the proposed expansion project is constructed and occupied.

- The Ocean Avenue approaches to the intersections with the Cumberland Farms site driveway are expected to operate at optimal levels (LOS A) under existing and future traffic-volume conditions. The Cumberland Farms site driveway is expected to operate at desirable levels (LOS B) under existing and future traffic-volume conditions. Volume-to-capacity ratios on the Cumberland Farms driveway are expected to be well below 1.00 and queues are not anticipated to exceed 1 vehicle, indicating that ample capacity will remain after the proposed expansion project is constructed and occupied.

Ms. Shukria Wiar, Planner
April 2, 2012
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- Waiver with supporting documentation regarding the driveway on Washington Avenue which does not meet Section 14-183 (b) (ingress-egress).
- Waiver with supporting documentation for the two non-conforming parking spaces.
- Narrative regarding the project's compliance with the B-2 design guidelines.
- Letter from Jason Plourde, P.E., of Greenman Pedersen, Inc., regarding the traffic comments. A waiver request with respect to the non-conforming site driveways has been attached.
- A stormwater management plan was requested as the parcel was found to be within the Urban Impaired Stream watershed by the City's consultant. Cumberland Farms has researched this issue and is of the opinion that such a plan is not required under Chapter 500 of the Maine DEP stormwater management rules. A narrative summarizing Cumberland Farm's position is enclosed.

Please note that we attempted to obtain a utility capacity letter from Central Maine Power but were unsuccessful. We have, however, included an email response from CMP regarding this request and their capacity to serve the proposed addition; given that this site is already functioning as a convenience store/gas station and that the proposed addition will not substantially change business operations, we hope this email is sufficient to meet the City's site plan requirements. If you do require a formal letter from CMP, please let us know and we will again attempt to obtain one.

In addition to the full-sized plans and above-referenced submissions, we have enclosed a CD containing all final application materials. If you have any questions regarding the enclosed, please do not hesitate to contact me.

Thank you for your attention to this matter.

Sincerely,



Jisel E. Lopez

JEL/jp

cc: Kathleen Sousa

Ms. Shukria Wiar, Planner

April 2, 2012

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- In her review of the application, Ms. Schmuckal indicated that the proposed front yard setback exceeded the maximum setback in the B-2 zone but that the Planning Board could approve a further setback upon request as set forth in Section 14-526(a)(27)(j). While the plans have not been revised to reflect this, we do want to make it clear that we are seeking this approval from the Board.
- The Department of Public Services has recommended the construction of an esplanade along Washington Avenue. Cumberland Farms will not be installing an esplanade since this represents an added expense and is not required under the Ordinance. Cumberland Farms is, however, willing to revisit this issue if the Board considers the esplanade to be essential to approval of the project.
- As requested by the Planning staff, Cumberland Farms will work with the Maine DEP and the City of Portland Fire Department to ensure that the new underground tanks are properly registered and that the old tanks are safely removed. Cumberland Farms will obtain the necessary permits from both agencies prior to the start of any work on the project.
- The existing HVAC units will remain in the store and the site plan has been updated to reflect this.
- Cumberland Farms is not proposing any new traffic or wayfinding signs and will therefore not be submitting a separate site plan. Existing signs have been marked on the submitted plans.
- As requested by the abutters to the rear of the property, Cumberland Farms will be restricting the dumpster pickup times. All pickups will now be restricted to between the hours of 7am and 7pm. Cumberland Farms has also added a 6' fence along the rear of the property (see above) to address the abutters' safety concerns.
- Both the Planning Board and the abutters had raised concerns about the side of the building being used as a "public bathroom." To address this issue, Cumberland Farms has added a fence along the side of the property to prevent easy access (see above). Additional lighting has not been included in an effort to avoid increased spillover to the residential abutters.
- There will be no Blue Rhino propane cylinder exchange at the site.
- Since rooftop signs are prohibited in the City, any such signs have been moved to below the roofline and the plans have been revised to reflect this. All signs are expected to meet the criteria in Chapter 22 of the City ordinance and all necessary permits will be requested when appropriate.

Additionally, enclosed please find the following narratives and waiver requests in support of the filed application:

Response 2: GPI concurs that long delays (LOS E/F) currently occur along the Ocean Avenue eastbound and westbound approaches that will be exacerbated with the addition of future traffic growth independent of the proposed expansion project. Based on documentation provided by the Institute of Transportation Engineers (ITE)³ and based on observations conducted by GPI at similar facilities containing a convenience store and gasoline fueling positions, there is poor correlation between the size of a convenience store and the number of trips generated by the facility. The most valid independent variables for predicting trip-generation rates for a facility containing a convenience store and gasoline fueling positions have been found to be based on the number of vehicle fueling positions and the volume of adjacent street traffic. Although the number of vehicle fueling positions and the adjacent street traffic are not proposed to be altered as part of the proposed expansion project, the change in size of the convenience store was selected to demonstrate an overestimated increase in site trips. Since the proposed expansion project will provide additional amenities to existing patrons and the expected increase in site trips is expected to be negligible per independent variable documentation, increases to traffic and delays at the Washington Avenue and Ocean Avenue signalized intersection are expected to be insignificant as a result of the proposed expansion project.

North of the signalized intersection with Ocean Avenue, Washington Avenue southbound consists of two travel lanes that transition to an exclusive left-turn lane and a shared through/right-turn lane at the Ocean Avenue intersection, thereby creating a left-turn trap for vehicles traveling in the inside lane. Subsequent to the collection of traffic counts and the safety evaluation conducted as part of the *Traffic Impact and Access Study*, a lane designation sign was posted on the Washington Avenue southbound approach to Ocean Avenue with the intent of alleviating driver confusion and vehicular incidents. Similarly, Washington Avenue northbound south of the signalized intersection with Ocean Avenue consists of two travel lanes that transition to an exclusive left-turn lane and a shared through/right-turn lane at the Ocean Avenue intersection, thereby creating a left-turn trap for vehicles traveling in the inside lane. Based on field observations and inventory, no lane designation signs are present on the Washington Avenue northbound approach or on the Ocean Avenue eastbound approach. Independent of the proposed expansion project since the expected increase in site trips is expected to be negligible, consideration should be given to posting lane designation signs on the Washington Avenue northbound approach and the Ocean Avenue eastbound approach for vehicular safety.

Comment 3: *Crash numbers are significant at the Washington Avenue/Ocean Avenue intersection. I need to review the crash patterns in more detail.*

³ Johnson, Kevin L., and Matthew I. Hammond. "Trip-Generation Characteristics for Convenience Stores." *ITE Journal* 71.8 (August 2001): 26-30; Ghezawi, Ramzi S., Frederick J. Wegmann, and Arun Chatterjee. "Convenience Store Trip Generation." *ITE Journal* 68.5 (May 1998).



REF.: NEX-2009626

March 30, 2012

Ms. Kathleen A. Sousa
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Framingham, MA 01702

SUBJECT: Response to Traffic Review Comments
Proposed Cumberland Farms Expansion
Portland, Maine

Dear Ms. Sousa:

Greenman-Pedersen, Inc. (GPI) has prepared this Response to Comments (RTC) memorandum to respond to the traffic-related comments made on the *Traffic Impact and Access Study*¹ prepared for a proposed expansion to the existing Cumberland Farms facility located at 301 Washington Avenue in Portland, Maine. An evaluation of the *Traffic Impact and Access Study* was conducted by the City of Portland's traffic peer review consultant, Mr. Thomas A. Errico, P.E. of T.Y. Lin International, and his comments were provided in a February 15, 2012 letter² prepared by Ms. Shukria Wiar, City of Portland Planner. These comments are also reflected in a March 13, 2012 memorandum prepared by Ms. Shukria Wiar to the Portland Planning Board. GPI has reviewed the comments made and has prepared this letter to address any outstanding concerns. A copy of the February 15, 2012 comment letter is attached to this letter.

Comment 1: *I concur with the Traffic Impact Study that the project does not require a Traffic Movement Permit.*

Response 1: Comment acknowledged.

Comment 2: *While the Washington Avenue/Ocean Avenue intersection operates at an acceptable level of service, certain turn movements are projected to operate at poor levels following project completion (It should be noted that the proposed project does not create the deficiency, but exacerbates an existing problem). The applicant should identify strategies for implementation that will eliminate the deficient conditions.*

¹ Greenman-Pedersen, Inc. *Traffic Impact and Access Study – Proposed Cumberland Farms Expansion, Portland, Maine*; September 6, 2011.

² Wiar, Shukria. "Staff Review Comments for Level II Site Plan and Conditional Use – Planning Board Review." Letter to Jisel Lopez. 15 Feb. 2012. Portland, Maine.



Ms. Kathleen A. Sousa
March 30, 2012
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been previously approved by the City of Portland, the proponent will submit a formal waiver request to the City of Portland for the driveway standards that are not satisfied.

Comment 7: *The site should provide facilities that allow pedestrians to safely access from abutting sidewalks.*

Response 7: Based on coordination efforts with the Site Engineer, MHF Design Consultants, Inc., the sidewalk located on Washington Avenue along the site frontage between the two existing site driveways is proposed to remain. At the Ocean Avenue site driveway, a crosswalk is proposed to be striped from the existing sidewalk perpendicular to Ocean Avenue and provide for pedestrian connectivity to the convenience store.

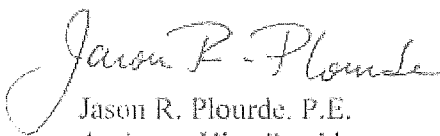
Comment 8: *Sidewalk and curb conditions abutting the property and ADA Ramp provisions at the corner will need to be reviewed by DPS.*

Response 8: Comment acknowledged.

Should you have any questions, or require additional information, please contact me.

Sincerely,

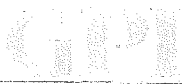
GREENMAN – PEDERSEN, INC.



Jason R. Plourde, P.E.
Assistant Vice President

enclosure(s)

cc. Ms. Jisel Lopez
Mr. Christopher Tymula



Ms. Kathleen A. Sousa
March 30, 2012
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Response 3: Comment acknowledged.

Comment 4: *Typically, traffic volume data should not be older than two years. The counts were conducted in 2009. However, I find the data to be acceptable given the likelihood that little traffic changes have occurred in the study area.*

Response 4: Comment acknowledged.

Comment 5: *The parking space dimensions do not meet City standards. The plans should be revised for spaces that are 9'x18' or a formal waiver from the City's Technical standards should be requested with supporting documentation.*

Response 5: Based on coordination efforts with the Site Engineer, MHF Design Consultants, Inc., the parking spaces have been redesigned to meet the City of Portland's guidelines for standard parking spaces (9 feet wide x 18 feet long).

Comment 6: *The applicant needs to provide an analysis of the existing driveways on both Washington Avenue and Ocean Avenue and whether conditions meet City standards. Specifically, driveway width, driveway separation and corner clearance standards should be reviewed. I would note that I am particularly concerned about the Washington Avenue driveway located nearest the intersection. If the driveways do not meet City standards, a formal waiver request will need to be provided with supporting documentation.*

Response 6: As detailed within *Response 2*, the proposed expansion project will provide additional amenities to existing patrons and traffic-volume increases on the existing site driveways are expected to be insignificant. The layout of the site and site driveways is conducive for on-site circulation and provides the opportunity for vehicles to enter and exit the site at different locations so as not to impact through traffic along adjacent roadways. Per the City of Portland's *Transportation Systems and Street Design Standards*, the existing Washington Avenue and Ocean Avenue driveways connect perpendicular to the respective roadways and meet the minimum width standard (20 feet), with the Washington Avenue north driveway (± 28 feet) and Ocean Avenue driveway (± 40 feet) exceeding the maximum width standard (24 feet) to accommodate heavy vehicles. The distance between the Washington Avenue driveways (± 120 feet) is slightly below the minimum driveway spacing standard (125 feet for a posted speed limit of 30 miles per hour). The corner clearance distance for the Washington Avenue northern driveway (± 150 feet) meets the minimum standard for arterial and collector streets (150 feet), with the Washington Avenue driveway (± 30 feet) and the Ocean Avenue driveway (± 95 feet) not meeting this standard. Although the expected increase in site trips is expected to be negligible (see *Response 2*) and the design and location of the existing driveways have

Cumberland Farms, Inc.
801 Washington Avenue
Waiver Request: Driveway on Washington Avenue

Section 14-183 provides that certain conditional uses, including major and minor auto service stations, are permitted in the B-2 zone if they meet certain requirements such as:

b. Circulation. No ingress or egress driveways shall be located within thirty (30) feet from an intersection. No entrance or exit for vehicles shall be in such proximity to a playground, school, church, other places of public assembly, or any residential zone that the nearness poses a threat or potential danger to the safety of the public.

As has been pointed out by the Planning staff, the project has three driveway cuts from Ocean Avenue and Washington Avenue; one of the driveways servicing Washington Avenue is located less than thirty feet from an intersection. Cumberland Farms is requesting that the Board waive the above requirement with respect to the non-conforming driveway as this driveway has been existing on the site for over 20 years and Cumberland Farms does not intend to make any changes to the driveway as part of its currently proposed expansion. As the attached letter shows, the subject parcel has been approved for operation as an auto service station since 1988. When Cumberland Farms purchased the property and sought approval from the Planning Division for the construction of the existing building, its application materials noted that “[e]xisting curb-cut openings on Washington Avenue will remain.” (See attached). These curb cuts therefore precede Cumberland Farms’ use of the property and Cumberland Farms is requesting that the Board accept them as a grandfathered use.

**Cumberland Farms, Inc.
801 Washington Avenue
Waiver Request: Parking Space Dimensions**

The City of Portland's Technical standards provide that all standard parking spaces shall measure 9 feet wide by 18 feet long. Cumberland Farms is proposing to add an additional ten parking spaces for a total of 32 parking spaces on the site (including eight at pumps). As originally proposed, these parking spaces measured 9 feet by 19 inches. Cumberland Farms has revised its site plan to conform to the City's technical requirements for parking space dimensions for the majority of the parking spaces on the site but is requesting a waiver of these standards for two parking spaces. As shown on the attached site plan, these spaces are located at the front of the retail store, in front of the new proposed Red Box video kiosk. Due to the fact that Cumberland Farms is proposing to extend the sidewalk directly abutting these spaces, a reduction in the length of these spaces is not feasible. Since the size of the property easily accommodates these larger parking spaces and, in Cumberland Farms' experience, customers generally prefer larger spaces, Cumberland Farms is requesting that the Planning Board approve the construction of two 9' x 18' spaces on the property.

RESPONSE TO LOCAL COMMENTS

Proposed Cumberland Farms Expansion - Portland, Maine

ATTACHMENT

STAFF REVIEW COMMENTS FOR LEVEL II SITE PLAN AND CONDITIONAL USE
-- PLANNING BOARD REVIEW LETTER

area for this project is resulting from the larger convenience store roof, the discharge from this roof would be considered clean run off. Also, the additional site pavement runoff is de minimus relative to the existing paved area runoff.

Cumberland Farms, Inc.
301 Washington Avenue
Response to Stormwater Management Comments

Cumberland Farms submits the following response to the comments of the City's consultant, David Senus, regarding the application of the stormwater management rules:

Comment 3.b.1- The City of Portland Code of Ordinances Section 14-525(b).3.b refers to the City Technical Manual for stormwater standards. The City Technical Manual refers to the Maine DEP Chapter 500 Stormwater Management Rules . The Chapter 500 Basic Standards (4.A.1) and General Standards (4.B.1) only apply when the project disturbs over 1 acre. The Flooding Standard (4.D.1) only applies when the project results in 3 acres or more of impervious area. Therefore, it is our opinion that the project is not subject to any of the Maine DEP standards.

Comment 3.b.2- We agree that the project falls within the Fall Brook watershed which is classified as an Urban Impaired Stream. However, since the project does not impact more than 1 acre, then no stormwater permits are required according to the Flowchart for Chapter 500 Standards.

Also, the DEP Chapter 500 Urban Impaired Stream Standard (4.D.1) states that the standard must be met when the project results in 3 acres or more of impervious area or 20 acres or more of developable area. Furthermore, Standard (4.D.3) also states that a project can receive an exception when there is a redevelopment of existing pavement which is not likely to increase stormwater impacts beyond the levels already present in the runoff from the existing impervious area. Since a majority of the new impervious

Cumberland Farms, Inc.
801 Washington Avenue
Waiver Request: driveways and corner clearance distances

As set forth in Response 6 in the March 30, 2012 letter from Jason Plourde, P.E., the site existing driveways do not meet current City standards. In particular, the distance between the Washington Avenue driveways is slightly below the minimum driveway spacing standard. One of the Washington Avenue driveways and the Ocean Avenue driveway also do not meet the minimum standard for arterial and collector streets.

Cumberland Farms is seeking a waiver of these requirements because it is not proposing any changes to the existing uses on the property and the design and location of these driveways were previously approved by the City of Portland (see attached documentation submitted in connection with other driveway waiver request). As shown on the traffic study submitted with the initial site plan application materials and as further explained in Mr. Plourde's letter, the existing layout of the site, including the driveways, is conducive for on-site circulation in a busy traffic area. While the proposed project will increase the retail space, this change is not expected to materially increase the number of site trips to the site. Furthermore, because the number of gasoline dispensers will remain unchanged, increases in traffic and delays at the Washington Avenue and Ocean Avenue intersection are expected to be insignificant as a result of this project.

Jamie Cough
Energy Services Advisor
Central Maine Power Company
162 Canco Road
Portland, ME 04103
207-842-2367 office
207-458-0382 cell
207-626-4082 fax

Attachment B.6

Kathleen Sousa

From: Dominic A. Taverna
Sent: Monday, October 17, 2011 8:33 AM
To: Kathleen Sousa
Cc: 'brent@dipoulin.com'; John Marth; Michael Durant
Subject: FW: Cumberland Farms Store- 801 Washington Ave Portland, ME 441-1065485-012

Categories: Red Category

Kathie

I received a voice mail on Friday from Jamie at Central Maine Power. I returned his VM and left my contact information on Friday. Please see the attached E-mail from Jamie regarding power at this location. Let me know if you need anything else on this.

Dom

Dominic A. Taverna
Senior Construction Project Manager
Cumberland Gulf Group of Companies
100 Crossing Boulevard
Frammingham, MA 01702

Office 508-270-4407
Cell 508-350-2678
Email: dtaverna@cumberlandgulf.com

This electronic message contains information from Cumberland Gulf Group of Companies, which may be privileged. The information is intended to be for the use of the addressee only. If you are not the addressee, note that any disclosure, copy, distribution or use of the contents of this message is prohibited.

From: Cough, Jamie [<mailto:Jamie.Cough@cmgco.com>]
Sent: Monday, October 17, 2011 6:32 AM
To: Dominic A. Taverna
Subject: Cumberland Farms Store- 801 Washington Ave Portland, ME 441-1065485-012

Dominick

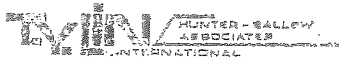
This store currently has a peak load of 58 kw (approximately 64 kva), and is served by a 3x50 kva bank (150 kvø total). This 600A service can be supported by the existing transformer and service drop. Should the actual demands approach the transformer rating of 150 kva, we would be looking at installing an additional run of 336 to the service drop. Given the diversification of your loads, it is not likely.

If you have any additional questions, please contact me.

Regards,

Jamie

1987 Traffic Study



BACKGROUND AND EXISTING CONDITIONS

Cumberland Farms, Inc. retained T. Y. Lin International/Hunter-Ballew Associates (TYLI/HBA) to prepare a traffic impact study in conjunction with the proposed revision/expansion of the existing Cumberland Farms Convenience Store/Gasoline Station located at the intersection of Washington Avenue/Ocean Avenue in Portland, Maine. Figure 1 presents the location of the proposed project.

Present plans indicate the proposed development will consist of the removal of the existing 1920 square foot building and construction of a new 2400 square foot convenience store and removal of the existing 2 gasoline islands and construction of 4 new gasoline islands. Existing curb-cut openings on Washington Avenue will remain, while the curb-cut openings on Ocean Avenue will be moved approximately 35 feet to the east.

The purpose of this study is to determine and evaluate the impact of traffic associated with the proposed revision/expansion on the operation of the Washington Avenue/Ocean Avenue intersection, and evaluate the proposed site access/circulation plan.

A manual turning movement count was conducted at the Washington Avenue/Ocean Avenue intersection on Thursday, November 19, 1987 from 7:00 - 9:00 AM and 3:30 - 5:30 PM. The results of these counts indicate the peak hours occur from 7:30 - 8:30 AM and 4:30 - 5:30 PM. Comparison of these volumes to historical volumes taken in 1982 (by the PACTS Traffic Signal Management Study) indicates volumes on the Ocean Avenue approaches have increased by approximately 52 percent per year during the AM peak hour and 27 percent per year during the PM peak hour. These large growth patterns probably are not associated with typical development growth or background growth but with major adjustments in travel patterns. All indicators seem to point toward the Tukey's Bridge reconstruction project and its associated delays and congestion as the underlying reason for the altered travel patterns. To provide a more realistic estimation of traffic volumes at the Washington Avenue/Ocean Avenue intersection following the completion of the Tukey's Bridge project, 1987 Ocean Avenue volumes were estimated by increasing the 1982 PACTS volumes by 28 percent or 5.6 percent per year. Following this adjustment, the peak hour traffic volumes were factored to estimate the 30th highest hour volumes (typically used for impact analysis) and daily traffic volumes adjusted to represent the Average Annual Daily Traffic volumes utilizing MDOT weekly group mean factors. Figure 2 presents the estimated 1987 30th highest hour volumes during the AM and PM peak hours and the 1987 Average Annual Daily Traffic volumes.

Attachment B7

CITY OF PORTLAND, MAINE

389 CONGRESS STREET
PORTLAND, MAINE 04101
(207) 778-5451



DEPARTMENT OF PLANNING & URBAN DEVELOPMENT

J. SAMUEL HOFFRES, CHIEF
INSPECTION SERVICES DIVISION

June 14, 1988

Cumberland Farms
777 Dedham Street
Canton, MA 02021

Re: 801 Washington Ave, Portland, ME

Dear Sir:

Your application to demolish existing building and construct a 2400 sq ft building has been reviewed and a permit is herewith issued subject to the following requirements:

Site Plan Review Requirements

Inspection Services--Approved W.J. Turner 6/8/88
Planning Division--Approved Ms. Maureen O'Meara 6/6/88
Public Works--Approved Mr. R.Roy 3/10/88
Fire Dept.--Approved F.L. J.R. Doblowski

Building Code Requirements

1. The demolition of this structure shall be done as per the building code.
2. All lot lines shall be clearly marked before calling for foundation inspection.
3. Roof shall be designed for a minimum of 50 psf live load. Also for canopy over gas pumps.
4. Gas storage tank shall be installed as per D.E.P. regulation.

If you have any questions regarding these requirements, please do not hesitate to contact this office.

Sincerely,

J. Samuel Hoffres
Chief, Inspection Services

PSH/jmv

LED AREA LIGHT - MEDIUM (XAM)



LUMINAIRE ORDERING INFORMATION

TYPICAL ORDER EXAMPLE: **XAM 2 LED 119 CW UE WHT PCI120**

Prefix	Distribution	Light Source	# of LEDs	LED Color Temperature	Line Voltage	Luminaire Finish	Options
XAM - LED Area Medium	2 - Type 2 3 - Type 3 FT - Forward Throw 5 - Type 5	LED	119	CW - Cool White (5750°K nom.)	UE - Universal Voltage (120-277) 347 480	BLK - Black BRZ - Bronze BUF - Buff GPT - Graphite MSV - Metallic Silver PLP - Platinum Plus SVG - Satin Verde Green WHT - White	Button Type Photocells PCI 120 - 120v PCI 208 - 208v PCI 240 - 240v PCI 277 - 277v PCI 347 - 347v PCR - Twistlock Photoelectric Control Receptacle TB - Terminal Block

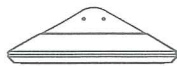
Beam Spread	EPA
Single	1.2
D180°	2.4
D90°	2.0
T90°	3.4
TN120°	3.4
Q90°	4.0

Description	Order Number
BKA-XBO-EC-6-CLR 6" Extension Arm	382136CLR ¹
BKS-XBO-WM-* ² -CLR Wall Mounting Bracket	382132CLR ¹
DVS - Direct View Shield (rear mount only) ²	390687CLR
X4RPP Round Pole Plate for 4" Poles	379967CLR ¹
X5RPP Round Pole Plate for 5" Poles	379968CLR ¹
PC 120 - Photocell for use w/ PCR option (120V)	122514
PC 208-277 - Photocell for use w/ PCR option (208V, 240V or 277V)	122515
PC 347 - Photocell for use w/ PCR option (347V)	159516
PC 480 - Photocell for use w/ PCR option (480V)	1225180

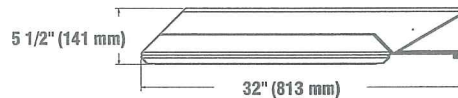
NOTES:

- 1- Replace 'CLR' with desired color (ex. 382136BRZ)
- 2- Direct View Shield adds to fixture EPA. Consult factory.

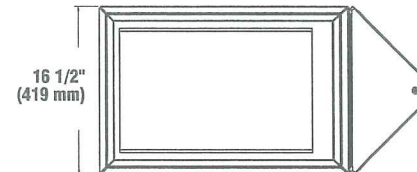
DIMENSIONS



Front View

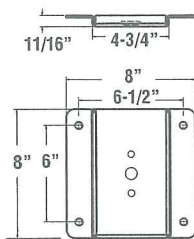


Side View



Bottom View

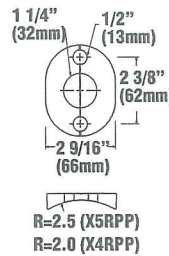
Wall Bracket



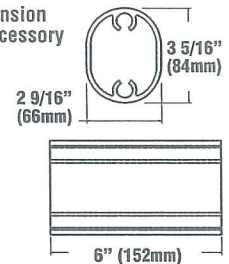
Top View

Front View

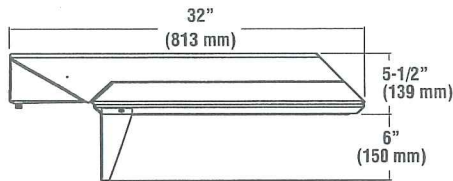
Round Pole Plate Accessory



6" Extension Arm Accessory



DVS - Direct View Shield (390687CLR)



06/07/10

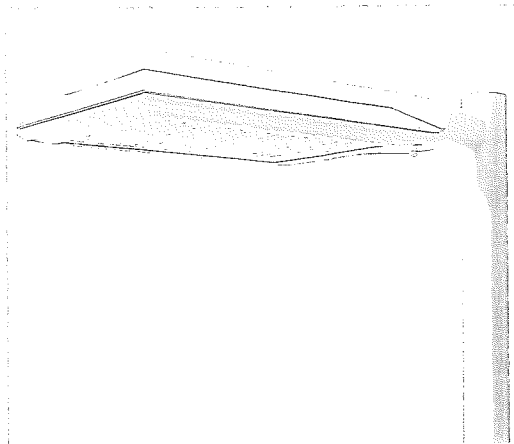
© 2010
LSI INDUSTRIES INC.

Project Name _____ Fixture Type _____

Catalog # _____



LED AREA LIGHT - MEDIUM (XAM)



U.S. and International patents pending

EXPECTED LIFE - Minimum 60,000 hours to 100,000 hours depending upon the ambient temperature of the installation location. See LSI web site for specific guidance.

LEDS - Select high-brightness LEDs. 5750°K color temperature (nominal).

DISTRIBUTION/PERFORMANCE - Types 2, 3, FT*, and 5. Exceptional uniformity with full cutoff creates bright environment at lower light levels. Improved backlight cutoff minimizes light trespass.

HOUSING - Housing is die-formed aluminum. Weather-tight housing contains factory prewired driver and field connections. Wiring access door is located underneath and utilizes tool-less thumbscrew fastener.

OPTICAL UNIT - Lens is clear, flat tempered glass, sealed to aluminum optics housing to create an IP67 rated, sealed optical unit.

MOUNTING - Tapered rear design allows fixtures to be mounted in a quad pattern without the need for extension arms. Use with Crossover steel round and steel square poles. See separate data sheets.

ELECTRICAL - Universal voltage power supply 120-277 VAC, (50/60 Hz) input. Also available in 347 volt and 480 volt. Supply voltage must be specified for use with optional button-type photocells. PCI is available in 120, 208, 240, 277 or 347 volt. Optional twistlock photocell receptacle is also available. Photocell must be ordered separately. See accessories.

DRIVER - State-of-the-art driver technology designed specifically for LSI LED light sources provides unsurpassed system efficiency. Components are fully encased in potting material for moisture resistance. Driver complies with IEC and FCC standards.

OPERATING TEMPERATURE - -40°C to +50°C (-40°F to +122°F).

FINISH - Fixtures are finished with LSI's DuraGrip® polyester powder coat finishing process. The DuraGrip finish withstands extreme weather changes without cracking or peeling, and is guaranteed for five full years. Standard colors include bronze, black, platinum plus, buff, graphite, satin verde green, metallic silver and white.

WARRANTY - LSI LED fixtures carry a limited 5-year warranty.

PHOTOMETRICS - Application layouts are available upon request. Contact LSI Applications Group at lighting.apps@lsi-industries.com

SHIPPING WEIGHT - 27.0 lbs (12.7 kg)

LISTING - ETL listed to U.S. and International safety standards. Suitable for wet locations.

* FT distribution is classified as semi-cutoff.

Crossover® LED Lighting

lighting facts^{CM}

A Program of the U.S. DOE

Light Output (Lumens)	9996
Watts	139.5
Lumens per Watt (Efficacy)	71

Color Accuracy Color Rendering Index (CRI)	70
---	----

Light Color
Correlated Color Temperature (CCT)

5750 (Daylight)

↓

Warm White	Bright White	Daylight
2700K	3000K	4500K

All results are according to IESNA LM-79-2008: *Approved Method for the Electrical and Photometric Testing of Solid-State Lighting*. The U.S. Department of Energy (DOE) verifies product test data and results. Products qualified under the DOE ENERGY STAR® program have the ENERGY STAR mark on this label.

Visit www.lightingfacts.com for the *Label Reference Guide*.

Registration Number: KGCN-FAQHKB
Model Number: XAM-2-LED-119-CW-UE
Type: Outdoor pole/arm-mounted area and roadway fixtures

This "Lighting Facts Label" represents LSI's commitment, through participation in the Department of Energy's SSL Quality Advocates program, to accurately report performance criteria of our LED fixtures. This label represents a single test of a single fixture. Input power below represents rated values for the family. Color temperature and lumen output for each variation of this Crossover® fixture can be found in the ordering information below as well as at www.lightingfacts.com.

LIGHT OUTPUT		
Fixture	Distribution	Lumens (Nominal)
XAM	Type 2	9900
	Type 3	9400
	Type FT	10,200
	Type 5	9000
Input Power of 137 watts		



LED WALL LIGHT - SMALL (XAWS) AND MEDIUM (XAWM)

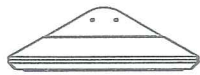


LUMINAIRE ORDERING INFORMATION

TYPICAL ORDER EXAMPLE: **XAWS 3 LED 63 CW UE MSV PCI277**

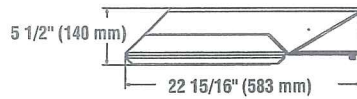
Prefix	Distribution	Light Source	# of LEDs	LED Color Temperature	Line Voltage	Luminaire Finish	Options
XAWS - LED Wall Mount Small	2 - Type 2 3 - Type 3 FT - Forward Throw	LED	63	CW - Cool White (5750°K nom.)	UE - Universal Voltage (120-277) 347 480	BLK - Black BRZ - Bronze BUF - Buif GPT - Graphite MSV - Metallic Silver PLP - Platinum Plus SVG - Satin Verde Green WHT - White	Button Type Photocells PCI 120 - 120v PCI 208 - 208v PCI 240 - 240v PCI 277 - 277v PCI 347 - 347v
XAWM - LED Wall Mount Medium	2 - Type 2 3 - Type 3 FT - Forward Throw		119				

DIMENSIONS

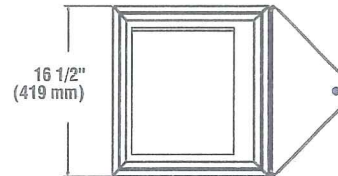


Front View

SMALL

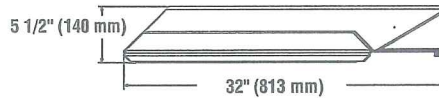


Side View

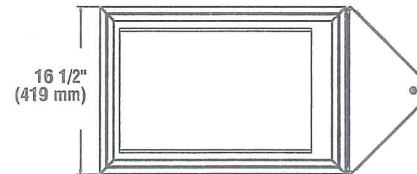


Bottom View

MEDIUM



Side View

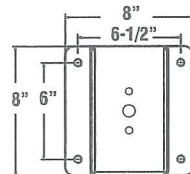


Bottom View

WALL BRACKET



Top View



Front View

04/21/10

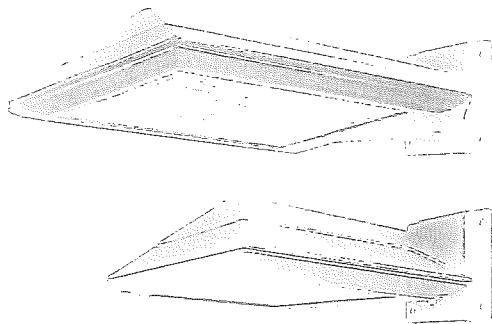
Project Name _____ Fixture Type _____

© 2010
LSI INDUSTRIES INC.

Catalog # _____



LED WALL LIGHT - SMALL (XAWS) AND MEDIUM (XAWM)



U.S. and International patents pending

EXPECTED LIFE - Minimum 60,000 hours to 100,000 hours depending upon the ambient temperature of the installation location. See LSI web site for specific guidance.

LEDS - Select high-brightness LEDs. 5750°K color temperature (nominal).

DISTRIBUTION/PERFORMANCE - Types 2, 3, and FT*. Exceptional uniformity with full cutoff creates bright environment at lower light levels.

HOUSING - Housing is die-formed aluminum. Weather-tight housing contains factory prewired driver and field connections. Wiring access door is located underneath and utilizes tool-less thumbscrew fastener.

OPTICAL UNIT - Lens is clear, flat tempered glass, sealed to aluminum optics housing to create an IP65 rated, sealed optical unit.

MOUNTING - Tapered rear design allows fixtures to be easily mounted in a junction box using a simple wall plate (supplied).

ELECTRICAL - Universal voltage power supply 120-277 VAC, (50/60 Hz) input. Also available in 347V and 480V. Supply voltage must be specified for use with optional button-type photocells. PCI is available in 120, 208, 240, 277, or 347 volt.

DRIVER - State-of-the-art driver technology designed specifically for LSI LED light sources provides unsurpassed system efficiency. Components are fully encased in potting material for moisture resistance.

OPERATING TEMPERATURE - -40°C to +50°C (-40°F to +122°F).

FINISH - Fixtures are finished with LSI's DuraGrip® polyester powder coat finishing process. The DuraGrip finish withstands extreme weather changes without cracking or peeling, and is guaranteed for five full years. Standard colors include bronze, black, platinum plus, buff, graphite, satin verde green, metallic silver and white.

WARRANTY - LSI LED fixtures carry a limited 5-year warranty.

PHOTOMETRICS - Application layouts are available upon request. Contact LSI Applications Group at lighting.apps@lsi-industries.com

SHIPPING WEIGHT - 21.0 lbs (9.5 kg) for small and 27.0 lbs (12.7 kg) for medium

LISTING - Listed to U.S. and Canadian safety standards. Suitable for wet locations.

* FT distribution is classified as semi-cutoff.

LIGHT OUTPUT		
Fixture	Distribution	Lumens (Nominal)
XAWS	Type 2	5103
	Type 3	4355
	Type FT	5350
Input Power of 70 watts		
XAWM	Type 2	9900
	Type 3	9400
	Type FT	10,200
Input Power of 127 watts		

IP67



RHS



American Innovation
American Made

Intertek
Suitable for wet locations



04/21/10

© 2010
LSI INDUSTRIES INC.

Project Name _____ Fixture Type _____
Catalog # _____



GENERATION 3 LED FOCUS CANOPY LIGHT (CRO3)

LUMINAIRE ORDERING INFORMATION

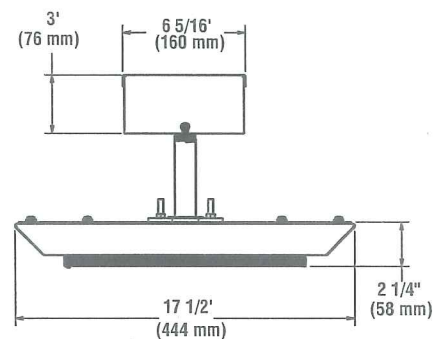
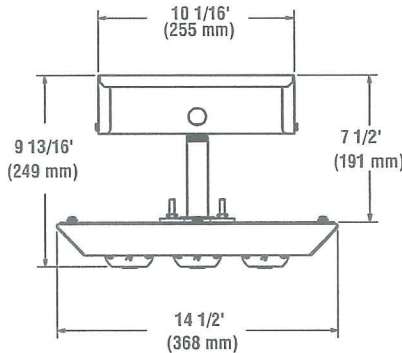
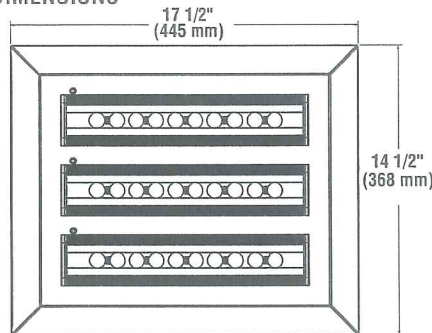
TYPICAL ORDER EXAMPLE: **CRO3 FO LED 30 350 CW UE WHT**

Prefix	Distribution	Light Source	# of LEDs	Drive Current	Color Temperature	Input Voltage	Finish
CRO3	FO - Focus	LED	30	350 - 350 mA	CW - Cool White (5000° K nom)	UE - Universal Voltage (120-277V AC)	WHT - White

ACCESSORY ORDERING INFORMATION (Accessories are field installed)			
Description	Order Number	Description	Order Number
Retrofit Panel - SC to CRO3, for 16" Deck Panel	430951	Retrofit 2x2 Cover Panel Blank (no holes)	357282
Retrofit Panel - EC / ECTA / SCF to CRO3, for 16" Deck Panel	430765	Retrofit RIC Cover Panel Blank (no holes)	354702
Retrofit Panel - SC to CRO3, for 12" Deck Panel	430797	Kit - Hole Plugs and Silicone (enough for 25 retrofits) ¹	1320540
Retrofit Panel - ECTA / SCF to CRO3, for 12" Deck Panel	430759	¹ Consists of (25) 7/8" hole plugs and (1) 10.3 oz tube of RTV	
Retrofit 2x2 Cover Panel (w/ centered hole pattern for CRO3)	430966		

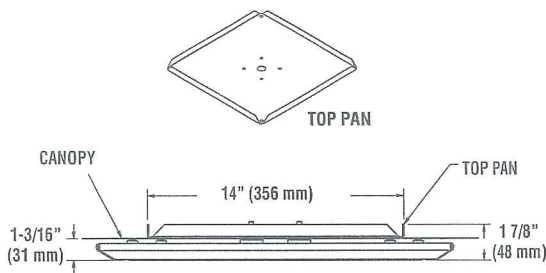
Superkits® are available to retrofit CRO3 Ambient and Focus fixtures into a wide variety of existing 2x2 and recessed housings. See separate spec sheets.

DIMENSIONS

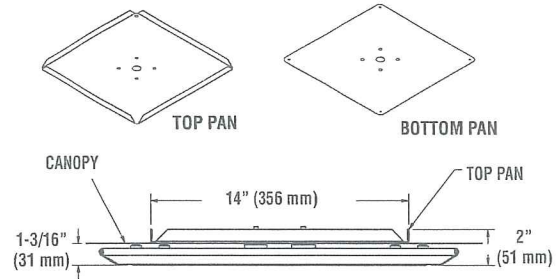


SHIPPING WEIGHTS	
12.2 lbs.	1 fixture
53.3 lbs.	4 fixtures

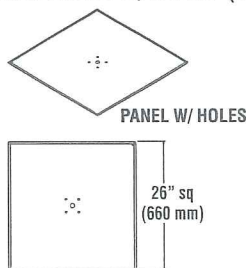
16" DECK RETROFIT PANEL - SC (#430951)



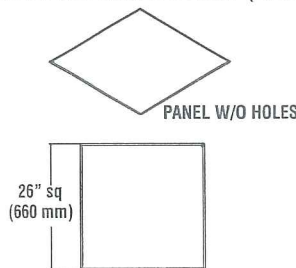
16" DECK RETROFIT PANELS - EC/ECTA/SCF (#430765)



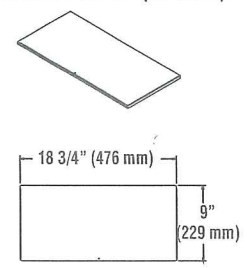
2X2 COVER PANEL W/ HOLES (430966)



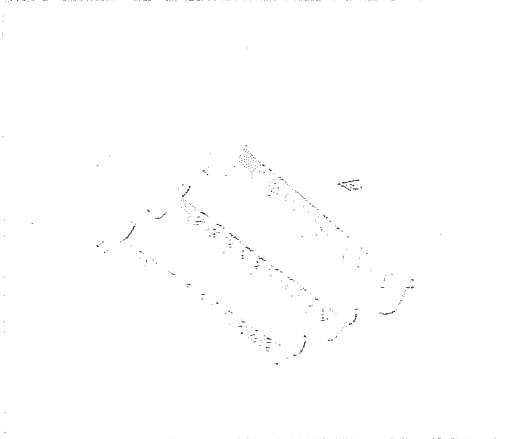
2X2 COVER PANEL BLANK (357282)



RIC COVER PANEL (354702)



GENERATION 3 LED FOCUS CANOPY LIGHT (CR03)



US patents D593106 & D574965 & 7828456 and US & Int'l. patents pending

LEDS - Select high-brightness LEDs. 5300°K color temperature, 70 CRI (nominal).

OPTICS / DISTRIBUTIONS - Ultra-High efficiency optics provide precise beam placement for optimal retail surface illumination. Each bank of LEDs in a light cartridge is independently adjustable between +/- 45° allowing targeted zone illumination. Regardless of light cartridge position luminaire provides cutoff.

OPTICAL UNIT - Featuring a slim 2" profile luminaire. Housing is die-formed aluminum with independently adjustable extruded aluminum light cartridges. Each light cartridge is provided with a gasketed clear tempered glass lens providing a water-resistant seal.

DRIVER - State-of-the-art driver technology designed specifically for LSI LED light sources provides unsurpassed system efficiency. Input power is 50 watts. Components are fully encased in potting for moisture resistance. Driver complies with IEC and FCC standards.

DRIVER HOUSING - Weather-tight aluminum driver/electrical enclosure is elevated above canopy deck to ensure no water entry and providing "knock-out" entry for primary wiring.

FINISH - Standard color is white. Fixture is finished with LSI's DuraGrip® polyester powder coat process. DuraGrip withstands extreme weather changes without cracking or peeling.

OPERATING TEMPERATURE - -40°C to +50°C (-40°F to +122°F)

ELECTRICAL - Universal voltage power supply 120-277V VAC, (50/60 Hz) input. Two-stage surge protection (including separate surge protection built into electronic driver) meets IEEE C62.41.2-2002, Scenario 1, Location Category C

INSTALLATION - Installs in a 12" or 16" deck pan. Deck penetration consists of 5 drilled holes simplifying installation and water sealing. Unit is designed to retrofit into existing Scottsdale® (4") hole as well as openings for Encore® and Encore® Top Access and to reconnect wiring from the SC/ECTA without having to relocate conduit. Retrofit panels are available for existing Scottsdales and Encores (see back page) as well as kits for recessed and 2x2 installations (see separate spec sheets).

EXPECTED LIFE - Minimum 60,000 hours to 100,000 hours depending upon the ambient temperature of the installation location. See LSI web site for specific guidance.

WARRANTY - Limited 5-year warranty

LISTING - ETL listed to UL1598, UL8750 and other U.S. and International safety standards. Suitable for wet locations.

PHOTOMETRICS - Application layouts are available upon request. Contact LSI Petroleum Lighting or petroleum.apps@lsi-industries.com

LIGHT OUTPUT - CR03	
Distribution	Lumens (Nominal)
Type FO	1000 (for each of 3 banks - total 3000)
	Input Power of 40 watts



Suitable for wet locations

IP67

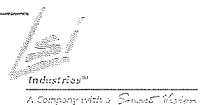
RoHS



07/23/11

© 2011
LSI INDUSTRIES INC.

Project Name _____ Fixture Type _____
Catalog # _____



GENERATION 3 LED AMBIENT CANOPY LIGHT (CRO3)



LUMINAIRE ORDERING INFORMATION

TYPICAL ORDER EXAMPLE: **CRO3 S LED 128 450 CW UE WHT IMS**

Prefix	Distribution	Light Source	# of LEDs	Drive Current	Color Temperature	Input Voltage	Finish	Options*
CRO3	S-Symmetric A - Asymmetric ES - Enhanced Symmetric	LED	64	350 - 350mA	CW - Cool White	UE - Universal Voltage (120-277 AC)	WHT - White	IPC - Integral Photocell IMS - Integral Motion Sensor
			84	450 - 450mA				
			104	550 - 550mA				
			128	350 - 350mA 450 - 450mA				

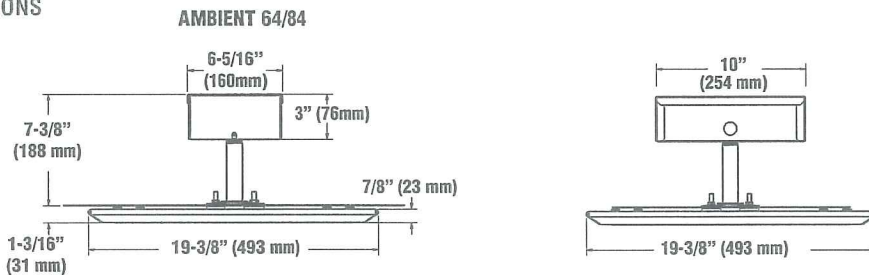
*Consult factory for availability

ACCESSORY ORDERING INFORMATION (Accessories are field installed)

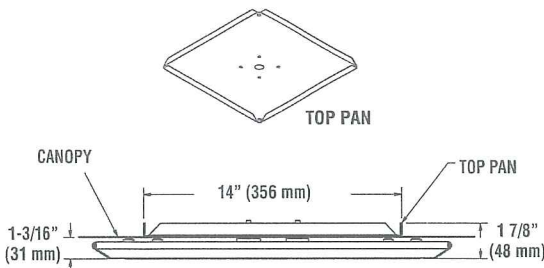
Description	Order Number	Description	Order Number
Retrofit Panel - SC to CRO3, for 16" Deck Panel	430951	Retrofit 2x2 Cover Panel Blank (no holes)	357282
Retrofit Panel - EC / ECTA / SCF to CRO3, for 16" Deck Panel	430765	Retrofit RIC Cover Panel Blank (no holes)	354702
Retrofit Panel - SC to CRO3, for 12" Deck Panel	430797	Kit - Hole Plugs and Silicone (enough for 25 retrofits) ¹	1320540
Retrofit Panel - ECTA / SCF to CRO3, for 12" Deck Panel	430759	¹ Consists of (25) 7/8" hole plugs and (1) 10.3 oz tube of RTV	
Retrofit 2x2 Cover Panel (w/ centered hole pattern for CRO3)	430966		

Superkits® are available to retrofit CRO3 Ambient and Focus fixtures into a wide variety of existing 2x2 and recessed housings. See separate spec sheets.

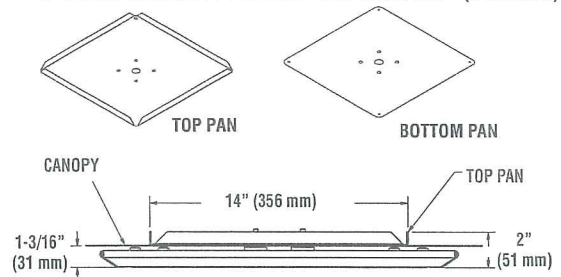
DIMENSIONS



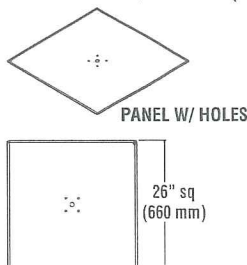
16" DECK RETROFIT PANEL - SC (#430951)



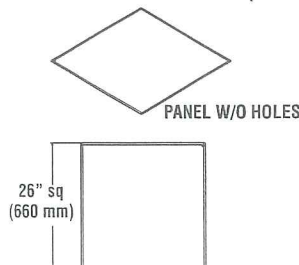
16" DECK RETROFIT PANELS - EC/ECTA/SCF (#430765)



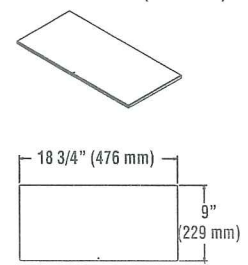
2X2 COVER PANEL W/ HOLES (430966)



2X2 COVER PANEL BLANK (357282)



RIC COVER PANEL (354702)



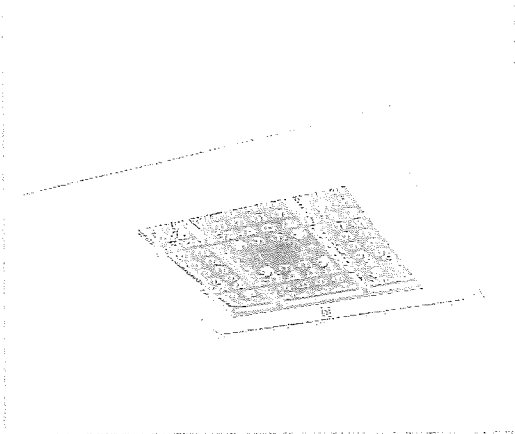
07/28/11

© 2011
LSI INDUSTRIES INC.

Project Name _____ Fixture Type _____
Catalog # _____



GENERATION 3 LED AMBIENT CANOPY LIGHT (CRO3)



US patent D574994 & 7828456 and US & Int'l. patents pending

SMARTTEC™ ENERGY SAVING FEATURES:

THERMAL CONTROL - Sensors in both optical unit and driver enclosure reduce drive current when ambient temperatures exceed 50°C. Current is lowered in imperceptible 5% increments every 5 minutes until recommended operating temp is reached.

DAYLIGHT SENSING - Eliminates day-burners (optional) by sensing daylight levels (not affected by the light coming from the LEDs). Turns fixture off at light levels approximately equal to sunrise and on at sunset.

OCCUPANCY SENSING - Small, built-in flush-mounted sensor (optional) detects movement in a 20-foot circle. Unit dims to 30% power when no presence is detected. Smart dim slowly lowers light level over a 15-second period.

LEDS - Four LED array choices; 64 and 84 (19.4" square optical unit) and 104 and 128 (21.7" square). Each feature select high-brightness LEDs; 5300°K color temperature, 70 CRI (nominal).

