



Memorandum
Planning and Urban Development Department
Planning Division

To: Carol Morrissette, Chair and Members of the Portland Planning Board
From: Shukria Wiar, Planner
Date: May 4, 2012
Re: Patient Care Center, College of Dental Medicine, One College Avenue, University of New England
Project #: 2012-483 **CBL:** 145-B-024
Meeting Date: May 8, 2012

I. INTRODUCTION

University of New England (UNE) is seeking to establish a new College of Dental Medicine on its Westbrook College Campus on Stevens Avenue in Portland. The program would be a professional four-year degree program with years one through three on the Westbrook College Campus and years four will be out on rotations in other parts of the State. The program will accommodate forty-six (46) students per class per year for a total of 138 additional students and faculty. The site is located in the Residential R-5 zone.

The applicant is proposing two parking lots to be located on the campus, one adjacent to the new Patient Care Center and a remote parking lot on the lower portion of the newly acquired Pike Industrial parcel. The remote parking lot will be accessed by a driveway that would follow the alignment of the existing driveway at the end of College Street.

The site plan proposal will be reviewed by the Planning Board under the Level III site plan standards and conditional use regulations for an institutional expansion in the Residential R-5 zone.

II. PROJECT DATA

Existing Zoning:	Residential R-5
Existing Use:	UNE is proposing a two-story Patient Care Center building on their Westbrook College Campus on Stevens Avenue.
Parcel Size:	14 acres on main campus; 19 acres for the Pike Industrial parcel
Impervious Surface Area on Main Campus:	
Existing:	310,300 sq. ft.
Proposed:	314,700 sq. ft.
Net Change:	4,400 sq. ft.
Total Disturbed Area:	37,000 sq.ft.
Building Area:	
Existing Building Area:	10,000 +/- (the total building area of the three houses)

Proposed Total Building Area: 39,500 sq. ft.
Proposed Building Net Change: 29,500 sq. ft. +/-
Existing Building Footprint: 4,459 sq. ft.
Proposed Building Footprint: 18,200 sq. ft.

Residential Data:

Existing Residential Units: One (1)
Proposed Residential Units: None
Number of Units to be Demolished: One (1)

Parking Spaces:

Existing: 467 spaces
Proposed: 270 spaces

Bicycle Parking Spaces:

Existing: Zero on proposed site
Proposed: 9

Estimated Cost of Project:

\$9 Million

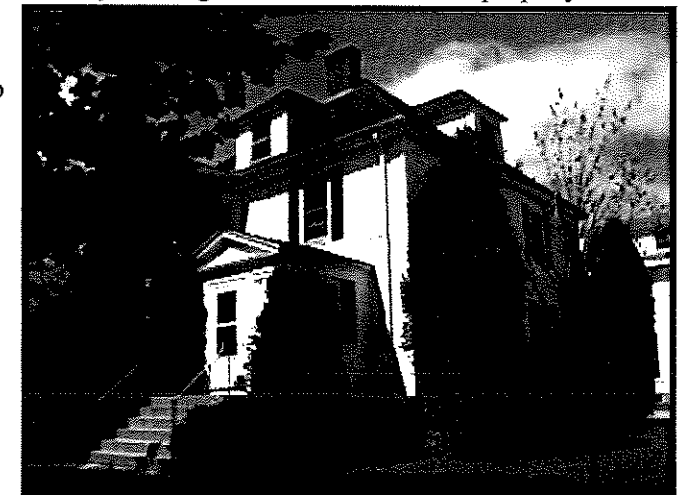
Uses in Vicinity:

The campus is surrounded by Evergreen Cemetery on the west, residential properties on the east, institutional uses on the north and south. The armory and a church are located on the north, and Catherine McAuley High School and the St. Brigid School are located to the south.

III. EXISTING CONDITIONS

The site within the main campus is abutted to the north by the Armory, to the south by College Street and University property, to the east by Stevens Avenue, and to the West by existing UNE structures. The property is within 100 feet of the Westbrook College Historic District.

Currently there are three buildings on the proposed site. Two of these buildings are being used as administration space and were formerly homes. The third house, a single family, is being leased out to graduate students. There are parking spaces to the north of the proposed site, which will be re-pave and re-striped. The proposal is to remove the three existing structures along Stevens Avenue and construct the Patient Care Center, which will provide additional classroom space and patient care facilities to serve the new College of Dental Medicine. The proposed demolition of the one single family house, as confirmed by the City records, does not trigger the housing replacement ordinance.



