

267-A-4

2000-0047

160 Riverside St.

Sleep Inn Hotel

J. Properties

add to Spreadsheet

**CITY OF PORTLAND, MAINE
DEVELOPMENT REVIEW APPLICATION
PLANNING DEPARTMENT PROCESSING FORM
Planning Department Copy**

20000047
I. D. Number

J. Properties

Applicant _____
 5735 Harrison Way, Boca Raton, FL 33496
 Applicant's Mailing Address _____
 Sebago Tech. - Steve Doe
 Consultant/Agent _____
 856-0277 856-2206
 Applicant or Agent Daytime Telephone, Fax _____

03/29/2000
 Application Date _____
 Sleep Inn Hotel
 Project Name/Description _____
 160 - 160 Riverside St, Portland, Maine 04103
 Address of Proposed Site _____
 267 A004
 Assessor's Reference: Chart-Block-Lot _____

Proposed Development (check all that apply): New Building Building Addition Change Of Use Residential
 Office Retail Manufacturing Warehouse/Distribution Parking Lot Other (specify) 74 rm. hotel 3 stories
 30,850 Proposed Building square Feet or # of Units 2.23 Acreage of Site B-4 Zoning

Check Review Required:

- Site Plan (major/minor) Subdivision # of lots _____ PAD Review 14-403 Streets Review
- Flood Hazard Shoreland Historic Preservation DEP Local Certification
- Zoning Conditional Use (ZBA/PB) Zoning Variance Other _____

Fees Paid: Site Plan \$500.00 Subdivisio _____ Engineer Review _____ Date 03/29/2000

Planning Approval Status:

Approved Approved w/Conditions See Attached Denied
 Reviewer Kandi Talbot

Approval Date 06/27/2000 Approval Expiration 06/27/2001 Extension to _____ Additional Sheets Attached
 signature _____ date _____

Performance Guarantee

Required* Not Required

* No building permit may be issued until a performance guarantee has been submitted as indicated below

<input type="checkbox"/> Performance Guarantee Accepted	_____	_____	_____
	date	amount	expiration date
<input type="checkbox"/> Inspection Fee Paid	_____	_____	
	date	amount	
<input type="checkbox"/> Building Permit Issue	_____		
	date		
<input type="checkbox"/> Performance Guarantee Reduced	_____	_____	_____
	date	remaining balance	signature
<input type="checkbox"/> Temporary Certificate of Occupancy	_____	<input type="checkbox"/> Conditions (See Attached)	_____
	date		expiration date
<input type="checkbox"/> Final Inspection	_____		
	date		signature
<input type="checkbox"/> Certificate Of Occupancy	_____		
	date		
<input type="checkbox"/> Performance Guarantee Released	_____		
	date		signature
<input type="checkbox"/> Defect Guarantee Submitted	_____	_____	_____
	submitted date	amount	expiration date
<input type="checkbox"/> Defect Guarantee Released	_____	_____	
	date	signature	

**CITY OF PORTLAND, MAINE
DEVELOPMENT REVIEW APPLICATION
PLANNING DEPARTMENT PROCESSING FORM
D.R.C. Copy**

20000047

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 Proposed Building square Feet or # of Units Acreage of Site Zoning

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 Zoning Conditional Use (ZBA/PB) Zoning Variance Other _____

Fees Paid: Site Plan \$500.00 Subdivision _____ Engineer Review _____ Date: 03/29/2000

DRC Approval Status:

Reviewer Jim Wendel

- Approved Approved w/Conditions see attache Denied

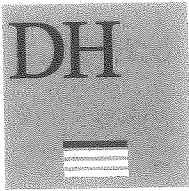
Approval Date 06/27/2000 Approval Expiration 06/27/2001 Extension to _____ Additional Sheets Attached

Condition Compliance _____ signature _____ date _____

Performance Guarantee Required* Not Required

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<input type="checkbox"/> Performance Guarantee Accepted	_____	_____	_____
	date	amount	expiration date
<input type="checkbox"/> Inspection Fee Paid	_____	_____	
	date	amount	
<input type="checkbox"/> Building Permit	_____		
	date		
<input type="checkbox"/> Performance Guarantee Reduced	_____	_____	_____
	date	remaining balance	signature
<input type="checkbox"/> Temporary Certificate Of Occupancy	_____	<input type="checkbox"/> Conditions (See Attached)	_____
	date		expiration date
<input type="checkbox"/> Final Inspection	_____	_____	
	date	signature	
<input type="checkbox"/> Certificate Of Occupancy	_____		
	date		
<input type="checkbox"/> Performance Guarantee Released	_____	_____	
	date	signature	
<input type="checkbox"/> Defect Guarantee Submitted	_____	_____	_____
	submitted date	amount	expiration date
<input type="checkbox"/> Defect Guarantee Released	_____	_____	
	date	signature	



DeLUCA-HOFFMAN ASSOCIATES, INC.
CONSULTING ENGINEERS

778 MAIN STREET
SUITE 8
SOUTH PORTLAND, MAINE 04106
TEL. 207 775 1121
FAX 207 879 0896

- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

MEMORANDUM

TO: Kandi Talbot, Planner

FROM: Jim Wendel, P.E., Development Review Coordinator

DATE: August 13, 1998

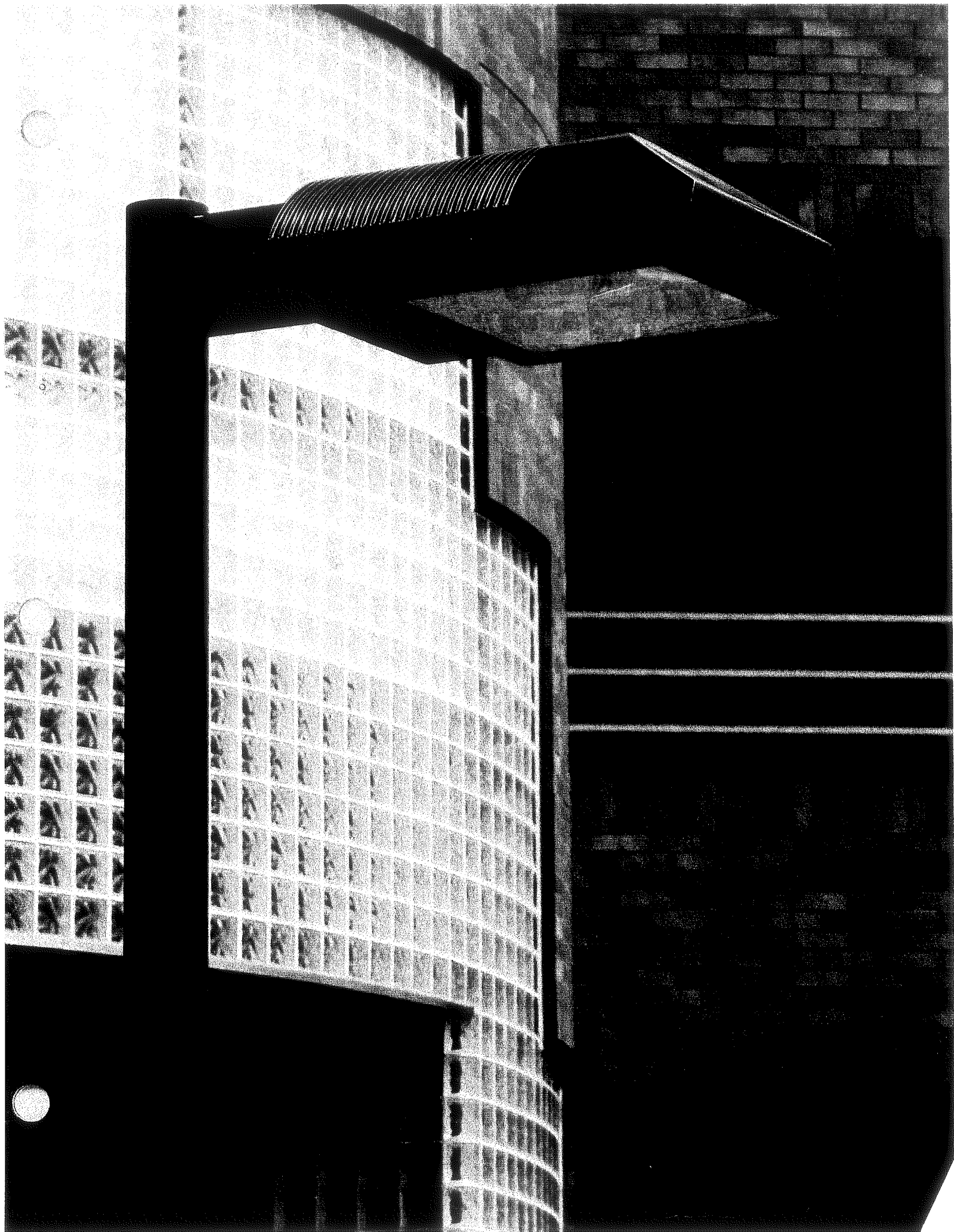
RE: Site Plan Review
Warehouse Facility
160 Riverside Street

Review of the revised stormwater management analysis based on previous comments in a memo to you dated August 3, 1998 has been completed. My comments are:

1. The revised stormwater analysis is satisfactory.
2. With regard to the maneuvering of a WB-50 around the truck dock, I have not received a revised plan noting the revisions discussed at the meeting with the applicant's engineer.

Should you have any questions, please call.

JN1359.10/1350.10disk6/160rivr2

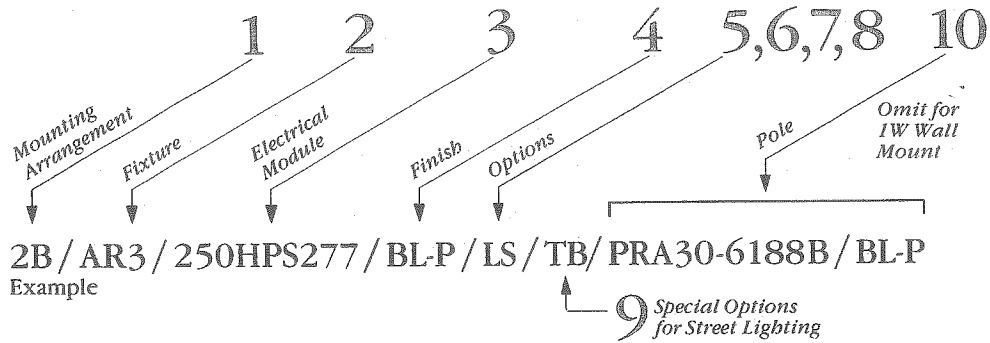


Ordering Information

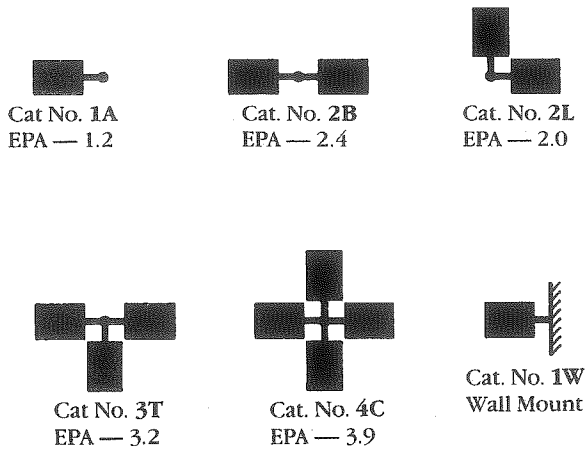
Ordering Guide

Fixture and pole are described by a single number sequence as illustrated at right.

Order lamps separately.

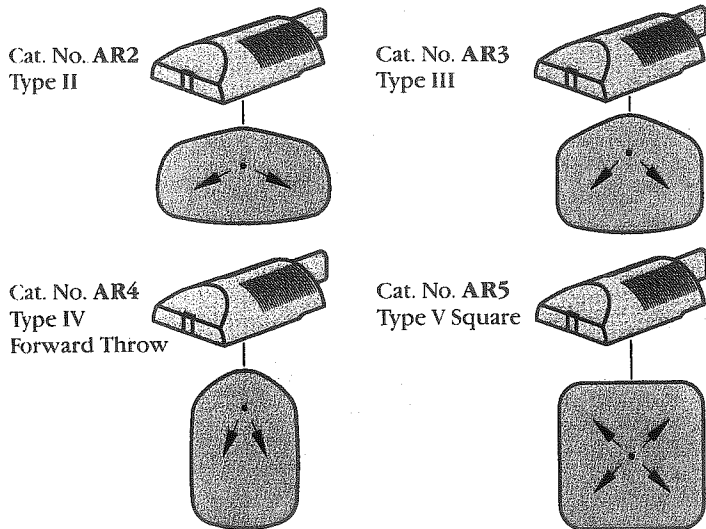


1 Mounting Arrangement



2 Fixture

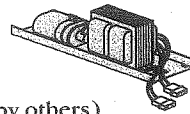
Catalog number indicates light distribution.



See catalog A1b for selection of fixture and mounting arrangement based on photometric performance.

3 Electrical Module

*Lamp available from Kim, see p. 17



Lamp (by others)	Ballast Module Cat. No.	Line Volts	Line Watts	Max. Amps.
70 Watt	70HPS120	120	82	0.85
High Pressure Sodium	70HPS208	208	88	0.48
E-23½ Clear Mogul Base	70HPS240	240	88	0.42
	70HPS277	277	88	0.37
	70HPS480	480	88	0.21
100 Watt	100HPS120	120	115	1.50
High Pressure Sodium	100HPS208	208	130	0.76
E-23½ Clear Mogul Base	100HPS240	240	130	0.66
	100HPS277	277	130	0.60
	100HPS480	480	130	0.33
150 Watt	150HPS120	120	170	2.20
High Pressure Sodium	150HPS208	208	188	1.15
E-23½ Clear Mogul Base	150HPS240	240	188	1.00
55 Volt	150HPS277	277	188	0.85
	150HPS480	480	188	0.44
250 Watt	250HPS120	120	300	2.75
High Pressure Sodium	250HPS208	208	300	1.60
E-18 Clear Mogul Base	250HPS240	240	300	1.38
	250HPS277	277	300	1.20
	250HPS480	480	300	0.69
400 Watt	400HPS120	120	465	4.30
High Pressure Sodium	400HPS208	208	465	2.48
E-18 Clear Mogul Base	400HPS240	240	465	2.15
	400HPS277	277	465	1.86
	400HPS480	480	465	1.07
175 Watt	175MH120	120	215	1.80
Metallic Halide BT-28	175MH208	208	215	1.04
or ED-28 Clear Pin	175MH240	240	215	0.90
Oriented Mogul Base	175MH277	277	215	0.78
	175MH480	480	215	0.45
250 Watt	250MH120	120	295	2.50
Metallic Halide BT-28	250MH208	208	295	1.44
or ED-28 Clear Pin	250MH240	240	295	1.25
Oriented Mogul Base	250MH277	277	295	1.08
	250MH480	480	295	0.65
400 Watt	400SMH120	120	455	4.00
Metallic Halide ED-28	400SMH208	208	455	2.30
Clear Venture* Pin	400SMH240	240	455	2.00
Oriented Mogul Base	400SMH277	277	455	1.75
	400SMH480	480	455	1.00
175 Watt	175MV120	120	200	1.75
Mercury Vapor E-28	175MV208	208	200	1.00
Coated Mogul Base	175MV240	240	200	0.87
	175MV277	277	200	0.78
	175MV480	480	200	0.44
250 Watt	250MV120	120	285	2.50
Mercury Vapor E-28	250MV208	208	285	1.50
Coated Mogul Base	250MV240	240	285	1.25
	250MV277	277	285	1.10
	250MV480	480	285	0.62

Uplight/Downlight with Minimal Light Trespass

SUNDOWNER™ 19

Sundowner offers light sculpturing and indirect lighting with controls that dramatizes walls, ceilings and surfaces with unparalleled uniformity... both indoor and outdoor.

Available in wattages from 150-400 watt, Sundowner's vertical lamp position and optical system develops a sharp 85-degree light cutoff and uniform light distribution that is unique for uplight applications.

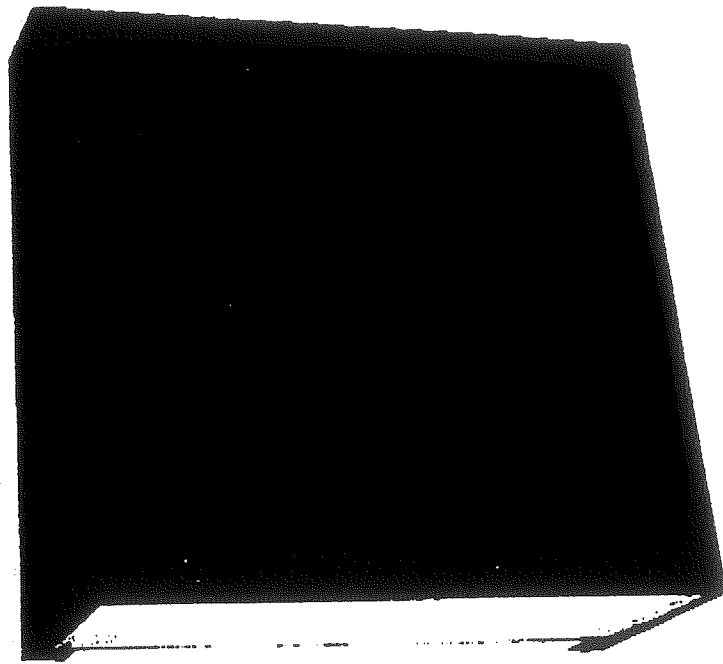
Ideal for lighting exterior walls, soffits and overhangs, tunnels, walkways, garages, stairwells and canopies.

Indoor applications include shopping malls, auditoriums and convention centers. Double-up Sundowner units to create an uplight/downlight combination that is architecturally arresting.

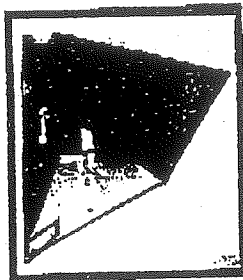
The corrosion resistant canopy is sealed to the mounting plate, and the tempered diffused glass lens is silicone sealed in fixture canopy to prevent water and minimize insect infiltration. Units are

Listed for Damp Locations for uplighting.

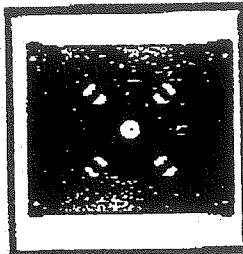
Sundowner - Another unique lighting tool for conquering demanding lighting requirements from Guth.



Canopy hinges for lamp or electrical maintenance and easily removes from backplate.



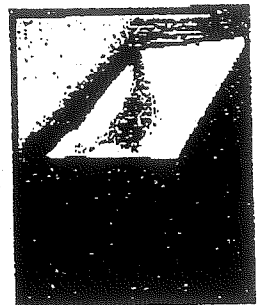
One-man hanging of SND 19 with adjustable mounting and leveling plate for conventional junction box.



Fixture canopy seals to backplate with quality silicone gasketing.



Diffusing glass lens is silicone sealed in canopy to resist moisture and insect infiltration.



Specifications/Features

GENERAL

- Sharp cutoff, wall mounted HID luminaire suitable for low glare applications and light trespass code compliance.
- Utilizes Metal Halide and High Pressure Sodium HID lamps up to 400W for best design options available.
- Wet location applications.
- Uplight mounting available. (Damp Location)

CONSTRUCTION

- Corrosion resistant .06" low copper content aluminum canopy and .09" back plate finished in baked bronze polyester powder coat.
- Easy one man installation with quick leveling, gasketed 19 ga 304 stainless steel mounting bracket; has extra holes for additional wall anchors; fixture simply attaches to 4 threaded studs on mounting plate.
- Canopy hinges and easily removable from back plate; enhances ease of installation.
- Prop rod included to hold fixture open and free hands for lamp replacement and maintenance.
- Specular aluminum reflectors produce front cutoff at 85 degree and 9/MH 2 73:1.

- Canopy sealed to back plate with extruded, high temperature, silicone gasket.
- Corrosion resistant stainless steel external hardware.
- 5/32" tempered diffused glass lens silicone sealed to prevent entrance of water, and minimize insect infiltration.
- Canopy secured by two captive stainless steel screws; optional temper resistant screws.

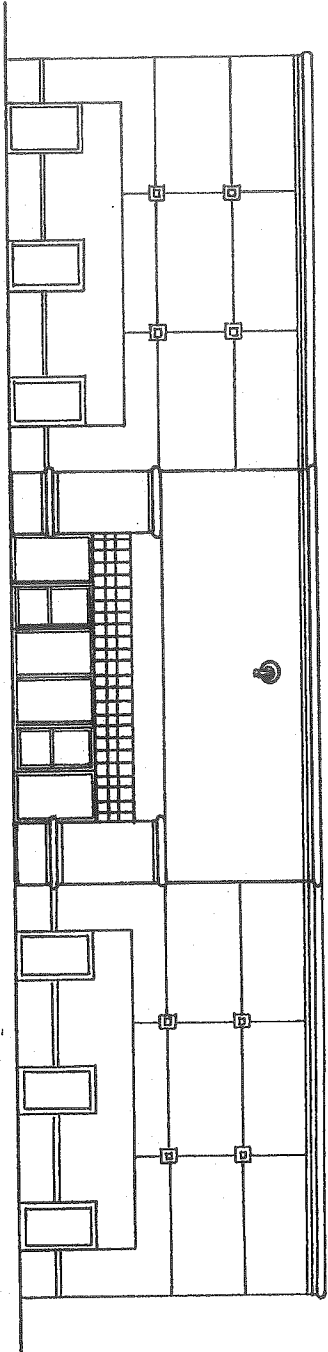
LISTINGS

- Listed 1572 Wet location for downlight and damp location for uplight versions.

ELECTRICAL

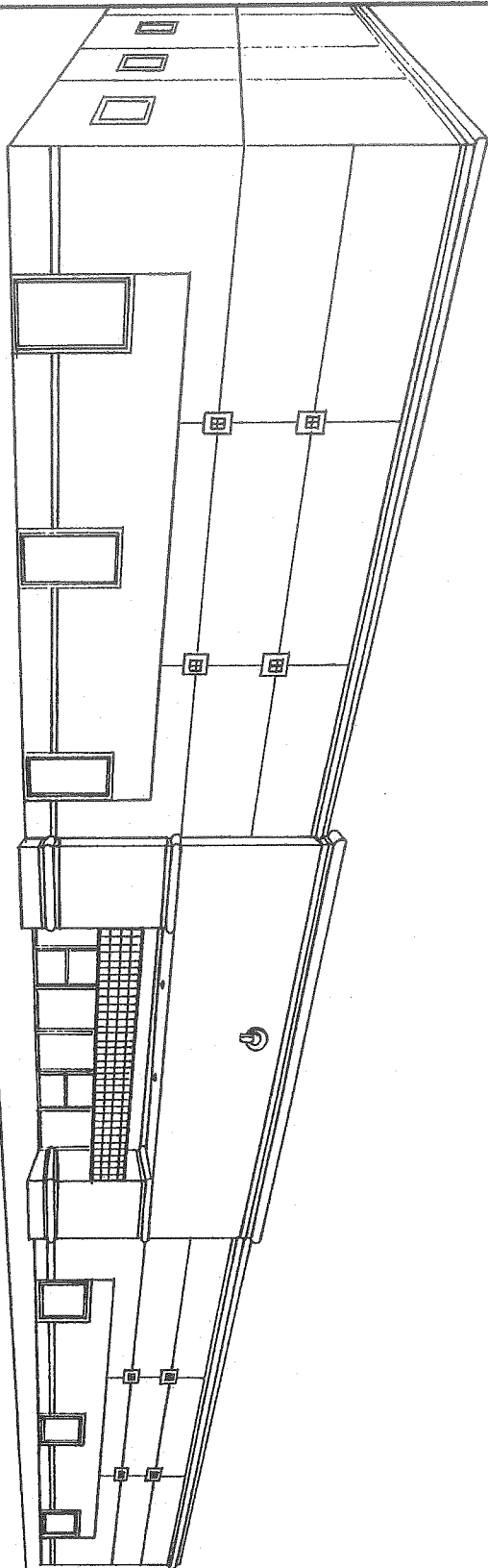
- Standard ballasts are 120V, HPS, maximum 400W mogul base HID lamp in vertical position.
- Ballast mounted to backplate with stake-in screws for positive grounding and secure mounting.
- Ground wire attached to backplate for positive grounding and quick installation.
- Optional button type photocell mounts in top of housing.
- All fixtures are IESW, Union made to ensure quality.

GUTH
LIGHTING

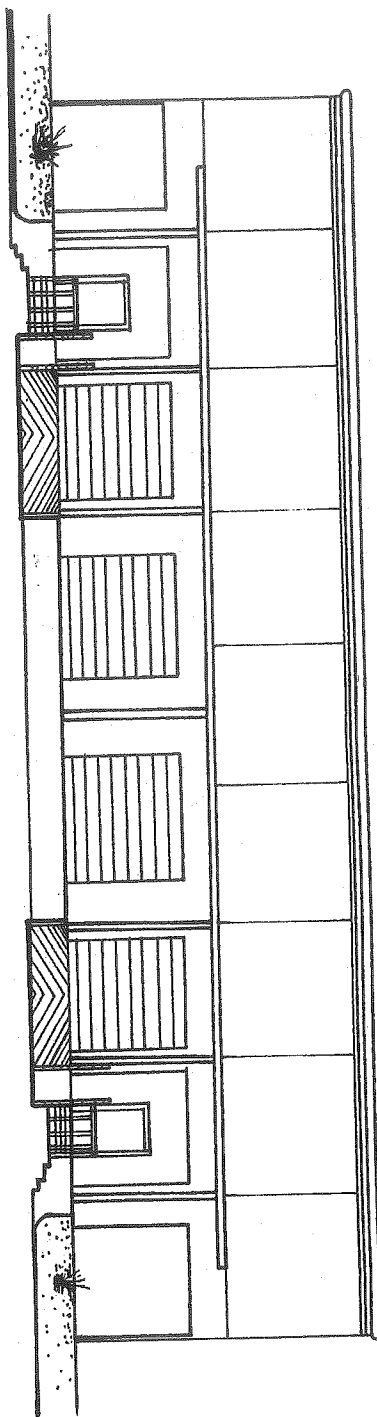
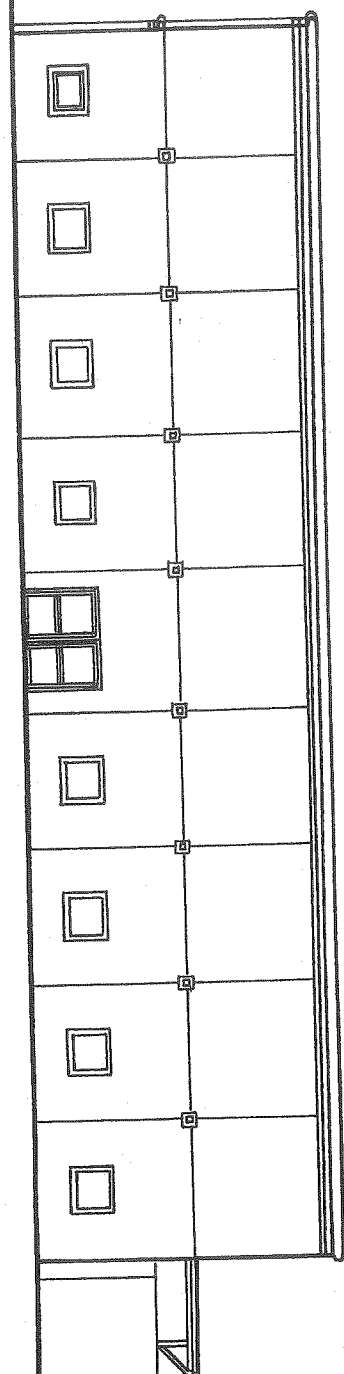


TOLERANCES		REVISIONS		
EXCEPT AS NOTED		NO.	DATE	BY
DECIMAL	1			
FRACTIONAL	2			
ANGULAR	3			
	4			
	5			

DRAWN BY **Y.A.**
 CHECKED
 DATE **05/98**
 APPROVED
 MATERIAL
 DRAWING NO.



TOLERANCES		REVISIONS			DRAWING BY T. A. SCHE		DATE 05/98		DRAWING NO.	
EXCEPT AS NOTED		NO.	DATE	BY	DRAWN BY T. A.		DATE 05/98		DRAWING NO.	
±	DECIMAL	1			CHECKED		APPROVED			
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TOLERANCES
EXCEPT AS NOTED

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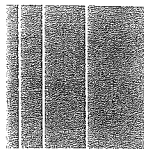
REVISIONS

NO. DATE BY

1
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SIDE + REAR

DRAWN BY Y.A.
CHECKED
DATE 05/98
SCALE
1/8" = 1'-0"
MATERIAL
DISCREPANCY



SebagoTechnics
Engineering & Planning for the Future

July 29, 1998
88017

Kandi Talbot, Planner
Planning & Urban Development
City of Portland
389 Congress Street
Portland, ME 04101

160 Riverside Street, J Properties

Dear Kandi:

Attached for your review and consideration are revised plans and Stormwater Management Report to address the comments raised by staff and the Portland Pipeline Corporation. Our revisions are as follows:

A. **General / Site Plan Change**

1. In response to concerns raised by the Portland Pipeline Corp. on development over their easement, we have moved the majority of pavement out of the easement area. This change also brought the building 10 feet closer to Riverside Street. Recent changes in PPC management of pipeline easements restrict placement of pavement on their easements. The inability to drive over the pipeline area will require tractor trailers to perform additional turning maneuvers in the rear of the building in order to exit the site. A sketch is attached showing how access will be accomplished.

B. **Responses to Public Works Engineering Comments from Anthony Lombardo dated July 10, 1998 (Attached)**

1. A Stormwater Management Report has been submitted to Jim Wendel at Deluca-Hoffman for review. Mr. Wendel has commented on the report and our response to his comments is attached.
2. A detail of the granite curb installation on Riverside Street is attached on Sheet D2, Detail #1.

3. The "typical trench detail" on Sheet D1, Detail #8 is designed to standard engineering practice and is specific for on-site construction. No new pipes are proposed within the City right-of-way which would require a separate trench detail per City standards.
4. Full plan sets are attached.
5. The site is designed for access by tractor trailers equal to a WB-50. The site entrance and internal drives have been modified to accomplish the turning movements of a WB-50. Truck movement in the loading area has been previously discussed. The entrance island at Riverside Street has been removed to improve truck flow onto the site.

C. Miscellaneous Comments from Staff

1. A fire hydrant exists at the entrance to the abutting property north of our site. This is noted on Sheet S2.

D. Responses to Jim Wendel Comments Dated July 21, 1998

1. Plans

- a. Response as previously discussed in Items A-1 and B-5.
- b. The limits of heavy duty pavement have been clarified.
- c. Guard rails are commonly installed in combination with geogrid fabric retaining walls and we have successfully installed them in various locations in Maine. We do not envision failure of the wall as a result of the guard rail.
- d. The roof drain has been downsized to an 8" diameter pipe and relocated to outlet into CB-2.
- e. Revised outlet control structure to include the following:
 - 1) change detail to include 12" inlet pipe from yard drain;
 - 2) change diameter of structure from 4' to 6' to allow for required amount of concrete between pipes;
 - 3) change manhole cover to "Bilco" style hatch to provide better access.

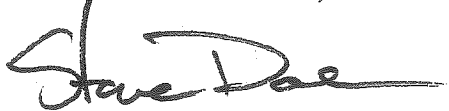
2. Stormwater Report

- a. A curve number of 80 was used because the bottom of the detention pond looks to be saturated throughout the year. This makes for a more conservative design.
- b. Revised analysis of Watershed 1 to reflect new time of concentration.
- c. Created Subcatchment 3 to account for the grass area between Watersheds 1 and 2. Subcatchment 3 will be routed through Pond 1.
- d. To better define the flow path for each watershed, I have enclosed a 1" = 30' scale drawing for Subcatchments 1 and 2.
- e. Provided additional calculations regarding incremental storage in pipes and structures. Provided a plan which shows the existing catch basins elevations. Roof drain is calculated as part of Watershed 1.

If you have any questions with regard to this response letter, please contact me.

Sincerely,

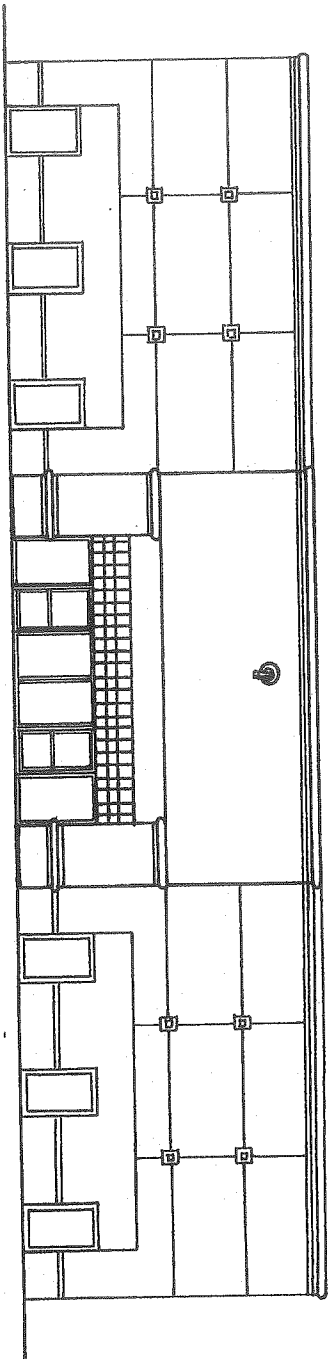
SEBAGO TECHNICS, INC.



Stephen G. Doe, R.L.A.
Landscape Architect

SGD:jc
Enc.

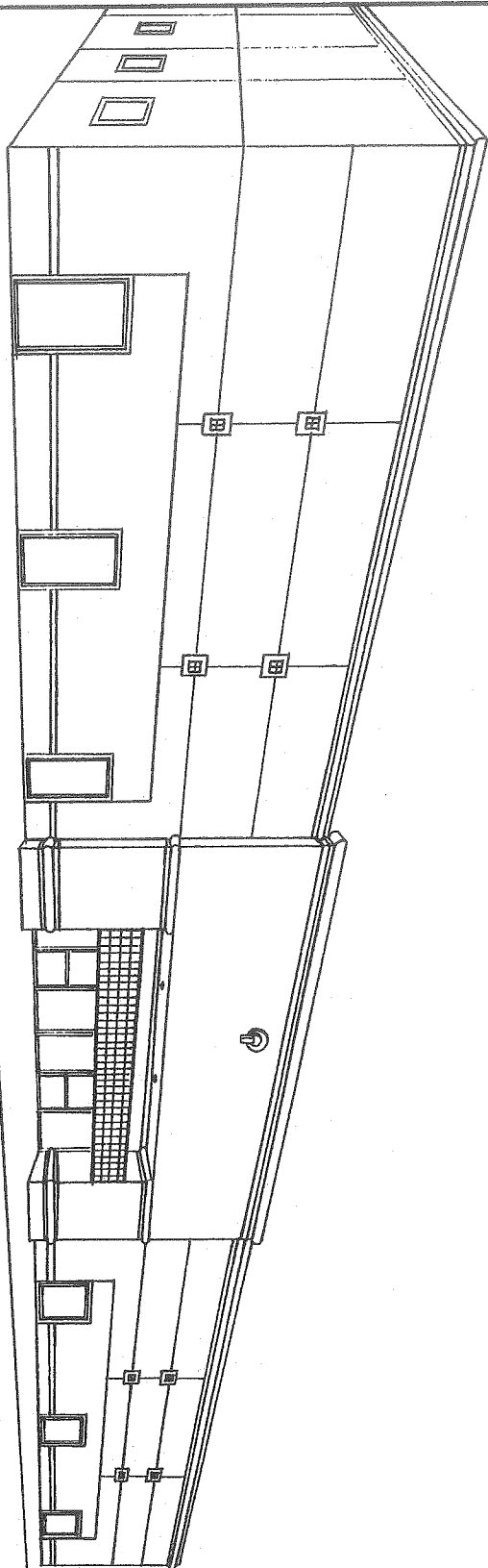
cc: John Locke, J Properties



TOLERANCES		REVISIONS			DRAWN BY		SCALE		DATE	
EXCEPT AS NOTED		NO	DATE	BY	Y. A.		DATE		DATE	
1		1			Y. A.		DATE		DATE	
2		2			Y. A.		DATE		DATE	
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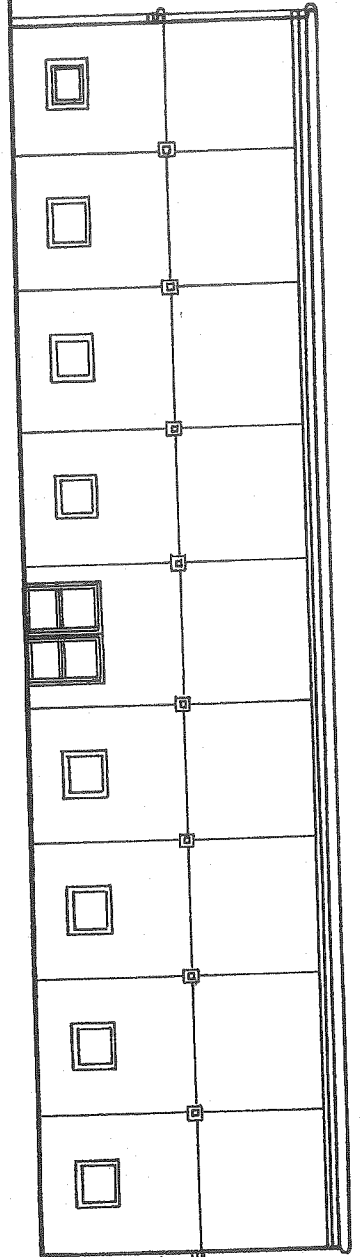
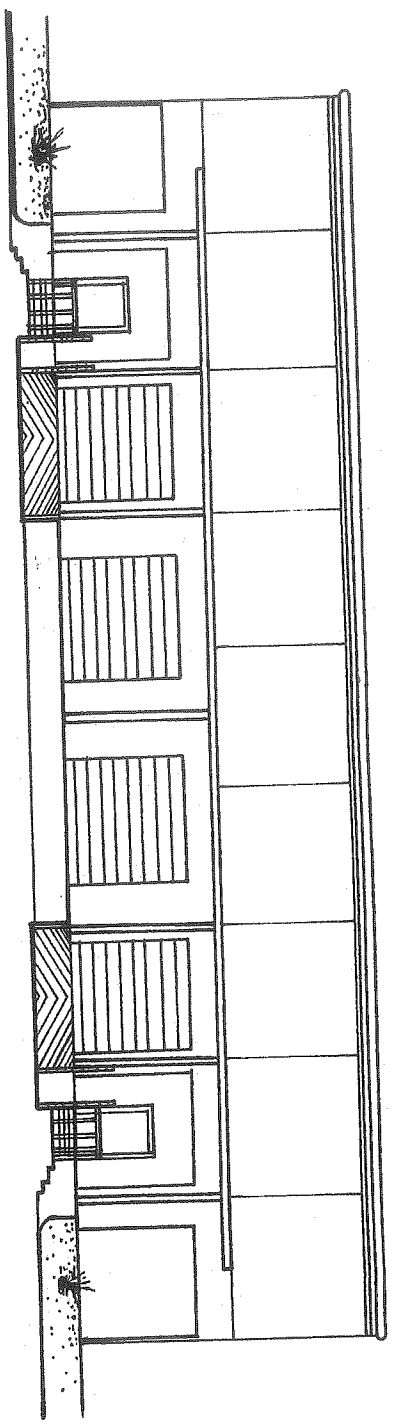
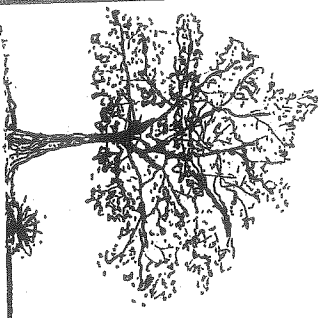
FRONT

DATE 05/98



TOLERANCES EXCEPT AS NOTED		REVISIONS			FRITZ IDE	
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DESIGNED BY	Y. A.	SCALE	
CHECKED		DATE	05/98
TRACED		APPROVED	



TOLERANCES EXCEPT AS NOTED

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REVISIONS

DESIGNED BY Y.A.

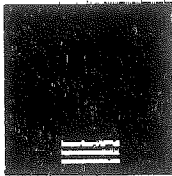
SCALE

DATE 05/98

APPROVED

DRAWING NO.

SIDE + REAR



DELUCA-HOFFMAN ASSOCIATES, INC.
CONSULTING ENGINEERS

778 MAIN STREET
SUITE B
SOUTH PORTLAND, MAINE 04106
TEL. 207 773 1131
FAX 207 879 0896

■ ROADWAY DESIGN
■ ENVIRONMENTAL ENGINEERING
■ TRAFFIC STUDIES AND MANAGEMENT
■ PERMITTING
■ AIRPORT ENGINEERING
■ SITE PLANNING
■ CONSTRUCTION ADMINISTRATION

MEMORANDUM

TO: Kandi Talbot, Planner

FROM: Jim Wendel, Development Review Coordinator

DATE: August 3, 1998

RE: Site Plan Review
Warehouse Facility
160 Riverside Street

Review of the site plan, Rev C, dated 7/15/98, stormwater and erosion control reports have been completed. I offer the following comments:

1. It is my opinion that access for WB-50 trucks to the facility does not work and that the site layout has not appropriately provided sufficient area at the truck dock for maneuvering a truck. This project is a trucking facility; it should be expected to operate effectively and efficiently for its intended use.
2. The stormwater analysis needs to be revised. To calculate the storage of the pond the analysis assumed that the storm drain system is flat. This assumption allows to great an error in the calculations. The system has pipe slopes of 0.2% to 0.5%. As an example, the difference in volume using the slope of the pipe is almost 5 times for the calculation at a water depth of 0.6ft.

Should you have any questions please call.

1350.10disk5/160rivr1

PUBLIC WORKS ENGINEERING
MEMORANDUM

To: Kandi Talbot, Planner

From: Anthony Lombardo, P.E., Project Engineer

Date: July 10, 1998

Subject: 160 Riverside Street.....Proposed Warehouse/Office Building

The following comments were generated during Public Works Engineering review of proposed warehouse/office building located at 160 Riverside Street. The application and plans were dated July 6, 1998.

The submittal package received at Public Works did not contain the "stormwater management report". The runoff from this site is collected into an existing system that contributes runoff to the environmentally sensitive Capisic Brook Watershed. The pre and post development peak runoff values must be reviewed for the proposed site, as well as the existing detention pond receiving proposed development runoff.

Applicant must provide a detail specific to the installation of granite curb in the Riverside Street right of way. This detail must be drawn to City of Portland Design and Technical Standards.

The "typical trench detail" on sheet D1 needs to be drawn to City of Portland Standards. The City's detail is specific for pipe work within the City's right of way. This detail specifies 12 inches of crushed stone above the top of flexible (PVC) pipe.

The plan set received at Public Works does not include Sheet D2.

Applicant needs to specify what size trucks will utilize this development. Further, the applicant must verify that the specified trucks can negotiate the Riverside Street entrance and the internal drives, parking and loading area.

Engineer Review and Site Inspection Fee Invoice Worksheet

Address: 160 Riverside Street..... DATE: 7/10/98

Engineering Review

To be filled out by Development Review Coordinator and Public Works at time of application.

Planning

of Hours Estimated: (Private Improvements)

Field Work _____

Memos/Corresp. _____

Review/Analysis _____

Meetings/phone calls _____

Total Hours _____ at _____ per hour

Review Fee (Private): \$ _____

Development Review Coordinator Signature

Public Works

of Hours Estimated: (Public Improvements)

Field Work 1.0

Memos/Corresp. 2.0

Review/Analysis 2.0

Meetings/phone calls 1.0

Total Hours 6.0 at \$35 per hour

Review Fee (Public): \$ \$210

Public Works Engineer Signature

Site Inspection

To be filled out by DRC and Public Works at time of Performance Guarantee approval.

Planning

____ Accept 1.7% of Private Improvements P.G.
\$ _____ (dollar amount)

of Hours Estimated:

Field Work _____

Memos/Corresp. _____

Review/Analysis _____

Meetings/phone calls _____

Total Hours _____ at _____ per hour

Alternate Inspection Fee (Private): \$ _____

Development Review Coordinator Signature

Public Works

____ Accept 1.7% of Private Improvements P.G.
\$ _____ (dollar amount)

of Hours Estimated:

Field Work 6.0

Memos/Corresp. 1.0

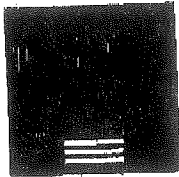
Review/Analysis _____

Meetings/phone calls 1.0

Total Hours 8.0 at \$35 per hour

Alternate Inspection Fee (Public): \$ \$280

Public Works Engineer Signature



DELUCA-HOFFMAN ASSOCIATES, INC.
CONSULTING ENGINEERS

778 MAIN STREET
SUITE 8
SOUTH PORTLAND, MAINE 04106
TEL. 207 775 1121
FAX 207 879 0896

- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

MEMORANDUM

TO: Kandi Talbot, Planner

FROM: Jim Wendel, Development Review Coordinator

DATE: July 21, 1998

RE: Site Plan Review
Warehouse Facility
160 Riverside Street

Review of the site plan, stormwater and erosion control reports have been completed. I offer the following comments:

I. Plans

1. What size trucks are expected to use this facility? It is my opinion that a WB-50 vehicle can not maneuver into the truck dock bays. The "S" maneuver in front of the building is likely to be tight but manageable; their would be interference if another vehicle was in this area. The 30' radii at the entrance are to small to keep a right turning vehicle within the receiving lane in Riverside Street; given the traffic congestion in Riverside Street this should be a requirement since this is a warehouse facility.
2. The limit of the heavy duty pavement is not clear. Is the dashed line beside the 11 parking spaces a boundary?
3. Installation of the guard rail beside the truck dock will likely damage the geogrid fabric for the retaining wall. How will this be prevented?
4. The angle between the storm drain pipe and the roof drain of CB-3 is too small to maintain the 1' minimum of concrete between the holes for the pipes.
5. The outlet control structure detail does not show the 12" inlet pipe from the yard drain; which side of the control weir does this 12" pipe connect? Also the diameter of the structure is too small for the required amount of concrete between pipes and particularly with the weir plate. Recommend a Bilco style hatch be provided for access.

**DeLUCA HOFFMAN ASSOCIATES, INC.
CONSULTING ENGINEERS**

6. An existing catch basin along the curblin in Riverside Street at the entrance and behind the southerly abutter is missing.
7. The construction schedule on sheet D1 needs to be revised; it does not match the schedule in the E & S report.

II. Stormwater Report

1. The applicant should justify the use of a CN value of 80 for the existing detention basin. Is it lined with clay? if it is constructed with native soils, Au Greu, than the CN value associated with the B hydrologic soil type should be used. Given the soil type, recommend the analysis consider an infiltration component within the detention basin.
2. The sheet flow component for proposed watershed 1 appears to be too long; I would estimate it to be no more than 50'-70'. Also it is my opinion that there is a shallow concentrated flow component along the pavement. Finally the length of the 12" pipe is approximately 460', not 515'. This analysis should be revised to reflect the revised site plan.
3. With the construction of this project the area of watershed 2 is larger. The increased area is lawn that is between the projects. This additional area is missing from the analysis.
4. The flow path for each watershed is not well defined as to location and the component lengths.
5. The calculation of storage for the basin is not clear; a segment of the existing 12" storm drain appears to be missing; the calculation for the vertical footage for catch basins is not clear; finally the calculation seems to ignore how the shape of the pond changes as the elevation increases. In the post condition the roof drain is ignored and it should not be and the actual length of pipe has changed as noted in item 2 above.

Should you have any questions please call.

1350.10disk5/160river

City of Portland Planning Department

389 Congress Street, 4th Floor
Portland, ME 04101
207-874-8721 or 207-874-8719
Fax: 207-756-8258

FAX TRANSMISSION COVER SHEET

Date: February 26, 1999
To: Tom Hey
From: Kandi Talbot
Fax: 854-4315
Re: 160 Riverside Street

YOU SHOULD RECEIVE 5 PAGE(S),
INCLUDING THIS COVER SHEET.
IF YOU DO NOT RECEIVE ALL THE PAGES,
PLEASE CALL 207-874-8721 or 207-874-8719.