DRIVE CURRENT OPTIONS - Choose from three drive current levels; 350 mA (most energy-efficient), 450 mA or 550 mA (best first-cost value).

OPTICS / DISTRIBUTIONS - Ultra-high efficiency reflectors provide three distributions to meet your lighting needs. Choose from symmetric, asymmetric (for directing light toward or away from selected areas), or enhanced symmetric (for increased brightness and attraction from the road). Luminaire is rated full cutoff.

OPTICAL UNIT - Featuring an ultra-slim 1" profile, housing is die-formed aluminum with a clear tempered glass lens. Unit is water-resistant, sealed to an IP67 rating. Patented integral single blade heat sink does not trap dirt and grime, ensuring cool running performance over the life of the fixture.

THE INDUSTRY'S ONLY BREATHABLE SEAL - Teflon® breather allows super-tight IP67 protection while preventing temperature cycling from building up internal pressures and vacuums that can stress optical unit seals and components.

DRIVER - Proprietary, state-of-the-art SmartTec™ driver technology provides unsurpassed system efficiency, control and protection. Components are fully encased in potting for IP67 moisture resistance. Complies with IEC and FCC standards.

DRIVER HOUSING - Wet location rated driver/electrical enclosure is elevated above canopy deck to help prevent water entry and to provide easy "knock-out" connection of primary wiring.

FINISH - Standard color is white. Finished with LSI's DuraGrip® polyester powder coat process. DuraGrip withstands extreme weather changes without cracking or peeling.

OPERATING TEMPERATURE - -40°C to +50°C (-40°F to +122°F)

ELECTRICAL - Universal voltage power supply, 120-277 VAC, 50/60 Hz input. Two-stage surge protection (including separate surge protection built into electronic driver) meets IEEE C62.41.2-2002, Scenario 1, Location Category C

INSTALLATION - Installs in a 12" or 16" deck pan. Deck penetration consists of 5 drilled holes simplifying installation and water sealing. Unit is designed to retrofit into existing Scottsdale® (4") hole as well as openings for Encore® and Encore® Top Access and to reconnect wiring from the SC/ECTA without having to relocate conduit. Retrofit panels are available for existing Scottsdales and Encores (see back page) as well as kits for recessed and 2x2 installations (see separate spec sheets).

EXPECTED LIFE - Minimum 60,000 hours to 100,000 hours depending upon the ambient temperature of the installation location. See LSI web site for specific guidance.

WARRANTY - Limited 5-year warranty.

LISTING - ETL listed to UL1598, UL8750 and other U.S. and International safety standards. Suitable for wet locations.

PHOTOMETRICS - Application layouts are available on request. Contact LSI Petroleum Lighting or petroleum.apps@lsi-industries.com

Crossover LED Lighting

lighting facts^{CM}

A Program of the U.S. DOE

Light Output (Lumens)	11950
Watts	146.5
Lumens per Watt (Efficacy)	81

Color Accuracy	67
Color Rendering Index (CRI)	

Light Color
Correlated Color Temperature (CCT)

5176 (Daylight)

↓

Warm White	Bright White	Daylight	
2700K	3000K	4500K	6500K

All results are according to IESNA LM-79-2008: Approved Method for the Electrical and Photometric Testing of Solid-State Lighting. The U.S. Department of Energy (DOE) verifies product test data and results.

Visit www.lightingfacts.com for the Label Reference Guide.

Registration Number: KGGN-8OLD1Y
Model Number: CRO3 S LED128 350 CW UE
Type: Canopy Light

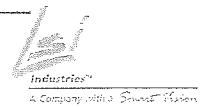
LIGHT OUTPUT - CRO3					
	# of LEDs	Distribution / Lumens			Watts
		S	A	ES	
350 mA	64	7087	6872	6272	75
	84	8996	8899	7996	99
	104	11243	10951	10318	123
	128	13599	13237	12416	150
450 mA	64	8616	8304	7440	99
	84	10753	10639	9567	131
	104	13302	12981	12346	162
	128	16031	15687	14722	199
550 mA	64	9767	9435	8386	128
	84	12158	11939	10796	163

American Innovation
American Made



IP67 RoHS CE

Suitable for wet locations



by staff. The Design Manual emphasizes that the review authority must assess the project "as a whole" and determines that it is "the best design response to the standards achievable for the overall design program".

In this case, the convenience store and gas station is an existing on the site and many of these conditions do not apply to the project. Applicable design standards include:

1. Standard 4: Windows. The applicant should consider installing one more window panel in the proposed building addition, on the façade facing Washington Avenue and maintain 50% visibility through the windows.
2. Standard 7: Development Relationship to the Street. Since the building sets back from the street, it is recommended that the applicant include more landscaping in the existing landscaped island. Addition street trees would also add vertical height and improve the streetscape along the two streets.

IV. STAFF RECOMMENDATION

Subject to the proposed motions and conditions of approval listed below, Planning Division staff recommends that the Planning Board approve the proposed development with conditions.

IX. PROPOSED MOTIONS

A. WAIVERS

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report # 17-12 relevant to the Portland Technical and Design Standards and Guidelines and other regulations and the testimony presented at the Planning Board hearing:

- b-0 That the Planning Board [waives / does not waive] Section 1.7.2.4., the maximum driveway width (two-way) of twenty-four (24) feet to be forty-six (46) feet, for the main entrance driveway on Ocean Avenue. *(b,0) Lewis absent to match existing.*
- b-0 That the Planning Board [waives / does not waive] Section 1.7.2.8. number of driveways: no more than two (2) driveways shall be permitted for ingress and egress purposes to allow three driveways to be permitted on site. *the driveway closest to the intersection be ingress only provided*
- b-0 That the Planning Board [waives / does not waive] Section 1.7.2.7. location and spacing of driveways: the access driveways to corner lots shall be located a minimum of thirty-five (35) feet from the intersection to allow driveway closest to the intersection on Washington Avenue to remain, *as ingress only*
- b-0 That the Planning Board [waives / does not waive] Section 1.14, Parking Space Design: Standard parking of 9' x 18' to allow two of the parking spaces on site to be 9' x 19'.

B. CONDITIONAL USE

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #17-12 for application #2012-418 relevant to the conditional use application and other regulations, and the testimony presented at the Planning Board public hearing, the Planning Board finds the conditional use for auto service stations [is / is not] in conformance with the conditional use standards of the Land Use Code subject to the following conditions:

- b-0 i. The collection of trash shall occur between the hours of 7 a.m. and 7 p.m.

- ii. ~~If Planning Board does not grant a waiver for the number of curb cuts and a curb cut closure is required, then revised plans shall be submitting showing the closure of the curb cut to meet technical standards and a five foot wide landscaped buffer along the street right-of-way line for the closed curb cut.~~

C. DEVELOPMENT REVIEW

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #17-12 for application #2012-418 relevant to the Site Plan and other regulations, and the testimony presented at the Planning Board hearing, the Planning Board finds the following:

1. SITE PLAN REVIEW

The Planning Board ^{approves} ~~votes~~ ^{insert vote} that the plan is in conformance with the site plan standards of the Land Use Code, subject to the following condition(s) of approval:

- 1 i. That prior to the issuance of a building permit, the applicant shall contribute \$5,000 towards the implementation of future traffic signal improvements.
- 2 ii. That the City shall monitor the future crash patterns at the intersection of Washington and Ocean Avenues and shall identify traffic signal phasing modifications if the crash rates increases.
- 3 ⁶⁻⁰ iii. That prior to the issuance of a building permit, the applicant shall submit revised plans showing closure the driveway closest to the intersection on Washington Avenue, replacing it with a permanent landscaped island.
- 4 iv. That the applicant shall install detectable warning panels on the sidewalk ramp at the crosswalk crossing on Washington and Ocean Avenues.
- 5 v. That the applicant shall address all the comments in David Senus memorandum dated 04.12.2012 in regard to stormwater on the final site plan prior to the issuance of a building permit.
- 6 vi. The applicant and all assigns, must comply with the conditions of Chapter 32 Storm Water including Article III. Post-Construction Storm Water Management, which specifies the annual inspections and reporting requirements. The developer/contractor/subcontractor must comply with conditions of the construction storm water management plan and sediment & erosion control plan based on City of Portland standards and state guidelines.
- vii. That the lighting plan shall be submitted to meet the light trespass of 0.1 foot candle at property line on the northwest corner, prior to the issuance of a building permit.

viii. If Planning Board does not grant a waiver for the number of curb cuts and a curb cut closure is required, then revised plans shall be submitting showing the closure of the curb cut to meet technical standards and a five foot wide landscaped buffer along the street right-of-way line for the closed curb cut.

Attachments:

7. That 5x — of esplanade on Washington Ave shall be installed by the applicant.

Planning Board Report Attachments

- 1. Marge Schmuckal Memorandum, Dated 03.09.2012

2. Tom Errico Memorandum, Dated 04.18.2012
3. Jeff Tarling Memorandum, Dated 03.08.2012
4. David Senus Memorandum, Dated 04.12.2012
5. David Margolis-Pineo Memorandum, Dated 04.13.2012
6. Captain Chris Pirone Memorandum
7. Design Standards for B-2
8. Abutter Comment

A - Applicant's Submittal, Dated 01.23.2012

- A.1 Application for Conditional Use
- A.2 Application for Site Plan
- A.3 Project Description
- A.4 Evidence of Right, Title and Interest
- A.5 Copies of State and/or Federal Permits
- A.6 Evidence of Financial Capacity
- A.7 Evidence of Utility Capacity
- A.8 Management of Solid Waste Generated On-Site
- A.9 Wastewater Application
- A.10 Traffic Impact and Access Study

B- Applicant's Submittal, Dated 04.02.2012

- B.1 Cover Letter Outlining Changes on Site Plan
- B.2 Response to Traffic Comments
- B.3 Waiver Request for Driveway on Washington Avenue
- B.4 Waiver Request for Parking
- B.5 Response to Stormwater Management Comments
- B.6 Evidence of Utility Capacity
- B.7 Demo Permit from 1988
- B.8 Lighting Catalogue Cuts

C- Site Plan

- C.1 Cover Letter
- C.2 Existing Conditions Plan
- C.3 Demolition Plan
- C.4 Site Plan
- C.5 Grading and Utility Plan
- C.6 Landscaping Plan
- C.7 Details
- C.8 Canopy Elevations
- C.9 Existing/ Proposed sign Drawing
- C.10 Proposed Floors Plans
- C.11 Exterior Elevations
- C.12 Exterior Elevations
- C.13 Proposed Life Safety Plan
- C.14 Lighting Plan/ Photometric Plan

Shukria Wiar - 801 Washington Avenue - Traffic Comments

From: Tom Errico <thomas.errico@tylin.com>
To: Shukria Wiar <SHUKRIAW@portlandmaine.gov>
Date: 4/18/2012 9:27 AM
Subject: 801 Washington Avenue - Traffic Comments
CC: David Margolis-Pineo <DMP@portlandmaine.gov>, Jeff Tarling <JST@portland...>

Shukria – The following is a status update on my February 5, 2012 comments.

February 5, 2012 Comment - I concur with the Traffic Impact Study that the project does not require a Traffic Movement Permit.

- o **Status – No response necessary**

February 5, 2012 Comment - While the Washington Avenue/Ocean Avenue intersection operates at an acceptable level of service overall, certain turn movements are projected to operate at poor levels of service following project completion (It should be noted that the proposed project does not create the deficiency, but exacerbates an existing problem). The applicant should identify strategies for implementation that will eliminate the deficient conditions.

- o **Status – The applicant has not identified recommendations that will improve the operating condition of the intersection. Intersection capacity expansion is not an option at this location. The City has been aggressively implementing traffic signal improvements along key corridors of the City as part of a regional traffic management system and Washington Avenue is a future candidate for the system. Accordingly, the applicant should contribute \$5,000 towards the implementation of future traffic signal improvements.**

February 5, 2012 Comment - Crash numbers are significant at the Washington Avenue/Ocean Avenue intersection. I need to review the crash patterns in more detail.

- o **Status – The majority of crashes at this location are rear-end type occurring on Washington Avenue. These crashes are typical for a congested intersection and improvements in signal equipment efficiency can be expected to improve conditions. No specific action is recommended at this time, other than upgrading the existing traffic signal system. A secondary pattern was identified, left-turn movements from eastbound Ocean Avenue colliding with Ocean Avenue westbound through vehicles. Only three crashes of this type occurred over the researched three-year period. I would suggest that the City monitor future crash patterns and identify traffic signal phasing modifications if rates intensify.**

February 5, 2012 - Typically, traffic volume data should not be older than two years. The counts were conducted in 2009. However, I find the data to be acceptable given the likelihood that little traffic changes have occurred in the study area.

- o **Status – No response necessary.**

February 5, 2012 - The parking space dimensions do not meet City standards. The plans should be revised for spaces that are 9'x18' or a formal waiver from the City's Technical standards should be requested with supporting documentation.

- o **Status – I support a waiver from the technical standards for the two noted spaces that are proposed to be 9'x19'.**

February 5, 2012 - The applicant needs to provide an analysis of the existing driveways on both Washington Avenue and Ocean Avenue and whether conditions meet City standards. Specifically, driveway width, driveway separation and corner clearance standards should be reviewed. I would note that I am particularly concerned about the Washington Avenue driveway located nearest the intersection. If the driveways do not meet City standards, a

Shukria Wiar - 801 Washington Ave - revised

From: Marge Schmuckal
To: Shukria Wiar
Date: 3/9/2012 12:48 PM
Subject: 801 Washington Ave - revised

801 Washington Avenue - 163-B-3, 4, 5, 14, 15, 17
#2012-418 Conditional Use & Level II Site Plan
B-2 Zone - 3/9/2012 - revision

Last year this property was rezoned to a B-2 business zone. The owners are proposing to expand the retail portion of the major auto service station. The new addition will be approximately 1,474 sq. ft. in size. The type of review is a conditional use appeal to the Planning Board with a level II site plan.

I have reviewed the submitted plans. The addition would not meet the maximum building setback requirement as state under 14-185(b)(3)a.ii. However, at the end of the dimensional requirements, apparently section 14-185 (b)(3)(7), there is a relief of the maximum building setback requirements for building additions. This addition is exempt from the maximum setback requirement.

All other dimensional requirements such as other setbacks, building height, impervious surface and parking requirements are being met.

It is noted that the plans indicate some new roof top signs. Roof top signs are not allowed in any zone. Separate permits are required for any new or replacement signs through Inspection Services. Separate permits are also required for any HVAC units that may be installed or upgraded. It is important to note that the B-2 zone has maximum noise limits that shall be met. Any submission for HVAC systems shall include dBA levels.

Marge Schmuckal
Zoning Administrator

Shukria Wiar - Cumberland Farms - Washington Avenue Project

From: Jeff Tarling
To: Shukria Wiar
Date: 3/8/2012 4:53 PM
Subject: Cumberland Farms - Washington Avenue Project

Hi Shukria -

In review of the proposed expansion project at the Washington Avenue Cumberland Farms - I would recommend that the project address buffering along the residential edge using fencing - solid wood 6' in height or black vinyl chain link with privacy slats. The same would apply for the dumpster enclosure. Landscape features - existing trees & shrubs along the Ocean Avenue and Washington Avenue frontage should be evaluated / improved (recent snowfall prevented complete inspection). Due to high traffic volumes and limited space, additional trees are not warranted in the space between the sidewalk and street. Additional landscape planting would be recommended if the Washington Avenue at Ocean Avenue curb-cut is eliminated.

I would be glad to work with the applicant to review the existing plant material recommendations.

Jeff Tarling
City Arborist

formal waiver request will need to be provided with supporting documentation.

- o Status – The driveway located on Washington Avenue nearest Ocean Avenue does not meet City corner clearance and driveway spacing standards and should be closed. The applicant has not provided documentation that supports the need to maintain this driveway from a site access/egress and on-site circulation perspective. I would also note that the proposed site exceeds the maximum number of driveways allowed per City standards (a maximum of two driveways is permitted).

February 5, 2012 - The site should provide facilities that allow pedestrians to safely access from abutting sidewalks.

- o Status – A pedestrian connection between Ocean Avenue and the building has been provided. No such facility is provided between Washington Avenue and the building. This would be expected to be the routing of the majority of pedestrians and accordingly the site should be revised to include a safe routing from Washington Avenue.

February 5, 2012 - Sidewalk and curb conditions abutting the property and ADA Ramp provisions at the corner will need to be reviewed by DPS.

- o Status – DPS has provided comments on this subject with the exception of sidewalk ramp condition at the corner of the Washington Avenue/Ocean Avenue intersection. This issue still needs resolution.

If you have any questions, please contact me.

Best regards,

Thomas A. Errico, PE
Senior Associate
Traffic Engineering Director

TYLIN INTERNATIONAL

12 Northbrook Drive
Falmouth, ME 04105
207.347.4354 direct
207.400.0719 mobile
207.781.4753 fax
thomas.errico@tylin.com
Visit us online at www.tylin.com

"One Vision, One Company"
Please consider the environment before printing.

February 9, 2012

April 13, 2012

TO: Barbara Barhydt
Shukria Wiar

FROM: David Margolis-Pineo
Dept. of Public Services

RE: Preliminary Review Comments: 801 Washington Ave
Cumberland Farms

This Department has the following preliminary comments. Final comments may differ from the comments listed here.

1. All property needs to be located by a registered land surveyor. The southerly property corner may be difficult due to the placement of existing light poles. Please address.
The plan is now stamped by are by a professional surveyor. No further comment.
2. Please state if the survey plan is on Maine State Plane Coordinates.
This statement has been made.
3. The northwest corner of the site is shown draining onto others property. There is no clear drainage off the abutting property and ponding water is resulting. This is not allowed. It is requested that the applicant address this issue.
Site plan still shows drainage onto others property. This issue still needs to be addressed.
4. Stormwater treatment, inspection and maintenance. Please see comments by David Senus.
Please see Dave Senus' comments dated 4-12-12
5. The sidewalk and curbing are in good repair. However the sidewalk along Washington Ave is wide and does not have an esplanade. Consideration may want to given to re-installing an esplanade.
The applicant is indicating reluctance to re-instating an esplanade on Washington Ave in front of #801. By removing a 5' by 90' section of bituminous sidewalk and replacing with loam and seed, the applicant could eliminate approximately 450 sq ft of impervious area. By closing the curb cut and landscaping, additional porous could be credited to meeting stormwater treatment requirements.
6. **At the corner of Washington and Ocean Ave, adjacent to the applicant's property, the applicant is requested to install a detectable warning panel on the sidewalk ramp which crosses Ocean Ave and to install a sidewalk ramp with a detectable warning panel for the crosswalk crossing Washington Ave.**

This Department has no further comments at this time.

MEMORANDUM

TO: Shukria Wiar, Planner
FROM: David Senus, P.E.
DATE: April 12, 2012
RE: Cumberland Farms, 801 Washington Ave



Woodard & Curran has reviewed the updated plans and response to comments for the Level II Site Plan Application for a Cumberland Farms Expansion at 801 Washington Avenue in Portland, Maine. The project involves a 1,474 square foot building addition and associated site improvements, including the replacement of underground storage tanks and piping.

Documents Reviewed

- Cover Letter dated April 2, 2012 from Jisel E. Lopez to Shukria Wiar, along with referenced attachments.
- Engineering Design Plans, Sheets 1, 3, 4, 4A, 5, 5A dated September 6, 2011 and revised on March 29, 2012, prepared by MHF Design Consultants, Inc. on behalf of Cumberland Farms.

Comments

- The site plans adequately address the Basic Standards with the exception of providing for a stabilized construction exit. A stabilized construction exit should be depicted on the plans and a detail should be provided.
- The City of Portland requires that all Level II site plan applications submit a stormwater management plan pursuant to the regulations of MaineDEP Chapter 500 Stormwater Management Rules, including the General and Flooding Standards (*Technical Manual, Section 5. II. Applicability in Portland. C. a. and City of Portland Code of Ordinances Sec. 14-526. Site plan standards. (b). 3. b.*). The applicability of the General and Flooding standards in the City of Portland applies to all regulated development (except single and two family homes); superseding the applicability requirements outlined in the MaineDEP Chapter 500 standards. The project proposes an additional 3,308 sf of new impervious area. Because the existing site is already fully developed, the applicant is only responsible for ensuring that runoff from an impervious surface area equal to 3,308 sf is managed in accordance with the General and Flooding Standards in order to meet the City's requirements for those Standards. Additional treatment requirements apply under the Urban Impaired Stream Standard, as noted herein.
- The project appears to at least partially fall within the Fall Brook Watershed, which is classified as an Urban Impaired Stream by the MaineDEP. The City of Portland requires that all development subject to City of Portland review, except single and two-family homes, shall be required to comply with the Urban Impaired Stream Standard pursuant to MaineDEP Chapter 500 Stormwater Management Rules if they are located within an Urban Impaired Stream watershed (*Technical Manual, Section 5. II. Applicability in Portland. A.*). Applicability of these standards within the City of Portland supersedes the applicability requirements outlined in the MaineDEP Chapter 500 standards. A project in the watershed of an urban impaired stream must mitigate project impacts by treating, reducing, or eliminating an off-site or on-site impervious stormwater source following the guidance outlined in the MaineDEP Chapter 500 standards. Because the site appears to partially fall within the Fall Brook Watershed, we request that the applicant clarify the locations where their site runoff enters the City's storm drain or combined sewer systems.
- Please clarify the discharge locations for the building and canopy roof leaders.

Appendix 2

City of Portland
Technical Standards and Design Guidelines

Development in the B-1, B-1b, B-2, B-2b shall meet the following guidelines in order to meet the Site Plan Standards

1. Building Location and Form

Buildings shall be located near the street so as to create an urban street wall.

An urban street wall is created by a pattern of buildings which line the street in a consistent manner, thereby establishing a desirable spatial relationship between the building in the commercial district and the major object. Location is one of several related factors defining the street environment.

Building Form, including height, bulk, and massing, contribute to the development of a street wall.

The desired condition is to have the building frame and enclose the street, which is achieved by providing building height that is proportionate to the width of the adjoining major street. A ratio of building height to street width of one-to-two creates a strong "room-like" street, while a one-to-three ratio provides good street definition and proportion. Shorter buildings of one story facing broad streets will not achieve the desired relationship.

In the B-2b zone, buildings adjacent to streets should approach 1:2 height to street width, with a minimum of 1:3.

For a fifty-foot street right-of-way, therefore, a minimum height of 15' is required, with 25' height preferred. An eighty-foot right-of-way requires about 27' to achieve the 1:3 proportion., with 40'-height preferred. Obviously, buildings located as close as possible to the street right-of-way will provide better definition and proportion than buildings set further back.

2. Building Function

An urban street and business district requires a substantial intensity and variety of uses.

It is beneficial to have mixed uses within portions of buildings situated near the street. For example, a retail first floor might have office or residential on the second or third floors. This provides both the scale of building height desired, as well as the economic vitality of the business district.

3. Orientation of Buildings and their Entrances to the Street

Major building entries shall be designed and located to provide the primary building access oriented to the public street and sidewalk.

Doorways should be prominent and obvious in appearance, so as to attract the users toward the entry. Major entry features should primarily address the street, with entry courts, display windows, signage, lights, walkways, and vestibules, as appropriate. Major entries should be adjacent to, or very close to, the street and public sidewalk.

Shukria Wiar - Re: 801 Washington Ave, Cumberland Farms (Application number 2012-418)

From: Shukria Wiar
To: Chris Pirone
Subject: Re: 801 Washington Ave, Cumberland Farms (Application number 2012-418)

Fire is all set.

Captain Chris Pirone
Portland Fire Department
Fire Prevention Bureau
380 Congress Street
Portland, ME 04101
(t) 207.874.8405
(f) 207.874.8410

Shukria Wiar - Cumberland Farms/Hersey/Powers

From: <pmcgehee@perkinsthompson.com>
To: <shukriaw@portlandmaine.gov>
Date: 3/9/2012 10:17 AM
Subject: Cumberland Farms/Hersey/Powers
CC: <jlopez@pmhlegal.com>

Dear Ms. Wiar:

Thank you for speaking with me my phone yesterday regarding the pending Washington Avenue Cumberland Farms conditional use application to expand its facility by approximately 60%. I had called on behalf of the Slemmons Road abutters behind Cumberland Farms, Nancy Hersey and Dave Powers and their two children. They have serious concerns about the expansion, which concerns we hope can be resolved with the cooperation of Cumberland Farms. In that regard, I have attempted to reach Cumberland Farms' attorney, Jisel Lopez, to discuss these concerns and proposed resolutions prior to the Planning Board's Tuesday workshop, and hope to speak with her shortly.

In your and my conversation yesterday, you asked if I could send an introductory e-mail to you, which this is, regarding Nancy and Dave's concerns.

Their overriding concerns are the safety of their children, visual impact, and peace and quiet.

As you know, there is presently only a low chain link fence behind Cumberland Farms. On 5 occasions over the past year Nancy and Dave have had people trespass on their property from Cumberland Farms, one running across their property to escape some wrongdoing apparently, whom Dave confronted. Although there is a former paper street between Nancy and Dave's property and Cumberland Farms, which you have advised has since been conveyed to the City Fire Station—that former paper street is vacant land and there is no structural or visual barrier between their backyard and Cumberland Farms.

Thus, should this application otherwise comply with the City's Site Plan measurable standards (I have not yet reviewed those standards as to such matters as setbacks, road frontage, parking and the like), Nancy and Dave respectfully request the Board require a seven foot stockade fence be installed along the back and portion of the sides of the site, for reasons of safety, and to minimize visual impact. They also request that hour limitations be imposed so there is no early morning or late night noise with dumpsters, bottles and the like.

It is our hope that Cumberland Farms will volunteer these measures, but we appreciate the City requiring these minimum protections in any case, as they are easily achievable and reasonable measures that will help minimize the adverse impact the proposed expansion will have on Nancy and Dave and their children, and other residents on Slemmons Road.

Thank you for your consideration of these matters.

Regards, Peggy

PEGGY L. MCGEHEE
ATTORNEY

PERKINS | THOMPSON

Crosswalks shall be provided within parking lots and across entrance driveways, directing pedestrians to building entrances.

Street trees shall be planted along property street frontage 25ft. on center.

9. Transit Connections

Development proposed along established transit corridors must design uninterrupted access from the proposed development to the transit stop.

An easement to place a transit shelter may be requested for development located along a transit corridor.

PETRUCCELLI, MARTIN & HADDOW, LLP

Attorneys at Law
50 Monument Square
Post Office Box 17555
Portland, Maine 04112-8555

JISEL E. LOPEZ
jlopez@pmhlegal.com

Via Hand Delivery

January 23, 2012

Barbara Barhydt, Development Review Services Mgr
Planning & Urban Development Department
389 Congress Street, 4th Floor
Portland, Maine 04101-3509

RE: Cumberland Farms, Inc. – 801 Washington Avenue

Dear Barbara:

Enclosed please find the following documents and things for filing with your office on behalf of Cumberland Farms, Inc.:

1. Level II Site Plan Application with attachments;
2. Conditional Use Application with attachments;
3. City of Portland Wastewater Capacity Application;
4. Traffic Impact and Access Study;
5. Full-size copy of plans;
6. 11 x 17 copy of plans;
7. CD with copies of Items 1 through 7; and
7. Cumberland Farms' Check No. 262166 in the amount of \$700.

Address the following criteria in your written application and any applicable conditional use standards contained in the zoning code for the specific use.

Upon showing that a proposed use is a conditional use under this article, a conditional use permit shall be granted unless the Board determines that:

- a. There are unique or distinctive characteristics or effects associated with the proposed conditional use;
- b. There will be an adverse impact upon the health, safety, or welfare of the public or the surrounding areas;
- c. Such impact differs substantially from the impact which would normally occur from such a use in that zone.

CONTACT INFORMATION:

<p>Applicant's Contact for electronic plans</p> <p>Name: James B. Haddow, Esq. e-mail: jhaddow@pmhlegal.com work #:207-775-0200</p>	
<p>Applicant – must be owner, Lessee or Buyer</p> <p>Name: Cumberland Farms, Inc. Business Name, if applicable: Address: 100 Crossing Blvd. City/State :Framingham, MA Zip Code: 01702</p>	<p>Applicant Contact Information</p> <p>Work # 508-270-1466 Home# Cell # Fax# 781-459-0569 e-mail: ksousa@cumberlandgulf.com</p>
<p>Owner – (if different from Applicant)</p> <p>Name: Address: City/State : Zip Code:</p>	<p>Owner Contact Information</p> <p>Work # Home# Cell # Fax# e-mail:</p>
<p>Billing Information</p> <p>Name: Cumberland Farms, Inc. Address: 100 Crossing Blvd. City/State :Framingham, MA Zip Code: 01702</p>	<p>Billing Information</p> <p>Work # 508-270-1466 Cell # Fax# 781-459-0569 e-mail: ksousa@cumberlandgulf.com</p>



**Conditional Use Application
Development Review Application
Portland, Maine**

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Conditional Use Review. General information pertaining to the thresholds of review for site plans, public noticing procedures and the fee structure is contained in site plan applications and within the Developer's Packet.

(Please submit the Conditional Use Application in addition to an applicable site plan application.)

PROJECT NAME: Cumberland Farms

CHART/BLOCK/LOT: Map 163, Block B, Lots 3, 4, 5, 14, 15, 17

RIGHT, TITLE OR INTEREST: (Please identify the status of the applicant's right, title, or interest in the subject property.)

Applicant is the owner in fee simple of the property by virtue of deed dated 9/26/83 from Gulf Oil Corporation to VSH Realty, Inc., and recorded in the Cumberland County Registry of Deeds at Book 6310, Page 145. Cumberland Farms, Inc., is the successor in interest by merger with VSH Realty, Inc.

(Please provide documentary evidence, attached to this application, of the applicant's right, title, or interest in the property. For Example – a deed, option or contract to purchase or least the subject property.)

VICINITY MAP: (Please attach a map showing the subject parcel and abutting parcels, labeled as to ownership and/or current use.)

EXISTING USE: Describe the existing use of the subject property.

Retail establishment with gasoline sales (major/minor auto service station)

TYPE OF CONDITIONAL USE PROPOSED:

Major/Minor auto service station

SITE PLAN: Provide a site plan of the property, showing existing and proposed improvements, which meets the submission requirements of the applicable level of site plan review.

CONDITIONAL USE AUTHORIZED BY: SECTION 14- 183

Address any specific conditional use standards for the specific use contained in the zoning code in the written submission.

STANDARDS – CRITERIA FOR CONDITIONAL USE APPEAL – Section 14-474

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

<p>Conditional Use</p> <p><input checked="" type="checkbox"/> Conditional Use \$100</p> <hr/> <p>The City invoices separately for the following:</p> <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) <p>Third party review is assessed separately.</p>	
<p>Site Plan Application</p> <p>Please submit a separate application for the applicable site plan review. Fees and charges are listed within the site plan application</p>	<p>Site Plan Application submitted and Fees Paid (office use) _____</p>

APPLICATION SUBMISSION

All site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

The application for a conditional use review must contain the following items:

- a. Conditional Use Application form that is completed and signed.
- b. Cover letter stating the nature of the project.
- c. Evidence of right, title and interest.
- d. Written Submittals that address the conditional use standards of Section 14-474 and any applicable standards of review contained in the zoning code for the specific use.
- e. One set of the paper plans and maps based upon the boundary survey at a scale not less than one (1) inch to fifty (50) feet and containing the information required for the applicable level of site plan review. The plan requirements and submission requirements are listed in the Site Plan Ordinance (Section 14 -527) of the Land Use Code. Refer to the application checklist for a detailed list of submittal requirements.
- f. One set of the plans at 11X17.
- g. The conditional use application fee and all other applicable fees as determined on the site plan application.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521) and the Conditional Use Standards (Section 14-47A). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov. Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Conditional Use review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: Cumberland Farms, Inc. By <i>Kathleen A. Sousa</i>	Date: <i>1/20/12</i>
--	-------------------------

Kathleen A. Sousa, Land Planning Manager

The Portland Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division Fourth Floor, City Hall 389 Congress Street (207) 874-8721 or 874-8719	Office Hours Monday thru Friday 8:00 a.m. – 4:30 p.m.
--	--

outstanding safety concerns. The exterior of the building will be renovated to reflect the new corporate colors and design themes, as shown on the business elevations provided with this application. Additional parking, including added handicapped spaces and bicycle racks, will be provided. Existing signage will be replaced with smaller signs, and LED lights where applicable, thereby reducing the potential impact on neighboring uses. Landscaping will meet all required buffers under the Zoning Ordinance and will in fact be enhanced to improve the appearance of the Property. Cumberland's proposed site improvements will enhance the physical appearance of the Property and allow it to continue to provide quality goods and services to the area. While auto service stations are an intensive use, the gasoline sales portion of the business will not be materially expanded and will therefore not cause any increased adverse impact upon the health, safety or welfare of the public or surrounding area.

In particular, the proposed store renovations will include:

- Renovation of existing dumpster on new concrete pad
- New concrete walk at the front and side of the store, including the proposed addition
- Installation of protective bollards (with sleeves) at parking spaces along the front and side of the store
- Relocation and expansion of parking spaces
- Addition of bicycle racks
- Landscaping to be enhanced (see Landscaping Plan)

Conditional Use Application Narrative
Cumberland Farms, Inc.
801 Washington Avenue, Portland, Maine

Cumberland Farms, Inc. ("Cumberland"), is the current owner in fee simple of the above-referenced property ("the Property") used as a retail establishment combined with gasoline sales. In 2010, the subject property was rezoned from Neighborhood Business (B-1) and Residential (R-3) to Community Business (B-2). While the purpose of the rezoning was to allow for the eventual expansion of the retail building, the rezoning also resulted in bringing the present use into conformity with the Portland Zoning Ordinance.¹ Major and minor auto service stations are conditional uses in the B-2 Community Business Zone pursuant to Section 14-183 of the Portland Zoning Ordinance.

Among the stated purposes of the B-2 Community Business Zone is "[t]o provide appropriate locations for the development and operation of community centers offering a mixture of commercial uses and services serving the adjoining neighborhoods and the larger community." Cumberland has been operating a retail store and gasoline station at the above-referenced property for a number of years without adverse impacts to the area or neighboring properties. Cumberland now proposes to construct a 1,474 s.f. addition to its existing 2,400 s.f. convenience store for a total retail gross floor area of 3,874 s.f. The proposed additions and changes are intended to incorporate Cumberland's new corporate branding, enhance the retail experience for Cumberland Farms customers, and address

¹ Section 14-47 of the Portland Zoning Ordinance provides that "retail establishments combined with gasoline, diesel or propane fuel sales shall be considered a single use for zoning purposes and shall constitute a major or minor auto service establishment." Major or minor auto service stations are not permitted in the B-1 and R-3 districts but are authorized conditional uses in the B-2 Community Business Zone.

Attachment C.1

PROPOSED SITE IMPROVEMENT PLANS

for

ASSESSORS MAP 163 BLOCK B LOTS 3, 4, 5, 14, 15 & 17

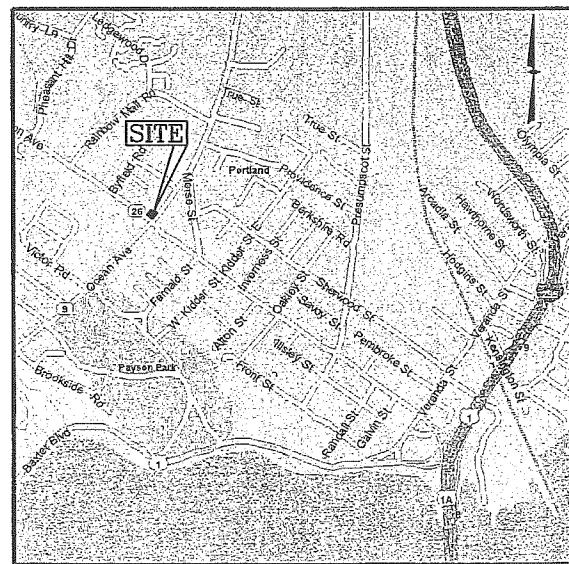
801 WASHINGTON AVENUE - ROUTE 26

PORTLAND, MAINE

Prepared for:



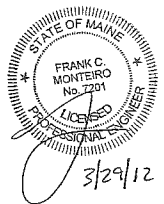
100 CROSSING BLVD, FRAMINGHAM, MASSACHUSETTS 01702



LOCATION MAP
(NOT TO SCALE)

INDEX TO DRAWINGS

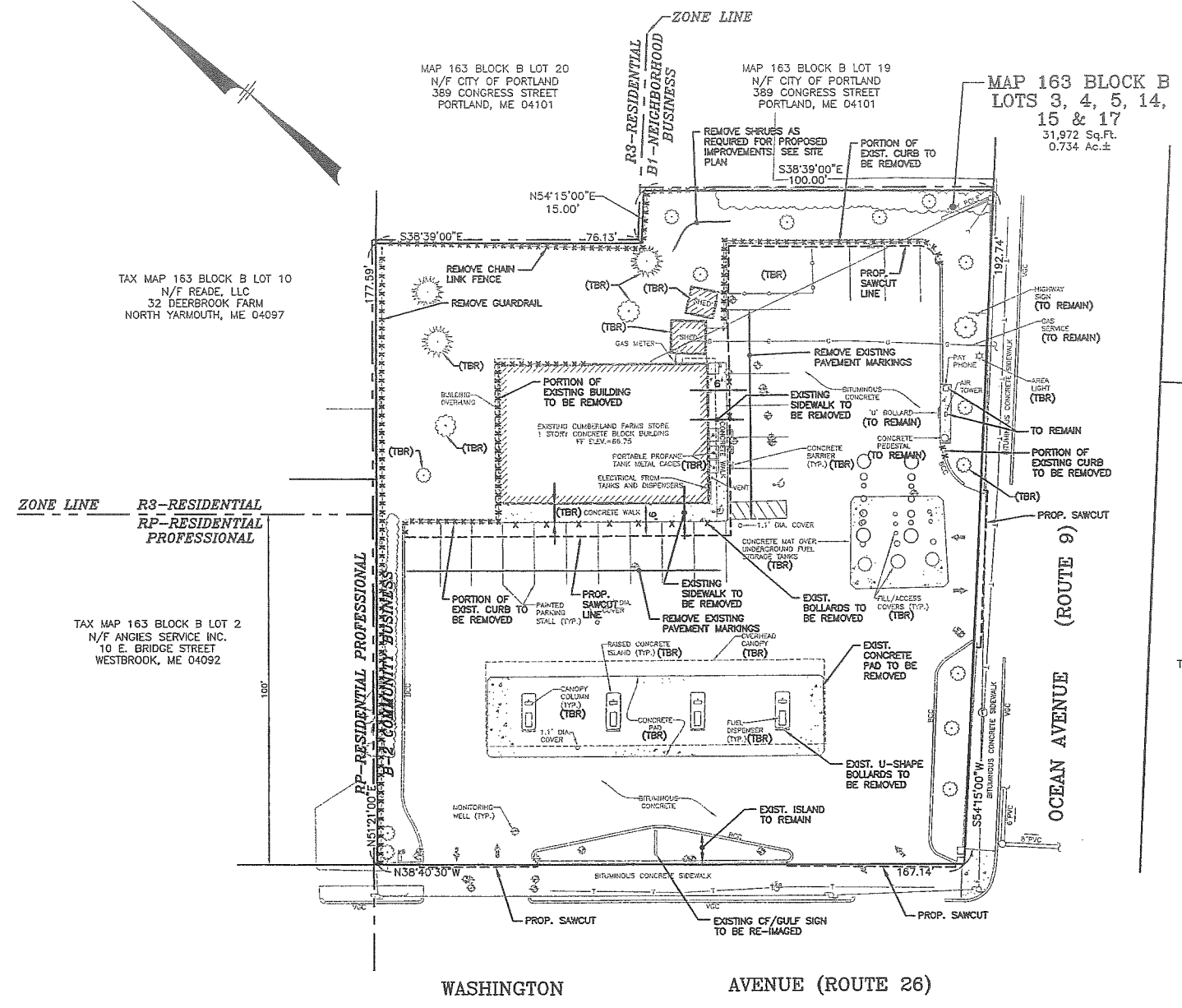
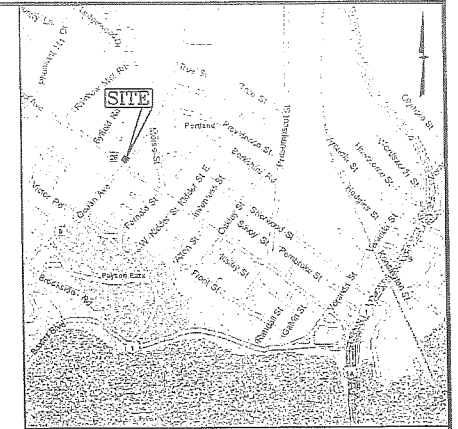
1. TITLE SHEET
2. EXISTING CONDITIONS PLAN
3. DEMOLITION PLAN
4. SITE PLAN
- 4A. GRADING & EROSION CONTROL PLAN
5. LANDSCAPE PLAN
- 5A. DETAIL SHEET
6. CANOPY ELEVATIONS
7. EXISTING/PROPOSED SIGN DRAWING
8. FLOOR PLAN (A1.1) (AHARONIAN)
9. BUILDING ELEVATIONS (A3.1) (AHARONIAN)
10. BUILDING ELEVATIONS (A3.2) (AHARONIAN)
11. LIFE SAFETY PLAN (LS1.1) (AHARONIAN)
- 1 OF 1 LIGHTING PLAN (LSI)



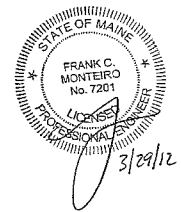
<p>44 Shiles Road, Suite One Salem, New Hampshire 03079 (603) 893-0720 ENGINEERS • PLANNERS • SURVEYORS www.mhfdesign.com</p>	<p>31,972 SQUARE FEET</p>
	<p>0.734 ACRES</p>
<p>801 Washington Ave. & Ocean Ave. PORTLAND, MAINE</p>	<p>V# 1079</p>
<p>Store# 5604</p>	<p>Gas Station# 851839</p>
<p> CUMBERLAND FARMS INC. 100 CROSSING BLVD. FRAMINGHAM, MA 01702</p>	<p>SCALE: NTS DATE: OCTOBER 5, 2009 FILE: 2666CVR.dwg DRAWN BY: MHF CHECKED BY:</p>
<p>TITLE SHEET</p>	

REVISIONS		
DATE	REV. BY.	DESCRIPTION
3/17/11	CMT	ADD SHT 5A, REV SHTS 1, 3-11
9/6/11	CMT	REV. SHEETS 3-5
3/29/12	CMT	REV. SHEETS 3-5A, 9 & LSI

Attachment C.3



- NOTES:**
- EXISTING CONDITIONS AS SHOWN IS A COMPILATION OF ON THE GROUND SURVEY PREPARED BY MHF DESIGN CONSULTANTS, INC. AND INFORMATION TAKEN FROM PLAN REFERENCE #1.
 - A DEMOLITION PERMIT MUST BE OBTAINED FROM THE CITY OF PORTLAND PRIOR TO COMMENCEMENT OF WORK. ANY APPLICABLE UTILITY DISCONNECTIONS MUST BE COORDINATED WITH RESPECTIVE UTILITY COMPANIES.
 - ALL DEMOLITION ACTIVITIES ARE TO BE PERFORMED IN STRICT ADHERENCE TO ALL FEDERAL, STATE AND LOCAL REGULATIONS. CONTRACTOR TO INSTALL EROSION CONTROL DEVICES AS REQUIRED PRIOR TO BEGINNING DEMOLITION ACTIVITIES.
 - CONDUCT ALL DEMOLITION OPERATIONS IN A MANNER THAT WILL PREVENT INJURY, DAMAGE TO STRUCTURES, ADJACENT BUILDINGS AND ALL PERSONS.
 - REFRAIN FROM USING EXPLOSIVES WITHOUT PRIOR WRITTEN CONSENT OF THE DEVELOPER AND APPLICABLE GOVERNMENTAL AUTHORITIES.
 - CONDUCT DEMOLITION SERVICES IN SUCH A MANNER TO INSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, WALKS AND OTHER ADJACENT FACILITIES. DO NOT CLOSE OR OBSTRUCT STREETS, WALKS OR OTHER OCCUPIED FACILITIES WITHOUT PRIOR WRITTEN PERMISSION OF THE DEVELOPER AND APPLICABLE GOVERNMENTAL AUTHORITIES. PROVIDE ALTERNATIVE ROUTES AROUND CLOSED OR OBSTRUCTED TRAFFIC WAYS IF REQUIRED BY APPLICABLE GOVERNMENTAL REGULATIONS.
 - USE WATERING, TEMPORARY ENCLOSURES AND OTHER SUITABLE METHODS, AS NECESSARY TO LIMIT THE AMOUNT OF DUST AND DIRT RISING AND SCATTERING IN THE AIR. CLEAN ADJACENT STRUCTURE AND IMPROVEMENTS OF ALL DUST AND DEBRIS CAUSED BY THE DEMOLITION OPERATIONS. RETURN ALL ADJACENT AREAS TO THE CONDITIONS EXISTING PRIOR TO THE START OF WORK.
 - ACCOMPLISH AND PERFORM THE DEMOLITION IN SUCH A MANNER AS TO PREVENT THE UNAUTHORIZED ENTRY OF PERSONS AT ANY TIME.
 - COMPLETELY FILL BELOW GRADE AREAS AND VOIDS RESULTING FROM THE DEMOLITION OF STRUCTURES AND FOUNDATIONS WITH SOIL MATERIALS CONSISTING OF STONE, GRAVEL AND SAND, FREE FROM DEBRIS, TRASH, FROZEN MATERIALS, ROOTS AND OTHER ORGANIC MATTER. STONES USED WILL NOT BE LARGER THAN 6 INCHES IN DIMENSION. MATERIAL FROM DEMOLITION MAY NOT BE USED AS FILL. PRIOR TO PLACEMENT OF FILL MATERIALS, UNDERTAKE ALL NECESSARY ACTION IN ORDER TO INSURE THAT AREAS TO BE FILLED ARE FREE OF STANDING WATER, FROZEN MATERIAL, TRASH, DEBRIS. PLACE FILL MATERIALS LAYERS NOT EXCEEDING 6 INCHES IN LOOSE DEPTH AND COMPACT EACH LAYER AT PLACEMENT TO 95% OPTIMUM DENSITY. GRADE SURFACE TO MEET ADJACENT CONTOURS AND TO PROVIDE SURFACE DRAINAGE.
 - REMOVE FROM THE DESIGNATED SITE, AT THE EARLIEST POSSIBLE TIME, ALL DEBRIS RUBBISH, SALVAGEABLE ITEMS, HAZARDOUS AND COMBUSTIBLE SERVICES. REMOVED MATERIALS MAY NOT BE STORED, SOLD OR BURNED ON SITE. REMOVAL OF HAZARDOUS AND COMBUSTIBLE MATERIALS SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE PROCEDURES AS AUTHORIZED BY THE FIRE DEPARTMENT OR OTHER APPROPRIATE REGULATORY AGENCIES AND DEPARTMENTS.
 - PROTECT EXISTING DRAINAGE SYSTEM(S) AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING DURING CONSTRUCTION.
 - THE LIMITS OF WORK SHALL BE CLEARLY MARKED IN THE FIELD PRIOR TO THE START OF CONSTRUCTION OR SITE CLEARING.
 - IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO NOTIFY "DIG SAFE" (1-888-344-7233) 72 HOURS PRIOR TO ANY EXCAVATION ON THIS SITE. CONTRACTOR SHALL ALSO NOTIFY LOCAL WATER DEPARTMENT TO MARK OUT THEIR UTILITIES.
 - NOTES ON THIS PLAN THAT READ "TBR" REPRESENT FEATURES TO BE REMOVED. ANY FEATURES NOT LABELED "TBR" OR "TO BE REMOVED" SHALL BE CONSIDERED EXISTING TO REMAIN.
 - ANY EXISTING IRRIGATION SYSTEM TO BE PROTECTED DURING DEMOLITION AND CONSTRUCTION. EXISTING IRRIGATION SYSTEM TO BE RELOCATED, EXTENDED AND CONNECTED TO ALL PROPOSED LANDSCAPED AREAS AS NECESSARY.