1 College Street



746 Stevens Avenue



750 Stevens Avenue

IV. PROPOSED DEVELOPMENT

The proposed project includes the construction of a two-story building, see Attachment B.9 and B.10. This new building will house the educational components and patient care facilities necessary for UNE's new Dental Medicine program. The program component includes the construction of a 36,000 square foot Patient Care Center for the proposed dental school at the corner of Stevens and College Avenues. The building will have entrances to the south (from the main campus) and north (from the parking lot) of the building. The south entrance will serve students, faculty and resident dentists; this entrance addresses the pedestrians from the main campus. The north entrance will be the access point from the parking lot. The façade facing Stevens Avenue includes fenestration along the lower level and glazing for two stories at the northeast corner of the building, which has the building entrance facing the north parking area. There are no proposal entrances along Stevens Avenue.

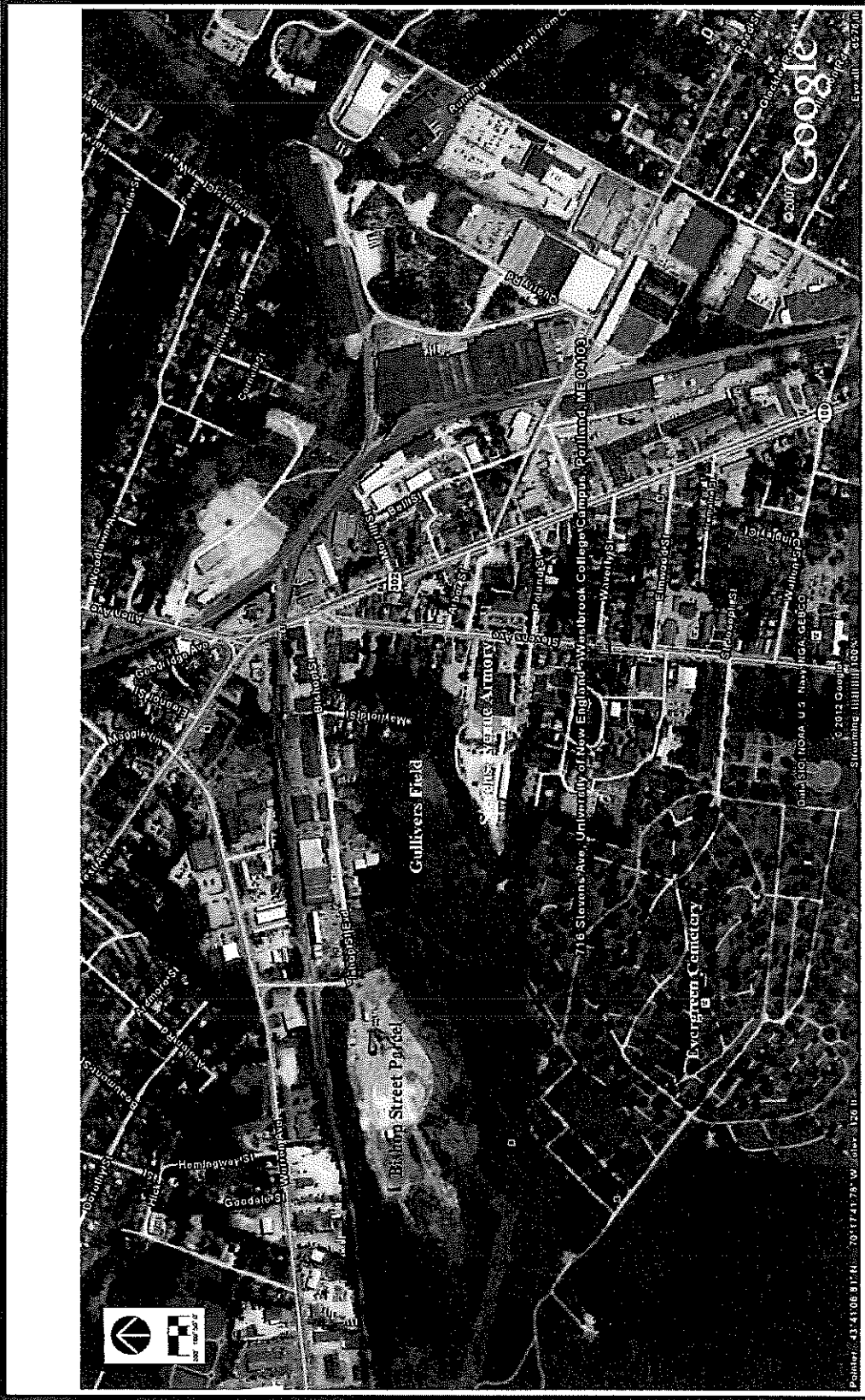
The building will be two-story with several large windows that will bring in natural light, and pitched roofs that will add definition to the elevations of the building. The facade materials will be a combination of brick, precast concrete and cementitious panels. The south entrance will be an atrium area.