Department of Planning and Urban Development
SUBDIVISION/SITE DEVELOPMENT

COST ESTIMATE OF IMPROVEMENTS TO BE COVERED BY PERFORMANCE GUARANTEE

Date 2/8/99

Name of Project J Properties
 Address/Location 160 RIVERSIDE ST.
 Developer J Properties
 Form of Performance Guarantee Fleet Bank of MAINE
 Type of Development: _____ Subdivision Site Plan (Major Minor)

TO BE FILLED OUT BY APPLICANT:

Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
1 STREET/SIDEWALK						
ROAD PAVEMENT (GRAVELS), EXCAVATION INCL.				1067 SF	20.-	21,341.-
Granite Curbing						
Sidewalks						
Esplanades						
Monuments						
Street Lighting						
Other						
2 SANITARY SEWER						
Manholes				2 EA.	1700.00	3400.-
Piping				317 LF	24.-	7608.-
Connections						
Other						
3 STORM DRAINAGE						
Manholes						
Catchbasins				4 EA.	1700.-	6800.-
Piping				820 LF	25.-	20,503.-
Detention Basin				45	1500.-	1500.-
Other INSULATION				45	450.-	450.-
SITE LIGHTING						
EROSION CONTROL				4/5	\$ 2220.00	\$ 2220
RECREATION AND OPEN SPACE AMENITIES						

FROM : ALFONSO FOODS

PHONE NO. : 9549742773

Feb. 08 1999 02:27PM P3

OCT-20-98 09:02 AM PLANNING DEPARTMENT

750#258

P. 04

Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
LANDSCAPING (Attach breakdown of plant materials, quantities, and unit costs)						
MISCELLANEOUS						
TOTAL						
GRAND TOTAL:						\$63,834.00

INSPECTION FEE (to be filled out by City)

	PUBLIC	PRIVATE	TOTAL
A. 1.7% of totals:	1,085.18		
of			
B. Alternative Assessment:			
Assessed by:	(name)	(name)	

Department of Planning and Urban Development
SUBDIVISION/SITE DEVELOPMENT

COST ESTIMATE OF IMPROVEMENTS TO BE COVERED BY PERFORMANCE GUARANTEE

Date 10/27/98

Name of Project J PROPERTIES
 Address/Location 160 RIVERSIDE ST
 Developer J PROPERTIES
 Form of Performance Guarantee FLEET BANK OF MAINE
 Type of Development: Subdivision Site Plan (Major/Minor)

TO BE FILLED OUT BY APPLICANT:

Item	Quantity	PUBLIC		PRIVATE		
		Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
1. STREET/SIDEWALK						
Road				3300SY	5.13	\$ 16,929.-
Granite Curbing				225LF	30.-	\$ 6,750.-
Sidewalks						
Esplanades						
Monuments						
Street Lighting						
Other <u>SAW CUTTING</u>				100LF	3.-	\$ 300.-
2. SANITARY SEWER						
Manholes						
Piping						
Connections						
Other						
3. STORM DRAINAGE						
Manholes						
Catchbasins						
Piping						
Detention Basins						
Other						
SITE LIGHTING						
EROSION CONTROL				162LF	2.50	\$ 405.-
RECREATION AND OPEN SPACE AMENITIES						

FROM : ALFONSO FOODS

PHONE NO. : 9549742773

Oct. 27 1999 01:33PM P4

CITY OF MIAMI PLANNING DEPARTMENT

P.L.M.

ITEM	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
* LANDSCAPING (Attach breakdown of plant materials, quantities, and unit costs)	_____	_____	_____	_____	_____	_____
* MISCELLANEOUS CLEARING, TOTAL LOAM, X	_____	_____	_____	.35A	_____	1715.-
GRAND TOTAL:	_____	_____	_____	_____	_____	26,290.-

INSPECTION FEE (to be filled out by City)

	PUBLIC	PRIVATE	TOTAL
A. 1.7% of totals:	_____	_____	_____
of	_____	_____	_____
B. Alternative Assessment:	_____	_____	_____
Assessed by:	_____	_____	_____
	(name)	(name)	

MEMORANDUM

TO: Kandi Talbot, Planner

FROM: Jim Wendel, PE, Development Review Coordinator

DATE: February 5, 1999

RE: Request for Reduction in Performance Guarantee
Retail Store
160 Riverside Street

The owner has requested a reduction in the performance guarantee. The current and original amount of the guarantee is \$26,290.00.

A phone conversation with Ken Gordin, the site contractor, was made to assess the work completed based on the performance guarantee estimate. The following work has been completed:

1. Entrance within the right of way and a portion of the parking field has been constructed to the base pavement surface; no curbing has been installed.

2. The sewer system is complete; this item is not part of the partial performance guarantee.

The sum based on the performance guarantee for the completed work above is \$13,050.00.

Should you have any questions, please call.

JN1359.10/1330.10dsk10/160hr

**CITY OF PORTLAND, MAINE
ENGINEERING REVIEW FORM**

Address of Proposed Site 160 Riverside Street Date 9/11/98
Project Description Warehouse/New Bldg Job # 19980080
Applicant J. Properties
Applicant's Mailing Address 5735 Harrington Way, Boca Raton
FL 33496

Site Review
(Planning Department)

Review Engineer: Jim Wendel
Number of Estimated Hours: 11
Cost Per Hour: \$48.00
Total Amount: \$528.00

Right-of-Way Review
(Public Works Department)

Review Engineer: Tony Lombardo
Number of Estimated Hours: 6
Cost Per Hour: \$35.00
Total Amount: \$210.00

An engineering fee has been assessed in the amount of \$738.00 for the review of your project located at 160 Riverside Street

Please make check payable to the City of Portland. The check should be submitted along with this form to the Portland Planning Department, City of Portland, 4th Floor, 389 Congress Street, Portland, ME 04101. Attn: Kandi Talbot

Office Use Only	
Invoice Date: <u>9/11/98</u>	Received: <u>10/5/98</u> date
Planning Revenue Code: <u>04 (101366011.9)</u>	
Public Works Revenue Code: <u>PV (101316011.9)</u>	

- cc: Applicant - white
- Planner - blue
- Engineer - green
- Public Works - yellow
- Financial Officer - pink
- Review/Inspection Fee File - golden

If you have any problems
receiving please notify me
at 954 960 1010 x4

From: John Locke
I Properties

ATTN: Candy TALBOT

COVER
Sheet

NOVEMBER 6, 1998
RE APPLICATION OF J-PROPERTIES SITE IMPROVEMENT
DEAR CAROL TALBOT
160 RIVERSIDE ST.

SITE PLAN/SUBDIVISION
PERFORMANCE GUARANTEE
ESCROW ACCOUNT
FLEET BANK

THIS WILL CERTIFY THAT I AGREE TO
ALL TERMS OF OUR PERFORMANCE GUARANTEE
ESCROW ACCOUNT I SO SWEAR
John Locke 11-6-98
J Properties Partnership
John E. Locke.

11-6-98

1 of 1

J. PROPERTIES
5735 HARRINGTON WAY
BOCA RATON, FL 33496

Financial
Management
Account



1654

62-15/311

2/11 19 95

PAY TO THE ORDER OF CITY OF PORTLAND \$ 1085.18

One Thousand Eighty Five - 18/100 DOLLARS

SMITHBARNNEY

PNC National Bank
Wilmington, DE

375
E000

FOR Inspection

⑆031100157⑆ 1204753315⑆ 1654



SITE PLAN/SUBDIVISION
PERFORMANCE GUARANTEE:
ESCROW ACCOUNT
FLEET BANK OF MAINE
[ACCOUNT # 9360199026]

February 8, 1999

Joseph E. Gray, Jr.
Director of Planning and Urban Development
City of Portland
389 Congress St.
Portland, ME 04101

RE: Application of J-Properties Site Improvements at 160 Riverside St., Portland, Me.

Dear Mr. Gray:

This will certify to you that Fleet Bank of Maine will hold the sum of SIXTY THREE THOUSAND EIGHT HUNDRED THIRTY FOUR DOLLARS (\$63,834.00) in an interest bearing escrow in the name of the City of Portland established with Fleet Bank of Maine. We will hold these funds as escrow agent for the benefit of the City of Portland on the following conditions:

1. These funds represent the estimated cost of installing site improvements as depicted on the site plan and estimated on Attachment 1. (Attachment 1 is the approved estimated form.)
2. The City of Portland may draw against this escrow account by presentation of a draft in the event that:
 - a) J-Properties Partnership fails to satisfactorily complete by November 1, 2000 the work as stipulated in Paragraph 1. Said draft shall be accompanied by a written statement from the Director of Planning and Urban Development that J-Properties Partnership has failed to satisfactorily complete such work, with a listing of improvements still to be completed, and the estimated cost of completing said improvements still to be completed as determined by the Department of Public Works.
 - b) J-Properties Partnership fails to inform the City for all inspections in conjunction with the installation of public improvements noted in paragraph 1: or
 - c) J-Properties Partnership fails to post a ten percent (10%) Defect bond or Guarantee as provided in Section 14-501 and 14-525.
3. The City of Portland may draw against this escrow for a period not to exceed 90 days after the expiration of this commitment; provided that J-Properties Partnership will give the City written notice, by certified mail, of the deadline of this escrow at least 90 days prior thereto to the Director of Planning & Urban Development; otherwise drafts must be submitted by the city of Portland no later than 90 days following written notice whenever given thereafter.



4. After all underground work in the public right of way has been completed and inspected to the satisfaction of the Department of Public Works, including but not limited to the installation of granite curbing, sidewalk, curb cut, electrical conduits, street trees and other required improvements constructed chiefly below grade, the City of Portland Director of Planning and Urban Development or the City of Portland Director of Finance, as provided in Section 14-501 of the Portland City Code, may authorize Fleet Bank of Maine, by written certification, to reduce the available amount of the escrowed money by a specified amount.
5. J-Properties Partnership will notify the City of Portland for inspections.
6. All costs associated with establishing, maintaining and disbursing funds from the escrow account shall be borne by J-Properties Partnership.
7. This escrow account expires on April 16, 2001, or as automatically extended according to paragraph 3, but may expire to this date when the City of Portland acknowledges in writing to Fleet Bank of Maine and J-Properties Partnership that said work as outlined has been completed in accordance with the City of Portland specifications.

Dated at Portland, Maine this 9th Feb 1999

Very truly yours,

Fleet Bank of Maine

By: [Signature]
Neil Creek Fleet Bank of Maine

Date: 2/9/99

Seen and Agreed to:

By: [Signature]
 John Locke, J-Properties Partnership

Date: 2/9/99

Approved pursuant to 14-501(a) of the Portland City Code:

By: _____
 Director of Planning and Urban
 Development

Date _____

By: [Signature]
 Corporation Counsel

Date 2/9/99

By: _____
 Finance Director

Date _____

FROM : ALFONSO FOODS

PHONE NO. : 9549742773

Feb. 08 1999 02:26PM P1



R. J. GRONDIN & SONS
General Contractors

TO : JOHN LOCKE
FROM : KEN GRONDIN
DATE : JANUARY 29, 1999

PROJECT : PHASE 2 SITEWORK FOR PROPOSED BUILDING
160 RIVERSIDE STREET
PORTLAND, MAINE.

DEAR JOHN,

INCLUDED IS BREAKDOWN OF PHASE 2 WORK TO BE DONE IN
FEB. MARCH OF 1999. OUR PRICE IS SIXTY THREE THOUSAND
EIGHT HUNDRED THIRTY FOUR DOLLARS (\$ 63,834.00)

THE FOLLOWING ITEMS ARE INCLUDED :

02050 DEMOLITION	\$ 250.00
02270 EROSION CONTROL.....	\$ 2,220.00
02814 PAVEMENT.....	\$ 21,341.00
02700 SANITARY SEWER.....	\$ 11,020.00
02720 STORM DRAIN.....	\$ 29,003.00

IF YOU HAVE ANY QUESTIONS DO NOT HESITATE TO CALL.

SINCERELY,

KENNETH GRONDIN
File 98-161 ph.2



SITE PLAN/SUBDIVISION
PERFORMANCE GUARANTEE:
ESCROW ACCOUNT
FLEET BANK OF MAINE
[ACCOUNT # 9360199026]

February 8, 1999

Joseph E. Gray, Jr.
Director of Planning and Urban Development
City of Portland
389 Congress St.
Portland, ME 04101

RE: Application of J-Properties Site Improvements at 160 Riverside St., Portland, Me.

Dear Mr. Gray:

This will certify to you that Fleet Bank of Maine will hold the sum of SIXTY THREE THOUSAND EIGHT HUNDRED THIRTY FOUR DOLLARS (\$63,834.00) in an interest bearing escrow in the name of the City of Portland established with Fleet Bank of Maine. We will hold these funds as escrow agent for the benefit of the City of Portland on the following conditions:

1. These funds represent the estimated cost of installing site improvements as depicted on the site plan and estimated on Attachment 1. (Attachment 1 is the approved estimated form.)
2. The City of Portland may draw against this escrow account by presentation of a draft in the event that:
 - a) J-Properties Partnership fails to satisfactorily complete by November 1, 2000 the work as stipulated in Paragraph 1. Said draft shall be accompanied by a written statement from the Director of Planning and Urban Development that J-Properties Partnership has failed to satisfactorily complete such work, with a listing of improvements still to be completed, and the estimated cost of completing said improvements still to be completed as determined by the Department of Public Works.
 - b) J-Properties Partnership fails to inform the City for all inspections in conjunction with the installation of public improvements noted in paragraph 1: or
 - c) J-Properties Partnership fails to post a ten percent (10%) Defect bond or Guarantee as provided in Section 14-501 and 14-525.
3. The City of Portland may draw against this escrow for a period not to exceed 90 days after the expiration of this commitment; provided that J-Properties Partnership will give the City written notice, by certified mail, of the deadline of this escrow at least 90 days prior thereto to the Director of Planning & Urban Development; otherwise drafts must be submitted by the city of Portland no later than 90 days following written notice whenever given thereafter.



4. After all underground work in the public right of way has been completed and inspected to the satisfaction of the Department of Public Works, including but not limited to the installation of granite curbing, sidewalk, curb cut, electrical conduits, street trees and other required improvements constructed chiefly below grade, the City of Portland Director of Planning and Urban Development or the City of Portland Director of Finance, as provided in Section 14-501 of the Portland City Code, may authorize Fleet Bank of Maine, by written certification, to reduce the available amount of the escrowed money by a specified amount.
5. J-Properties Partnership will notify the City of Portland for inspections.
6. All costs associated with establishing, maintaining and disbursing funds from the escrow account shall be borne by J-Properties Partnership.
7. This escrow account expires on April 16, 2001, or as automatically extended according to paragraph 3, but may expire to this date when the City of Portland acknowledges in writing to Fleet Bank of Maine and J-Properties Partnership that said work as outlined has been completed in accordance with the City of Portland specifications.

Dated at Portland, Maine this 9th Feb 1999

Very truly yours,

Fleet Bank of Maine

By: [Signature]

Date: 2/9/99

Neil Creek Fleet Bank of Maine

Seen and Agreed to:

By: [Signature]

Date: 2/9/99

John Locke, J-Properties Partnership

Approved pursuant to 14-501(a) of the Portland City Code:

By: [Signature]

Date: 2/10/99

Director of Planning and Urban
Development

By: [Signature]

Date: 2/9/99

Corporation Counsel

By: _____

Date: _____

Finance Director



OCT 30 1998

SITE PLAN/SUBDIVISION
PERFORMANCE GUARANTEE:
ESCROW ACCOUNT
FLEET BANK OF MAINE
[ACCOUNT # 9360199026]

October 29, 1998

Joseph E. Gray, Jr.
Director of Planning and Urban Development
City of Portland
389 Congress St.
Portland, ME 04101

RE: Application of J-Properties Site Improvements at 160 Riverside St., Portland, Me.

Dear Mr. Gray:

This will certify to you that Fleet Bank of Maine will hold the sum of TWENTY SIX THOUSAND TWO HUNDRED NINETY DOLLARS (\$26,290.00) in an interest bearing escrow in the name of the City of Portland established with Fleet Bank of Maine. We will hold these funds as escrow agent for the benefit of the City of Portland on the following conditions:

1. These funds represent the estimated cost of installing site improvements as depicted on the site plan and estimated on Attachment 1. (Attachment 1 is the approved estimated form.)
2. The City of Portland may draw against this escrow account by presentation of a draft in the event that J-Properties Partnership fails to complete by November 1, 2000 the work as stipulated in Paragraph 1. Said draft shall be accompanied by a written statement from the Director of Parks and Public Works or the Director of Planning and Urban Development that J-Properties Partnership has failed to complete such work, with a listing of improvements still to be completed, and the estimated cost of completing said improvements still to be completed as determined by the Department of Public Works.
3. The City of Portland may draw against this escrow for a period not to exceed 90 days after the expiration of this commitment; provided that J-Properties Partnership will give the City written notice of the deadline of this escrow at least 90 days prior thereto; otherwise drafts must be submitted no later than 90 days following written notice whenever given thereafter.
4. After all work in the public right of way has been completed and inspected to the satisfaction of the Department of Public Works, including but not limited to the installation of granite curbing, sidewalk, curb cut and street trees, Fleet Bank of Maine shall be eligible to receive a reduction in its obligations hereunder equal to the estimated cost of improvements. In no case, however, shall the obligations of Fleet Bank of Maine hereunder be reduced to an amount which is less than the

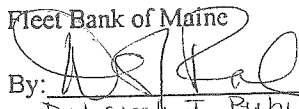


estimated cost of completing all prescribed improvements as determined by the Department of Public Works, as described above.

- 5. J-Properties Partnership will notify the City of Portland for inspections.
- 6. All costs associated with establishing, maintaining and disbursing funds from the escrow account shall be borne by J-Properties Partnership.
- 7. This escrow account expires on April 16, 2001 but may expire prior to the date when the City of Portland acknowledges in writing to Fleet Bank of Maine and J-Properties Partnership that said work as outlined has been completed in accordance with the City of Portland specifications.

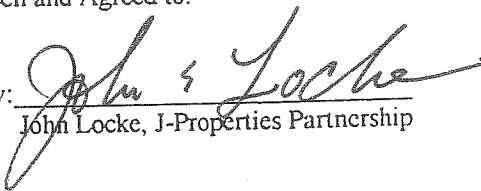
Dated at Portland, Maine this 28 October 1998

Very truly yours,

Fleet Bank of Maine
 By: 
 Deborah J. Rubino
 Business Specialist Fleet Bank of Maine

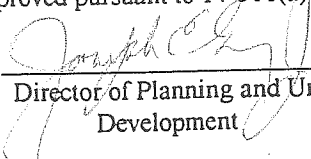
Date: 10/28/98

Seen and Agreed to:


By: 
 John Locke, J-Properties Partnership

Date: 10/28/98

Approved pursuant to 14-501(a) of the Portland City Code:

By: 
 Director of Planning and Urban
 Development

Date: 2/10/99

By: 
 Corporation Counsel

Date: 3/10/99

By: _____
 Finance Director

Date: _____



R. J. GRONDIN & SONS
11 Bartlett Road
Gorham, Maine 04038
(207) 854-1147 • FAX (207) 854-4316

INVOICE 44012

J. Properties
2211 NW 30th Place
Pompano, FL 33069

DATE:	3/10/00
JOB #:	2351
CUST. #:	53700

JOB NAME	JOB LOCATION	REQUISITION #	PURCHASE ORDER #	
Site Work	160 Riverside, PtInd	Requisition #4		
DATE	QUANTITY	PRICE	AMOUNT	

Work to Date (3/10/00)	91,704.86	
Less Prior Requisitions	<u>71,014.86</u>	
This Requisition		\$ <u>20,690.00</u>



This Requisition	20,690.00
Past Due Balance	<u>57,774.86</u>
Total DUE	<u>78,464.86</u>



TERMS: NET 30 DAYS, 1% MONTHLY INTEREST, EQUAL TO 12% ANNUALLY, CHARGED AFTER 30 DAYS.

MEMORANDUM

TO: Duane Kline, Finance Department
FROM: Joe Gray Jr., Director of Planning and Urban Development
DATE: January 20, 1999
RE: Request for Reduction in Performance Guarantee

A request by the owner of the commercial project at 160 Riverside Street has been received for a reduction in the performance guarantee amount. This is the first reduction requested.

Original Amount	\$133,587.00
Current Amount	\$133,587.00
Reduction Requested	<u>\$28,202.00</u>
Remaining Amount	\$105,385.00

The remainder is the sum, based on the performance guarantee, necessary to complete the work as outlined in a memo from the DRC dated January 20, 1999 to Kandi Talbot and Code Enforcement.

APPROVED

Joe Gray Jr., Director
Planning and Urban Development

c. Kandi Talbot, Planner
Code Enforcement
James Wendel, DRC

1359.10/1350.10disk#9/bnd160r

MEMORANDUM

To: Kandi Talbot, Planner
From: James T. Wendel, PE, Development Review Coordinator
Date: January 20, 1999
Re: Request for Reduction in Performance Guarantee
Retail Store
160 Riverside Street

The owner has requested a reduction in the amount of the performance guarantee. The current and original amount of the guarantee is \$133,587.00.

A phone conversation with Ken Grondin, the site contractor, was made to assess the work completed based on the performance guarantee estimate. The following work has been completed:

1. Entrance within the right of way has been constructed to the base pavement surface; no curbing installed.
2. The sewer system is complete.
3. The subbase gravel within the site has been placed.

The sum for the completed work above is \$28,202.00

Should you have any questions please call.

1359.10/1350.10disk#9/160rivr

160 RIVERSIDE ST.
 PORTLAND, ME

LANDSCAPE COST BREAKDOWN

ITEM	QTY	UNIT COST	TOTAL
AP	4	\$ 200	\$ 800
PN	4	\$ 150	600
VM	7	\$ 25-	175
TH	39	\$ 50-	1950
SB	18	\$ 15-	270
TO	13	\$ 80	1360
DL	153	\$ 4-	612
PJM	2	\$ 45-	90
PLANT MATERIAL TOTAL			\$ 5857
INSTALLATION X 2.2			\$ 12885.40

**CITY OF PORTLAND, MAINE
ENGINEERING REVIEW FORM**

Address of Proposed Site 160 Riverside Street Date 9/11/98
Project Description Warehouse/New Bldg Job # 1998 0080
Applicant J. Properties
Applicant's Mailing Address 5735 Harrington Way, Boca Raton
FL 33496

Site Review
(Planning Department)

Review Engineer: Jim Wendel
Number of Estimated Hours: 11
Cost Per Hour: \$48.00
Total Amount: \$528.00

Right-of-Way Review
(Public Works Department)

Review Engineer: Tony Lombardo
Number of Estimated Hours: 6
Cost Per Hour: \$35.00
Total Amount: \$210.00

An engineering fee has been assessed in the amount of \$738.00 for the review of your project located at 160 Riverside Street

Please make check payable to the City of Portland. The check should be submitted along with this form to the Portland Planning Department, City of Portland, 4th Floor, 389 Congress Street, Portland, ME 04101. Attn: Kandi Talbot

Office Use Only

Invoice Date: _____ Received: _____
date
Planning Revenue Code: _____
Public Works Revenue Code: _____

- cc: Applicant - white
Planner - blue
Engineer - green
Public Works - yellow
Financial Officer - pink
Review/Inspection Fee File - golden

J. PROPERTIES
5735 HARRINGTON WAY
BOCA RATON, FL 33496



1640

62-15/311

Oct 5 19 98

PAY TO THE ORDER OF CITY OF PORTLAND \$ 738.00 / 100
Seven Hundred Thirty Eight and 00/100 DOLLARS

SMITH BARNEY
PNC National Bank
Wilmington, DE

375
O
P
E

FOR

JAC

⑆03⑆⑆00⑆57⑆ ⑆20⑆75⑆33⑆75⑆⑆

J. PROPERTIES
5735 HARRINGTON WAY
BOCA RATON, FL 33496



1617

62-15/311

10/30 19 98

PAY TO THE ORDER OF CITY OF PORTLAND \$ 446.93
Four Hundred forty six and 93/100 DOLLARS

SMITH BARNEY
PNC National Bank
Wilmington, DE

375
O
P
E

FOR

John Jackson

⑆03⑆⑆00⑆57⑆ ⑆20⑆75⑆33⑆75⑆⑆



SITE PLAN/SUBDIVISION
PERFORMANCE GUARANTEE:
ESCROW ACCOUNT
FLEET BANK OF MAINE
[ACCOUNT # 9360199026]

October 29, 1998

Joseph E. Gray, Jr.
Director of Planning and Urban Development
City of Portland
389 Congress St.
Portland, ME 04101

RE: Application of J-Properties Site Improvements at 160 Riverside St., Portland, Me.

Dear Mr. Gray:

This will certify to you that Fleet Bank of Maine will hold the sum of TWENTY SIX THOUSAND TWO HUNDRED NINETY DOLLARS (\$26,290.00) in an interest bearing escrow in the name of the City of Portland established with Fleet Bank of Maine. We will hold these funds as escrow agent for the benefit of the City of Portland on the following conditions:

1. These funds represent the estimated cost of installing site improvements as depicted on the site plan and estimated on Attachment 1. (Attachment 1 is the approved estimated form.)
2. The City of Portland may draw against this escrow account by presentation of a draft in the event that J-Properties Partnership fails to complete by November 1, 2000 the work as stipulated in Paragraph 1. Said draft shall be accompanied by a written statement from the Director of Parks and Public Works or the Director of Planning and Urban Development that J-Properties Partnership has failed to complete such work, with a listing of improvements still to be completed, and the estimated cost of completing said improvements still to be completed as determined by the Department of Public Works.
3. The City of Portland may draw against this escrow for a period not to exceed 90 days after the expiration of this commitment; provided that J-Properties Partnership will give the City written notice of the deadline of this escrow at least 90 days prior thereto; otherwise drafts must be submitted no later than 90 days following written notice whenever given thereafter.
4. After all work in the public right of way has been completed and inspected to the satisfaction of the Department of Public Works, including but not limited to the installation of granite curbing, sidewalk, curb cut and street trees, Fleet Bank of Maine shall be eligible to receive a reduction in its obligations hereunder equal to the estimated cost of improvements. In no case, however, shall the obligations of Fleet Bank of Maine hereunder be reduced to an amount which is less than the



estimated cost of completing all prescribed improvements as determined by the Department of Public Works, as described above.

- 5. J-Properties Partnership will notify the City of Portland for inspections.
- 6. All costs associated with establishing, maintaining and disbursing funds from the escrow account shall be borne by J-Properties Partnership.
- 7. This escrow account expires on April 16, 2001 but may expire prior to the date when the City of Portland acknowledges in writing to Fleet Bank of Maine and J-Properties Partnership that said work as outlined has been completed in accordance with the City of Portland specifications.

Dated at Portland, Maine this 28 October 1998

Very truly yours,

Fleet Bank of Maine

By: [Signature]
Deborah J. Rubino
Business Specialist Fleet Bank of Maine

Date: 10/28/98

Seen and Agreed to:

By: [Signature]
John Locke, J-Properties Partnership

Date: 10/28/98

Approved pursuant to 14-501(a) of the Portland City Code:

By: _____
Director of Planning and Urban
Development

Date _____

By: _____
Corporation Counsel

Date _____

By: _____
Finance Director

Date _____

Department of Planning and Urban Development
SUBDIVISION/SITE DEVELOPMENT

COST ESTIMATE OF IMPROVEMENTS TO BE COVERED BY PERFORMANCE GUARANTEE

Date 9.10.98

Name of Project 160 RIVERSIDE STREET

Address/Location 160 RIVERSIDE ST. / PORTLAND, ME

Developer J. PROPERTIES

Form of Performance Guarantee LETTER OF CREDIT

Type of Development: _____ Subdivision Site Plan (Major/Minor)

TO BE FILLED OUT BY APPLICANT:

Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
1. STREET SIDEWALK						
Road	<u>2450 SF</u>	<u>\$2/FT</u>	<u>\$4,900</u>	<u>33400 SF</u>	<u>\$2/FT</u>	<u>\$66,800</u>
Granite Curbing	<u>168 LF</u>	<u>\$25/FT</u>	<u>\$4,200</u>	<u>90</u>	<u>\$25</u>	<u>\$2,250</u>
Sidewalks	<u>95 LF</u>		<u>192.50</u>	<u>200 LF</u>	<u>\$1.50/</u>	<u>\$300</u>
Esplanades						
Monuments						
Street Lighting						
Other						
2. SANITARY SEWER						
Manholes				<u>1</u>	<u>\$1200</u>	<u>\$1500</u>
Piping	<u>15</u>	<u>\$22/FT</u>	<u>\$330</u>	<u>426 LF</u>	<u>\$22/FT</u>	<u>\$9372</u>
Connections	<u>1</u>	<u>\$500</u>	<u>\$500</u>			
Other						
3. STORM DRAINAGE						
Manholes				<u>1</u>	<u>\$1200</u>	<u>\$1500</u>
Catchbasins				<u>5</u>	<u>\$1200</u>	<u>\$7500</u>
Piping				<u>500 LF</u>	<u>\$22</u>	<u>\$11,000</u>
Detention Basin						
Other (CONTROL STR.)				<u>1</u>	<u>\$3,000</u>	<u>3,000</u>
4. SITE LIGHTING				<u>9</u>	<u>\$1500</u>	<u>13500</u>
5. EROSION CONTROL				<u>1</u>	<u>L.S.</u>	<u>\$800</u>
6. RECREATION AND OPEN SPACE AMENITIES						
SUBTOTAL			<u>\$9930</u>			<u>\$110772</u>
			<u>10,072.50</u>			

Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
7 LANDSCAPING (Attach breakdown of plant materials, quantities, and unit costs)				1	12,885 see attached	\$ 12,885
8 MISCELLANEOUS						
TOTAL:			\$ 9,930 10,072.50			\$ 12,365
GRAND TOTAL:			\$ 133,507 public			\$ 133,587 public & private

133,729.50 - new total

INSPECTION FEE (to be filled out by City)

	PUBLIC	PRIVATE	TOTAL
A 1.7% of totals:	171.23	2,102.17	2,102.17
or			
B: Alternative Assessment:	\$280.00		280.00
Assessed by:	TL		2382.17
	(name)	(name)	

PUBLIC WORKS ENGINEERING
MEMORANDUM

To: Kandi Talbot, Planner

From: Anthony Lombardo, P.E., Project Engineer

Date: August 7, 1998

Subject: 160 Presumpscot Street.....Proposed Building

The following comments were generated during Public Works Engineering review of the revised plans and letter submitted on July 29, 1998, by Sebago Technics:

- *The applicant has made all of the requested changes, indicated in my memo dated July 10, 1998, with the exception of providing a "pipe trench detail" drawn to City of Portland Technical Standards. Item No. 3, in STI's response letter, indicates that no proposed pipe is specified within the Riverside Street right of way. Grading and Utility Plan, sheet S2, however specifies a proposed six (6) inch sanitary service connecting into the sanitary main in the Riverside Street right of way. Public Works, therefore, again requests that a "pipe trench detail", specific to installations within the public right of way, be provided on the plans.*

6/18/98

ALEX.

Here is a preliminary Site plan & Grading Plan
For J. Properties land on Riverside St.

I look forward to your comments on this
plan. We will be formally submitting this
plan next week. - plan is for a less than

10,000 s.f. BLDG ~~with~~ with a use that is
of low impact to the Riverside St. Area.

Such as Trade Services. - Business w/ warehousing.

NOT Retail. (our client refers to it as flex space)

No tenants as of yet. The large quantity

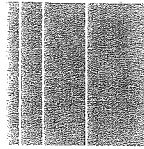
of parking is NOT an anticipation of the use.

our client prefers ample area on site for

trucks to be out of the way if the docks are

occupied. (He is a ~~re~~ distributor and has concerns
and experience of not having areas on site to pull trucks
out of the way.) I think this parking may be an
issue with the city. Yes?

— Any way call me with
comments or if you would like to sit down & discuss
thanks — Steve Doe



SebagoTechnics
Engineering & Planning for the Future

Show turning
template analysis
how trucks
(NB50) would
ingress + egress site
Need 50' Radius
@ Riverside St driveway

July 8, 1998
88017

Ms. Kandy Talbot
Planning & Urban Development
City of Portland
389 Congress Street
Portland, Maine 04101

Minor Site Plan Application
160 Riverside Street, J. Properties

Dear Kandy:

On behalf of J. Properties, I am submitting seven (7) copies of the attached site plan and supportive documents for a proposed 9,900 s.f. warehouse/office building on their property at 160 Riverside Street. The property is shown on Tax Map 26, Block A as Lot 4. The project consist of construction of a 100' x 99' one story high bay building designed for two tenants. Each unit will have one loading dock and one drive-in bay at the rear of the building. The building entrance will be facing Riverside Street. We propose 25 parking spaces as well as a large paved area in the front of the building to serve as a staging area for trucks waiting to utilize the loading docks. Currently there are not tenants leases secured, however, the owner anticipates construction to begin summer 1998.

The building will be served by public sewer from Riverside Street. Public water will be accessed from the abutting property to the north. A verbal agreement has been obtained from the Boulous Company and Mahoney and we are waiting on the formal easement documents from the attorneys. Power will be underground from a pole on Riverside Street. Stormwater detention will occur in the rear of the property where we will share the existing basin constructed for the development to the north. Our development will require a new outlet control structure be installed in the southeast corner of the abutting land. Our attached stormwater management report and site plan set outlines the specifics of this system.

Fore hydrant location
might need another one,

Specific items submitted consist of the following documents.

A. Plans

1. Site Plan
2. Grading, Drainage & Utility Plan
3. Landscape Plan
4. Details

B. Supportive Documents

1. Deed
2. Stormwater Management Report
3. Erosion & Sedimentation Control Plan
4. Building Elevations

I hope this package is complete enough for your review. If you require additional information or have questions, please call me.

Sincerely,

SEBAGO TECHNICS, INC.



Stephen G. Doe, R.L.A.
Project Manager

SGD:dlf

Enclosure

8a

STATUTORY SHORT FORM
OF
QUITCLAIM DEED WITH COVENANT

Bounty Realty, a Maine general partnership
of Portland, Cumberland County, Maine
for consideration paid, release to J Properties, a Maine
limited partnership
of Portland, Cumberland County, Maine
whose mailing address is C/O Locke Office Products, Inc.
150 Riverside Street
Portland, Maine 04102
with QUITCLAIM COVENANT the land in Portland, Cumberland
County, Maine

See Schedule A attached hereto and made a part hereof.

Also hereby conveying all rights, easements, privileges, and
appurtenances belonging to the premises hereinabove described.

IN WITNESS WHEREOF, Bounty Realty, a Maine general partnership
has caused this instrument to be signed in its partnership name
by Joseph F. Boulos, one of its General
Partners, thereunto duly authorized this 11th day
of December, 1986.

WITNESS his hand and seal this 11th day
of December, 1986

BOUNTY REALTY

Wendy D. McLaughlin
Witness

By: Joseph F. Boulos
Joseph F. Boulos, General Partner

8b

STATE OF MAINE
County of Cumberland ss. December 11, 1986.

Then personally appeared the above
named Joseph F. Boulos and acknowledged the
foregoing instrument to be his free act and deed in his
said capacity and the free act and deed of Bounty Realty

Before me,

Mary Lea Clifford
Notary Public -XXXXXXXXXXXX
XXXXXXXXXXXX
Mary Lea Clifford

MY COMMISSION EXPIRES FEBRUARY 5, 1988

Schedule A

8C

The real estate situated in the City of Portland, County of Cumberland, and State of Maine and more specifically described as follows:

Beginning at an iron pin marking the northerly most point of a parcel of land now or formerly of J Properties which parcel was formerly of Ameri-Cana Transport, Inc. (described in Book 3175 at Page 471 in the Cumberland County Registry of Deeds);

Thence: proceeding on a heading of S 27° 58.5' E a distance of approximately 192.9 feet to a point (hereinafter called the "Point of Beginning");

Thence: proceeding on a heading of S 62° 1.5' W a distance of approximately 25 feet to a point on the sideline of the land now or formerly of Central Maine Power Co. of Augusta;

Thence: proceeding on a heading of S 27° 58.5' E a distance of approximately 364 feet to a point at the intersection of the lands now or formerly of Bounty Realty, William A. Berger and Central Maine Power Co. of Augusta;

Thence: proceeding on a heading of S 63° 41.5' E a distance of approximately 42.82 feet along the land now or formerly of William A. Berger;

Thence: proceeding on a heading of N 27° 58.5' W a distance of approximately 327.3 feet to the Point of Beginning.

Said parcel being comprised of approximately 9,523 square feet and being a portion of the parcel previously conveyed to Bounty Realty by warranty deed from William W. Sexton and Hans F. Paul and dated January 30, 1986 and recorded in the Cumberland County Registry of Deeds in Book 7056 at Page 115;

Also hereby conveying the right in common with the Grantor herein and its successors and assigns, to drain and impound water from land of the Grantee herein and from land adjacent thereto of Locke Properties and from the premises hereinabove conveyed (Collectively called Locke Land) in a detention basin on land of the Grantor herein (The Detention Basin), being located partially on the Northwesterly portion of property conveyed to the Grantor herein by William Sexton et al by deed dated January 30, 1986 and recorded in the Cumberland County Registry of Deeds in Book 7056, Page 115, and partially

88

on the easement and use of the triangular piece of land conveyed by Grantee to Grantor, (provided that the additional surface water drained into the Detention Basin from the Locke Land shall not, after satisfying the Grantor's drainage needs, exceed the capacity of The Detention Basin) and further conveying to the Grantee and Locke Properties, their respective successors and assigns, the right to expand the Detention Basin onto the remaining land of the Grantee herein and use in connection therewith the Grantor's piping and underground drainage system to Riverside Street. The Grantee agrees that any work to be done to increase the capacity of the Detention Basin shall first be approved as to reasonable engineering matters by Sebago Technics, Inc. or such successor engineering firm as the Grantee shall previously in writing have designated to the Grantor, with all costs for such expansion being borne by Grantee.

AH.9

From: Larry Ash
To: Kandi Talbot
Date: Fri, Jun 23, 2000 2:14 PM
Subject: Exit 8 Traffic Signal Improvements/Sleep Inn

Kandi: The moving of the traffic signal pole on the exit 8 side of the intersection of Riverside St/exit 8 will in effect result in 3 travel lanes on that side of the intersection. This will encourage 2 thru lanes of vehicular traffic from the other side of the intersection and will therefore improve the Level of Service operation of the intersection. This lane improvement has been depicted in a drawing that has no identification but is for the Sleep Inn project.

If you have any questions please call.

CC: Wjb

Att. 10

From: "949 HowardJohnson" <949@hotel.cendant.com>
To: Portland.CityHall(KCOTE)
Date: Thu, May 4, 2000 5:42 PM
Subject: 160 Riverside Street

I am writing concerning the proposed construction of the 3 story, 74 room Sleep Inn at 160 Riverside Street. Notwithstanding our interest in not having a new competitor across the street from us, we have serious concerns about the traffic problems such a development may cause. As you are no doubt aware, Riverside Street currently experiences traffic delays in the vicinity of Exit 8. Our concern is that the traffic waiting on Riverside Street to get on the Turnpike will prevent vehicles from turning left from Riverside Street into the 160 Riverside Street development. At many times during the day taking such a left turn would be virtually impossible. When this occurs, vehicles could back up as far as the Exit 8 ramp (which is not very far). This would also complicate the process of getting out of our parking lot.

I hope that any approval of the 160 Riverside Street development will correct the traffic flow problems that such a development will exacerbate, if that is possible.

Respectfully Submitted, Mark Heisler
General Manager
Howard Johnson Hotel
155 Riverside Street
Portland, ME 04103
774-5861

AH.8

**BOULOS
PROPERTY
MANAGEMENT**

One Canal Plaza
Portland, ME 04101
(207) 871-1290
Fax: (207) 772-2647
www.boulos.com

June 6, 2000

Steve Doe
Sebago Technics
PO Box 1339
Westbrook, Maine 04098

RE: Locke, 150 Riverside St Portland, Me

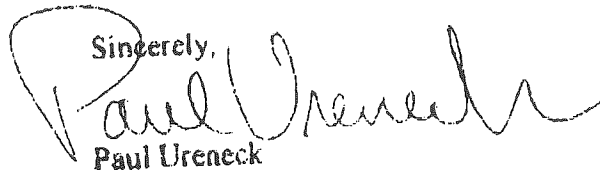
Dear Steve,

It is my understanding that the requirements of the proposed motel development, as they relate to Bounty Realty, have changed. Specifically, the water main connection will not be necessary due to Portland Water District regulations. Storm water flow across our property and into the detention pond remains. Bounty Realty has no objections to the proposed storm water design contingent upon the requirements of the Deed, recorded in the Cumberland County Registry of Deeds, Book 7619, and Pages 0120-0123, being met.

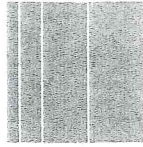
A letter, from Sebago Technics, verifying that the capacity of the existing detention pond is sufficient for both developments and that, if necessary, any improvements to the detention pond will be the full and sole responsibility of the Locke development, is requested.

Thank you.

Sincerely,



Paul Ureneck
Vice President
Project Management



Sebago Technics

Engineering & Planning for the Future

AH. 2

March 28, 2000
88017

Kandice Talbot, Planner
City of Portland
389 Congress Street
Portland, ME 04101

Major Site Plan Application – Sleep Inn, 160 Riverside Street

Dear Kandi:

On behalf of J. Properties, I am pleased to submit an application fee of \$500.00 and seven (7) copies of plans and supporting documents for a proposed three-story, 74-room Sleep Inn on their property at 160 Riverside Street. The parcel is 2.23 acres in size and is shown on Tax Map 267, Block A as Lot 4 and is zoned B-4. This site was previously approved for a 9,900 square foot, two tenant flex space project in 1998. Some site work (including sewer and storm lines installation, topsoil stripping, and fill activities) was started as part of this project, but was stopped when no tenant was secured. This new project proposes the same curb cut configuration and utility connections.

For your review and consideration for the Planning Board workshop, I offer the following plans and supporting information:

1. Site Plan showing building and site layout, and identification of site materials. Also listed are easements on the property.
2. Grading Drainage & Utility Plan. The drainage patterns on this plan closely follow the previously approved project; however, actual storm line locations will change. Stormwater is directed through a series of catch basins to an existing detention basin at the rear of the parcel. This basin is shared with the abutting Boulos property and Mark's Showplace. We may be modifying the existing outlet control structure for the system to accommodate our project. While we have not finalized our stormwater management plan, we do not anticipate any major changes from the previously approved plan. As required, an oil and grit separator will be installed. In addition, a fire hydrant has been added near the building as requested by the Fire Department.
3. A Landscape Plan is attached which indicates proposed plantings and plant sizes.

4. Traffic Improvements. A traffic study was requested by Larry Ash in our pre-application meeting with the City to determine project impacts. You also indicated a traffic improvement assessment will be required of J. Properties to assist in the proposed improvements to the Riverside Street area. As you know, an extensive traffic study was performed recently for the Portland Commons development which proposed major improvements to the Riverside Street/Exit 8 area. This study and improvement plan more than accommodate any minor impacts this project may have. In lieu of preparing a traffic study which will provide minimal new information than the previous study, we respectfully request that the cost of the study be spent on funding some of the needed improvements identified in the Portland Commons study. We understand assessments on other developers in the area range from \$6,000.00 to \$10,000.00. We have contracted with John L. Murphy to work with Mr. Ash in developing a plan for necessary improvements near the Exit 8/Riverside Street intersection. Once a plan and cost estimate are completed, we will present them to the City.
5. Site Lighting. Our plan does indicate proposed light fixture locations and wattage. We are currently having a point-by-point photometric plan prepared which will conform to the City's technical and design standards for site lighting.
6. Copy of deeds for the property indicating title of J. Properties to the property.
7. Construction Schedule. Construction is anticipated to commence upon receipt of final plan approval. The Sleep Inn franchise has been obtained and J. Properties is working with the Sheridan Corp. in developing a construction plan.
8. There are no unusual natural areas, wildlife or fisheries habitats, or archaeological sites located on or near the site.
9. Sample architectural plans of a Sleep Inn. We will be providing more specific plans prior to the workshop.

I hope this package provides you with enough information to schedule us for a workshop with the Planning Board on April 18th. We will continue to develop our final plan documents and will forward them to you as we complete them. In the interim, if you have questions or need additional information, please call.

Sincerely,

SEBAGO TECHNICS, INC.


Stephen G. Doe, R.L.A.
Landscape Architect

SGD:jc
Enc.

cc: John Locke, J. Properties
Robert Shackelton, Group One
Jim Hoy, Sheridan Corp.

CITY OF PORTLAND, MAINE
SITE PLAN CHECKLIST

SLEEP INN 160 RIVERSIDE ST.
Project Name, Address of Project

I.d. Number

Submitted () & Date	Item	Required Information	Section 14-525 (b,c)
3/23/00	(1)	Standard boundary survey (stamped by a registered surveyor, at a scale of not less than 1 inch to 100 feet and including:	1
3/23/00	(2)	Name and address of applicant and name of proposed development	a
3/23/00	(3)	Scale and north points	b
3/23/00	(4)	Boundaries of the site	c
3/23/00	(5)	Total land area of site	d
3/23/00	(6)	Topography - existing and proposed (2 feet intervals or less)	e
3/23/00	(7)	Plans based on the boundary survey including:	2
	(8)	Existing soil conditions	a
3/23/00	(9)	Location of water courses, marshes, rock outcroppings and wooded areas	b
3/23/00 *PARTIAL	(10)	Location, ground floor area and grade elevations of building and other structures existing and proposed, <u>elevation drawings of exterior facades, and materials to be used.</u> FORTHCOMING	c
3/23/00	(11)	Approximate location of buildings or other structures on parcels abutting the site	d
3/23/00	(12)	Location of on-site waste receptacles	e
3/23/00	(13)	Public utilities	e
3/23/00	(14)	Water and sewer mains	e
3/23/00	(15)	Culverts, drains, existing and proposed, showing size and directions of flows	e
3/23/00	(16)	Location and dimensions, and ownership of easements, public or private rights-of-way, both existing and proposed	f
3/23/00	(17)	Location and dimensions of on-site pedestrian and vehicular accessways	g
3/23/00	(18)	Parking areas	g
N/A	(19)	Loading facilities	g
3/23/00	(20)	Design of ingress and egress of vehicles to and from the site onto public streets	g
3/23/00	(21)	Curb and sidewalks	g
3/23/00	(22)	Landscape plan showing:	h
	(23)	Location of existing proposed vegetation	h
	(24)	Type of vegetation	h
	(25)	Quantity of plantings	h
	(26)	Size of proposed landscaping	h
	(27)	Existing areas to be preserved	h
	(28)	Preservation measures to be employed	h
	(29)	Details of planting and preservation specifications	h
3/23/00	(30)	Location and dimensions of all fencing and screening	i
	(31)	Location and intensity of outdoor lighting system	j
3/23/00	(32)	Location of fire hydrants, existing and proposed	k
3/23/00	(33)	Written statement	c
3/23/00	(34)	Description of proposed uses to be located on site	l
N/A	(35)	Quantity and type of residential, if any	l
3/23/00	(36)	Total land area of the site	b2
3/23/00	(37)	Total floor area and ground coverage of each proposed building and structure	b2
3/23/00	(38)	General summery of existing and proposed easements or other burdens	c3
	(39)	Method of handling solid waste disposal	4

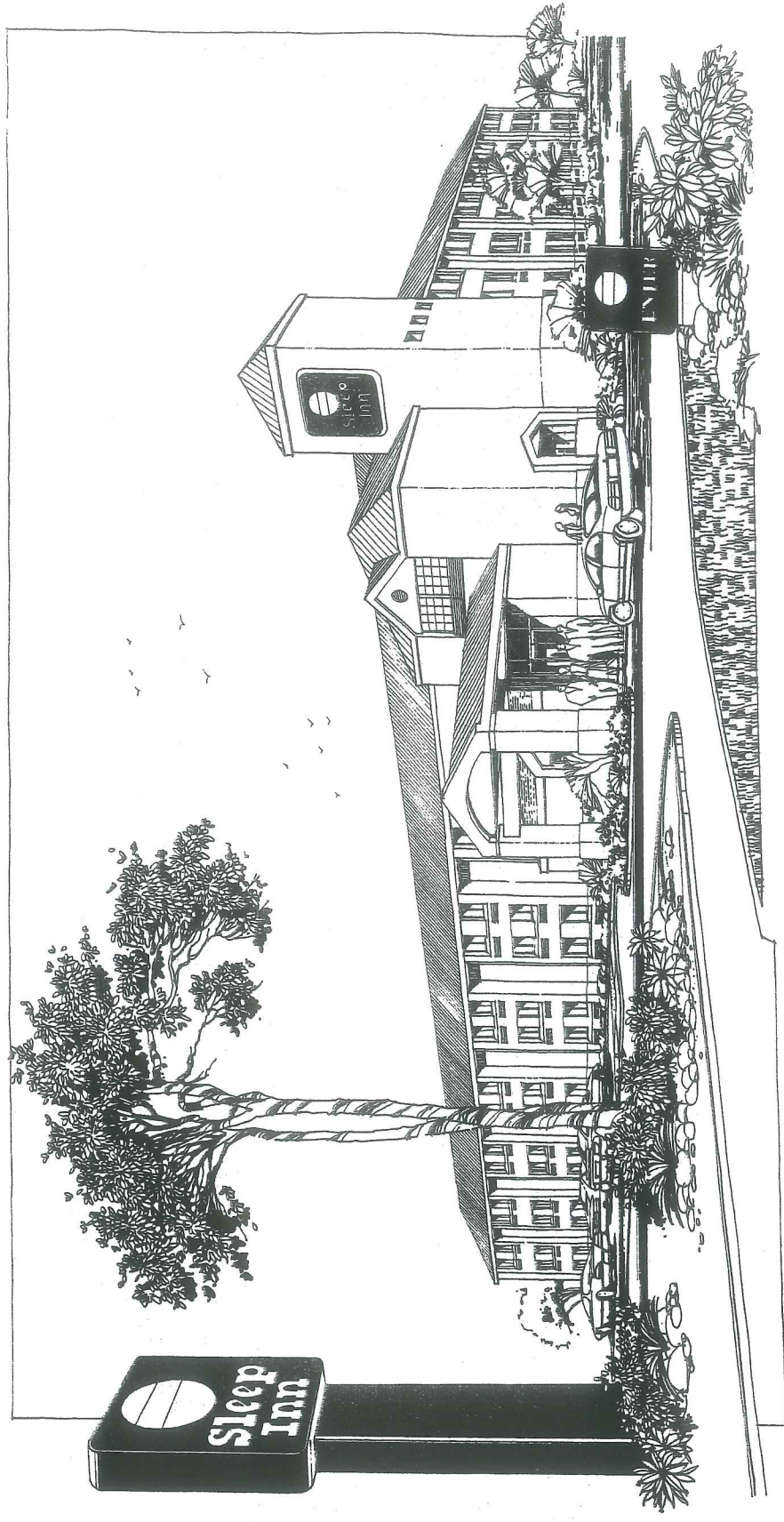
2c

<u>3/28/00</u>	(40)	Applicant's evaluation of availability of off-site public facilities, including sewer, water and streets	5
<u> </u>	(41)	Description of any problems of drainage or topography, or a representation that there are none	6
<u>3/28/00</u>	(42)	An estimate of the time period required for completion of the development	7
<u> </u>	(43)	A list of all state and federal regulatory approvals to which the development may be subject	8
<u>N/A</u>	(44)	The status of any pending applications	8
<u> </u>	(45)	Anticipated timeframe for obtaining such permits	h8
<u>N/A</u>	(46)	A letter of non jurisdiction	h8
<u> </u>	(47)	Evidence of financial and technical capability to undertake and complete the development including a letter from a responsible financial institution stating that is has reviewed the planned development and would seriously consider financing it when approved.	