PLAN REFERENCES:

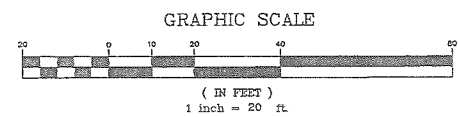
- EXISTING CONDITIONS PLAN OF CUMBERLAND FARMS STORE, PREPARED FOR V.S.H. REALTY, INC.; PREPARED BY MAINE SURVEY CONSULTANTS, INC.; SCALE: 1"=20'; DATE: APRIL 1999

ABUTTER ACROSS INTERSECTION
 TAX MAP 163 BLOCK E LOT 6
 N/F LAURA ANGELONE
 28 WHALEBOAT ROAD
 PORTLAND, ME 04103

TAX MAP 163 BLOCK A LOT 8
 N/F ANGIES SERVICE INC.
 10 E. BRIDGE STREET
 WESTBROOK, ME 04092

LEGEND

0	IRON PIN FOUND/SET	U	UTILITY POLE
SIGN	SEWER MANHOLE	⊙	TELEPHONE MANHOLE
OBSERVATION WELL	⊙	⊕	WATER VALVE
TREE	TBR		TO BE REMOVED
LIMIT OF EXISTING PAVEMENT TO BE REMOVED			



REVISIONS			31,972 SQUARE FEET
DATE	REV. BY	DESCRIPTION	0.734 ACRES
3/17/11	CMT	MISC. REVISIONS	V# 1079
9/6/11	CMT	REV.BLDG FOOTPRINT, MISC. REVS.	Store# 5604
3/29/11	CMT	ADDRESS CITY COMMENTS	Gas Station# 851839

MHF Design Consultants, Inc.
 44 Siles Road, Suite One
 Salem, New Hampshire 03079
 (603) 893-0720
 ENGINEERS • PLANNERS • SURVEYORS
 www.mhfdesign.com

PORTLAND, MAINE

801 Washington Ave. & Ocean Ave.

Cumberland Farms Inc.
 100 CROSSING BLVD.
 FRAMINGHAM, MA 01702

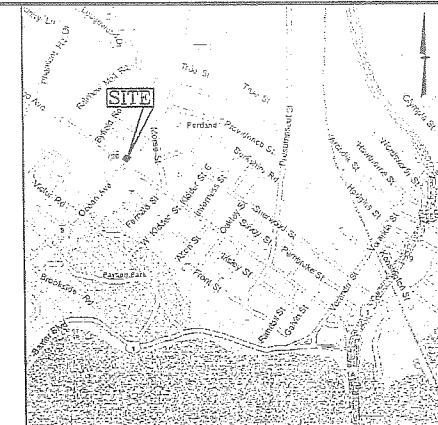
DATE: OCTOBER 5, 2009
 FILE: 2666SP.dwg
 DRAWN BY: MHF
 CHECKED BY:

DEMOLITION PLAN

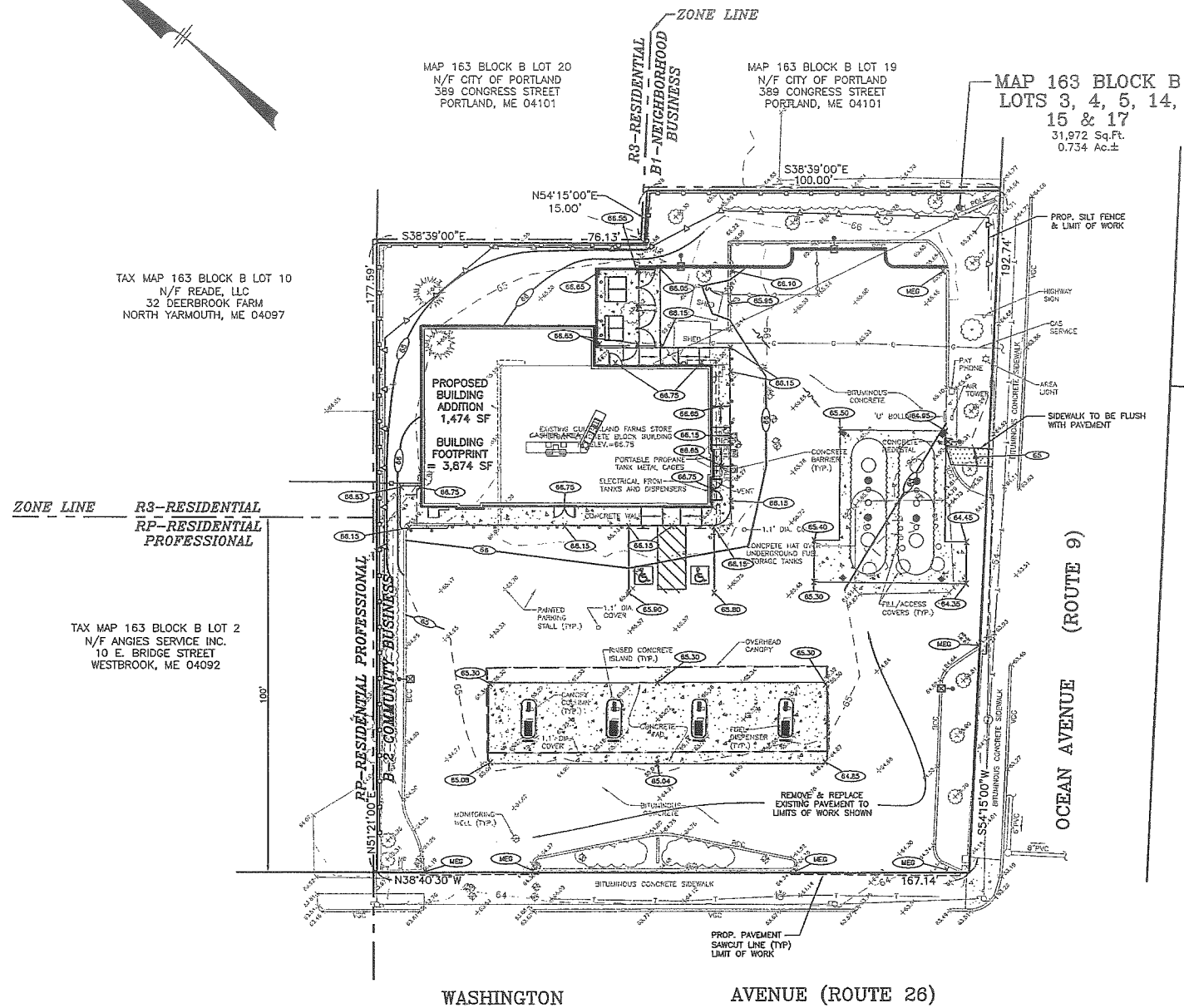
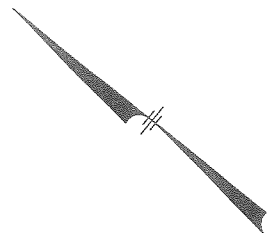


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Attachment C.5

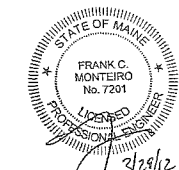


LOCATION MAP
(NOT TO SCALE)



NOTES:

- 1) ALL SITE DRAINAGE PIPE SHALL BE CORRUGATED HIGH-DENSITY POLYETHYLENE PIPE WITH STANDARD JOINTS, DUAL-WALL, SMOOTH INTERIOR AS MANUFACTURED BY ADS, INC., OR APPROVED EQUAL, UNLESS OTHERWISE NOTED ON PLAN.
- 2) ALL ROOF AND CANOPY DRAIN PIPE SHALL BE 6" PVC (SDR-35), EXCEPT WITHIN 10 FEET OF A BUILDING FOUNDATION WHERE CAST IRON PIPE SHALL BE USED, MIN. SLOPE=2.0%.
- 3) ALL SANITARY SEWER PIPE SHALL BE PVC (SDR-35), UNLESS OTHERWISE NOTED.
- 4) ALL WATER PIPE SHALL BE COPPER, UNLESS OTHERWISE NOTED.
- 5) ELEVATIONS ARE BASED ON NGVD 1929 DATUM.
- 6) ALL PROPOSED ELEVATIONS AS SHOWN ARE BOTTOM OF CURB ELEVATIONS, UNLESS OTHERWISE NOTED.
- 7) ANY UTILITY FIELD ADJUSTMENTS SHALL BE APPROVED BY THE LOCAL AUTHORITIES AND THE DEVELOPER PRIOR TO INSTALLATION.
- 8) THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR IS TO VERIFY EXACT LOCATION PRIOR TO CONSTRUCTION. THE CONTRACTOR IS TO NOTIFY THE DESIGN ENGINEER OF ANY DISCREPANCIES. CONSTRUCTION SHALL COMMENCE BEGINNING AT THE LOWEST INVERT (POINT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACE POINTS (CROSSINGS) WITH EXISTING UNDERGROUND INSTALLATIONS SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- 9) ALL CONSTRUCTION SHALL CONFORM TO MUNICIPAL DPW AND ALL APPLICABLE STATE AND FEDERAL STANDARDS.
- 10) THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIG-SAFE (1-888-344-7233) PRIOR TO COMMENCING ANY EXCAVATION.
- 11) ALL WATER AND SEWER CONSTRUCTION SHALL CONFORM TO DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS.
- 12) ALL ELECTRIC, TELEPHONE AND CABLE TV LINES ARE TO BE UNDERGROUND AND INSTALLED IN CONFORMANCE WITH APPLICABLE UTILITY CO. SPECIFICATIONS.
- 13) THIS SITE WILL NOT REQUIRE A USEPA NPDES PERMIT FOR STORMWATER DISCHARGE FOR THE SITE CONSTRUCTION SINCE THE DISTURBANCE DOES NOT EXCEED ONE ACRE.
- 14) THE CONTRACTOR IS TO COORDINATE WITH THE MUNICIPAL DPW REGARDING WATER PRESSURE AT SERVICE. THE CONTRACTOR IS TO VERIFY IF PRESSURE REDUCING VALVE IS REQUIRED.
- 15) ANY UTILITIES TO BE TAKEN OUT OF SERVICE SHALL BE DISCONNECTED AS DIRECTED BY UTILITY COMPANY AND LOCAL DPW.
- 16) ALL TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION SIGNAGE ARRANGEMENTS, ACCEPTABLE TO MDOT AND PORTLAND DEPARTMENT OF PUBLIC WORKS, SHALL BE EMPLOYED DURING OPERATIONS WITHIN THE PUBLIC RIGHT-OF-WAY.
- 17) ALL ADA ACCESSIBLE WALKWAYS CANNOT EXCEED 5% RUNNING SLOPE AND 2% CROSS SLOPE, RAMP CANNOT EXCEED 8.33% RUNNING SLOPE AND 2% CROSS SLOPE, AND HOV PARKING STALLS AND ACCESS AISLES CANNOT EXCEED 2% SLOPE IN ANY DIRECTION. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES.



PLAN REFERENCES:

- 1) EXISTING CONDITIONS PLAN OF CUMBERLAND FARMS STORE, PREPARED FOR V.S.H. REALTY, INC.; PREPARED BY MAINE SURVEY CONSULTANTS, INC.; SCALE 1"=20'; DATE: APRIL 1999



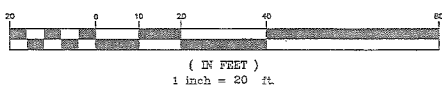
LEGEND

○	IRON PIN FOUND/SET	○	UTILITY POLE
□	SIGN	⊙	SEWER MANHOLE
○	OBSERVATION WELL	⊙	TELEPHONE MANHOLE
○	TREE	⊕	WATER VALVE
—	CONTOUR ELEVATION	C.O.	PROP. CLEANOUT
—	MEET EXISTING GRACE	CB-1	PROP. CATCH BASIN
—	PROP. SPOT ELEVATION	DMH-1	PROP. DRAIN MANHOLE
—	PROP. CONTOUR ELEVATION	SMH-1	PROP. SEWER MANHOLE
		—	PROP. GATE VALVE

TAX MAP 163 BLOCK A LOT 8
N/F ANGIES SERVICE INC.
10 E BRIDGE STREET
WESTBROOK, ME 04092

ABUTTER ACROSS INTERSECTION
TAX MAP 163 BLOCK E LOT 6
N/F LAURA ANGELONE
28 WHALEBOAT ROAD
PORTLAND, ME 04103

GRAPHIC SCALE

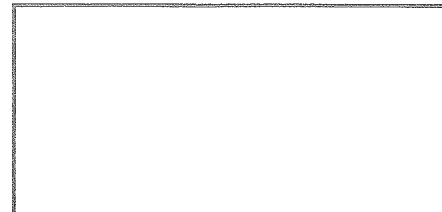


REVISIONS		
DATE	REV. BY.	DESCRIPTION
9/9/11	CMT	REV.BLDG FOOTPRINT, MISC. REVS.
3/29/11	CMT	ADDRESS CITY COMMENTS

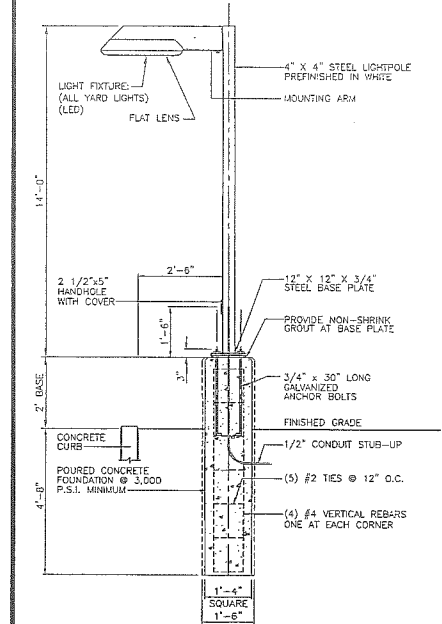
31,972 SQUARE FEET
0.734 ACRES
V# 1079
Store# 5604
Gas Station# 851839

MHF Design Consultants, Inc.
801 Washington Ave. & Ocean Ave.
PORTLAND, MAINE
SCALE: 1"= 20'
DATE: JANUARY XX, 2010
FILE: 2666SP.dwg
DRAWN BY: MHF
CHECKED BY:

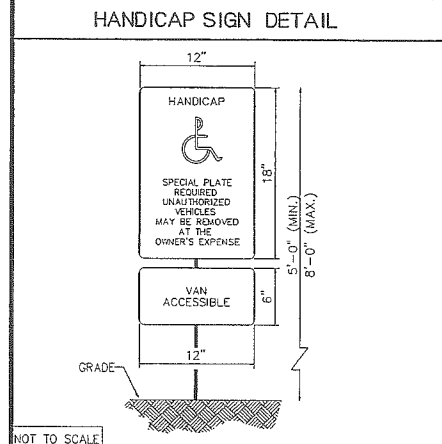
Att. C.7



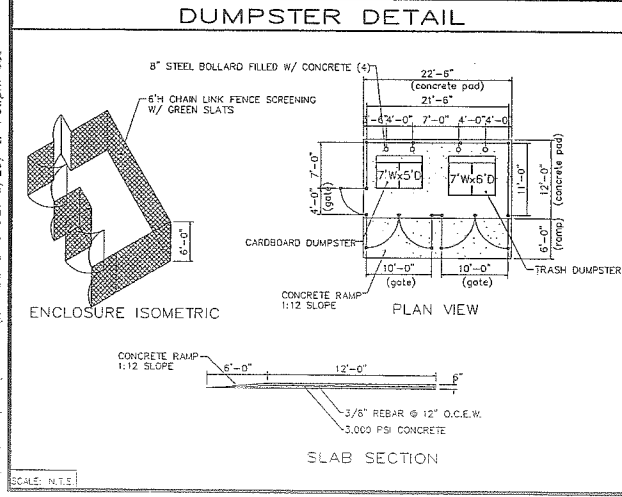
LIGHT POLE DETAIL



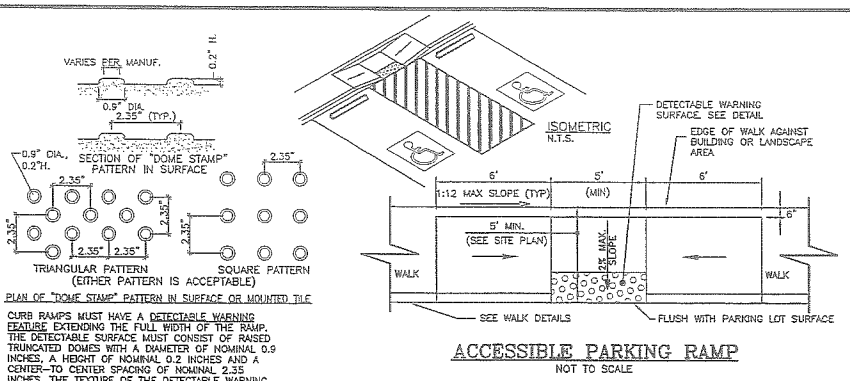
LED CROSSOVER AREA LIGHT BY LSI
IESNA FULL CUTOFF



HANDICAP SIGN DETAIL



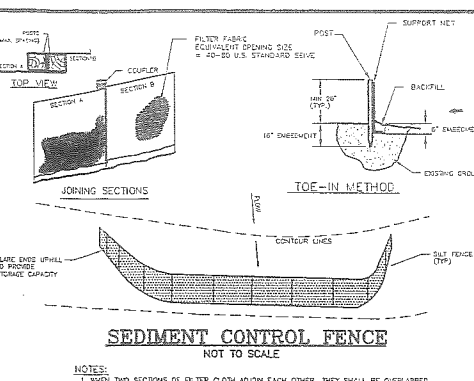
DUMPSTER DETAIL



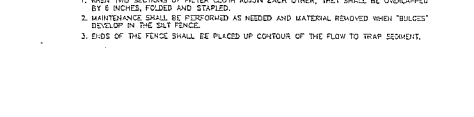
ACCESSIBLE PARKING RAMP
NOT TO SCALE



DETECTABLE WARNING SURFACE
NOT TO SCALE

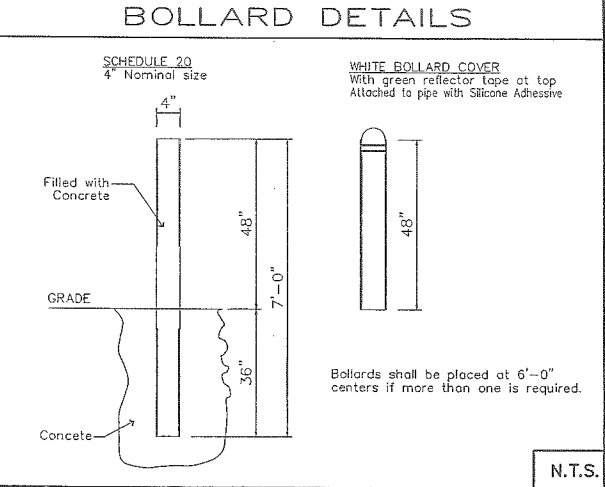


SEDIMENT CONTROL FENCE
NOT TO SCALE

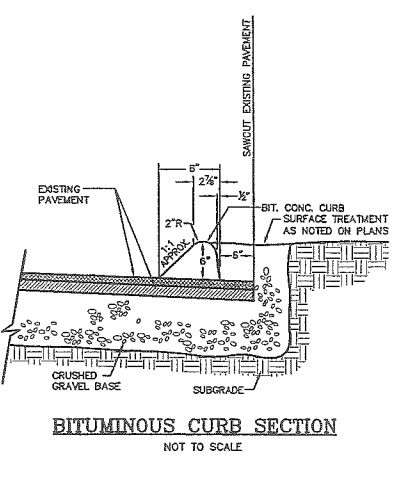


SILTSACK DETAIL-ON OR OFF SITE
NOT TO SCALE

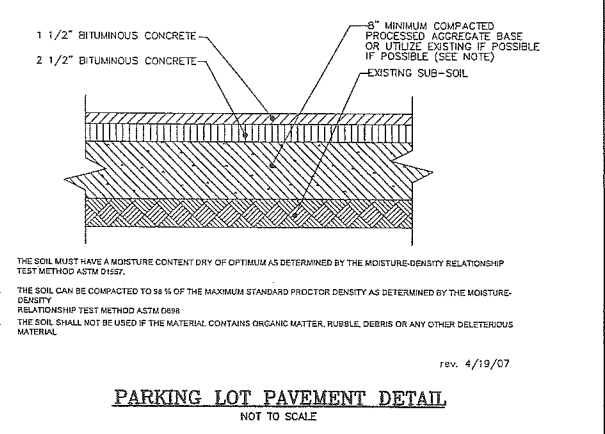
- CONSTRUCTION SEQUENCE:**
1. INSTALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES AS REQUIRED.
 2. CUT AND STUMP AREAS OF PROPOSED CONSTRUCTION.
 3. REMOVE AND STOCKPILE TOPSOIL. STOCKPILE SHALL BE SEEDED TO PREVENT EROSION.
 4. PROTECT CULVERT INLETS AND CATCH BASINS WITH SEDIMENTATION BARRIERS.
 5. PERFORM SITE GRADING, PLACING SILTATION FENCES AS REQUIRED TO CONTROL SOIL EROSION.
 6. INSTALL UNDERGROUND UTILITIES.
 7. BEGIN TEMPORARY AND PERMANENT SEEDING AND MULCHING. ALL CUT AND FILL SLOPES SHALL BE SEEDED OR MULCHED IMMEDIATELY AFTER THEIR CONSTRUCTION.
 8. DAILY, OR AS REQUIRED, CONSTRUCT, INSPECT, AND IF NECESSARY, RECONSTRUCT TEMPORARY BERMS, DRAINS, DITCHES, SILT FENCES AND SEDIMENT TRAPS INCLUDING MULCHING AND SEEDING. REFER TO OPERATION AND MAINTENANCE PLAN FOR ADDITIONAL REQUIREMENTS AND INFORMATION. COPIES OF ALL INSPECTION REPORTS ARE TO BE PROVIDED TO THE CONSERVATION COMMISSION DURING CONSTRUCTION AND AVAILABLE UPON REQUEST AFTER CONSTRUCTION IS COMPLETED.
 9. BEGIN EXCAVATION FOR AND CONSTRUCTION OF BUILDINGS.
 10. FINISH PAVING ALL DRIVES AND PARKING AREAS.
 11. COMPLETE PERMANENT SEEDING AND LANDSCAPING.
 12. AFTER GRASS HAS BEEN FULLY GERMINATED IN ALL SEEDED AREAS, REMOVE ALL TEMPORARY EROSION CONTROL MEASURES.



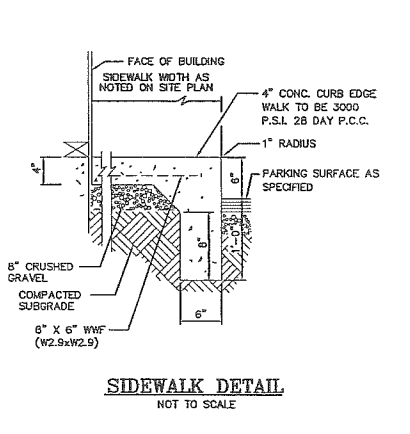
BOLLARD DETAILS



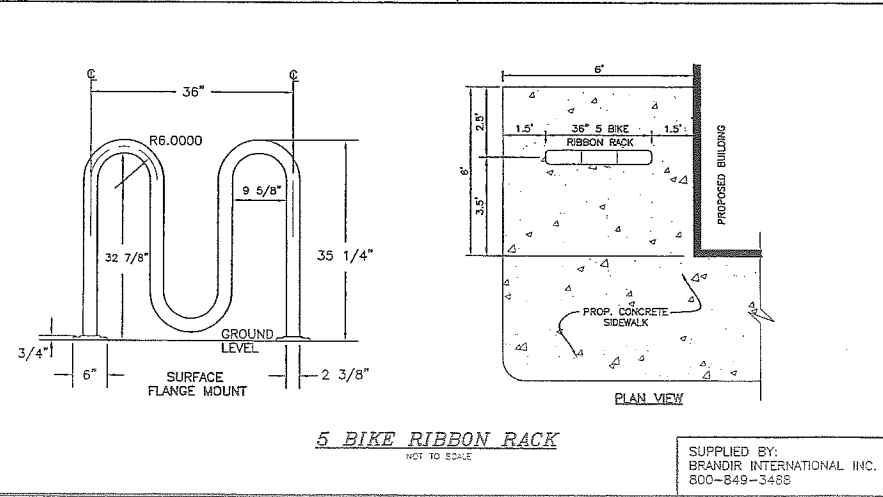
BITUMINOUS CURB SECTION
NOT TO SCALE



PARKING LOT PAVEMENT DETAIL
NOT TO SCALE



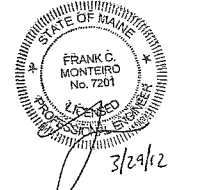
SIDEWALK DETAIL
NOT TO SCALE



5 BIKE RIBBON RACK
NOT TO SCALE

- TEMPORARY EROSION CONTROL MEASURES:**
1. SEDIMENT TRAPS SHALL BE INSTALLED AS REQUIRED. BARRIERS AND TRAPS ARE TO BE MAINTAINED AND CLEANED UNTIL ALL SLOPES HAVE A HEALTHY STAND OF GRASS.
 2. MULCH SHALL BE MOWINGS OF ACCEPTABLE HERBACEOUS GROWTH, FREE FROM NOXIOUS WEEDS OR WOODY STEMS, AND SHALL BE DRY. NO SALT HAY SHALL BE USED.
 3. FILL MATERIAL SHALL BE FREE FROM STUMPS, WOOD, ROOTS, ETC.
 4. STOCKPILED MATERIALS SHALL BE PLACED ONLY IN AREAS SHOWN ON THE PLANS. STOCKPILES SHALL BE PROTECTED BY SILTATION FENCE AND SEEDED TO PREVENT EROSION. THESE MEASURES SHALL REMAIN UNTIL ALL MATERIAL HAS BEEN PLACED OR DISPOSED OFF SITE.
 5. ALL DISTURBED AREAS SHALL BE LOAMED AND SEEDED. A MINIMUM OF 6 INCHES OF LOAM SHALL BE INSTALLED WITH NOT LESS THAN ONE POUND OF SEED PER 50 SQUARE YARDS OF AREA.
 6. SEED MIX SHALL BE EQUAL PARTS OF RED FESCUE (CREEPING), KENTUCKY BLUEGRASS, REDTOP, PERENNIAL RYEGRASS.
 7. AFTER ALL DISTURBED AREAS HAVE BEEN STABILIZED THE TEMPORARY EROSION CONTROL MEASURES ARE TO BE REMOVED.
 8. PAVED ROADWAYS AND PARKING LOTS MUST BE KEPT CLEAN AT ALL TIMES. PROVIDE SWEEPING ON A DAILY BASIS OR AS DIRECTED BY THE CITY OF LOWELL.
 9. ALL CATCH BASIN INLETS WILL BE PROTECTED WITH INLET PROTECTION - SEE DETAIL.

C:\p\m\m\k\2012\06663\2566DET.DWG DET 3/29/12 4:38pm ecc



REVISIONS			
DATE	REV. BY.	DESCRIPTION	
3/29/12	CMT	ADD DETAILS	

MHF Design Consultants, Inc.
801 Washington Ave. & Ocean Ave.
PORTLAND, MAINE

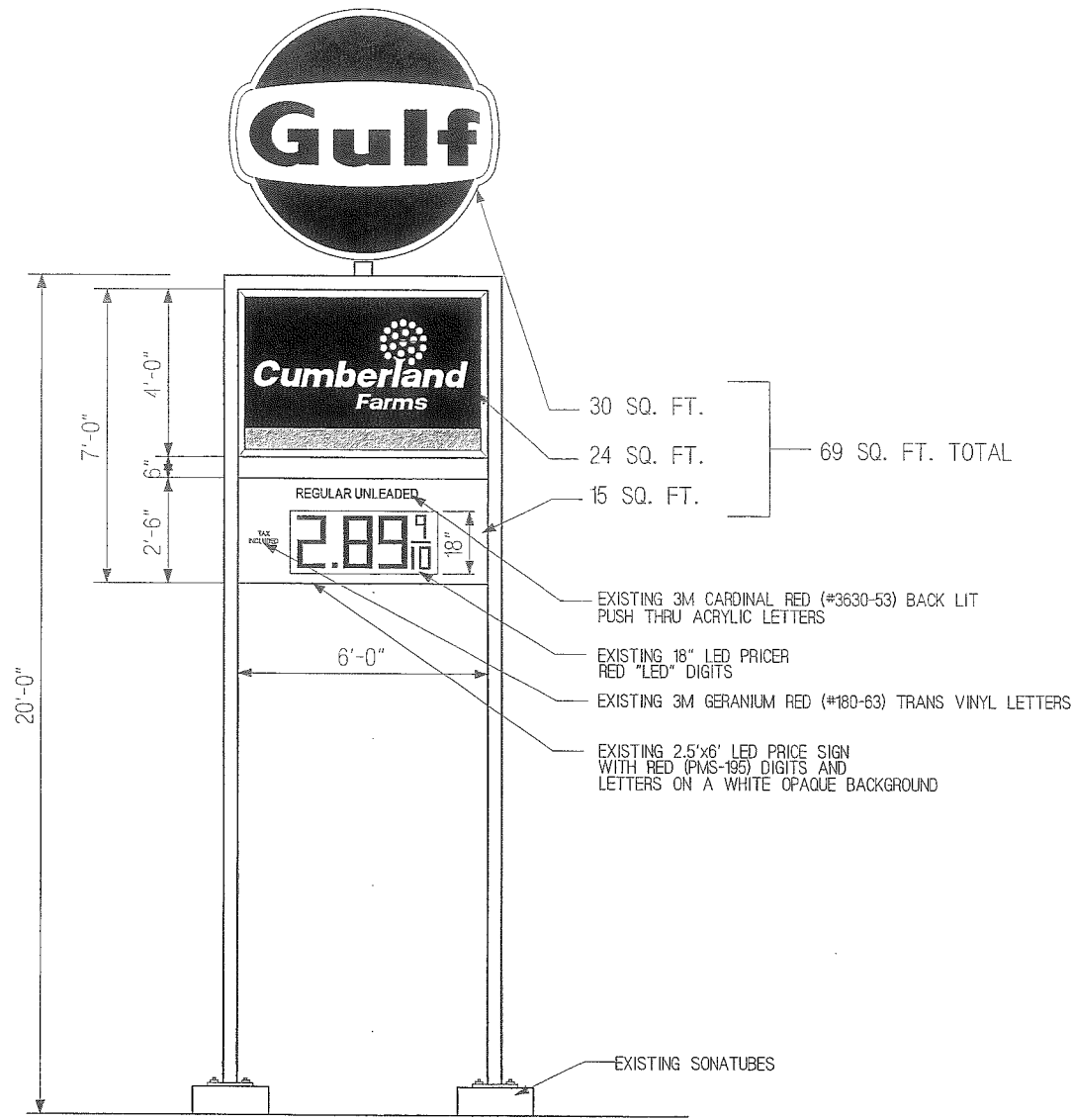
44 Siles Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
ENGINEERS • PLANNERS • SURVEYORS
www.mhfdesign.com

31,972 SQUARE FEET
0.734 ACRES
V# 1079
Store# 5604
Gas Station# 851839

SCALE: NTS
DATE: MARCH 17, 2011
FILE: 2666DET.dwg
DRAWN BY: MHF
CHECKED BY:

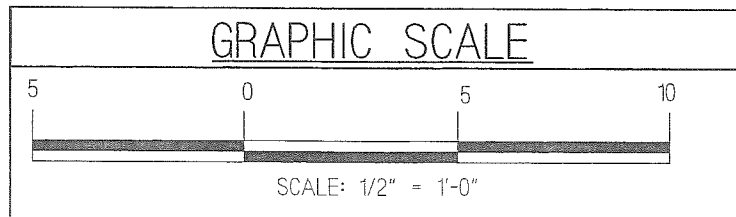
DETAIL SHEET

MHF PROJECT NO. 266609 SHEET 5A OF 11

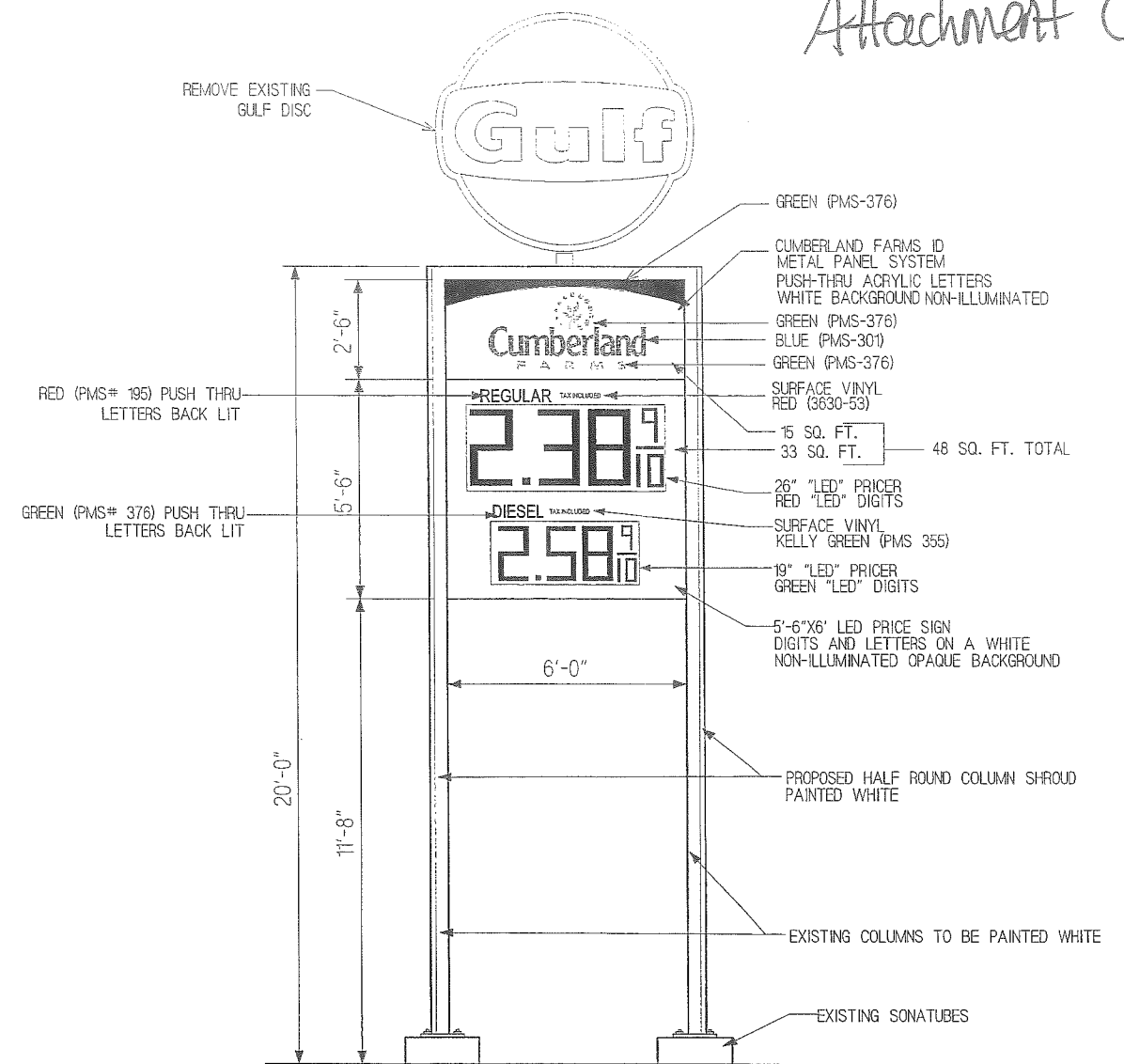


EXISTING ELEVATION

1/2" = 1'-0"

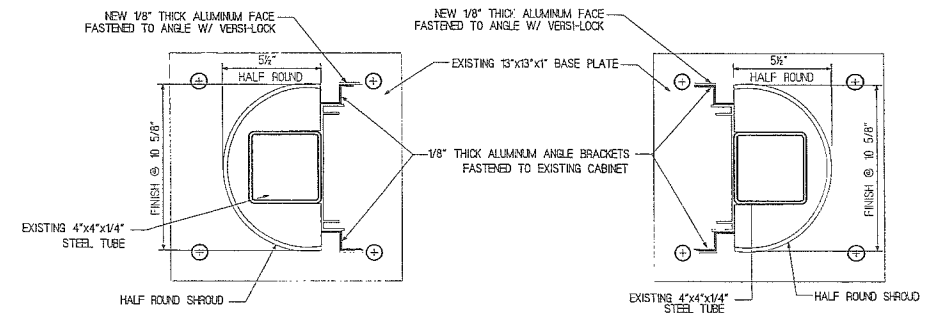


Attachment C-9



PROPOSED ELEVATION

1/2" = 1'-0"



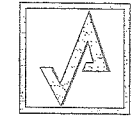
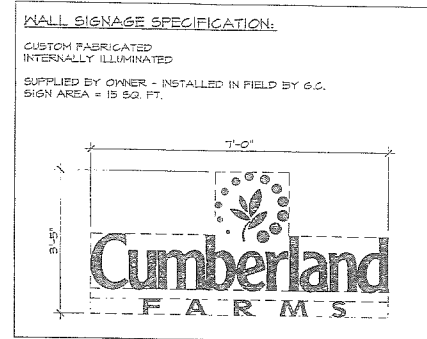
COLUMN SHROUD DETAIL

NOT TO SCALE

REVISIONS				V# 1079	801 WASHINGTON AVE & OCEAN AVE
DATE	REV. BY.	DESCRIPTION		Store# 5604	PORTLAND, MAINE
4-01-11	DR	added DIESEL LED		Facility# 180096	
7-26-11	DR	revised LED price sign			

Cumberland Farms, Inc. 100 Crossing Boulevard Franklin, Massachusetts 01862	SCALE: 1/2" = 1'-0" DATE: 2-1-11 FILE: SIGN01V0079.dwg DRAWN BY: RSP CHECKED BY:
---	--

Attachment C.11



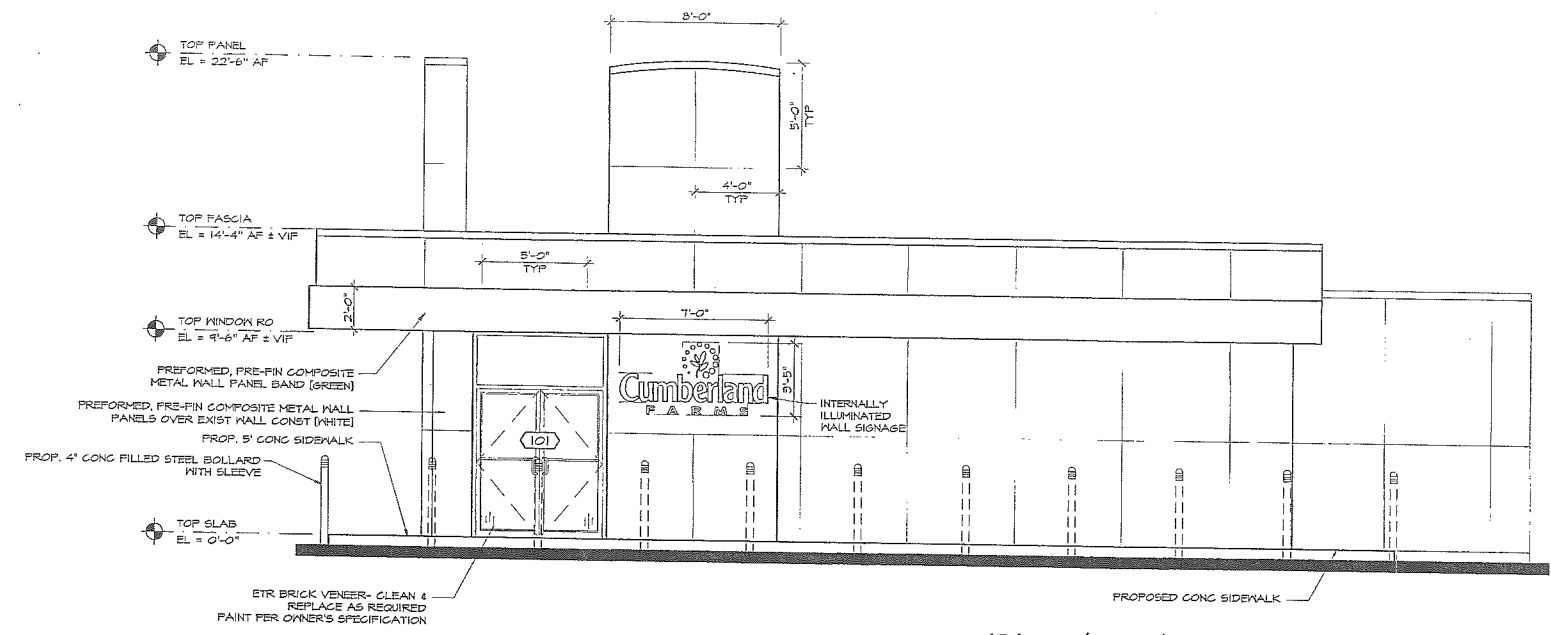
AHARONIAN & ASSOCIATES INC.
ARCHITECTS
310 George Washington Highway
Suite 100
Smithfield, Rhode Island
0 2 9 1 7
T 401-232-5010
F 401-232-5060
WWW.ARCH-ENG.COM

DISCLAIMER

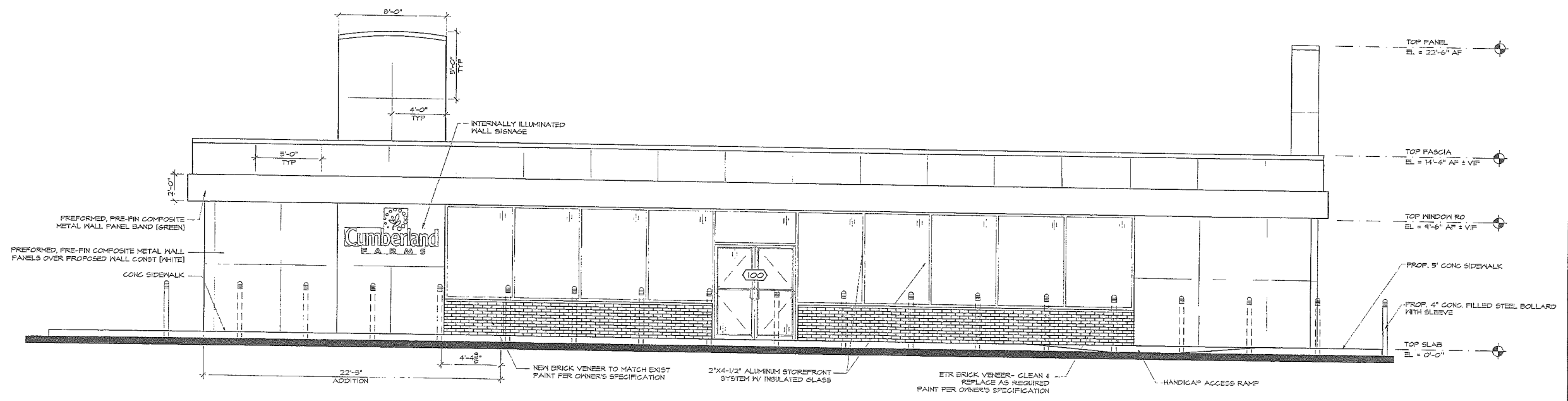
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REVISIONS

NUMBER	REMARKS	DATE
1	REVISED FLOOR PLAN	7-29-11
2	REVISED SIGNAGE	3-20-12



② EAST (SIDE) ELEVATION
SCALE: 1/4"=1'-0"



① SOUTH (FRONT) ELEVATION
SCALE: 1/4"=1'-0"

NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION

PRELIMINARY

PROJECT TITLE
Cumberland FARMS
100 Crossing Blvd, Framingham, Massachusetts
STORE # 5604
V1079
801 WASHINGTON AVE
PORTLAND, ME
Cumberland County

DRAWING TITLE
EXTERIOR ELEVATIONS
DATE: JULY 18, 2011
PROJ NO: 11112
DRAWN BY: SD/RSP
CHECKED BY: AZ

DRAWING NUMBER
A3.1

GENERAL BUILDING DATA
 THE FOLLOWING CODES WERE USED IN THE DESIGN OF THIS PROJECT:
 FEDERAL, STATE AND MUNICIPAL LAWS
 TOWN OF PORTLAND, MAINE RULES AND/OR REGULATIONS
 INTERNATIONAL BUILDING CODE 2009
 STATE OF MAINE PLUMBING CODE
 STATE OF MAINE ACCESSIBILITY CODE (ADAAS)
 NFPA I, NFPA 101, NFPA 72

ANY PREVAILING RULES, REGULATIONS PERTAINING TO ADEQUATE PROTECTION AND/OR GUARDING OF ANY MOVING PARTS OR OTHERWISE HAZARDOUS LOCATIONS.

OCCUPANCY CLASSIFICATION - 304
 MERCANTILE - GROUP M

ALLOWABLE HEIGHT AND BUILDING AREAS - TABLE 503

HEIGHT LIMITATION	BUILDING HEIGHT
1 STORY	1 STORY - 14'-4"

AREA LIMITATION
 EXISTING BUILDING AREA - TO REMAIN
 9,000 SQUARE FEET
 9,874 ± SF

TYPE OF CONSTRUCTION - CHAPTER 6
 CONSTRUCTION CLASSIFICATION
 TYPE 5B UNPROTECTED - COMBUSTIBLE

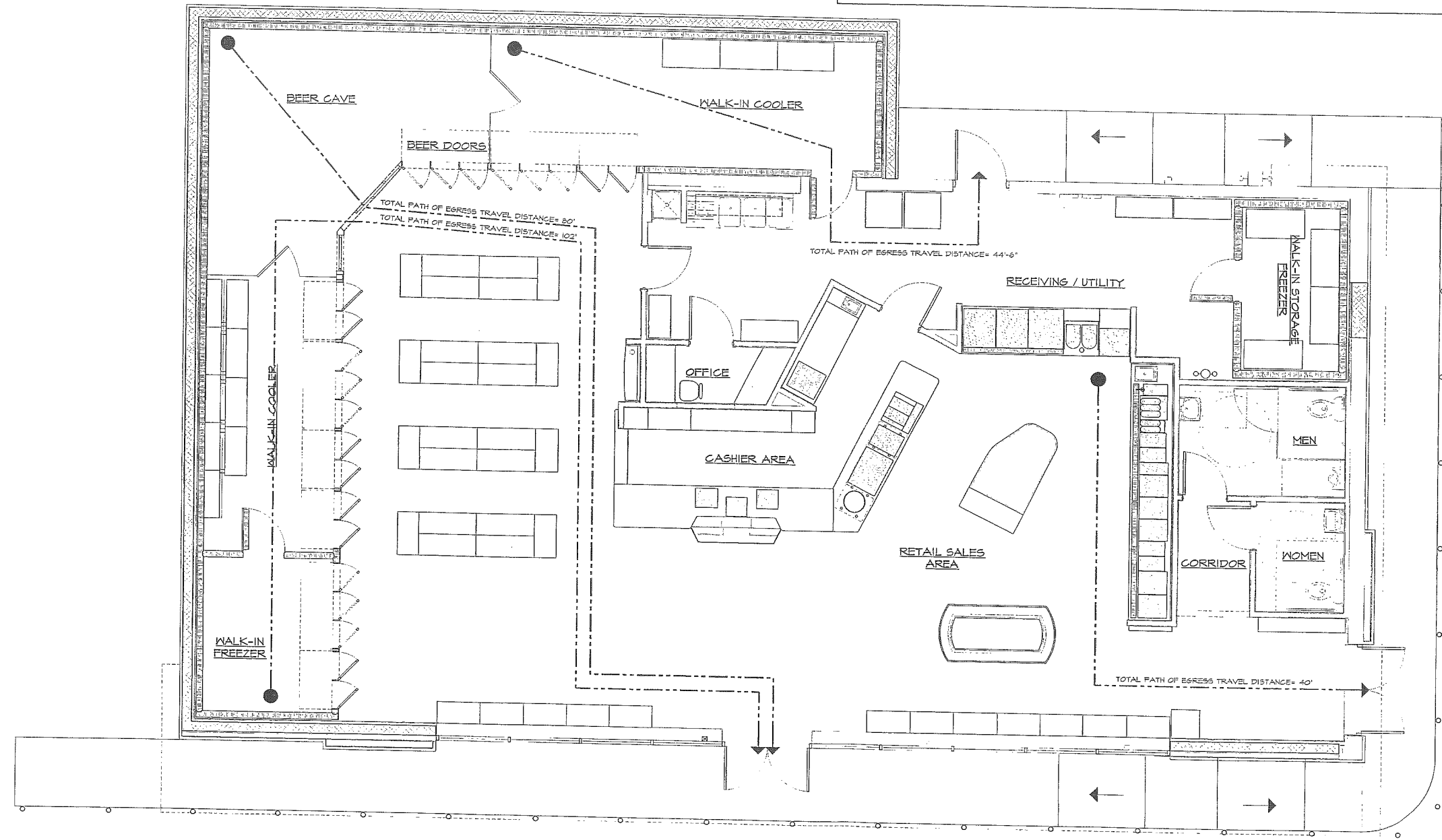
MAXIMUM FLOOR AREA ALLOWANCES PER OCCUPANT (TABLE 1004.1.2)

OCCUPANCY	AREA	MINIMUM OCCUPANT LOAD	OCCUPANTS
MERCANTILE AREA (PUBLIC)	1,750	50 SF/PERSON	58
UTILITY AREA (NON-PUBLIC)	2,124	500 SF/PERSON	7
TOTAL			65

EGRESS WIDTH PER OCCUPANT SERVED - TABLE 1005.1

OCCUPANCY	OCCUPANTS	WIDTH PER PERSON	WIDTH REQUIRED	WIDTH PROVIDED
MERCANTILE AREA (PUBLIC)	58	2"	11.6"	144"
UTILITY AREA (NON-PUBLIC)	7	2"	1.4"	42"

EXIT ACCESS TRAVEL DISTANCE - TABLE 1016.1
 15' MAXIMUM COMMON PATH OF EGRESS TRAVEL
 200' MAXIMUM (WITHOUT SPRINKLER SYSTEM)



NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION



AHARONIAN & ASSOCIATES INC.
 ARCHITECTS
 310 George Washington Highway
 Suite 100
 Smithfield, Rhode Island
 0 2 9 1 7
 T 401-333-5610
 F 401-232-5080
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REVISIONS

NUMBER	REMARKS	DATE
1	REVISED	7-29-11
	FLOOR PLAN	
1	CODE INFO	9-20-11

PRELIMINARY

PROJECT TITLE

Cumberland FARMS
 100 Crossing Blvd, Framingham, Massachusetts
 STORE # 5604
 V1079

801 WASHINGTON AVE
 PORTLAND, ME
 Cumberland County

DRAWING TITLE
 PROPOSED LIFE SAFETY PLAN

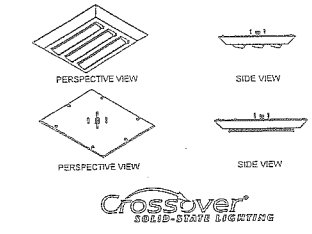
DATE JULY 18, 2011	PROJ NO 11112
DRAWN BY DH/DR	CHECKED BY AZ

DRAWING NUMBER
 LS1.1

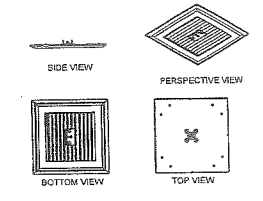
① LIFE SAFETY PLAN
 SCALE: 1/4"=1'-0"

CRO3-FO
LED Crossover Focus Light

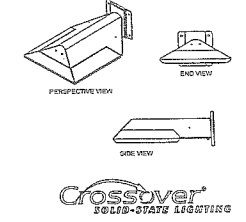
Attachment C.14



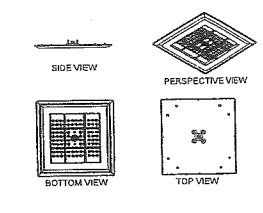
CRO3-A-LED-104
LED Crossover Asymmetric Canopy Light



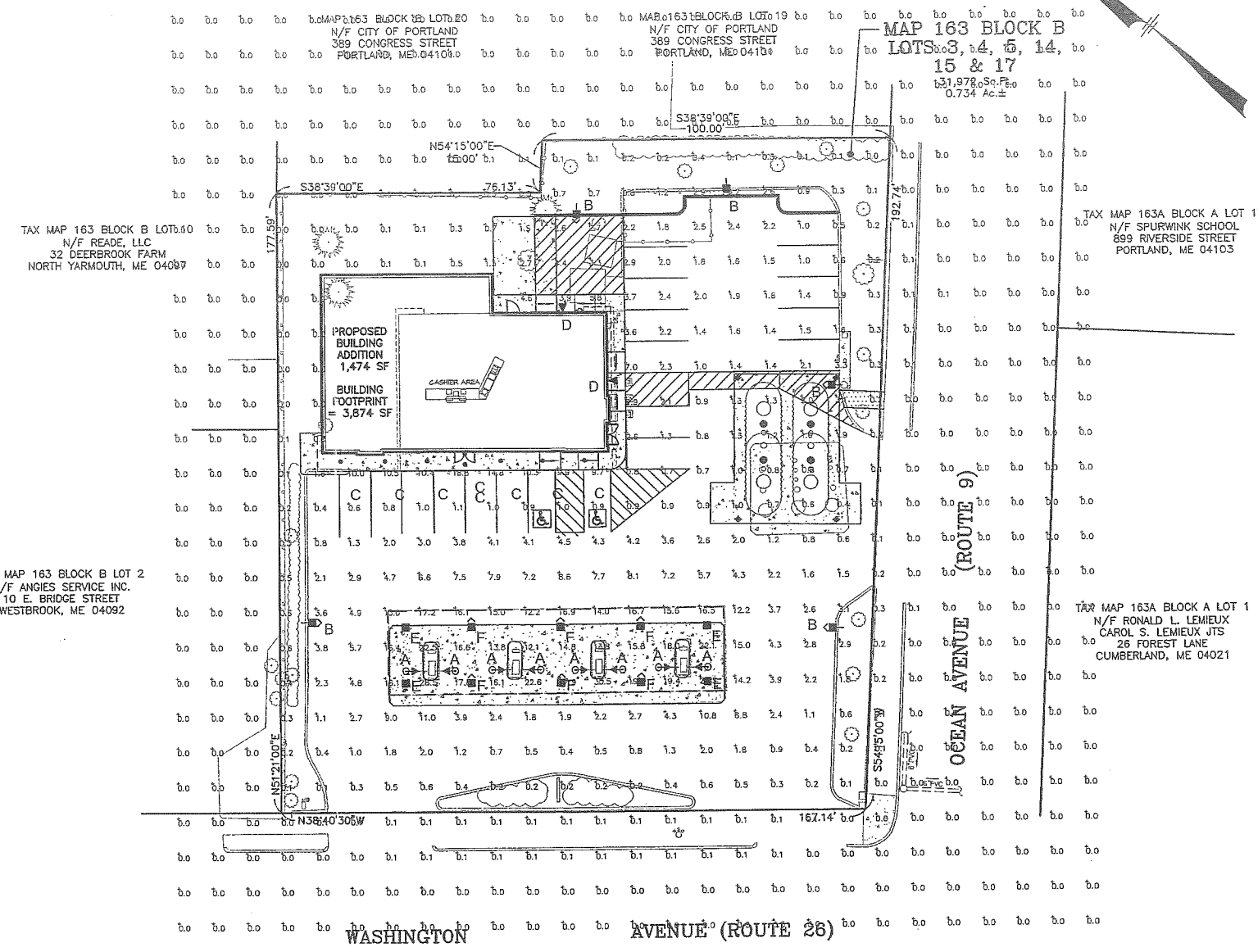
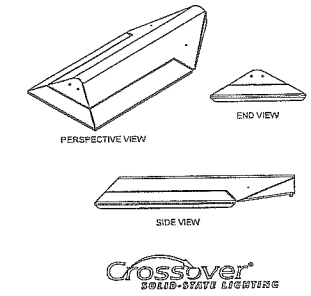
XAWS3
LED Crossover Wall Light



CRO3-ES-LED-104
LED Crossover Enhanced Symmetric Canopy Light



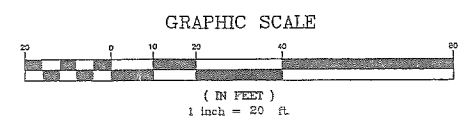
XAM3
LED Crossover Area Light



Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
ALL CALC POINTS	Illuminance	Fc	1.62	30.5	0.0	N.A.	N.A.
DISPENSER VERTICAL READINGS_1	Illuminance	Fc	50.25	73.9	24.7	2.03	2.99
APPROACH AND DRIVE SUMMARY	Illuminance	Fc	0.87	2.0	0.2	4.35	10.00
CANOPY SUMMARY	Illuminance	Fc	19.14	30.5	12.1	1.58	2.52
PARKING SUMMARY	Illuminance	Fc	1.45	2.5	0.4	3.63	6.25

Total Project Watts
Total Watts = 1816.2

Symbol	Qty	Label	Arrangement	Description	LLF	Lumens/Lamp	Arr. Lum. Lumens	Arr. Watts
■	6	F	SINGLE	CRO3-A-LED-64-350-CW-UE	1.000	N.A.	6872	76
■	4	E	SINGLE	CRO3-S-LED-64-350-CW-UE	1.000	N.A.	7088	75
■	2	D	SINGLE	XAWS3-3-LED-63-350-CW-UE	1.000	N.A.	4922	75
■	8	C	SINGLE	OMLED27U-R6LED30KMDCS	1.000	N.A.	1416	26.9
■	5	B	SINGLE	XAS3-FT-LED-63-350-CW-UE-HSSS SINGLE ON 14' POLE + 2' BASE	1.000	N.A.	3764	75
■	8	A	SINGLE	CRO3-FO-LED-30-CW-120-240V	1.000	3004	3004	40



Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine applicability of the layout to existing or future field conditions.

This lighting pattern represents illuminance levels calculated from laboratory data taken under controlled conditions utilizing current industry standard lamp ratings in accordance with Illuminating Engineering Society approved methods. Actual performance of any manufacturer's luminaire may vary due to variation in electrical voltage, tolerance in lamps and other variable field conditions. Calculations do not include obstructions such as buildings, curbs, landscaping, or any other architectural elements unless noted.

lel INDUSTRIES®
LIGHTING PROPOSAL FOR
CUMBERLAND FARMS
WASHINGTON AVE. & OCEAN
PORTLAND, ME

SCALE: 1/2"=1'-0"
DATE: 3/2/12
BY: LLC

LO- 105880-5

TRITON FOODMART / GAS STATION

1884 FOREST AVENUE
PORTLAND, MAINE

APPLICANT:

JOHN CHAU

75 ACADIA STREET

PORTLAND, MAINE 04103

ENGINEER / SURVEYOR:



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One Chapel Street 250 Goddard Rd - Suite B

Westbrook, ME 04095-1329 Lewiston, ME 04240

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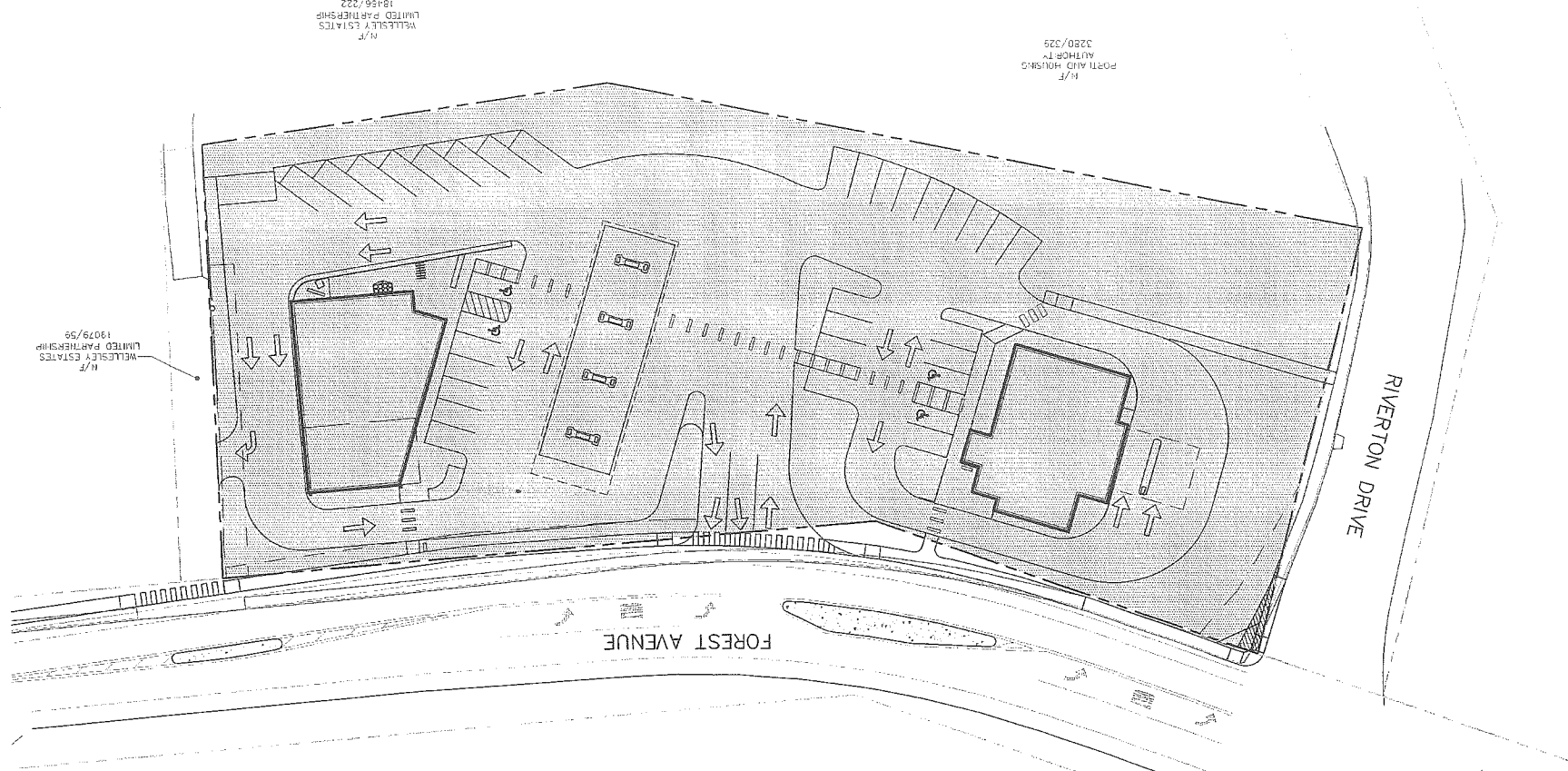
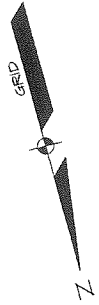
WWW.SEBAGOENGINEERS.COM

PROJECT ARCHITECT:

TFH ARCHITECTS

80 MIDDLE STREET

PORTLAND, MAINE

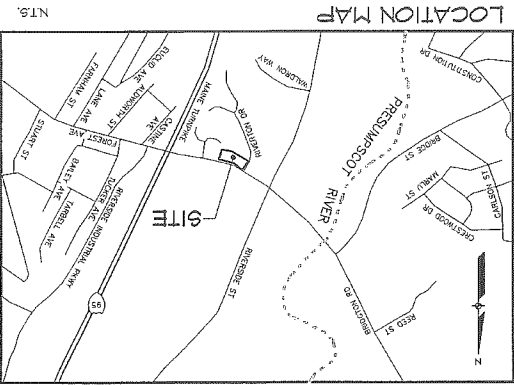


SCALE: 1" = 30'

SHEET INDEX:

SHEET DESCRIPTION

- | | |
|---|--------------------------|
| 1 | COVER SHEET |
| 2 | EXISTING CONDITIONS PLAN |
| 3 | SITE PLAN |
| 4 | GRADING AND UTILITY PLAN |
| 5 | LANDSCAPING PLAN |
| 6 | DETAILS |
| 7 | DETAILS |
| 8 | DETAILS |
| 9 | DETAILS |



Sebago Technics
Engineering Experts You Can Build On
One Colonial Street
Waterville, ME 05671
Tel (207) 853-8277
Fax (207) 853-8277
www.sebago-technics.com

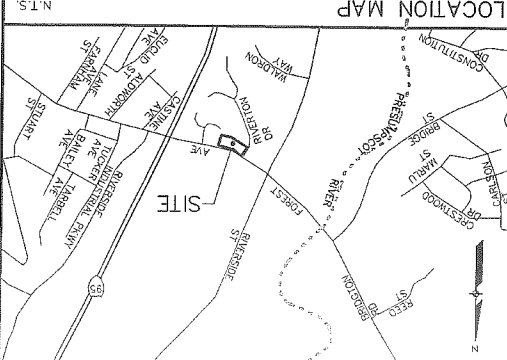
290 Colonial Road - Suite B
Lewiston, ME 04240
Tel (207) 753-5898

PROJECT NO. FIELD BOOK DESIGN CHKD DRAWN
11142

REV.	DATE	BY	STATUS	DESCRIPTION
D	03/19/12	JRS	REVISED PER FINAL PLAN COMMENTS	
C	03/10/12	JRS	SUBMITTED FOR FINAL PLAN REVIEW	
B	07/10/12	JRS	SUBMITTED FOR PRELIMINARY REVIEW	
A	07/19/11	JRS	SUBMITTED FOR PRELIMINARY REVIEW	
REV.	07/19/11	JRS	STATUS	

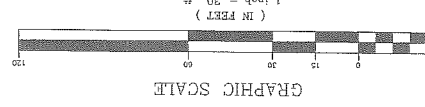
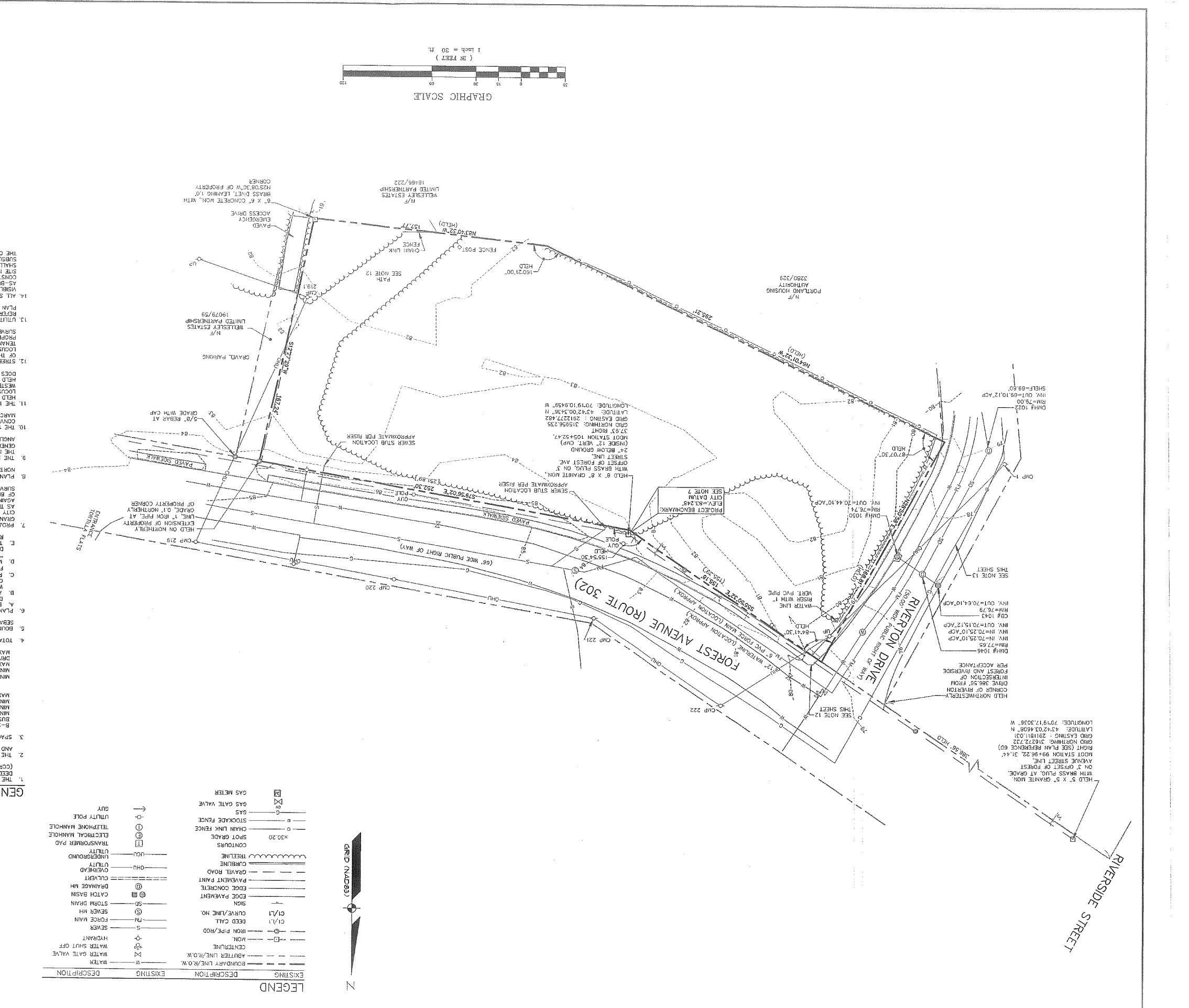
THIS PLAN SHALL NOT BE ADORPED WITHOUT WRITTEN PERMISSION FROM SEBAGO TECHNICS, INC. ANY ALTERATIONS, ADDITIONS OR OTHERWISSE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC.

WILIAM C. SHIPPER P.L.S. 22118
MAINE PROFESSIONAL LAND SURVEYOR
NO. 2118



GENERAL NOTES:

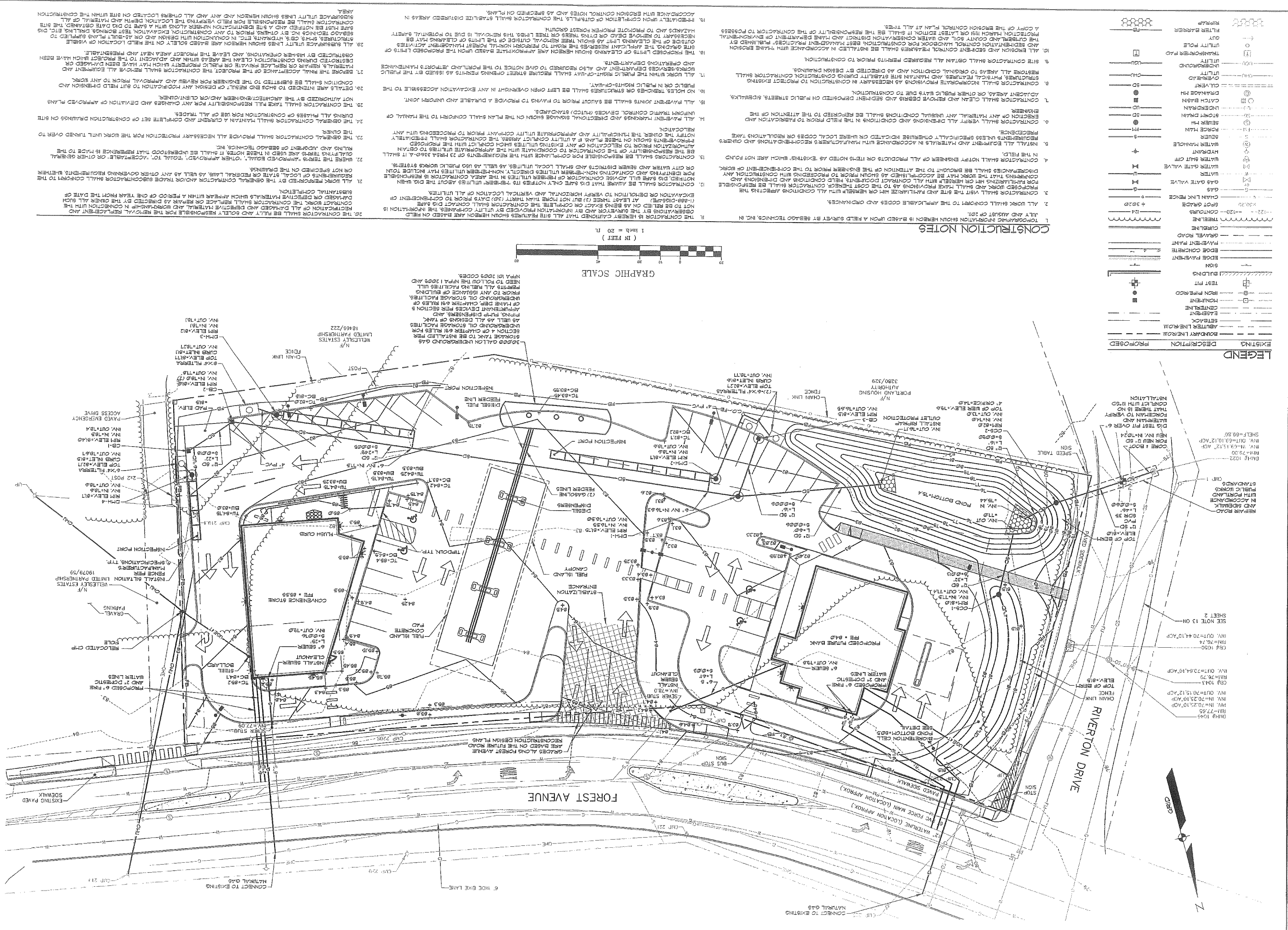
1. THE RECORD OWNER OF THE LOCUS PARCEL IS JOHN CHAU AND PHONG NEMANG BY WARRANTY DEED DATED JUNE 20, 2008 AND RECORDED AT THE CUMBERLAND COUNTY REGISTRY OF DEEDS (CGRD) ON JUNE 23, 2008 IN BOOK 25145, PAGE 266.
2. THE PROPERTY IS SHOWN AS LOT 3, BLOCK B, ON THE CITY OF PORTLAND ASSESSORS MAP 327 AND IS LOCATED IN THE B-2 ZONE.
3. SPACE AND BULK CRITERIA:
B-2 ZONE
BUSINESS AND NON-RESIDENTIAL USES
MIN. LOT SIZE: 10,000 S.F.
MIN. STREET FRONTAGE: 50 FEET
MIN. FRONT YARD: 0 FEET
MIN. SIDE YARD: 0 FEET
MIN. REAR YARD: 20 FEET (ABUTTING RESIDENTIAL USE)
MAX. BUILDING HEIGHT: 45 FEET
MAX. THROUGH SEPARATOR: 40 FEET
MAX. IMPERVIOUS SURFACE RATIO: 50%
4. TOTAL AREA OF PARCEL IS APPROXIMATELY 1.61 ACRES.
BOUNDARY AND TOPOGRAPHIC INFORMATION SHOWN HEREON IS BASED UPON A FIELD SURVEY BY SEBAGO TECHNICS DURING JULY AND AUGUST OF 2011 AND JANUARY OF 2012.
5. PLAN REFERENCES:
A. BLUE SHEETS 34 AND 35 OF FOREST AVENUE AND ON FILE WITH THE PORTLAND DEPARTMENT OF PUBLIC WORKS.
B. AMENDED SUBDIVISION PLAN ON FOREST AVENUE, PORTLAND, MAINE MADE FOR OWNER, WESTLEY ESTATES, LLC DATED OCTOBER 8, 2002 AND REVISED THROUGH 12-18-02 BY OWEN HASKELL, INC. AND RECORDED IN PLAN BOOK 202 PAGE 744 CORD.
C. PLAN OF RIVERTON DRIVE, BY THE CITY OF PORTLAND PUBLIC WORKS DEPARTMENT, AND ON FILE AS 493/9 WITH THE SAME.
D. MAINE DEPARTMENT OF TRANSPORTATION (MDOT), RIGHT OF WAY MAP FOR STATE HIGHWAY 147, US ROUTE 302, FOREST AVENUE, DATED MAY 2011, AND ON FILE WITH THE SAME AS D.O.T. FILE NO. 3-572.
E. TURNKEY FAMILY HOUSING, FOREST AVENUE, PORTLAND, MAINE, BY OWEN HASKELL, INC. REVISED THROUGH JUNE 8, 1971.
7. PROJECT ELEVATIONS SHOWN HEREON ARE BASED ON THE TOP OF THE BRASS PILING IN A GRANITE BENCHMARK LOCATED AT THE NORTHEASTLY CORNER OF THE LOCUS PROPERTY. THE GRANITE BENCHMARK IS APPROXIMATELY 10 FEET NORTH OF THE INTERSECTION OF FOREST AVENUE AND RIVERTON DRIVE. THE ELEVATION OF THIS BENCHMARK WAS CHECKED AGAINST THE ELEVATION FROM A SURVEY PERFORMED IN 2006. THE CORRELATION OF THIS BENCHMARK TO CITY DATUM IS HELD (SEE LEVEL BOOK ONE PAGE 100) AS THE DATUM AS SHOWN HEREON. THE ELEVATION OF THIS BENCHMARK WAS CHECKED AGAINST THE ELEVATION FROM A SURVEY PERFORMED IN 2006. THE CORRELATION OF THIS BENCHMARK TO CITY DATUM IS HELD (SEE LEVEL BOOK ONE PAGE 100) AS THE DATUM AS SHOWN HEREON.
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8. NORTH AMERICAN DATUM OF 1983 (NAD83) IS USED FOR ALL COORDINATE SYSTEM, WEST ZONE. THE STREET LINE OF FOREST AVENUE IS BASED ON HOWING THE THREE FOOT CORNER MON. AT THE INTERSECTION OF FOREST AND RIVERTON STREETS AND THE THREE FOOT CORNER MON. AT THE INTERSECTION OF FOREST AND RIVERSIDE STREETS AND THE THREE FOOT CORNER MON. AT ANGLE OF 155° 54' 30" PER PLAN REFERENCE BA.
9. THE 16 FOOT WIDE EASEMENT ACCESS EASEMENT SHOWN ON PLAN REFERENCE BA HAS BEEN COVERED IN FEET TO WESTLEY ESTATES LIMITED PARTNERSHIP PER WARRANTY DEED DATED MARCH 21, 2008 AND RECORDED AT CGRD ON MARCH 29, 2008 IN BOOK 19078, PAGE 99.
10. THE 1 INCH RAMP FOUND AT THE NORTHEASTLY CORNER OF THE LOCUS PROPERTY WAS FOUND ON THE RIVERVIEW DIVISION OF THE EASERS PROPERTY LINE AS CALLED FOR IN THE DOES NOT CALL FOR AN RAMP AS IT RUNS ALONG THE LOCUS PARCEL.
11. STREET IMPROVEMENTS (E.G. PAVED AND CURBED) ARE SHOWN AS THE NORTHEASTLY CORNER OF THE LOCUS PROPERTY AND ARE SHOWN HEREON. THE SOUTHWESTLY PORTION OF THE LOCUS PROPERTY CURRENTLY USED AS AN ACCESS TO AND FROM FOREST AVENUE BY THE TRAMWAYS OF THE PORTLAND HOUSING RECORD ASSESSMENTS MAY EXIST OVER THE LOCUS PROPERTY AS A FULL TITLE SEARCH WAS NOT PROVIDED OR PERFORMED AS A RESULT OF THIS SURVEY.
12. UTILITIES SHOWN IN RIVERTON DRIVE ARE DERIVED FROM FIELD LOCATIONS AND PLAN REFERENCE BA. UTILITIES SHOWN IN FOREST AVENUE ARE DERIVED FROM FIELD LOCATIONS AND PLAN REFERENCE BA. PHYSICAL LOCATIONS SHOULD BE FIELD VERIFIED BEFORE DIGGING.
13. ALL SUBSURFACE UTILITY LINES SHOWN HEREON ARE BASED SOLELY ON THE FIELD LOCATION OR VISIBLE STRUCTURES (E.G., HYDRANTS, ETC.), IN CONJUNCTION WITH DESIGN AND OR AS-BUILT PLANS SHIPPED TO SEBAGO TECHNICS BY OTHERS PRIOR TO ANY CONSTRUCTION. EXCAVATION, TEST BORINGS, DRILLING, ETC. DISCOVERED MUST BE NOTED AND A SITE IDENTIFICATION NUMBER OBTAINED WITH A SAFE TO DIG DATE OBTAINED. THE SITE CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD VERIFYING THE LOCATION, DEPTH AND MATERIAL OF ALL SUBSURFACE UTILITY LINES SHOWN HEREON AND ANY AND ALL OTHERS LOCATED ON SITE WITHIN THE CONSTRUCTION AREA.



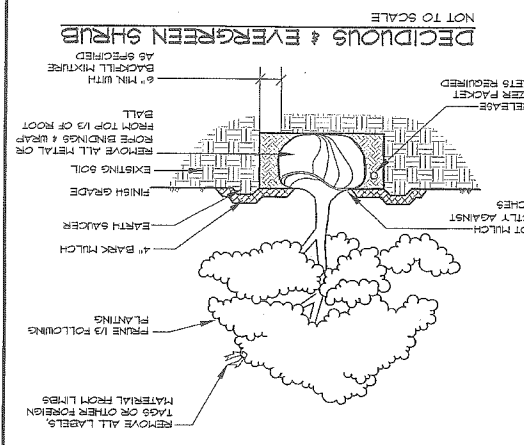
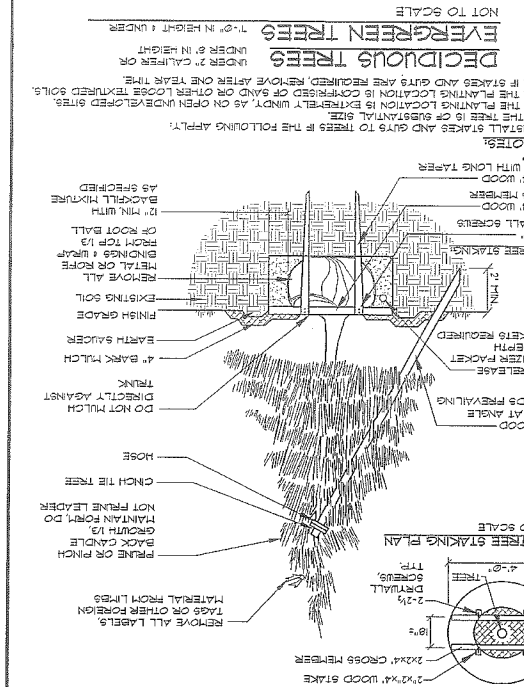
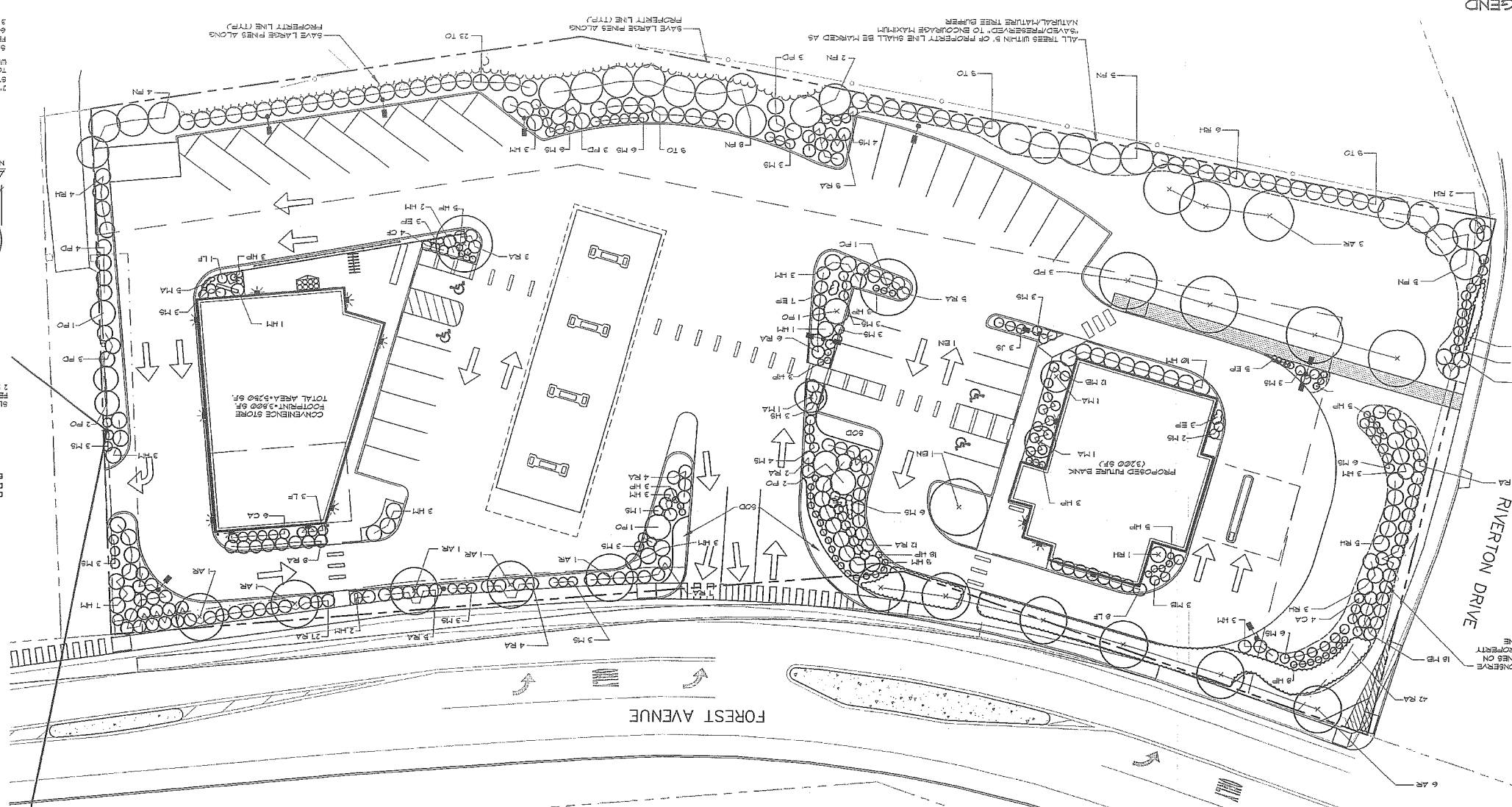
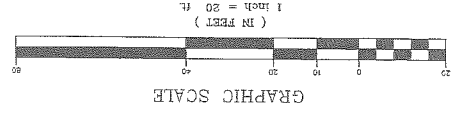
LEGEND

DESCRIPTION	EXISTING
BOUNDARY LINE/R.O.W.	---
WATER	W
WATER SHUT OFF	W
MON.	○
IRON PIPE/ROD	○
DEED CALL	○
CURVE/LINE NO.	○
EDGE CONCRETE	○
PAVEMENT PAINT	○
CULVERT	○
OVERHEAD UTILITY	○
CURBLINE	○
UNDERGROUND UTILITY	○
CONTOURS	○
TRANSFORMER PAD	○
ELECTRICAL MANHOLE	○
TELEPHONE MANHOLE	○
UTILITY POLE	○
GAS	○
GAS GATE VALVE	○
GAS METER	○





EXISTING	DESCRIPTION	PROPOSED
---	BOUNDARY LINE/LOCAL	---
---	BOUNDARY LINE/STATE	---
---	MONUMENT	---
---	IRON PIPE/ROD	---
---	BLDG.	---
---	EDGE PAVEMENT	---
---	PAVEMENT PAINT	---
---	GRAVEL ROAD	---
---	CURBLINE	---
---	CHAIN LINK FENCE	---
○	DECIDUOUS TREE	○
○	CONIFEROUS TREE	○
---	MULCH LINE	---



- LANDSCAPE NOTES**
- PLANT QUANTITIES SHOWN ON PLANS ARE FOR CONFORMANCE TO THE MATERIAL INSTALLATION AS SHOWN ON PLANS.
 - THE LATEST EDITION OF U.S.A. STANDARDS FOR NURSERY STOCK, BY THE AMERICAN ASSOCIATION OF NURSERYMEN, INC.
 - ALL PLANT MATERIAL SHALL BE FREE FROM INSECTS AND DISEASE.
 - ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH ACCEPTABLE HORTICULTURAL PRACTICES. THIS IS TO INCLUDE PROPER PLANTING MIX, PLANT BED AND TREE FIT PREPARATION, PRUNING, STAKING AND ADJUSTABLE MAINTENANCE UNTIL ACCEPTANCE BY THE OWNER.
 - PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR BY THE CONTRACTOR AND A PERIOD OF TWO YEARS THEREAFTER BY THE SUPPLIER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE GUARANTEE PERIOD. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE GUARANTEE PERIOD. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE GUARANTEE PERIOD.
 - ALL GRASS, OTHER VEGETATION AND DEBRIS SHALL BE REMOVED FROM ALL PLANTING AREAS PRIOR TO PLANTING.
 - EXISTING TREES TO BE PRESERVED WILL BE PROTECTED DURING CONSTRUCTION AND SHALL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR.
 - THE LANDSCAPE CONTRACTOR IS ADVISED OF THE PRESENCE OF THE FOLLOWING UTILITIES AND SHALL VERIFY THE EXISTENCE AND LOCATION OF SAME BEFORE COMMENCING WORK. THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF PORTLAND, OREGON.
 - ALL SHRUB BEDS SHALL BE FILLED WITH 3\"/>

LANDSCAPING PLAN
TRITON FOODMART / GAS STATION
 1844 FOREST AVE.
 PORTLAND, MAINE
 FOR: **JOHN CHAU**
 75 ACADIA ST.
 PORTLAND, MAINE 04103

Sebago Technics
 Engineering Expertise You Can Build On
 One Chapel Street
 250 Oakland Road - Suite 8
 Westbrook, ME 04090-1333
 Tel (207) 666-0277
 Fax (207) 780-5858
 WWW.SEBAGO-TECHNICS.COM

PROJECT NO. 11142 FIELD BOOK SAG DESIGN CHD DRAWN JRS

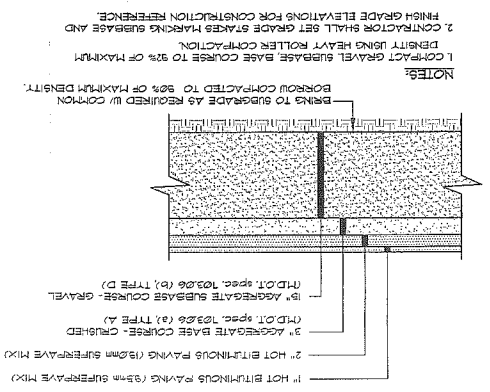
REV.	BY:	DATE:	STATUS:
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B	JRS	01/10/12	SUBMITTED FOR FINAL PLAN REVIEW
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D	JRS	09/11	SUBMITTED FOR PRELIMINARY REVIEW

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DATE: 8/3/11 SCALE: 1" = 20'

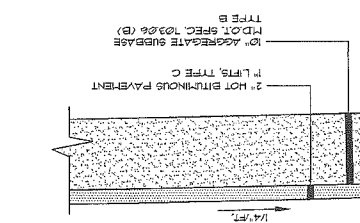
SHEET 5 OF 9

TYP. PAVED PARKING LOT SECTION



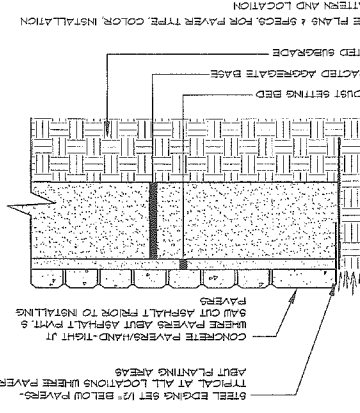
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BITUMINOUS SIDEWALK



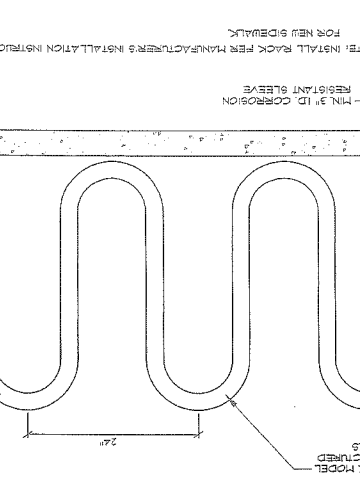
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SPECIALTY PAVEMENT



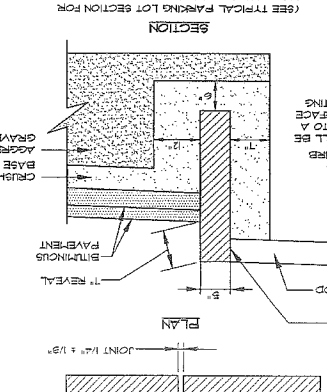
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BIKE RACK DETAIL



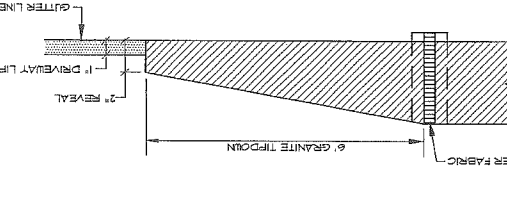
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VERTICAL GRANITE CURB



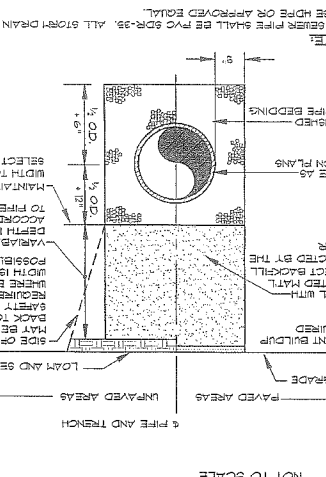
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TYPICAL TIPDOWN CURB INSTALLATION



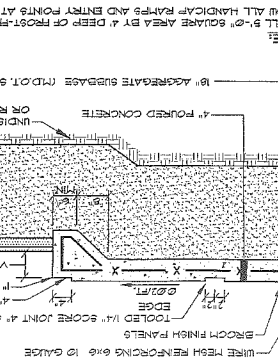
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TYPICAL TRENCH SECTION



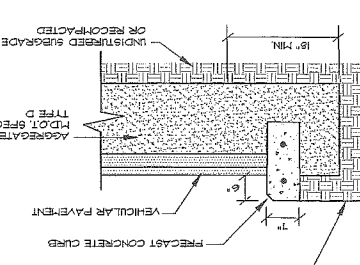
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CONCRETE SIDEWALK



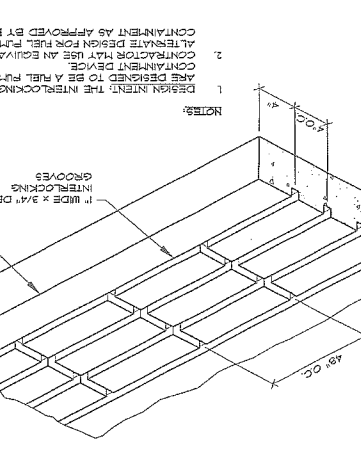
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PRECAST CONCRETE CURB



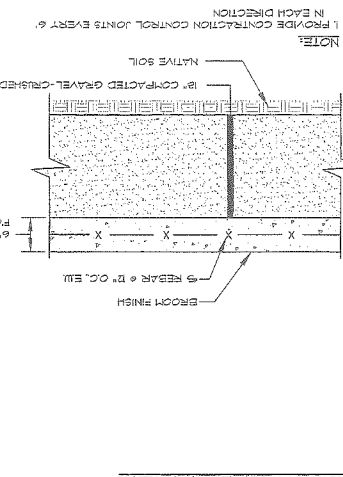
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POSITIVE LIMITING BARRIER



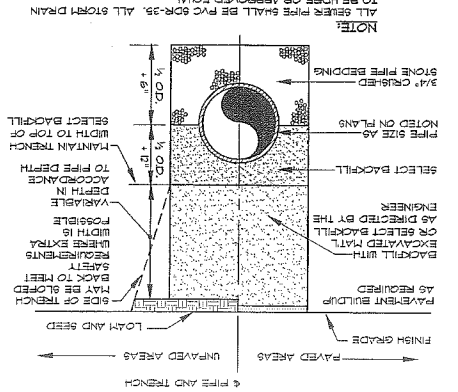
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TYPICAL CONCRETE SLAB



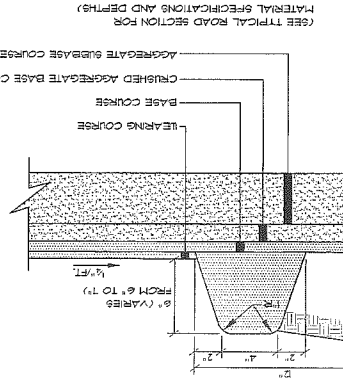
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PRIVATE TYPICAL TRENCH SECTION



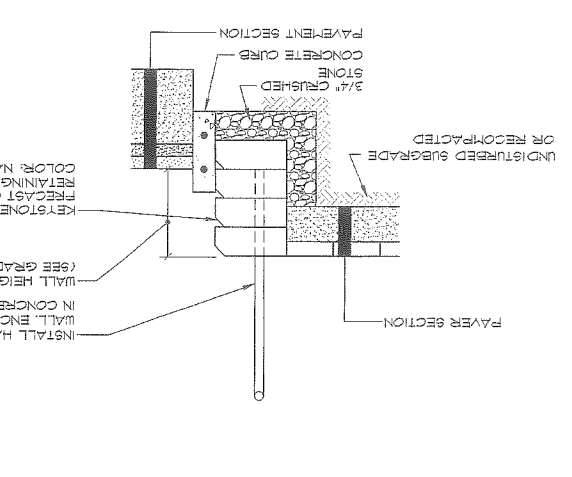
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BITUMINOUS CURB SECTION



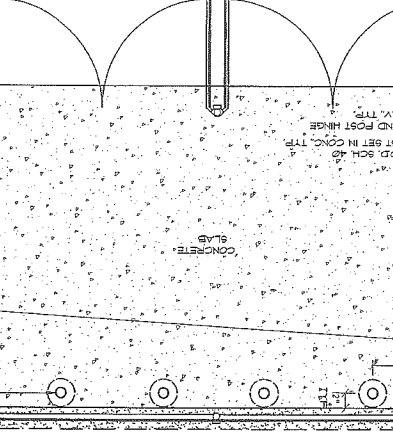
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CONCRETE BLOCK RETAINING WALL



NOTES:
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TYPICAL DUMPSTER ENCLOSURE



NOTES:
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DETAILS

OF:
TRITON FOODMART / GAS STATION
1881 FOREST AVE
PORTLAND, MAINE
FOR:
JOHN CHAU
78 AKADIA ST
PORTLAND, MAINE 04103

DATE	SCALE
8/31/11	AS NOTED

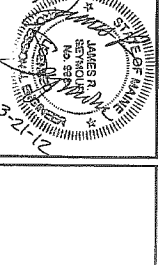
SHEET 7 OF 9

Sebago Technics
Engineering, Expediting, Van, Can, Build On
One Orono Street
290 Orono Road, Sebago
Winthrop, ME 04095-1339
Tel (207) 852-2277
Fax (207) 735-8888
WWW.SEBAGOTECHNICS.COM

PROJECT NO.	FIELD BOOK	DESIGN	CHECK	DRAWN
11142		SAG	JNS	JCS/SH

D	JNS	03/19/12	REVISED PER FINAL PLAN COMMENTS
C	JNS	03/08/12	SUBMITTED FOR FINAL PLAN REVIEW
B	JNS	01/10/12	SUBMITTED FOR PRELIMINARY REVIEW
A	JNS	11/19/11	SUBMITTED FOR PRELIMINARY REVIEW

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AMERICAST
 PRECAST FILTERA® UNIT
 DATE: 09-04-07 DWG: FTLN-3
 DRAWINGS AVAILABLE IN THE FORMAT: DWG, FTLN-3

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

SECTION A-A
 FILTER MEDIA PROVIDED BY AMERICAST
 UNDERBRAIN STONE PROVIDED BY AMERICAST
 UNTERBRAIN SYSTEM
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 INTERLOCKING JOINT (TYP.)
 TOP SLAB AND GUTTER (BY OTHERS)
 CURB AND GUTTER (BY OTHERS)
 CAST-IN-PLACE CONCRETE (NOT SHOWN FOR CLARITY)
 4x4" TREE FRAME BY AMERICAST
 PLANT AS SUPPLIED
 PLAN VIEW
 CLEANOUT COVER
 CAST IN TOP SLAB
 TOP SLAB
 INTERLOCKING JOINT (TYP.)
 CURB AND GUTTER (BY OTHERS)
 LOCATION (W/ OTHERS)
 AS NOTED ON PLANS
 AS NOTED ON PLANS

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

AMERICAST
 FILTERA® THROAT OPENING AND GUTTER OR FLUME DETAIL
 DATE: 02-08-08 DWG: COT-4
 DRAWINGS AVAILABLE IN THE FILE FORMAT: DWG, COT-4

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

SECTION VIEW
 STANDARD FILTERA THROAT OPENING
 PRECAST BOX WALL
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 INTERLOCKING JOINT (TYP.)
 TOP SLAB AND GUTTER (BY OTHERS)
 CURB AND GUTTER (BY OTHERS)
 CAST-IN-PLACE CONCRETE (NOT SHOWN FOR CLARITY)
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 PLAN VIEW
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 CAST IN TOP SLAB
 TOP SLAB
 INTERLOCKING JOINT (TYP.)
 CURB AND GUTTER (BY OTHERS)
 LOCATION (W/ OTHERS)
 AS NOTED ON PLANS
 AS NOTED ON PLANS

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

AMERICAST
 FILTERA® TYPICAL FLOWLINE AND OUTLET PIPE RELATIONSHIP
 DATE: 07-07-06 DWG: FLP-2
 DRAWINGS AVAILABLE IN THE FILE FORMAT: DWG, FLP-2

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

CROSS SECTION
 FILTERA UNIT
 PERFORMED BY AMERICAST
 UNDERBRAIN SYSTEM
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 INTERLOCKING JOINT (TYP.)
 TOP SLAB AND GUTTER (BY OTHERS)
 CURB AND GUTTER (BY OTHERS)
 CAST-IN-PLACE CONCRETE (NOT SHOWN FOR CLARITY)
 4x4" TREE FRAME BY AMERICAST
 PLANT AS SUPPLIED
 PLAN VIEW
 CLEANOUT COVER
 CAST IN TOP SLAB
 TOP SLAB
 INTERLOCKING JOINT (TYP.)
 CURB AND GUTTER (BY OTHERS)
 LOCATION (W/ OTHERS)
 AS NOTED ON PLANS
 AS NOTED ON PLANS

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

AMERICAST
 TYPICAL DRAINAGE STRUCTURE
 DATE: 09-04-07 DWG: FTLN-3
 DRAWINGS AVAILABLE IN THE FORMAT: DWG, FTLN-3

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

SECTION A-A
 FILTER MEDIA PROVIDED BY AMERICAST
 UNDERBRAIN STONE PROVIDED BY AMERICAST
 UNTERBRAIN SYSTEM
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 INTERLOCKING JOINT (TYP.)
 TOP SLAB AND GUTTER (BY OTHERS)
 CURB AND GUTTER (BY OTHERS)
 CAST-IN-PLACE CONCRETE (NOT SHOWN FOR CLARITY)
 4x4" TREE FRAME BY AMERICAST
 PLANT AS SUPPLIED
 PLAN VIEW
 CLEANOUT COVER
 CAST IN TOP SLAB
 TOP SLAB
 INTERLOCKING JOINT (TYP.)
 CURB AND GUTTER (BY OTHERS)
 LOCATION (W/ OTHERS)
 AS NOTED ON PLANS
 AS NOTED ON PLANS

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

AMERICAST
 TYPICAL CHAIN LINK FENCE
 DATE: 07-07-06 DWG: FLP-2
 DRAWINGS AVAILABLE IN THE FILE FORMAT: DWG, FLP-2

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

SECTION VIEW
 STANDARD FILTERA THROAT OPENING
 PRECAST BOX WALL
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 INTERLOCKING JOINT (TYP.)
 TOP SLAB AND GUTTER (BY OTHERS)
 CURB AND GUTTER (BY OTHERS)
 CAST-IN-PLACE CONCRETE (NOT SHOWN FOR CLARITY)
 4x4" TREE FRAME BY AMERICAST
 PLANT AS SUPPLIED
 PLAN VIEW
 CLEANOUT COVER
 CAST IN TOP SLAB
 TOP SLAB
 INTERLOCKING JOINT (TYP.)
 CURB AND GUTTER (BY OTHERS)
 LOCATION (W/ OTHERS)
 AS NOTED ON PLANS
 AS NOTED ON PLANS

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

AMERICAST
 OUTLET STRUCTURE DETAIL OCS-2
 DATE: 07-07-06 DWG: FLP-2
 DRAWINGS AVAILABLE IN THE FILE FORMAT: DWG, FLP-2

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

SECTION VIEW
 STANDARD FILTERA THROAT OPENING
 PRECAST BOX WALL
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 INTERLOCKING JOINT (TYP.)
 TOP SLAB AND GUTTER (BY OTHERS)
 CURB AND GUTTER (BY OTHERS)
 CAST-IN-PLACE CONCRETE (NOT SHOWN FOR CLARITY)
 4x4" TREE FRAME BY AMERICAST
 PLANT AS SUPPLIED
 PLAN VIEW
 CLEANOUT COVER
 CAST IN TOP SLAB
 TOP SLAB
 INTERLOCKING JOINT (TYP.)
 CURB AND GUTTER (BY OTHERS)
 LOCATION (W/ OTHERS)
 AS NOTED ON PLANS
 AS NOTED ON PLANS