Note: Depending on the size and scope of the proposed development, the Planning Board or Planning Authority may request additional information, including (but not limited to):

- drainage patterns and facilities;
- erosion and sedimentation controls to be used during construction;
- a parking and/or traffic study;
- a noise study;
- an environmental impact study;
- a sun shadow study;
- a study of particulates and any other noxious emissions; and
- a wind impact analysis.

Other comments:



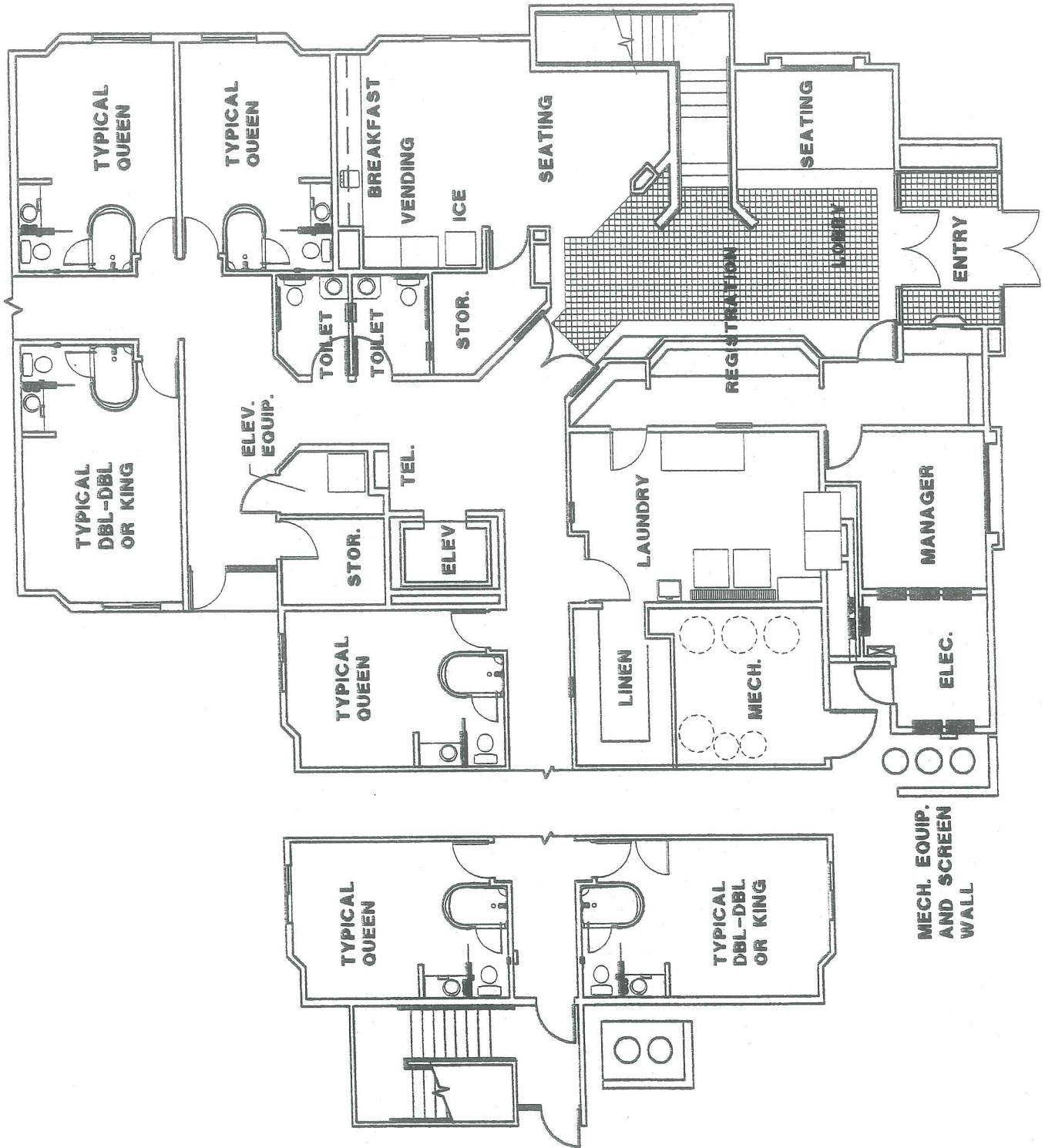
2d

**SLEEP INN
THREE STORY**

CHOICE HOTELS INTERNATIONAL, INC.



2e



* NOTE: ELEVATOR REQUIREMENTS WILL BE ADJUSTED FOR LOCAL CODES AND BUILDING HEIGHT - ELEVATOR SHAFT AND EQUIPMENT ROOM WILL BE STORAGE IF ELEVATOR IS NOT REQUIRED.



2f



**SLEEP INN
SECOND FLOOR LOBBY**

CHOICE HOTELS INTERNATIONAL, INC.



EXHIBIT A

*Locke
Office
sit.*

A certain lot or parcel of land together with the buildings thereon, situated on the westerly side of relocated Riverside Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at a stake on the westerly sideline of relocated Riverside Street marking the point of tangency of a curve whose radius is seven hundred seventy-six and twenty hundredths feet (776.20') at station 43+85.28 as shown on plan of Brighton Avenue Interchange made by the Maine Turnpike Authority and recorded in the Cumberland County Registry of Deeds in Plan Book 41, Page 70, which stake is located forty and four hundredths feet (40.04') southeasterly of the southeasterly corner of the General Motors Building so-called; thence running southerly by said relocated Riverside Street as shown on said plan on a curve to the left whose radius is seven hundred seventy-six and twenty hundredths feet (776.20'), a distance of ninety-nine and fifty-five hundredths feet (99.55') when measured on the arc thereof to a point which is the northeasterly corner of Parcel 2 hereof; thence running along the northerly sideline of said Parcel 2 North sixty-three degrees forty-one and one-half minutes West (N 63° 41-1/2' W) three hundred fifty-six and ninety-seven hundredths feet (356.97') to a point, which is situated fifty feet (50') easterly when measured at right angles from the center line of a Central Maine Power Company pole line and which point is the northwest corner of said Parcel 2; thence running by land now or formerly of Guy E. Knowles parallel with and fifty feet (50') distant from said pole line, North twenty-seven degrees fifty-eight and one-half minutes West (N 27° 58-1/2' W) thirty-six and seventy-four hundredths feet (36.74') to a point; thence running by land now or formerly of Whitney Deal Estate, Inc. on the following described courses; North thirty-five degrees eight minutes East (N 35° 08' E) one hundred forty-seven and eighty-eight hundredths feet (147.88') to a point; thence South fifty-four degrees fifty-two minutes East (S 54° 52' E) three hundred seventy-nine and thirteen hundredths feet (379.13') to the westerly sideline of said relocated Riverside Street; thence by said relocated Riverside Street South thirty-five degrees eight minutes West (S 35° 08' W) ten and forty-five hundredths feet (10.45') to the point of beginning. Said above described parcel contains fifty-three thousand (53,000) square feet.

Also a certain lot or parcel of land situated on the westerly side of relocated Riverside Street in the City of Portland, County of Cumberland and State of Maine, and bounded and described as follows:

Beginning at a point on the westerly sideline of relocated Riverside Street which point is ninety-nine and fifty-five hundredths feet (99.55') southwesterly on a curve to the left from a stake on said westerly sideline which marks the point of tangency of a curve whose radius is seven hundred seventy-six and twenty hundredths feet (776.20') at station 43+85.28 as shown on plan of Brighton Avenue Interchange made by the Maine Turnpike Authority and recorded in the Cumberland County Registry of Deeds in Plan Book 41, Page 70 and which point marks the southeasterly corner of Parcel 1 hereof; thence running along said westerly sideline of Riverside Street twenty feet (20') to a point; thence running by land now or formerly of Guy E. Knowles North sixty-three degrees forty-one and one-half minutes West (N 63° 41-1/2' W) three

*Building
Locke property*

*Chasle Locke
to
Locke property*

2h

hundred twenty-eight and eighty-nine hundredths foot (128.89') to a point which is situated fifty foot (50') easterly when measured at right angles from the center line of a Central Maine Power Company pole line; thence running by other land of said Power Company parallel with and fifty foot (50') distant from said pole line, North twenty-seven degrees fifty-eight and one-half minutes West (N 27° 58-1/2' W) thirty-four and twenty, six hundredths foot (34.26') to a point; thence running by the southerly sideline of Parcel 1 hereof South sixty-three degrees forty-one and one-half minutes East (S 63° 41-1/2' E) three hundred fifty-six and ninety-seven hundredths (356.97') feet to the point of beginning; said above described parcel contains six thousand eight hundred and fifty-eight (6,858) square feet.

The above described courses are true North and the above described parcels or lots of land are subject to the following encumbrances:

1. Pipeline easement from Dennis D. Decormier to Portland Pipe Line Company dated October 10, 1941, recorded in said Registry of Deeds in Book 1646, Page 289.

2. Easement from Whitney Peal Estate, Inc. to Central Maine Telephone Company and New England Telephone and Telegraph Company dated September 22, 1964, recorded in said Registry of Deeds in Book 2860, Page 60.

By and Between Fred J. Foley, Jr. and The Sperry and Hutchinson Company dated as of September 1, 1977, as evidenced by Memorandum of Lease, recorded in Cumberland County Registry of Deeds in Book 4082, Page 221.

Being the same premises conveyed by the Grantee herein to the Grantor herein by warranty deed dated and recorded herewith.

ATTN:
WALT

Att. 3

From: Larry Ash
To: Kandi Talbot
Date: Mon, May 1, 2000 12:46 PM
Subject: Sleep Inn, 160 Riverside Street

Kandi: To follow up on our conversation this morning, Sleep Inn will be required to resurface the Exit 8 Intersection as part of their site development. I would expect this to cost in the neighborhood of \$5,000 to \$6,000. The exact resurfacing area will have to be more precisely determined after I talk with Tony Lombardo.

They cannot do what has already been planned as part of the Kennan Auction Site Development.

If you have any questions please call me at 8894.

CC: Anthony Lombardo, William Bray

Att. 4

From: Anthony Lombardo
To: Kandi Talbot
Date: Wed, Apr 12, 2000 9:43 AM
Subject: Sleep Inn.....160 Riverside St. 4/12/00

I have reviewed the plans and have no Public Works issues.

AH-5

From: "Steve Bushey" <srbushey@maine.rr.com>
To: "Kandi Talbot" <KCOTE@ci.portland.me.us>
Date: Wed, May 3, 2000 3:28 PM
Subject: Sleep Inn

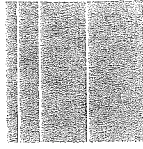
Kandi,

I have reviewed the submission materials by sebage Technics for the proposed sleep Inn. I offer the following comments:

1. the site plan should have a baseline or other means of geometric layout for the paved surfaces and building.
2. A snow storage area should be identified on the plan.
3. The limits of works, particularly in the vicinity of Riverside street should be clearly identified and the work clearly spelled out as to repaving, trench repair etc.
4. the applicants engineer should review the storm drain structures for adequate clearances for pipe installation. Several are very shallow and may require special structures to allow the pipe, cover and casting to be installed.
5. Additionall data should be provided on the flow splitter.
6. I'm uncertain about the outlet control structure inverts, since they appear lower than the Detention basin.
7. The Planning Board/Staff may want to consider requiring granite curb instead of Bit. curb since this will be a very tight parking lot whith high traffic use.

If you have any questions regarding these comments please call.

Steve Bushey Acting Development review Coordinator



Sebago Technics

Engineering & Planning for the Future

Att. 6

May 30, 2000
88017

Kandice Talbot, Planner
City of Portland
389 Congress Street
Portland, ME 04101

Sleep Inn, 160 Riverside Street

Dear Kandi:

On behalf of J. Properties, I am submitting the following plans and supportive documents in response to comments made by staff and the Planning Board. The applicant has also initiated a change in the building to better serve their customers. This is discussed under Item 1.

1. Revised site plan at a scale of 1"=30' showing a revised building footprint which represents a 3,788 square foot increase in building size. The new footprint allows for an upscale of 16 rooms to suites. There is no increase in room totals from the previous building. The new building expansion occurs in the front of the building near the handicapped parking spaces.

Other changes consist of narrowing the width of the travel lane in the rear of the building from 24' to 20'. We also continued the sidewalk along Riverside Street to the site. Snow storage areas are also indicated on the plan.

2. Grading plan showing proposed stormwater systems and Vortech unit.
3. Landscape plan showing proposed plantings.
4. Construction details of improvements.
5. Stormwater Management. We have sized the Vortech unit and designed the flow splitter for the project. Details are included in the plan set. Our discussions with Steve Bushy indicated that the Stormwater Management Report prepared for the previous development did not need to be updated since this project's impervious surface and stormwater control measures are nearly identical with the previous proposal. The Stormwater Management Report indicates that the shared detention basin has the capacity to handle the 2, 10 and 25-year storm events from this development. The new outlet control structure, installed in 1998, modified the existing system to handle this project. We are currently working with The Boulos Company to obtain the necessary agreement to utilize the common detention basin. A copy of this correspondence is attached. The final document will be submitted upon receipt.

69

6. Traffic analysis prepared by John L. Murphy. The report concludes that the current Exit 8/Riverside Street intersection operates at a Level F. The report further notes that the hotel will not have a measurable impact on the Exit 8 intersection. In Mr. Murphy's report, he proposes a geometric change to the Larrabee Road approach. This improvement upgrades the intersection to a Level E. We understand Mr. Ash has discussed this report with Mr. Murphy and is in agreement with the study and the proposed upgrades. This improvement is estimated to cost approximately \$13,000.00. A copy of the proposed improvement is attached for review.
7. Attached are the photometric plan and light fixture cut sheets for review and comments.
8. Capacity of existing utilities. We have requested capacity letters from the Sewer District and Portland Water District and requested their responses be directed to your attention. We do not anticipate any utility capacity problems in the area.
9. Portland Pipeline agreement. Attached is correspondence from Ralph Wink of the Portland Pipeline which discusses their position on improvements within their easement. Our plans are responsive to this agreement.

I trust this information addresses both staff and Planning Board comments. If you have any questions on this material or need additional information, please call.

Sincerely,

SEBAGO TECHNICS, INC.



Stephen G. Doe, RLA
Project Manager

SGD:ama/jc

Enc.

cc: John Locke
Bob Shackelton, Group One Inc.
Jim Hoy, The Sheridan Corp.
Christine S. Thomas, CHA Choice Hotels

6b

May 23, 2000

Jeffery Gee
Berkshire Life Insurance Company
C/o
New England Realty Resources
Suite 100
Ten Winthrop Square
Boston, MA 02110 1264

RE: 190 Riverside St. Portland, Me.

Dear Mr. Gee:

Attached please find a proposed site plan, grading & utility plan, and easement description for a planned "Sleep Inn" adjacent to our property. The owner of the abutting parcel is requesting both our and lender permission to connect to our existing 8" water main and to also disperse storm water into the detention pond that is located on both of our lots. We have reviewed this plan and do not find it objectionable. Please review this request with the lender and let me know if it is acceptable. The abutter ultimately needs to submit a "permission" letter to the City of Portland as part of the local approval process.

Thank you.

Sincerely,

Paul Ureneck

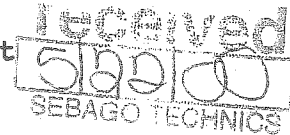
JOHN L. MURPHY, P.E.

60c

Civil Engineer
Traffic Engineer

RR1, BOX 6300
WEST BALDWIN, MAINE 04091-9745
207-625-8222

Traffic Analysis
Sleep Inn, 160 Riverside Street



General

The developer, J. Properties, proposes to build a 74 room motel at 160 Riverside Street. Based upon the Institute of Transportation Engineers' publication Trip Generation, the project can be expected to generate 35 PM peak hour trips, 19 entering and 16 exiting the site. The City of Portland has requested additional traffic analysis of the Exit 8/Riverside Street intersection.

Exit 8/Riverside Street/Larrabee Road

I have attached a copy of a May 12, 2000 4 PM to 6 PM peak period turning movement count of the intersection. I have also attached two highway capacity analyses of the 4:30 to 5:30 PM peak hour. These analyses used the latest possible highway capacity methodology specified in the December 1997 Highway Capacity Manual. One analysis assumed existing conditions and shows that the intersection operates at level of service F, which is the worst level of service defined by the Highway Capacity Manual. The second analysis used the same traffic volumes and green time assignments, but included geometric changes to permit Larrabee Road traffic in the curb lane to proceed either straight or right. This improvement changed the intersection level of service from F, with 91 seconds per vehicle average delay, to E (capacity) with 58.3 seconds/vehicle average delay. The approach delay for Larrabee Road decreased from 210 seconds/vehicle to 65.8 seconds, a 69% improvement for this approach.

Traffic Impact of Motel

The minor number of trips from the proposed project (35/hour) during the heaviest hour of the day will not have any measurable impact on the Exit 8 intersection.

Conclusion

Implementation of the proposed offsite improvement to the Exit 8 intersection will more than compensate for the minor traffic impact of this low volume project.

John L. Murphy
5/19/00

HCS: Signalized Intersections Release 3.1c

6d

Inter: Riverside/Exit8/Larrabee
 Analyst: JL Murphy
 Date: 05/16/2000
 E/W St: Larrabee/Exit8

City/St: Portland, Me.
 Proj #: 0100
 Period: 16:30-17:00 5/12/00
 N/S St: Riverside St.

SIGNALIZED INTERSECTION

SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	<u>T</u>	<u>R</u>	L	T	R	L	T	R	L	T	R
No. Lanes	1	<u>1</u>	<u>1</u>	1	2	0	1	2	0	1	2	0
LGConfig	L	T	R	L	T		L	T		L	TR	
Volume	288	391	101	285	399		35	447		486	522	368
Lane Width	12.0	12.0	12.0	12.0	12.0		12.0	12.0		12.0	12.0	
RTOR Vol			30									45

Duration 1.00 Area Type: All other areas
 Signal

Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left	A				NB Left	A		
Thru		A			Thru		A	
Right		A			Right			
Peds					Peds			
WB Left	A				SB Left	A	A	
Thru		A			Thru		A	A
Right					Right		A	A
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right			
Green	27.0	27.0			5.0	37.0	22.0	
Yellow	3.5	3.5			3.5	3.5	3.5	
All Red	1.5	1.5			1.5	1.5	1.5	
Cycle Length:	143.0		secs					

6e

Intersection Performance

Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
L	334	1736	0.86	0.192	79.5	E		
T	351	1827	1.11	0.192	335.3	F	210.0	F
R	293	1553	0.24	0.189	49.5	D		
Westbound								
L	331	1719	0.86	0.192	78.9	E		
T	661	3438	0.60	0.192	53.9	D	64.3	E
Northbound								
L	62	1770	0.56	0.035	75.3	E		
T	557	3539	0.80	0.157	66.4	E	67.1	E
Southbound								
L	582	1752	0.84	0.332	75.7	E		
TR	1490	3304	0.57	0.451	29.3	C	46.2	D
Intersection Delay = 91.0 (sec/veh)					Intersection LOS = F			

HCS: Signalized Intersections Release 3.1c

John L. Murphy
 John L. Murphy P. E.
 RR1 Box 6300
 West Baldwin , Me. 04091

Phone: 207-625-8222

Fax: 207-625-8222

E-Mail: kjmurphy@cybertours.com

OPERATIONAL

ANALYSIS

Intersection: Riverside/Exit8/Larrabee
 City/State: Portland, Me.
 Analyst: JL Murphy
 Project No: 0100
 Time Period Analyzed: 16:30-17:00 5/12/00
 Date: 05/16/2000
 East/West Street Name: Larrabee/Exit8
 North/South Street Name: Riverside St.

HCS: Signalized Intersections Release 3.1c

6f

Inter: Riverside/Exit8/Larrabee
 Analyst: JL Murphy
 Date: 05/16/2000
 E/W St: Larrabee/Exit8

City/St: Portland, Me.
 Proj #: 0100
 Period: 16:30-17:00 5/12/00
 N/S St: Riverside St.

SIGNALIZED INTERSECTION

SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	0	1	2	0	1	2	0	1	2	0
LGConfig	L	TR		L	T		L	T		L	TR	
Volume	288	391	101	285	399		35	447		486	522	368
Lane Width	12.0	12.0		12.0	12.0		12.0	12.0		12.0	12.0	
RTOR Vol			30									45

Duration 1.00 Area Type: All other areas
 Signal

Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left	A				NB Left	A		
Thru		A			Thru		A	
Right		A			Right			
Peds					Peds			
WB Left	A				SB Left	A	A	
Thru		A			Thru		A	A
Right					Right		A	A
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right			
Green	27.0	27.0			5.0	37.0	22.0	
Yellow	3.5	3.5			3.5	3.5	3.5	
All Red	1.5	1.5			1.5	1.5	1.5	
Cycle Length:	143.0 secs							

Intersection Performance

69

Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
L	334	1736	0.86	0.192	79.5	E		
TR	652	3391	0.71	0.192	57.3	E	65.8	E
Westbound								
L	331	1719	0.86	0.192	78.9	E		
T	661	3438	0.60	0.192	53.9	D	64.3	E
Northbound								
L	62	1770	0.56	0.035	75.3	E		
T	557	3539	0.80	0.157	66.4	E	67.1	E
Southbound								
L	582	1752	0.84	0.332	79.9	E		
TR	1490	3304	0.57	0.451	29.3	C	47.8	D

Intersection Delay = 58.3 (sec/veh) Intersection LOS = E

HCS: Signalized Intersections Release 3.1c

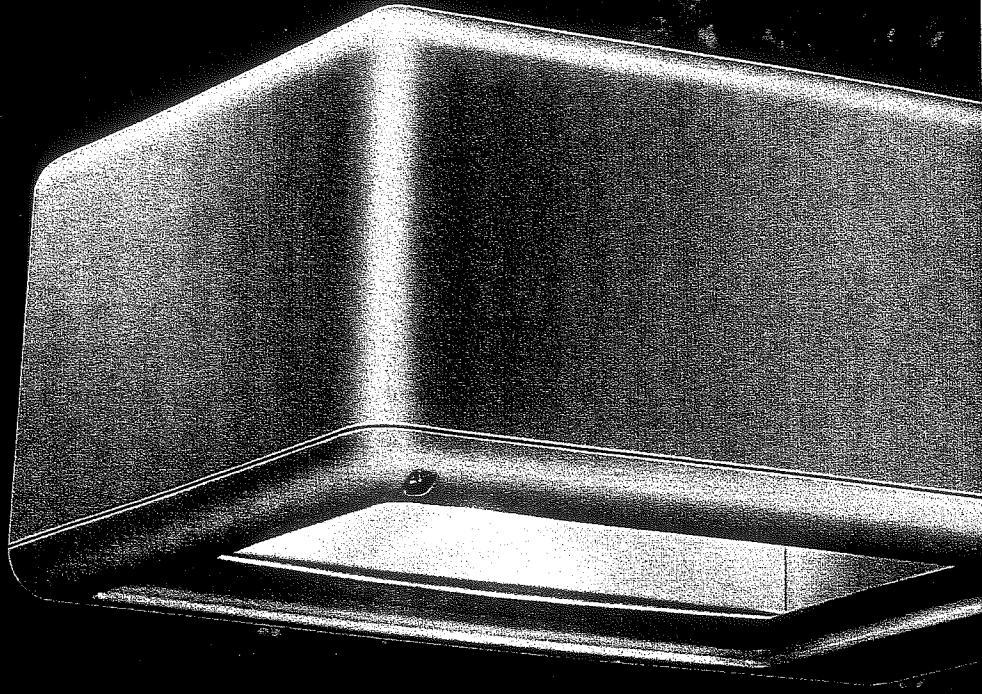
John L. Murphy
 John L. Murphy P. E.
 RR1 Box 6300
 West Baldwin , Me. 04091

Phone: 207-625-8222 Fax: 207-625-8222
 E-Mail: kjmurphy@cybertours.com

OPERATIONAL

ANALYSIS

Intersection: Riverside/Exit8/Larrabee
 City/State: Portland, Me.
 Analyst: JL Murphy
 Project No: 0100
 Time Period Analyzed: 16:30-17:00 5/12/00
 Date: 05/16/2000
 East/West Street Name: Larrabee/Exit8
 North/South Street Name: Riverside St.



CAMBRIDGE

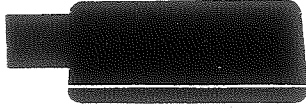
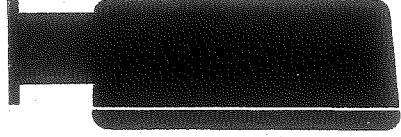
SPALL DING LIGHTING, INC.

601

UL & CSA Listed.


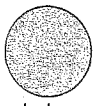

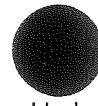
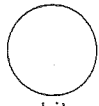




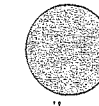
LUMINAIRE ORDERING GUIDE

Model	CEI - small size	CEII - large size
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Mounting Mode	PM	WB
		
	Pole Mount	Wall Bracket

Lamp Type/Watts	small size						large size				
	S100	S150	S250	S400	M175	M250	M400	S400	S1000	M400	M1000
Reflector	I-asymmetric			III-asymmetric			IV-forward throw		VS-symmetric square		
Voltage	120	208	240	277	347	480	MT-multi-tap				

Options
 PC - photoelectric cell 120-277v, up to 400w. SF - single fuse
 PR - photo receptacle (less cell) DF - double fuse
 VG - polycarbonate vandal guard CS - house side cutoff shield

Colors for Luminaire and Pole	DBZ	SSB	RRN	SGB	WHT	FGP	TBP	RBP	CMB	SOS
										
	dark bronze	beige	rocket red	black	white	forest green	teal blue	royal blue	burgundy	silver

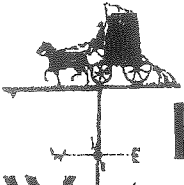
Luminaire Ordering Example:

MODEL	MOUNTING MODE	LAMP TYPE WATTS	REFLECTOR	VOLTAGE	OPTIONS	COLOR
CEII	PM	S400	IV	MT	PC	SOS

CEI CEII	PM: pole mount: std-6" arm for CEI std-10" arm for CEII WB: wall bracket	small S100 S150 S250 S400 M175 M250 M400	large S400 S1000 M400 M1000	I: symmetric III: asymmetric IV: forward throw VS: V-square	120 208 240 277 347 480 MT, multi-tap	PC: photoelectric cell 120-277v, up to 400w PR: photo receptacle (less cell) SF: single fuse DF: double fuse VG: polycarbonate vandal guard CS: house side cutoff shield	DBZ: dark bronze SSB: beige RRN: rocket red SGB: black WHT: white FGP: forest green TBP: teal blue RBP: royal blue CMB: burgundy SOS: silver
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POLE ORDERING

Refer to Poles/Brackets Section for ordering information.



Portland Water District

6j

225 Douglass St. • P.O. Box 3553 • Portland, ME 04104-3553

(207) 774-5961
FAX (207) 761-8307
www.pwd.org

May 30, 2000

Stephen O. Doe, RLA
Sebago Technics
PO Box 1339
Westbrook, Me. 04098

Re: Sleep Inn- 160 Riverside St.

Dear Steve,

This letter is to confirm there should be an adequate supply of clean and healthful water to serve the needs of the proposed sleep Inn at 160 riverside street in Portland. Checking District records, I find there is a 12" water main on the even side of the street.

The current data from the nearest hydrant indicates there should be adequate capacity of water.

Hydrant Location: Bounty Rd. @Riverside St
Hydrant # 1754
Static pressure = 83 PSI
Flow = 1264GPM
Last Tested = 6/25/96

If the district can be of further assistance in this matter, please let us know.

Sincerely,
Portland Water District

Jim Pandiscio
Jim Pandiscio
Means Coordinator

To	Steve Doe	Date	5/30/00
Co.	Sebago Technics	# of Pgs.	1
Dept		From	Jim Pandiscio
Fax No	856-2206	Phone #	774-5961



PORTLAND PIPE LINE CORPORATION
Safety, Environment, Customer, Community



Sebago Technics
88017
lk

RALPH C. WINK
DIRECTOR OF ENGINEERING

May 3, 2000

TELEPHONE
(207) 767-0449

FACSIMILE
(207) 767-0455

E-MAIL
RALPH.WINK@PMPL.COM

Mr. John Locke
Mr. Charles E. Locke
7828 Mandarin Drive
Boca Raton, FL 33433

Subject: Sleep Inn Hotel

Dear Mr. Locke:

As a follow-up to our telephone conversation of April 20, Mr. Steve Doe of Sebago Technics met with me on April 21 to review the development plans that have been proposed to build a Sleep Inn Hotel off of Riverside Street in Portland, Maine on a parcel of property that is adjacent to high pressure crude oil pipelines operated by Portland Pipe Line Corporation (PPLC). The plan presented by Mr. Doe shows that the proposed building is to be constructed just off of the easement boundary with an access driveway and approximately 30 parking spaces located on the PPLC pipeline easement.

PPLC owns and operates 18-inch and 24-inch high pressure petroleum pipelines within this easement. There is also an inactive 12-inch line in the easement, which now serves as an anode to protect the integrity of the other two lines.

The easement or right-of-way, conveyed to PPLC in 1941, granted PPLC the right to construct, maintain, operate, alter, repair, remove, change the size of and replace pipelines within the easement. The original 1941 easement, while conveying all these rights to PPLC, was of undefined width. In 1981 at your request, PPLC agreed to limit the easement width to 50 feet from the centerline of its 12-inch line.

As I mentioned during our recent telephone discussions, PPLC does not allow permanent facilities to be installed within the easement areas where pipelines are located or where pipelines can be constructed because the U. S. Department of Transportation (Office of Pipeline Safety-OPS) requires pipeline operators to monitor the pipeline operation and regularly perform physical surveillance of pipeline routes to ensure public safety. Regular surveillance by air and ground is enhanced by visibility and lack of encroachment on the right of way. Other DOT regulations require additional protective cover if a line is located within 50 feet of certain buildings or places

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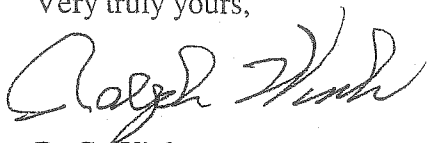
of public assembly. Industry sources indicate that the greatest danger of damage to, and spills from, oil pipelines is third party damage through excavation, construction and similar activities. From time to time, PPLC may have to excavate portions of the pipelines to inspect or perform maintenance or repair. This involves the use of heavy equipment which requires access and operating space.

PPLC does allow access roads to be constructed within easement zones to permit property owners access for development purposes. After reviewing the proposed development plan for the Sleep Inn facility with Mr. Doe, we would be willing to allow the construction of an access drive at a distance of 25 feet from the 12-inch pipeline. We would, however, have to insist that the 25 feet adjacent to the 12-inch line be kept free of permanent facilities, since the Pipe Line has the right to relocate or construct an additional pipeline in the easement; and if a pipeline were built in the easement, we need to have the ability to provide the required surveillance and maintenance of the pipeline.

PPLC will need to preserve its right of access over the entire 50 foot easement segment in order to perform maintenance, repair or construction work on the pipelines. As we discussed on May 3, PPLC will not be responsible to repair the driveway inside the easement should the driveway encounter damage during maintenance or construction work on the pipeline.

We believe this modification will permit your project to be constructed while maintaining PPLC's easement for future construction. We request that you review our proposal and advise us if the plans will be revised prior to the workshop meeting planned with the Portland Planning Board.

Very truly yours,

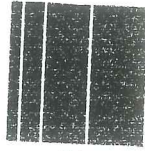


R. C. Wink

RCW/dj

cc: Sebago Technics
Mr. Graham Hollihan

bcc: Jeff White, PA
Dave Cyr
Tom Hardison
ROW I-30D/I-30E



Sebago Technics
Engineering & Planning for the Future

GM

May 24, 2000
88017

Mr. Frank Brancely
Portland Public Works
55 Portland Street
Portland, ME 04101-2921

Sleep Inn, 160 Riverside Street

Dear Mr. Brancely:

On behalf of J. Properties, I wish to request a sanitary sewer capacity letter from you for a proposed 74 room hotel located at 160 Riverside Street, Tax Map 267, Block A, Lot 4. This facility will connect to an existing sanitary sewer line that was installed on the property in 1998 as part of a previous development that was never completed. This line location is shown on the attached Grading Plan.

We estimate this facility to generate 7,460 gallons per day based on the Maine State Plumbing Code for hotels with private bathrooms. We will be submitting our plans and supportive documents to the City on May 30th for a public hearing on June 27th. I would appreciate it if you could please send your response to Kandice Talbot in Planning with a copy to me before June 27th.

Sincerely,

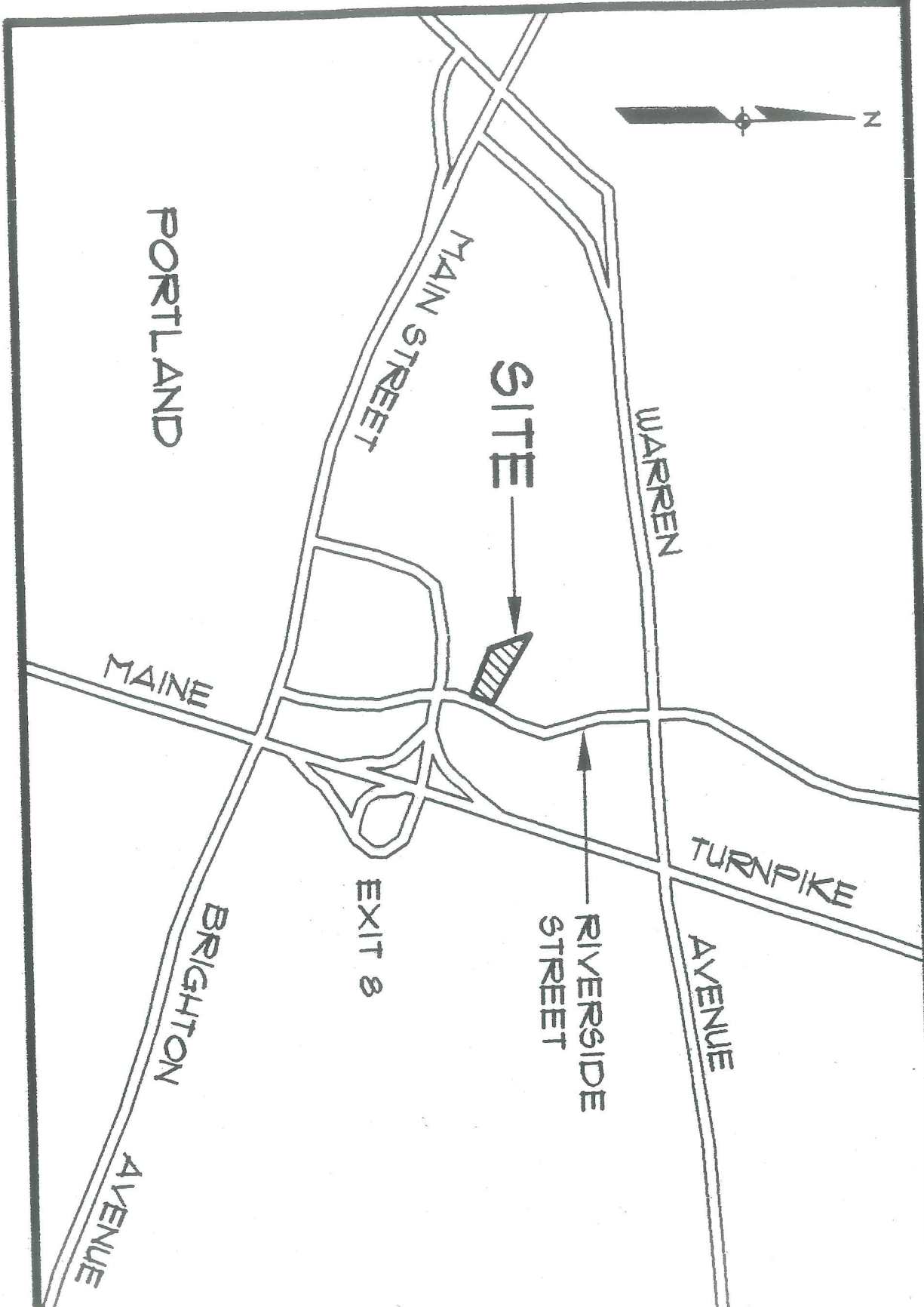
SEBAGO TECHNICS, INC.

Stephen G. Doe
Project Manager

SGD:ama/jc

cc: John Locke
Kandice Talbot

Att. 1



LOCATION MAP

PLANNING DIVISION

N.T.S.

I. INTRODUCTION

J. Properties is requesting review of a 34,638 sq. ft. hotel located at 160 Riverside Street. The Sleep Inn will be 3 stories high with 74 rooms. The site is approximately 2.23 acres and zoned B-4.

The site was previously approved in 1998 for a 9,900 sq. ft. two tenant flex space. Some work was started, such as the curb cut, sewer and storm lines, and fill.

The site is a "porkchop" shaped lot located off from Riverside Street. It is located on the westerly side of Riverside Street, between Mark's Showplace and a cellular telephone business, near Exit 8. A vicinity map is included as Attachment 1.

A legal ad appeared in the June 19th and 20th editions of the Portland Press Herald. 43 notices have been sent to area property owners in the vicinity of the project.

II. FINDINGS

Zoning:	B-4 Commercial Corridor Zone
Land Area:	2.23 acres
Floor Area:	34,638 sq. ft.
Proposed use:	Hotel
Parking:	
Proposed:	81 spaces
Required:	19 spaces

III. STAFF REVIEW

The proposed development has been reviewed for compliance with the standards of the Site Plan Ordinance of the Land Use Code. Review of the proposal has been completed by Planning, Public Works, Parks and Recreation and Fire Prevention staff.

IV. SITE PLAN REVIEW

1. Traffic/Circulation/Parking

Access to the proposed site will be from Riverside Street. The applicant is proposing a 41 ft. curb cut to allow for a three lane driveway, with two lanes for egress. The applicant is proposing 81 parking spaces. Zoning requires 1 space per every 4 guest rooms, which would be 19 parking spaces required.

The building will be located at the northern end of the property. Access to the building will be through the parking area and the applicant is providing a "loop" road around the hotel for fire access. This "loop" road is located within a 50 ft. Portland Pipe Line easement. Portland Pipe Line will not allow parking within the easement, but will allow the access road. The Portland Pipe Line agreement is attached.

The applicant is proposing to install sidewalk and granite curb along the frontage of Riverside Street. An internal sidewalk is also proposed from the hotel to Riverside Street.

A major concern with this site is traffic. The Traffic Engineer had requested that the applicant provide a traffic study for the proposed use. The applicant has prepared a traffic analysis which is included as Attachment 6c. The report states that the Exit 8/Riverside Street intersection operates Level F, but notes that the hotel will not have a measurable impact on the Exit 8 intersection. The report is recommending a geometric change to the Larrabee Road approach. This would move the island at the Exit 8 intersection and create a third lane at the approach to Exit 8. This improvement would upgrade the intersection to a Level E. This improvement plan is included as Attachment 12.

The Traffic Engineer has reviewed the proposal and concurs that this proposed change to the Exit 8/Larrabee Road intersection will improve the Level of Service operation of the intersection. The Traffic Engineer's memo is included as Attachment 9.

2. Bulk, Location, Height of Buildings and Uses Thereof

The applicant is proposing a three story 74 room hotel. The facade will be constructed of EIFS siding. Elevations of the building are included as Attachment 7.

3. Utilities/Easements/Solid Wastes

Utilities will connect to existing lines in Riverside Street. Portland Water District has submitted a capacity letter which is included as Attachment 6j. A potential condition of approval is:

- that a capacity letter from the Portland Sewer Division be provided to staff prior to issuance of a building permit.

4. Landscaping

The applicant is proposing a planted area near Riverside Street. This planted area will include three (3) Toba Hawthornes and a number of Spirea, Rhododendron and Bearberry. The applicant is also proposing a number of foundation plantings around the building. The landscape plan is included as Attachment 11c.

The City Arborist is currently reviewing the landscape plan and comments will be provided at the Public Hearing. A potential condition of approval is:

- that the landscape plan be reviewed and approved by the City Arborist.

5. Drainage

The applicant is proposing that the storm water will be directed through a series of catch basins to an existing detention pond at the rear of the parcel. The basin is shared with the Boulos property and Mark's Showplace. The applicant is also proposing an oil-grit separator for storm water treatment. An agreement letter from Boulos Property Management along with a quit claim deed for the detention basin rights is included as Attachment 8.

Public Works has reviewed the plans and does not have any issues with the plan. Public Works' memo is included as Attachment 4.

The Development Review Coordinator reviewed the proposed plan and required additional information. This additional information included limits of work, particularly in the vicinity of Riverside Street, and that it should be clearly identified and the work clearly spelled out as to repaving, trench repair, etc. Also, additional information was needed regarding the outlet control structure inverts and the flow splitter. The applicant has resubmitted plans addressing the DRC's concerns. The DRC with the new plans, however still has a concern with the storm drain structures and whether they will have adequate clearances for pipe installation. A potential condition of approval is:

- that the applicant review the storm drain structures for adequate clearances for pipe installation and if necessary provide special structures to allow the pipe, cover and casting to be installed.

6. Lighting

The applicant is proposing twelve (12) 150 watt, 20 ft. high light poles around the perimeter of the parking area. A number of the lights are located very near to the property line. A photometric plan is included as Attachment 11b. The photometric plan does show some spillover onto abutting properties, however given the business uses on the abutting properties it does not create much of a concern. Catalogue cuts of the lighting are included as Attachment 6h.

7. Fire Safety

The site plan has been reviewed and approved by the Fire Department.

8. Financial Capability

A letter of financial capability has not been provided at this time. A potential condition of approval is:

- that the applicant provide a Letter of Financial Capability prior to issuance of a building permit.

9. Natural Resources

It does not appear that this proposal will have any adverse impact on the natural resources of the area.

IV. MOTIONS FOR THE BOARD TO CONSIDER

On the basis of plans and materials submitted by the applicant and on the basis of information provided in Planning Report #27-00, the Planning Board finds:

- i. That the site plan ⁽¹⁾is/is not in conformance with the Site Plan Ordinance of the Land Use Code.

Potential Conditions of Approval:

1. that a capacity letter from the Portland Sewer Division be provided to staff prior to issuance of a building permit.
2. that the landscape plan be ~~reviewed and approved~~ ^{revised and approved 5/1/00} by the City Arborist.
3. ~~that~~ the applicant provide a Letter of Financial Capability prior to issuance of a building permit.
4. that the applicant review the storm drain structures for adequate clearances for pipe installation and if necessary provide special structures to allow the pipe, cover and casting to be installed.

*Malone
against
Hagge,
Baron Rosen*

Attachments:

1. Vicinity Map
2. Applicant's Submittal dated 3/28/00
3. Traffic Engineer's Memo dated 5/1/00
4. Public Works' Memo
5. DRC's Memo
6. Applicant's Submittal dated 5/30/00
7. Elevations
8. Letter from Boulos Property and Quit Claim Deed
9. Traffic Engineer's Memo dated 6/23/00
10. Neighbor's Letter
11. Plans
12. Traffic Improvement Plan

PLANNING BOARD REPORT #27-00

**160 RIVERSIDE STREET
SITE PLAN
J PROPERTIES, APPLICANT**

Submitted to:
Portland Planning Board
Portland, Maine

June 27, 2000

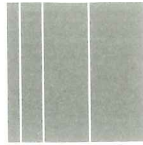
**CITY OF PORTLAND, MAINE
MEMORANDUM**

TO: Kandi Talbot, Planner
FROM: Jeff Tarling, City Arborist
DATE: June 27, 2000
RE: Sleep Inn Hotel, Riverside Street

I have reviewed the landscape plan for the Sleep Inn located on Riverside Street. Below are my comments:

1. The row of existing White Pines along the northerly edge of the property have some gaps within it. The applicant shall plant twelve, 6 ft. White Pine to fill in the gaps along this property line.
2. At the entrance, near the cellular phone business, the applicant shall install a Pin Oak, 2-1/2" caliper.
3. Two additional Austrian Pines shall be planted along the southerly side of the parking area.
4. A Summit Green Ash, 2-1/2" caliper shall be planted in the island on the southerly side of the property located next to the pedestrian cross walk.
5. There is an area of mature trees along the southerly property line abutting the Central Maine Property. The applicant shall try to preserve these trees, and if not able to shall replace the trees in accordance with the City Arborist.

Attached is a copy of a marked up plan showing location of additional landscaping that I feel is necessary. Thank you.



Sebago Technics

Engineering & Planning for the Future

July 18, 2000
88017

Kandice Talbot, Planner
City of Portland
389 Congress Street
Portland ME 04101

Sleep Inn, Riverside Street, Portland, Maine

Dear Kandi:

On behalf of John Locke, I am enclosing a copy of the Articles of Incorporation for Dover Development Corporation (the landowner) and PWM Properties, Inc., (the operating company) for the Sleep Inn development. Also attached is a revised landscape plan indicating additional plantings as requested by Jeff Tarling.

I hope these items address some of the conditions of the approval. If you need additional items regarding the corporate structure of the project, please call either Albert G. Ayres at 824-6077 or me.

Sincerely,

SEBAGO TECHNICS, INC.

Stephen G. Doe, R.L.A.
Project Manager

SGD:es/df

Enclosures

cc: John Locke
Albert Ayres

LAW OFFICES
ALBERT G. AYRE

120 Exchange Street, Suite 202 ☉ Portland, Maine 04101-5004
Where the quest for justice begins

Voice (207) 874-6077
Fax (207) 874-0531
e-mail: alayre@gwi.net

June 29, 2000

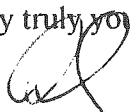
Stephen Doe
Sebago Technics, Inc.
12 Westbrook Commons
Westbrook, ME 04092

Re: Locke Project

Dear Steve:

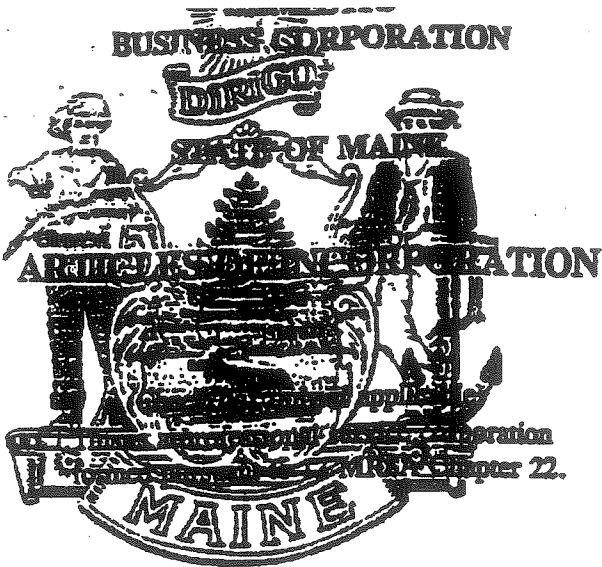
I understand that when you spoke with Candy prior to the meeting Tuesday she indicated that she would need "some additional information" re: the corporate structures. Enclosed are copies of the Articles of Incorporation for Dover Development Corp. (the land owner) and PWM Properties, Inc. (the operating company). Please find out what additional information she needs and let me know—I'll do my best to provide whatever she needs promptly.