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

AMERICAST
 RIPRAP APRON
 DATE: 07-07-06 DWG: FLP-2
 DRAWINGS AVAILABLE IN THE FILE FORMAT: DWG, FLP-2

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

SECTION VIEW
 STANDARD FILTERA THROAT OPENING
 PRECAST BOX WALL
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 INTERLOCKING JOINT (TYP.)
 TOP SLAB AND GUTTER (BY OTHERS)
 CURB AND GUTTER (BY OTHERS)
 CAST-IN-PLACE CONCRETE (NOT SHOWN FOR CLARITY)
 4x4" TREE FRAME BY AMERICAST
 PLANT AS SUPPLIED
 PLAN VIEW
 CLEANOUT COVER
 CAST IN TOP SLAB
 TOP SLAB
 INTERLOCKING JOINT (TYP.)
 CURB AND GUTTER (BY OTHERS)
 LOCATION (W/ OTHERS)
 AS NOTED ON PLANS
 AS NOTED ON PLANS

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

AMERICAST
 DETENTION POND SECTION
 DATE: 07-07-06 DWG: FLP-2
 DRAWINGS AVAILABLE IN THE FILE FORMAT: DWG, FLP-2

INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

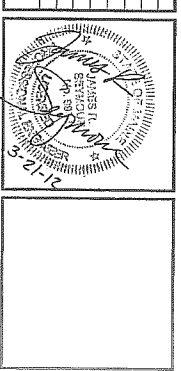
SECTION VIEW
 STANDARD FILTERA THROAT OPENING
 PRECAST BOX WALL
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 4" PRECAST BY AMERICAST
 1/2" TO 1/4" TOP
 INTERLOCKING JOINT (TYP.)
 TOP SLAB AND GUTTER (BY OTHERS)
 CURB AND GUTTER (BY OTHERS)
 CAST-IN-PLACE CONCRETE (NOT SHOWN FOR CLARITY)
 4x4" TREE FRAME BY AMERICAST
 PLANT AS SUPPLIED
 PLAN VIEW
 CLEANOUT COVER
 CAST IN TOP SLAB
 TOP SLAB
 INTERLOCKING JOINT (TYP.)
 CURB AND GUTTER (BY OTHERS)
 LOCATION (W/ OTHERS)
 AS NOTED ON PLANS
 AS NOTED ON PLANS

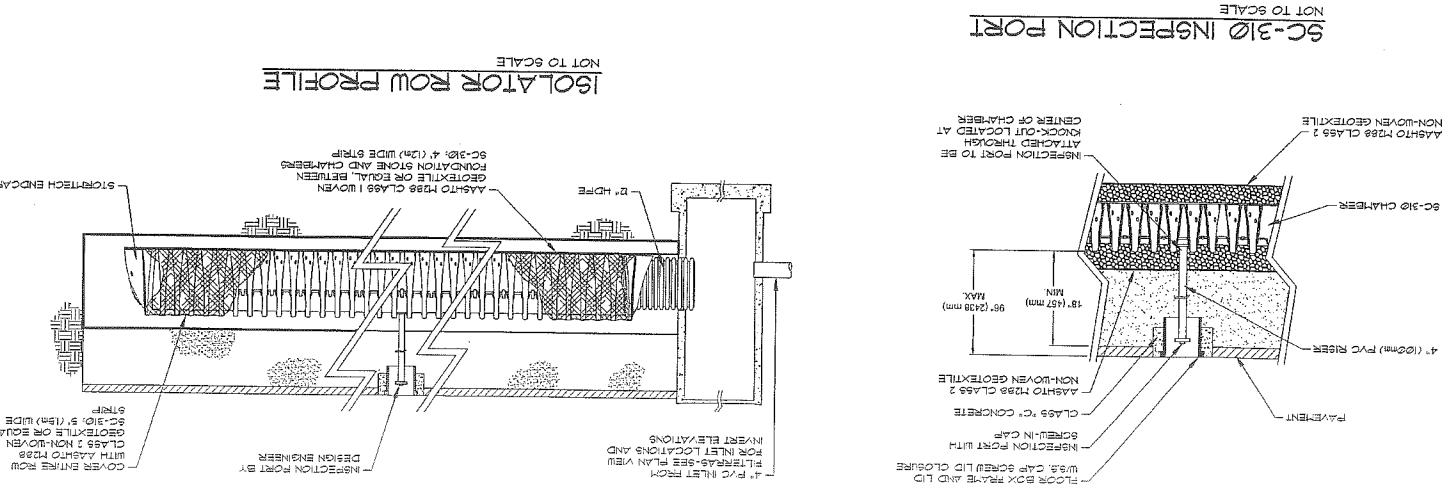
INDICATIONS OF DRAWINGS ARE ONLY PERMITTED BY WRITTEN AUTHORIZATION FROM FILTERA

DETAILS
 TRITON FOODMART / GAS STATION
 1899 FOREST AVE
 PORTLAND, MAINE
 JOHN CHAU
 75 AQUINA ST
 PORTLAND, MAINE 04103

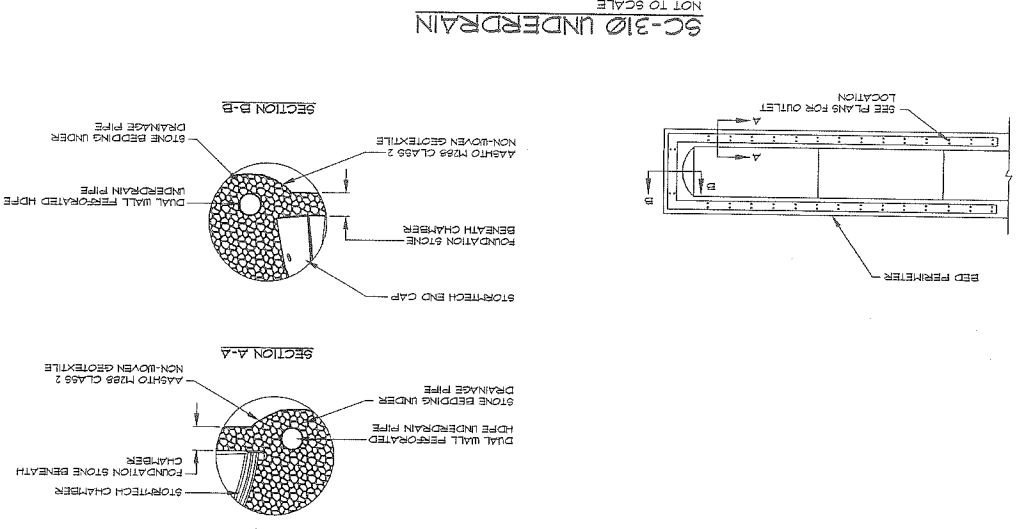
Sebago Technics
 Engineering Experts You Can Build On
 One Center Street
 290 Gaudet Road, Suite B
 Westbrook, ME 04090-1319
 Tel (207) 854-2777 Fax (207) 793-8888
 www.sebagotechnics.com

NO.	DATE	BY	STATUS
D	03/19/12	JRS	REVISED PER FINAL PLAN COMMENTS
C	03/09/12	JRS	SUBMITTED FOR FINAL PLAN REVIEW
B	01/07/12	JRS	RESUBMITTED FOR PRELIMINARY REVIEW
A	11/15/11	JRS	SUBMITTED FOR PRELIMINARY REVIEW

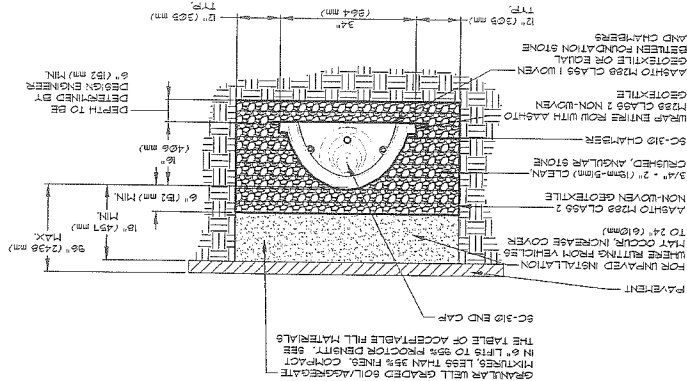




SC-310 INSPECTION PORT
NOT TO SCALE



SC-310 UNDERDRAIN
NOT TO SCALE



SC-310 STANDARD CROSS SECTION
NOT TO SCALE

1. THE INSTALLED CHAMBER SYSTEM SHALL PROVIDE THE LOAD FACTORS SPECIFIED IN THE ASHTO LTPP BRIDGE DESIGN SPECIFICATION FOR BATH AND LIVE LOADS WITH CONSIDERATION FOR IMPACT AND MULTIPLE VEHICLE PRESENCE.
2. CHAMBERS SHALL MEET ASTM F 1463 STANDARD SPECIFICATION FOR POLYPROPYLENE (PP) CORROSION RESISTANT COLLECTION CHAMBERS.
3. THE DESIGN ENGINEER IS RESPONSIBLE FOR ENSURING THE REQUIRED BEARING CAPACITY OF SUBGRADE SOILS.
4. THIS SECTION DETAILS THE REQUIREMENTS NECESSARY TO SATISFY THE LOAD FACTORS SPECIFIED IN THE ASHTO LTPP BRIDGE DESIGN SPECIFICATIONS SECTION 112 FOR BATH AND LIVE LOADS USING STORMTECH CHAMBERS.

SHEET 9 OF 9

DATE	8/3/11
SCALE	AS NOTED

DETAILS
OF:
TRITON FOODMART / GAS STATION
1884 FOREST AVE.
PORTLAND, MAINE
FOR:
JOHN CHAU
75 ACADIA ST.
PORTLAND, MAINE 04103

Sebago Technics
Engineering Experts You Can Build On
One Chubb Street
Walden, ME 04868-1433
Tel (207) 856-5277
www.sebagotechnics.com

291 Geddes Road, Suite 8
Walden, ME 04868
Tel (207) 753-8858

PROJECT NO.	11142	FIELD BOOK	DESIGN	CHAD	DRAWN
			SAG	JRS	JMS/JRH

REV.	BY:	DATE:	STATUS:	REVISIONS:
D	JRS	03/19/12	REVISED PER FINAL PLAN COMMENTS	
C	JRS	03/06/12	SUBMITTED FOR FINAL PLAN REVIEW	
B	JRS	01/10/12	RESUBMITTED FOR PRELIMINARY REVIEW	
A	JRS	11/15/11	SUBMITTED FOR PRELIMINARY REVIEW	

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TRITON FOODMART / GAS STATION

1884 FOREST AVENUE
 PORTLAND, MAINE

APPLICANT:

JOHN CHAU

15 ACADIA STREET
 PORTLAND, MAINE 04103

ENGINEER / SURVEYOR:



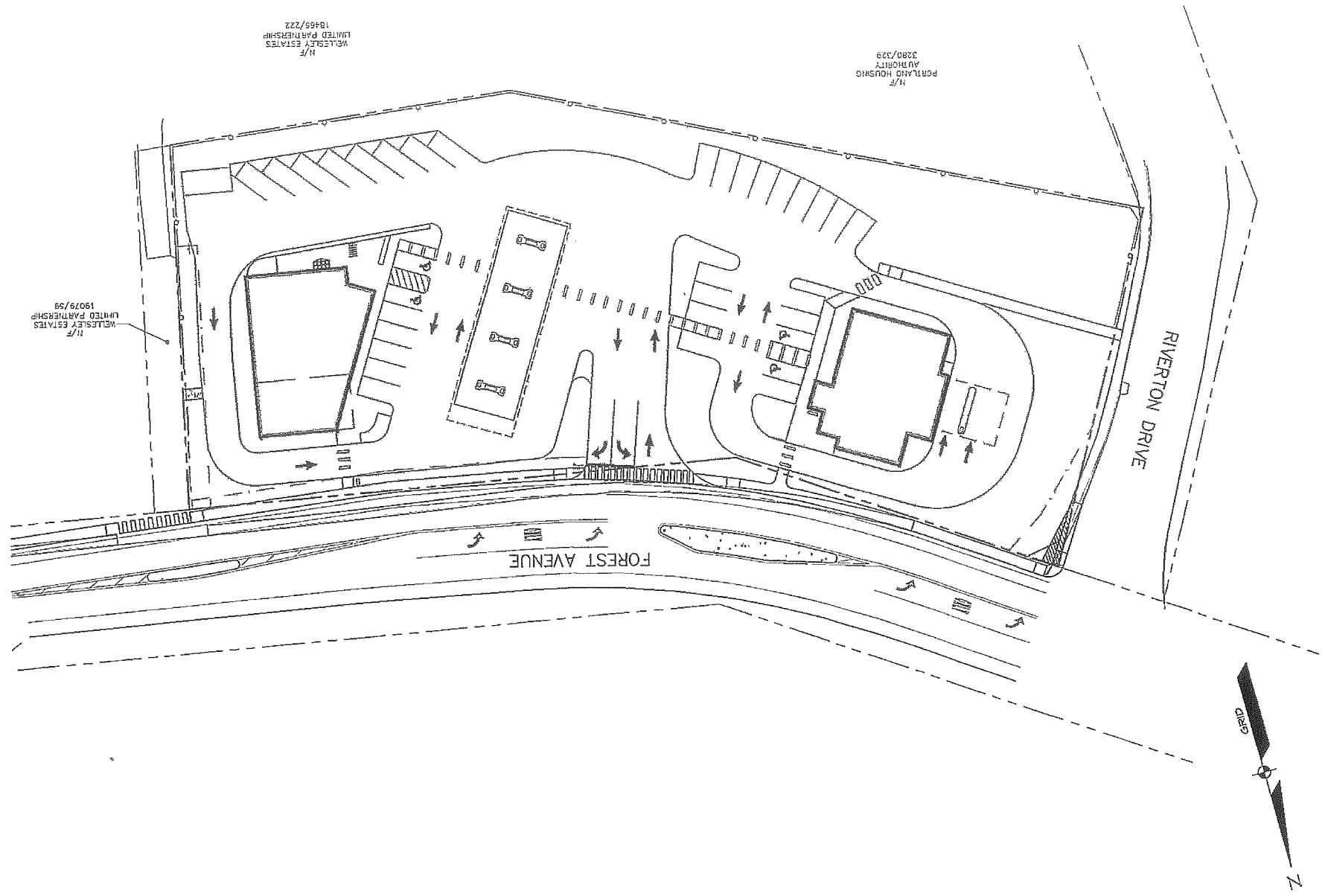
Sebago Technics

Engineering Expertise You Can Build On
 One Clark Street, 250 Goddard Rd - Suite B
 Lewiston, ME 04240
 Washburn, ME 04098-1338
 Tel (207) 858-2277
 Tel (207) 793-5556
 WWW.SEBAGOTECHNICS.COM

PROJECT ARCHITECT:

TFH ARCHITECTS

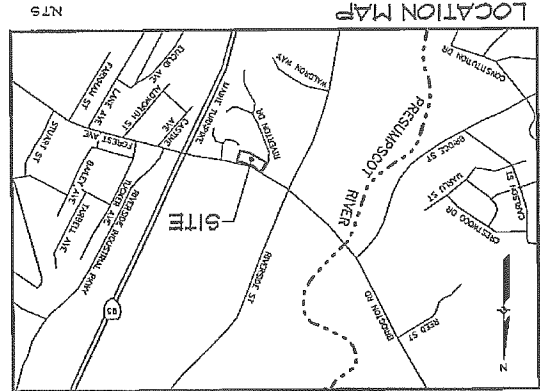
80 MIDDLE STREET
 PORTLAND, MAINE



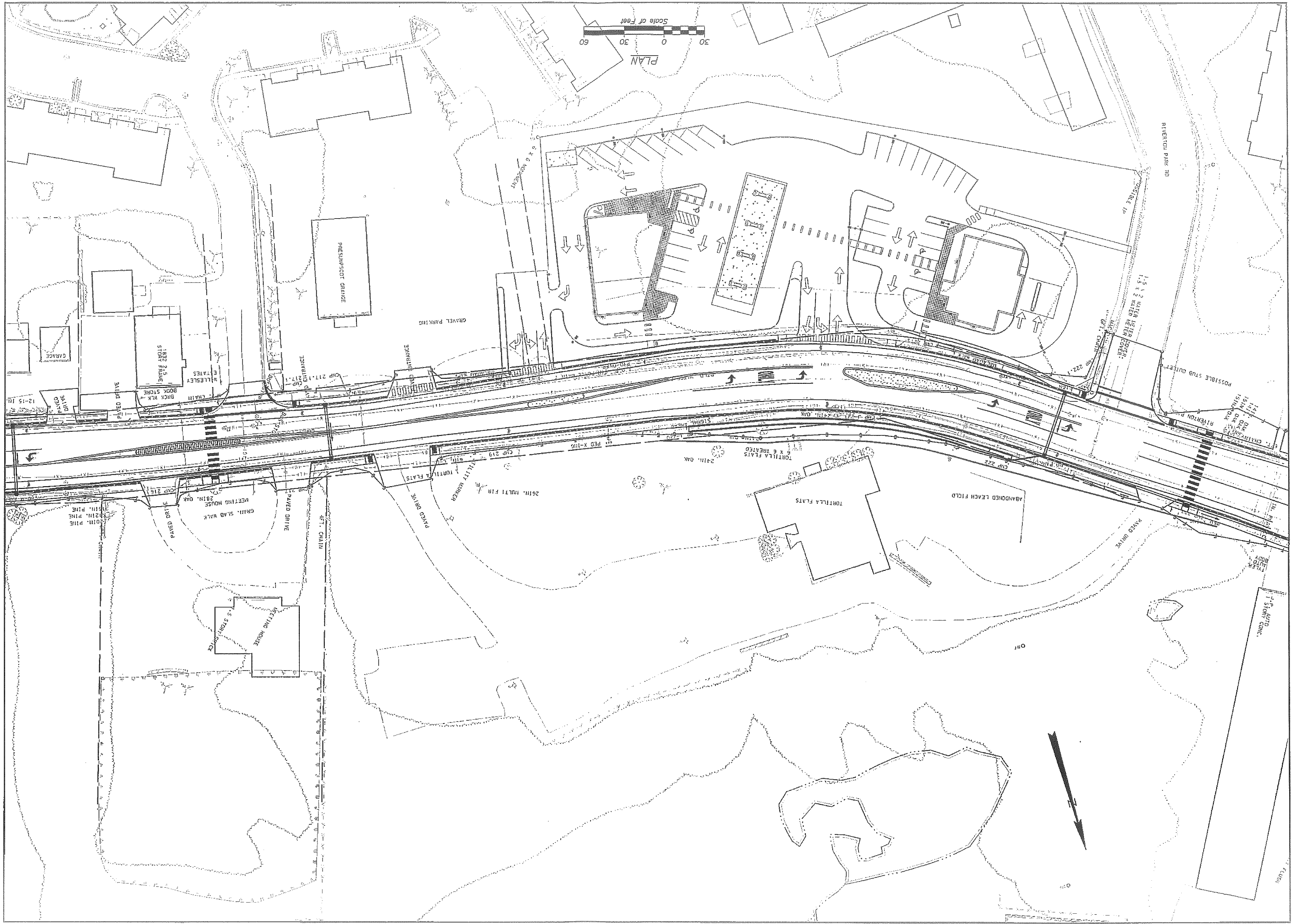
SCALE: 1" = 30'

SHEET INDEX:

SHEET	DESCRIPTION
1	COVER SHEET
2	EXISTING CONDITIONS PLAN
3	SITE PLAN
4	GRADING AND UTILITY PLAN
5	LANDSCAPING PLAN
6	DETAILS
7	DETAILS
8	DETAILS
9	DETAILS



Att.C.1



SHEET 1 OF 2
 DATE 4-5-12
 SCALE 1"=30'
 OF
 1884 FOREST AVENUE DEVELOPMENT
 ROUTE 302
 FORTLAND, MAINE
 FOR
 MR. JOHN CHAU
 PORTLAND, MAINE 04103
 RECORD OWNER
 OWNERS STREET
 OWNERS CITY, STATE ZIP

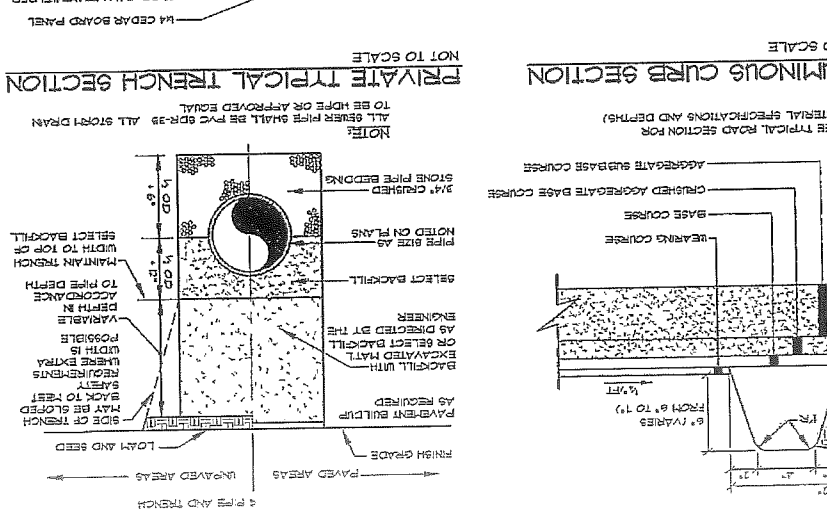
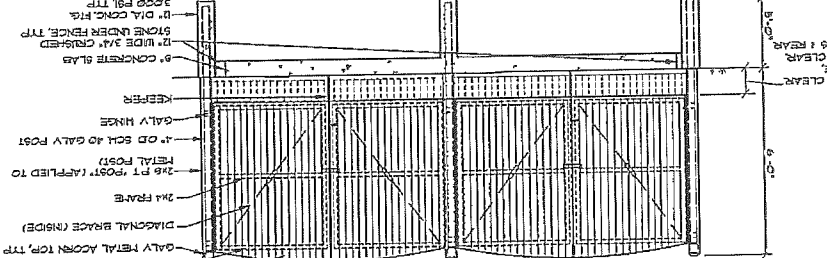
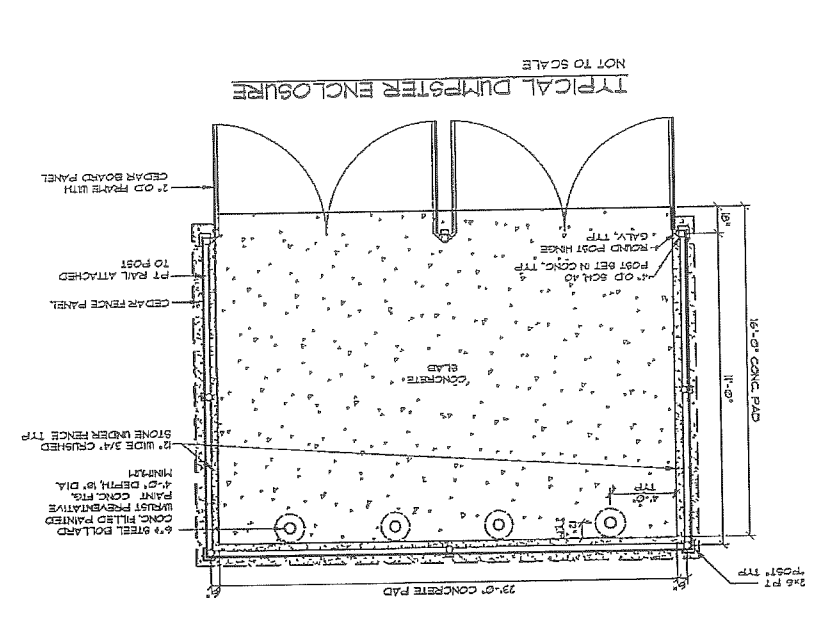
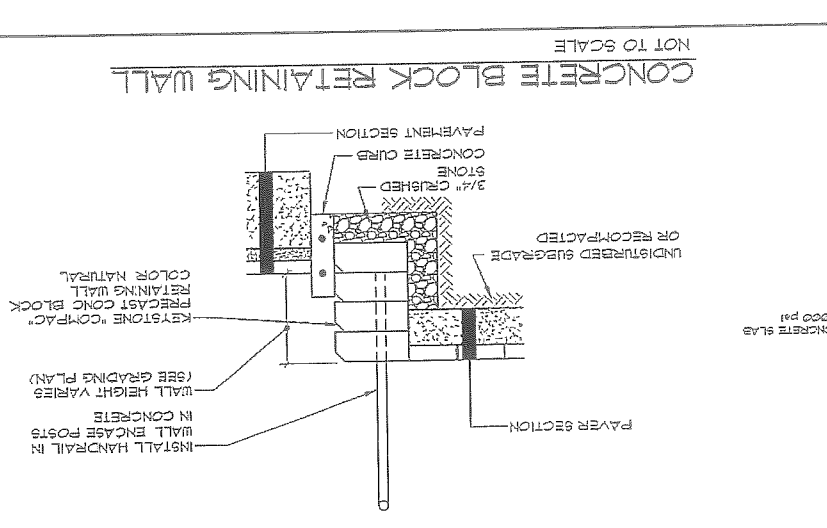
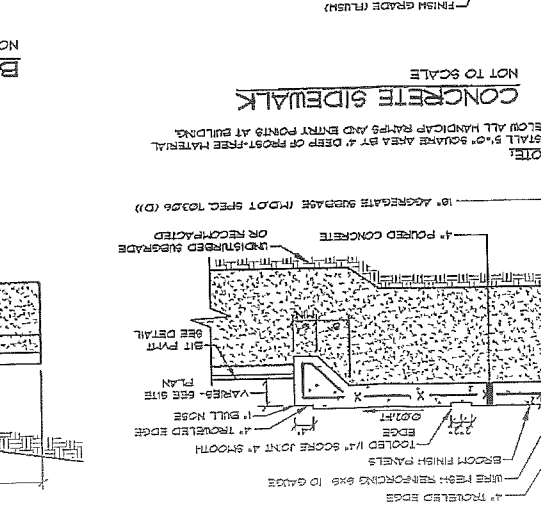
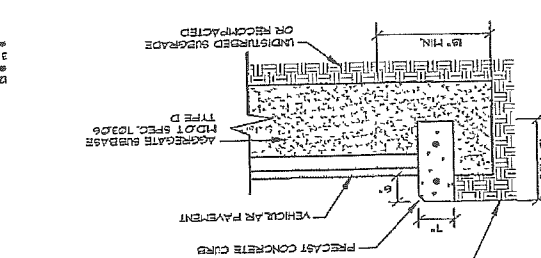
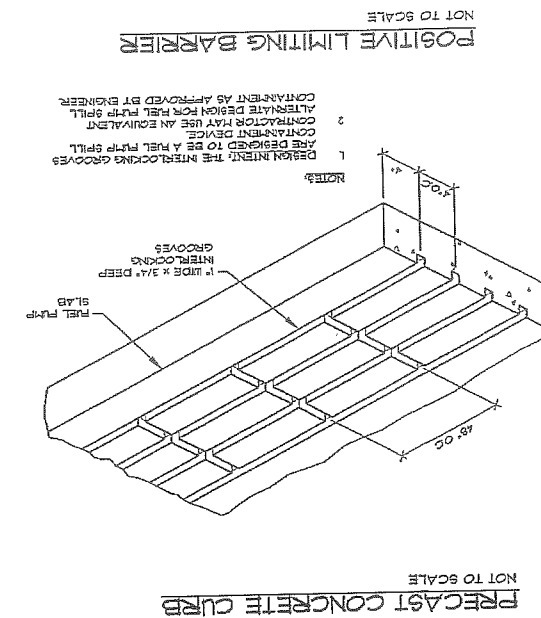
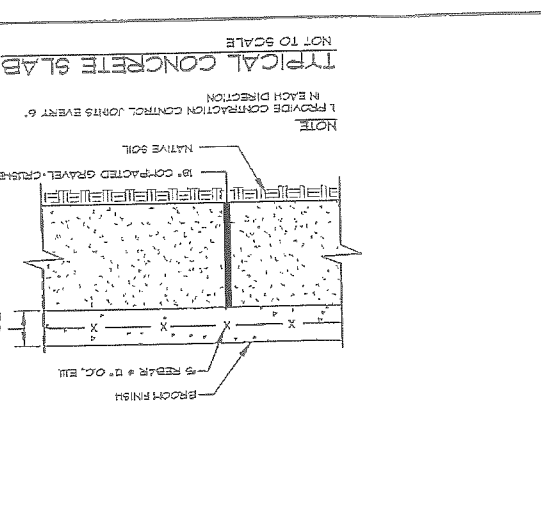
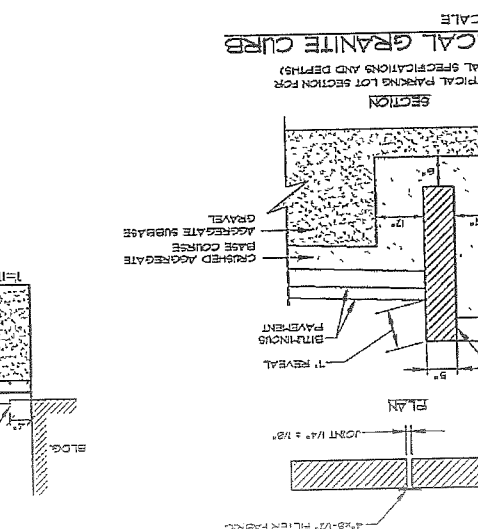
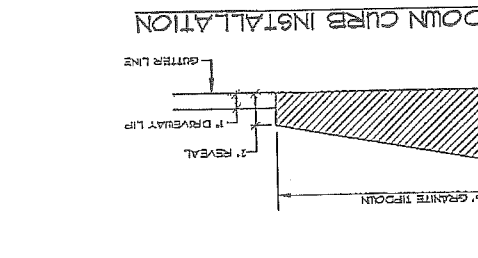
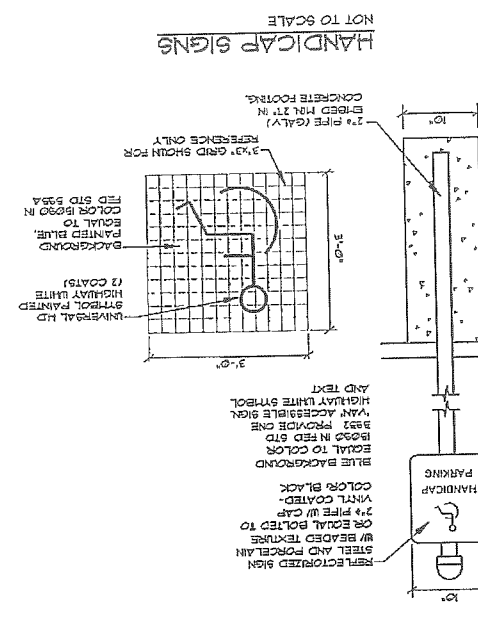
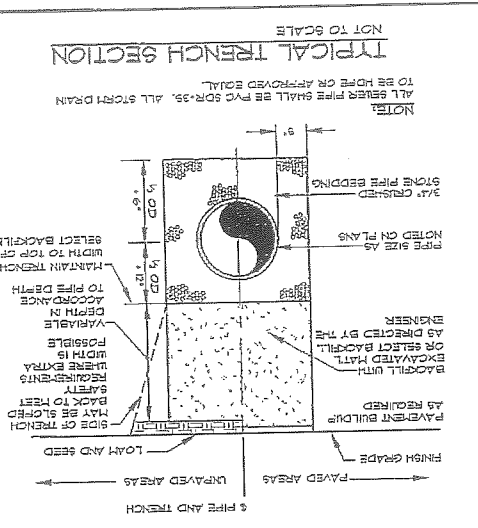
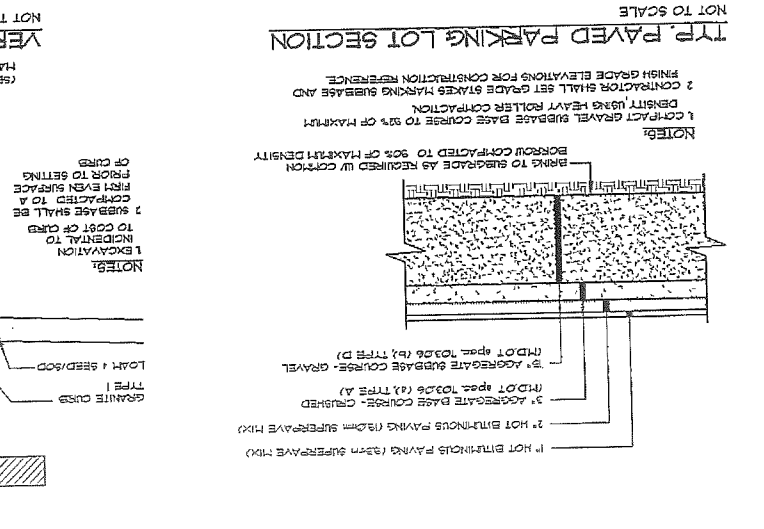
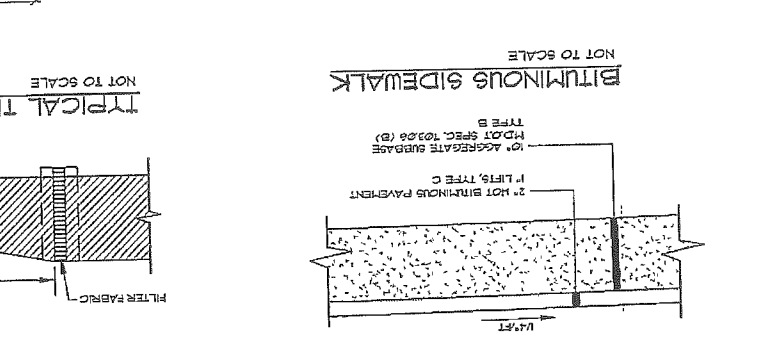
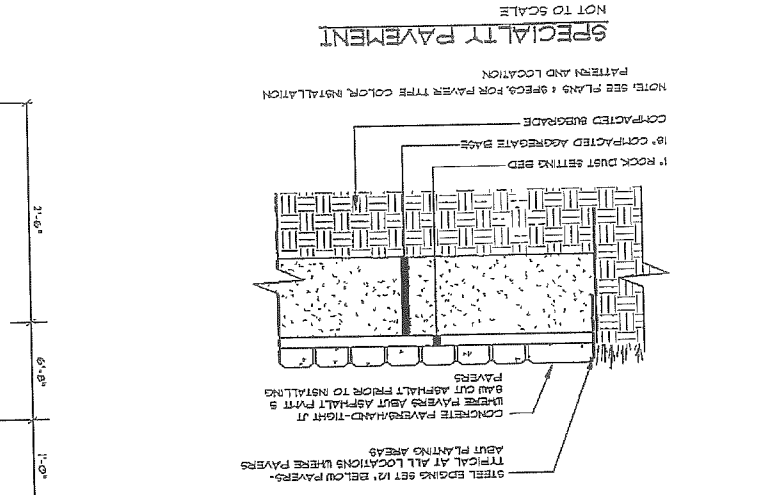
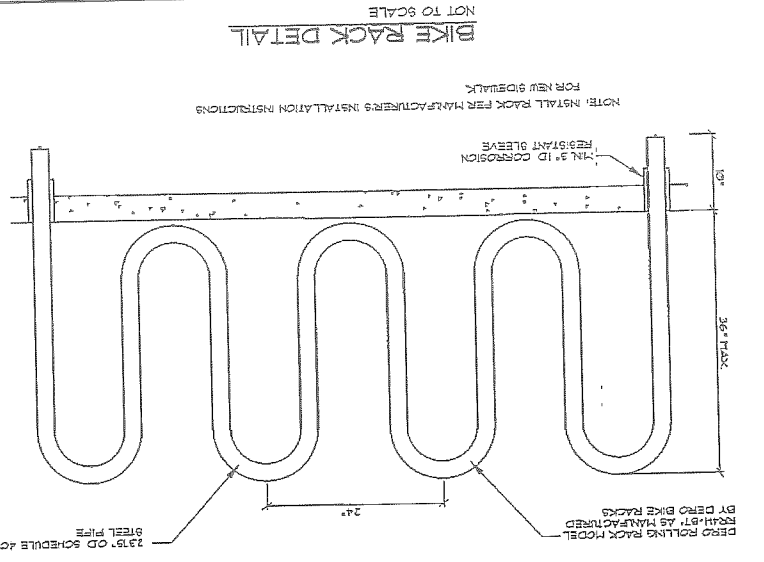
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 250 Goddard Road - Suite B
 Portland, ME 04103
 Tel (207) 763-8888

PROJECT NO.	FIELD BOOK	DESIGN	CHKD	DRAWN
1142				

REV.	DATE	STATUS	PRELIMINARY CONCEPT PLAN
A	7-25-11		
BRL			
BRT			

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C.14
 NOT FOR CONSTRUCTION



SHEET 7 OF 9

DATE	SCALE	AS NOTED
8/31/11		

DETAILS

TRITON FOODMART / GAS STATION

1884 FOREST AVE
PORTLAND, MAINE
75 ACADIA ST
PORTLAND, MAINE 04103

FOR: JOHN CHAU

Sebago Technics

Engineering Experience You Can Build On.

230 Oakland Road - Suite B
Lewiston, ME 04240
Tel (207) 755-4277
Tel (207) 755-5555
WWW.SEBAGOENGINEERING.COM

PROJECT NO. 11142
FIELD BOOK
DESIGN
SACG
JRS
DRAWN
JKS/BRH

REV	DATE	STATUS
A	11/16/11	SUBMITTED FOR PRELIMINARY REVIEW
B	01/10/12	SUBMITTED FOR FINAL PLAN REVIEW
C	03/08/12	SUBMITTED FOR FINAL PLAN COMMENTS
D	03/19/12	REVISION PER FINAL PLAN COMMENTS
E	04/12/12	REVISION SITE PLAN/REMOVED COFFEE DRIVE THRU

THIS PLAN SHALL NOT BE USED WITHOUT THE WRITTEN PERMISSION FROM SEBAGO TECHNICS, INC. ANY ALTERATIONS
NOTIFICATION ON THE REVISIONS SHALL BE AT THE SEBAGO SCALE BAR AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC.

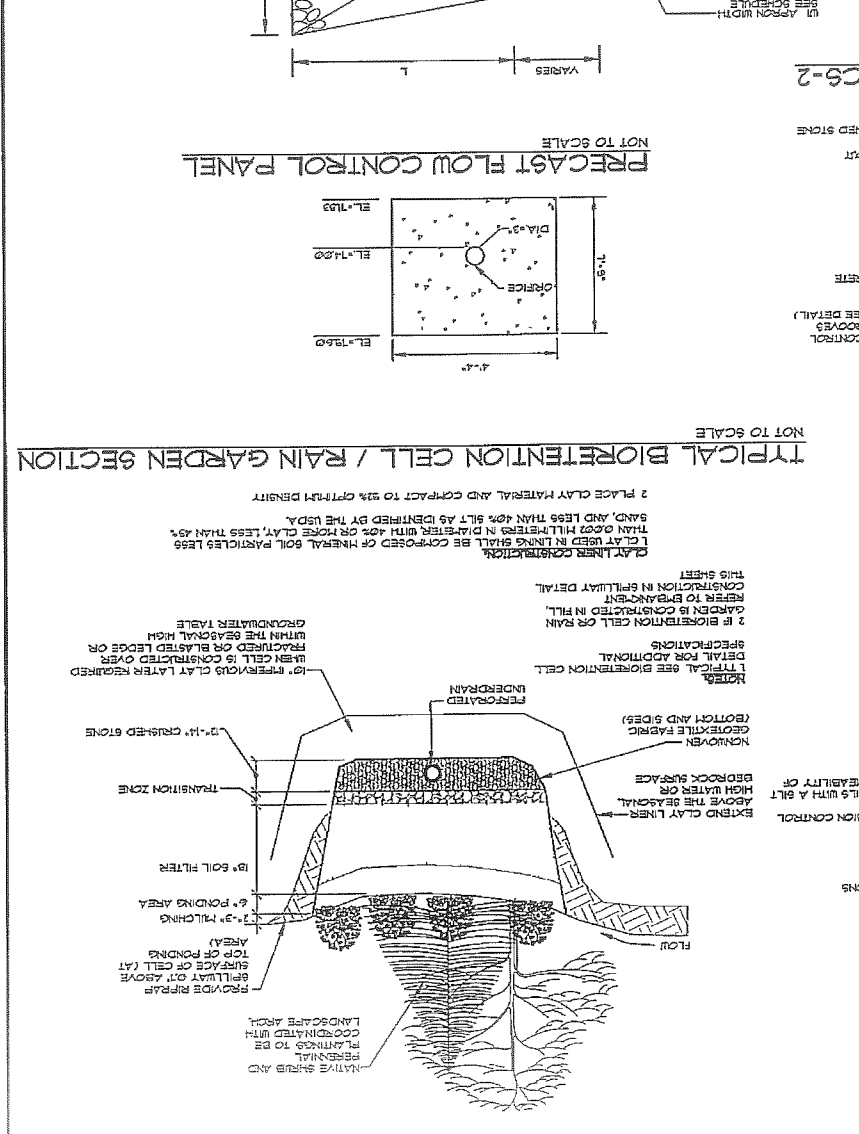
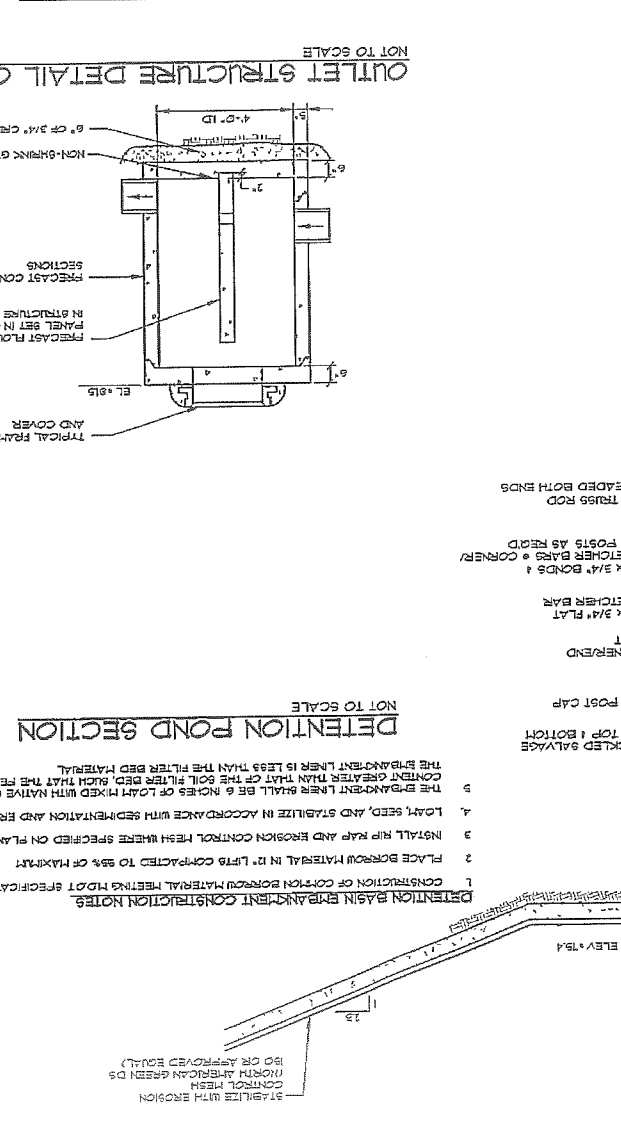
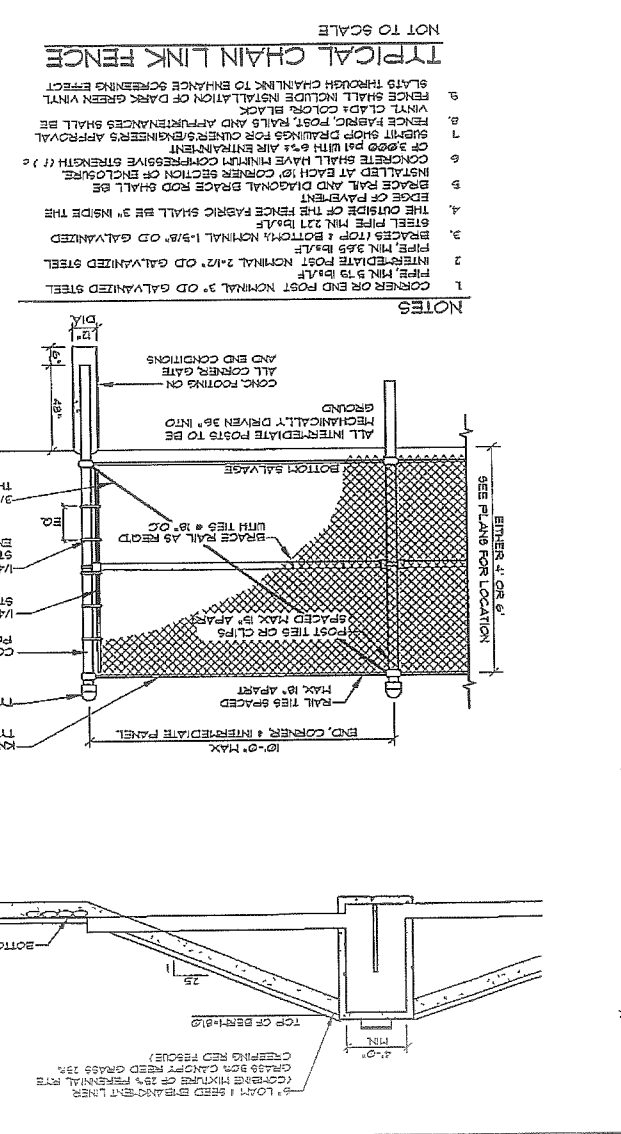
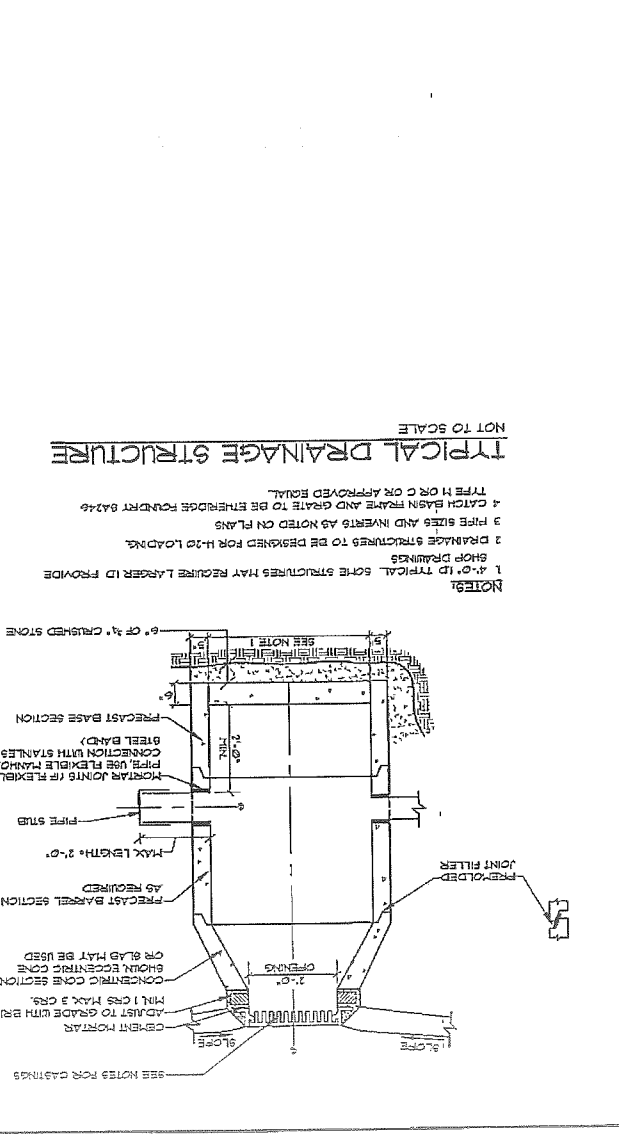
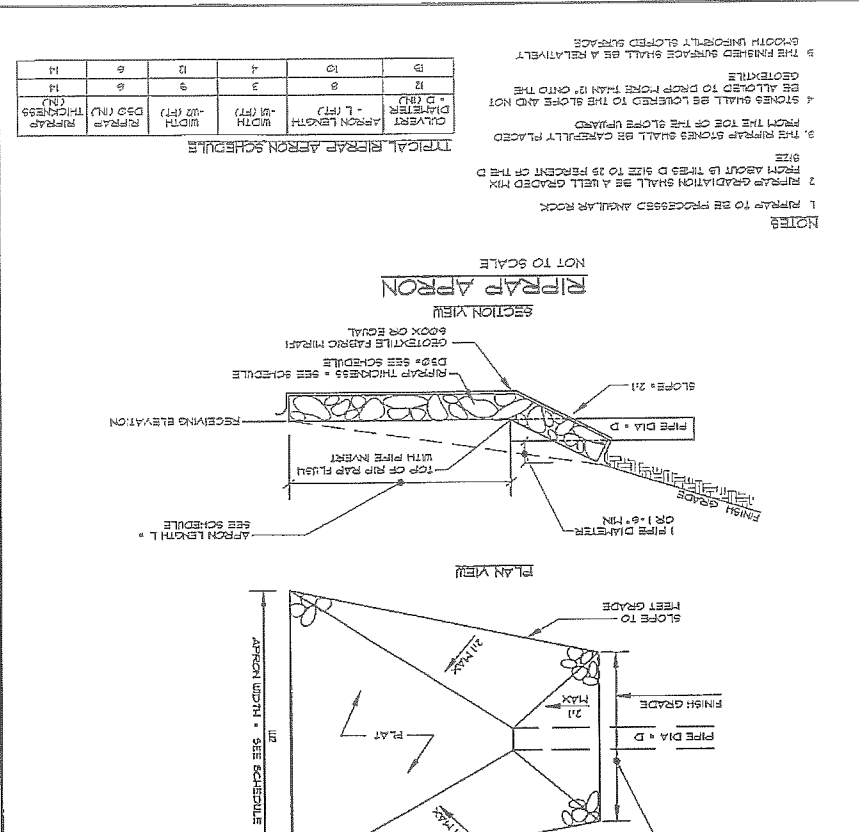
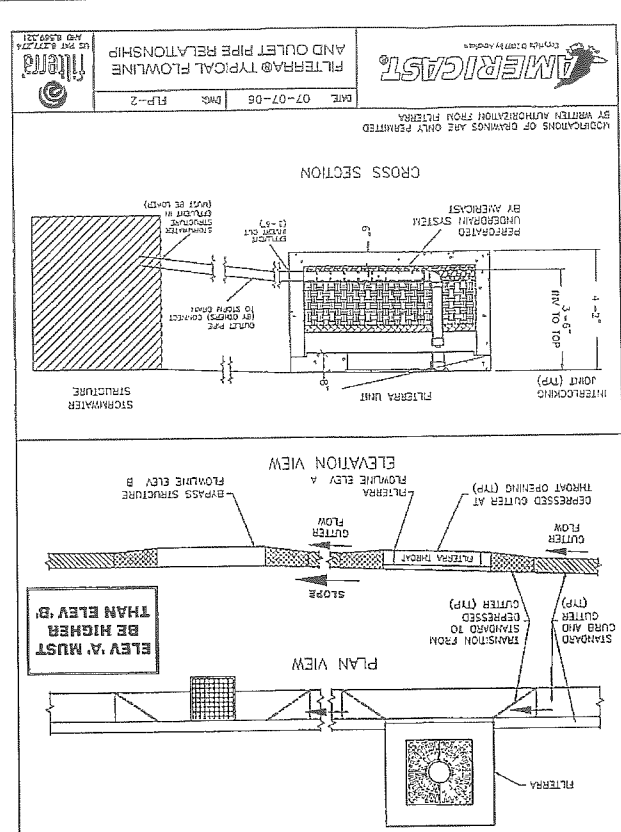
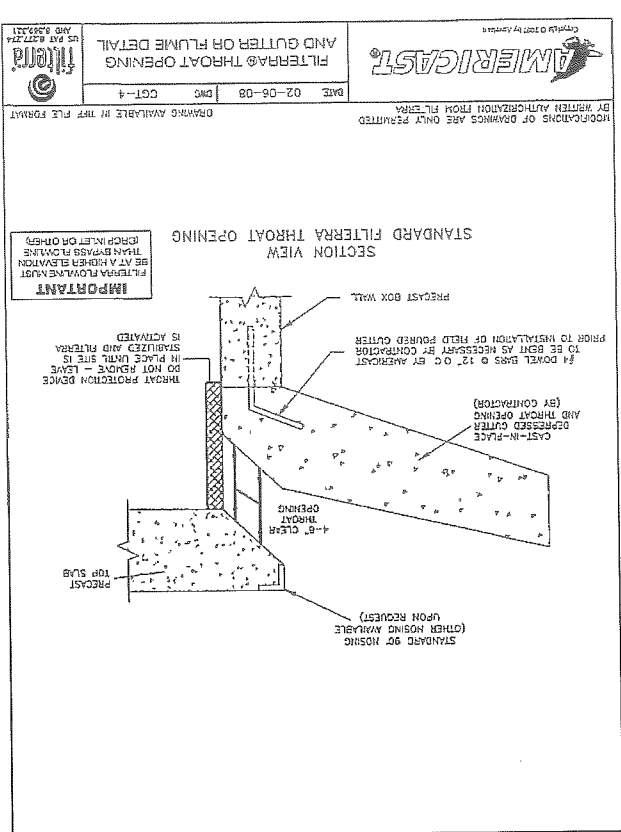
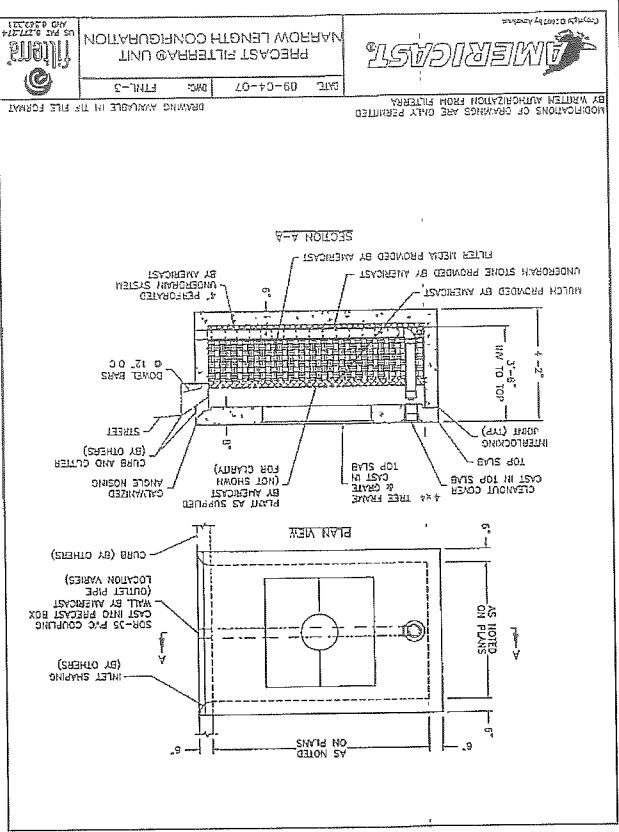
MAINE REGISTERED PROFESSIONAL ENGINEER

NO. 18884

JOHN CHAU

4/12/12

C.8



TRITON FOODMART / GAS STATION
OF
188 FOREST AVE
PORTLAND, MAINE
JOHN CHAU
75 ACADIA ST
PORTLAND, MAINE 04103

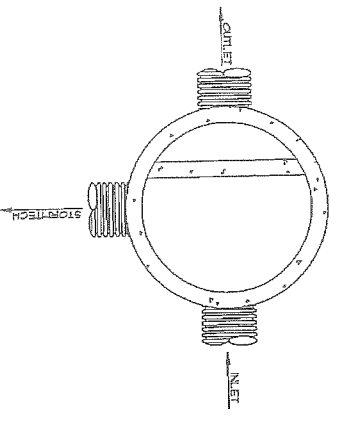
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250 Oakdale Road, Portland, ME 04103
Tel: (207) 758-4277
www.sebagoengineers.com

PROJECT NO: FIELDBOOK DESIGN: CHKD: DRAWN: JRS
DATE: 11/2/11

C.9

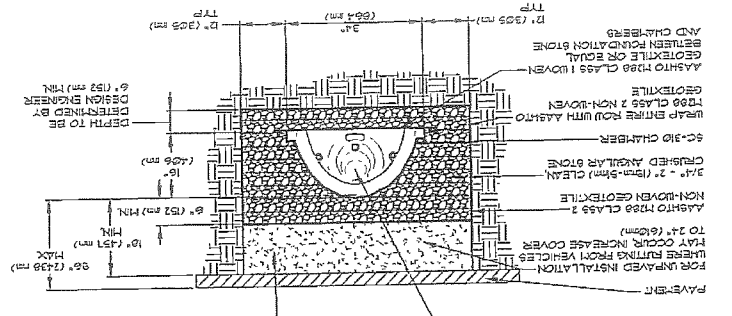
NOT TO SCALE
DIVERSION MANHOLE DETAIL (TYPICAL)

NOTES
 1. ALL PIPES FROM STOR-TTECH CHAMBERS AND DIVERSION MANHOLE PIPE ORIENTATION AND INVERT AND BIRL ELEVATIONS.
 2. SEE GRADING AND UTILITY PLAN SHEET 4 OF 9 FOR DIVERSION MANHOLE PIPE ORIENTATION AND INVERT AND BIRL ELEVATIONS.

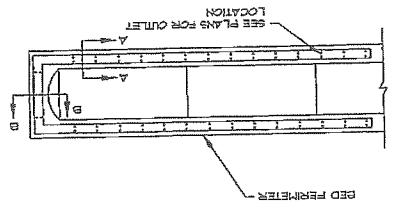
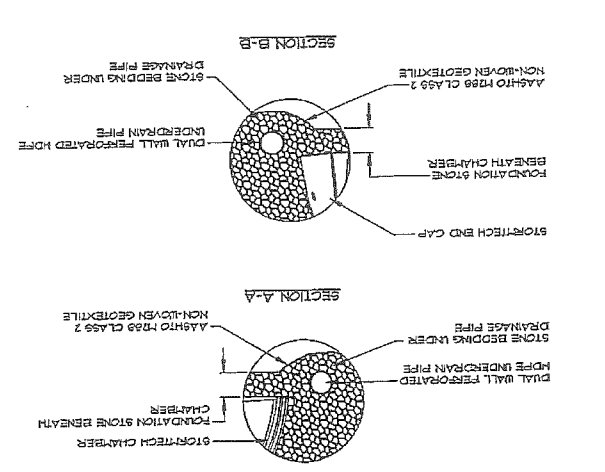


NOT TO SCALE
9C-310 STANDARD CROSS SECTION

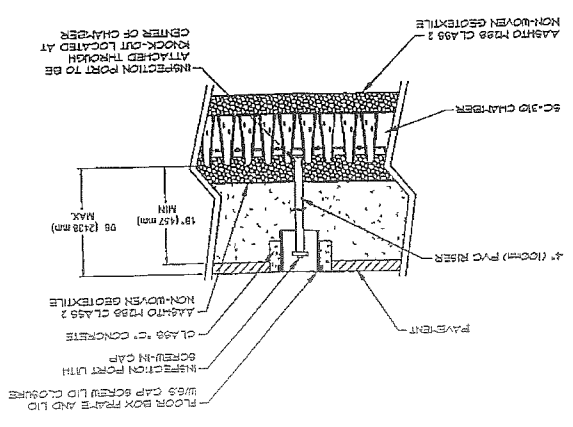
1. THE INSTALLED CHAMBERS SYSTEM SHALL PROVIDE THE LOAD FACTORS SPECIFIED IN THE ASPHALT LFB BRIDGE DESIGN SPECIFICATIONS SECTION 112 FOR EARTH AND LIVE LOADS, WITH CONSIDERATION FOR IMPACT AND MULTIPLE VEHICLE PRESENCE.
 2. CHAMBERS SHALL MEET ASTM F 1487-03 STAKEOUT SPECIFICATION FOR POLYPROPYLENE (PP) CONSOLIDATED WALL STAKEOUT COLLECTION CHAMBERS.
 3. THE DESIGN ENGINEER IS RESPONSIBLE FOR DETERMINING THE REQUIRED BEARING CAPACITY OF SUBGRADE SOILS.
 4. THIS SECTION DETAILS THE REQUIREMENTS NECESSARY TO SATISFY THE LOAD FACTORS SPECIFIED IN THE ASPHALT LFB BRIDGE DESIGN SPECIFICATIONS SECTION 112 FOR EARTH AND LIVE LOADS USING STOR-TTECH CHAMBERS.



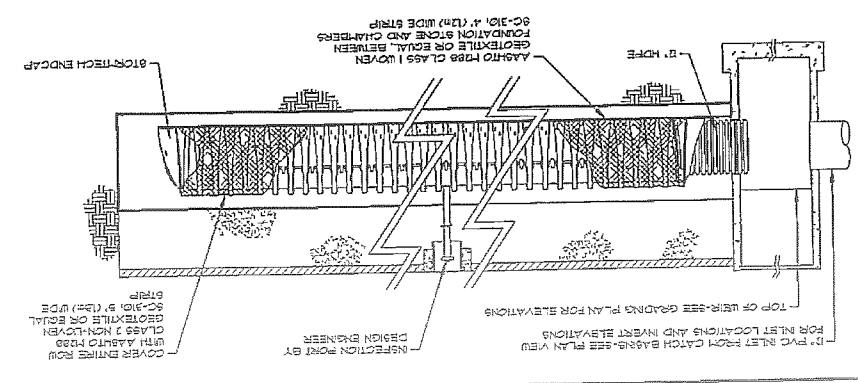
NOT TO SCALE
9C-310 UNDERDRAIN



NOT TO SCALE
9C-310 INSPECTION PORT



NOT TO SCALE
ISOLATOR ROW PROFILE



DATE SCALE AS NOTED
 03/11

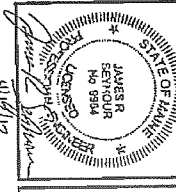
DETAILS OF
TRITON FOODMART / GAS STATION
 1804 FOREST AVE
 PORTLAND, MAINE
 FOR: **JOHN CHAU**
 76 ACADIA ST
 PORTLAND, MAINE 04103

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 One Oyster Street
 Westbrook, ME 04092-1339
 Tel: (207) 568-8277
 Fax: (207) 568-8281
 www.sebagotech.com

PROJECT NO: 11142 FIELD BOOK: DESIGN: SAG DRAWN: JRS/SLR

REV.	BY	DATE	STATUS
E	JRS	04/12/12	REVISED SITE PLAN/REMOVED COPPER DRIVE THRU
D	JRS	03/19/12	REVISED PER FINAL PLAN COMMENTS
C	JRS	03/06/12	SUBMITTED FOR FINAL PLAN REVIEW
B	JRS	01/10/12	RESUBMITTED FOR PRELIMINARY REVIEW
A	JRS	11/15/11	SUBMITTED FOR PRELIMINARY REVIEW

THIS PLAN SHALL NOT BE ADJUSTED WITHOUT WRITTEN PERMISSION FROM SEBAGO TECHNICS, INC. ANY ALTERATIONS AUTHORIZED OR OTHERWISE SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC.



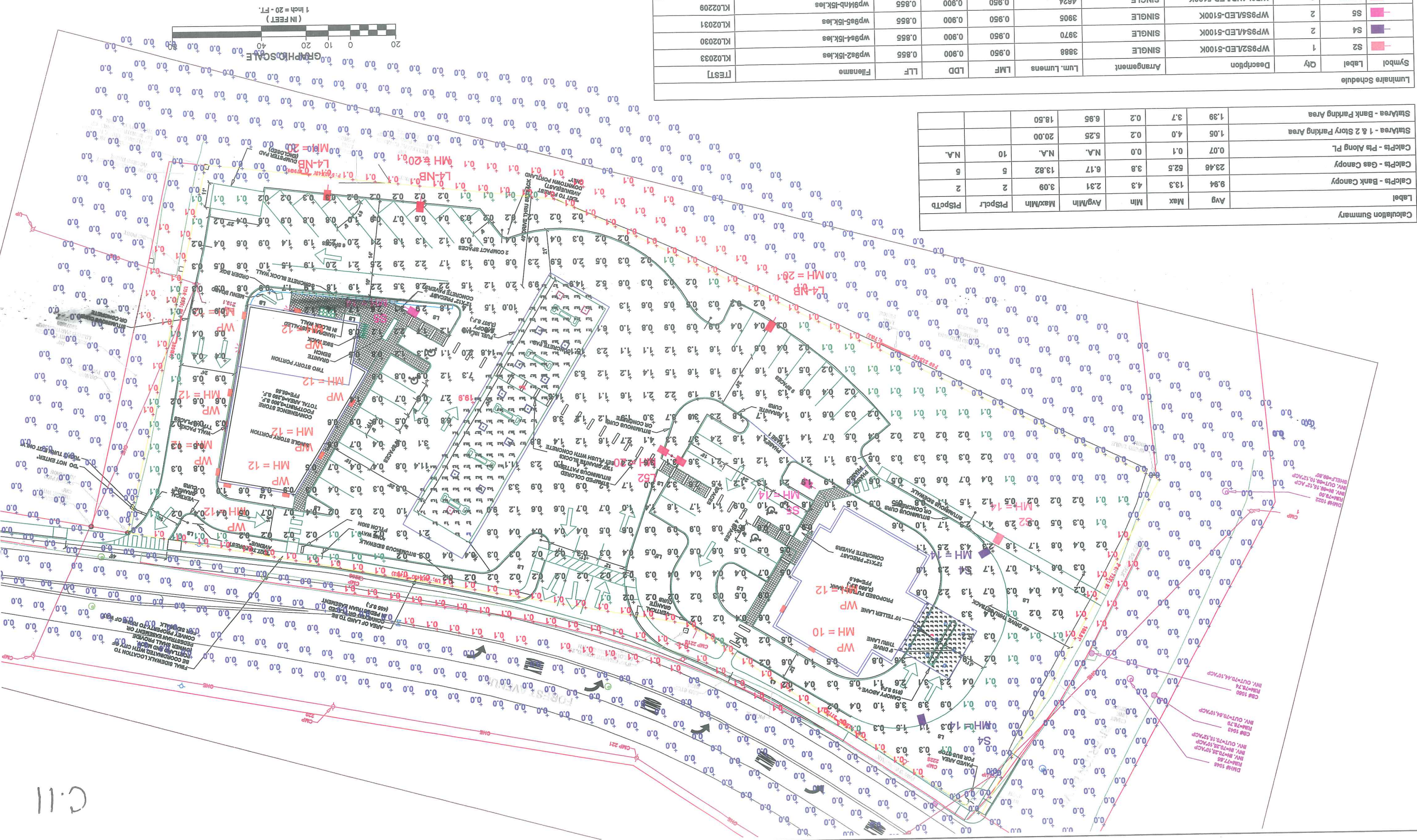
C.10
 4/12/12

Symbol	Label	Qty	Description	Arrangement	Lum. Lumens	LMF	LDP	LLF	Filename	[TEST]
S2	WP9S2LED-5100K	1	SINGLE	0.950	0.900	0.855			WP9S2-5k.ies	KL02033
S4	WP9S4LED-5100K	2	SINGLE	0.950	0.900	0.855			WP9S4-5k.ies	KL02030
S5	WP9S5LED-5100K	2	SINGLE	0.950	0.900	0.855			WP9S5-5k.ies	KL02031
L4-NB	WP9L4NBLED-5100K	3	SINGLE	0.950	0.900	0.855			WP9L4NB-5k.ies	KL02209
L52	WP9L5LED-5100K	1	BACK-BACK	0.950	0.900	0.855			WP9L5-5k.ies	KL02041
C1	UCL LED - 60-G-700-F	10	SINGLE	0.950	0.900	0.855			UCL LED - 60-G-700-F50.IES	3173-24-OW
C2	UCL LED - 36-G-700-F	3	SINGLE	0.950	0.900	0.855			UCL LED - 36-G-700-F50.IES	3173-24-OW-36
C3	LFL6LED 6LFL6D6 40K	9	SINGLE	1.000	1.000	1.000			LFL6Frame LFL6LED 40K.IES	3333
WP	LNC5LU5KAD	10	SINGLE	0.950	0.900	0.855			LNC5LU5KAD.IES	4363

Label	Avg	Min	Avg/Min	Max/Min	Pispctlr	Pispctb
Calculation Summary	9.94	13.3	4.3	2.31	3.09	2
Calcifs - Bank Canopy	23.46	52.5	3.8	6.17	13.82	5
Calcifs - Gas Canopy	0.07	0.1	0.0	N.A.	N.A.	10
Calcifs - Pts Along Pl	1.05	4.0	0.2	5.25	20.00	
StarArea - 1 & 2 Story Parking Area	1.39	3.7	0.2	6.95	18.50	

DATE	BY	REVISION
1-26-12	YMY	N/A
1-27-12	YMY	AS NOTED
1257758		

PHOTOMETRIC STUDY
CHAU GAS STATION

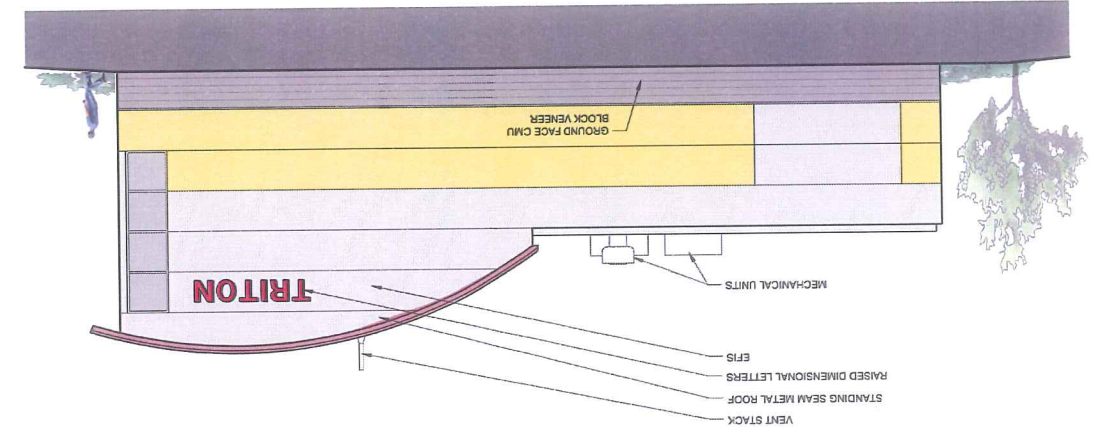


C.11

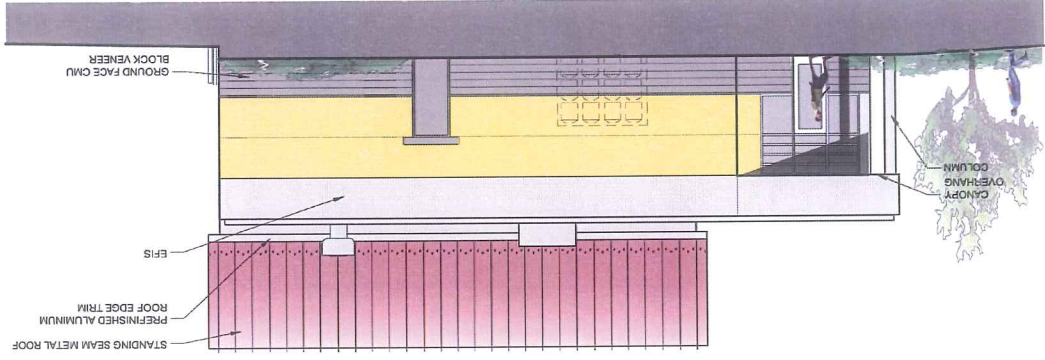
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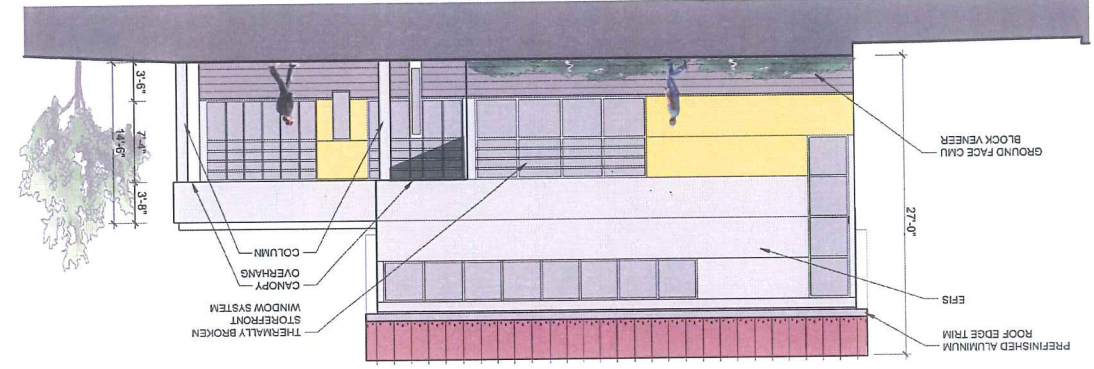
EAST ELEVATION



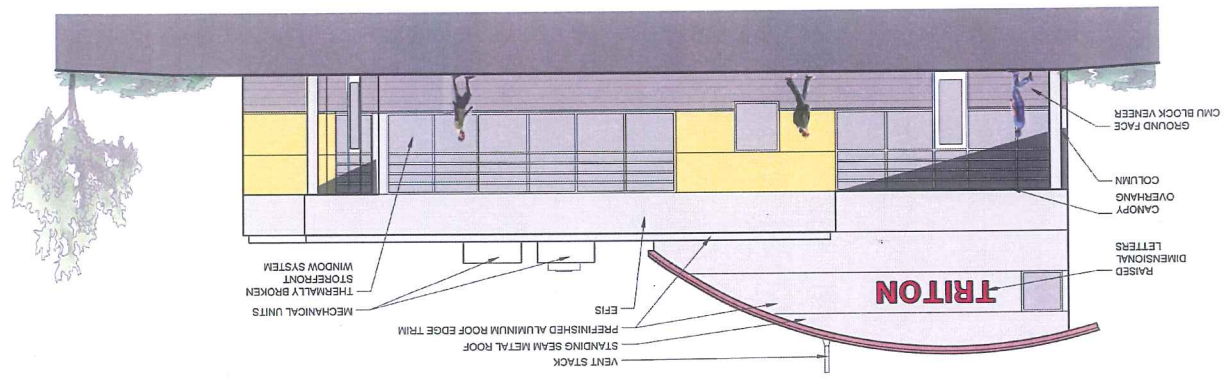
SOUTH ELEVATION



NORTH ELEVATION



WEST ELEVATION



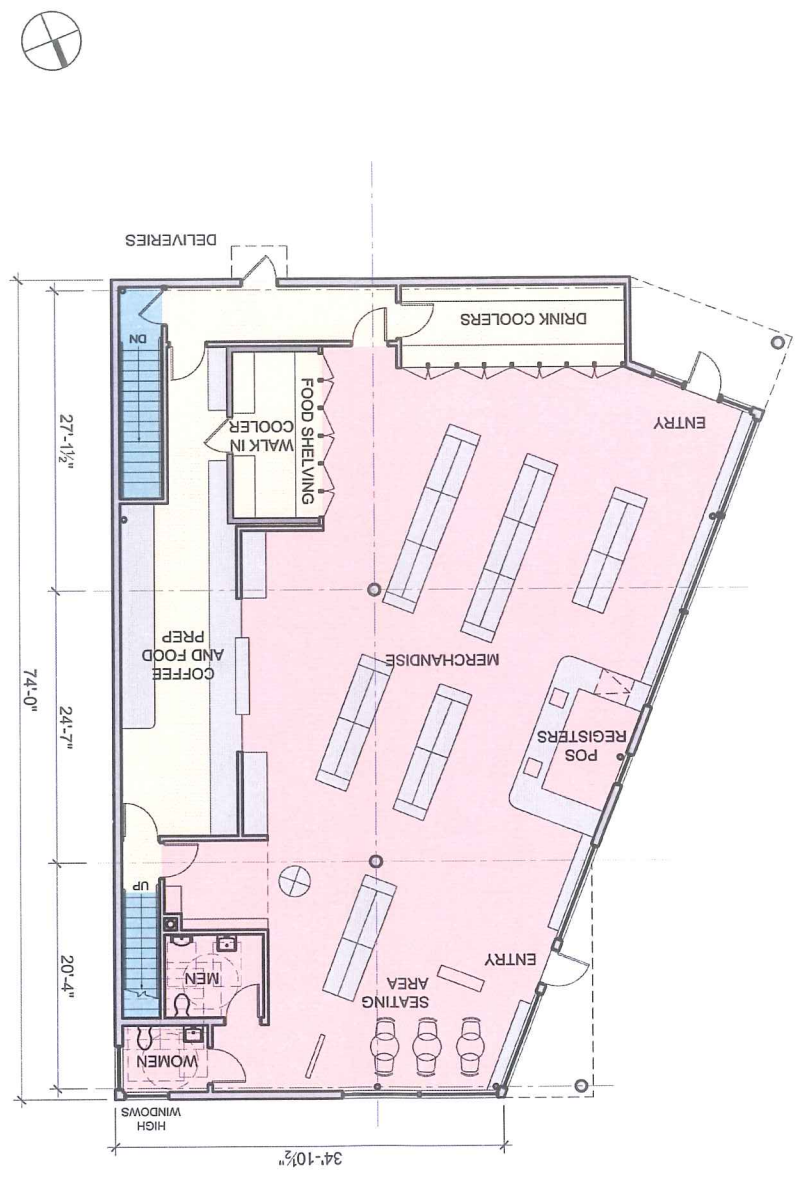
1884 FOREST AVENUE GAS STATION / CONVENIENCE STORE PORTLAND, MAINE



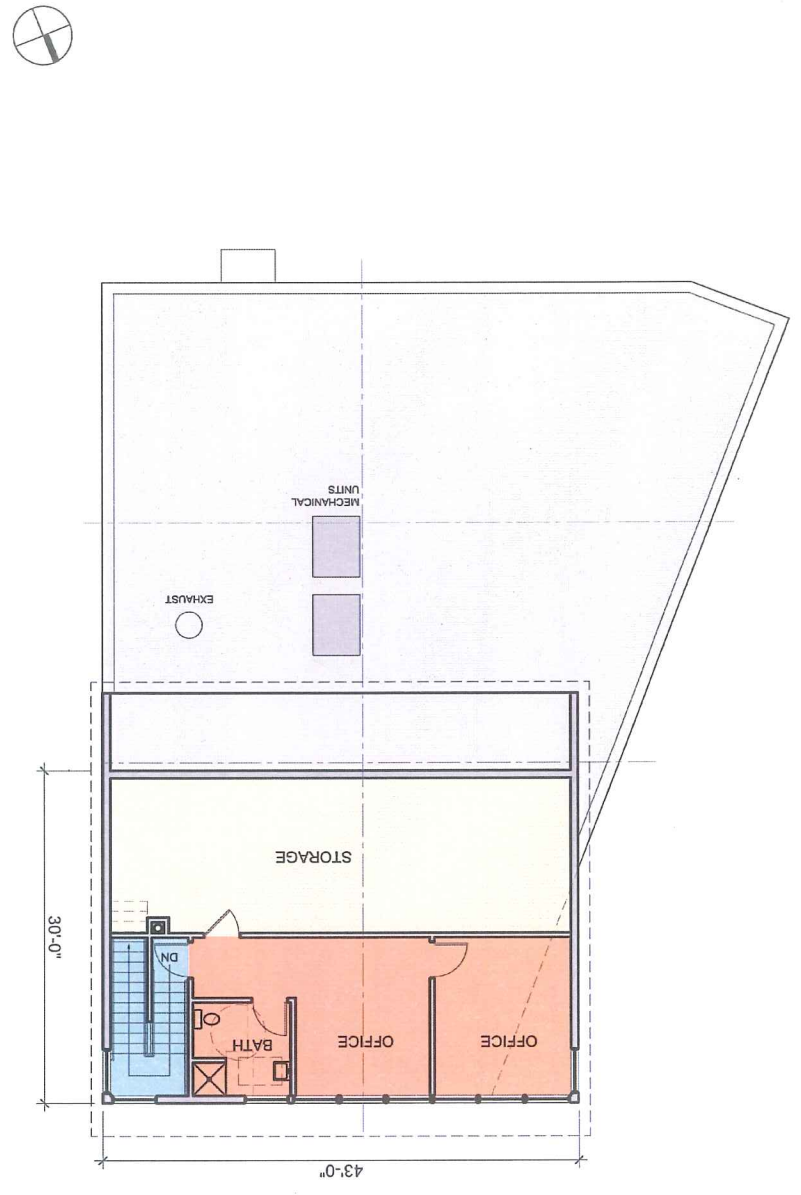
BASEMENT PLAN



FIRST FLOOR PLAN



SECOND FLOOR PLAN



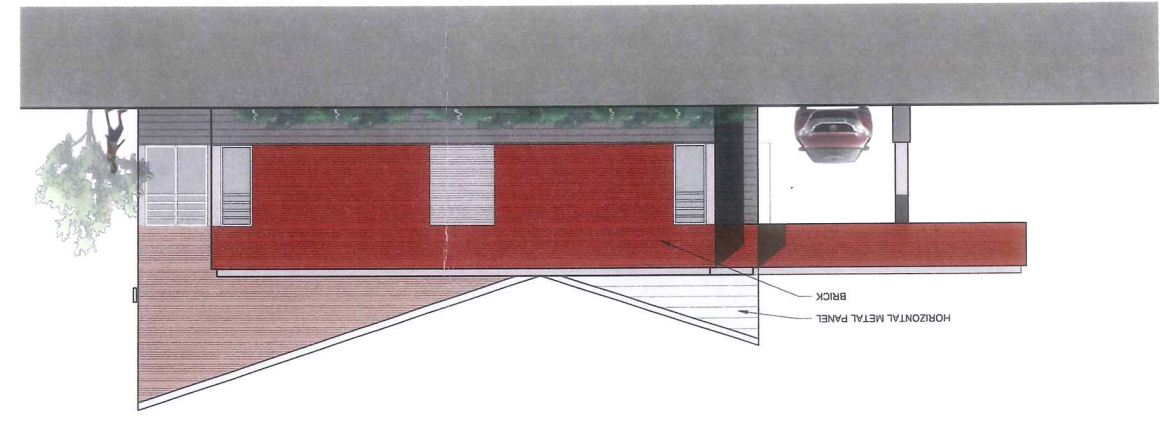
BRANCH BANK 1884 FOREST AVENUE PORTLAND, MAINE



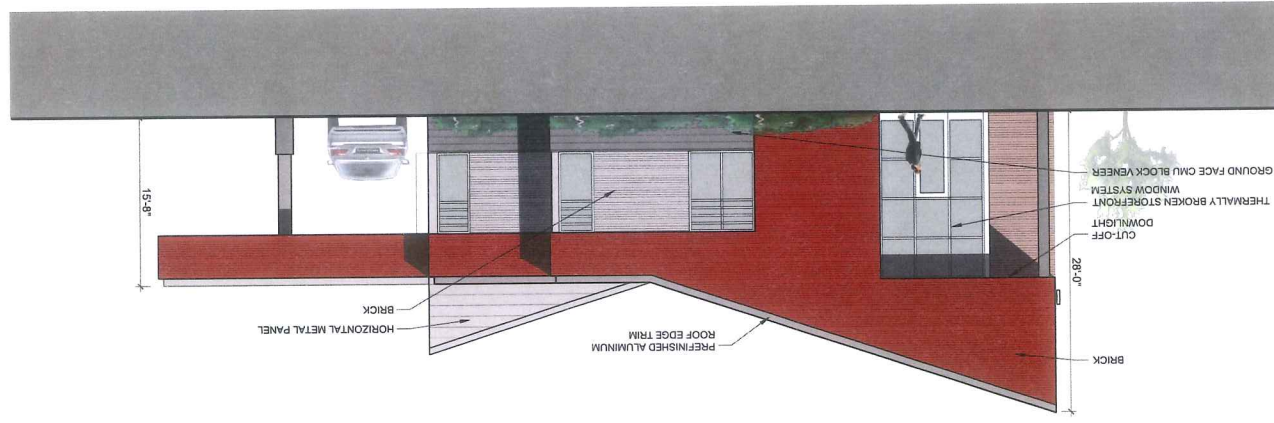
EAST ELEVATION



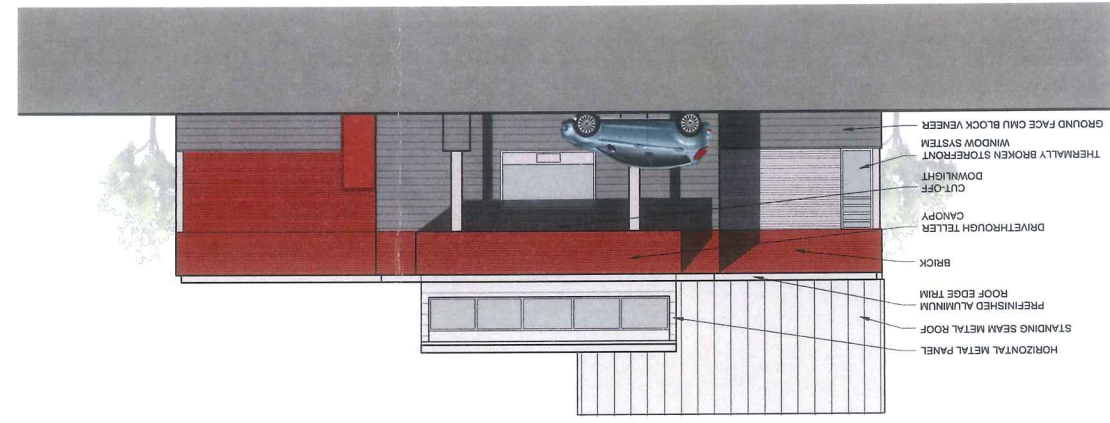
SOUTH ELEVATION



NORTH ELEVATION



WEST ELEVATION

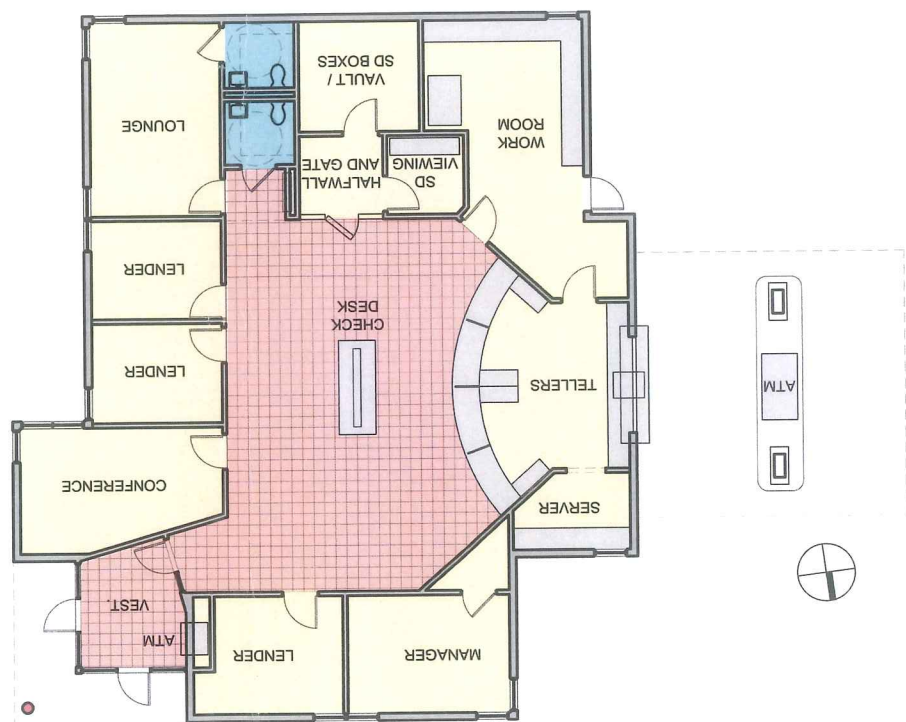


G.14

BRANCH BANK 1884 FOREST AVENUE PORTLAND, MAINE



FLOOR PLAN



PROPOSED SITE IMPROVEMENT PLANS

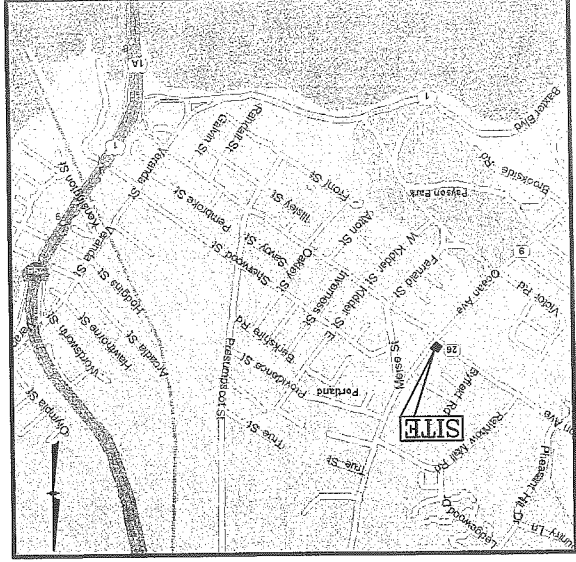
for

ASSESSORS MAP 163 BLOCK B LOTS 3, 4, 5, 14, 15 & 17
 801 WASHINGTON AVENUE - ROUTE 26
 PORTLAND, MAINE

Prepared for:



100 CROSSING BLVD, FRAMINGHAM, MASSACHUSETTS 01702



INDEX TO DRAWINGS

1	TITLE SHEET
2	EXISTING CONDITIONS PLAN
3	DEMOLITION PLAN
4	SITE PLAN
4A	GRADING & EROSION CONTROL PLAN
5	LANDSCAPE PLAN
5A	DETAIL SHEET
6	CANOPY ELEVATIONS
7	EXISTING/PROPOSED SIGN DRAWING
8	FLOOR PLAN (A11) (AHARONIAN)
9	BUILDING ELEVATIONS (A3.1) (AHARONIAN)
10	BUILDING ELEVATIONS (A3.2) (AHARONIAN)
11	LIFE SAFETY PLAN (LS11) (AHARONIAN)
1 OF 1	LIGHTING PLAN (LSI)

DATE	REV. BY	DESCRIPTION
3/17/11	CMT	ADD SHT 5A, REV SHTS 1, 3-11
9/6/11	CMT	REV. SHEETS 3-5
3/29/12	CMT	REV. SHEETS 3-5A, 9 & LSI

31,972 SQUARE FEET
0.734 ACRES
V# 1079
Store# 5604
Gas Station# 851839

REVISIONS
31,972 SQUARE FEET
0.734 ACRES
V# 1079
Store# 5604
Gas Station# 851839

TITLE SHEET
3/29/12
DATE: OCTOBER 5, 2009
FILE: 2666V.R.dwg
DRAWN BY: MHF
CHECKED BY:

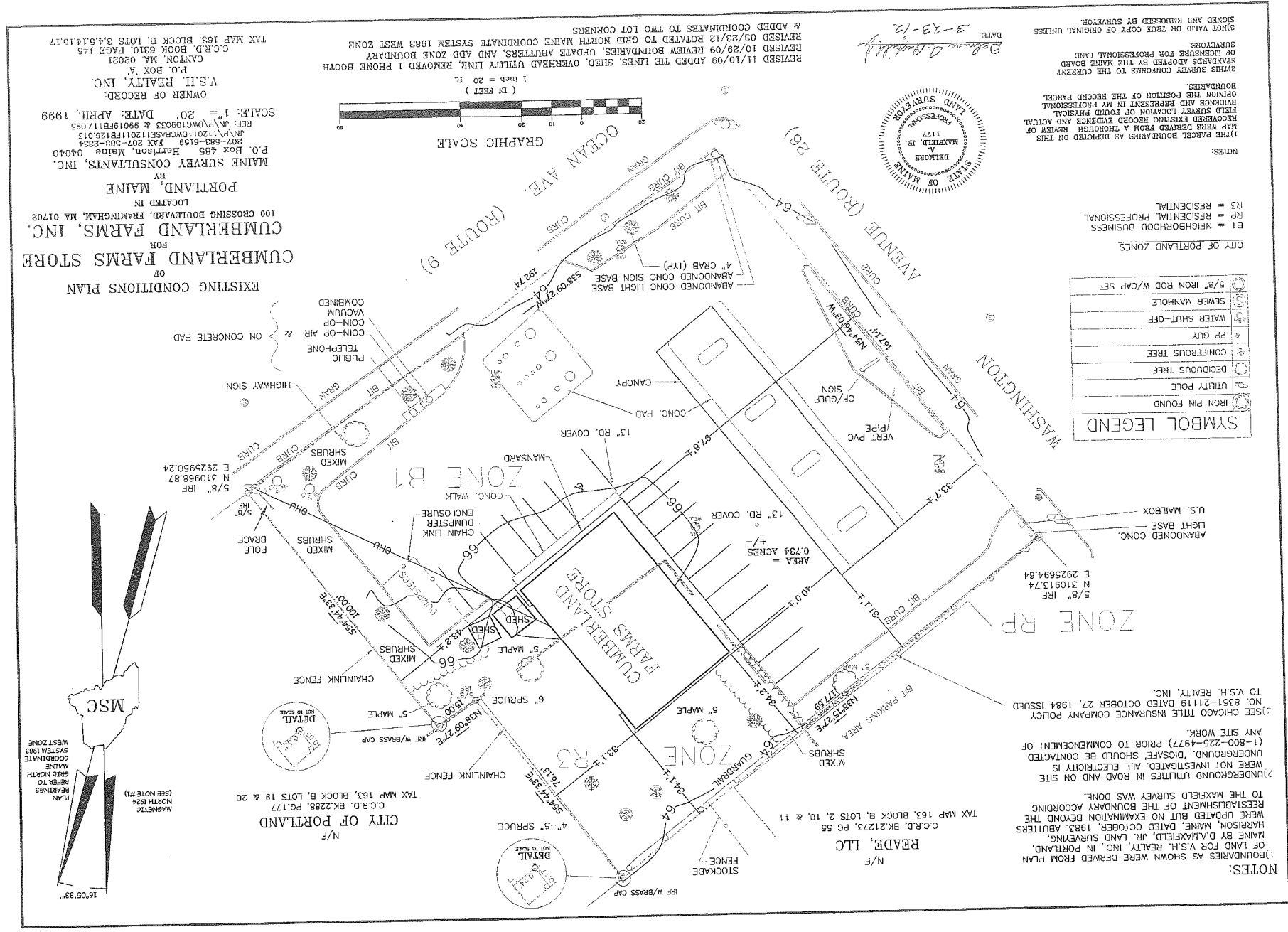
MHF Design Consultants, Inc.
44 Silas Road, Suite One
Salmon, New Hampshire 03079
ENGINEERS • PLANNERS • SURVEYORS
(603) 893-0720
www.mhfdesign.com

Cumberland Farms
100 CROSSING BLVD.
FRAMINGHAM, MA 01702
DATE: OCTOBER 5, 2009
FILE: 2666V.R.dwg
DRAWN BY: MHF
CHECKED BY:



Attachment C.1

Attachment C.2



SYMBOL LEGEND

○	IRON PIN FOUND
○	UTILITY POLE
○	DECIDUOUS TREE
○	CONIFEROUS TREE
○	PP GUY
○	WATER SHUT-OFF
○	SEWER MANHOLE
○	5/8" IRON ROD W/CAP SET

NOTES:

- BOUNDARIES AS SHOWN WERE DERIVED FROM PLAN OF LAND FOR V.S.H. REALTY, INC. IN PORTLAND, MAINE BY D.A. MAXFIELD, JR. LAND SURVEYING, HARRISON, MAINE, DATED OCTOBER, 1983. ABUTTERS WERE UPDATED BUT NO EXAMINATION BEYOND THE REESTABLISHMENT OF THE BOUNDARY ACCORDING TO THE MAXFIELD SURVEY WAS DONE.
- UNDERGROUND UTILITIES IN ROAD AND ON SITE WERE NOT INVESTIGATED. ALL ELECTRICITY IS UNDERGROUND. "DIGSAFE" SHOULD BE CONTACTED (1-800-225-4977) PRIOR TO COMMENCEMENT OF ANY SITE WORK.
- SEE CHICAGO TITLE INSURANCE COMPANY POLICY NO. 6331-21119 DATED OCTOBER 27, 1984 ISSUED TO V.S.H. REALTY, INC.

CITY OF PORTLAND ZONES

B1 = NEIGHBORHOOD BUSINESS
 RP = RESIDENTIAL PROFESSIONAL
 R3 = RESIDENTIAL

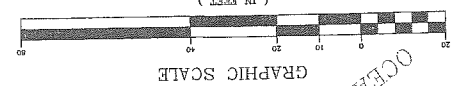
NOTES:

- THE PARCEL BOUNDARIES AS DEPICTED ON THIS MAP WERE DERIVED FROM A THROUGH REVIEW OF RECORDED EXISTING RECORD EVIDENCE AND ACTUAL FIELD SURVEY LOCATION OF FOUND PHYSICAL EVIDENCE AND REPRESENT IN MY PROFESSIONAL OPINION THE POSITION OF THE RECORD PARCEL BOUNDARIES.
- THIS SURVEY CONFORMS TO THE CURRENT STANDARDS ADOPTED BY THE MAINE BOARD OF LICENSURE FOR PROFESSIONAL LAND SURVEYORS.
- NOT VALID OR TRUE COPY OF ORIGINAL UNLESS SIGNED AND EMBOSSED BY SURVEYOR.

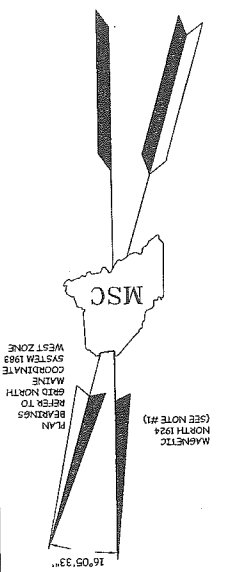


DATE: 5-23-12
 [Signature]

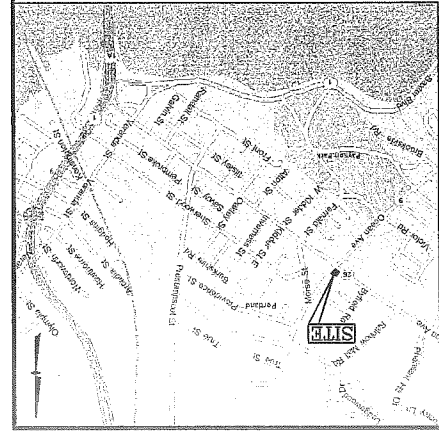
REVISED 11/10/09 ADDED THE LINES, SHED, OVERHEAD UTILITY LINE, REMOVED 1 PHONE BOOTHS
 REVISED 10/29/09 REVIEW BOUNDARIES, UPDATE ABUTTERS, AND ADD ZONE BOUNDARY
 REVISED 03/23/12 ROTATED TO GRID NORTH MAINE COORDINATE SYSTEM 1983 WEST ZONE
 & ADDED COORDINATES TO TWO LOT CORNERS



CUMBERLAND FARMS STORE
 OR
EXISTING CONDITIONS PLAN
 FOR
CUMBERLAND FARMS, INC.
 LOCATED IN
 100 CROSSING BOULEVARD, FRAMINGHAM, MA 01702
 PORTLAND, MAINE
 BY
MAINE SURVEY CONSULTANTS, INC.
 P.O. Box 485, Harrison, Maine 04040
 REF: MNP\DMG109023 & 99019FB117.095
 SCALE: 1" = 20' DATE: APRIL, 1999
 OWNER OF RECORD:
V.S.H. REALTY, INC.
 P.O. Box N,
 CANTON, MA 02021
 C.R.D. BOOK 6310, PAGE 145
 TAX MAP 163, BLOCK B, LOTS 3,4,5,14,15,17



CITY OF PORTLAND
 N/F
 C.R.D. BK. 2288, PG. 177 & 20
 TAX MAP 163, BLOCK B, LOTS 19 & 20
 REFER TO GRID NORTH COORDINATE SYSTEM 1983 WEST ZONE



Attachment C.5

LOCATION MAP
(NOT TO SCALE)

NOTES:

- 1) ALL SITE DRAINAGE PIPE SHALL BE CONCRETE HIGH-DENSITY POLYETHYLENE PIPE WITH STAINLESS-STEEL-GROOVED INTERIOR. AS MANUFACTURED BY ADS, INC. OR APPROVED EQUAL. UNLESS OTHERWISE NOTED ON PLAN.
- 2) ALL ROOF AND CANOPY DRAIN PIPE SHALL BE 6" PVC(SDR-35) EXCEPT WITHIN 10 FEET OF A BUILDING FOUNDATION WHERE CAST IRON PIPE SHALL BE USED. MIN. SLOPE=2.0%.
- 3) ALL SANITARY SEWER PIPE SHALL BE PVC (SDR-35), UNLESS OTHERWISE NOTED.
- 4) ALL WATER PIPE SHALL BE COPPER, UNLESS OTHERWISE NOTED.
- 5) ELEVATIONS ARE BASED ON NAD 1929 DATUM.
- 6) ALL PROPOSED ELEVATIONS AS SHOWN ARE BOTTOM OF CURB ELEVATIONS, UNLESS OTHERWISE NOTED.
- 7) ANY UTILITY FIELD ADJUSTMENTS SHALL BE APPROVED BY THE LOCAL AUTHORITIES AND THE DEVELOPER PRIOR TO INSTALLATION.
- 8) THE LOCATIONS OF UNDERGROUND UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR IS TO VERIFY EXACT LOCATION PRIOR TO CONSTRUCTION. CONSTRUCTION SHALL COMMENCE BEGINNING AT THE LOWEST INVERT OF CONNECTION) AND PROGRESS UP GRADIENT. PROPOSED INTERFACES (CROSSINGS) WITH EXISTING UNDERGROUND INSTALLATIONS SHALL BE FIELD VERIFIED BY TEST PIT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- 9) ALL CONSTRUCTION SHALL CONFORM TO MUNICIPAL DPW AND ALL APPLICABLE STATE AND FEDERAL STANDARDS.
- 10) THE CONTRACTOR SHALL CALL AND COORDINATE WITH DIG-SAFE (1-888-344-7233) PRIOR TO COMMENCING ANY EXCAVATION.
- 11) ALL WATER AND SEWER CONSTRUCTION SHALL CONFORM TO DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS.
- 12) ALL ELECTRICAL, TELEPHONE AND CABLE TV LINES ARE TO BE UNDERGROUND AND INSTALLED IN CONFORMANCE WITH APPLICABLE UTILITY CO. SPECIFICATIONS.
- 13) THIS SITE WILL NOT REQUIRE A USEPA PERMIT FOR STORMWATER DISCHARGE FOR THE CONSTRUCTION SINCE THE DISTURBANCE DOES NOT EXCEED ONE ACRE.
- 14) THE CONTRACTOR IS TO COORDINATE WITH THE MUNICIPAL DPW REGARDING PRESSURE REDUCING VALVE IS REQUIRED.
- 15) ANY UTILITIES TO BE TAKEN OUT OF SERVICE SHALL BE DISCONNECTED AS DIRECTED BY UTILITY COMPANY AND LOCAL DPW.
- 16) ALL TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION SIGNAGE ARRANGEMENTS, ACCEPTABLE TO TOWN AND PORTLAND DEPARTMENT OF PUBLIC WORKS, SHALL BE EMPLOYED DURING OPERATIONS WITHIN THE PUBLIC RIGHT-OF-WAY.
- 17) ALL ADA ACCESSIBLE WALKWAYS CANNOT EXCEED 5% RUNNING SLOPE AND 2% CROSS SLOPE. RAMP CANNOT EXCEED 8.33% RUNNING SLOPE AND 2% CROSS SLOPE. RAMP SLOPE AND FINISHING SHALL BE ACCESSIBLE CANNOT EXCEED 2% SLOPE IN ANY DIRECTION. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES.