Very truly yours,



Albert G. Ayre
cc: John Locke

Received
7-3-00
SEBAGO TECHNICS



File No. 20001533 0 Pages 3
 Fee Paid \$ 105
 DCN 2000471500017 ARTI
 FILED
 02/14/2000

Julie R. Flynn

Deputy Secretary of State

A True Copy When Attested By Signature

Julie R. Flynn
 Deputy Secretary of State

Pursuant to 13-A MRSA §403, the undersigned, acting as incorporator(s) of a corporation, adopt(s) the following Articles of Incorporation:

ARTICLE I: The name of the corporation is Dover Development Corp.
 and its principal business location in Maine is 160 Riverside Street, Portland, ME 04103
 (physical location - street (not P.O. Box), city, state and zip code)

ARTICLE II: The name of its Clerk, who must be a Maine resident, and the registered office shall be:
Albert G. Ayre, Esq.
 (name)
120 Exchange Street, Suite 202, Portland, ME 04101
 (physical location - street (not P.O. Box), city, state and zip code)
 (mailing address if different from above)

THIS FORM MUST BE ACCOMPANIED BY FORM MBCA-18A (Acceptance of Appointment as Clerk §304.2-A.).

ARTICLE III: ("X" one box only)

A. 1. The number of directors constituting the initial board of directors of the corporation is 2 (See §703.1.A.)

2. If the initial directors have been selected, the names and addresses of the persons who are to serve as directors until the first annual meeting of the shareholders or until their successors are elected and shall qualify are:

NAME	ADDRESS
<u>John E. Locke</u>	<u>831 Dover Street, Boca Raton, FL 33487</u>
<u>Jeffrey A. Locke</u>	<u>4130 Georges Way, Boca Raton, FL 33434</u>

3. The board of directors is is not authorized to increase or decrease the number of directors.

4. If the board is so authorized, the minimum number, if any, shall be 1 directors, (See §703.1.A.) and the maximum number, if any, shall be 3 directors.

B. There shall be no directors initially; the shares of the corporation will not be sold to more than twenty (20) persons; (See §701.2.)

FOURTE: ("X" one box only)

There shall be only one class of shares (title of class) Common

Par value of each share (if none, so state) \$1.00 Number of shares authorized 100,000

There shall be two or more classes of shares. The information required by §403 concerning each such class is set out in Exhibit attached hereto and made a part hereof.

SUMMARY

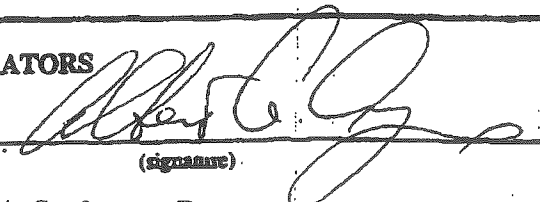
The aggregate par value of all authorized shares (of all classes) having a par value is \$ 100,000

The total number of authorized shares (of all classes) without par value is -0- shares

FIFTH: ("X" one box only) Meetings of the shareholders may may not be held outside of the State of Maine.

SIXTH: ("X" if applicable) There are no preemptive rights.

SEVENTH: Other provisions of these articles, if any, including provisions for the regulation of the internal affairs of the corporation, are set out in Exhibit attached hereto and made a part hereof.

INCORPORATORS

(signature)
Albert G. Ayre, Esq.
(type or print name)

DATED 2/11/00
Street 120 Exchange Street, Suite 202
(residence address)
Portland, ME 04101
(city, state and zip code)

(signature)

Street _____
(residence address)

(type or print name)

(city, state and zip code)

(signature)

Street _____
(residence address)

(type or print name)

(city, state and zip code)

Corporate Incorporators*

Name of Corporate Incorporator _____

(signature of officer)

Street _____
(principal business location)

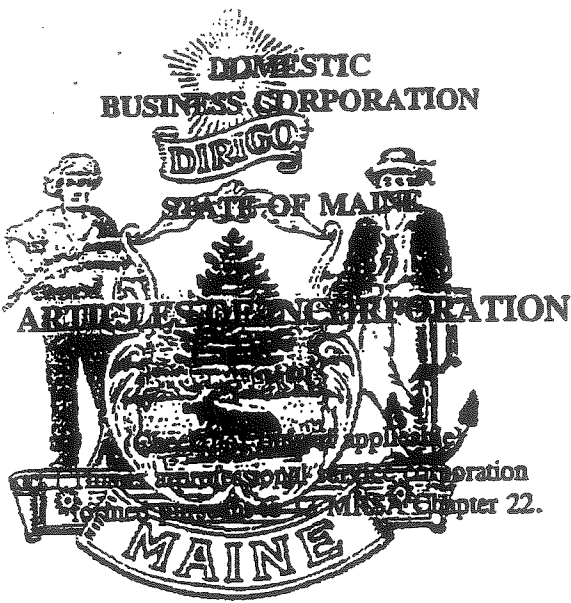
(type or print name and capacity)

(city, state and zip code)

Articles are to be executed as follows:

The corporation is an incorporator (§402), the name of the corporation should be typed and signed on its behalf by an officer of the corporation. The articles of incorporation must be accompanied by a certificate of an appropriate officer of the corporation, not the signing the articles, certifying that the person executing the articles on behalf of the corporation was duly authorized to do

**SUBMIT COMPLETED FORMS TO: CORPORATE EXAMINING SECTION, SECRETARY OF STATE,
101 STATE HOUSE STATION, AUGUSTA, ME 04333-0101**



Minimum Fee \$105. (See §1403 sub-§§1 and 2)

File No. 20001543 D Pages 3
 Fee Paid \$ 105
 DCN 2000471801016 ARTI
 FILED
 02/14/2000

Julie K. Flinn
 Deputy Secretary of State

A True Copy When Attested By Signature

Julie K. Flinn
 Deputy Secretary of State

Pursuant to 13-A MRSA §403, the undersigned, acting as incorporator(s) of a corporation, adopt(s) the following Articles of Incorporation:

FIRST: The name of the corporation is PWM Properties, Inc.
 and its principal business location in Maine is 160 Riverside Street, Portland, ME 04103
(physical location - street (not P.O. Box), city, state and zip code)

SECOND: The name of its Clerk, who must be a Maine resident, and the registered office shall be:
Albert G. Ayre, Esq.
(name)
120 Exchange Street, Suite 202, Portland, ME 04101
(physical location - street (not P.O. Box), city, state and zip code)
(mailing address if different from above)

THIS FORM MUST BE ACCOMPANIED BY FORM MBCA-18A (Acceptance of Appointment as Clerk §304.2-A.).

THIRD: ("X" one box only)
 A. 1. The number of directors constituting the initial board of directors of the corporation is 2 (See §703.1.A.)

2. If the initial directors have been selected, the names and addresses of the persons who are to serve as directors until the first annual meeting of the shareholders or until their successors are elected and shall qualify are:

NAME	ADDRESS
<u>John E. Locke</u>	<u>831 Dover Street, Boca Raton, FL 33487</u>
<u>Jeffrey A. Locke</u>	<u>4130 Georges Way, Boca Raton, FL 33434</u>

3. The board of directors is is not authorized to increase or decrease the number of directors.

4. If the board is so authorized, the minimum number, if any, shall be 1 directors, (See §703.1.A.) and the maximum number, if any, shall be 3 directors.

B. There shall be no directors initially; the shares of the corporation will not be sold to more than twenty (20) persons; the business of the corporation will be managed by the shareholders. (See §701.2.)

BIRTH: ("X" one box only)

There shall be only one class of shares (title of class) Common

Par value of each share (if none, so state) \$1.00 Number of shares authorized 100,000

There shall be two or more classes of shares. The information required by §403 concerning each such class is set out in Exhibit attached hereto and made a part hereof.

SUMMARY

The aggregate par value of all authorized shares (of all classes) having a par value is \$ 100,000

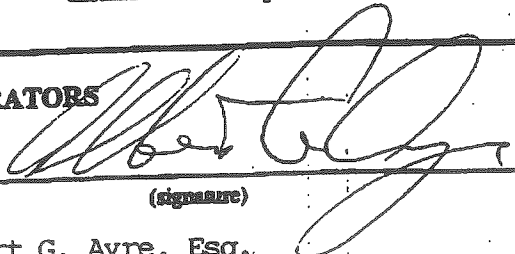
The total number of authorized shares (of all classes) without par value is -0- shares

MEETINGS: ("X" one box only) Meetings of the shareholders may may not be held outside of the State of Maine.

PREEMPTIVE RIGHTS: ("X" if applicable) There are no preemptive rights.

OTHER PROVISIONS: Other provisions of these articles, if any, including provisions for the regulation of the internal affairs of the corporation, are set out in Exhibit attached hereto and made a part hereof.

INCORPORATORS



(signature)

Albert G. Ayre, Esq.

(type or print name)

DATED 2/11/00

Street 120 Exchange Street, Suite 202
(residence address)

Portland, ME 04101

(city, state and zip code)

Street _____
(residence address)

(city, state and zip code)

Street _____
(residence address)

(city, state and zip code)

OR Corporate Incorporators*

Name of Corporate Incorporator _____

By _____
(signature of officer)

(type or print name and capacity)

Street _____
(principal business location)

(city, state and zip code)

Articles are to be executed as follows:

If the corporation is an incorporator (§402), the name of the corporation should be typed and signed on its behalf by an officer of the corporation. The articles of incorporation must be accompanied by a certificate of an appropriate officer of the corporation, not the person signing the articles, certifying that the person executing the articles on behalf of the corporation was duly authorized to do so.



CITY OF PORTLAND

17 November 2000

Stephen G. Doe, R.L.A.,
Project Manager,
Sebago Technics, Inc.,
P.O. Box 1339,
Westbrook, Maine 04098-1339

**RE: The Capacity to Handle Wastewater Flows, from the Proposed
"Sleep Inn" Hotel, to be Located, at 160 Riverside Street.**

Dear Mr. Doe:

The eight-inch diameter asbestos concrete sanitary sewer pipe, located in Riverside Street has adequate capacity to transport the anticipated wastewater flows of 7,400 GPD, from your proposed hotel. The Portland Water District sewage treatment facilities, located off Marginal Way, have adequate capacity to treat the anticipated wastewater flows of 7,400 GPD, from your proposed hotel.

Anticipated Wastewater Flows from the Proposed Health Food Store	
Proposed 74 Room Hotel @ 100 GPD/Bedroom	=7,400 GPD
Total Proposed Increase in Wastewater Flows for this Project	=7,400 GPD

If I can be of further assistance, please call me at 874-8832.

Sincerely,
CITY OF PORTLAND

Frank J. Brancely, BA, MA
Senior Engineering Technician

FJB/hld

- CC: Joseph E. Gray, Director, Department of Planning, & Urban Development, City of Portland
- ✓Kandice Talbot, Planner, Dept. of Planning, and Urban Development, City of Portland
- Katherine A. Staples, PE, City Engineer, City of Portland
- Bradley A. Roland, PE, Environmental Projects Engineer, City of Portland
- Anthony W. Lombardo, PE, Project Engineer, City of Portland
- Stephen K. Harris, Assistant Engineer, City of Portland
- Desk File

**CITY OF PORTLAND, MAINE
MEMORANDUM**

TO: Kandi Talbot, Planner
FROM: Jeff Tarling, City Arborist
DATE: June 27, 2000
RE: Sleep Inn Hotel, Riverside Street

I have reviewed the landscape plan for the Sleep Inn located on Riverside Street. Below are my comments:

1. The row of existing White Pines along the northerly edge of the property have some gaps within it. The applicant shall plant twelve, 6 ft. White Pine to fill in the gaps along this property line.
2. At the entrance, near the cellular phone business, the applicant shall install a Pin Oak, 2-1/2" caliper.
3. Two additional Austrian Pines shall be planted along the southerly side of the parking area.
4. A Summit Green Ash, 2-1/2" caliper shall be planted in the island on the southerly side of the property located next to the pedestrian cross walk.
5. There is an area of mature trees along the southerly property line abutting the Central Maine Property. The applicant shall try to preserve these trees, and if not able to shall replace the trees in accordance with the City Arborist.

Attached is a copy of a marked up plan showing location of additional landscaping that I feel is necessary. Thank you.

CITY OF PORTLAND, MAINE

PLANNING BOARD

Jaimey Caron, Chair
Deborah Krichels, Vice Chair
Kenneth M. Cole III
Cyrus Y. Hagge
Erin Rodriguez
Mark Malone
Orlando E. Delogu

June 28, 2000

Mr. John Locke
J. Properties
2211 N.W. 30th Place
Pompano Beach, FL 33069

re: Sleep Inn, 160 Riverside Street

Dear Mr. Locke:

On June 27, 2000 the Portland Planning Board voted 4-1 (Malone against, Caron, Hagge absent) to approve your application to construct a 74 room hotel located at 160 Riverside Street. The Board found that the application met the standards of the Site Plan ordinance of the Land Use code.

The approval was granted for the project with the following conditions:

1. that a capacity letter from the Portland Sewer Division be provided to staff prior to issuance of a building permit.
2. that the landscape plan be revised and approved by the City Arborist.
3. that the applicant provide a letter of financial capability prior to issuance of a building permit.
4. that the applicant review the storm drain structures for adequate clearances for pipe installation and if necessary provide special structures to allow the pipe, cover and casting to be installed.

The approval is based on the submitted site plan and the findings related to site plan review standards as contained in Planning Report #27-00, which is attached.

Please note the following provisions and requirements for all site plan approvals:

1. A performance guarantee covering the site improvements as well as an inspection fee payment of 1.7% of the guarantee amount and 7 final sets of plans must be submitted to and approved by the Planning Division and Public Works prior to the release of the building permit. If you need to make any modifications to the approved site plan, you must submit a revised site plan for staff review and approval.
2. The site plan approval will be deemed to have expired unless work in the development has commenced within one (1) year of the approval or within a time period agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the expiration date.

3. A defect guarantee, consisting of 10% of the performance guarantee, must be posted before the performance guarantee will be released.
4. Prior to construction, a preconstruction meeting shall be held at the project site with the contractor, development review coordinator, Public Work's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the preconstruction meeting.
5. If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)
6. The Development Review Coordinator (who is located at DeLuca Hoffman at 775-1121) must be notified five (5) working days prior to date required for final site inspection. Please make allowances for completion of site plan requirements determined to be incomplete or defective during the inspection. This is essential as all site plan requirements must be completed and approved by the Development Review Coordinator prior to issuance of a Certificate of Occupancy. Please schedule any property closing with these requirements in mind.

If there are any questions, please contact the Planning Staff.

Sincerely,



Jaimey Caron, Chair
Portland Planning Board

cc: Joseph E. Gray, Jr., Director of Planning and Urban Development
Alexander Jaegerman, Chief Planner
✓Kandice Talbot, Planner
P. Samuel Hoffses, Building Inspector
Marge Schmuckal, Zoning Administrator
Tony Lombardo, Project Engineer
Development Review Coordinator
William Bray, Director of Public Works
Nancy Knauber, Associate Engineer
Jeff Tarling, City Arborist
Charlie Lane, Associate Corporation Counsel
Lt. Gaylen McDougall, Fire Prevention
Inspection Department
Lee Urban, Director of Economic Development
Don Hall, Appraiser, Assessor's Office
Susan Doughty, Assessor's Office
Approval Letter File
Mr. Steve Doe, Sebago Technics, 12 Westbrook Common, Westbrook, ME 04098-1339



R. J. GRONDIN & SONS

11 Bartlett Road
Gorham, Maine 04038

(207) 854-1147 * FAX (207) 854-4315

78156

INVOICE 44012

J. Properties
2211 NW 30th Place
Pompano, FL 33069

DATE:	3/10/00
JOB #:	2351
CUST. #:	53700

JOB NAME	JOB LOCATION	REQUISITION #	PURCHASE ORDER #	
Site Work	160 Riverside, Ptblnd	Requisition #4		
DATE	DESCRIPTION	PRICE	AMOUNT	

Work to Date (3/10/00)	91,704.86	
Less Prior Requisitions	<u>71,014.86</u>	
This Requisition		\$ <u>20,690.00</u>

This Requisition	20,690.00
Past Due Balance	<u>57,774.86</u>
Total DUE	<u>78,464.86</u>

CONTINUATION SHEET (A/A)											
GEN CONTRACTOR/OWNER Job 2351											
SUB CONTRACTOR John Locke											
PROJECT NAME R.J. Grondin & Sons, Gorham, Maine											
APPLICATION #3 180 Riverside Street											
APPLICATION DATE: 4/30/99											
PERIOD FROM: 02/01/98 PERIOD TO: 04/30/98											
A	B	C	D	E	F	G	H	I	J		
ITEM NO.	DESCRIPTION OF WORK	SCHEDULED VALUE	WORK COMPLETED PREVIOUS	WORK COMPLETED THIS PERIOD	MATERIALS PRESENTLY STORED	TOTAL COMPLETED AND STORED TO DATE	BALANCE TO FINISH	RETAINAGE	% COMP.		
2050	Demolition	1,135.00	1,135.00	-	-	1,135.00	-	-	100%		
2110	Clearing	1,715.00	1,715.00	-	-	1,715.00	-	-	100%		
2200	Building	31,100.00	-	-	-	-	31,100.00	-	0%		
2210	E/B Electrical	10,548.00	-	-	-	-	10,548.00	-	0%		
2270	Erosion Control	3,320.00	1,106.66	-	-	1,106.66	2,213.34	-	33%		
2514	Pavement	59,623.00	14,905.75	14,905.75	-	29,811.50	29,811.50	-	50%		
2520	Site Concrete Prep	1,756.00	-	-	-	-	1,756.00	-	0%		
2525	Curb	11,638.00	-	-	-	-	11,638.00	-	0%		
2660	Waterline	9,325.00	-	-	-	-	9,325.00	-	0%		
2700	Sanitary Sewer	11,020.00	11,020.00	-	-	11,020.00	-	-	100%		
2720	Storm Drain	29,003.00	9,667.66	16,435.04	-	26,102.70	2,900.30	-	90%		
2900	Loam Placement	9,738.00	-	-	-	-	9,738.00	-	0%		
	Extra Permit - Inv. 41897	124.00	124.00	-	-	124.00	-	-	100%		
	TOTAL	160,045.00	39,674.07	31,340.79	-	71,014.86	109,030.14	-	39%		

Finance Department

Duane G. Kline
Director



CITY OF PORTLAND

April 14, 2000

Deborah J. Rubino, Business Specialist
Fleet Bank of Maine
50 Market Street
South Portland, ME 04106

Re: J.-Properties Site Improvements @ 160 Riverside Street
Escrow Acct #9360199026: Letter #1 dated October 29, 1998, Letter #2 dated February 8, 1999

Dear Ms. Rubino:

J-Properties has two performance guarantees (Letter #1 dated October 29, 1998 and Letter #2 dated February 8, 1999) for site improvements at 160 Riverside Street, both under escrow account #9360199026. This is to inform you that I am authorizing the reduction of these two guarantees as follows:

Letter 1 Current Balance:	\$13,240.00
Today's Reduction Request:	<u>7,161.00</u>
Letter 1 Amount Remaining:	\$ 6,079.00
Letter 2 Current Balance:	\$63,834.00
Today's Reduction Request:	<u>52,316.00</u>
Letter 2 Amount Remaining:	\$11,518.00

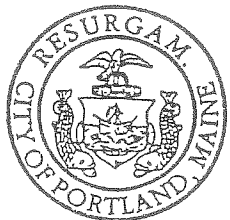
I will be out of the office next week, but if you have any questions, please call my assistant Jennifer Babcock at 874-8645.

Sincerely,

A handwritten signature in cursive script, appearing to read "D. Kline".

Duane G. Kline
Finance Director

pc: Joseph Gray, Director of Planning & Urban Development
Kandice Talbot, Planner



CITY OF PORTLAND
Planning and Urban Development Department

MEMORANDUM

TO: Duane Kline, Finance Department

FROM: Joseph E. Gray, Jr., Director of Planning and Urban Development

DATE: March 10, 2000


SUBJECT: Request for Reduction in Performance Guarantee
160 Riverside Street

A request by the owner of the commercial property at 160 Riverside Street has been received for a reduction in the performance guarantee amount. This is the second reduction requested.

Original Amount	\$26,290.00
Current Amount	\$26,290.00
Reduction Requested	<u>\$13,050.00</u>
Remaining Amount	\$13,240.00
Second PG submitted	<u>\$63,834.00</u>
Total Amount	\$77,074.00
Reduction Requested	<u>\$63,913.50</u>
Remaining Amount	\$13,160.50

The remainder is the sum, based on the performance guarantee, necessary to complete the work.

Approved:



Joseph E. Gray, Jr.
Director of Planning and Urban Development

cc: Kandi Talbot, Planner
Code Enforcement
Development Review Coordinator

Tuesday, June 27, 2000
via Fax Transmission

Bob Shackellton
Group One Architects
21 West Third Street
South Boston, Ma 02127

Dear Bob:

Re: Traffic Observations - Hotel Site

On Monday Evening June 26th from 4:15 pm to 6:00 pm and Tuesday Morning June 27th from 7:20 am to 9:00 am, HRI conducted observations of vehicular traffic entering and exiting the Howard Johnson Hotel and Friendly Restaurant parking lots at 155 Riverside Street, directly across the street from the site proposed for the new Sleep Inn & Suites Hotel. Noted below are the results of those observations.

June 26th Evening Observation June 27th Morning Observation

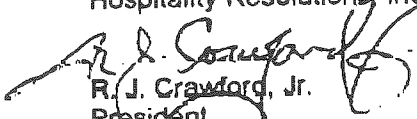
Hotel Arrivals	5 cars, 1 bus	1 car
Hotel Departures	5 cars	3 cars 1 bus
Restaurant Arrivals	6 cars	4 cars
Restaurant Departures	5 cars	5 cars

At no time was the drive-time of commuter traffic on Riverside Street impacted by vehicles entering or leaving the observed area. The Howard Johnson Hotel has 50 more rooms than the Proposed Sleep Inn. The Friendly Restaurant (sharing the driveway) generated an equal or greater amount of traffic than the hotel - no similar restaurant is planned at the proposed site.

It is HRI's view, that traffic entering and leaving the Proposed Hotel during peak commuter hours will be insignificant and (as in the case of the Howard Johnson Hotel) will not impact drive-time on Riverside.

Sincerely,

Hospitality Resolutions, Inc.


R. J. Crawford, Jr.
President

June 23, 2000

City of Portland, Maine-Dept. of Planning
389 Congress Street, 4th Floor
Portland, Maine 04101

Dear Sir/Madam:

Enclosed please find a copy of a notice I recently received pertaining to proposed land use in the City of Portland, ME. Please be advised that Guilford Transportation Industries, Inc. manages real estate matters for several corporations. This response is sent on behalf of the following corporations in reply to your notices received on June 21, 2000:

- Boston and Maine Corporation;
- Maine Central Railroad Company;
- Portland Terminal Company;
- Springfield Terminal Railway Company;
- Hudson River Estates, Inc.;
- Delaware River Estates, Inc.; or
- DH Estates, Inc. (formerly Delaware and Hudson Railroad).

As you may well be aware, the railroads listed above receive numerous notices on a daily basis. Based upon the limited time frame existing between the date on which the notice is received and the scheduled hearing date, I respectfully request your cooperation regarding this matter.

Although the railroads are concerned with each and every notice, there are certain instances which are of particular concern to the railroads. You may be able to advise me if the petition involves land plans showing a crossing of railroad property. Specifically the railroads' concern exists even in the event that your petitioner indicates that it is the holder of a valid railroad crossing. Also, the railroads must be advised if the petition proposes any development within 25' from the centerline of an existing railroad track, even if such development occurs on non-railroad property. As such, petitions for variances from set back requirements can be particularly troublesome.

City of Portland, ME
Dept. of Planning & Urban Development
June 23, 2000
Page Two

Please be advised that this letter should not be construed as a waiver of any other objections which the railroads may have to a proponent's plan. However, the railroads respectfully request your assistance in bringing their attention to any such matters.

Thank you for your anticipated cooperation in this regard.

Sincerely,

A handwritten signature in cursive script, reading "Kellie A. Dunn". To the right of the signature is a circular stamp containing the initials "KAD".

Kellie A. Dunn, Vice President - Real Estate

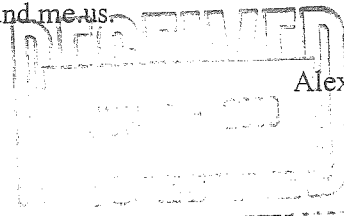
KAD/djl
Enclosure
D:\DATA\WP\KAD\ZONING\ME-PORTL.A40

TO RESIDENTS AND PROPERTY OWNERS IN THE VICINITY OF
160 RIVERSIDE STREET

On Tuesday, June 27, 2000, the Portland Planning Board will consider a plan by J. Properties to construct a 3-story, 74 room Sleep Inn located at 160 Riverside Street. The site is 2.23 acres and zoned B-4. The proposal will be reviewed for conformance with the Site Plan Ordinance.

The public hearing is scheduled to begin at 7:00 p.m. in Room 209, City Hall, 389 Congress Street, Portland, Maine. Should you wish to review the plans in advance, they are available in the Portland Planning Department, City Hall. If you are unable to attend the public meeting of the Board, please submit your written comments to Joseph E. Gray, Jr., Director of Planning and Urban Development, City Hall, 389 Congress Street, Portland, Maine 04101 or e-mail your comments to kcote@ci.portland.me.us.

Alexander Jaegerman
Chief Planner



From: "Donna M. Bean" <donna.bean@cmpco.com>
To: Portland.CityHall(KCOTE)
Date: Fri, Jun 23, 2000 1:26 PM
Subject: 160 Riverside Street, Sleep Inn Proposal

Mr. Cote:

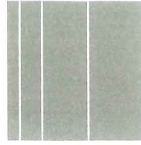
I am a Real Estate Agent employed by Central Maine Power Company. I am in receipt of a notice from the City of Portland of a plan under consideration by the Portland Planning Board proposed by J. Properties to construct a 3-story, 74-room Sleep Inn to be located at 160 Riverside Street.

After consulting a very old set of tax maps, it appears that this premises may be located on City of Portland Tax Map 267, Lot 4. If I am incorrect, please let me know.

If I am correct, it also appears that Lot 4 does not actually abut Central Maine Power Company's Transmission Line property, identified on Tax Map 267 as Lot 3. It appears that a narrow strip of land that is part of Lot 5 comes between Lot 3 and Lot 4.

If all of the above is true, CMP has no objection to the proposed development as long as it does not impact on CMP's transmission line property.

Donna-Mae Bean
Associate Real Estate Agent
Central Maine Power Company
83 Edison Drive
Augusta, ME 04336
Tel: 621-3872



Sebago Technics
Engineering & Planning for the Future

May 30, 2000
88017

Kandice Talbot, Planner
City of Portland
389 Congress Street
Portland, ME 04101

Sleep Inn, 160 Riverside Street

Dear Kandi:

On behalf of J. Properties, I am submitting the following plans and supportive documents in response to comments made by staff and the Planning Board. The applicant has also initiated a change in the building to better serve their customers. This is discussed under Item 1.

1. Revised site plan at a scale of 1"=30' showing a revised building footprint which represents a 3,788 square foot increase in building size. The new footprint allows for an upscale of 16 rooms to suites. There is no increase in room totals from the previous building. The new building expansion occurs in the front of the building near the handicapped parking spaces.

Other changes consist of narrowing the width of the travel lane in the rear of the building from 24' to 20'. We also continued the sidewalk along Riverside Street to the site. Snow storage areas are also indicated on the plan.

2. Grading plan showing proposed stormwater systems and Vortechincs unit.
3. Landscape plan showing proposed plantings.
4. Construction details of improvements.
5. Stormwater Management. We have sized the Vortechincs unit and designed the flow splitter for the project. Details are included in the plan set. Our discussions with Steve Bushy indicated that the Stormwater Management Report prepared for the previous development did not need to be updated since this project's impervious surface and stormwater control measures are nearly identical with the previous proposal. The Stormwater Management Report indicates that the shared detention basin has the capacity to handle the 2, 10 and 25-year storm events from this development. The new outlet control structure, installed in 1998, modified the existing system to handle this project. We are currently working with The Boulos Company to obtain the necessary agreement to utilize the common detention basin. A copy of this correspondence is attached. The final document will be submitted upon receipt.

6. Traffic analysis prepared by John L. Murphy. The report concludes that the current Exit 8/Riverside Street intersection operates at a Level F. The report further notes that the hotel will not have a measurable impact on the Exit 8 intersection. In Mr. Murphy's report, he proposes a geometric change to the Larrabee Road approach. This improvement upgrades the intersection to a Level E. We understand Mr. Ash has discussed this report with Mr. Murphy and is in agreement with the study and the proposed upgrades. This improvement is estimated to cost approximately \$13,000.00. A copy of the proposed improvement is attached for review.
7. Attached are the photometric plan and light fixture cut sheets for review and comments.
8. Capacity of existing utilities. We have requested capacity letters from the Sewer District and Portland Water District and requested their responses be directed to your attention. We do not anticipate any utility capacity problems in the area.
9. Portland Pipeline agreement. Attached is correspondence from Ralph Wink of the Portland Pipeline which discusses their position on improvements within their easement. Our plans are responsive to this agreement.

I trust this information addresses both staff and Planning Board comments. If you have any questions on this material or need additional information, please call.

Sincerely,

SEBAGO TECHNICS, INC.



Stephen G. Doe, RLA
Project Manager

SGD:ama/jc

Enc.

- cc: John Locke
Bob Shackelton, Group One Inc.
Jim Hoy, The Sheridan Corp.
Christine S. Thomas, CHA Choice Hotels

May 23, 2000

Jeffery Gee
Berkshire Life Insurance Company
C/o
New England Realty Resources
Suite 100
Ten Winthrop Square
Boston, MA 02110 1264

RE: 190 Riverside St. Portland, Me.

Dear Mr. Gee:

Attached please find a proposed site plan, grading & utility plan, and easement description for a planned "Sleep Inn" adjacent to our property. The owner of the abutting parcel is requesting both our and lender permission to connect to our existing 8" water main and to also disperse storm water into the detention pond that is located on both of our lots. We have reviewed this plan and do not find it objectionable. Please review this request with the lender and let me know if it is acceptable. The abutter ultimately needs to submit a "permission" letter to the City of Portland as part of the local approval process.

Thank you.

Sincerely,

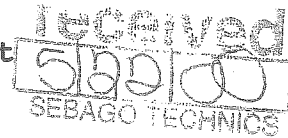
Paul Ureneck

JOHN L. MURPHY, P.E.

Civil Engineer
Traffic Engineer

RR1, BOX 6300
WEST BALDWIN, MAINE 04091-9745
207-625-8222

Traffic Analysis
Sleep Inn, 160 Riverside Street



General

The developer, J. Properties, proposes to build a 74 room motel at 160 Riverside Street. Based upon the Institute of Transportation Engineers' publication Trip Generation, the project can be expected to generate 35 PM peak hour trips, 19 entering and 16 exiting the site. The City of Portland has requested additional traffic analysis of the Exit 8/Riverside Street intersection.

Exit 8/Riverside Street/Larrabee Road

I have attached a copy of a May 12, 2000 4 PM to 6 PM peak period turning movement count of the intersection. I have also attached two highway capacity analyses of the 4:30 to 5:30 PM peak hour. These analyses used the latest possible highway capacity methodology specified in the December 1997 Highway Capacity Manual. One analysis assumed existing conditions and shows that the intersection operates at level of service F, which is the worst level of service defined by the Highway Capacity Manual. The second analysis used the same traffic volumes and green time assignments, but included geometric changes to permit Larrabee Road traffic in the curb lane to proceed either straight or right. This improvement changed the intersection level of service from F, with 91 seconds per vehicle average delay, to E (capacity) with 58.3 seconds/vehicle average delay. The approach delay for Larrabee Road decreased from 210 seconds/vehicle to 65.8 seconds, a 69% improvement for this approach.

Traffic Impact of Motel

The minor number of trips from the proposed project (35/hour) during the heaviest hour of the day will not have any measurable impact on the Exit 8 intersection.

Conclusion

Implementation of the proposed offsite improvement to the Exit 8 intersection will more than compensate for the minor traffic impact of this low volume project.

John L. Murphy
5/19/00

HCS: Signalized Intersections Release 3.1c

Inter: Riverside/Exit8/Larrabee
 Analyst: JL Murphy
 Date: 05/16/2000
 E/W St: Larrabee/Exit8

City/St: Portland, Me.
 Proj #: 0100
 Period: 16:30-17:00 5/12/00
 N/S St: Riverside St.

SIGNALIZED INTERSECTION

SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	1	1	1	2	0	1	2	0	1	2	0
LGConfig	L	T	R	L	T		L	T		L	TR	
Volume	288	391	101	285	399		35	447		486	522	368
Lane Width	12.0	12.0	12.0	12.0	12.0		12.0	12.0		12.0	12.0	
RTOR Vol			30									45

Duration 1.00 Area Type: All other areas
 Signal

Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left	A				NB Left	A		
Thru		A			Thru		A	
Right		A			Right			
Peds					Peds			
WB Left	A				SB Left	A	A	
Thru		A			Thru		A	A
Right					Right		A	A
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right			
Green	27.0	27.0			5.0	37.0	22.0	
Yellow	3.5	3.5			3.5	3.5	3.5	
All Red	1.5	1.5			1.5	1.5	1.5	
Cycle Length:	143.0 secs							

Intersection Performance

Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
L	334	1736	0.86	0.192	79.5	E		
T	351	1827	1.11	0.192	335.3	F	210.0	F
R	293	1553	0.24	0.189	49.5	D		
Westbound								
L	331	1719	0.86	0.192	78.9	E		
T	661	3438	0.60	0.192	53.9	D	64.3	E
Northbound								
L	62	1770	0.56	0.035	75.3	E		
T	557	3539	0.80	0.157	66.4	E	67.1	E
Southbound								
L	582	1752	0.84	0.332	75.7	E		
TR	1490	3304	0.57	0.451	29.3	C	46.2	D

Intersection Delay = 91.0 (sec/veh) Intersection LOS = F

HCS: Signalized Intersections Release 3.1c

John L. Murphy
 John L. Murphy P. E.
 RR1 Box 6300
 West Baldwin , Me. 04091

Phone: 207-625-8222

Fax: 207-625-8222

E-Mail: kjmurphy@cybertours.com

OPERATIONAL

ANALYSIS

Intersection: Riverside/Exit8/Larrabee
 City/State: Portland, Me.
 Analyst: JL Murphy
 Project No: 0100
 Time Period Analyzed: 16:30-17:00 5/12/00
 Date: 05/16/2000
 East/West Street Name: Larrabee/Exit8
 North/South Street Name: Riverside St.

HCS: Signalized Intersections Release 3.1c

Inter: Riverside/Exit8/Larrabee
 Analyst: JL Murphy
 Date: 05/16/2000
 E/W St: Larrabee/Exit8

City/St: Portland, Me.
 Proj #: 0100
 Period: 16:30-17:00 5/12/00
 N/S St: Riverside St.

SIGNALIZED INTERSECTION

SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	0	1	2	0	1	2	0	1	2	0
LGConfig	L	TR		L	T		L	T		L	TR	
Volume	288	391	101	285	399		35	447		486	522	368
Lane Width	12.0	12.0		12.0	12.0		12.0	12.0		12.0	12.0	
RTOR Vol			30									45

Duration 1.00 Area Type: All other areas
 Signal

Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left	A				NB Left	A		
Thru		A			Thru		A	
Right		A			Right			
Peds					Peds			
WB Left	A				SB Left	A	A	
Thru		A			Thru		A	A
Right					Right		A	A
Peds					Peds			
NB Right					EB Right			
SB Right					WB Right			
Green	27.0	27.0			5.0	37.0	22.0	
Yellow	3.5	3.5			3.5	3.5	3.5	
All Red	1.5	1.5			1.5	1.5	1.5	
Cycle Length:	143.0	secs						

Intersection Performance

Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
L	334	1736	0.86	0.192	79.5	E		
TR	652	3391	0.71	0.192	57.3	E	65.8	E
Westbound								
L	331	1719	0.86	0.192	78.9	E		
T	661	3438	0.60	0.192	53.9	D	64.3	E
Northbound								
L	62	1770	0.56	0.035	75.3	E		
T	557	3539	0.80	0.157	66.4	E	67.1	E
Southbound								
L	582	1752	0.84	0.332	79.9	E		
TR	1490	3304	0.57	0.451	29.3	C	47.8	D

Intersection Delay = 58.3 (sec/veh) Intersection LOS = E

HCS: Signalized Intersections Release 3.1c

John L. Murphy
 John L. Murphy P. E.
 RR1 Box 6300
 West Baldwin , Me. 04091

Phone: 207-625-8222

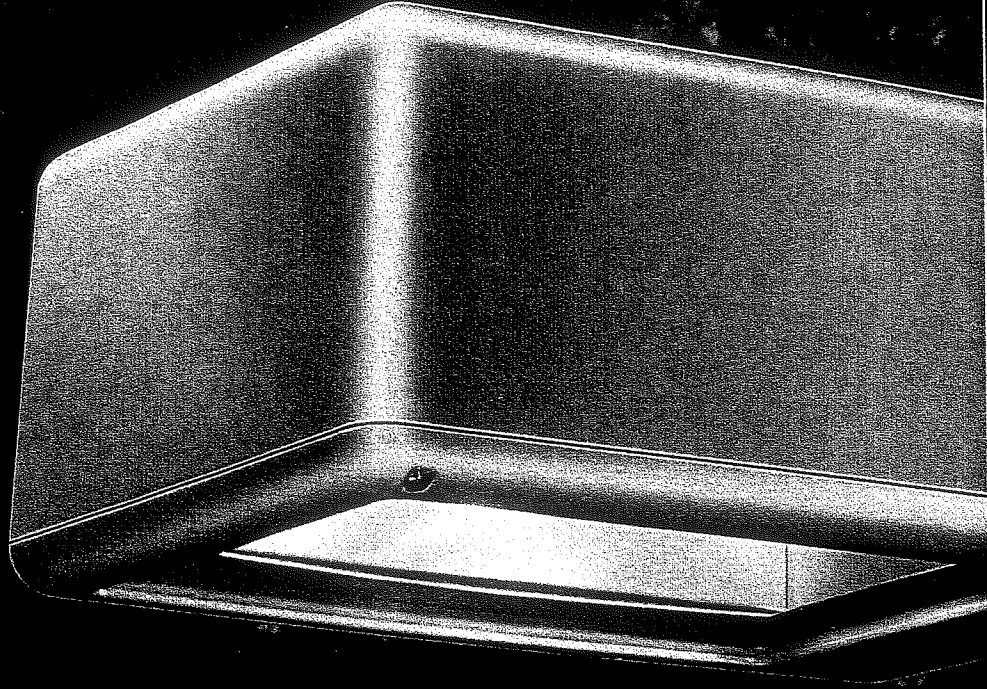
Fax: 207-625-8222

E-Mail: kjmurphy@cybertours.com

OPERATIONAL

ANALYSIS

Intersection: Riverside/Exit8/Larrabee
 City/State: Portland, Me.
 Analyst: JL Murphy
 Project No: 0100
 Time Period Analyzed: 16:30-17:00 5/12/00
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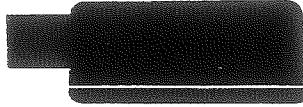













CAMBRIDGE

SPAULDING LIGHTING, INC.

LUMINAIRE ORDERING GUIDE

UL & CSA Listed.

Model	CEI - small size						CEII - large size			
Mounting Mode	PM						WB			
										
	Pole Mount						Wall Bracket			
Lamp Type/Watts	small size S100 S150 S250 S400 M175 M250 M400						large size S400 S1000 M400 M1000			
Reflector	I-asymmetric		III-asymmetric			IV-forward throw		VS-symmetric square		
Voltage	120	208	240	277	347	480	MT-multi-tap			
Options	PC - photoelectric cell 120-277v, up to 400w. PR - photo receptacle (less cell) VG - polycarbonate vandal guard						SF - single fuse DF - double fuse CS - house side cutoff shield			

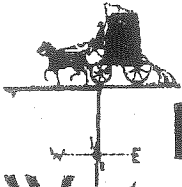
Colors for Luminaire and Pole	DBZ	SSB	RRN	SGB	WHT	FGP	TBP	RBP	CMB	SOS
										
	dark bronze	beige	rocket red	black	white	forest green	teal blue	royal blue	burgundy	silver

Luminaire Ordering Example:

MODEL	MOUNTING MODE	LAMP TYPE WATTS		REFLECTOR	VOLTAGE	OPTIONS	COLOR
CEII	PM	S400		IV	MT	PC	SOS
CEI CEII	PM; pole mount: std-6" arm for CEI std-10" arm for CEII WB; wall bracket	small S100 S150 S250 S400 M175 M250 M400	large S400 S1000 M400 M1000	I; symmetric II; asymmetric IV; forward throw VS; V-square	120 208 240 277 347 480 MT, multi-tap	PC; photoelectric cell 120-277v, up to 400w PR; photo receptacle (less cell) SF; single fuse DF; double fuse VG; polycarbonate vandal guard CS; house side cutoff shield	DBZ; dark bronze SSB; beige RRN; rocket red SGB; black WHT; white FGP; forest green TBP; teal blue RBP; royal blue CMB; burgundy SOS; silver

POLE ORDERING

Refer to Poles/Brackets Section for ordering information.



Portland Water District

225 Douglass St. • P.O. Box 3553 • Portland, ME 04104-3553

(207) 774-5961
FAX (207) 761-8307
www.pwd.org

May 30, 2000

Stephen O. Doe, RLA
Sebago Technics
PO Box 1339
Westbrook, Me. 04098

Re: Sleep Inn- 160 Riverside St.

Dear Steve,

This letter is to confirm there should be an adequate supply of clean and healthful water to serve the needs of the proposed sleep Inn at 160 riverside street in Portland. Checking District records, I find there is a 12" water main on the even side of the street.

The current data from the nearest hydrant indicates there should be adequate capacity of water.

Hydrant Location: Bounty Rd. @Riverside St
Hydrant # 1754
Static pressure = 83 PSI
Flow = 1264GPM
Last Tested = 6/25/96

If the district can be of further assistance in this matter, please let us know.

Sincerely,
Portland Water District

Jim Pandiscio
Jim Pandiscio
Means Coordinator

To	Steve Doe	Date	5/30/00
Co.	Sebago Technics	# of Pgs.	1
Dept.		From	Jim Pandiscio
Fax No.	856-2206	Phone #	774-5961

Portland Water District
225 Douglass St. - Portland, ME 04104
(207) 774-5961 • Fax (207) 761-8307



PORTLAND PIPE LINE CORPORATION
Safety, Environment, Customer, Community



Sebago Technics
2000

RALPH C. WINK
DIRECTOR OF ENGINEERING

May 3, 2000

TELEPHONE
(207) 767-0449

FACSIMILE
(207) 767-0455

E-MAIL
RALPH.WINK@PMPL.COM

Mr. John Locke
Mr. Charles E. Locke
7828 Mandarin Drive
Boca Raton, FL 33433

Subject: Sleep Inn Hotel

Dear Mr. Locke:

As a follow-up to our telephone conversation of April 20, Mr. Steve Doe of Sebago Technics met with me on April 21 to review the development plans that have been proposed to build a Sleep Inn Hotel off of Riverside Street in Portland, Maine on a parcel of property that is adjacent to high pressure crude oil pipelines operated by Portland Pipe Line Corporation (PPLC). The plan presented by Mr. Doe shows that the proposed building is to be constructed just off of the easement boundary with an access driveway and approximately 30 parking spaces located on the PPLC pipeline easement.

PPLC owns and operates 18-inch and 24-inch high pressure petroleum pipelines within this easement. There is also an inactive 12-inch line in the easement, which now serves as an anode to protect the integrity of the other two lines.

The easement or right-of-way, conveyed to PPLC in 1941, granted PPLC the right to construct, maintain, operate, alter, repair, remove, change the size of and replace pipelines within the easement. The original 1941 easement, while conveying all these rights to PPLC, was of undefined width. In 1981 at your request, PPLC agreed to limit the easement width to 50 feet from the centerline of its 12-inch line.

As I mentioned during our recent telephone discussions, PPLC does not allow permanent facilities to be installed within the easement areas where pipelines are located or where pipelines can be constructed because the U. S. Department of Transportation (Office of Pipeline Safety-OPS) requires pipeline operators to monitor the pipeline operation and regularly perform physical surveillance of pipeline routes to ensure public safety. Regular surveillance by air and ground is enhanced by visibility and lack of encroachment on the right of way. Other DOT regulations require additional protective cover if a line is located within 50 feet of certain buildings or places

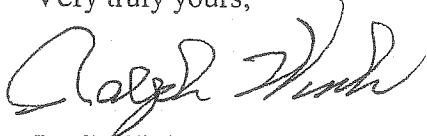
of public assembly. Industry sources indicate that the greatest danger of damage to, and spills from, oil pipelines is third party damage through excavation, construction and similar activities. From time to time, PPLC may have to excavate portions of the pipelines to inspect or perform maintenance or repair. This involves the use of heavy equipment which requires access and operating space.

PPLC does allow access roads to be constructed within easement zones to permit property owners access for development purposes. After reviewing the proposed development plan for the Sleep Inn facility with Mr. Doe, we would be willing to allow the construction of an access drive at a distance of 25 feet from the 12-inch pipeline. We would, however, have to insist that the 25 feet adjacent to the 12-inch line be kept free of permanent facilities, since the Pipe Line has the right to relocate or construct an additional pipeline in the easement; and if a pipeline were built in the easement, we need to have the ability to provide the required surveillance and maintenance of the pipeline.

PPLC will need to preserve its right of access over the entire 50 foot easement segment in order to perform maintenance, repair or construction work on the pipelines. As we discussed on May 3, PPLC will not be responsible to repair the driveway inside the easement should the driveway encounter damage during maintenance or construction work on the pipeline.

We believe this modification will permit your project to be constructed while maintaining PPLC's easement for future construction. We request that you review our proposal and advise us if the plans will be revised prior to the workshop meeting planned with the Portland Planning Board.

Very truly yours,



R. C. Wink

RCW/dj

cc: Sebago Technics
Mr. Graham Hollihan

bcc: Jeff White, PA
Dave Cyr
Tom Hardison
ROW I-30D/I-30E

**CITY OF PORTLAND, MAINE
MEMORANDUM**

TO: Chair Caron and Members of the Portland Planning Board

FROM: Kandice Talbot, Planner

DATE: May 9, 2000

SUBJECT: Sleep Inn, 160 Riverside Street

Introduction

J. Properties is requesting review of a 30,850 sq. ft. hotel located at 160 Riverside Street. The Sleep Inn will be 3 stories high with 74 rooms. The site is approximately 2.23 acres and zoned B-4.

This site was previously approved in 1998 for a 9,900 sq. ft. two tenant flex space. Some work was started, such as the curb cut, sewer and storm lines, and fill.

The site is a "porkchop" shaped lot located off from Riverside Street. It is located on the westerly side of Riverside Street, between Mark's Showplace and a cellular telephone business, near Exit 8. A vicinity map is included as Attachment 1.

Access/Circulation

Access to the proposed site will be from Riverside Street. The applicant is proposing a 41 ft. curb cut to allow for a three lane driveway, with two lanes for egress. The applicant is proposing 81 parking spaces. Zoning requires 1 space per every 4 guest rooms, which would be 19 parking spaces required.

The building will be located at the northern end of the property. Access to the building will be through the parking area and the applicant is providing a "loop" road around the hotel for fire access. This "loop" road is located within a 50 ft. Portland Pipe Line easement. Portland Pipe Line will not allow parking within the easement, but will allow the access road.

The Traffic Engineer had requested that the applicant provide a traffic study for the proposed use. As the Board is aware, an extensive traffic study was performed recently for the Portland Commons development which proposed major improvements to the Riverside Street/Exit 8 area. In lieu of preparing a traffic study, the applicant is requesting that the cost of the study be spent on funding some of the needed improvements identified in the Portland Commons study. Larry Ash has stated that in lieu of a traffic study, that the applicant would be required to resurface the Exit 8 intersection as part of their site development. This improvement will cost approximately \$5,000 to \$6,000. The Traffic Engineer's memo is included as Attachment 3.

The applicant is proposing to install sidewalk and granite curb along the frontage of Riverside Street.

Drainage and Utilities

The applicant is proposing that the stormwater will be directed through a series of catch basins to an existing detention pond at the rear of the parcel. This basin is shared with the Boulos property and Mark's Showplace. The applicant is also proposing an oil-grit separator for stormwater treatment.