PLAN REFERENCES:

- 1) EXISTING CONDITIONS PLAN OF CUMBERLAND FARMS STORE, PREPARED FOR V.S.H. REALTY, INC., PREPARED BY MAINE SURVEY CONSULTANTS, INC., SCALE: 1"=20'; DATE: APRIL 1999

MHF Design Consultants, Inc. 44 Sills Road, Suite One Salem, New Hampshire 03079 (603) 893-0720 ENGINEERS • PLANNERS • SURVEYORS www.mhfdesign.com	PORTLAND, MAINE 307 Washington Ave. & Ocean Ave.	CUMBERLAND FARMS INC. 100 CROSSING BLVD FRAMINGHAM, MA 01702 DATE: 2665P.dwg DRAWN BY: MHF CHECKED BY:
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DATE	REV. BY	DESCRIPTION
9/6/11	CMT	REV. BLDG FOOTPRINT, MISC. REVS.
3/29/11	CMT	ADDRESS CITY COMMENTS

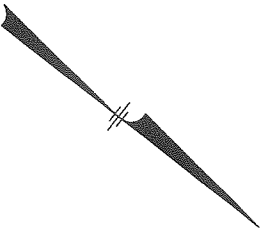
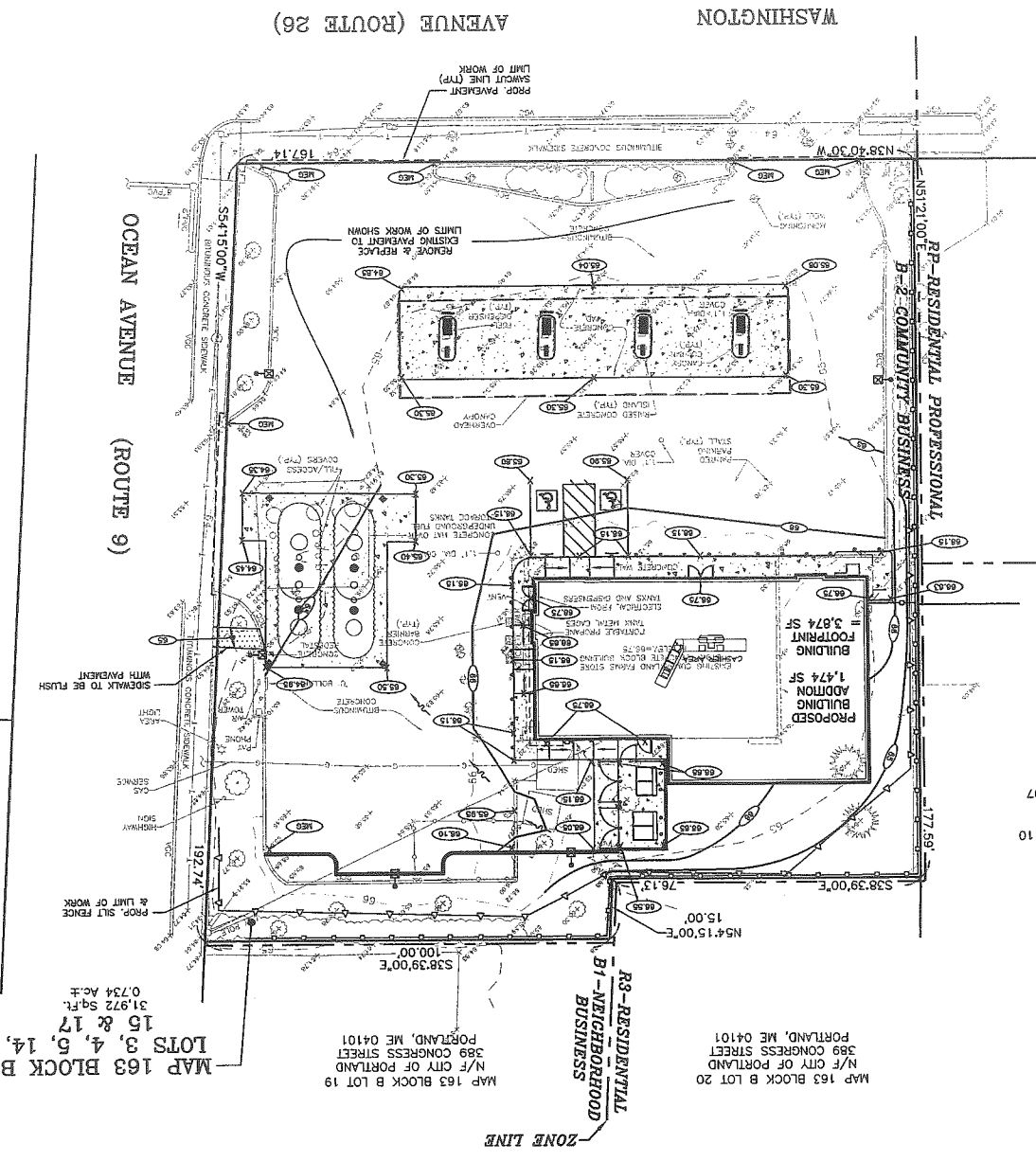
31,972 SQUARE FEET
0.734 ACRES
V# 1079
Store# 5604
Gas Station# 851839

ABUTTER ACROSS INTERSECTION
N/F RONALD L LEMIEUX JTS
26 FOREST LANE
CUMBERLAND, ME 04021

TAX MAP 163A BLOCK A LOT 17
N/F SPURWINK SCHOOL
699 RIVERSIDE STREET
PORTLAND, ME 04103

TAX MAP 163A BLOCK A LOT 1
N/F RONALD L LEMIEUX JTS
26 FOREST LANE
CUMBERLAND, ME 04021

MAP 163 BLOCK B
LOTS 3, 4, 5, 14,
15 & 17
31,972 Sq.Ft.
0.734 Ac.F



TAX MAP 163 BLOCK B LOT 10
N/F RACE, LC
32 DERBROOK FARM
NORTH YARMOUTH, ME 04097

TAX MAP 163 BLOCK B LOT 2
N/F ANGIES SERVICE INC.
10 E BRIDGE STREET
WESTBROOK, ME 04092

RP-RESIDENTIAL
PROFESSIONAL
ZONE LINE

RP-RESIDENTIAL PROFESSIONAL
B-2 COMMUNITY BUSINESS



LEGEND

○	IRON PIN FOUND/SET	○	UTILITY POLE
○	SEWER MANHOLE	○	TELEPHONE MANHOLE
○	OBSERVATION WELL	○	WATER VALVE
○	TREE	○	PROP. CLEANOUT
○	CONTOUR ELEVATION	○	PROP. CATCH BASIN
○	MEET EXISTING GRADE	○	PROP. DRAIN MANHOLE
○	PROP. SPOT ELEVATION	○	PROP. SEWER MANHOLE
○	PROP. CONTOUR ELEVATION	○	PROP. GATE VALVE

ME 04103
1-888-344-7233
NO. 7201
FRANK C. MONTIERO
D.P.E.
STATE OF MAINE

LANDSCAPE PLAN

CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

DATE: OCTOBER 5, 2009
FILE: 266609.dwg
DRAWN BY: MHF
CHECKED BY:

SCALE: 1" = 20'

PORTLAND, MAINE
801 Washington Ave. & Ocean Ave.
MHF Design Consultants, Inc.
www.mhfdesign.com
ENGINEERS • PLANNERS • SURVEYORS

44 Sills Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720

REV. BY	DATE	DESCRIPTION
9/6/11	CMT	REV. BLDG FOOTPRINT, MISC. REVS.
3/29/11	CMT	3/29/11

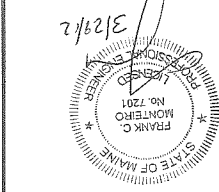
31,972 SQUARE FEET

0.734 ACRES

1079

Store# 5604

Gas Station# 851839



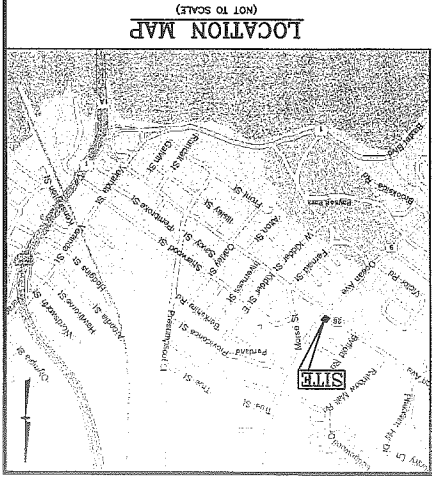
Attachment C.6

PLANTING SCHEDULE

PLANT QTY	BOTANICAL NAME	COMMON NAME	MIN. INSTALL SIZE	MATURE SIZE/REMARKS
3	PICEA PLUGENS GLAUCA	COLORADO BLUE SPRUCE	7' - 8' HT.	- SCREEN-SPECIMEN
18	AZALEA	AZALEA	3'-5' HT. MAX./ROSY-RED FLOWERS (MOTHERS DAY OR SIMILAR)	
2	PHLOX	PHLOX	2'-3' HT. MAX./LAVENDER PINK	
8	THUNBERGIA VITICIFOLIA	GERMAN CUCKOO FLOR	8'-7' HT.	
1	SP. MAX./TIGHT COLUMNAR (SMALLER OR SIMILAR)		3'-4' HT. MAX./TIGHT COLUMNAR (SMALLER OR SIMILAR)	

PLANT DESIGNATION: Proposed sod

- NOTES:**
- 1) ALL PLANT STOCK SHALL CONFORM TO ANSI Z60.1 - NURSERY STOCK, LATEST EDITION (AMERICAN ASSOCIATION OF NURSERMEN, INC.).
 - 2) 4" AGED PINEBARK MULCH AND A WEED BARRIER (TY-PAN FABRIC OR APPROVED EQUIVALENT) SHALL BE APPLIED TO ALL SHRUBS AND GROUNDCOVER BEDS. INSTALL WEED BARRIER AS PER MANUFACTURERS RECOMMENDATIONS.
 - 3) PLANT PITS SHALL BE MIXED AT A RATE OF 7 PARTS OF TOPSOIL TO 2 PARTS OF DEHYDRATED COW MANURE. SLOW RELEASE FERTILIZER SHALL BE APPLIED AS PER MANUFACTURERS RECOMMENDATIONS. USE EXISTING ON-SITE TOPSOIL AS PART OF BACKFILL WHEN AVAILABLE.
 - 4) ALL LANDSCAPED AREAS NOT PLANTED WITH TREES, SHRUBS OR GROUNDCOVER SHALL BE RESTORED WITH SEED OR SOD AS INDICATED ON PLANS.
 - 5) ALL SOO, SEED, SHRUB AND TREE AREAS SHALL RECEIVE 6" PH CORRECTED TOPSOIL. AFTER TOPSOIL IS SPREAD EVENLY OVER ENTIRE AREA, ALL CLODS, LUMPS, STONES AND OTHER DELETERIOUS MATERIAL SHALL BE RAKED UP AND REMOVED.
 - 6) APPLICATION OF GRASS SEED, FERTILIZERS AND MULCH SHALL BE ACCOMPLISHED BY BROADCASTING SEEDING OR HYDROSEEDING AT THE RATES OUTLINED BELOW:
LIME: 100 LBS./1,000 SQUARE FEET.
FERTILIZER: 500 LBS./ACRE OF 10-20-20 OR 1000 LBS./ACRE OF 5-10-10.
MULCH: 100 LBS./ACRE APPROXIMATELY 3 TONS/ACRE.
 - 7) ANY CHANGES IN PLANT LOCATIONS OR TYPES SHALL BE APPROVED BY THE DEVELOPER PRIOR TO INSTALLATION.
 - 8) FOR SEED (4 SOO) AREAS USE EXISTING TOPSOIL, IF AVAILABLE, FOR A 4" DEPTH AND TOP DRESS WITH 2" OF SCREENED TOPSOIL, UNLESS OTHERWISE NOTED ON PLAN. ALL TOP DRESS WITH 2" OF SCREENED TOPSOIL, UNLESS OTHERWISE NOTED ON PLAN. ALL AREAS AS DIRECTED BY DEVELOPER TO MEET MINIMUM REQUIREMENTS.
 - 9) PLANTINGS SHALL BE GUARANTEED BY THE CONTRACTOR FOR ONE YEAR AFTER WRITTEN ACCEPTANCE OF THE DEVELOPER.

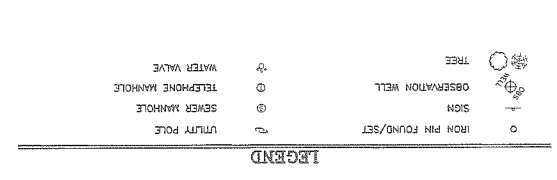
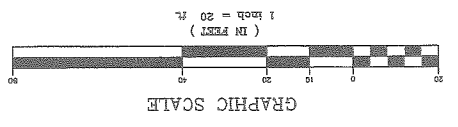
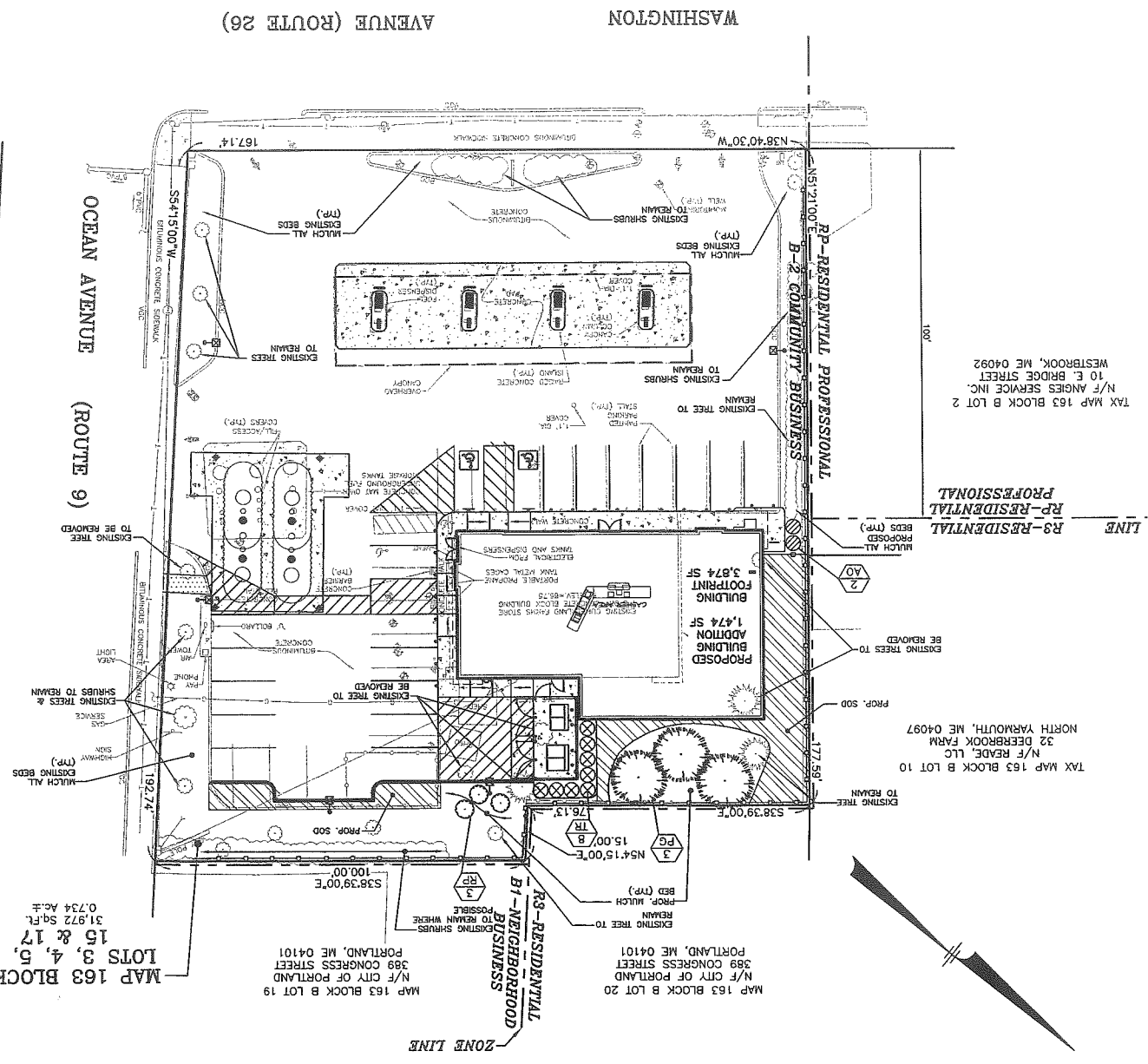


GRUTTER ACROSS INTERSECTION
TAX MAP 163 BLOCK E LOT 6
N/F LAURA ANGELO
28 WHALBOAT ROAD
WESTBROOK, ME 04092

TAX MAP 163A BLOCK A LOT 1
N/F RONALD L. LEMIEUX
CAROL S. LEMIEUX JTS
26 FOREST LANE
CUMBERLAND, ME 04021

TAX MAP 163A BLOCK A LOT 17
N/F SPURINK SCHOOL
899 RIVERSIDE STREET
PORTLAND, ME 04103

MAP 163 BLOCK B
LOTS 3, 4, 5, 14,
15 & 17
0.734 AC. ±
31,972 SQ. FT.



DETAIL SHEET

CUMBERLAND FARMS INC.
100 CROSSING BLVD.
FRAMINGHAM, MA 01702

DATE: MARCH 17, 2011
FILE: 266609.DWG
DRAWN BY: MHF
CHECKED BY:

31,972 SQUARE FEET
0.734 ACRES
V# 1079
Store# 5604
Gas Station# 851839

REVISIONS

DATE	REV. BY	DESCRIPTION
3/29/12	CMT	ADD DETAILS

MHF Design Consultants, Inc.
44 Shilb Road, Suite One
Salem, New Hampshire 03079
(603) 893-0720
www.mhfdesign.com
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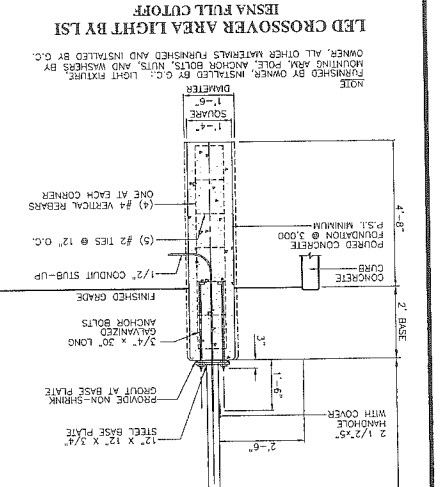
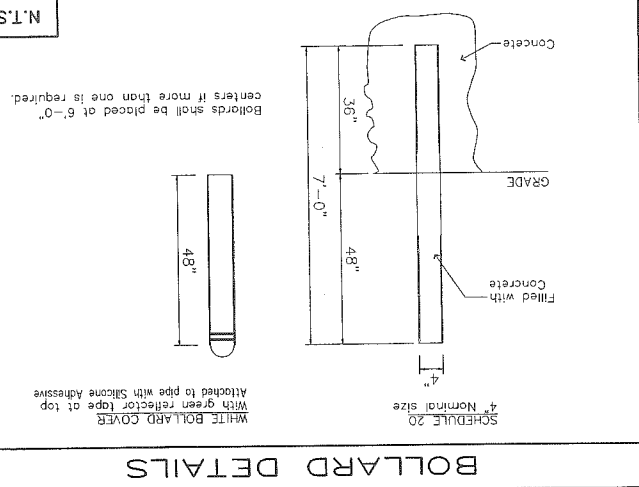
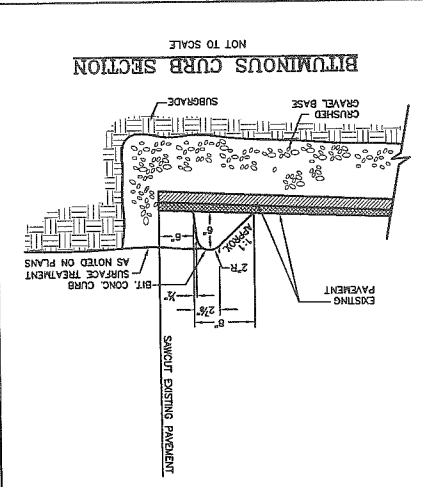
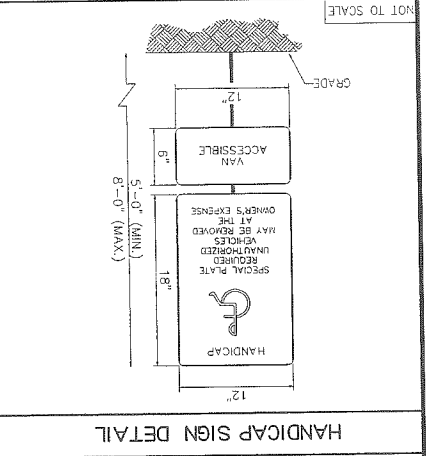
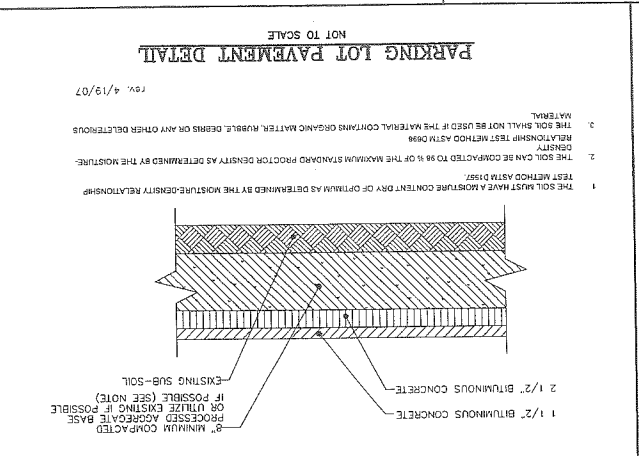
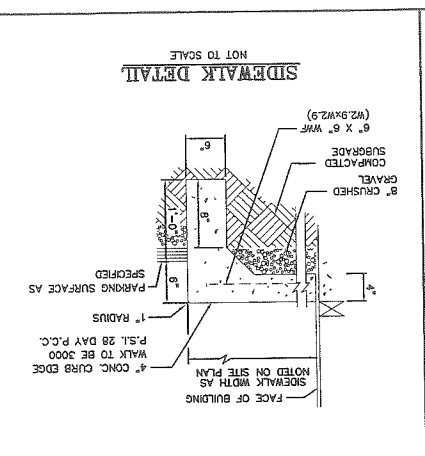
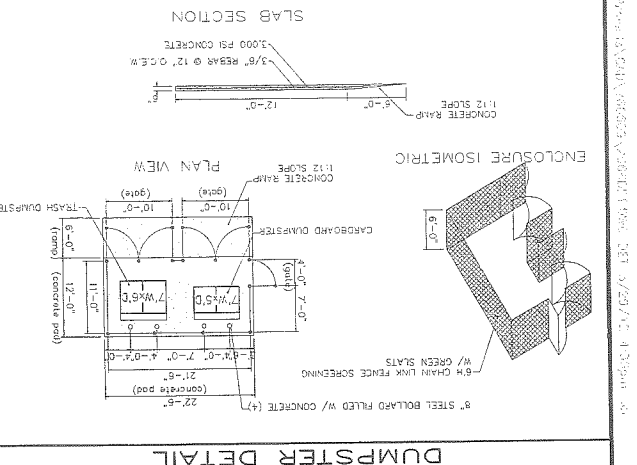
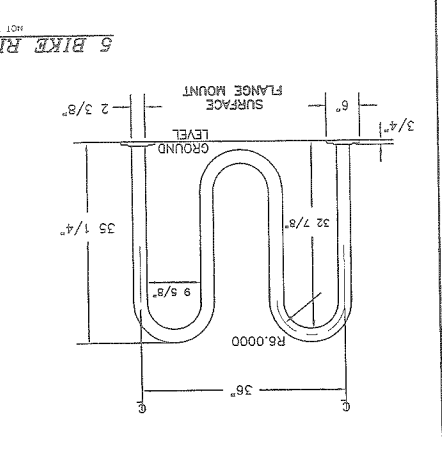
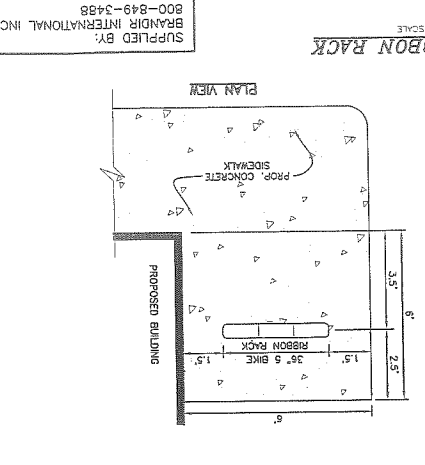
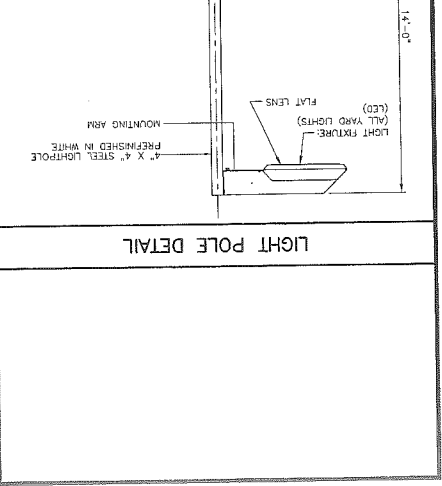
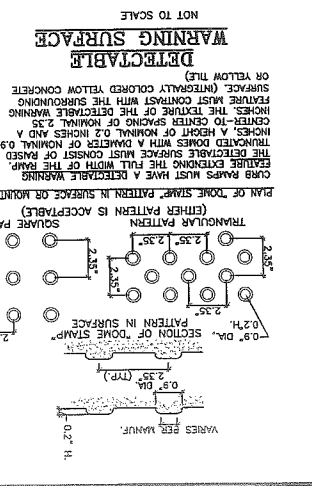
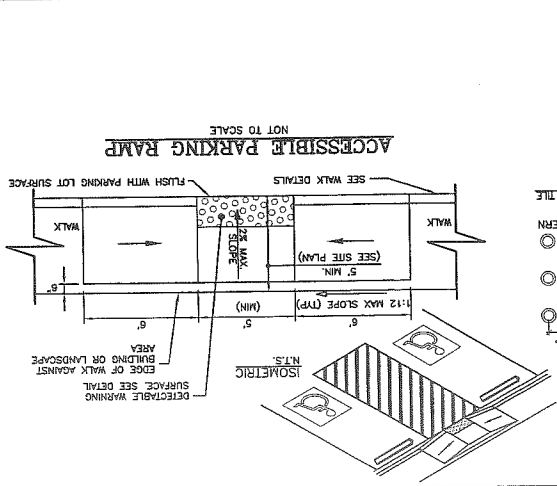
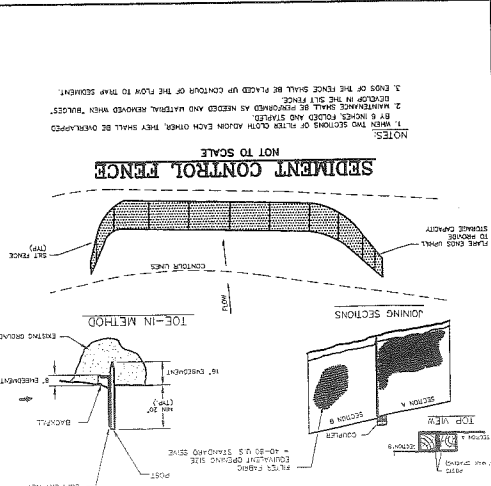
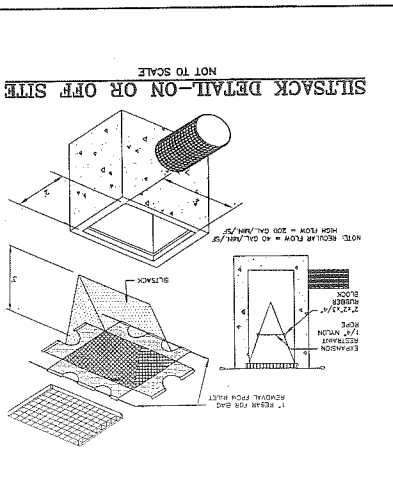
PORTLAND, MAINE
801 Washington Ave. & Ocean Ave.

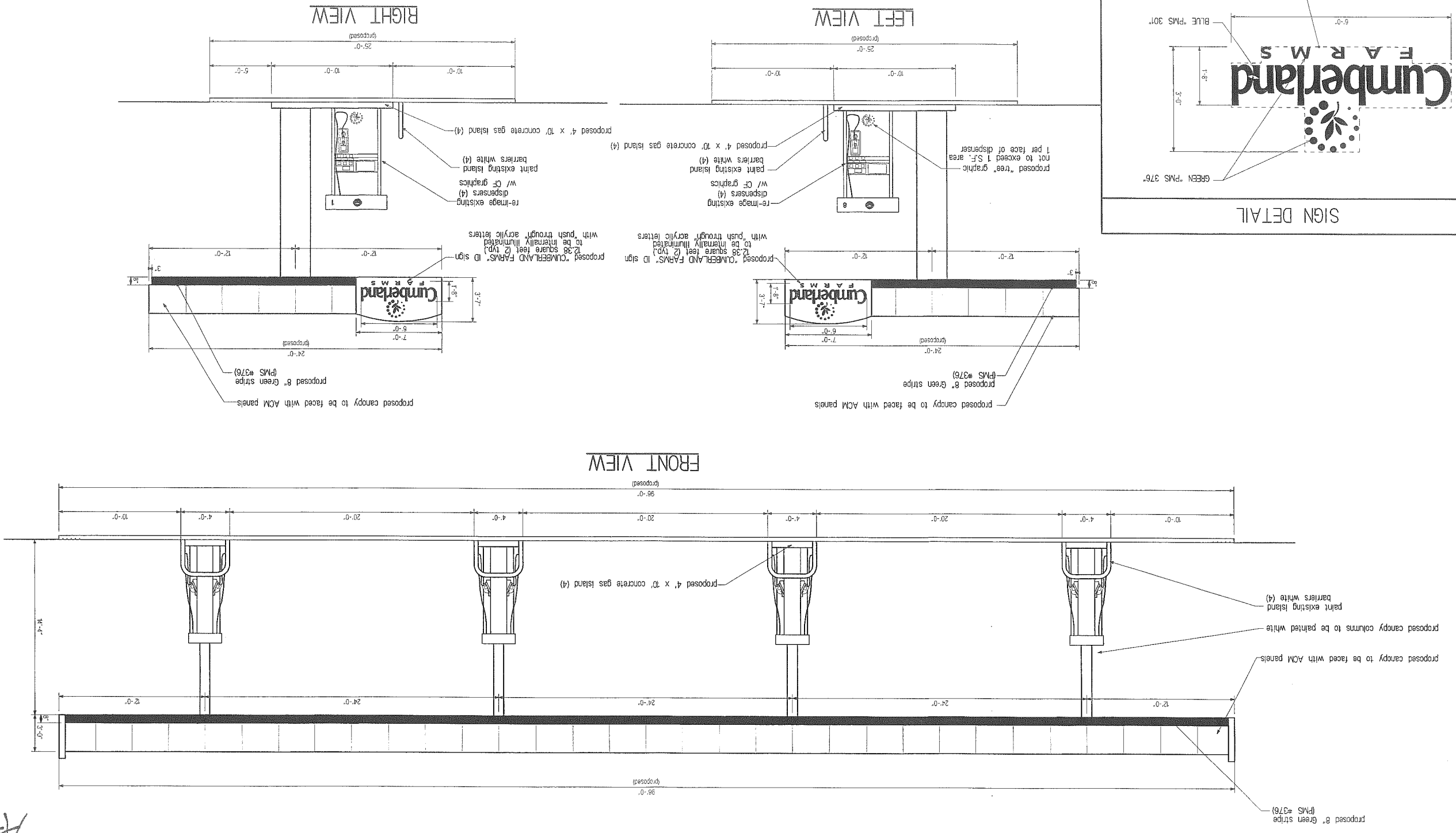
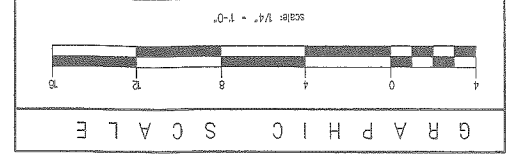
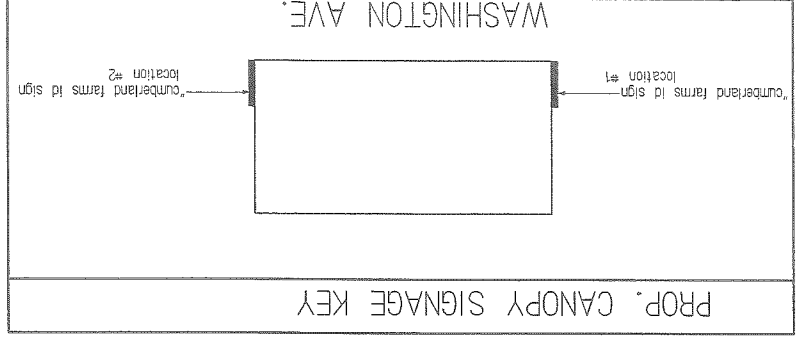
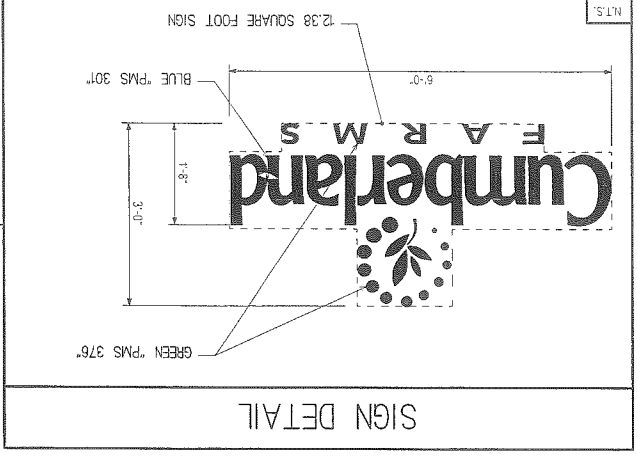


- TEMPORARY EROSION CONTROL MEASURES:**
- 1) SEDIMENT TRAPS SHALL BE INSTALLED AS REQUIRED BARRIERS AND TRAPS ARE TO BE MAINTAINED AND CLEANED UNTIL ALL SLOPES HAVE A HEALTHY STAND OF GRASS.
 - 2) MULCH SHALL BE MONITORED FOR ACCEPTABLE HERBACEOUS GROWTH, FREE FROM NOXIOUS WEEDS OR WOODY STEMS, AND SHALL BE DIRT, NO SALT SHALL BE USED.
 - 3) FILL MATERIAL SHALL BE FREE FROM STUMPS, WOOD, ROOTS, ETC.
 - 4) STOCKPILED MATERIALS SHALL BE PLACED ONLY IN AREAS SHOWN ON THE PLANS. STOCKPILES SHALL BE PROTECTED BY STATION FENCE AND SEED TO PREVENT EROSION. THESE MEASURES SHALL REMAIN UNTIL ALL MATERIAL HAS BEEN PLACED OR DISPOSED OFF SITE.
 - 5) ALL DISTURBED AREAS SHALL BE SEED AND SOIL A MINIMUM OF 6 INCHES OR LOAM SHALL BE INSTALLED WITH NOT LESS THAN ONE POUND OF SEED PER 50 SQUARE YARDS OF AREA.
 - 6) SEED MIX SHALL BE EQUAL PARTS OF RED FESCUE (CHEEPIK), KENTUCKY BLUEGRASS, REEDTOP, PENNUNIA.
 - 7) AFTER ALL DISTURBED AREAS HAVE BEEN STABILIZED THE TEMPORARY EROSION CONTROL MEASURES ARE TO BE REMOVED.
 - 8) PAVED ROADS AND PARKING LOTS MUST BE KEPT CLEAN AT ALL TIMES. PROVIDE SWEEPING ON A DAILY BASIS OR AS DIRECTED BY THE CITY OF LOWELL.
 - 9) ALL CATCH BASIN INLETS WILL BE PROTECTED WITH INLET PROTECTION - SEE DETAIL.

Att. C.7

- CONSTRUCTION SEQUENCE:**
1. INITIAL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES AS REQUIRED.
 2. CUT AND STUMP AREAS OF PROPOSED CONSTRUCTION.
 3. REMOVE AND STOCKPILE TOPSOIL. STOCKPILE SHALL BE SEED TO PREVENT EROSION.
 4. PROTECT CULVERT INLETS AND CATCH BASINS WITH SEDIMENTATION BARRIERS.
 5. PREPARE SITE GRADING, PLACING STATION FENCES AS REQUIRED.
 6. INSTALL UNDERGROUND UTILITIES.
 7. BEGIN TEMPORARY AND PERMANENT SEEDING AND MULCHING. ALL IMMEDIATELY AFTER THEIR CONSTRUCTION.
 8. DAILY OR AS REQUIRED, CONSTRUCT INSPECT, AND IF NECESSARY, RECONSTRUCT TEMPORARY BARRIERS, DITCHES, AND SEDIMENTATION AND SEDIMENT TRAPS INCLUDING MULCHING AND SEEDING. REFER TO EROSION AND MAINTENANCE PLAN FOR ADDITIONAL REQUIREMENTS AND INFORMATION. COPIES OF ALL INSPECTION REPORTS ARE TO BE PROVIDED TO THE CONSTRUCTION COMMISSION DURING CONSTRUCTION AND AVAILABLE UPON REQUEST AFTER CONSTRUCTION IS COMPLETED.
 9. BEGIN EXCAVATION FOR AND CONSTRUCTION OF BUILDINGS.
 10. FINISH PAVING ALL DRIVES AND PARKING AREAS.
 11. COMPLETE PERMANENT SEEDING AND LANDSCAPING.
 12. AFTER GRASS HAS BEEN FULLY ESTABLISHED IN ALL SEEDING AREAS, REMOVE ALL TEMPORARY EROSION CONTROL MEASURES.





6 OF 11	
CHECKED BY:	
DRAWN BY:	R.S.P.
FILE:	X\CANOPY\019
DATE:	11-20-09
SCALE:	1/4" = 1'-0"
PORTLAND, MAINE	
801 WASHINGTON & OCEAN AVE.	
V# 1079	Store# 5604
Facility# 190098	

REVISIONS	
DATE	DESCRIPTION
1-2-11	DB revised to show new canopy & islands
2-1-11	RSP revised for sign tags
4-04-11	DB revised dispenser detail to meet allowable square footage

CANOPY ELEVATIONS

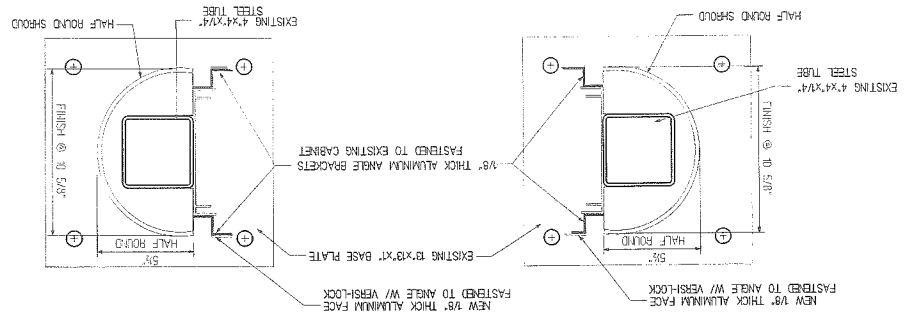
Cumberland Farms, Inc.
100 Commercial Blvd.
Portland, ME 04102

Attachment C.8

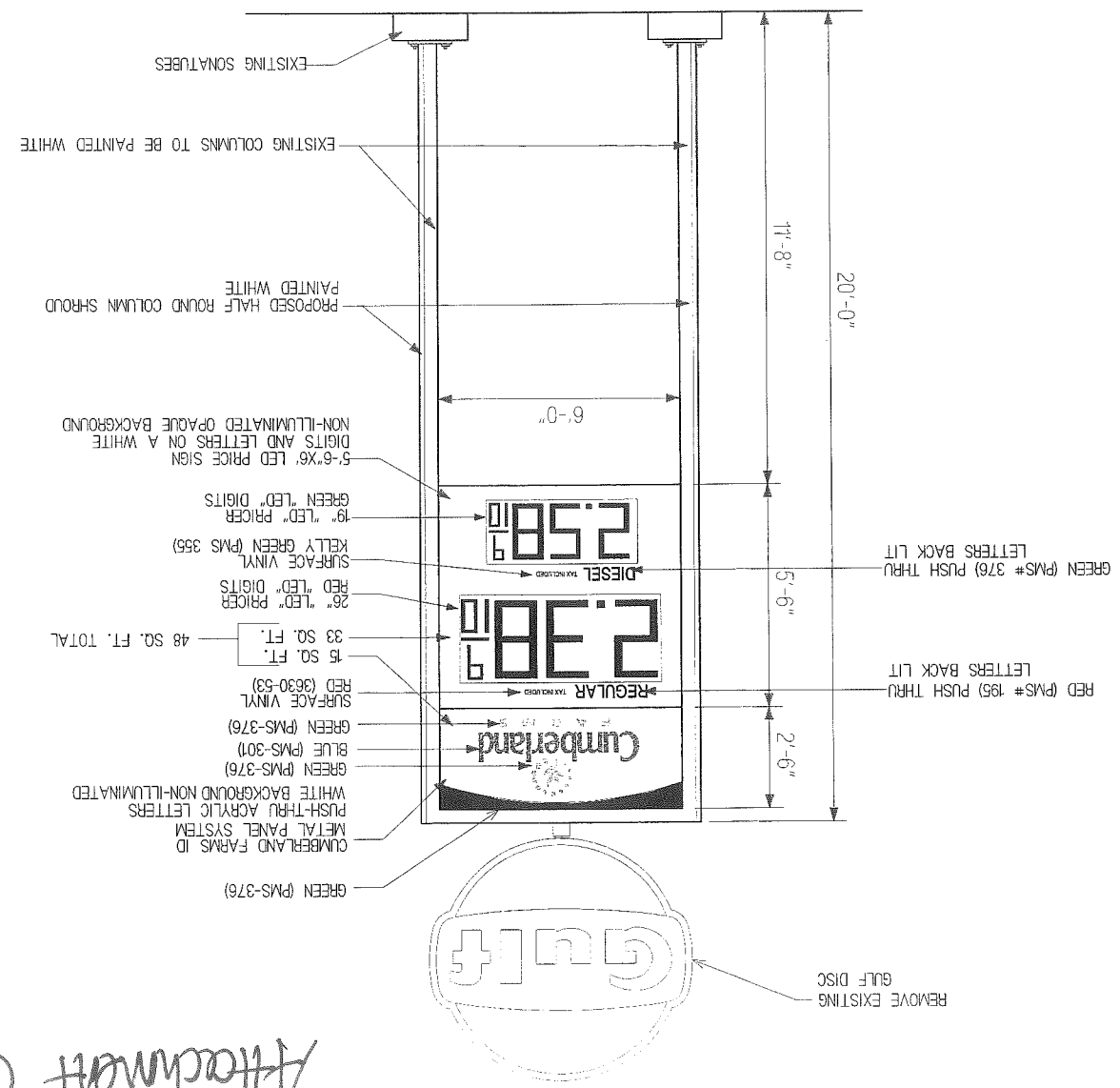
REVISIONS	V# 1079	Store# 5604	Facility# 180098
DATE	REV. BY.	DESCRIPTION	
7-23-11	DR	REVISD LED PRIC SIGN	
4-20-11	DR	REVISD DIESEL LED	

PORTLAND, MAINE
801 WASHINGTON AVE & OCEAN AVE
Cumberland Farms
SCALE: 1/2" = 1'-0"
DATE: 2-11
FILE: SIGN/W/079.dwg
DRAWN BY: RSP
CHECKED BY:

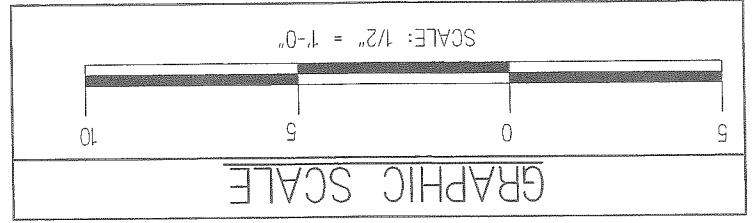
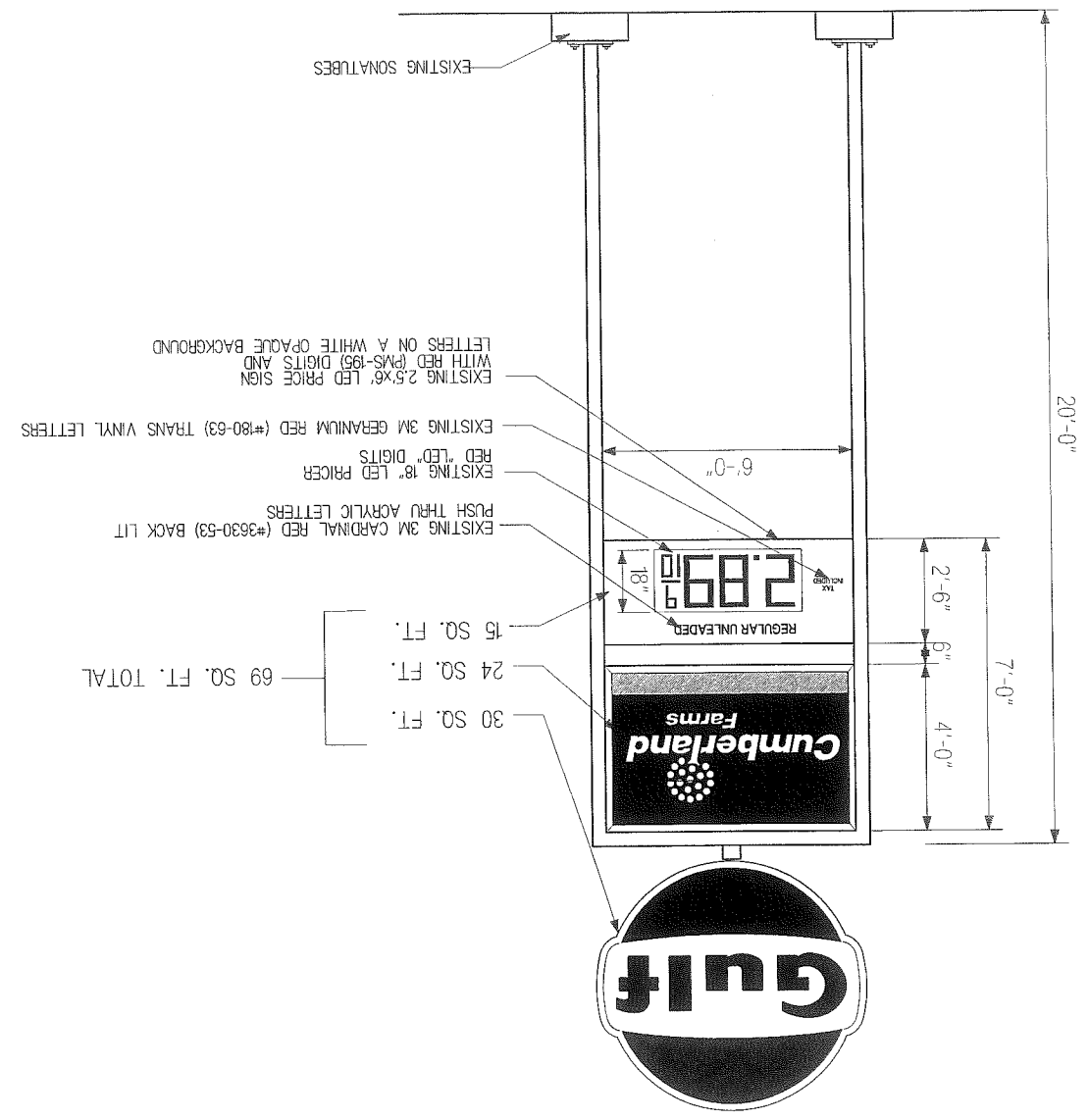
COLUMN SHROUD DETAIL