Utilities will connect to existing lines in Riverside Street. The applicant will need to supply staff with capacity letters from the Utility Companies.

Public Works has reviewed the plans and does not have any issues with the plan. Public Works' memo is included as Attachment 4. The Development Review Coordinator has reviewed the proposed plans and is asking for additional information. The DRC is recommending that the applicant show limits of work, particularly in the vicinity of Riverside Street, and that it should be clearly identified and the work clearly spelled out as to repaving, trench repair, etc. Also, additional information should be provided regarding outlet control structure inverts and the flow splitter. The DRC's memo is included as Attachment 5.

Lighting

The applicant is proposing eleven (11) 150 watt, 20 ft. high light poles around the perimeter of the parking area. A number of the lights are located very near to the property line. The applicant will need to provide catalogue cuts of the lights and a lighting photometric plan to ensure that the lighting will not spillover onto abutting property and that the lighting does not exceed the maximum light levels.

Landscaping

The applicant is proposing a planted area near Riverside Street. This planted area will include three (3) Toba Hawthornes and a number of Spirea, Rhododendron and Bearberry. The applicant is also proposing a number of foundation plantings around the building. The City Arborist is currently reviewing the plan. The landscape plan is included as Attachment 6.

Building Design

The applicant is proposing a three story 74-room hotel. The facades will be constructed of EIFS siding. Elevations of the building are included as Attachment 7.

Attachments:

1. Vicinity Map
2. Applicant's Submittal
3. Traffic Engineer's Memo
4. Public Works' Memo
5. Development Review Coordinator's Memo
6. Plans
7. Elevations

From: Anthony Lombardo
To: Kandi Talbot
Date: Wed, Apr 12, 2000 9:43 AM
Subject: Sleep Inn.....160 Riverside St. 4/12/00

I have reviewed the plans and have no Public Works issues.

MEMORANDUM

88017

*The Sheridan Corporation
P.O. Box 689
Westbrook, ME 04098*

To: Jim Hoy
From: Steve Doe
Date: March 15, 2000
Subject: Sleep Inn
Minutes of Meeting, March 13, 2000
cc: Kandi Talbot, John Locke

On Wednesday, March 15, 2000, Steve Doe of Sebago Technics, Inc. and Jim Hoy of Sheridan met with Kandi Talbot, Lt. Gaylan McDougal, Marge Schmuckal and Larry Ash from the City of Portland for a preliminary review of the proposed Sleep Inn at 160 Riverside Street. Issues discussed are as follows:

1. Need fire hydrant on site near building area. Site plan has good circulation for fire trucks.
2. A traffic study will be required which addresses the following:
 - AM/PM peak hour
 - Impact on Exit 8 intersection, Home Depot light, and Riverside Street and Warren Avenue
 - What is distance from site entrance to Exit 8 and new light proposed at Portland Commons project.
3. Applicant will be assessed a fee as a contribution for traffic improvements in the Riverside Street area. This has been assessed on new developments in the area. Fees have ranged from \$6,500.00 to \$10,000.00, or applicant can contribute to completing some improvements to the area as determined by the City.

4. An oil/grit separator will be required for parking lots over 25 spaces. This item will cost approximately \$20,000.00.
5. Project must comply with new lighting standards for City.
6. The earliest Planning Board workshop we can be scheduled for is April 18th. Submission of material needs to be in by March 28th.
7. The project is located in the B-4 Zone which has a maximum impervious surface restriction of 80%. There is not a 10' parking setback from property lines in this zone.

SGD:jc

Site Work 160 Riverside St.

Page 1

12/28/98

	Phase 1 December	Phase 2 January	Phase 3 Spring	Total
2050 Demolition	885	250	0	1135 ✓
2110 Clearing	1715	0	0	1715 ✓
2200 Building	0	17000	14100	31100 ✓
2210 E/B Electric	0	0	10548	10548 ✓
2270 Erosion Control	0	2220	1100	3320 ✓
2514 Pavement	16940	21,341	42683	59623 ✓
2520 Site Concrete Prep.	0	0	1756	1756 ✓
2525 Curb	6750	0	4888	11638 ✓
2660 Waterline	0	9325	0	9325 ✓
2700 Sanitary Sewer	0	11020	0	11020 ✓
2720 Storm Drain	0	29003	0	29003 ✓
2900 Loam Replacement	0	0	9738	9738 ✓
	26290	68818	84813	179921
				179921

Reduction 4/14/00

	Phase I	Phase II
Demolition	885	250
Clearing	1715	
Building		17000
Erosion Control		1,107
Paving	16,940	12,871
Curb	3,300	
Sanitary Sewer		Page 111,020
Storm Drain		26,103.00
	<u>22,840.00</u>	51,351.00 51,351.00

From: "Steve Bushey" <srbushey@maine.rr.com>
To: "Kandi Talbot" <KCOTE@ci.portland.me.us>
Date: Wed, May 3, 2000 3:28 PM
Subject: Sleep Inn

Kandi,

I have reviewed the submission materials by sebago Technics for the proposed sleep Inn. I offer the following comments:

1. the site plan should have a baseline or other means of geometric layout for the paved surfaces and building.
2. A snow storage area should be identified on the plan.
3. The limits of works, particularly in the vicinity of Riverside street should be clearly identified and the work clearly spelled out as to repaving, trench repair etc.
4. the applicants engineer should review the storm drain structures for adequate clearances for pipe installation. Several are very shallow and may require special structures to allow the pipe, cover and casting to be installed.
5. Additionall data should be provided on the flow splitter.
6. I'm uncertain about the outlet control structure inverts, since they appear lower than the Detention basin.
7. The Planning Board/Staff may want to consider requiring granite curb instead of Bit. curb since this will be a very tight parking lot whith high traffic use.

If you have any questions regarding these comments please call.

Steve Bushey Acting Development review Coordinator

From: Larry Ash
To: Kandi Talbot
Date: Mon, May 1, 2000 12:46 PM
Subject: Sleep Inn, 160 Riverside Street

Kandi: To follow up on our conversation this morning, Sleep Inn will be required to resurface the Exit 8 Intersection as part of their site development. I would expect this to cost in the neighborhood of \$5,000 to \$6,000. The exact resurfacing area will have to be more precisely determined after I talk with Tony Lombardo.

They cannot do what has already been planned as part of the Kennan Auction Site Development.

If you have any questions please call me at 8894.

CC: Anthony Lombardo, William Bray

FROM : ALFONSO FODDS

PHONE NO. : 9549742773

Feb. 08 1999 02:26PM PM

OCT-26-98 09:01 AM PLANNING DEPARTMENT

7369200

P. 03

Department of Planning and Urban Development
SUBDIVISION/SITE DEVELOPMENT

COST ESTIMATE OF IMPROVEMENTS TO BE COVERED BY PERFORMANCE GUARANTEE

Date 2/8/99

Name of Project J Properties

Address/Location 160 RIVERSIDE ST.

Developer J Properties

Form of Performance Guarantees First Bank of MAINE

Type of Development: Subdivision Site Plan (Major/Minor)

TO BE FILLED OUT BY APPLICANT:

Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
1. STREET/SIDEWALK						
AND <u>PAVEMENT (PANELS), EXCAVATION INCL.</u>				<u>106751</u>	<u>20.-</u>	<u>21,341.-</u>
Grass Curbing						
Sidewalk						
Expanses						
Monuments						
Street Lighting						
Other						
2. SANITARY SEWER						
Manholes				<u>2 EA.</u>	<u>1700</u>	<u>3400</u>
Piping				<u>317 LF</u>	<u>24.-</u>	<u>7608.-</u>
Connections						
Other						
3. STORM DRAINAGE						
Manholes						
Catchbasins						
Pipes				<u>4 EA.</u>	<u>1700.-</u>	<u>6800.-</u>
Detention Basin				<u>200 LF</u>	<u>25.-</u>	<u>5000.-</u>
Other <u>INSULATION</u>				<u>45</u>	<u>1500.-</u>	<u>6750.-</u>
				<u>45</u>	<u>450.-</u>	<u>2025.-</u>
4. SITE LIGHTING						
5. EROSION CONTROL				<u>415</u>	<u>\$ 2220.00</u>	<u>\$ 7220</u>
6. RECREATION AND OPEN SPACE AMENITIES						

FROM : ALFONSO FOODS

PHONE NO. : 9549742773

Feb. 08 1999 02:27PM PJ

OCT-20-98 07:02 AM PLANNING DEPARTMENT

7849380

P. 04

Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
7. LANDSCAPING (attach breakdown of plant materials, quantities, and unit costs)	_____	_____	_____	_____	_____	_____
8. MISCELLANEOUS	_____	_____	_____	_____	_____	_____
TOTAL	_____	_____	_____	_____	_____	_____
GRAND TOTAL:	_____	_____	_____	63,834.00		

INSPECTION FEE (to be filled out by City)

	PUBLIC	PRIVATE	TOTAL
A. 1.7% of totals:	_____	_____	_____
or			
B. Alternative Assessment:	_____	_____	_____
Assessed by:	_____ (NAME)	_____ (NAME)	_____

JOHN L. MURPHY, P.E.

*Civil Engineer
Traffic Engineer*

RR1, BOX 6300
WEST BALDWIN, MAINE 04091-9745
207-625-8222

**Traffic Analysis
Portland Commons
Riverside Street, Portland
Supplemental Report**

General

There was a meeting on 5/11/99 regarding this project and the related traffic analysis. Prior to the meeting, a letter was written by Larry Ash, City Traffic Engineer, asking for additional information. The letter was addressed to Bill Needleman of the Planning Department and was dated May 10, 1999. Some of the items requested in the May 10, 1999 letter were answered during the May 11, 1999 meeting at which all parties in attendance agreed that an additional lane would be required on Riverside Street between a point roughly 100 feet northerly of the project driveway and Exit 8/Larrabee Road.

Capacity Analysis (letter of 5/10/99 #1 and #5)

The meeting of 5/11/99 resulted in a conclusion that existing volumes projected to the year 2000 would be used for analysis. No new Turnpike access from Rand Road was assumed to be constructed.

Year 2000 design hour volumes were developed as follows:

1. Base weekday and Saturday peak hour volumes were assembled for Riverside Street between Warren Avenue and Brighton Avenue. Saturday data was all collected on April 10, 1999.
2. The data for Saturday and weekday were balanced for a base no build network. The weekday PM peak hour was balanced from the July 1997 count at Exit 8/Riverside Street. The Saturday data was balanced over the corridor around the Exit 8/Riverside Street count. This provided a July 1997 weekday base no build network and an April 10, 1999 Saturday base no build network.
3. The July 1997 data was factored by 1.03 (1%/year growth) to a weekday PM peak hour no build network for the year 2000. The April 10, 1999 data was factored by 1.18 to a summer 2000 no build base peak hour (17% for seasonal adjustment and 1% growth factor).

4. Saturday and weekday peak hour volumes were determined based upon project square footage and assumed usage of buildings.

5. The other project in the study area was a 9900 square foot warehouse, and Saturday plus weekday volumes were estimated from this use for inclusion in the study.

6. The project traffic and the warehouse traffic was then added to the no build base data for Saturday and weekday peak hours for 2000 for a design hour network.

Since a decision had already been made that a lane was required on Riverside Street due to capacity problems at Exit 8, analysis was performed on Saturday and weekday design hour volumes for the year 2000 with the project traffic included (build condition). Capacity analysis using the 1994 highway capacity software is included for the following locations as requested by Larry Ash, City Traffic Engineer.

1. Riverside Street/Warren Avenue
2. Riverside Street/Home Depot
3. Riverside Street/Exit 8
4. Riverside Street/Portland Commons

The analysis resulted in level of service D or better for all requested locations for Saturday and weekday peak hour build volumes for the year 2000.

The eight capacity analysis printouts are in the appendix, along with the data related to the development of design hour volumes described in items 1 through 6.

Accident Review (letter of 5/10/99 #2)

Accident diagrams are attached in the appendix for all MDOT defined high accident locations (HALs) in the study area. Basically, the accidents present a picture of a congested corridor where left turns from and into unsignalized driveways cause angle and rear end collisions. The existing alignment with fairly narrow lanes for the number of trucks causes lane change and sideswipe accidents on Riverside Street between Exit 8 and Warren Avenue.

Exit 8 just exceeded the threshold to be considered a high accident location due to a series of rear end accidents in 1996 (probably during construction of through-right turn lanes on Larrabee Road and Riverside Street). Warren Avenue at Riverside Street has several accident patterns, however, the City has recently installed a left turn arrow (5 section head) facing the southbound Riverside Street approach. This will address the more serious angle accident pattern involving southbound left turn and through vehicles northbound on Riverside Street.

Warrant Data (letter of 5/10/99 #3)

Based upon the estimated volumes for Portland Commons, the 150 vehicle threshold for the peak hour volume warrant will be exceeded during the weekday PM peak hour and during a Saturday midday peak hour. The four hour volume warrants will probably also be exceeded if normal shopping center type traffic flow occurs from Portland Commons.

Sight Distance (letter of 5/10/99 #4)

The proposed driveway will be signalized, but it has 425 feet of sight distance to the north and 920 feet to the south. This was considered by MDOT to be sufficient for the 35 mile per hour speed limit on Riverside Street.

Miscellaneous Items (letter of 5/10/99 #6, 7, 8 and 9)

6. A scaled drawing showing Riverside Street between Warren Avenue and Exit 8 has been provided. This drawing also includes the proposed improvement to Riverside Street.

7. The year 2002 volumes are not being used for analysis as previously discussed.

8. Pass-by trips are estimated to be 38% of total trips for Saturday and weekday design hour volumes.

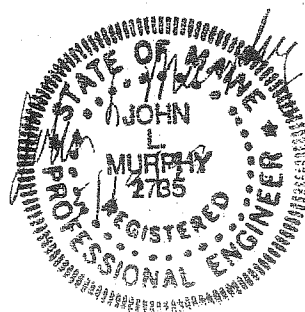
9. The traffic report will conform to the conditions requested by the DEP in the scoping meeting for the project.

Required Improvements for Level of Service D or Better

1. New lane southbound on the westerly side of Riverside Street from a point 100 feet northerly of Portland Commons Drive to Exit 8.

2. Reset curbing on traffic island on the southeasterly side of Riverside Street opposite the Larrabee Road approach to Exit 8.

3. Install an overhead sign assembly facing the Larrabee Road traffic approaching Exit 8.



APPENDIX

- A. Capacity Analyses Saturday Build 2000 Weekday Build 2000
 - Riverside/Exit 8
 - Riverside/Portland Commons
 - Riverside/Home Depot
 - Riverside/Warren Ave.

- B. Network Volume Data Saturday
 - 1. Project Impact
 - 2. Counts 4/10/99
 - 3. Balanced No Build Saturday 1999
 - 4. Balanced Saturday No Build Factored by 1.18 to Year 2000 No Build
 - 5. Balanced Saturday Build 2000

- C. Network Volume Data Weekday Peak Hour 2000
 - 1. Project Impact
 - 2. No Build Volumes (Counts)
 - 3. Balanced No Build Weekday Base July 1997
 - 4. Balanced No Build Weekday Base July 2000 (Factor 1.03)
 - 5. Build Portland Commons 2000 Weekday DHV
 - 6. Other Projects Weekday & Saturday

- D. Accident Data High Accident Locations
 - 1. Node Map
 - 2. Nodes 6306 - 9090, Brighton - Exit 8 1993-1997
 - 3. Node 9090, Exit 8 1995-1997
 - 4. Nodes 9090 - 6307, Exit 8 - Riverside Court 1993-1997
 - 5. Nodes 6307 - 6309, Riverside Court to Old RR Track 1993-1997
 - 6. Node 6310, Riverside Street/Warren Avenue 1994-1997

Streets: (N-S) RIVERSIDE ST. (E-W) EXIT 8
 Analyst: JLM File Name: EX8BSAT.HC9
 Area Type: Other 4-10-99 PM PK.
 Comment: Build Portland Commons Saturday 2000

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	1	1	2	< 0	1	2	< 0	1	2	1
Volumes	47	673	224	413	674	269	371	198	66	264	197	479
Lane W (ft)	12.0	12.0	12.0	12.0	12.0		12.0	12.0		12.0	12.0	12.0
RTOR Vols			100			0			0			100
Lost Time	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
NB Left	*				EB Left	*	*	
Thru			*		Thru	*	*	
Right			*		Right	*	*	
Peds					Peds			
SB Left	*	*			WB Left	*		
Thru		*	*		Thru		*	
Right		*	*		Right		*	
Peds					Peds			
EB Right					NB Right	*	*	*
WB Right	*	*			SB Right			
Green	7.0A	20.0A	25.0A		Green	22.0A	2.0A	12.0A
Yellow/AR	5.0	5.0	5.0		Yellow/AR	5.0	5.0	5.0

Cycle Length: 118 secs Phase combination order: #1 #2 #3 #5 #6 #7

Intersection Performance Summary

Lane	Group:	Adj Sat	v/c	g/C	Delay	LOS	Approach:		
							Delay	LOS	
Mvmts	Cap	Flow	Ratio	Ratio					
NB	L	134	1752	0.367	0.076	34.2	D	30.4	D
	T	861	3762	0.863	0.229	34.6	D		
	R	1057	1599	0.124	0.661	4.8	A		
SB	L	505	1752	0.861	0.288	35.6	D	22.8	C
	TR	1587	3601	0.657	0.441	17.5	C		
EB	L	465	1770	0.841	0.263	35.6	D	32.6	D
	TR	626	3517	0.465	0.178	28.5	D		
WB	L	360	1770	0.772	0.203	35.5	D	27.5	D
	T	442	3725	0.491	0.119	32.2	D		
	R	638	1568	0.626	0.407	19.4	C		

Intersection Delay = 27.3 sec/veh Intersection LOS = D
 Lost Time/Cycle, L = 12.0 sec Critical v/c(x) = 0.807

Streets: (N-S) Riverside St. (E-W) Portland Commons
 Analyst: JLM File Name: PCRBSAT.HC9
 Area Type: Other 5-17-99 PM
 Comment: Build Portland Commons 2000 Sat.

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	< 0	1	2	< 0	0	> 2	< 0	1	1	< 0
Volumes	18	1354	150	135	1189	18	6	1	25	140	1	130
PHF or PK15	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Lane W (ft)	12.0	12.0		12.0	12.0		12.0			12.0	12.0	
Grade		0			0			0			0	
% Heavy Veh	2	2	2	2	2	2	2	2	2	2	2	2
Parking	N	N		N	N		N	N		N	N	
Bus Stops			0			0			0			0
Con. Peds			0			0			0			0
Ped Button	(Y/N) N			(Y/N) N			(Y/N) N			(Y/N) N		
Arr Type	3	3		3	3			3		3	3	
RTOR Vols			0			0			0			0
Lost Time	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00
Prop. Share												
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
NB Left	*				EB Left	*		
Thru		*			Thru	*		
Right		*			Right	*		
Peds					Peds			
SB Left	*				WB Left	*		
Thru		*			Thru	*		
Right		*			Right	*		
Peds					Peds			
EB Right					NB Right			
WB Right					SB Right			
Green		6.0A	45.0A		Green	12.0A		
Yellow/AR		5.0	5.0		Yellow/AR	5.0		
Cycle Length:	78 secs Phase combination order: #1 #2 #5							

Intersection Performance Summary

Lane	Group:	Adj Sat	v/c	g/C	Delay	LOS	Approach:	Delay	LOS
Mvmts	Cap	Flow	Ratio	Ratio					
NB	L	181	1770	0.105	0.103	20.5	C	8.5	B
	TR	2211	3670	0.752	0.603	8.3	B		
SB	L	181	1770	0.782	0.103	35.1	D	9.3	B
	TR	2240	3717	0.596	0.603	6.5	B		
EB	LTR	523	2911	0.067	0.179	17.2	C	17.2	C
WB	L	287	1601	0.512	0.179	20.0	C	19.8	C
	TR	285	1585	0.485	0.179	19.6	C		

Intersection Delay = 9.8 sec/veh Intersection LOS = B
 Lost Time/Cycle, L = 9.0 sec Critical v/c(x) = 0.706

Streets: (N-S) Riverside St. (E-W) Home Depot
 Analyst: JLM File Name: HDRIVB.HC9
 Area Type: Other 5-17-99 PMPHr.
 Comment: Balanced Build Saturday 2000

	Northbound			Southbound			Eastbound			Westbound				
	L	T	R	L	T	R	L	T	R	L	T	R		
No. Lanes	0	2	< 0	0	2	< 0	0	> 1	< 0	1	1	< 0		
Volumes		1138	352		1069	3		13	1	13		270	3	59
PHF or PK15		0.95	0.95		0.95	0.95		0.95	0.95	0.95		0.95	0.95	0.95
Lane W (ft)		12.0			12.0			12.0				12.0	12.0	
Grade		0			0			0				0		
% Heavy Veh		2	2		2	2		2	2	2		2	2	2
Parking	N		N	N		N	N		N		N		N	
Bus Stops			0			0			0					0
Con. Peds			0			0			0					0
Ped Button	(Y/N)	N		(Y/N)	N		(Y/N)	N		(Y/N)	N		(Y/N)	N
Arr Type		3			3			3			3	3		
RTOR Vols			0			0			0					0
Lost Time		3.00	3.00		3.00	3.00		3.00	3.00	3.00		3.00	3.00	3.00
Prop. Share														
Prop. Prot.														

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
NB Left					EB Left	*		
Thru	*				Thru	*		
Right	*				Right	*		
Peds					Peds			
SB Left					WB Left	*		
Thru	*				Thru	*		
Right	*				Right	*		
Peds					Peds			
EB Right					NB Right			
WB Right					SB Right			
Green	45.0A				Green	25.0A	5.0A	
Yellow/AR	5.0				Yellow/AR	5.0	5.0	
Cycle Length: 90 secs Phase combination order: #1 #5 #6								

Intersection Performance Summary

Lane Group:	Adj Sat	v/c	g/C	Approach:			
Mvmts	Cap	Flow	Ratio	Delay	LOS	Delay	LOS
NB TR	1877	3593	0.878	0.522	15.9	C	15.9 C
SB TR	1945	3724	0.609	0.522	10.1	B	10.1 B
EB LTR	118	1518	0.246	0.078	25.4	D	25.4 D
WB L	531	1770	0.535	0.300	17.8	C	17.3 C
TR	479	1596	0.136	0.300	14.9	B	

Intersection Delay = 14.0 sec/veh Intersection LOS = B
 Lost Time/Cycle, L = 9.0 sec Critical v/c(x) = 0.709

Streets: (N-S) RIVERSIDE ST (E-W) WARREN AVE
 Analyst: JLM File Name: RWBSAT.HC9
 Area Type: Other 5-17-99 PM
 Comment: Build Saturday Peak Hour 2000

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	1	1	2	< 0	1	1	< 0	1	1	< 0
Volumes	26	587	587	160	526	90	91	333	48	498	278	158
PHF or PK15	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Lane W (ft)	12.0	12.0	12.0	12.0	12.0		12.0	12.0		12.0	12.0	
Grade			0			0			0			0
% Heavy Veh	2	2	2	5	5	5	2	2	2	1	1	1
Parking	N	N	N	N	N	N	N	N	N	N	N	N
Bus Stops			0			0			0			0
Con. Peds			0			0			0			0
Ped Button	(Y/N) N			(Y/N) N			(Y/N) N			(Y/N) N		
Arr Type	3	3	3	3	3		3	3		3	3	
RTOR Vols			0			0			0			0
Lost Time	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00
Prop. Share				0								
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
NB Left	*							
Thru		*						
Right			*					
Peds								
SB Left		*	*					
Thru			*					
Right				*				
Peds								
EB Right								*
WB Right								*
Green		6.0A	27.0A			25.0A	35.0A	
Yellow/AR		5.0	5.0			5.0	5.0	

Cycle Length: 113 secs Phase combination order: #1 #2 #5 #6

Intersection Performance Summary

Lane	Group:	Adj Sat	v/c	g/C	Delay	LOS	Approach:	Delay	LOS
Mvmts	Cap	Flow	Ratio	Ratio					
NB	L	125	1770	0.216	0.071	32.1	D	40.5	E
	T	956	3725	1.034	0.257	58.7	E		
	R	925	1583	0.668	0.584	11.7	B		
SB	L	186	1719	0.903	0.354	47.3	E	31.4	D
	TR	908	3540	0.750	0.257	27.4	D		
EB	L	423	1770	0.227	0.239	22.4	C	40.5	E
	TR	437	1827	0.921	0.239	44.8	E		
WB	L	585	1787	0.895	0.327	35.0	D	31.3	D
	TR	583	1779	0.788	0.327	27.2	D		

Intersection Delay = 36.3 sec/veh Intersection LOS = D
 Lost Time/Cycle, L = 12.0 sec Critical v/c(x) = 0.951

John L. Murphy P. E. Traffic Engineer

Streets: (N-S) RIVERSIDE ST. (E-W) EXIT 8
 Analyst: JLM File Name: EX88.HC9
 Area Type: Other 4-10-99 PM PK.
 Comment: Build Portland Commons Weekday 2000

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	1	1	2	< 0	1	2	< 0	1	2	1
Volumes	41	532	414	482	532	415	301	318	85	300	363	546
PHF or PK15	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Lane W (ft)	12.0	12.0	12.0	12.0	12.0		12.0	12.0		12.0	12.0	12.0
Grade		0			0			0			0	
% Heavy Veh	2	4	5	5	5	2	2	2	2	5	3	5
Parking	N	N		N	N		N	N		N	N	
Bus Stops			0			0			0			0
Con. Peds			0			0			0			0
Ped Button	(Y/N) N			(Y/N) N			(Y/N) N			(Y/N) N		
Arr Type	3	3	3	3	3		3	3		3	3	3
RTOR Vols			100			0			0			100
Lost Time	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00
Prop. Share				17			48			55		
Prop. Prot.												

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
NB Left	*				EB Left	*		
Thru			*		Thru		*	
Right			*		Right		*	
Peds					Peds			
SB Left	*	*			WB Left	*		
Thru		*	*		Thru		*	
Right		*	*		Right		*	
Peds					Peds			
EB Right					NB Right	*	*	
WB Right	*	*			SB Right			
Green	6.0A	26.0A	20.0A		Green	25.0A	18.0A	
Yellow/AR	5.0	5.0	5.0		Yellow/AR	5.0	5.0	

Cycle Length: 120 secs Phase combination order: #1 #2 #3 #5 #6

Intersection Performance Summary

Lane Group:	Adj Sat	v/c	g/C	Approach:				
Mvmts	Cap	Flow	Ratio	Delay	LOS	Delay	LOS	
NB L	118	1770	0.364	0.067	35.5	D	28.3	D
NB T	670	3654	0.878	0.183	39.8	D		
NB R	961	1538	0.344	0.625	7.0	B		
SB L	559	1719	0.907	0.325	38.3	D	24.9	C
SB TR	1508	3414	0.694	0.442	18.4	C		
EB L	398	1770	0.796	0.225	35.7	D	34.8	D
EB TR	601	3608	0.740	0.167	34.1	D		
WB L	387	1719	0.817	0.225	37.3	D	26.9	D
WB T	615	3689	0.652	0.167	31.9	D		
WB R	756	1538	0.622	0.492	15.6	C		

Intersection Delay = 27.9 sec/veh Intersection LOS = D

Lost Time/Cycle, L = 12.0 sec Critical v/c(x) = 0.848

John L. Murphy P. E. Traffic Engineer

Streets: (N-S) Riverside St. (E-W) Portland Commons

Analyst: JLM File Name: PCRIVB.HC9

Area Type: Other 5-17-99 PM

Comment: Build Portland Commons 2000

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	< 0	1	2	< 0	0	> 2	< 0	1	1	< 0
Volumes	36	1232	110	95	1261	11	8	1	48	113	1	102
PHF or PK15	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Lane W (ft)	12.0	12.0		12.0	12.0		12.0			12.0	12.0	
Grade		0			0			0			0	
% Heavy Veh	2	2	2	2	2	2	2	2	2	2	2	2
Parking	N	N		N	N		N	N		N	N	
Bus Stops			0			0			0			0
Con. Peds			0			0			0			0
Ped Button	(Y/N) N			(Y/N) N			(Y/N) N			(Y/N) N		
Arr Type	3	3		3	3			3		3	3	
RTOR Vols			0			0			0			0
Lost Time	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00
Prop. Share												
Prop. Prot.												

Signal Operations

Phase Combination 1	2	3	4	5	6	7	8
NB Left	*			EB Left	*		
Thru		*		Thru	*		
Right			*	Right	*		
Peds				Peds			
SB Left	*			WB Left	*		
Thru		*		Thru	*		
Right			*	Right	*		
Peds				Peds			
EB Right				NB Right			
WB Right				SB Right			
Green	6.0A	45.0A		Green	12.0A		
Yellow/AR	5.0	5.0		Yellow/AR	5.0		
Cycle Length: 78 secs Phase combination order: #1 #2 #5							

Intersection Performance Summary

Lane	Group:	Adj Sat	v/c	g/C	Approach:				
		Cap	Flow	Ratio	Ratio	Delay	LOS	Delay	LOS
NB	L	181	1770	0.209	0.103	20.8	C	7.6	B
	TR	2217	3680	0.669	0.603	7.2	B		
SB	L	181	1770	0.551	0.103	24.2	C	8.0	B
	TR	2242	3720	0.627	0.603	6.8	B		
EB	LTR	530	2952	0.119	0.179	17.3	C	17.3	C
WB	L	266	1485	0.447	0.179	19.3	C	19.0	C
	TR	285	1586	0.379	0.179	18.6	C		

Intersection Delay = 8.7 sec/veh Intersection LOS = B

Lost Time/Cycle, L = 9.0 sec Critical v/c(x) = 0.610

Streets: (N-S) Riverside St. (E-W) Home Depot
 Analyst: JLM File Name: HDRIVB.HC9
 Area Type: Other 5-17-99 PMPHr.
 Comment: Balanced Build Weekday 2000

	Northbound			Southbound			Eastbound			Westbound				
	L	T	R	L	T	R	L	T	R	L	T	R		
No. Lanes	0	2	< 0	0	2	< 0	0	> 1	< 0	1	1	< 0		
Volumes		1232	109		1223	11		12	1	15		129	1	22
PHF or PK15		0.95	0.95		0.95	0.95		0.95	0.95	0.95		0.95	0.95	0.95
Lane W (ft)		12.0			12.0			12.0				12.0	12.0	
Grade		0			0			0				0		
% Heavy Veh		2	2		2	2		2	2	2		2	2	2
Parking	N		N	N		N	N		N		N		N	
Bus Stops			0			0			0					0
Con. Peds			0			0			0					0
Ped Button	(Y/N)	N		(Y/N)	N		(Y/N)	N		(Y/N)	N		(Y/N)	N
Arr Type		3			3			3			3	3		
RTOR Vols			0			0			0					0
Lost Time		3.00	3.00		3.00	3.00		3.00	3.00	3.00		3.00	3.00	3.00
Prop. Share														
Prop. Prot.														

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
NB Left					EB Left	*		
Thru	*				Thru	*		
Right	*				Right	*		
Peds					Peds			
SB Left					WB Left	*		
Thru	*				Thru	*		
Right	*				Right	*		
Peds					Peds			
EB Right					NB Right			
WB Right					SB Right			
Green	45.0A				Green	25.0A	5.0A	
Yellow/AR	5.0				Yellow/AR	5.0	5.0	
Cycle Length:	90 secs Phase combination order: #1 #5 #6							

Intersection Performance Summary

Lane Group	Adj Sat	v/c	g/C	Approach:		
				Delay	LOS	
Mvmts	Cap	Flow	Ratio	Ratio	Delay	LOS
NB TR	1922	3680	0.772	0.522	12.5	B
SB TR	1943	3720	0.702	0.522	11.3	B
EB LTR	117	1510	0.256	0.078	25.5	D
WB L	531	1770	0.256	0.300	15.5	C
TR	478	1595	0.050	0.300	14.5	B

Intersection Delay = 12.2 sec/veh Intersection LOS = B
 Lost Time/Cycle, L = 9.0 sec Critical v/c(x) = 0.555

John L. Murphy P. E. Traffic Engineer

Streets: (N-S) RIVERSIDE ST (E-W) WARREN AVE
 Analyst: JLM File Name: RWARNB.HC9
 Area Type: Other 5-17-99 PM
 Comment: Build Weekday PM Peak Hour 2000

	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	1	1	2	< 0	1	1	< 0	1	1	< 0
Volumes	51	765	452	58	793	67	87	193	84	357	225	84
PHF or PK15	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Lane W (ft)	12.0	12.0	12.0	12.0	12.0		12.0	12.0		12.0	12.0	
Grade		0			0			0			0	
% Heavy Veh	5	5	4	4	5	5	4	5	5	5	5	4
Parking	N	N		N	N		N	N		N	N	
Bus Stops			0			0			0			0
Con. Peds			0			0			0			0
Ped Button	(Y/N) N			(Y/N) N			(Y/N) N			(Y/N) N		
Arr Type	3	3	3	3	3		3	3		3	3	
RTOR Vols			0			0			0			0
Lost Time	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00
Prop. Share				0								
Prop. Prot.												

Signal Operations

Phase Combination 1	2	3	4	5	6	7	8
NB Left	*	*		EB Left	*		
Thru		*		Thru	*		
Right		*		Right	*		
Peds				Peds			
SB Left	*	*		WB Left	*		
Thru		*		Thru	*		
Right		*		Right	*		
Peds				Peds			
EB Right				NB Right	*		
WB Right				SB Right			
Green	4.0A	38.0A		Green	22.0A	27.0A	
Yellow/AR	5.0	5.0		Yellow/AR	5.0	5.0	

Cycle Length: 111 secs Phase combination order: #1 #2 #5 #6

Intersection Performance Summary

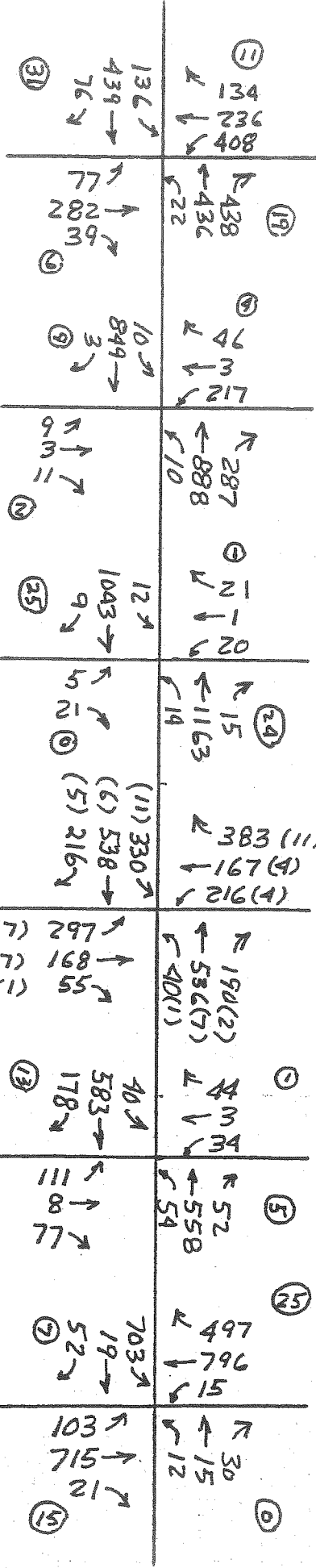
Lane Group	Adj Sat	v/c	g/C	Approach:				
				Delay	LOS			
Mvmts	Cap	Flow	Ratio	Ratio	Delay	LOS	Delay	LOS
NB L	158	1719	0.342	0.441	14.2	B	30.3	D
T	1304	3619	0.988	0.360	39.4	D		
R	965	1553	0.493	0.622	7.7	B		
SB L	160	1736	0.381	0.441	16.9	C	21.3	C
TR	1289	3577	0.738	0.360	21.6	C		
EB L	375	1736	0.245	0.216	23.3	C	31.0	D
TR	374	1727	0.779	0.216	33.4	D		
WB L	449	1719	0.837	0.261	34.1	D	31.2	D
TR	454	1736	0.717	0.261	27.8	D		

Intersection Delay = 28.2 sec/veh Intersection LOS = D

Lost Time/Cycle, L = 12.0 sec Critical v/c(x) = 0.873



RIVERSIDE



WARREN
11:15-12:15
4/10/99 (SAT)

HOME DEPOT
12:15-1:15
4/10/99 (SAT)

PROJECT
12:15-1:15
4/10/99 (SAT)

EXIT 8

VIP/TEXACO

BRIGHTON
11:30-12:30
4/10/99 (SAT)

WICKES

BOUNTY

LARRABEE
12:15-1:15
4/10/99
(SAT)

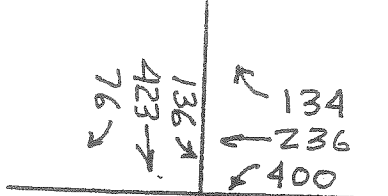
BRADLEYS
12:15-1:15
4/10/99
(SAT)

SATURDAY COUNTS
4/10/99
MID-DAY

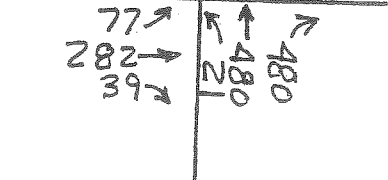
PENNYS



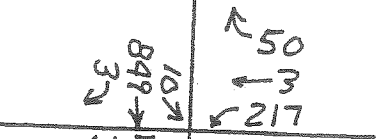
RIVERSIDE



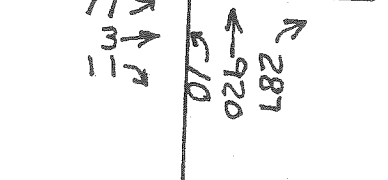
WARREN



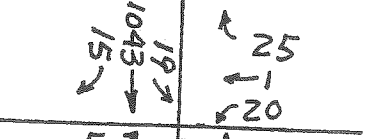
WICKES



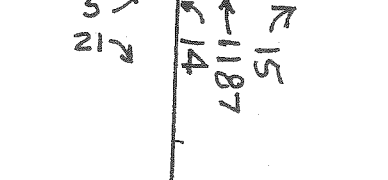
HOME DEPOT



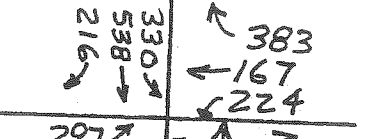
BOUNTY



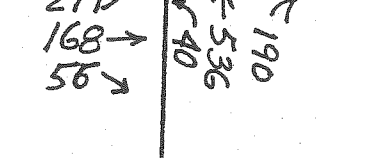
PROJECT



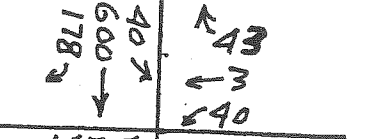
LARRABEE



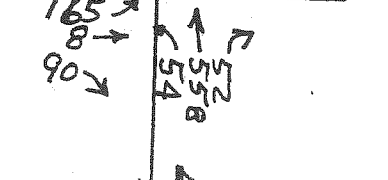
EXIT 8



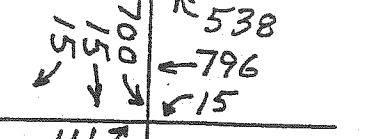
BRADLEYS



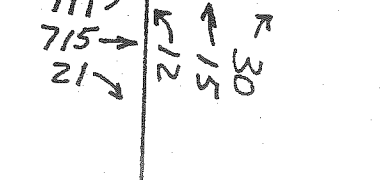
VIP/TEXACO



BRIGHTON



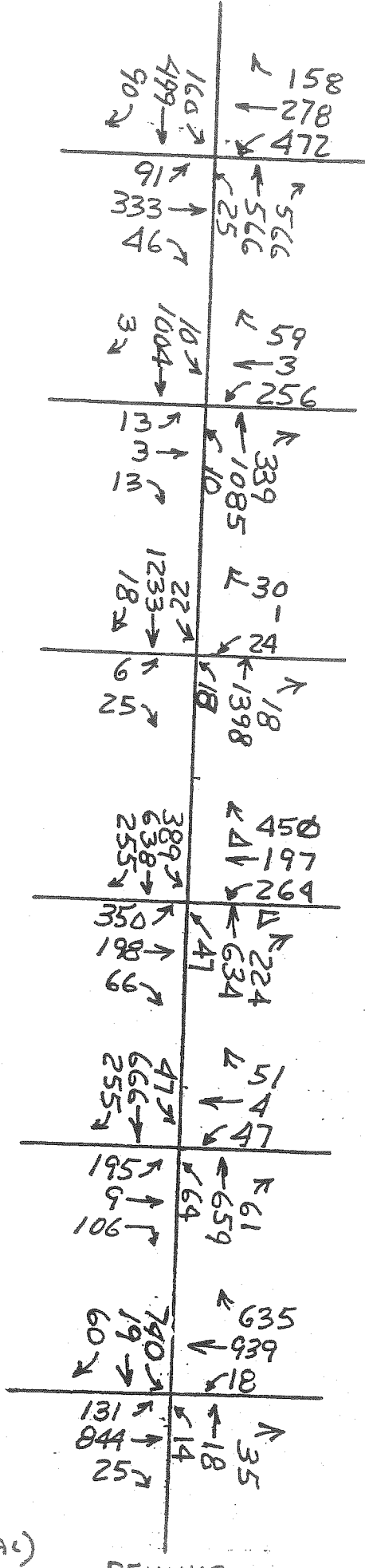
BALANCED No Build
SATURDAY 4/10/99



DENNYS



RIVERSIDE



WARREN

HOME DEPOT

PROJECT

EXIT 8

VIP/TEXACO

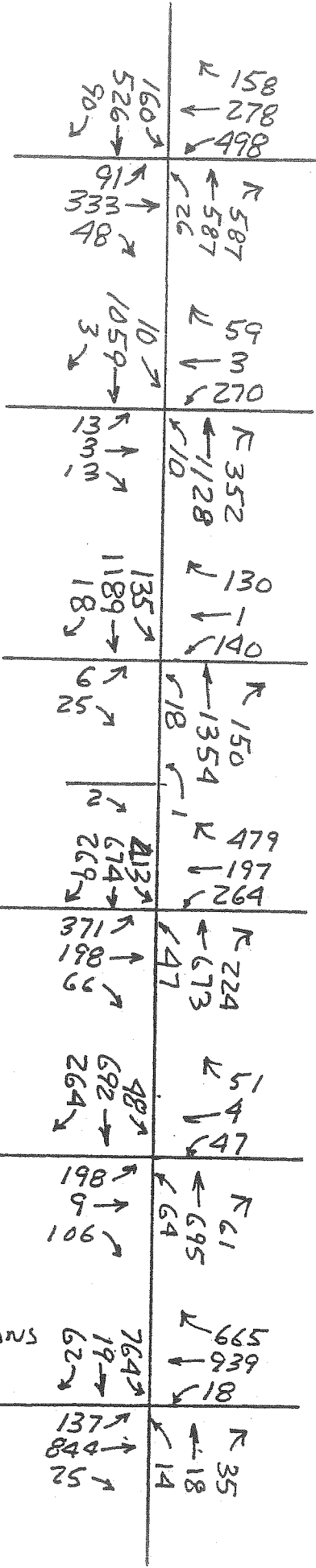
BRIGHTON

BALANCED No Build
 Factor SAT 4/10/99
 BY 1.18 for
 (1YR GROWTH + SEASONAL)

DENNYS



RIVERSIDE



WARREN

HOME DEPOT

PROJECT

EXIT B

VIP/TEXACO

BRIGHTON

DENNY'S

WICKES

BOUNTY

LARRABEE

BRADLEYS

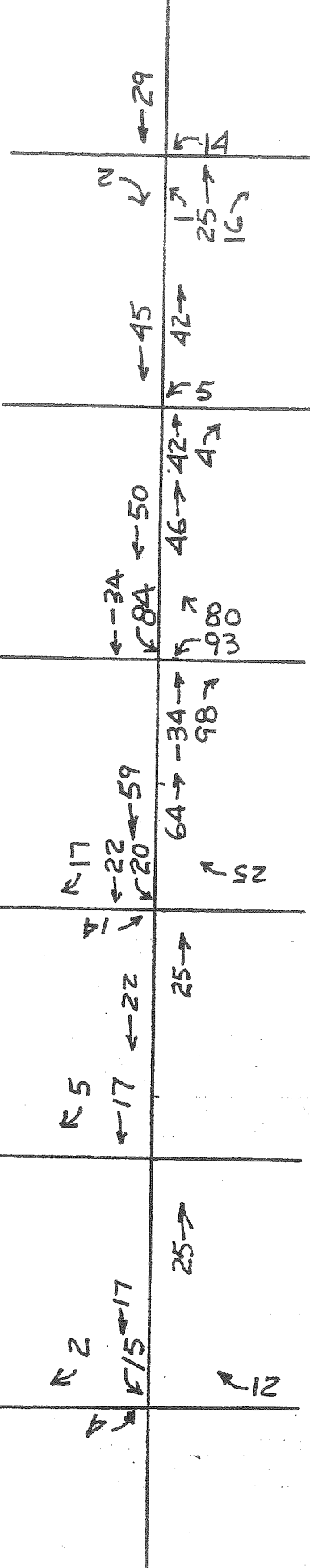
BUILD PORTLAND Commons

SATURDAY 2000

MID-DAY DHV



RIVERSIDE



WARREN

WICKES

HOME DEPOT

BOUNTY

PROJECT

LARRABEE

EXIT 8

GRADLEYS

VIP/TEXACO

BRIGHTON

DENNY'S

PROJECT IMPACT
WEEKDAY P.M. PEAK HOUR

TRIPS
XX = NEW + DIVERTED

LARRABEE RD.



RIVERSIDE ST

DENNY'S H_v=2%
H_v=5%
H_v=3%
H_v=5%

SHOP N SAJE

MARKS

WICKES

WARREN AVE.

HOME DEPOT

PORTLAND COMMONS PROJECT

EXIT 8

TEXACO

BRIGHTON AVE.