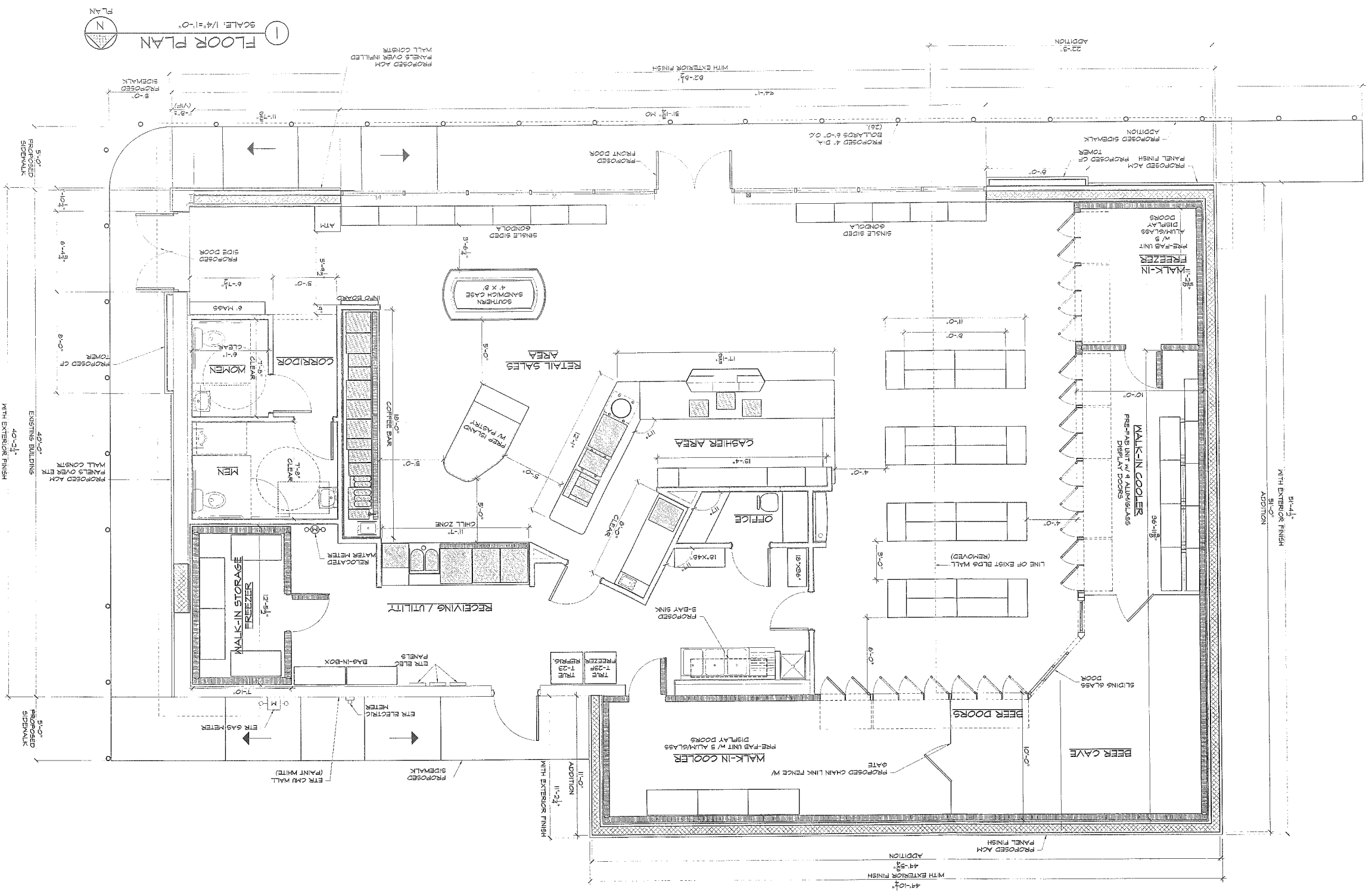
PROPOSED ELEVATION



EXISTING ELEVATION



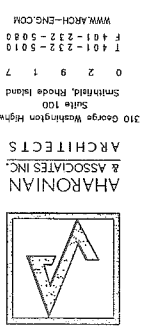
Attachment C-9



① FLOOR PLAN
SCALE: 1/4"=1'-0"
PLAN

NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION

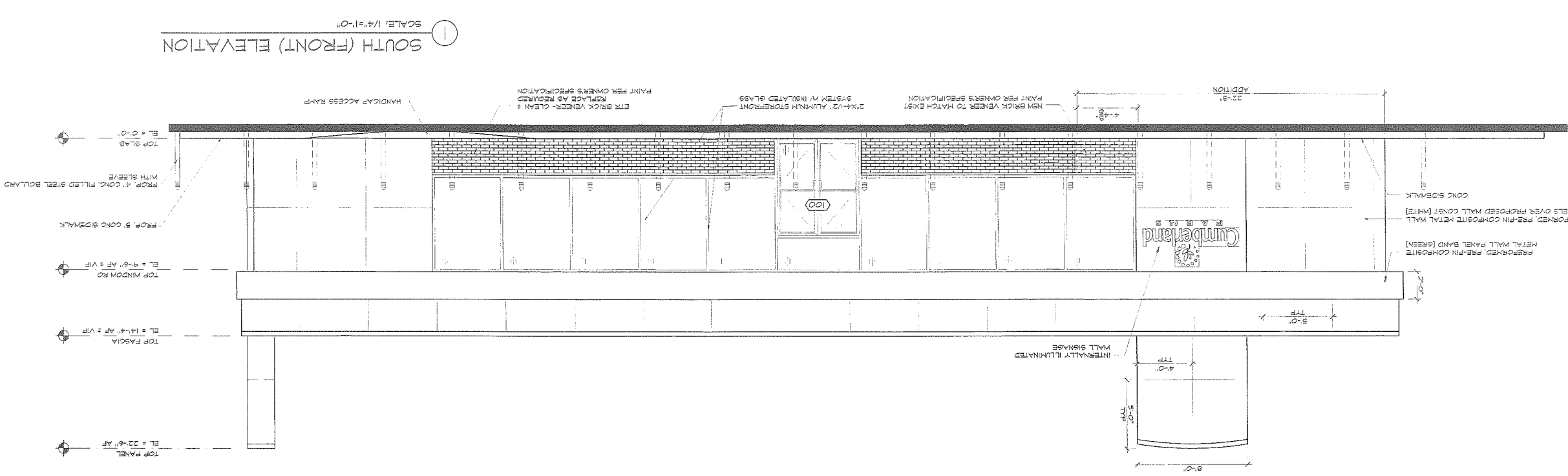
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<p>REVISIONS</p> <table border="1"> <thead> <tr> <th>NUMBER</th> <th>REMARKS</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>REVISED</td> <td>7-29-11</td> </tr> </tbody> </table>	NUMBER	REMARKS	DATE	1	REVISED	7-29-11	<p>FLOOR PLAN</p>
NUMBER	REMARKS	DATE					
1	REVISED	7-29-11					
<p>PRELIMINARY</p>							
<p>PROJECT TITLE</p> <p>Cumberland Farms</p> <p>100 Crossing Blvd., Framingham, Massachusetts STORE # 5604 V1079 801 WASHINGTON AVE PORTLAND, ME Cumberland County</p>							
<p>PROPOSED FLOOR PLAN</p>							
<p>DATE: JULY 18, 2011</p> <p>PROJ. NO. 11112</p>	<p>CHECKED BY: AZ</p> <p>DRAWN BY: DHR</p>						
<p>DRAWING NUMBER: A1.1</p>							



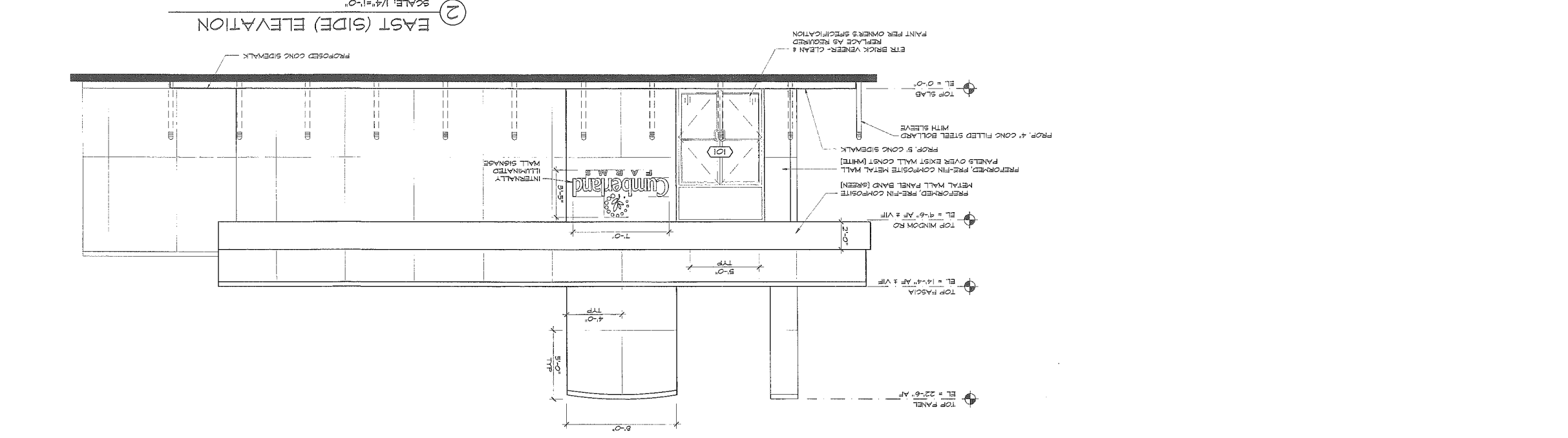
AHARONIAN & ASSOCIATES, INC. ARCHITECTS

310 George Washington Highway
Suite 100
Smithfield, Rhode Island
02917
F 401-232-5010
F 401-232-5080
WWW.ARCH-ENG.CO.UK

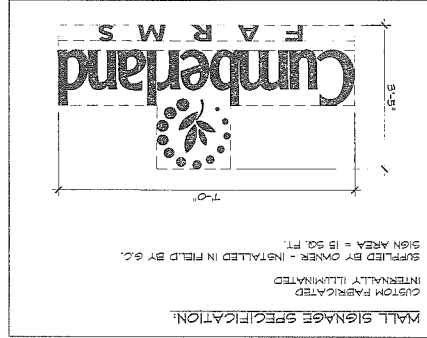
Attachment C.10



① SOUTH (FRONT) ELEVATION
SCALE: 1/4"=1'-0"



② EAST (SIDE) ELEVATION
SCALE: 1/4"=1'-0"



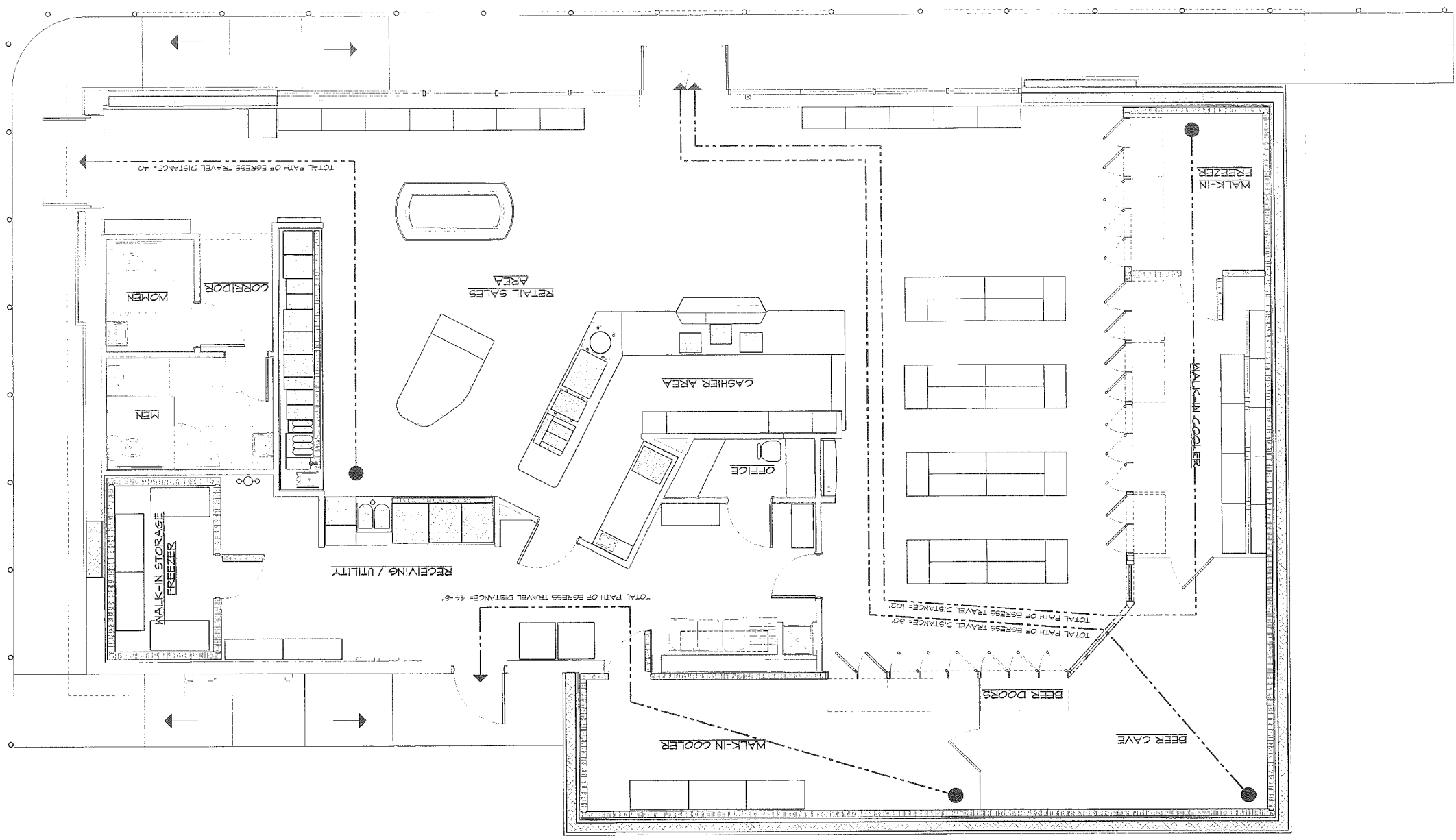
WALL SIGNAGE SPECIFICATION:
CUSTOM FABRICATED
INTERNALLY ILLUMINATED
SUPPLIED BY OWNER - INSTALLED IN FIELD BY GC.
SIGN AREA = 15 SQ. FT.

NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION

A3.1	
DRAWING NUMBER	AZ
DRAWN BY SD/RSP	CHECKED BY AZ
DATE JULY 19, 2011	PROJ NO 11112
EXTERIOR ELEVATIONS	
DRAWING TITLE	
801 WASHINGTON AVE PORTLAND, ME Cumberland County	
PRELIMINARY	
100 Crossing Blvd, Framingham, Massachusetts STORE # 5604 V1079	
310 George Washington Highway Suite 100 Smithfield, Rhode Island 02917 401-232-5010 F 401-232-5080 WWW.ARCH-ENG.COM	
AHARONIAN & ASSOCIATES, INC. ARCHITECTS	
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REVISIONS	
NUMBER	REMARKS
DATE	REVISIONS
7-24-11	REVISED
3-20-12	REVISED
	SIGNAGE

Attachment C11

① LIFE SAFETY PLAN
SCALE: 1/4"=1'-0"
PLAN



GENERAL BUILDING DATA
THE FOLLOWING CODES WERE USED IN THE DESIGN OF THIS PROJECT:
FEDERAL STATE AND MUNICIPAL LAWS
TOWN OF PORTLAND BUILDING CODE 2004
INTERNATIONAL BUILDING CODE 2004
STATE OF MAINE FIREWORKS CODE
STATE OF MAINE ACCESSIBILITY CODE
(ADA)

OCCUPANCY CLASSIFICATION - 304
MERCANTILE - GROUP M
ALLOWABLE HEIGHT AND BUILDING AREAS - TABLE 503
BUILDING HEIGHT
1 STORY - 14'-4 1/2"
AREA LIMITATION
EXISTING BUILDING AREA - TO REMAIN
9,374 ± SF
TYPE OF CONSTRUCTION - CHAPTER 6
CONSTRUCTION CLASSIFICATION
TYPE 5B UNPROTECTED - COMBUSTIBLE

MAXIMUM FLOOR AREA ALLOWANCES PER OCCUPANT (TABLE 1004.12)

OCCUPANCY	MINIMUM OCCUPANT LOAD	OCCUPANTS
MERCANTILE AREA (PUBLIC)	1.750	58
UTILITY AREA (NON-PUBLIC)	500 SF/PERSON	7
TOTAL		65

EGRESS WIDTH PER OCCUPANT SERVED - TABLE 1005.1

OCCUPANCY	MINIMUM OCCUPANT LOAD	WIDTH REQUIRED	WIDTH PROVIDED
MERCANTILE AREA (PUBLIC)	1.750	11'-6"	14'-4"
UTILITY AREA (NON-PUBLIC)	500 SF/PERSON	2'-0"	1'-4"
TOTAL		2'-0"	4'-2"

EXIT ACCESS TRAVEL DISTANCE - TABLE 1016.1
75' MAXIMUM COMMON PATH OF EGRESS TRAVEL
200' MAXIMUM (WITHOUT SPRINKLER SYSTEM)

NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION - NOT FOR CONSTRUCTION

LS1.1

DRAWING NUMBER

DATE: JULY 16, 2011
PROJ NO: 1112

DRAWN BY: DHR
CHECKED BY: AZ

PROPOSED LIFE SAFETY PLAN

DRAWING TITLE

801 WASHINGTON AVE
PORTLAND, ME
Cumberland County

100 Crossing Blvd, Framingham, Massachusetts
STORE # 5604
V1079

Cumberland FARMS

PROJECT TITLE

PRELIMINARY

REVISIONS	NUMBER	REMARKS	DATE
1	1	REVISED	7-29-11
	1	CODE INFO	9-20-11

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310 George Washington Highway
Suite 100
Smithfield, Rhode Island

WWW.AH-A-ENG.COM
F 401-222-5888
T 401-222-5889

1112

DATE: JULY 16, 2011

PROJ NO: 1112

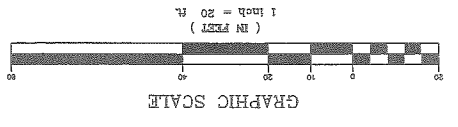
DRAWN BY: DHR

CHECKED BY: AZ

Attachment C-13



Based on the information provided, all dimensions and numbers herein are deemed to be correct. The engineer and architect shall not be responsible for any errors or omissions in the drawings or specifications, or for any other architectural elements unless noted. This lighting system represents illumination levels calculated from laboratory data in accordance with Illuminating Engineering Society approved methods. Final performance shall be determined by field measurements. The engineer and architect shall not be responsible for any errors or omissions in the drawings or specifications, or for any other architectural elements unless noted.



Symbol	Qty	Label	Arrangement	Description
F	6			CRO3-A-LED-64-350-CW-UE
E	4			CRO3-S-LED-64-350-CW-UE
D	2			XAWS3-4-LED-63-350-CW-UE
C	6			OM6LED27U-R6LED30KMDKSS
B	5			XAS3-FI-LED-63-350-CW-UE-HSS8 SINGLE ON 1/4 POLE + 2 BASE
A	8			CRO3-FO-LED-30-CW-120-240V

Calculation Summary	Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
ALL CALC POINTS		Footcandle	fc	1.62	30.5	0.0	N/A	N/A
DISPENSER VERTICAL READINGS 1		Footcandle	fc	50.25	73.9	24.7	2.03	2.99
APPROACH AND DRIVE SUMMARY		Footcandle	fc	0.87	2.0	0.2	4.35	10.00
CANOPY SUMMARY		Footcandle	fc	19.14	30.5	12.1	1.58	2.52
PARKING SUMMARY		Footcandle	fc	1.45	2.5	0.4	3.63	6.25

MHF PROJECT NO. 266609 SHEET 1 OF 1

DATE: 02/27/12

PROJECT: 105880-5

CLIENT: CUMBERLAND FARMS

LOCATION: WASHINGTON AVE & OCEAN

PORTLAND, ME

INDUSTRIES

ABUTTER ACROSS INTERSECTION
 TAX MAP 163 BLOCK E LOT 6
 N/F LAURA ANGELO
 28 WHALEBOAT ROAD
 PORTLAND, ME 04103

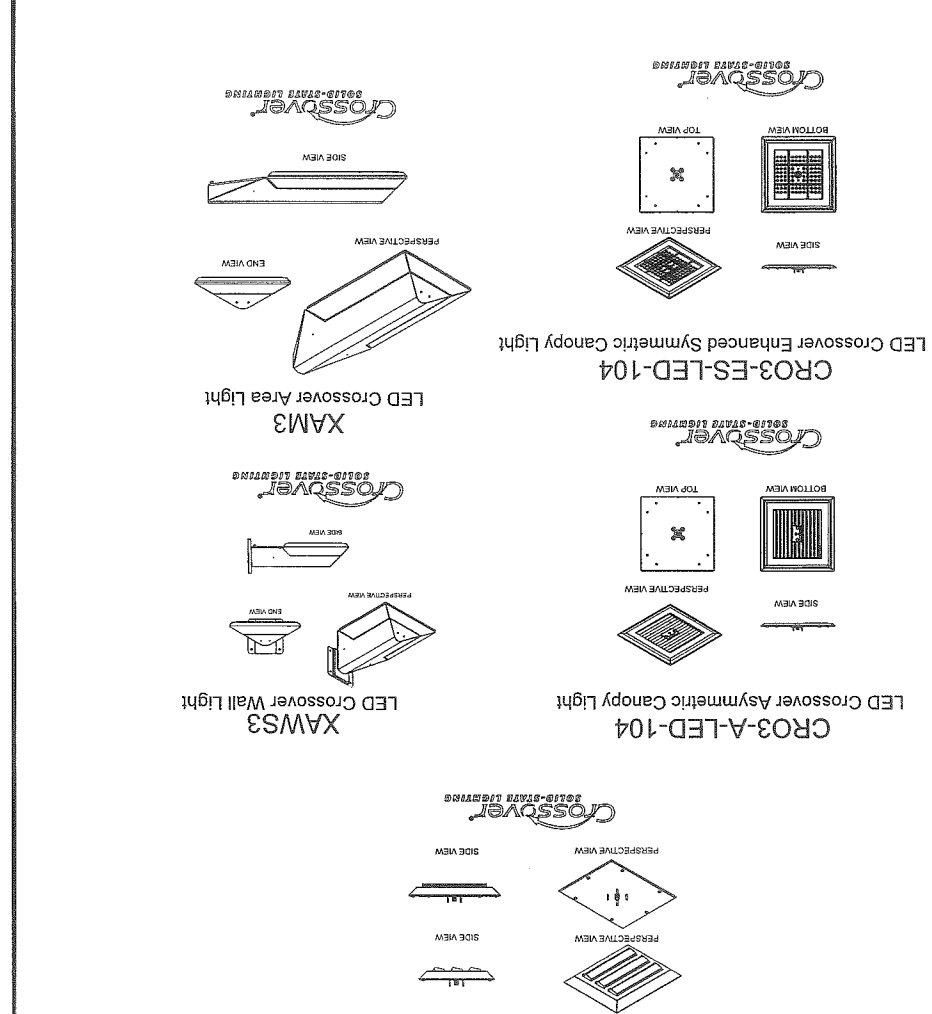
TAX MAP 163 BLOCK A LOT 8
 N/F ANGES SERVICE INC.
 10 E BRIDGE STREET
 WESTBROOK, ME 04092

TAX MAP 163A BLOCK A LOT 1
 N/F RONALD L LEMUEX
 CAROL S LEMUEX JTS
 26 FOREST LANE
 CUMBERLAND, ME 04021

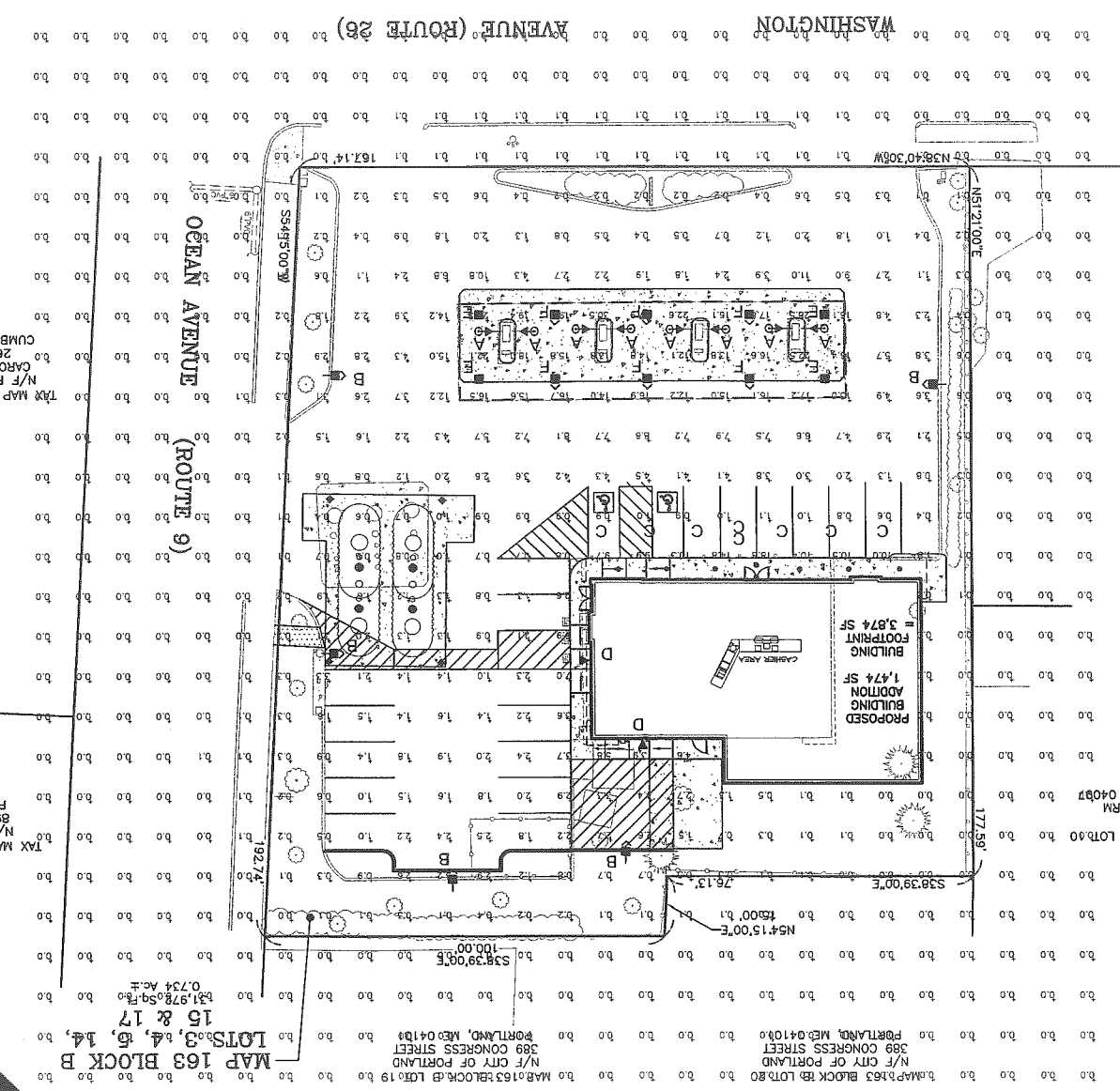
TAX MAP 163 BLOCK B LOT 2
 N/F ANGES SERVICE INC.
 10 E BRIDGE STREET
 WESTBROOK, ME 04092

TAX MAP 163 BLOCK B LOT 10
 N/F RAEORC LLC
 32 DEERBROOK FARM
 NORTH YARMOUTH, ME 04067

TAX MAP 163A BLOCK A LOT 17
 N/F SPURWINK SCHOOL
 899 RIVERSIDE STREET
 PORTLAND, ME 04103



CRO3-FO LED Crossover Focus Light
 Attachment C.14



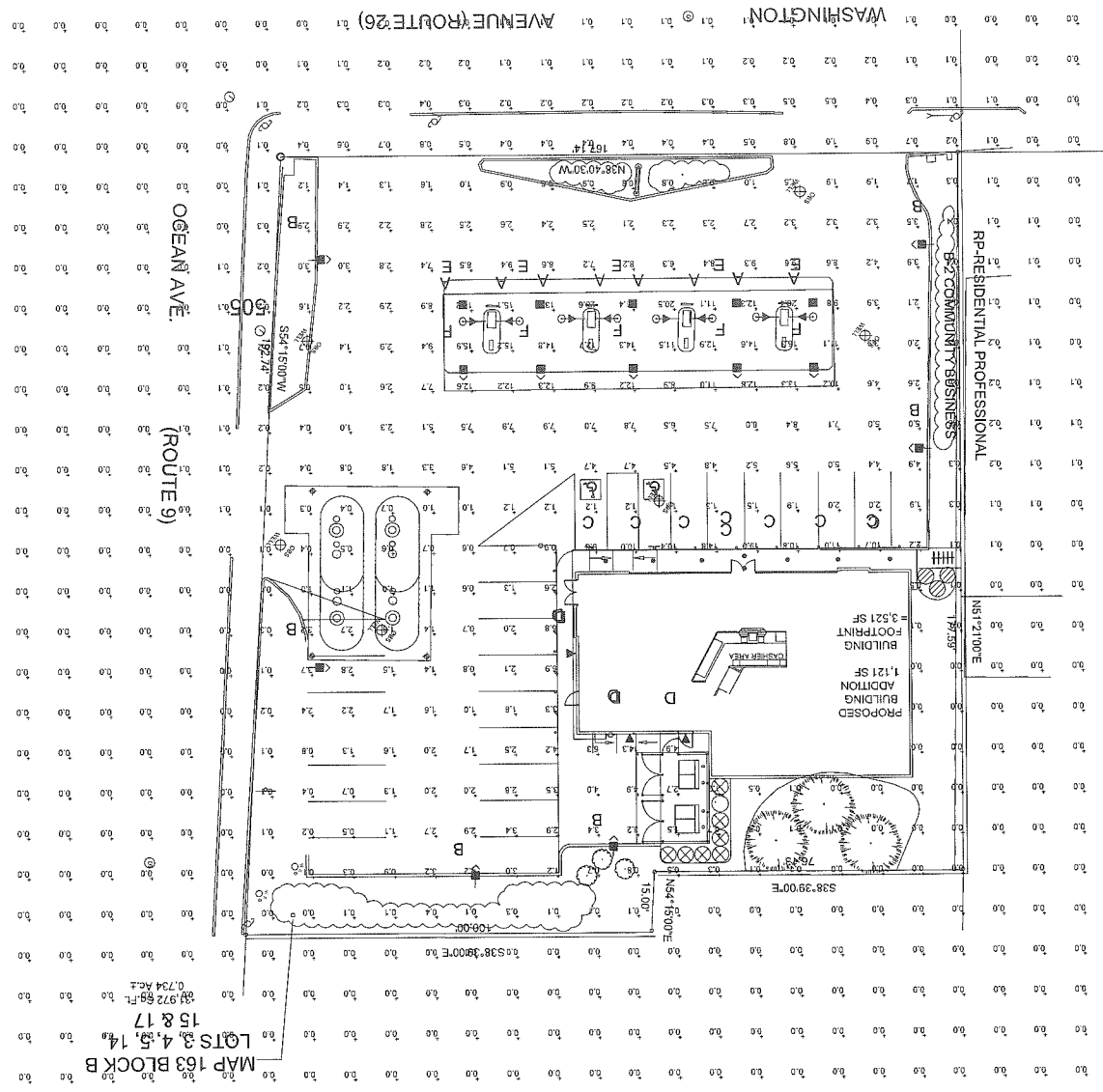
Based on the information provided, all dimensions and finished locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions. This lighting fixture schedule summarizes fixture calculations from laboratory data in accordance with the Illuminating Engineering Society approved methods. Field performance may vary due to system electrical, voltage, temperature, and other conditions. Calculations do not include obstructions such as buildings, signs, landscaping, or any other architectural elements unless noted.

Qty	Label	Arrangement	Description	Units	Arr. Lumens	Arr. Watts
5	F	SINGLE	CRO3-A-LED-64-350-CW-UE	1,000	N.A.	6872
5	E	SINGLE	CRO3-ES-LED-64-350-CW-UE	1,000	N.A.	6272
2	D	SINGLE	XAWS3-3-LED-63-350-CW-UE	1,000	N.A.	4922
8	C	SINGLE	OM6LEDZ7U-R6LED30KMDCS	1,000	N.A.	1416
6	B	SINGLE	XAS3-FI-LED-63-350-CW-UE-HSS8 SINGLE ON 14' POLE	1,000	N.A.	3764
8	A	SINGLE	CRO3-FO-LED-30-CW-120-240V	1,000	3004	40

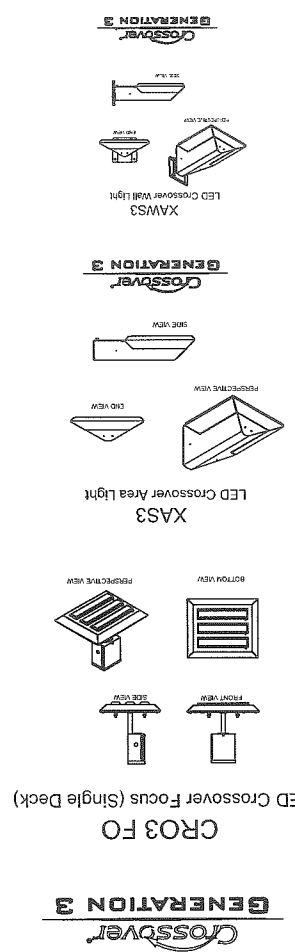
Luminaire Schedule

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
ALL CALC POINTS	Illuminance	Fc	1.62	28.6	0.0	N.A.	N.A.
DISPENSER VERTICAL READINGS, 1'	Illuminance	Fc	49.49	73.0	24.1	2.05	3.03
APPROACH AND DRIVE SUMMARY	Illuminance	Fc	2.26	3.2	1.0	2.26	3.20
CANOPY SUMMARY	Illuminance	Fc	15.29	28.6	11.1	1.38	2.58
PARKING SUMMARY	Illuminance	Fc	1.65	3.4	0.5	3.30	6.80

Calculation Summary



NOTE: LSI INDUSTRIES WILL NOT BE RESPONSIBLE FOR THE PHOTOMETRIC ACCURACY OF ANY FIXTURES USED IN A LIGHTING PLAN NOT MANUFACTURED OR TESTED BY LSI.



LSI INDUSTRIES
 LIGHTING PROPOSAL FOR
 CLAMBERLAND FARMS
 WASHINGTON AVE. & OCEAN
 PORTLAND, ME
 PROJECT NO. 105880-3
 SHEET 1 OF 1

Total Project Hours = 1890.2

TRITON FOODMART / GAS STATION

1884 FOREST AVENUE
PORTLAND, MAINE

APPLICANT:

JOHN CHAU

75 ACADIA STREET
PORTLAND, MAINE 04103

ENGINEER / SURVEYOR:



Sebago Technics

Experience Expertise You Can Build On.

One Global Street, 250 Goddard Rd - Suite B
Waldobrook, ME 04098-1309, Lewiston, ME 04240

Tel (207) 858-0277, Fax (207) 753-5555

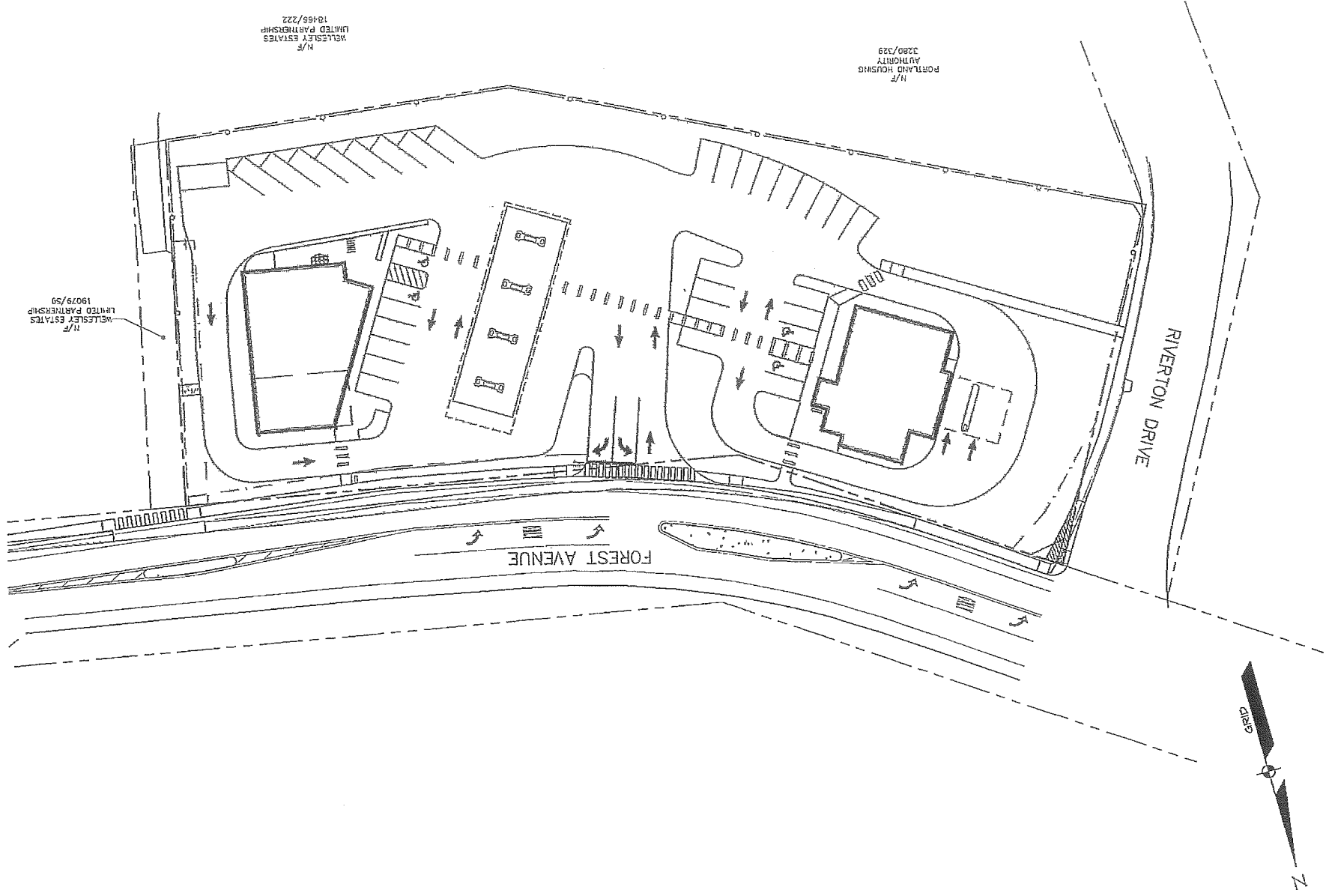
WWW.SEBAGOARCHITECTS.COM

PROJECT ARCHITECT:

TFH ARCHITECTS

80 MIDDLE STREET

PORTLAND, MAINE



SCALE: 1" = 30'

H/F
WELLSLEY ESTATES
18465/222

H/F
PORTLAND HOUSING
AUTHORITY
3280/329

H/F
WELLSLEY ESTATES
19079/50

SHEET INDEX:

SHEET DESCRIPTION

1 COVER SHEET

2 EXISTING CONDITIONS PLAN

3 SITE PLAN

4 GRADING AND UTILITY PLAN

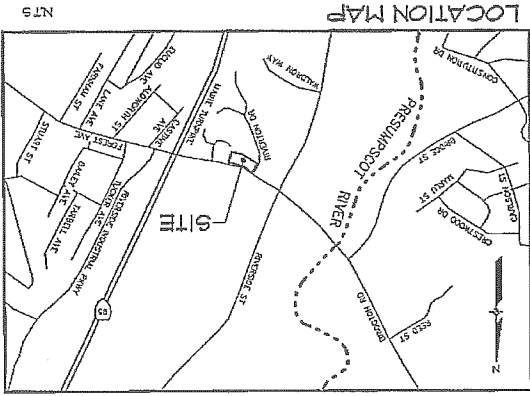
5 LANDSCAPING PLAN

6 DETAILS

7 DETAILS

8 DETAILS

9 DETAILS



LOCATION MAP

NTS

A.H.C.1

DATE	1-26-12	N/A
SCALE	AS NOTED	1-27-12
PROJECT NO.	1257758	1257708
DESIGNER	HUBBELL	HUBBELL
CLIENT	CHAU GAS STATION	PHOTOMETRIC STUDY

Symbol	Label	Qty	Description	Arrangement	Lum. Lumens	LDF	LLF	Filename	[TEST]
S2	WP92/LED-5100K	1	SINGLE	3888	0.950	0.855	0.900	WP92-15k.ies	
S4	WP94/LED-5100K	2	SINGLE	3970	0.950	0.855	0.900	WP94-15k.ies	
S5	WP95/LED-5100K	2	SINGLE	3905	0.950	0.855	0.900	WP95-15k.ies	
L4-NB	WP94NB/LED-5100K	3	SINGLE	4624	0.950	0.855	0.900	WP94NB-15k.ies	
L52	WP94/LED-5100K	1	BACK-BACK	7173	0.950	0.855	0.900	WP94-15k.ies	
C1	UCL LED - 60-G-700-F	10	SINGLE	11232	0.950	0.855	0.900	UCL LED - 60-G-700-F50.IES	3173-24-OW
C2	UCL LED - 36-G-700-F	3	SINGLE	6365	0.950	0.855	0.900	UCL LED - 36-G-700-F50.IES	3173-24-OW-36
C3	LFL LED 6LFL6D5 40K	9	SINGLE	912	1.000	1.000	1.000	LFLFrame LFL6D 40K.IES	3333
WP	LNC6LUSKAD	10	SINGLE	615	0.950	0.855	0.900	LNC6LUSKAD.IES	4363

Label	Avg	Max	Min	Avg/Min	Max/Min	PSPct	PSPctb
Calcifs - Bank Canopy	9.94	13.3	4.3	2.31	3.09	2	2
Calcifs - Gas Canopy	23.46	52.5	3.8	6.17	13.82	5	5
Calcifs - Pts Along Pl	0.07	0.1	0.0	N.A.	N.A.	10	N.A.
StarArea - 1 & 2 Story Parking Area	1.05	4.0	0.2	5.25	20.00		
StarArea - Bank Parking Area	1.39	3.7	0.2	6.95	18.50		

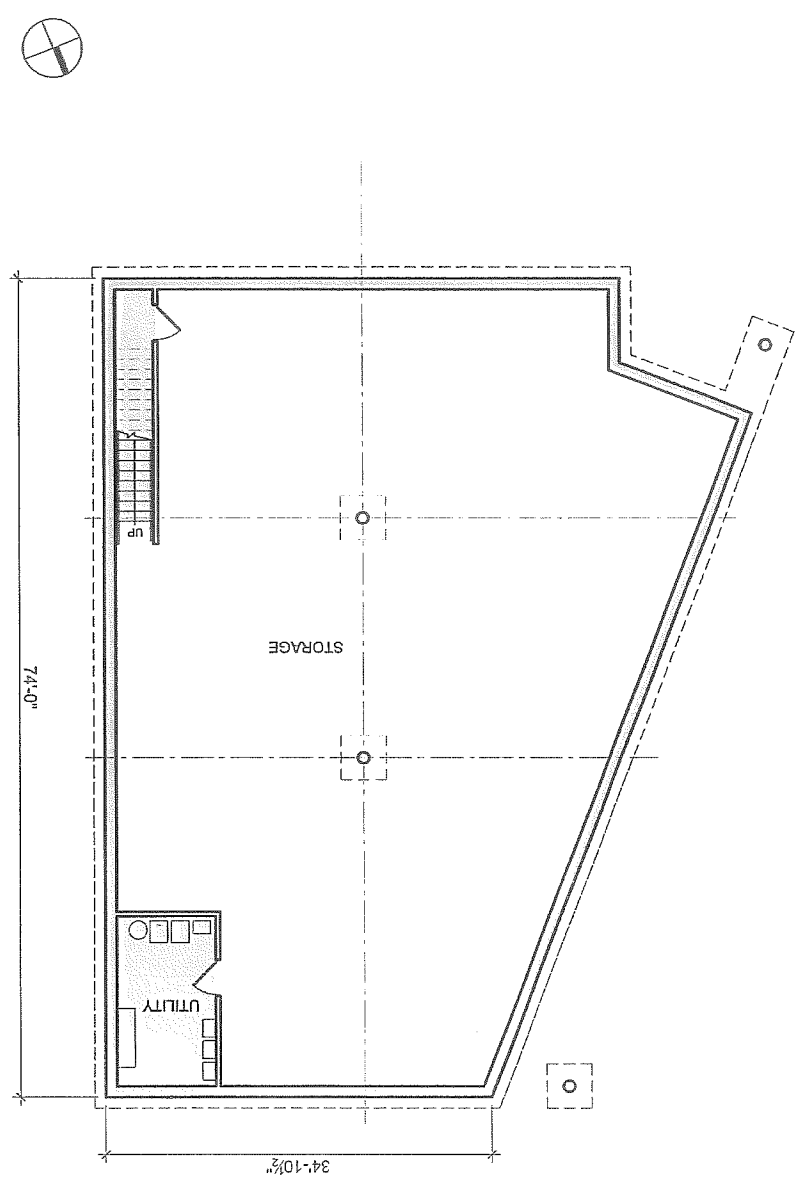


G.11

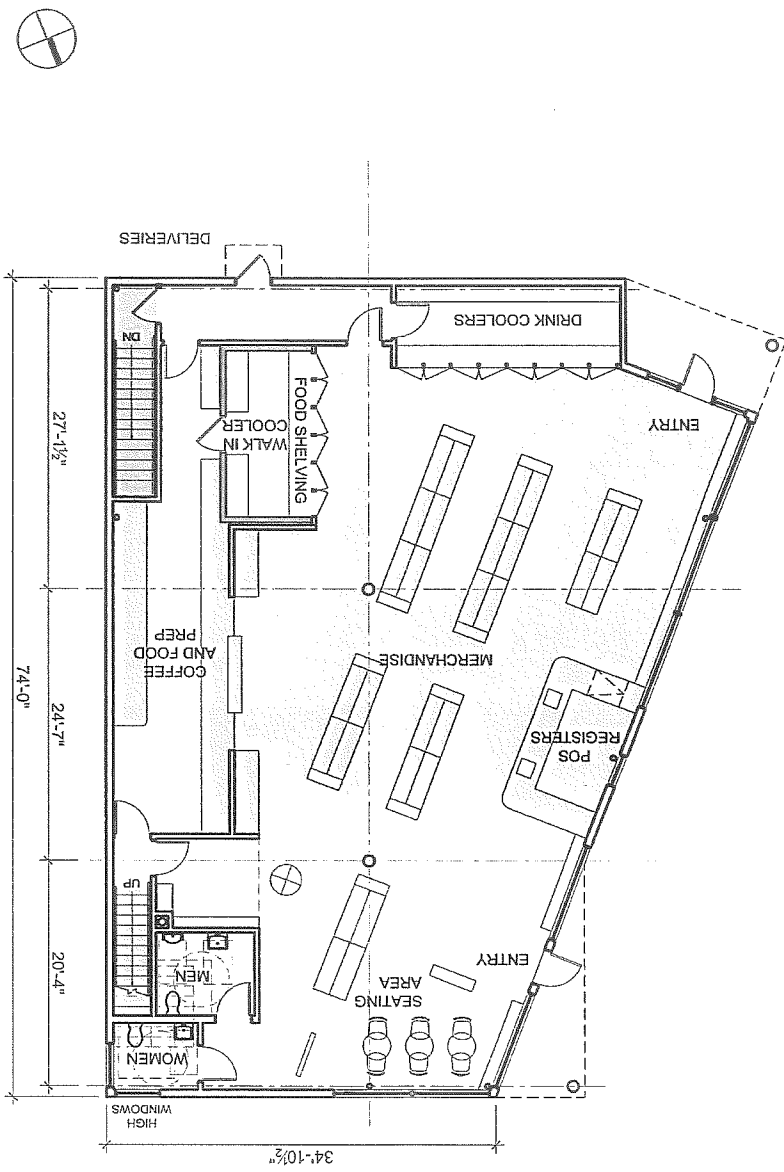
1884 FOREST AVENUE PORTLAND, MAINE GAS STATION / CONVENIENCE STORE



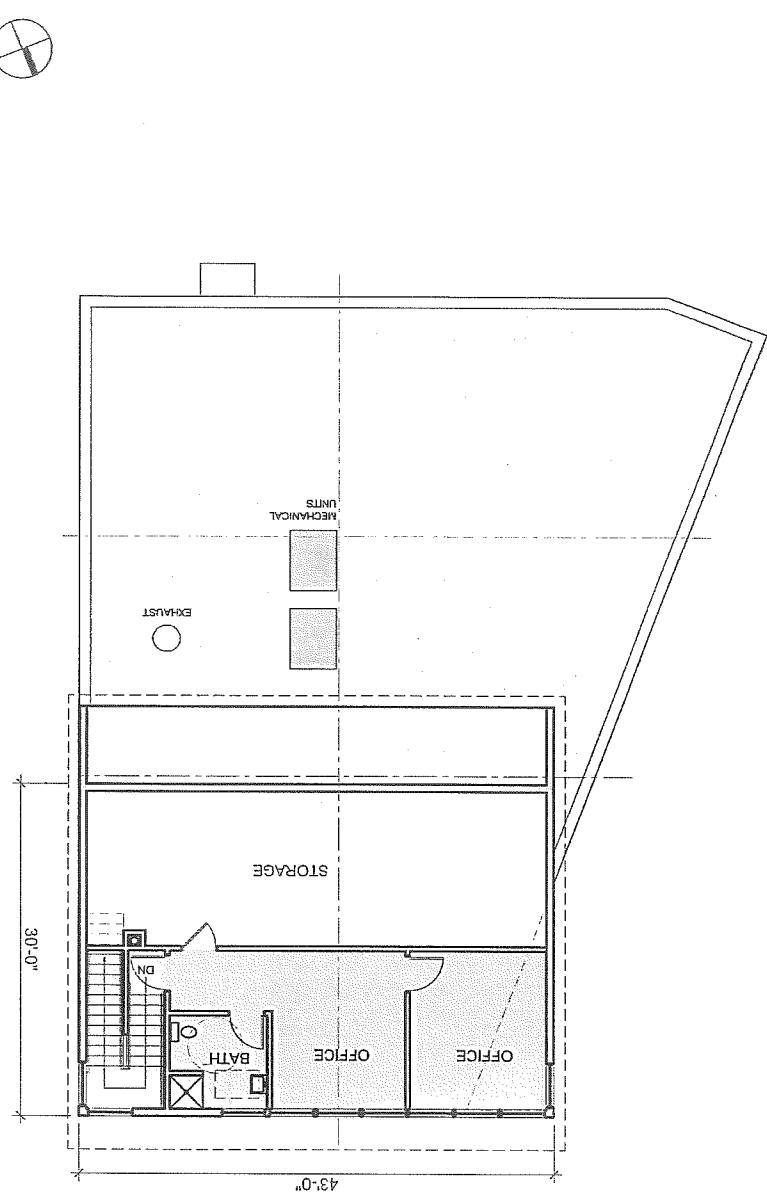
BASEMENT PLAN



FIRST FLOOR PLAN



SECOND FLOOR PLAN



BRANCH BANK
1884 FOREST AVENUE
PORTLAND, MAINE

FLOOR PLAN