THURS 7/31/97 (PACTS STUDY)

TUE 7/22/97 Phf=.99

WED 1/28/98 Phf=.98

TUE 7/29/97 (PACTS STUDY)

TUE 2/16/99 Phf=.97

WED 7/17/99 Phf=.95

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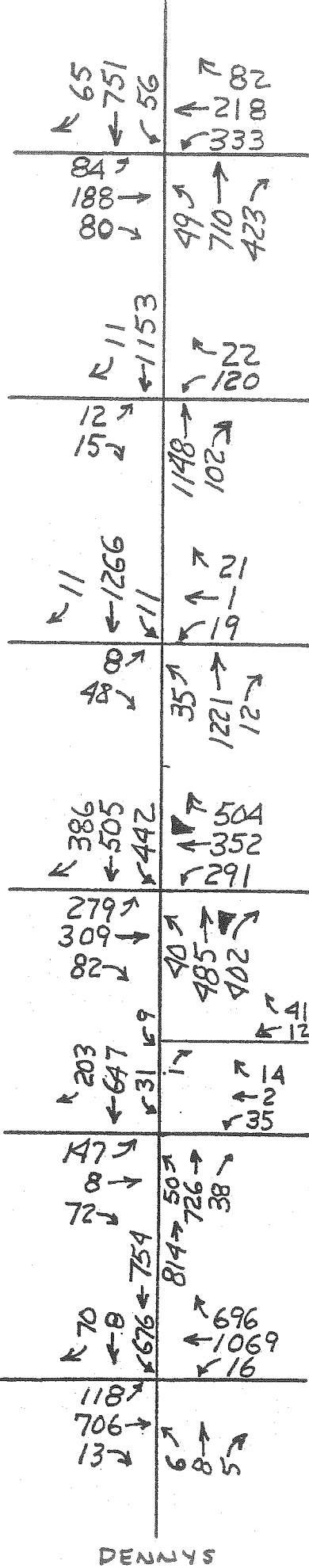
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RIVERSIDE



WARREN

WICKES

HOME DEPOT

BOUNTY

PROJECT

LARRABEE

EXIT 8

BRADLEYS

VIP/TEXACO

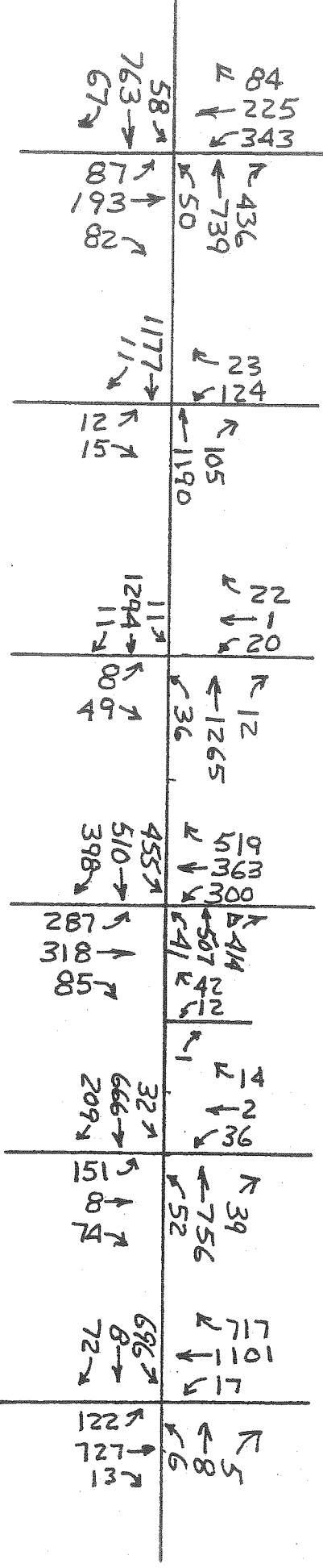
BRIGHTON

DENNY'S

BALANCED. NO BUILD
WEEKDAY BASE
JULY 1997



RIVERSIDE



WARREN

HOME DEPOT

PROJECT

EXIT B

VIP/TEXACO

BRIGHTON

BALANCED NO BUILD

WEEKDAY PM BASE

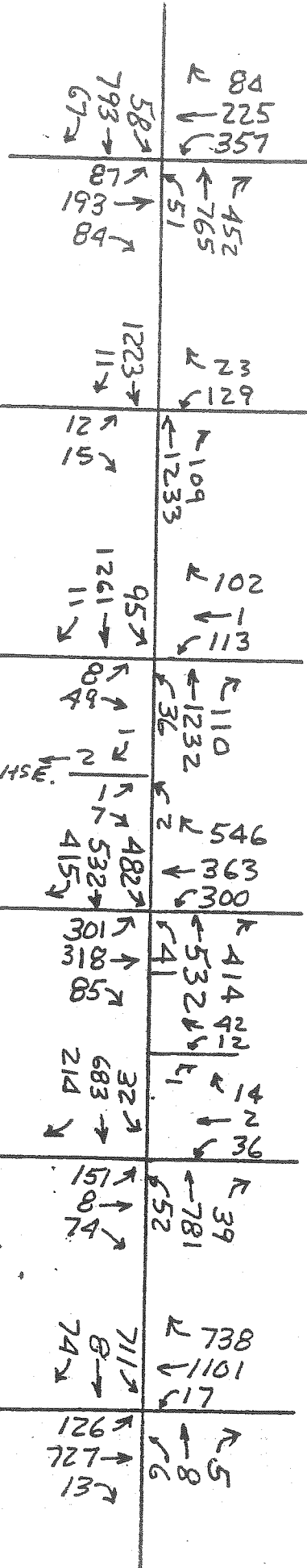
July 2000

(July 1997 x 1.03)

DENNYS



RIVERSIDE



WARREN

HOME DEPOT

PROJECT

EXIT 8

VIP/TEXACO

BRIGHTON

WICKES

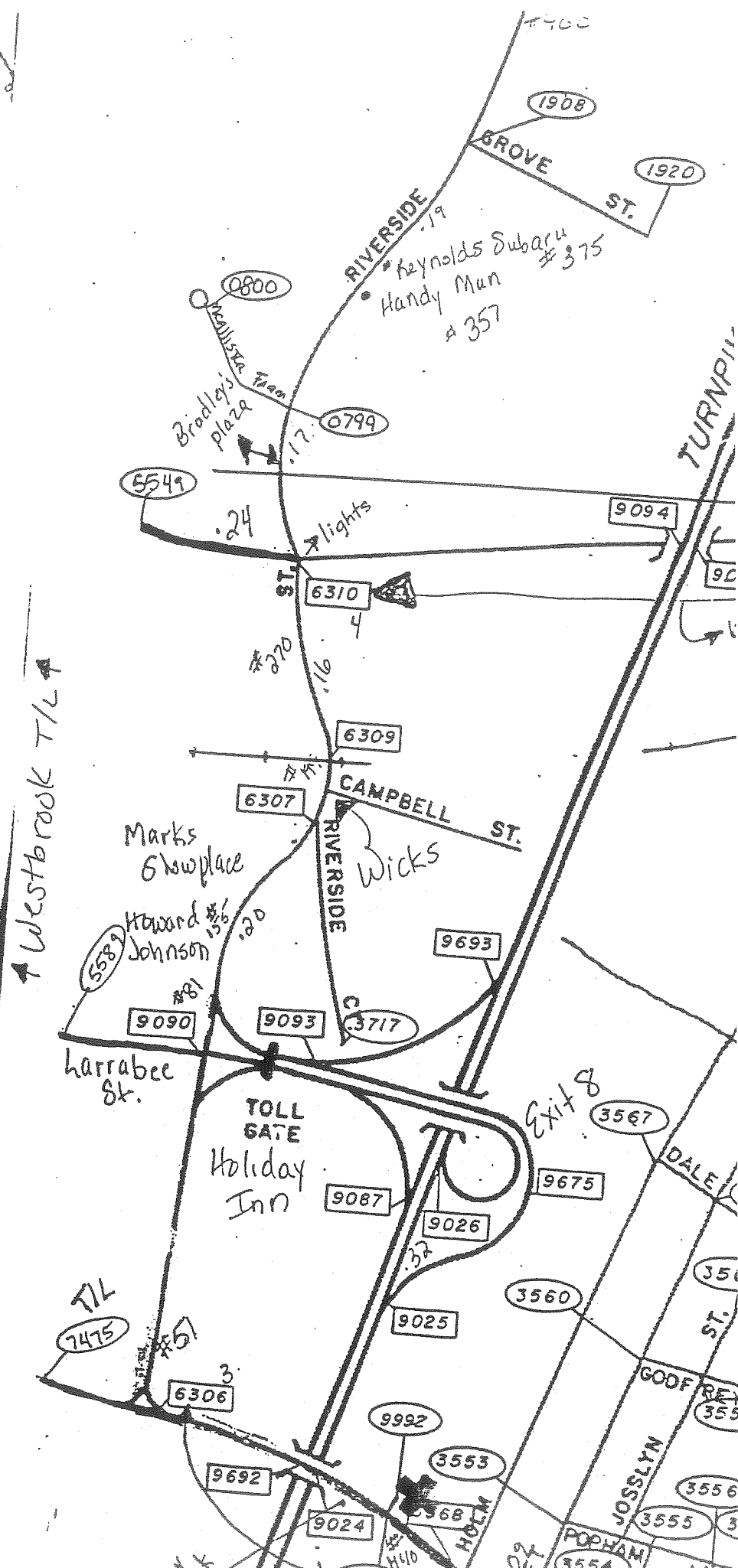
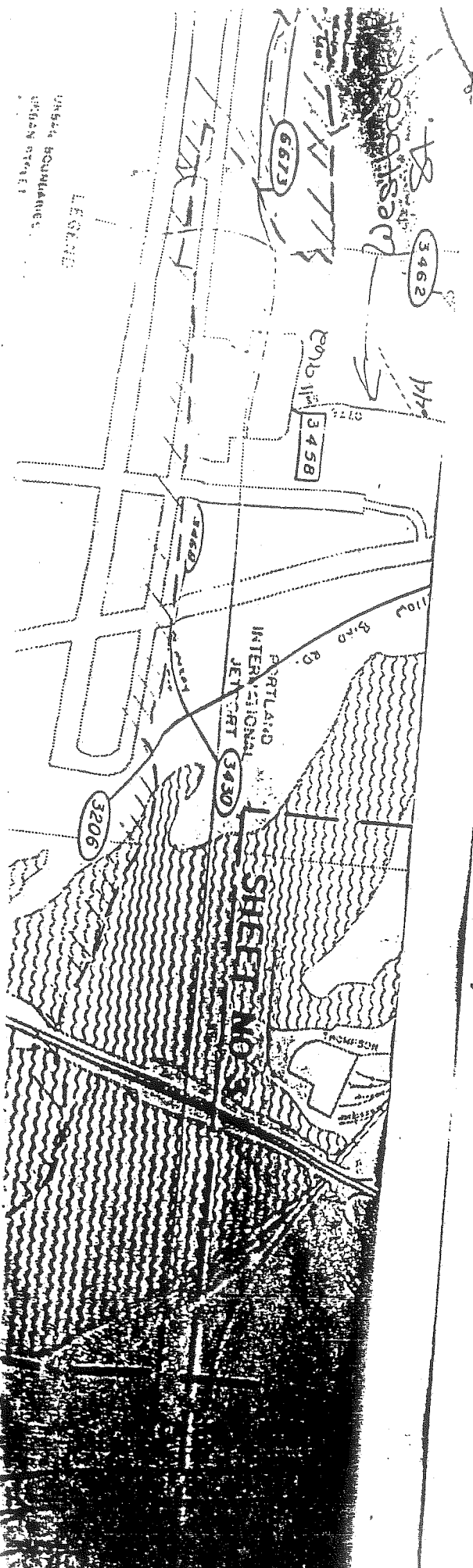
BOUNTY

LARRABEE

BRADLEYS

BUILD PORTLAND COMMONS
2000 WEEKDAY D.M.V.

DENNYS



BRADLEE'S
Shopping
Center

PORTLAND
1993-1995
LINK - 6306-9090
of Acc. - 21



MOBILE GAS

7090

RIVERSIDE ST.

0554 11/19/93 10:30 PM die
road conditions

01 11/19/93 12:20 PM die fail to yield
00 11/19/93 2:30 PM die improper pass
03 11/19/93 4:00 PM die fail to yield
22 11/19/93 5:40 PM die fail to yield

10757 2/24/94 1:30 PM die improper pass

17407 11/23/93 3:05 PM die
fail to yield

1197 11/20/93 die
with 0 Pa

15007 5/19/95 10:20 PM die due to inattention

11544 3/24/94 3:15 PM die fail to yield

11944 2/14/95 3:51 PM die fail to yield

15404 5/11/95 4:00 PM die fail to yield

18805 10/12/93 5:08 PM die fail to yield

20115 7/10/93 11:25 AM die inattention

1199 11/25/93 die fail to yield

1199 3:39 PM die after ha else

20127 2/5/95 4:37 PM die unsafe speed

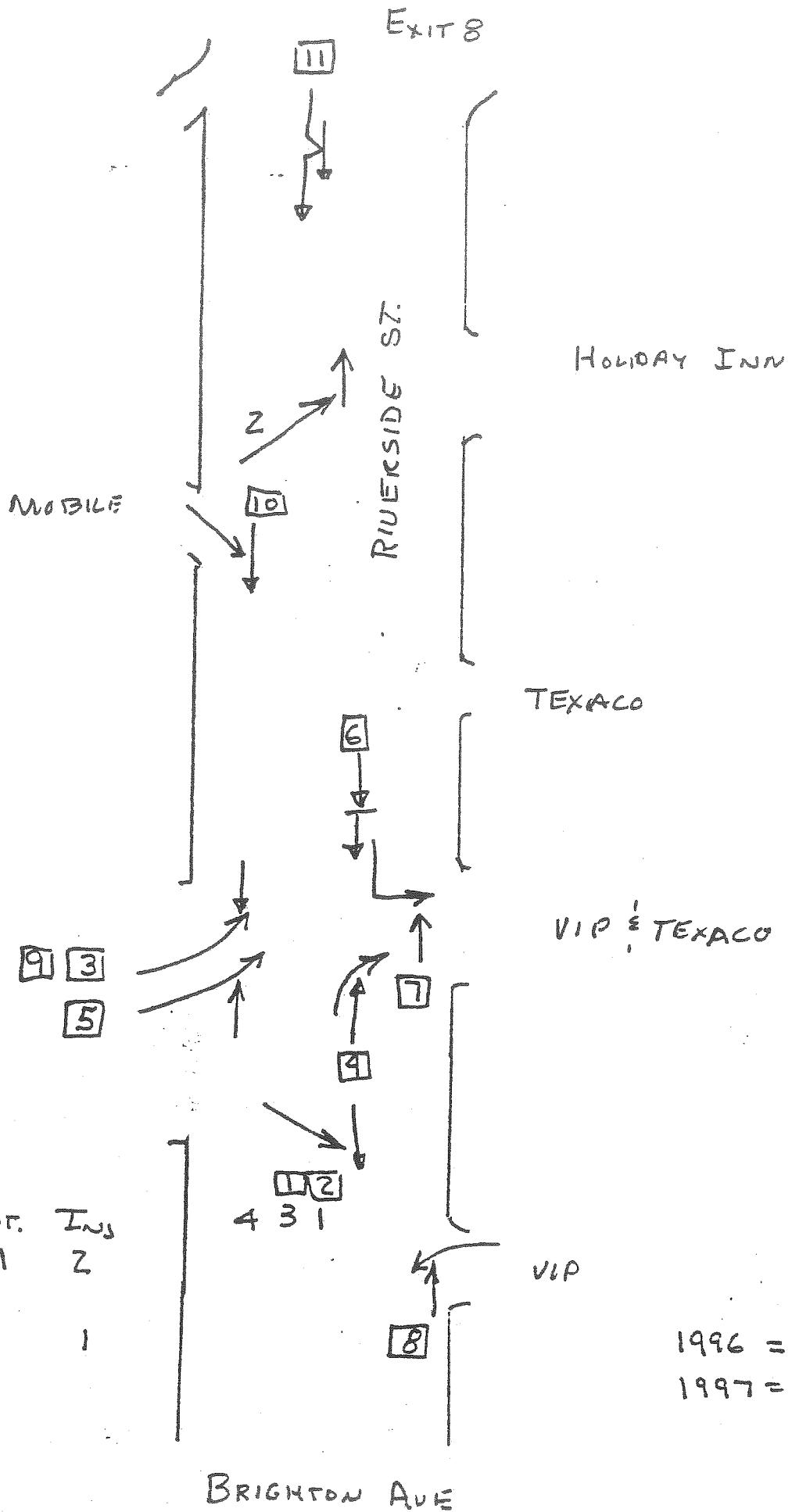
019728 2/11/95 12:05 PM die unsafe speed

MORRIS B

TERAZO

HOLIDAY INN

EXIT 8



	TOT.	INJ.
1996	11	2
1997	4	1

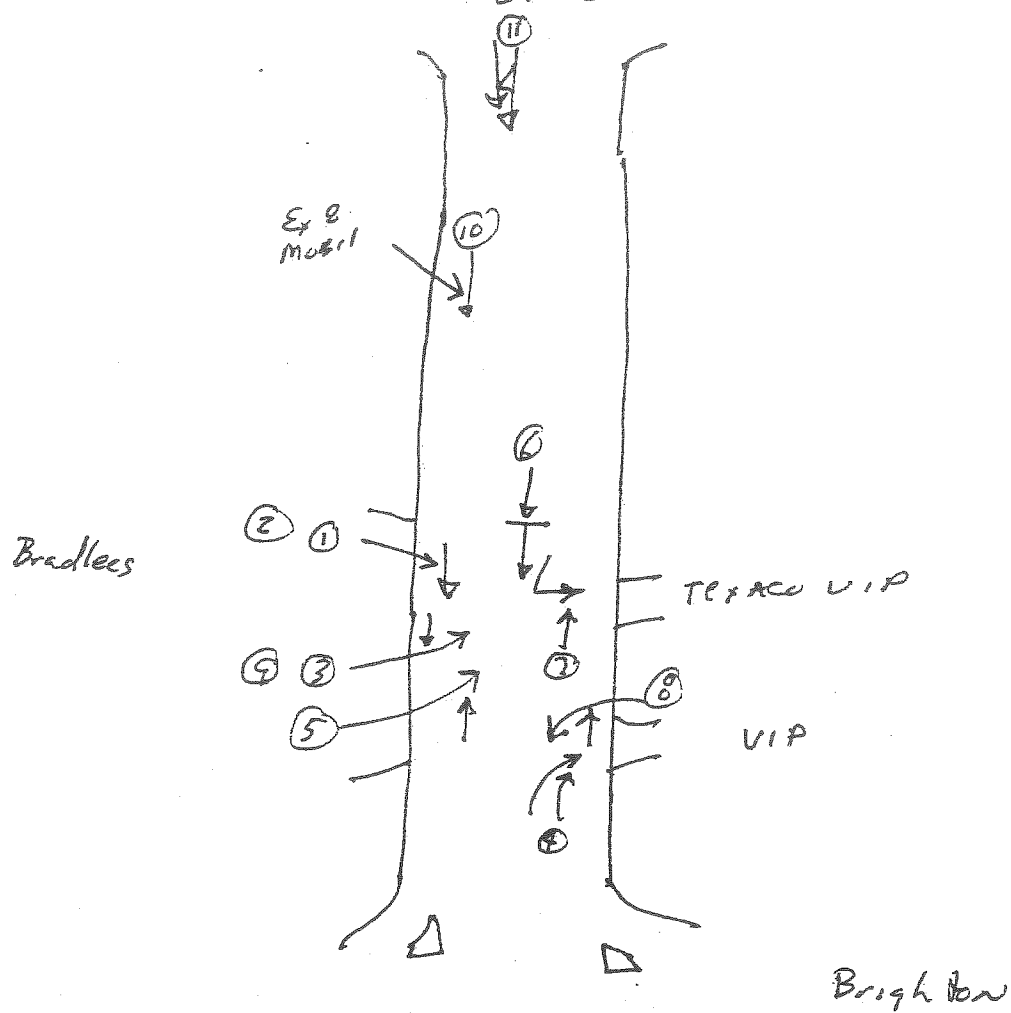
4 3 1

1996 = X
 1997 = X

BRIGHTON AVE

B/R
6306

Exit 9
9090
Exit 2



- 96.00082 ① 11/2/96 Tue 2315 (vision blocked snow bank)
- 96.13139 ② 2/4/96 THU ~~1030~~ ¹⁰³⁰
- 96.05407 ③ 2/6/96 Tue 1900 ③
- 96.11139 ④ 3/12/96 Tue 0837
- 96.18393 ⑤ 5/29/96 Fri 1623
- 96.17996 ⑥ 5/17/96 Fri 1300
- 96.19378 ⑦ 6/3/96 Mon 1120 - vision blocked by RV (Trap Block)
- 96.29137 ⑧ 7/13/96 SAT 1920 ③
- 96.04904 ⑨ 1/9/96 Tue 1700 icy
- 96.16910 ⑩ 5/11/96 Sat 1319
- 96.19310 ⑪ 6/3/96 Mon 1540 lane a

1997 only
6306 - 9090
R/R Exit 8

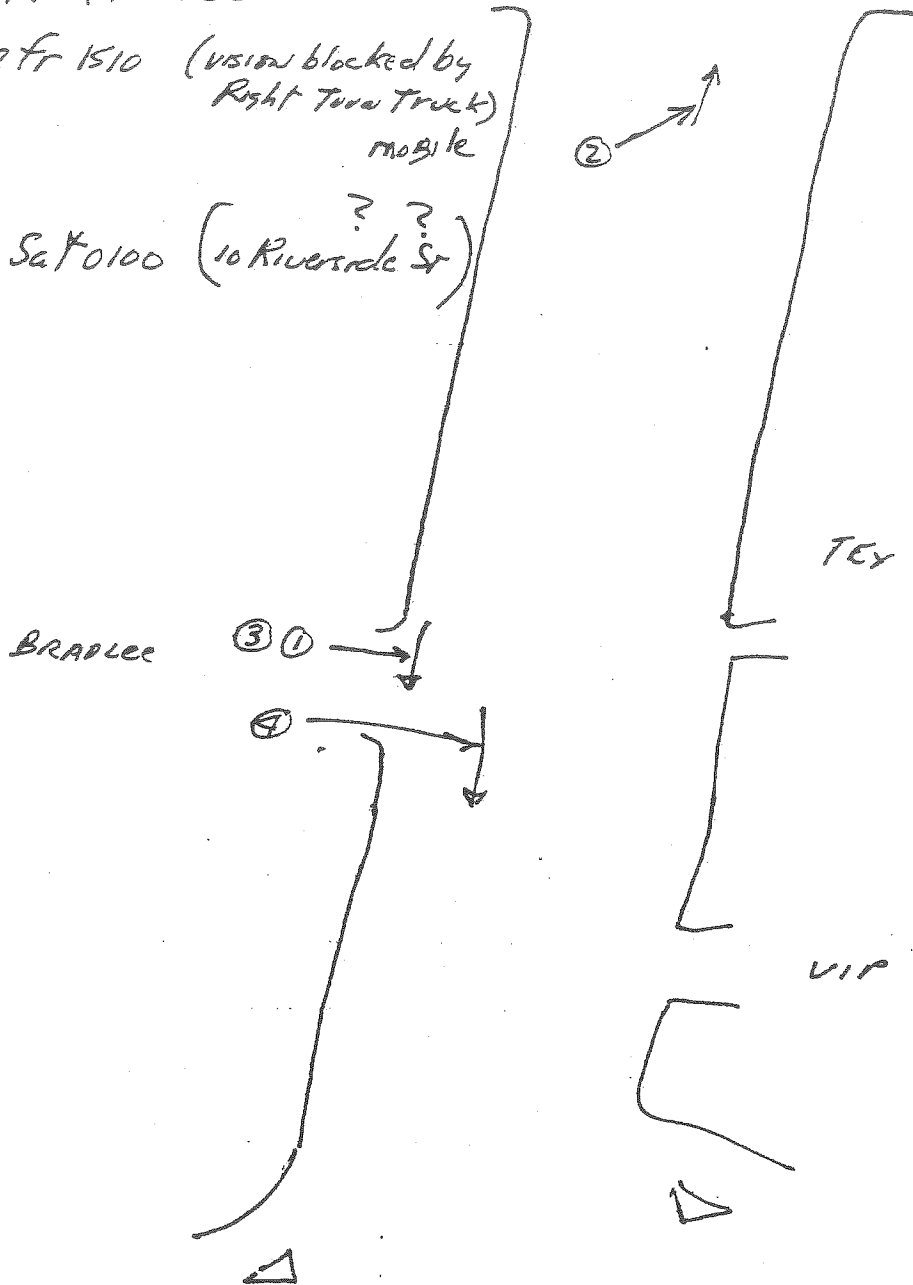
97.29562 ① 9/21/97 Th 1720 ②

97.49456 ② 12/6/97 Sat 1720

97.34169 ③ 10/3/97 Fr 1510 (vision blocked by
Right Turn Truck)
mobile

97.08302 ④ 3/1/97 Sat 0100 (10 Riverside St)
? ?

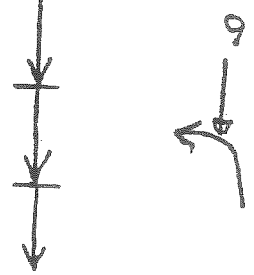
Exit 8



Brig hite

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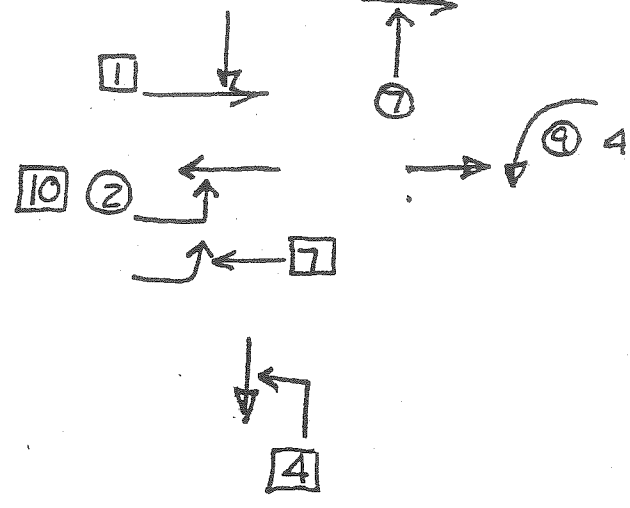
119
5 6 8 12



OLD PKG LOT
(REMOVED
1998)

EXXON
DRIVE
11

LARRABEE
RD

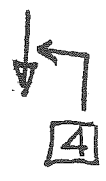
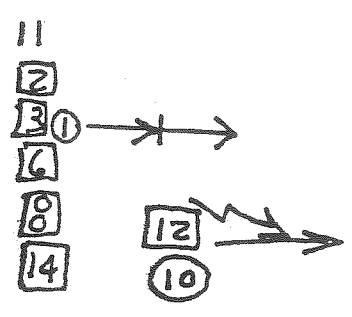


13

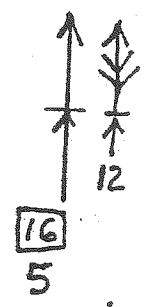
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EXIT
8



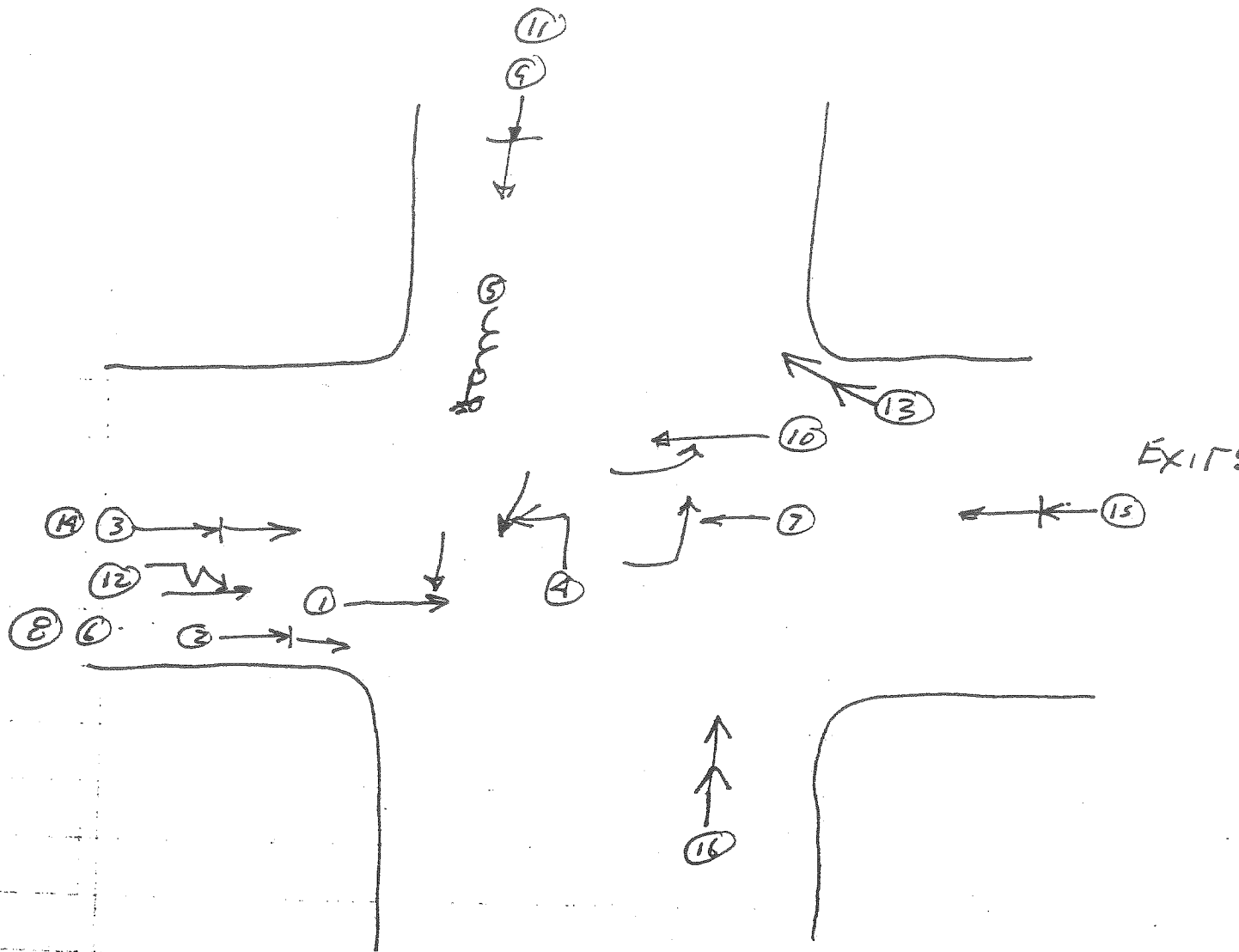
	TOTAL	INS
1995 - 13	-	7
1996 - 16	-	4
1997 - 12	-	0
	<u>41</u>	<u>11</u>



⊗ = 1995
 ⊠ = 1996
 X = 1997

RIVERSIDE ST

9891 Exit 8 1996



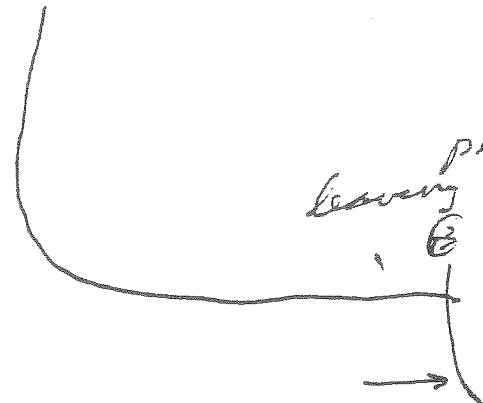
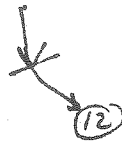
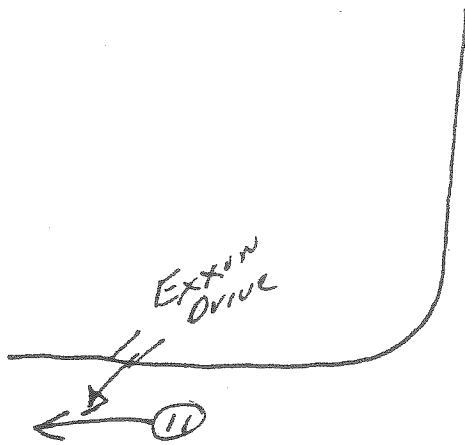
- 96.36939 ① 10/30/96 Wed 1905 ✓
- 96.39296 ② 11/16/96 Sat 1349 ✓
- 96.37299 ③ 11/1/96 - Fri 1055 ✓
- 96.22973 ④ 8/6/96 - Tue 1697 © ✓
- 96.24967 ⑤ 8/2/96 - F 1330 © ✓ Brake on MC Locked
- 96.27652 ⑥ 8/13/96 Tue 0955 © ✓
- 96.18945 ⑦ 5/29/96 Wed 1155 © ✓
- 96.23947 ⑧ 7/12/96 Fri 1336 ✓
- 96.09005 ⑨ 3/2/96 Sat 1130 ✓
- 96.29307 ⑩ 8/27/96 Tue 1833 ✓

- 96.30631 ⑪ 9/9/96 M 1345 ✓
- 96.95757 ⑫ 12/25/96 W 1825 ✓
- 96.33509 ⑬ 10/3/96 Th 1205 ✓
- 96.25277 ⑭ 7/23/96 Tu 1431 ✓
- 96.02559 ⑮ 1/15/96 Mo 1747 ✓
- 96.01262 ⑯ 1/9/96 Tue 1447 ✓

9090 Ex. 78

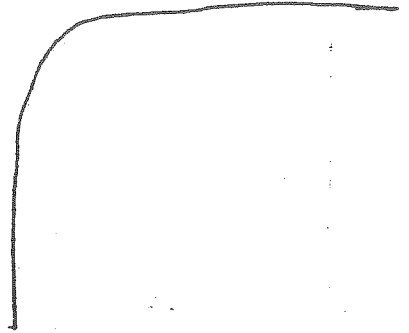
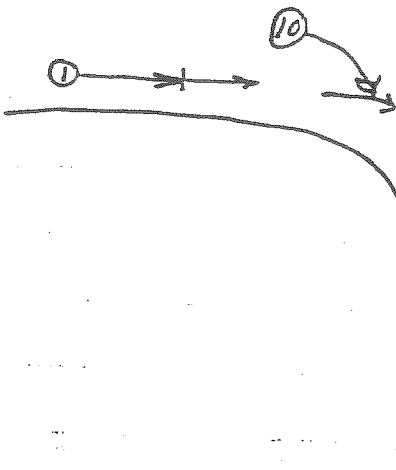
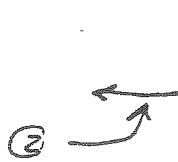


⑧
⑥ Riverside



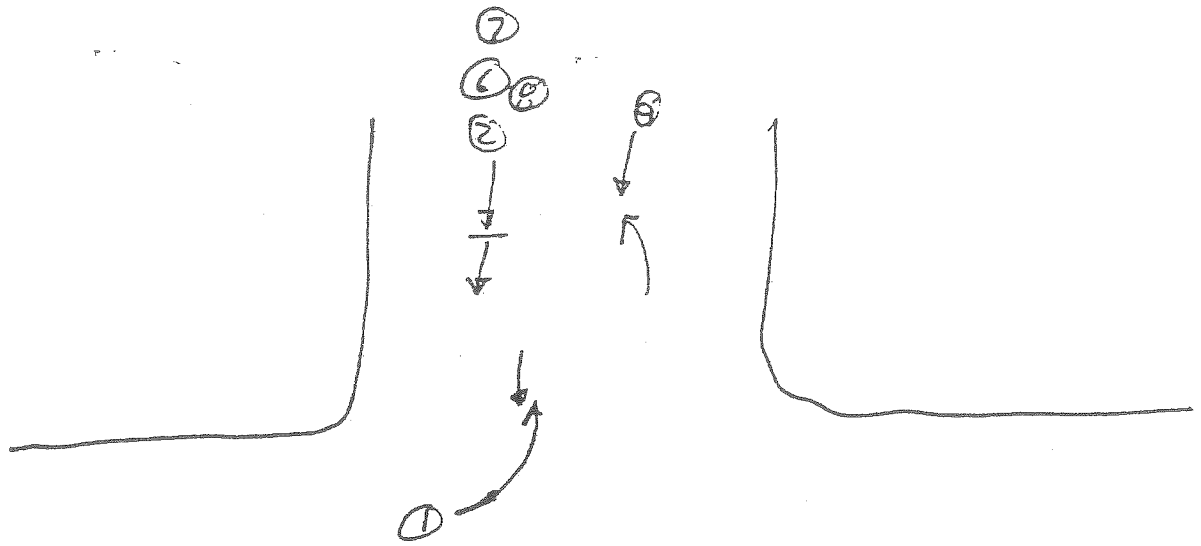
leaving p...

LARABEE

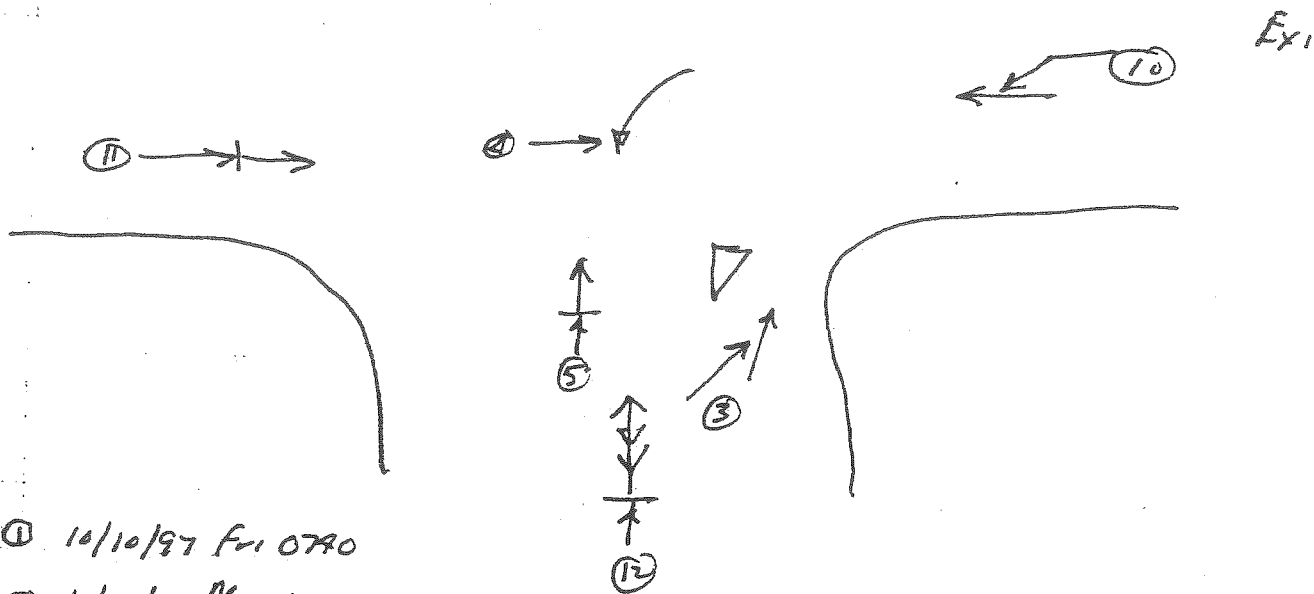


- 95.26003 ① 8/1/95 Tue 0747 ✓
- 95.26731 ② 8/25/95 Fri 1743 ③ ✓
- 95.43817 ③ 12/26/95 Tue 1630 ② (Truck) ✓
- 95.29453 ④ 9/19/95 Tue 0745 (2 Trucks Mirrors hit) ✓
- 95.16929 ⑤ 6/1/95 Thurs 1705 ⑥ -
- 95.32887 ⑥ 10/18/95 Wed 1835 ⑦ -
- 95.32529 ⑦ 10/15/95 Wed 0934 ⑧
- 95.08048 ⑧ 3/7/95 Tu 1450 (wet) -
- 95.08969 ⑨ 3/8/95 Wed 1245 ⑩
- 95.13918 ⑩ 5/2/95 Tue 0550 ⑪
- 95.01344 ⑪ 1/6/95 Fri 1450 (miss filed)
- 95.04474 ⑫ 1/31/95 Tu 1741 ⑬
- 95.05838 ⑬ 2/9/95 Th 0840 leaving Pk Lot (Same Nov)

1997 Exit 8



LARABEE



- 97.35495 ① 10/10/97 Fri 0740
- 97.36690 ② 10/20/97 Mon 1200
- 97.34565 ③ 10/3/97 Fri 1545
- 97.32627 ④ 9/17/97 W 1700
- 97.21715 ⑤ 6/21/97 Sat 1540
- 97.25085 ⑥ 7/17/97 Thu 0930
- 97.25393 ⑦ 7/19/97 Sat 2030
- 97.26494 ⑧ 7/22/97 Tue 1510
- 97.20458 ⑨ 6/11/97 Wed 1746 (??) Driveway Acc.
- 97.15208 ⑩ 8/22/97 Tue 1614
- 97.39879 ⑪ 11/11/97 Tue 1525
- 97.01013 ⑫ 1/17/97 Tue 0835 Backup.

LOCATION THUNDER - WHITE TUXEDO

TOWN Portland

NODE NO(S) _____ SYSTEM _____

URBAN _____ RURAL _____ R/U

YEARS REVIEWED 93~95 PREPARED BY DAF

DATE PREPARED 7/21/97

NODE 6307-6309 [RR TRACKS (covered now)]

CRITICAL RATE FACTOR _____

EQUIV. PROP. DAMAGE ACC/YEAR _____

ACC/MEV _____

LIGHT CONDITIONS

- 1 DARK
- 2 DAYLIGHT
- 3 DUSK
- 4 DARK (STREET LIGHTS ON)
- 5 DARK (NO STREET LIGHTS)
- 6 DARK (STREET LIGHTS OFF)
- 7 OTHER

ROAD CONDITIONS

- 1 DRY
- 2 WET
- 3 SNOW, SLUSH - SANDED
- 4 ICE, PACKED SNOW - SANDED
- 5 SLUSHY
- 6 GREASE
- 7 OILY
- 8 SNOW, SLUSH - NOT SANDED
- 9 ICE, PACKED SNOW - NOT SANDED
- 10 OTHER

APPARENT PHYSICAL CONDITION

- 1 NORMAL
- 2 UNDER THE INFLUENCE
- 3 HAD BEEN DRIVING
- 4 HAD BEEN USING DRUGS
- 5 ASLEEP
- 6 FATIGUED
- 7 ALL
- 8 HANDICAPPED
- 9 OTHER

APPARENT CONTRIBUTING FACTORS

- 1 NO IMPROPER ACTION
- 2 FAIL TO YIELD RIGHT OF WAY
- 3 ILLEGAL, UNSAFE SPEED
- 4 FOLLOW TOO CLOSE
- 5 MISREGARD TRAFFIC CONTROL DEVICE
- 6 DRIVING LEFT OF CENTER - NO PASSING
- 7 IMPROPER PASS - OVERTAKING
- 8 IMPROPER, UNSAFE LANE CHANGE
- 9 IMPROPER PASSING, START, STOP
- 10 IMPROPER TURN
- 11 UNSAFE BACKING
- 12 NO SIGNAL OR IMPROPER SIGNAL
- 13 IMPENDING TRAFFIC
- 14 DRIVER DISTRACTION - DISTRACTION
- 15 DRIVER INEXPERIENCE
- 16 PEDESTRIAN VIOLATION ERROR
- 17 PHYSICAL IMPAIRMENT
- 18 VISION OBSCURED - WINDSHIELD GLASS
- 19 VISION OBSCURED - SUN, HEADLIGHTS
- 20 OTHER VISION OBSCUREMENT
- 21 OTHER HUMAN VIOLATION FACTOR
- 22 HIT AND RUN
- 23 DEFECTIVE BRAKES
- 24 DEFECTIVE TIRE - TIRE FAILURE
- 25 DEFECTIVE LIGHTS
- 26 DEFECTIVE SUSPENSION
- 27 DEFECTIVE STEERING
- 28 OTHER VEHICLE DEFECT OR FACTOR
- 29 UNKNOWN

INJURY TYPE

- 1 FATAL
- 2 SERIOUS
- 3 NON-SERIOUS
- 4 POSSIBLE INJURY
- 5 NO INJURY



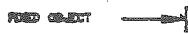
PEDESTRIAN P

FATAL ACCIDENT



REAR END

SNARLY ACCIDENT



SIDE SWIPE

VEHICLE(S)



TURNING MOVE

BICYCLE



CHANGE LANE

AIRIAL



OUT OF CONTROL

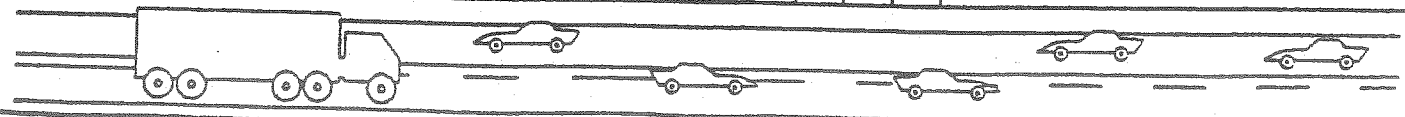
SLID



ITEM #	REPORT NUMBER	DATE	TIME	INJURES #28				LIGHT COND. #29	ROAD COND. #30	CONTRIBUTING FACTOR				PHYSICAL CONDITION		OTHER		
				1	2	3	4			#15	#16	#17	#18	#19	#20			
1		9/2/93	1715	5	5			2	1	1	4			1	1	11	1	
2		2/22/93	1253	5	3			2	9	1	1			1	1	1	60	
3		1/20/93	1931	4	5	5		4	1	1	2			1	2	1	6 left to Verillo's ?	
4		12/27/93	1057	5	5	5/5	5/5	2	1	8	1			1	1			
5		12/5/94	1200	5	5	5		2	2	1	20			1	1	1	17	
6		11/22/94	1115	5	5			2	1	2	1			1	1	6	1	
7		12/3/94	0100	4				5	1	4	1	5	1	1	1		—	hit 6R
8		10/10/94	1102	5	5	5	5	2	1	1	7	1	1	1	1	5	18	
9		3/15/95	0100	5				4	1	30				2		99	—	blew a tire
10		4/13/95	1718	5	5			2	2	10	1	20	1	1	1	6	1	

STATION KIVET DICK TOWN _____
 CODE NO(S) _____ SYSTEM _____ URBAN _____ RURAL _____ R/U _____
 YEARS REVIEWED _____ PREPARED BY _____ DATE PREPARED _____

ITEM #	REPORT NUMBER	DATE	TIME	INJURIES #25				LIGHT COND. #5	ROAD COND. #8	CONTRIBUTING FACTOR				PHYSICAL CONDITION		OTHER		
				1	2	3	4			#5	#6	#7	#8	#9	#20			
11		8/24/95	0555	5	3			2	2	1	1			1	1	1	54	hit - run, hit ped
12		12/10/95	1541	5	5	5	5	4	4	10	1	14		1	1	6	1	
13		5/3/95	1453	5	5	5		2	1	14	1			1	1	1	10	
14	97-36278	10/17/97	FRI 1500															ent to Accident Cleaners
15	97-1766	5/6/97	FRI 1225															
16	97-6800	3/1/97	SAT 1230															
17	97-05981	2/3/97	MON 1711															turning to #195 Riverside
18		10/17/96	THU 1545	C														
19		11/22/96	FRI 1545															
20		11/26/96	TUE 1842	C														SLUSH on road
21		7/31/96	WED 1255	B														
22		3/2/96	SAT 1530															ICY road

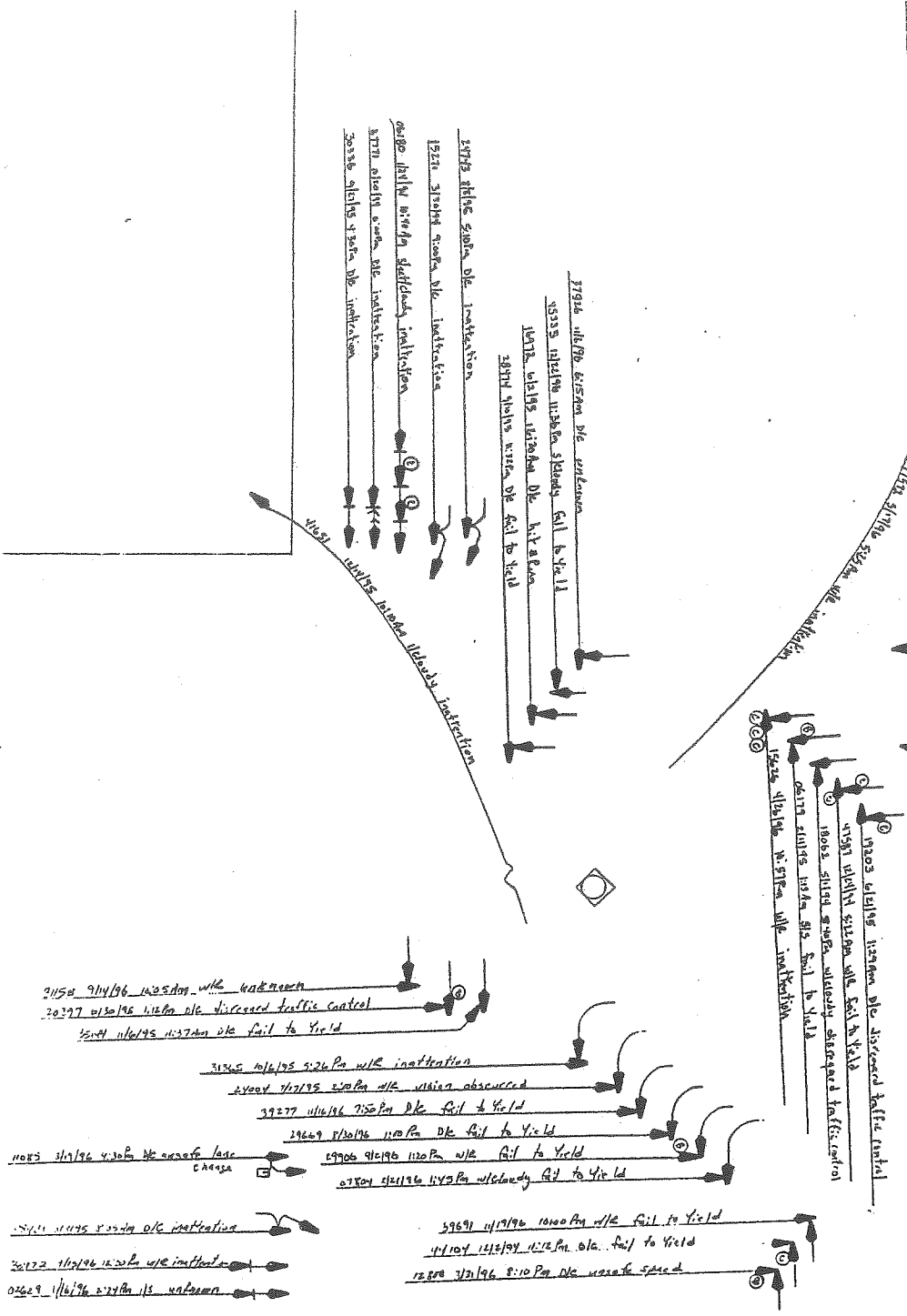


Portland
 1994, 1996
 Node # - 6310
 # of Acc - 43
 Traffic signal

RIVERSIDE ST.

- 27015 7/20/94 2:35 PM w/o indication
- 21160 8/16/94 1:55 PM w/o indication
- 98355 10/11/95 7:50 PM w/o indication
- 90065 11/11/95 3:20 PM w/o indication
- 05557 2/17/96 7:24 PM w/o indication
- 25346 7/18/96 2:00 PM No usable lane change
- 41118 5/17/95 10:10 AM w/o indication

- 15235 4/12/96 11:57 AM w/o indication
- 04113 2/11/96 11:40 AM w/o indication
- 11003 5/11/96 11:57 AM w/o indication
- 41591 11/11/95 11:57 AM w/o indication
- 11003 5/11/96 11:57 AM w/o indication



- 21154 9/14/96 1:35 PM w/o indication
- 20377 11/30/96 1:15 PM w/o indication
- 15011 11/12/95 11:57 AM w/o indication
- 31345 10/16/95 5:26 PM w/o indication
- 27007 1/17/95 2:50 PM w/o indication
- 39277 11/11/96 7:50 PM w/o indication
- 29669 8/30/96 1:10 PM w/o indication
- 11085 3/17/96 4:30 PM w/o indication
- 09906 9/10/96 11:20 PM w/o indication
- 07804 5/11/96 11:57 PM w/o indication
- 15111 11/17/95 5:30 PM w/o indication
- 20172 1/17/96 12:20 PM w/o indication
- 02629 1/16/96 2:27 PM w/o indication

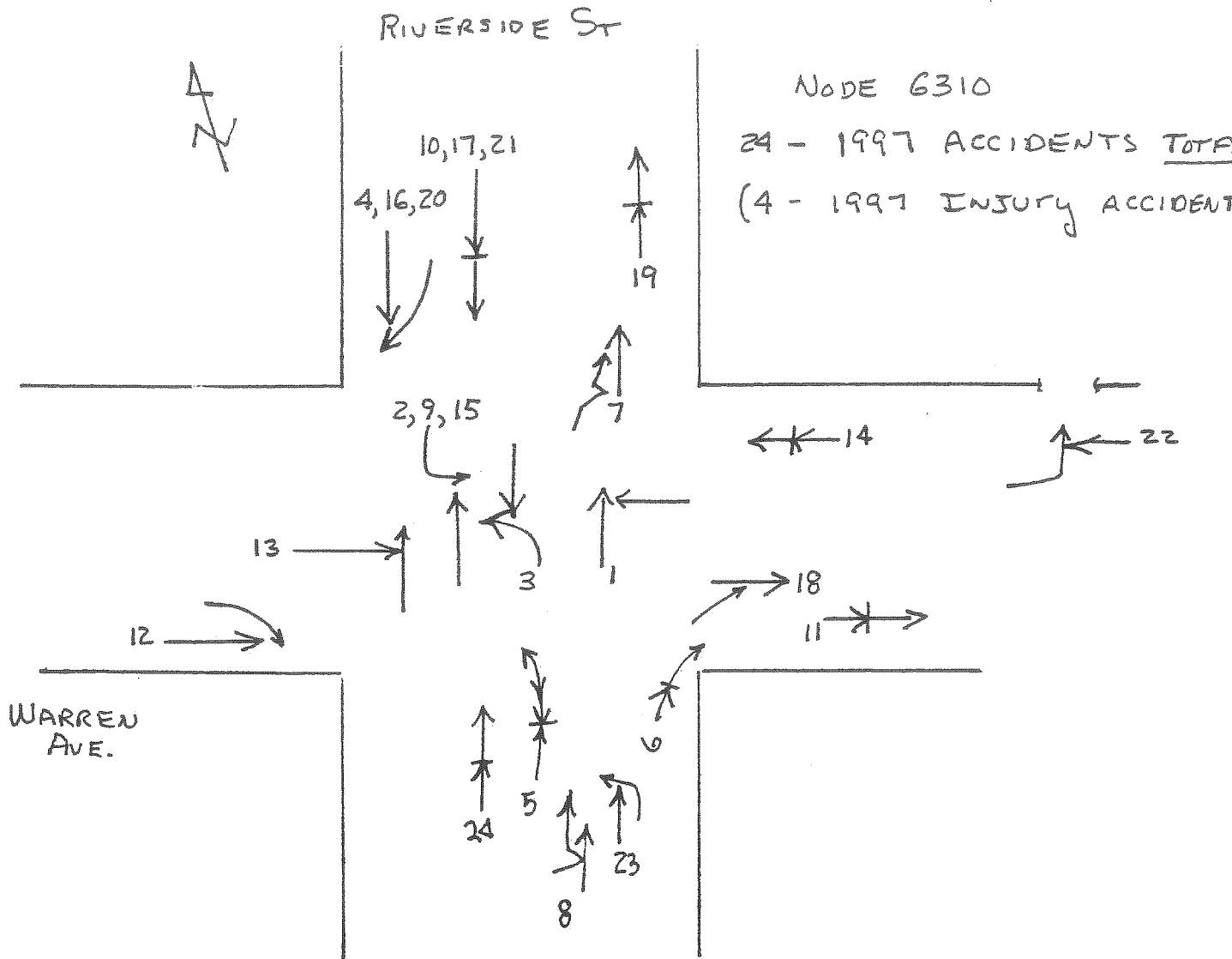
- 59691 11/17/96 10:00 PM w/o indication
- 41104 12/2/94 11:12 PM w/o indication
- 12820 7/21/96 8:10 PM w/o indication

WARREN AVE.

- 03318 11/17/96 11:57 AM w/o indication
- 10438 9/11/96 11:57 AM w/o indication
- 25135 11/20/96 11:57 AM w/o indication
- 201 11/17/95 11:57 AM w/o indication

6310

↑
Warren



- | | | |
|---|--|--|
| ① 9/28/97 M 0543 F1 | ⑨ 8/20/97 W 0835 | ⑰ 4/16/97 W 1325 (rear end) |
| ② 12/4/97 Th 1818 | ⑩ 7/22/97 Tue 1800 <small>rear end</small> | ⑱ 6/5/97 Thu 0750 (merge) |
| ③ 11/16/97 Sun 0510 F1 (ICE) | ⑪ 7/2/97 W 1290 | ⑲ 4/7/97 Mon 0830 |
| ④ 10/9/97 Th 2010 (TRAILER TRUCK) | ⑫ 7/8/97 Tue 1641 | ⑳ 2/12/97 Wed 1735 (Trailer truck rt turn) |
| ⑤ 9/21/97 Sun 1350 (BACKING) | ⑬ 7/2/97 W 1615 (ran red) | ㉑ 11/2/97 Sun 1247 |
| ⑥ 12/30/97 Tue 1730 | ⑭ 6/10/97 Tue 1610 (B) | ㉒ 2/19/97 Wed 1713 (BLOCK VIEW) |
| ⑦ 9/11/97 M 1040 (merge) | ⑮ 5/3/97 Sat 2000 (B) | ㉓ 2/3/97 M. 2000 |
| ⑧ 11/14/97 Fri 1740 (Trailer truck LANE CHANGE) | ⑯ 5/13/97 Tue 1840 | ㉔ 2/1/97 Sat 1752 (wet) |



2211 N.W. 30th Place • Pompano Beach, FL 33069
954-960-1010 • 800-370-7278 • FAX: 954-974-2773

To: Candy Talbot
From: John Locke

Copies of information
requested. Thank you

John

Please call me if you need
anything else


```

*****
*
*                               TRANSACTION REPORT                               *
*
*                               APR-14-00 09:20 AM                               *
*   FOR: PLANNING DEPARTMENT      7568258                                     *
* _____ *
*   SEND                                                                    *
* _____ *
*   DATE   START   RECEIVER          PAGES   TIME   NOTE *
* _____ *
*   APR-14 09:19 AM 97747875          0     **'**"  BUSY *
* _____ *
*****

```

From: "Steve Bushey" <srbushey@maine.rr.com>
To: "Kandi Talbot" <KCOTE@ci.portland.me.us>
Date: Sat, Dec 11, 1999 7:35 AM
Subject: 160 riverside St.

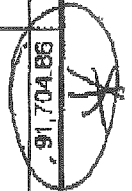
Kandi,

I reviewed the R.J. Grondin pay req. and find it to be acceptable as it relates to release of performance guarantee funds. As we discussed I recommend release of 90% of the requested amount, thus leaving 10% for those items which have been completed, but may need to be repaired prior to final acceptance and full release of the performance guarantee amount.

Steve

CONTINUATION SHEET (MA)
 Job 2351
 GEN CONTRACTOR/OWNER John Locke
 SUB CONTRACTOR R.J. Grondin & Sons, Gorham, Maine
 PROJECT NAME 160 Riverside Street
 APPLICATION # APPLICATION DATE: 3/10/00 PERIOD FROM: 05/01/99 PERIOD TO: 03/10/00

A	B	C	D	E	F	G	H	I	J
ITEM NO.	DESCRIPTION OF WORK	SCHEDULED VALUE	WORK COMPLETED PREVIOUS	WORK COMPLETED THIS PERIOD	MATERIALS PRESENTLY STORED	TOTAL COMPLETED AND STORED TO DATE	BALANCE TO FINISH	RETAINAGE	% COMP.
2050	Demolition	1,135.00	1,135.00	-	-	1,135.00	-	-	100%
2110	Clearing	1,715.00	1,715.00	-	-	1,715.00	-	-	100%
2200	Building	31,100.00	-	17,390.00	-	17,390.00	13,710.00	-	56%
2210	EEB Electrical	10,548.00	-	-	-	-	10,548.00	-	0%
2270	Erosion Control	3,320.00	1,106.66	-	-	1,106.66	2,213.34	-	33%
2514	Pavement	59,623.00	29,811.50	-	-	29,811.50	29,811.50	-	50%
2520	Site Concrete Prep	1,756.00	-	-	-	-	1,756.00	-	0%
2525	Curb	11,638.00	-	3,300.00	-	3,300.00	8,338.00	-	28%
2660	Waterline	9,325.00	-	-	-	-	9,325.00	-	0%
2700	Sanitary Sewer	11,020.00	11,020.00	-	-	11,020.00	-	-	100%
2720	Storm Drain	29,003.00	26,102.70	-	-	26,102.70	2,900.30	-	90%
2900	Lean Placement	9,738.00	-	-	-	-	9,738.00	-	0%
Extra	Permit - Inv. 41997	124.00	124.00	-	-	124.00	-	-	100%
	TOTAL	180,045.00	71,014.86	20,690.00	-	91,704.86	88,340.14	-	51%



Finance Department



Duane G. Kline
Director

CITY OF PORTLAND

February 19, 1999

Deborah J. Rubino, Business Specialist
Fleet Bank of Maine
50 Market Street
South Portland, Maine 04106

Re: J-Properties Site Improvements @ 160 Riverside Street, Portland
Escrow Account #9360199026, letter dated October 29, 1998

Dear Ms. Rubino:

This is to inform you that I am authorizing the reduction of the above-named escrow account, as follows:

Original Amount	\$26,290.00
Reduction Amount	<u>13,050.00</u>

Remaining Amount	\$13,240.00
------------------	-------------

If you have any questions, please let me know.

Sincerely,

A handwritten signature in dark ink, appearing to read 'D. Kline', is written over the typed name.

Duane G. Kline
Finance Director



Assist Last Clip Escape

Information Query

Primary Data - SAV

JOHN E LOCKE	LAST DEP DT	02/08/1999	ACCT NBR	9360199026
FLEET BANK OF MAINE, ESC AGENT	LAST DEP	\$63,834	BRANCH NUMBER	90902
PERFORMANCE GUARANTEE CITY OF	TIN/SSNO	006726576	CLOSE OVERRIDE	" "
58 MARKET ST	FLEET1 ID	000000000	LAST STMT	11/30/1999
ATTN: DEBBIE RUBINO			NEXT STMT	12/31/1999
S PORTLAND	ME 041060000		EMPLOYEE CODE	B
			ANALYSIS ACCT	R
ONLINE BAL	\$78,156.23	CUR MARKUP		\$.00
AVAIL BAL	\$78,156.23			
CUR BAL	\$78,156.23	DDA AFFILIATE	0000000000	
FUND BAL	\$.00			
CASH FLOAT	\$.00			

Account has Restrictions

To see any acct restrictions, press a key; for general info, press [F8]/Assist
Caps Numeric



(FAX 3 PAGES TOTAL)
TO: CANDI TALBOT
FROM: JOHN LOCKE 954 960 1010 X4
JOB SITE: 160 RIVERSIDE ST EXIT8

CANDI
HERE IS THE COMPLETE LIST OF WORK THAT HAS BEEN DONE.
I HAVE ALSO SENT A COPY OF THE PERFORMANCE GUARANTEE FROM FLEET BANK
PLEASE LET ME KNOW IF THERE IS ANYTHING ELSE I CAN DO.


THANKS JOHN



2211 N.W. 30th Place • Pompano Beach, FL 33069
954-960-1010 • 800-370-7278 • FAX: 954-974-2773

To: Candy TALBOT
From: John Locke
160 Riverside St development.

Amount Due and completed AT SITE
\$78,464.86 TOTAL

Please let me know if
there is any other information
needed. The Fax number at

Fleet Bank is 879 2727

ATTN: Jim Stone OR Deborah Rubino

Thank you John Locke ☺



CITY OF PORTLAND
Planning and Urban Development Department

MEMORANDUM

TO: Duane Kline, Finance Department
FROM: Joseph E. Gray, Jr., Director of Planning and Urban Development
DATE: April 14, 2000
SUBJECT: Request for Reduction in Performance Guarantee
160 Riverside Street

A request by the owner of the commercial property at 160 Riverside Street has been received for a reduction in the performance guarantee amount. This is the second reduction requested.

First PG submitted	\$26,290.00
First Reduction	<u>\$13,050.00</u>
Remaining Amount	\$13,240.00
Second Reduction	<u>\$ 7,161.00</u>
Remaining Amount of 1st PG	\$ 6,079.00
Second PG submitted	\$63,834.00
Reduction Requested	<u>\$52,316.00</u>
Remaining Amount of 2nd PG	\$11,518.00
Total Amount Released	\$59,477.00

The remainder is the sum, based on the performance guarantee, necessary to complete the work.

Approved: _____

Joseph E. Gray, Jr.
Joseph E. Gray, Jr.
Director of Planning and Urban Development

cc: Kandi Talbot, Planner
Code Enforcement
Development Review Coordinator

11-00
519

SEBAGO TECHNICS
 Engineering & Planning for the Future
 One Ocala Square
 Westbrook, ME 04098-1339
 Tel: (207) 955-0277

J. PROPERTIES
 2211 N.W. 30th Place
 Pompano Beach, FL 33069

SLEEP INN
 160 RIVERSIDE STREET
 PORTLAND, MAINE

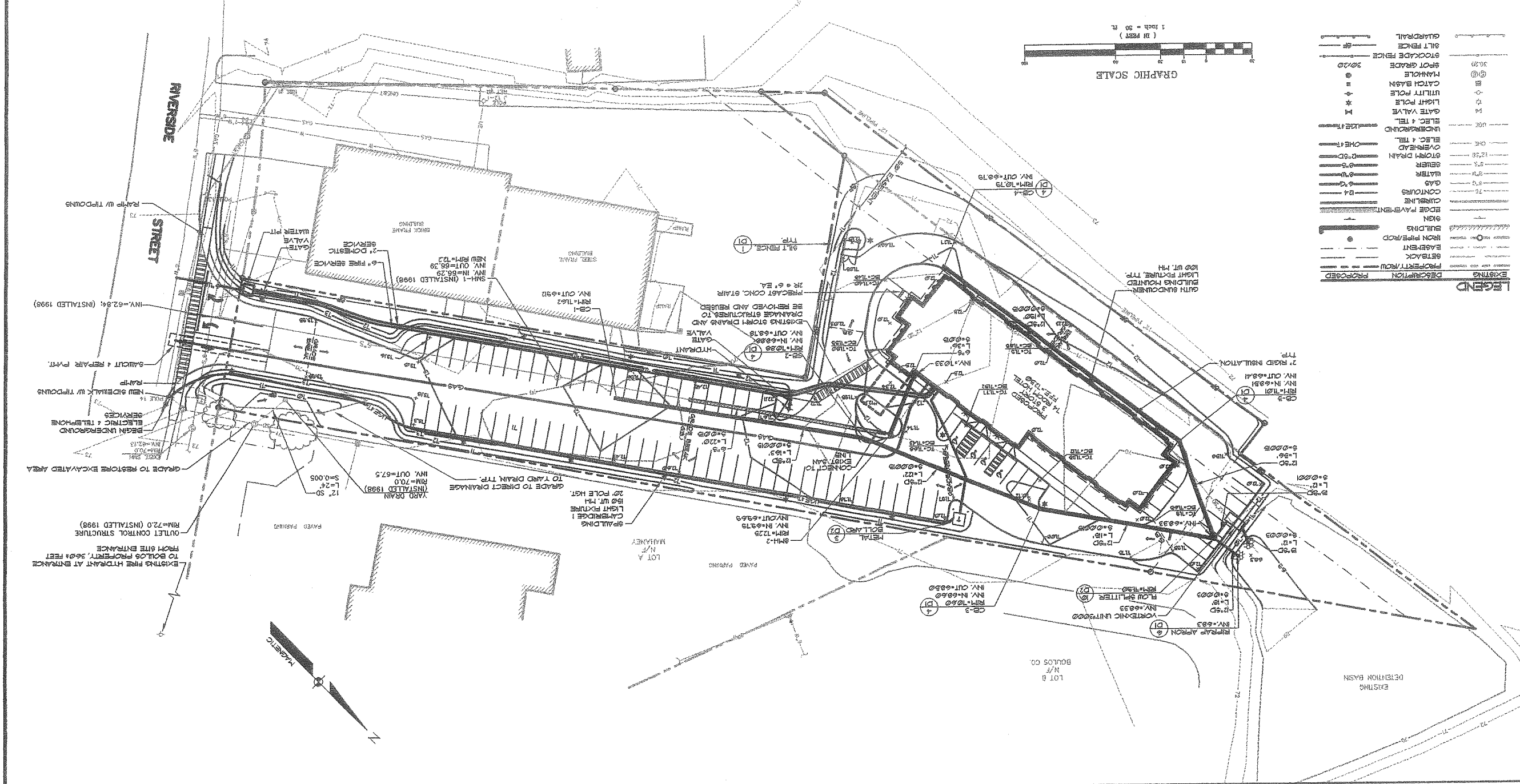
GRADING AND DRAINAGE PLAN

THIS PLAN SHALL NOT BE MOVED WITHOUT WRITTEN PERMISSION FROM SEBAGO TECHNICS, INC. ANY ALTERATIONS, ADDITIONS OR DELETIONS SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC.

REV.	BY	DATE	STATUS
B	SDD	5-30-00	ISSUED FOR PLANNING BOARD REVIEW
A	SDD	3-28-00	ISSUED FOR PLANNING BOARD WORKSHOP

DESIGN BY: SDD
 CHECKED BY: JLB
 DATE: 3-5-00
 SCALE: 1"=30'
 FIELD BK: 88017
 PROJ. NO: 88017
 DRAWING: 88017A01
SHEET 52

- CONSTRUCTION NOTES**
1. THE CONTRACTOR SHALL PROVIDE ADEQUATE MEANS OF CLEANING AND PROTECT EXISTING AND PROPOSED DRAINAGE SYSTEMS FROM TRUCKS AND OTHER EQUIPMENT PRIOR TO EXTERIOR PUBLIC STREETS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CLEAN ALL DIRT, DUST, AND TAKE WHATEVER MEASURES ARE NECESSARY TO INSURE THAT THE STREETS ARE MAINTAINED IN A CLEAN AND DUST FREE CONDITION AT ALL TIMES.
 2. ALL DISTURBED AREAS SHALL BE LOADED AND SEDED IN ACCORDANCE WITH THE EROSION AND SEDIMENTATION CONTROL PLAN.
 3. ALL STORM DRAINAGE PIPES SHALL BE SMOOTH BORE HDPE PIPES OR AS NOTED ON PLANS. ALL SEWER PIPES SHALL BE PVC SDR 35.
 4. PROTECT CLEAN AND REMOVE SEDIMENT FROM ALL DRAINAGE STRUCTURES (INCLUDING DETENTION POND) WITHIN THE CONSTRUCTION AREA.
 5. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL INSPECT THE SITE FOR GENERAL CONDITIONS AND ANY SPECIAL REQUIREMENTS NECESSARY FOR PROJECT SERVICES IN CONDUCT TO SITE LIGHTING, BUILDING CONTRACTS WITH APPLICABLE CODES, COORDINATE WITH OTHER CIP AND ARCHITECTURAL AND CIP PLANS. PRIOR TO CONSTRUCTION CONTRACTOR SHALL COMPLY WITH APPLICABLE ELECTRICAL AND LOCATION OF UTILITIES.
 6. CONTRACTOR SHALL PROVIDE AND INSTALL ALL NECESSARY ELECTRICAL SERVICES IN CONDUCT TO SITE LIGHTING, BUILDING CONTRACTS WITH APPLICABLE CODES, COORDINATE WITH OTHER CIP AND ARCHITECTURAL AND CIP PLANS. PRIOR TO CONSTRUCTION CONTRACTOR SHALL COMPLY WITH APPLICABLE ELECTRICAL AND LOCATION OF UTILITIES.
 7. REMOVE TOPSOIL AND ROUGHEN UNDERLAY TO ESTABLISH SUITABLE INTERFACES FOR ALL FILL. FILL AROUND TREE SAVERS ARE SUBJECT TO SPECIAL PROVISIONS THAT ARE NOTED IN SPECIFICATIONS AND PLANS.
 8. IMMEDIATELY UPON COMPLETION OF CUT/FILLS, THE CONTRACTOR SHALL STABILIZE DISTURBED AREAS IN ACCORDANCE WITH EROSION CONTROL NOTES AND AS SPECIFIED ON PLANS.
 9. ALL FILL SHALL BE PLACED LAYERS NOT MORE THAN 12" LOOSE DEPTH AND COMPACTED BY HEAVY COMPACTION EQUIPMENT, MINIMUM COMPACTION SHALL BE 98% OF MAXIMUM DENSITY ASTM 957, MODIFIED AND FIELD DENSITY SHALL BE 95% (NUCLEAR METHOD).
 10. CONTRACTOR SHALL BE CONTAINERIZED AND DISPOSED OF IN ACCORDANCE WITH APPLICABLE SOLID WASTE REGULATIONS.
 11. ANY DAMAGE TO PUBLIC OR PRIVATE PROPERTY RESULTING FROM CONSTRUCTION ACTIVITIES SHALL BE REPAIRED BY THE DEVELOPER/CONTRACTOR AT THEIR EXPENSE.
- CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS PRIOR TO CONSTRUCTION.
- BEFORE TO CONSTRUCTION THE SITE CONTRACTOR SHALL CORRECT SETBACKS, BUILDING DRAWINGS AND COORDINATE BETWEEN SITE AND ARCHITECTURAL DRAWINGS. CONTRACTOR SHALL NOTIFY ENGINEER AND OWNER OF ANY DIFFERENCES AFFECTING BUILDING OR SITE CONSTRUCTION. COORDINATE SITE WORK WITH REQUIREMENTS OF ARCHITECTURAL, MECHANICAL, ELECTRICAL AND FOUNDATION PLANS.
- H. METHODS AND MATERIALS USED IN THE CONSTRUCTION OF THE CONSTRUCTION HEREIN SHALL CONFORM TO THE CURRENT CITY CONSTRUCTION STANDARDS AND SPECIFICATIONS AND/OR CURRENT MDOT STANDARDS AND SPECIFICATIONS.
- I. WARNING SIGNS, MARKERS, BARRICADES OR FLAGGING, APPROPRIATE FOR THE TYPE OF CONSTRUCTION MUST BE EMPLOYED TO REGULATE TRAFFIC AND IN ACCORDANCE WITH MDOT STANDARDS.
- J. CONSTRUCTION SHALL BE IN ACCORDANCE WITH MDOT STANDARDS.



SEBAGO TECHNICS
 Engineering & Planning for the Future
 One Grand Street
 Watford, MA 02458-1339
 Tel (207) 956-0277

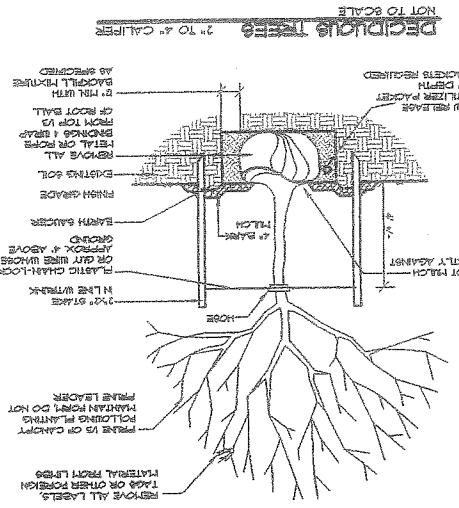
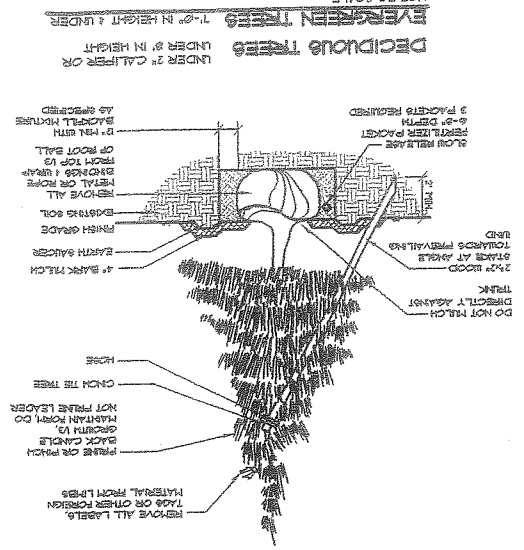
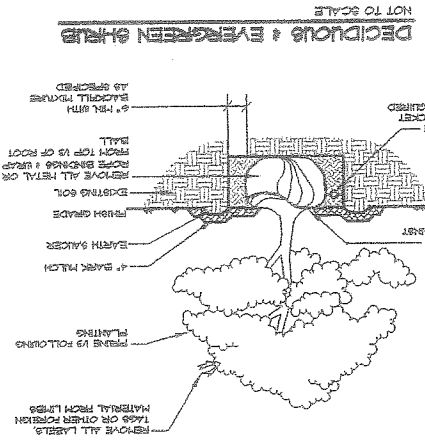
LANDSCAPE PLAN
 SLEEP INN
 160 RIVERSIDE STREET
 PORTLAND, MAINE
 02711 N.W. 30th PLACE
 POMFRET BEACH, FL 33069

DESIGN BY: SDO
 DRAWN BY: ALB
 CHECKED BY: JLB
 DATE: 3-6-00
 SCALE: 1"=30'
 FIELD BK: 88017
 PROJ. NO: 88017
 DRAWING: 88017AL

SHEET 54

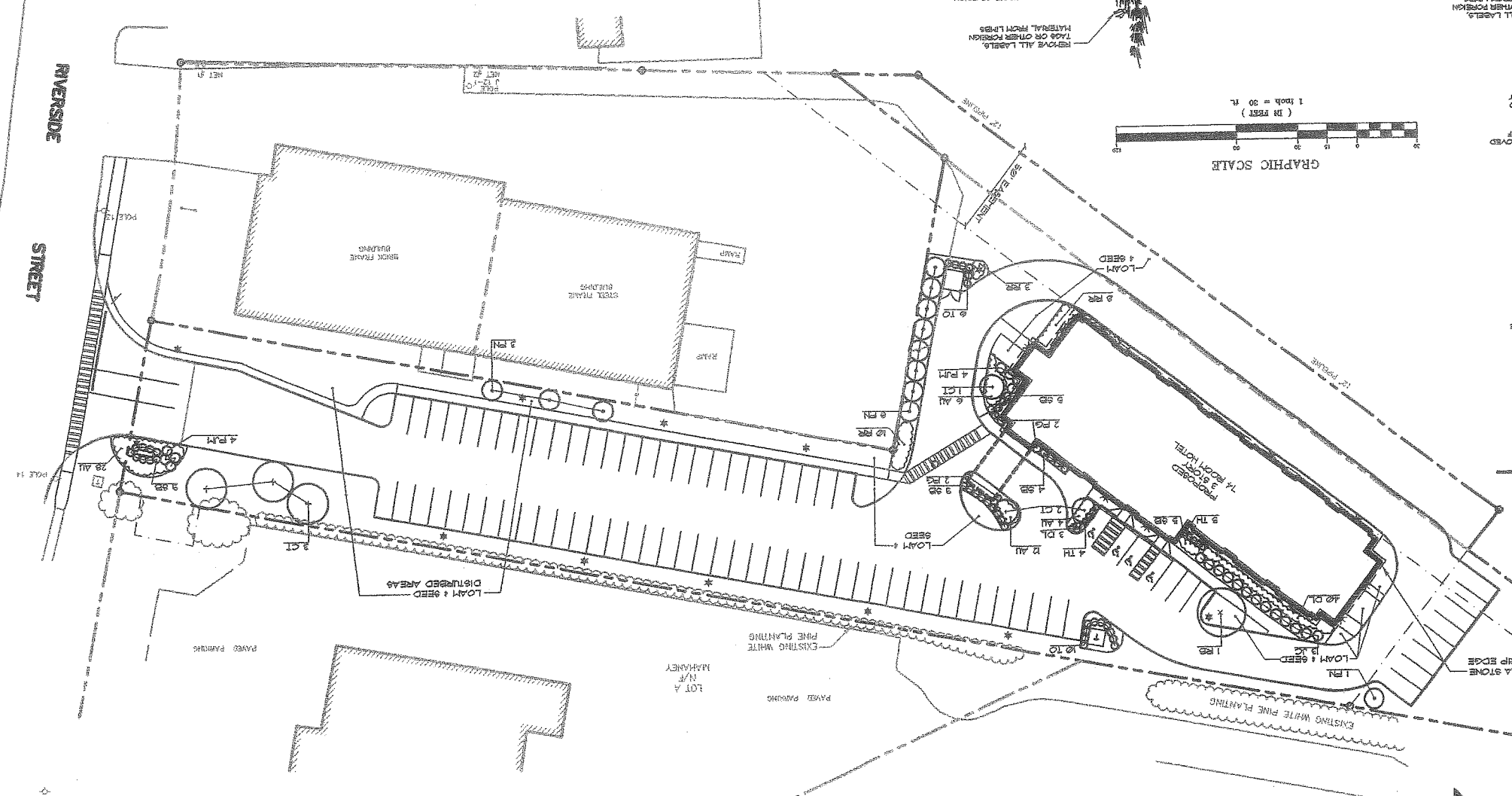
REV.	BY:	DATE:	STATUS:
A	SDO	3-28-00	ISSUED FOR PLANNING BOARD WORKSHOP
B	SDO	5-2-00	REVISED PER PPL REVIEW COMMENTS
C	SDO	5-30-00	REVISED FOR PLANNING BOARD REVIEW

THIS PLAN SHALL NOT BE MOVED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC. ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC.



LANDSCAPE NOTES

1. PLANT QUANTITIES SHOWN ON PLANT LISTS ARE FOR CONFORMANCE TO THE CONTRACTOR ONLY. THE CONTRACTOR IS RESPONSIBLE FOR ALL PLANT MATERIAL INSTALLATION AS SHOWN ON PLANS.
2. SEE AND OBTAIN STANDARDS OF PLANT MATERIALS SHALL CONFORM TO THE LATEST EDITION OF THE STANDARD FOR NURSERY STOCK BY THE AMERICAN ASSOCIATION OF NURSERYMEN, INC.
3. ALL PLANT MATERIALS SHALL BE FREE FROM INSECTS AND DISEASE.
4. ALL PLANTING SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR.
5. BY THE CONTRACTOR AND A PERIOD OF TWO YEARS GUARANTEE PERIOD, DEAD PLANT MATERIAL SHALL BE REPLACED AT NO COST TO THE OWNER FROM DATE OF INSTALLATION. DURING THE CONTRACTOR SHALL MAINTAIN AND ACCEPTANCE FROM THE OWNER.
6. ALL GRASS, OTHER VEGETATION AND DEBRIS SHALL BE REMOVED FROM PLANTING AREAS PRIOR TO PLANTING.
7. EXISTING TREES TO BE PRESERVED WILL BE PROTECTED BY THE CONTRACTOR.
8. THE LANDSCAPE CONTRACTOR IS ADVISED OF THE PRESENCE OF THE UNDERGROUND UTILITIES AND SHALL VERIFY THE EXISTENCE AND LOCATION OF THESE UTILITIES PRIOR TO PLANTING OPERATIONS. THE LANDSCAPE CONTRACTOR SHALL REPLACE OR REPAIR UTILITIES DAMAGED DURING THE COURSE OF PLANTING OPERATIONS. THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR THIS WORK AT AN ADDITIONAL COST TO THE OWNER.
9. ALL SHRUBS SEEDS SHALL BE PLACED WITH 3" CLEAN SHARDED BARK MULCH.
10. THE CONTRACTOR SHALL PROVIDE A LOAM FOR ALL AREAS TO BE PREPARED. THE LANDSCAPE CONTRACTOR SHALL COORDINATE SUBGRADE PREPARATION WITH THE GENERAL CONTRACTOR PRIOR TO PLACING LOAM.
11. ANY DEVIATION FROM THE LANDSCAPE PLAN INDICATED PLANT LOCATION, SELECTION, SIZE, QUANTITY OR POSITION SHALL BE REVIEWED AND APPROVED BY THE OWNER AND LANDSCAPE ARCHITECT (AND HORIZONTAL ALIGNMENT, IF APPLICABLE) PRIOR TO INSTALLATION ON SITE.
12. WHERE INDICATED ON PLAN PLANTING SOIL, NURSERY FOR PESTICIDES AND FUNGICIDES SHALL BE USED. PLANTING SHALL BE REVIEWED AND APPROVED BY THE OWNER AND LANDSCAPE ARCHITECT (AND HORIZONTAL ALIGNMENT, IF APPLICABLE) PRIOR TO INSTALLATION ON SITE.
13. PLANTING SHALL BE REVIEWED AND APPROVED BY THE OWNER AND LANDSCAPE ARCHITECT (AND HORIZONTAL ALIGNMENT, IF APPLICABLE) PRIOR TO INSTALLATION ON SITE.
14. PLANTING SHALL BE REVIEWED AND APPROVED BY THE OWNER AND LANDSCAPE ARCHITECT (AND HORIZONTAL ALIGNMENT, IF APPLICABLE) PRIOR TO INSTALLATION ON SITE.



PLANTING LIST

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE
RB	Betula nigra	River Birch	6" Hgt.
FN	Fraxinus nigra	American Frax	6" Hgt.
RR	Rosa rugosa 'Falsam'	Rugosa Rose	6" Hgt.
TH	Taxus x media 'Hickill'	Hick's Tree	14" - 30"
SB	Spiraea x bumalda 'Chapel'	Chapel Spirea	18" - 24"
TO	Thuja occidentalis 'Nigra'	Dark American Arborv.	5' - 6" Hgt.
DL	Hamamelis virginica 'Dora'	Witch's Tree	No. 1 cont.
PJM	Prunella japonica 'Moulin'	Japanese Spirea	14" - 30"
AU	Arctostaphylos uva-ursi	Bearberry	1" cont.
JC	Juniperus horizontalis 'Bar Harbor'	Bar Harbor Juniper	18" - 24"
PG	Picea canadensis	White Spruce	3' - 4" Hgt.
CT	Catalpa bignonioides	Tropic Tree	2-2 1/2' cal.

SEBAGO TECHNICS
 Engineering & Planning for the Future
 12 WESTROCK COMMON
 WESTROCK, MA 04093-1330
 TEL: (207) 854-0277

PROJECT PROPERTIES
 FOR: SLEEP INN
 160 RIVERSIDE STREET
 PORTLAND, MAINE
 2211 N.W. 30TH PLACE
 PCAPANO BEACH, FL 33089

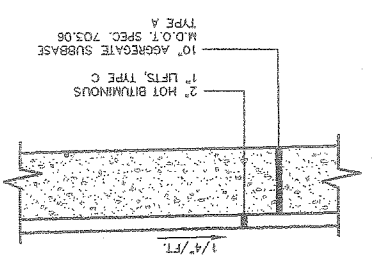
DESIGN B.Y.: J.B. SGO
DRAWN B.Y.: J.B.
CHECKED B.Y.: J.B.
DATE: 3-24-00
SCALE: AS SHOWN
FIELD BK.: 88017
PROJ. NO.: 88017
DRAWING: 8801702

SHEET 02

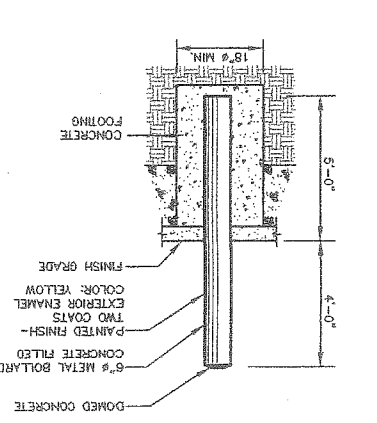
REV.	BY	DATE	STATUS
B	SGO	5-30-00	ADD FLOW SPLITTER DETAIL
A	SGO	3-28-00	ISSUED FOR PLANNING BOARD WORKSHOP

THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM SEBAGO TECHNICS, INC. ANY ALTERATIONS AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC.

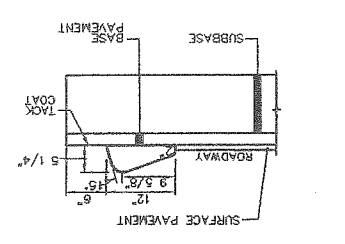
4 BITUMINOUS SIDEWALK
NOT TO SCALE



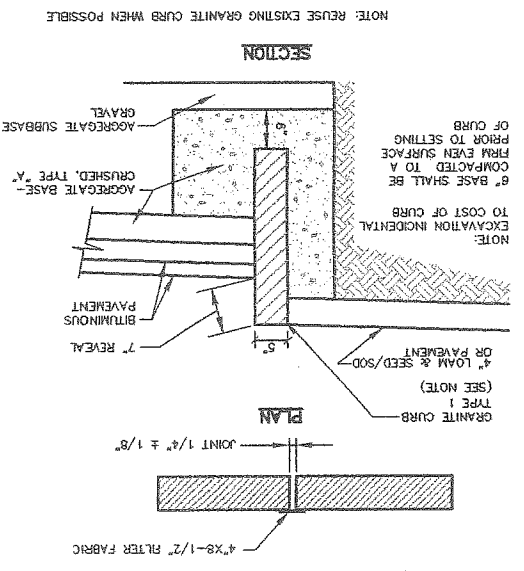
3 METAL BOLLARD
NOT TO SCALE



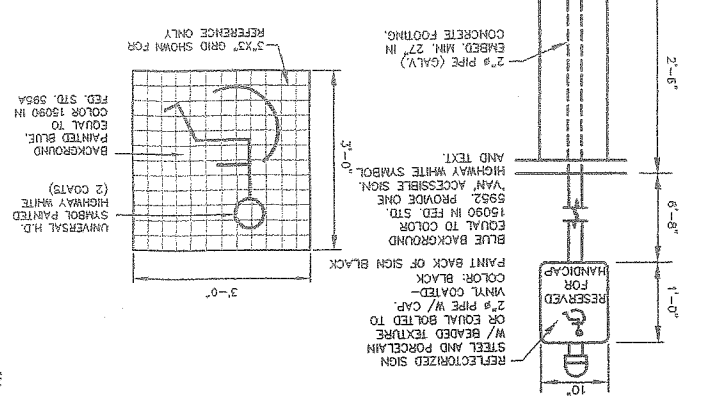
2 CAPE COD CURB DETAIL
NOT TO SCALE



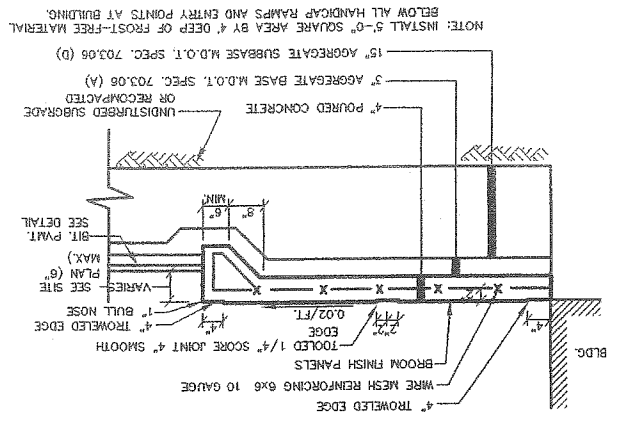
1 VERTICAL CURB INSTALLATION DETAIL
NOT TO SCALE



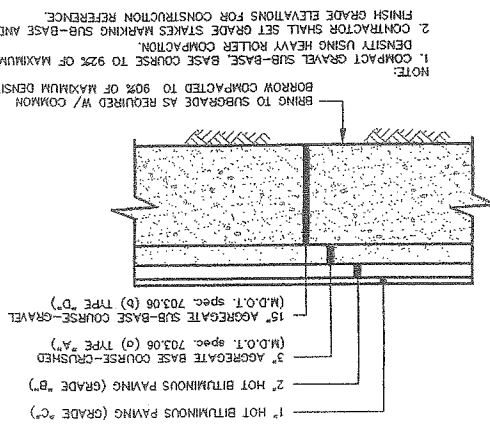
7 HANDICAP SIGNS
NOT TO SCALE



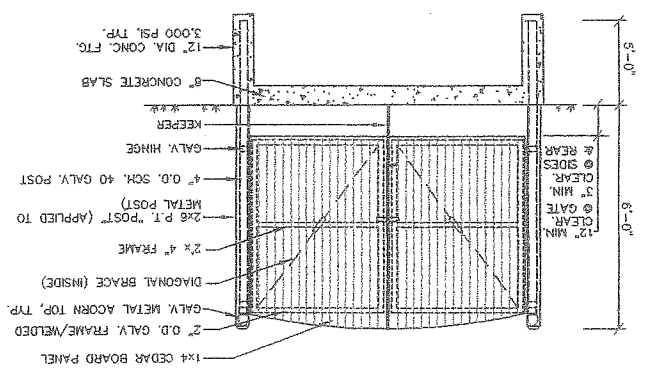
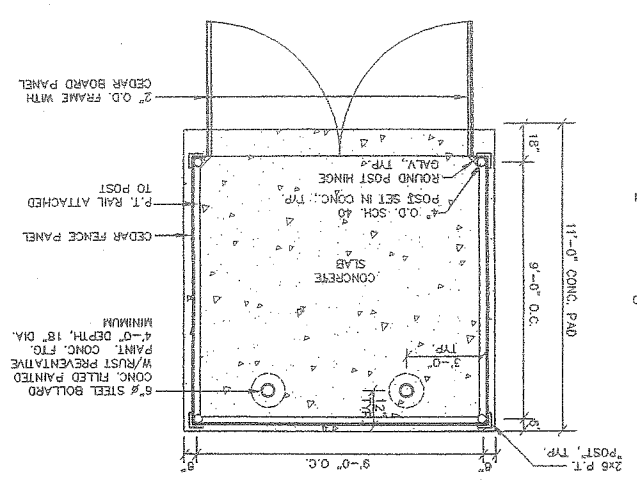
6 CONCRETE SIDEWALK
NOT TO SCALE



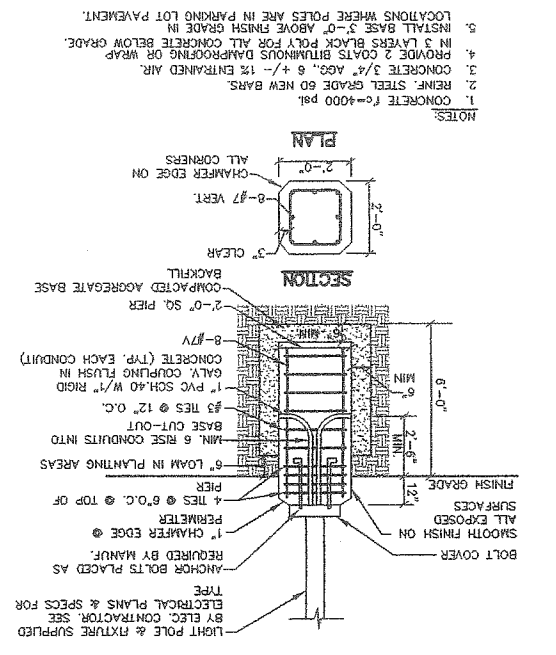
5 TYPICAL PAVED PARKING LOT SECTION
NOT TO SCALE



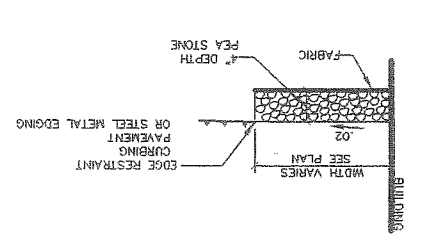
9 DUMPSTER ENCLOSURE
NOT TO SCALE



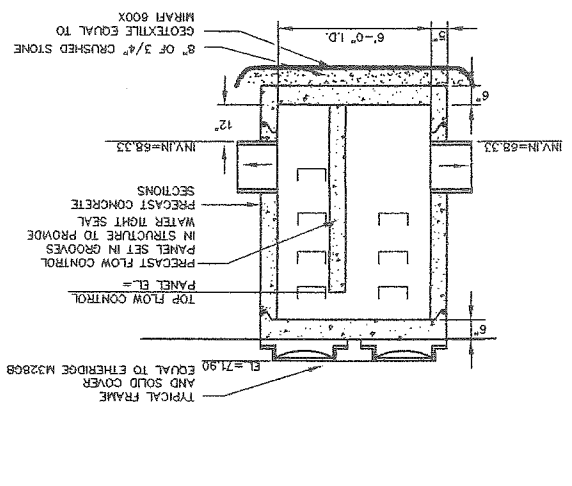
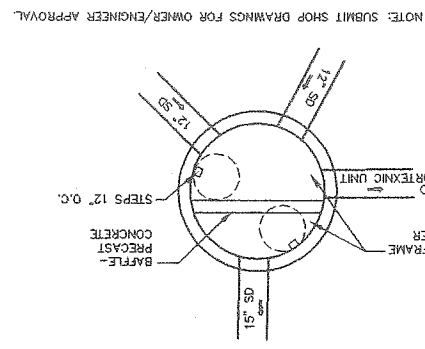
8 LIGHT POLE BASE
NOT TO SCALE



11 PEA STONE DRAIN EDGE
NOT TO SCALE



10 FLOW SPLITTER
NOT TO SCALE

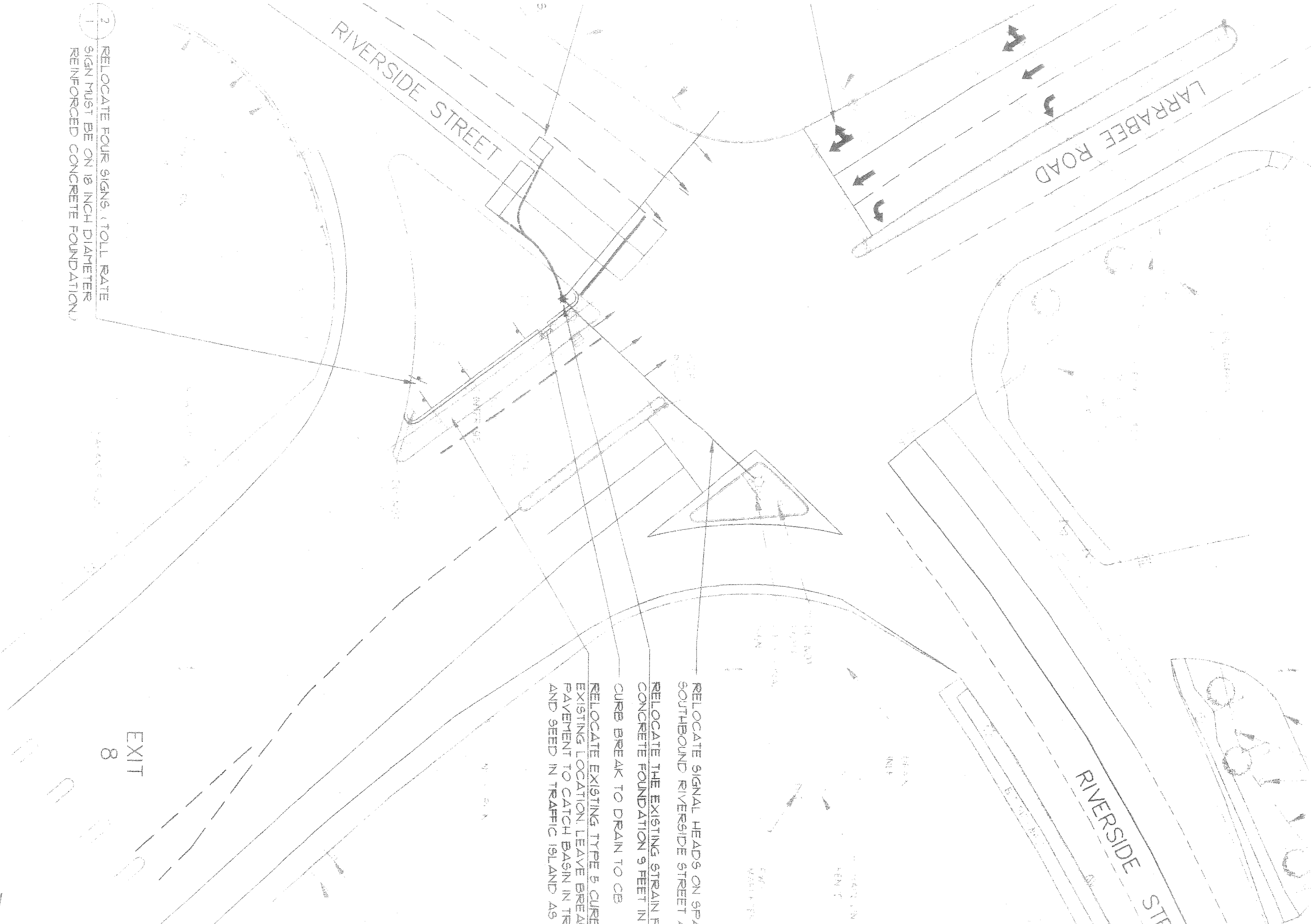


llr

- 2 RELOCATE FOUR SIGNS, (TOLL RATE SIGN MUST BE ON 18 INCH DIAMETER REINFORCED CONCRETE FOUNDATION)
- 1

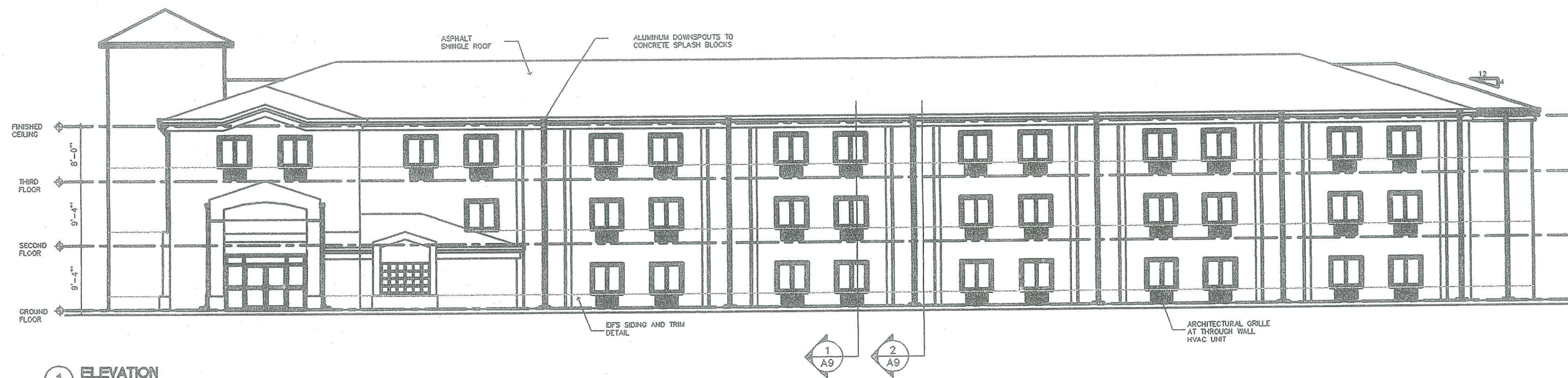
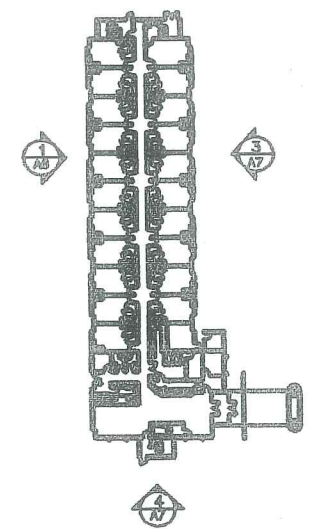
LEGEND

EXISTING	DESCRIPTION	PROPOSED
---	SIGN	---
---	EDGE PAVEMENT	---
---	CURB	---

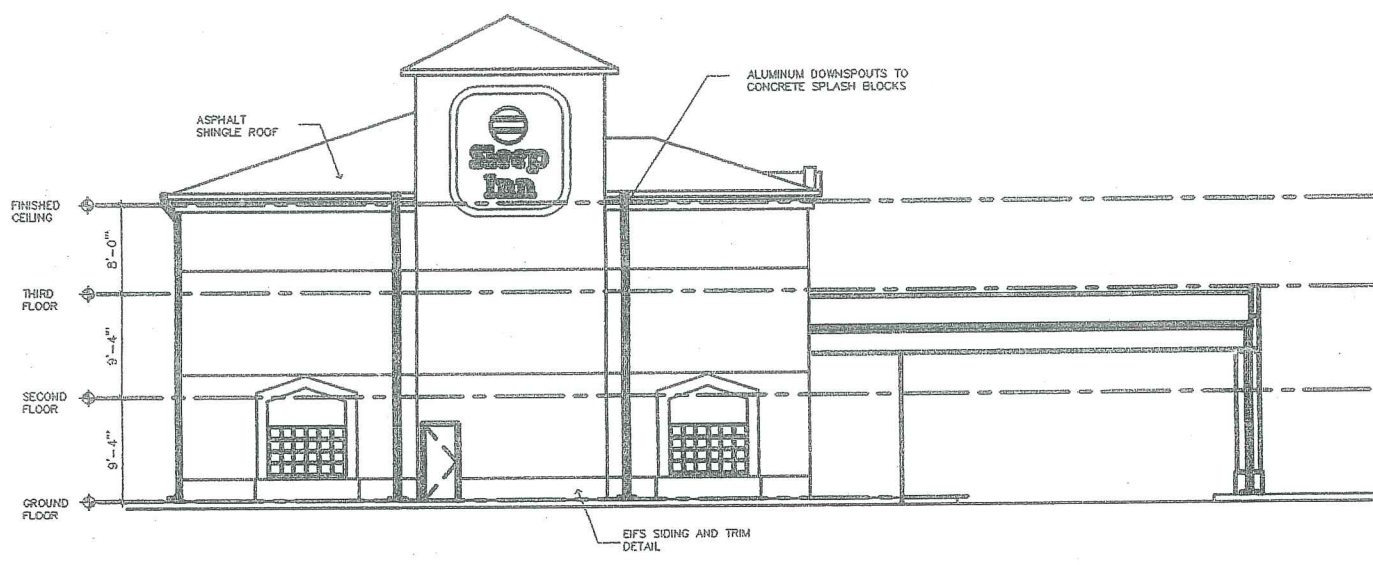


Att. 12

Att 7



1 ELEVATION
SCALE 1/8" = 1'-0"



2 ELEVATION
SCALE 1/8" = 1'-0"

PROGRESS 4-1-00

NO.	REVISION	DATE
SCALE: 1/8" = 1'-0"	DRAWN BY: jo	
DATE: 4/1/00	CHECKED BY: RCS	

PROJECT

SLEEP INN
PORTLAND, ME

JOB# 425

TITLE

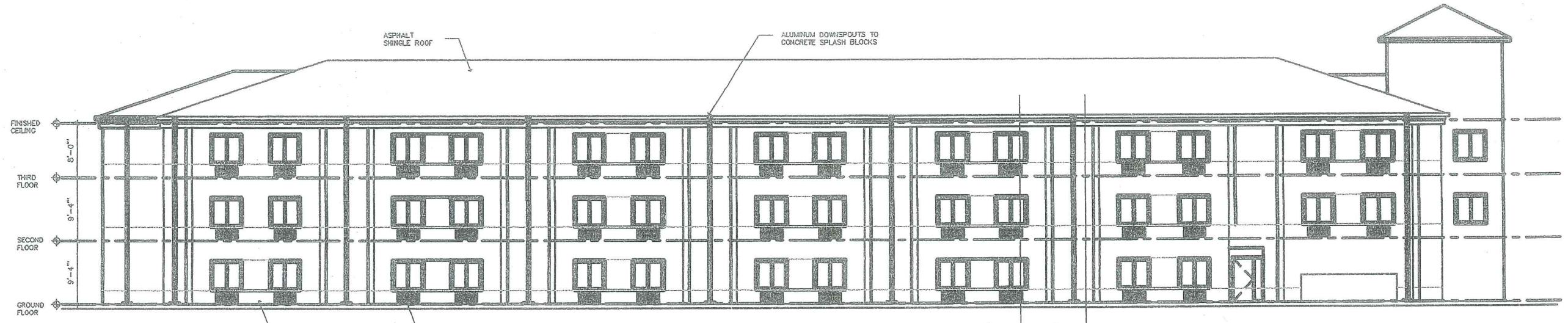
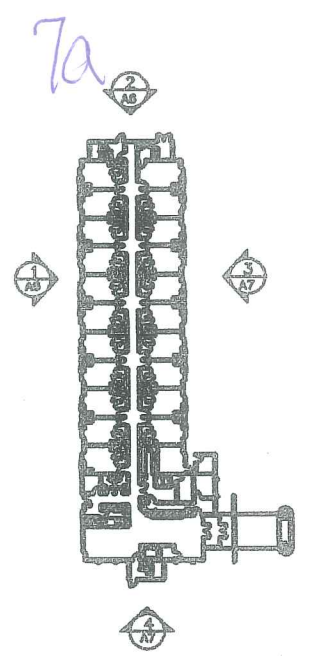
ELEVATIONS

GROUP ONE INCORPORATED
21 W. THIRD STREET, BOSTON, MA 02127
TEL: (617)268-7000 FAX: (617)268-0200

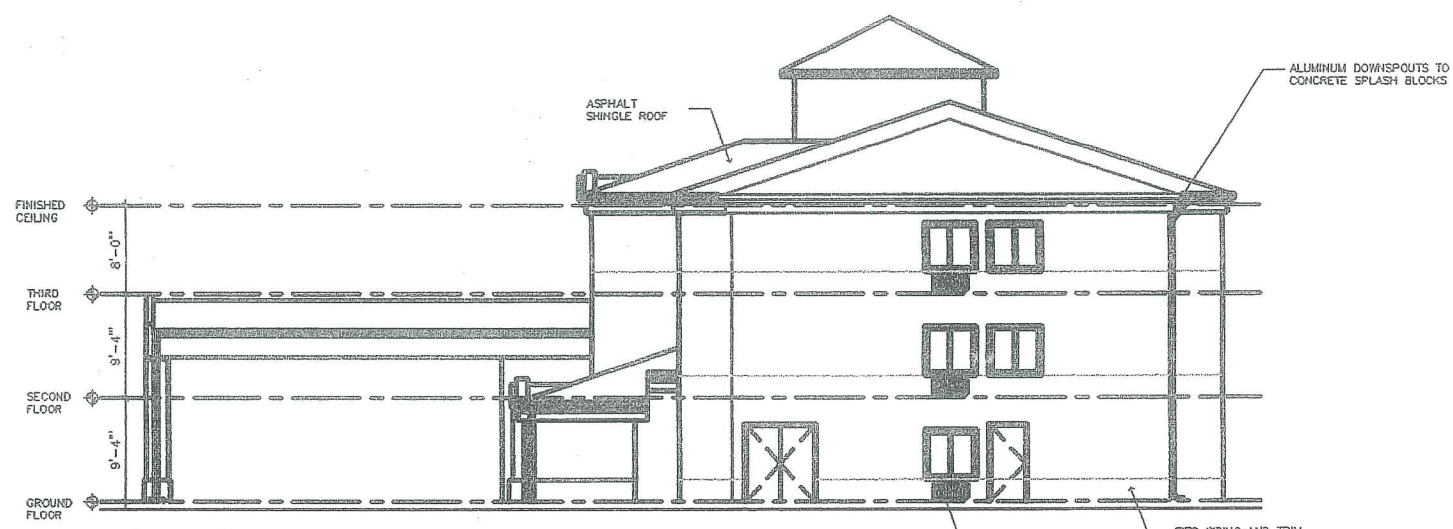
DRAWING NO.



A6



3 ELEVATION
SCALE 1/8" = 1'-0"



4 ELEVATION
SCALE 1/8" = 1'-0"

PROGRESS 4-1-00

NO.	REVISION	DATE
SCALE: 1/8" = 1'-0"	DRAWN BY: jo	
DATE: 4/1/00	CHECKED BY: RCS	

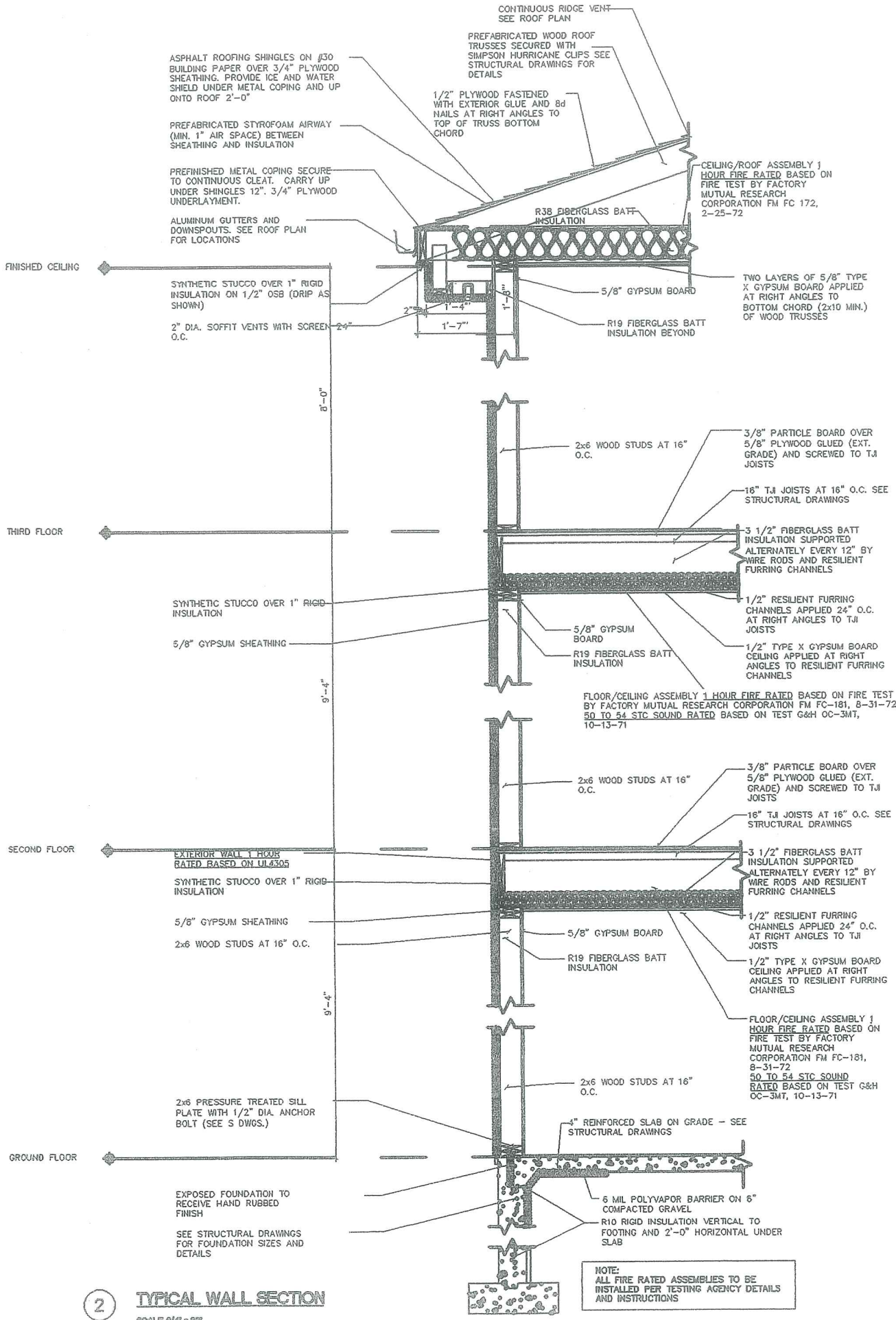
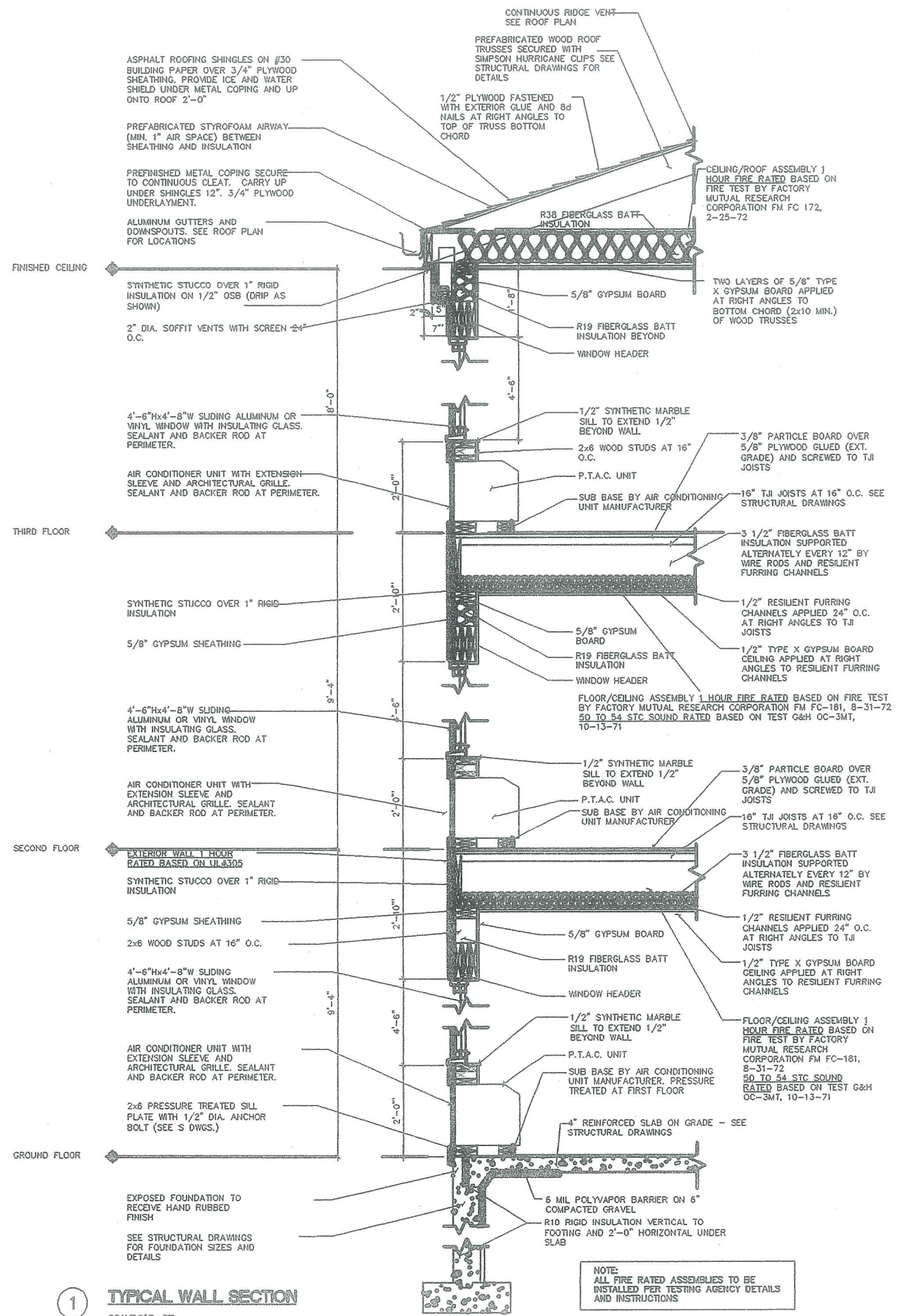
PROJECT
SLEEP INN
PORTLAND, ME
JOB# 486

TITLE
ELEVATIONS

GROUP ONE INCORPORATED
21 W. THIRD STREET, BOSTON, MA 02127
TEL: (617)293-7000 FAX: (617)293-0230

DRAWING NO.
A7

7b



1 TYPICAL WALL SECTION
SCALE 3/4" = 1'-0"

NOTE:
ALL FIRE RATED ASSEMBLIES TO BE
INSTALLED PER TESTING AGENCY DETAILS
AND INSTRUCTIONS

2 TYPICAL WALL SECTION
SCALE 3/4" = 1'-0"

NOTE:
ALL FIRE RATED ASSEMBLIES TO BE
INSTALLED PER TESTING AGENCY DETAILS
AND INSTRUCTIONS

PROGRESS 4-1-00

NO.	REVISION	DATE

SCALE: 3/4" = 1'-0" DRAWN BY: jo
DATE: 4/1/00 CHECKED BY: RCS

PROJECT
SLEEP INN
PORTLAND, ME

TITLE
TYPICAL WALL SECTIONS

GROUP ONE INCORPORATED
21 W. THIRD STREET, BOSTON, MA 02127
TEL: (617)268-7000 FAX: (617)268-0200

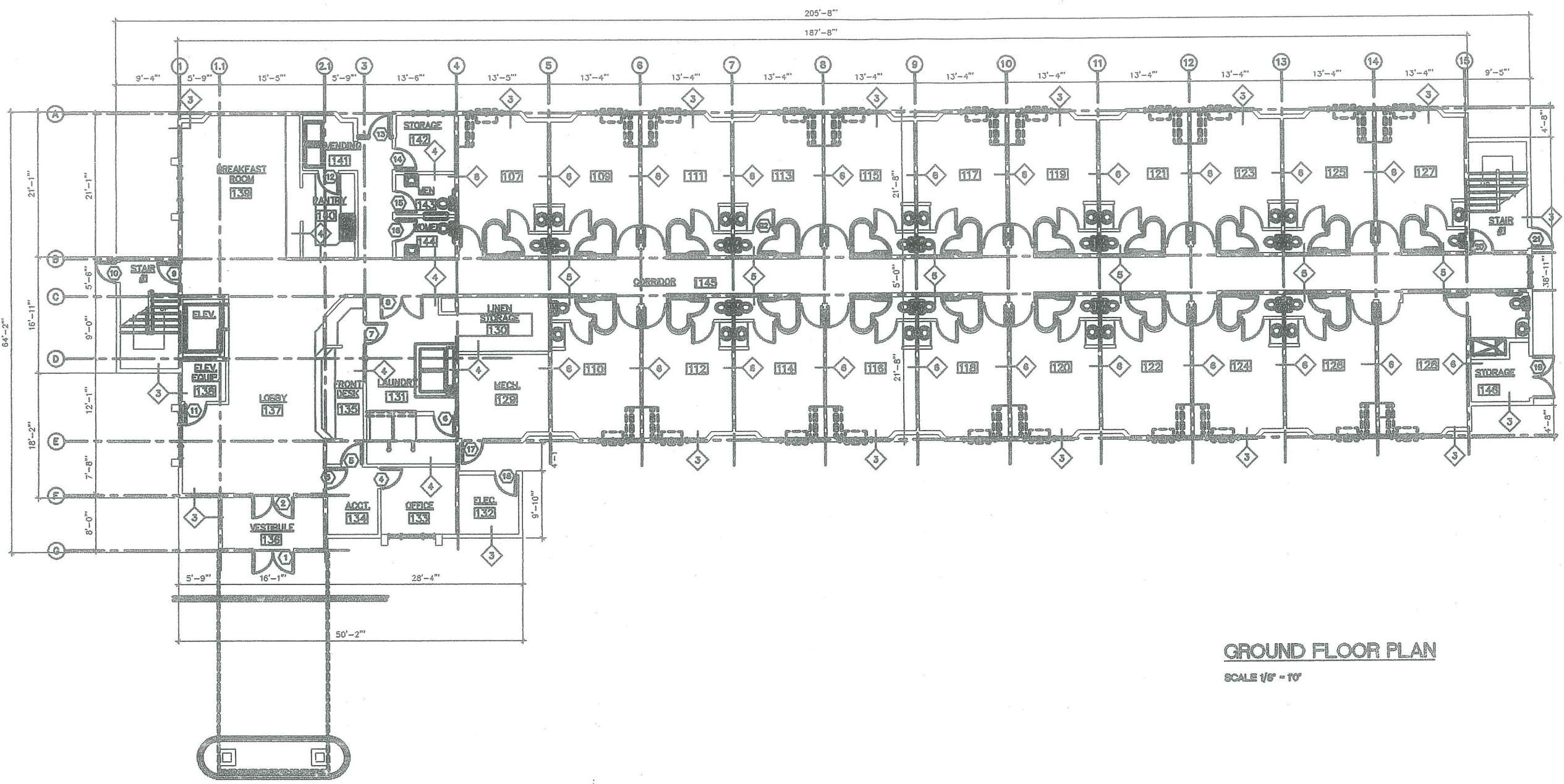
DRAWING NO.
A9

AH.7c

LEGEND OF SYMBOLS

- 1 WALL TYPE NUMBER SEE DWG. —
- 1 DOOR NUMBER SEE DWG. —
- 101 ROOM NUMBER SEE DWG. —
- A WINDOW TAG SEE DWG. —
- 1-1 DETAIL OR WALL SECTION NUMBER DWG. NUMBER WHERE DETAIL APPEARS
- A BUILDING SECTION DESIGNATION DWG. NUMBER WHERE SECTION APPEARS
- A COLUMN LINE
- 5 INTERIOR ELEVATION INDICATOR ELEVATION NUMBER DWG. NUMBER WHERE ELEVATION APPEARS
- 1-1 DETAIL NUMBER DWG. NUMBER WHERE DETAIL APPEARS

FIRE EXTINGUISHER
 FE - LARSEN FIRE EXTINGUISHER AND 1hr FIRE RATED CABINET WITH VERTICAL DUO DOOR AND LARSEN-LOC. SEMI-RECESSED WITH 3 1/2" PROJECTION MAX.



GROUND FLOOR PLAN
 SCALE 1/8" = 10'

PROGRESS 4-1-00

NO.	REVISION	DATE

SCALE: 1/8"=10" DRAWN BY: jo
 DATE: 4/1/00 CHECKED BY: RCS

PROJECT

SLEEP INN
 PORTLAND, ME

JOB# 468

TITLE
GROUND FLOOR PLANS AND WALL TYPES

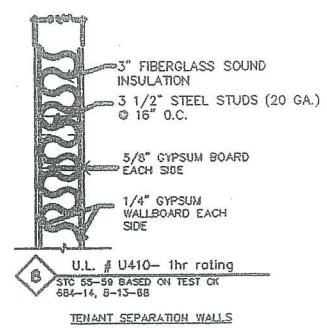
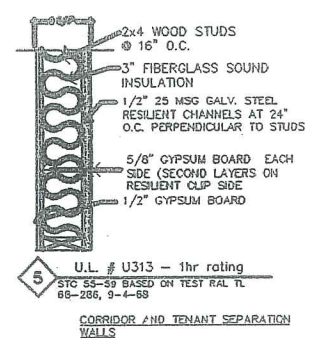
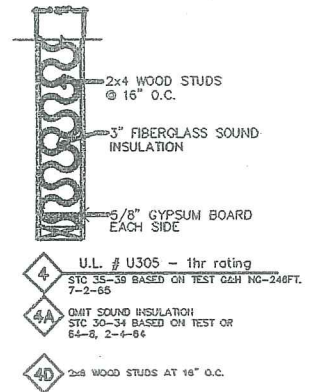
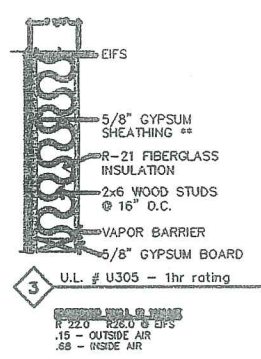
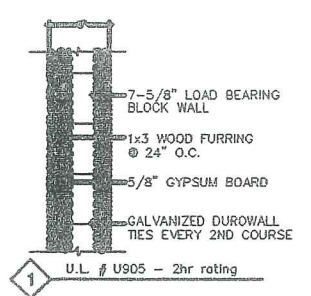
GROUP ONE INCORPORATED
 21 W. THRD STREET, BOSTON, MA 02227
 TEL: (617)268-7000 FAX: (617)268-0208

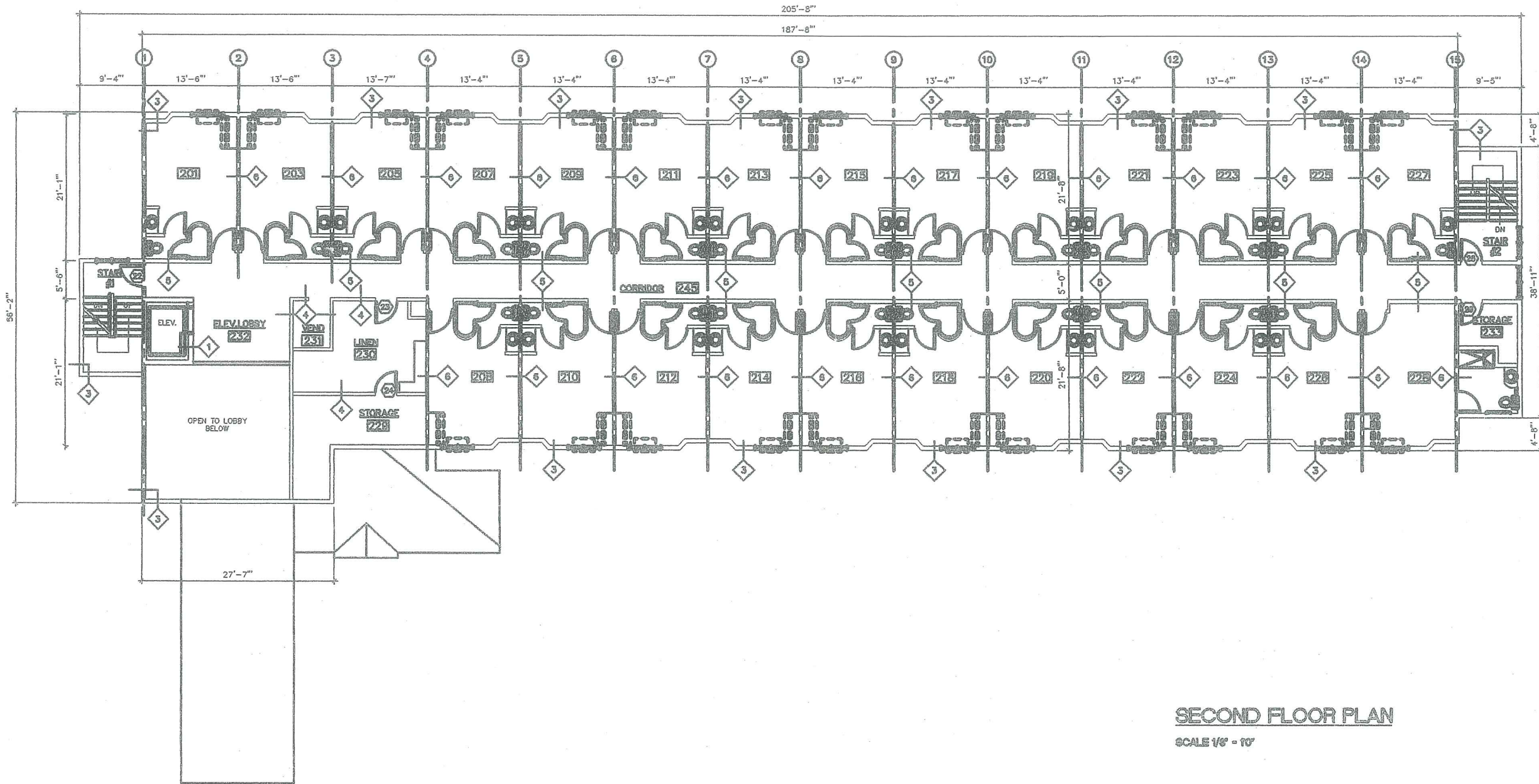
DRAWING NO.

A1

WALL TYPES










NTB





SECOND FLOOR PLAN
SCALE 1/8" = 1'-0"

LEGEND OF SYMBOLS

-  WALL TYPE NUMBER
SEE DWG. —
-  DOOR NUMBER
SEE DWG. —
-  ROOM NUMBER
SEE DWG. —
-  WINDOW TAG
SEE DWG. —
-  DETAIL OR WALL SECTION
NUMBER
DWG. NUMBER WHERE DETAIL
APPEARS
-  BUILDING SECTION
DESIGNATION
DWG. NUMBER WHERE
SECTION APPEARS
-  COLUMN LINE
-  INTERIOR ELEVATION
INDICATOR
ELEVATION NUMBER
DWG. NUMBER WHERE
ELEVATION APPEARS
-  DETAIL NUMBER
DWG. NUMBER WHERE DETAIL
APPEARS

FIRE EXTINGUISHER
FE - LARSEN FIRE EXTINGUISHER AND 1st FIRE RATED CABINET WITH VERTICAL BUD DOOR AND LARSEN-LOC. SEMI-RECESSED WITH 3 1/2" PROJECTION MAX.

PROGRESS 4-1-00

NO.	REVISION	DATE

SCALE: 1/8" = 1'-0" DRAWN BY: jo
DATE: 4/1/00 CHECKED BY: RCS

PROJECT

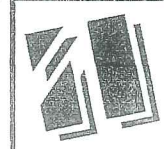
SLEEP INN
PORTLAND, ME

JC26 488

TITLE

SECOND FLOOR PLAN










GROUP ONE INCORPORATED
21 W. THIRD STREET, BOSTON, MA 02127
TEL: (617)222-7000 FAX: (617)222-0200



DRAWING NO.

A2

7e
LEGEND OF SYMBOLS

-  WALL TYPE NUMBER
SEE DWG. ___
-  DOOR NUMBER
SEE DWG. ___
-  ROOM NUMBER
SEE DWG. ___
-  WINDOW TAG
SEE DWG. ___
-  DETAIL OR WALL SECTION
NUMBER
DWG. NUMBER WHERE DETAIL
APPEARS
-  BUILDING SECTION
DESIGNATION
DWG. NUMBER WHERE
SECTION APPEARS
-  COLUMN LINE
-  INTERIOR ELEVATION
INDICATOR
ELEVATION NUMBER
DWG. NUMBER WHERE
ELEVATION APPEARS
-  DETAIL NUMBER
DWG. NUMBER WHERE DETAIL
APPEARS

FIRE EXTINGUISHER
FE - LARSEN FIRE EXTINGUISHER AND 1hr FIRE RATED
CABINET WITH VERTICAL DUG DOOR AND
LARSEN-LOC. SEMI-RECESSED WITH 3 1/2"
PROJECTION MAX.

PROGRESS 4-1-00

NO.	REVISION	DATE

SCALE: 1/8"=1'-0" DRAWN BY: jo
DATE: 4/1/00 CHECKED BY: RCS

PROJECT

SLEEP INN
PORTLAND, ME

JOB# 486

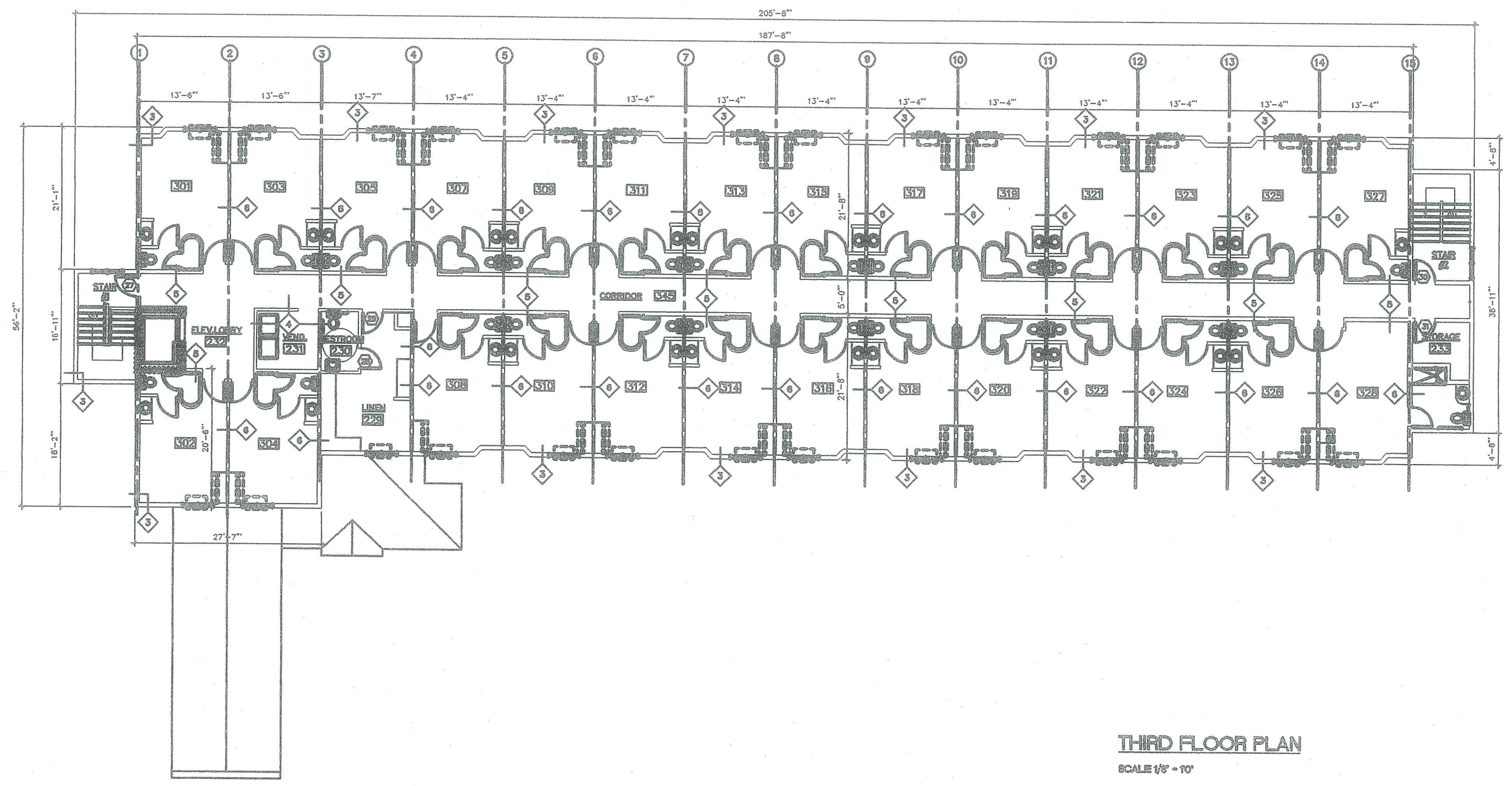
TITLE

**THIRD FLOOR
PLAN**

GROUP ONE INCORPORATED
21 W. THIRD STREET, BOSTON, MA 02127
TEL: (617)268-7000 FAX: (617)268-0203

DRAWING NO.

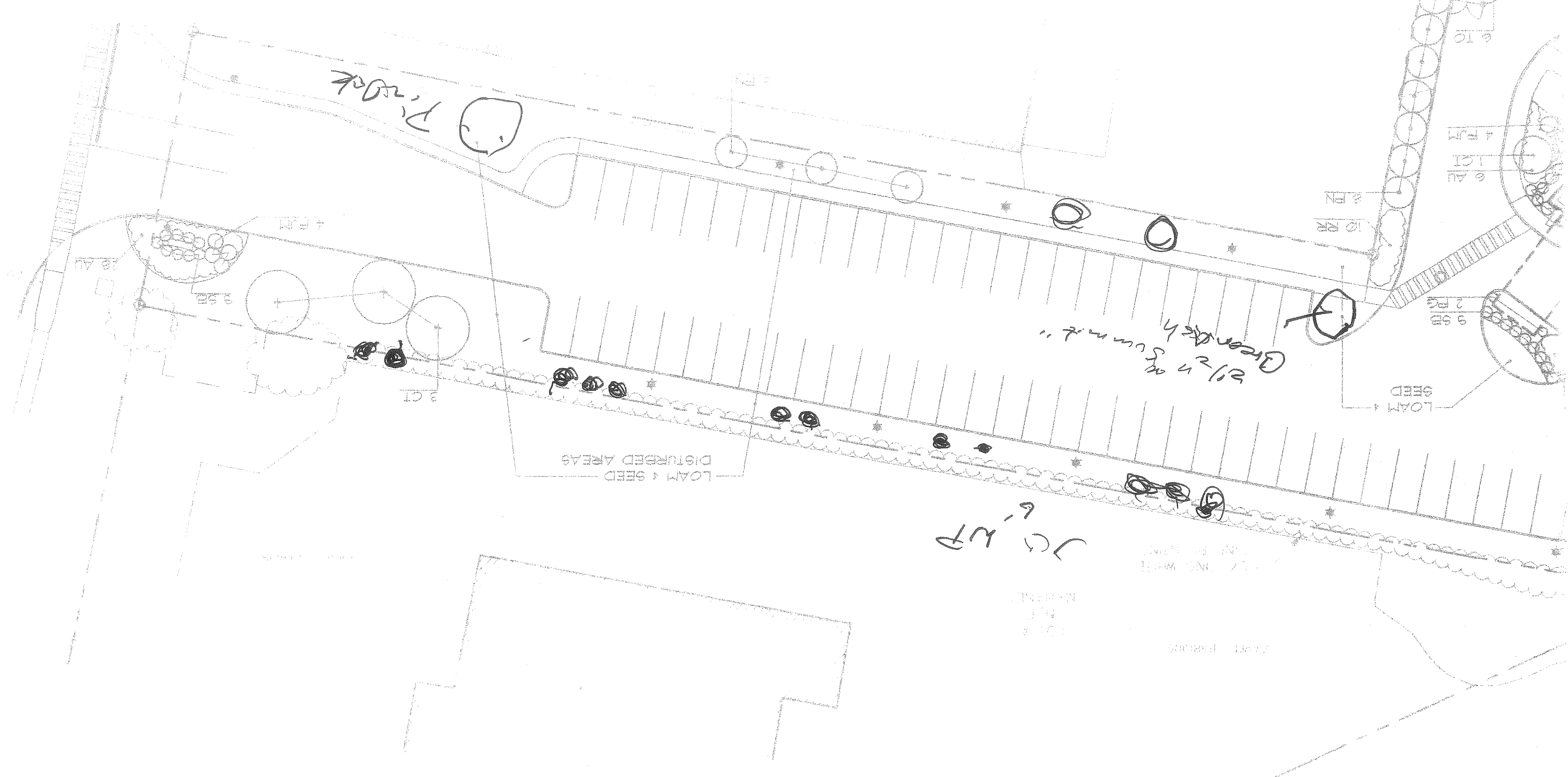
A3



THIRD FLOOR PLAN
SCALE 1/8" = 1'-0"

- 12 River Birch
- 51 Austrian Pine
- 15 Rubrum Rugosa Rose
- 15 Hick's Yew
- 24 Crispa Spirea
- 18 Dark American Arborvitae
- 51 Stella D'ora Daylily
- NC New Patriot Rhododendron
- 24 New Patriot Rhododendron
- 4 Bearberry
- 18 Bar Harbor Juniper
- 31 Dwarf Alberta Spruce
- 2-1 Toba Hawthorns

- TA Betula nigra
- IZ Pinus nigra
- TR Rosa Rugosa 'Rubrum'
- TI Taxus x media 'Hickell'
- SB Spiraea x bumalda 'Crispa'
- TO Thuja occidentalis 'Nigra'
- DL Hemerocallis 'Stella D'ora'
- PJM Rhododendron 'New Patriot'
- AU Arctostaphylos uva-ursi
- JC Juniperus horizontalis 'Bar Harbor'
- RG Picea glauca 'Conica'
- CT Crataegus 'Toba'



D.C. KP
6' KP

10 TR
M. H. S.
M. H. S.

10 TR
M. H. S.
M. H. S.

LOAM & SEED

2 1/2 m Summer Greenhouse

DISTURBED AREAS
LOAM & SEED AREAS

Pine

6 IO
4 PJM
1 CT
6 AU

10 TR
8 PN

9 SB
2 PJM

4 PJM

9 SB

3 CT

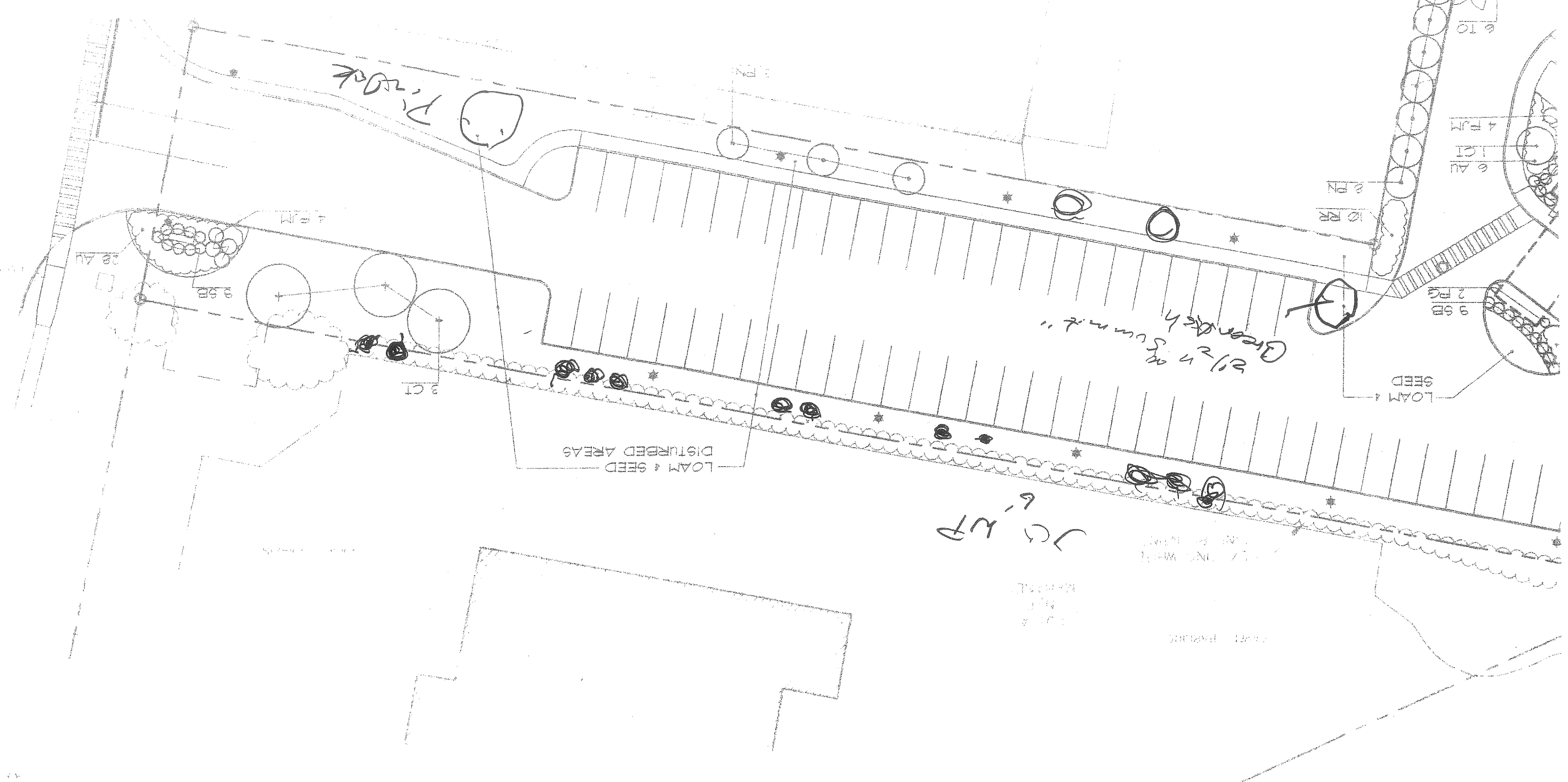
26 AU

10
5
15
24
12
5
No
24
9
18
3
2-

- River Birch
- Austrian Pine
- Rubrum Rugosa Rose
- Hick's Tree
- Crispa Spirea
- Dark American Arborvitae
- Stella D'ora Daylily
- New Patriot Rhododendron
- Bearberry
- Bar Harbor Juniper
- Dwarf Alberta Spruce
- Toba Hawthorne

- Pinus nigra
- Rosa Rugosa 'Rubrum'
- Taxus x media 'Hickall'
- Epiraea x bumaliba 'Crispa'
- Thuja occidentalis 'Nigra'
- Hemmerocallis 'Stella D'ora'
- Rhododendron 'New Patriot'
- Arctostaphylos uva-ursi
- Juniperus horizontalis 'Bar Harbor'
- Picea glauca 'Conica'
- Crataegue 'Toba'

- PI
- RA
- TI
- SB
- TO
- DL
- PJM
- AU
- JC
- PG
- CT



6' x 12'

2 1/2 in diameter
Greenhouse

LOAM & SEED
DISTURBED AREAS

LOAM &
SEED

Pink

2 1/2 in diameter
Greenhouse

6 TO
4 PJM
1 CT
6 AU
2 PN
10 RR
9 SB
2 PG