The applicant's narrative starts:

Due to its location on Stevens Avenue, and the demographics of its program, it is important this project address its connection to the existing campus, and a connection to its public use.

Thus, the University has chosen the proposed location for the building on the main campus rather than having the structure and the program located on the former Pike Industrial property.

The Patient Care Center serves as a teaching clinic and the clinic is primary use on the first floor of the proposed building. Also included on the first floor are the patient simulation and teaching areas, which are mainly for the use of the first and second year students.



UNIE UNIVERSITY OF
NEW ENGLAND

Campus Locale Plan 2012

The second floor of the building consists of the comprehensive care areas. This area is specific to upper-level students that work on actual patients rather than stimulators. The specialty care area is also on the second floor that consists of exams, imaging (X-rays) and urgent care facilities for the use of the graduate students and resident dentists.

The proposed College of Pharmacy building has 39,500 square feet with a footprint of 18,200 sq ft.

<i>Floor Levels</i>	<i>Square Footage</i>
Lower Level- Mechanical / Support	3,100 sf
First Floor- Patient Care Center	18,200 sf
Second Floor- Specialty Care Area	18,200 sf

The applicant is proposing two parking lots to meet the demand of the new students, faculty and patients for the Dental Medicine program. One lot is proposed on the campus (adjacent to the new Dental Medicine Building) and a remote parking lot is proposed on the lower portion of the newly acquired Pike Industrial parcel ([Attachment B.2 and B.9](#)). The remote parking lot will be accessed by a driveway that would follow the alignment of the existing drive at the end of College Street. The remote parking will have a shutter bus or van running between the main campus and the Pike Industrial parcel, please refer to [Attachment A.16](#) for additional details.

The student population projections are in the following figure and incorporate the projections for the proposed Dental School. The student population is expected to grow annually from 818 students enrolled for the 2011-12 academic year to 1189 in 2016-17.

The anticipated student population (FTE) on campus over the next five years is the following:

	2011-12	2012-13	2013-14	2014-15	2015-16	2016-17
Student Projections	818	941	1097	1143	1189	1189

The layout and engineered design of the lot are in a conceptual stage since Pike Industries is in the process of removing equipment and survey work will be conducted upon completion of their closeout of the site. Please see [Attachment A.16](#) for additional details.

V. PUBLIC COMMENT

A Neighborhood Meeting will need to be held by the applicant. As date of this memo, no public comments were submitted.

VI. RIGHT, TITLE AND INTEREST AND FINANCIAL/TECHNICAL CAPACITY

a. The owner of the property is University of New England. The applicant has provided a copy of a warranty deeds, recorded at the Cumberland County Registry of Deeds, which demonstrates their right, title and interest in the property.

b. The estimated cost of the development is \$9 Million. The applicant has submitted a letter from Nicole Labbe-Trufant, Vice President for Fiscal Affairs for the University, dated March 30, 2012, as demonstration of their financial and technical capacity to complete the proposed development.

VII. ZONING ASSESSMENT

A. ZONING REVIEW (Marge Schmuckal)

Marge Schmuckal, Zoning Administrator has prepared a preliminary review of the plans and is seeking additional information in order to address zoning compliance. Her comments are as follows:

This project is in the R-5 Zone which allows university/college uses under a conditional use appeal to the Planning Board. The conditional use has separate requirements for university/colleges over 10 acres in size which I believe this project could be meeting. The site plan for the new building is covering the buildings to be demolished and is very difficult to read. Property lines are also a little difficult to read, although some setbacks are shown on the plan. It should be fleshed out more. I was also aware that some structural details may be encroaching on the setbacks. Those details may be allowable under zoning, but I would first like to see what those architectural features are before I make a final decision on this.

Parking requirements were not fully addressed within this application. I am aware that there is a proposal under the master plan to provide parking as part of an intermodal transportation facility on the old Pike Industries lot. No specific plans have been submitted.

I am expecting more information in the future to review before any final determinations on this project.

B. CONDITIONAL USE STANDARDS

The proposed development has been reviewed by staff for conformance with the relevant Conditional Use standards of Portland's Zoning Ordinance and Section 14-474.

1. Specific Review Criteria

The following standards apply for review of an institutional expansion in the R-5 zone, Section. 14- 118 (b) (6):

- a. In the case of expansion of existing such uses onto land other than the lot on which the principal use is located, it shall be demonstrated that the proposed use cannot reasonable be accommodated on the existing site through more efficient utilization of land or building, and will not cause significant physical encroachment into established residential areas.

Staff Comment: The placement of the addition will be within the school's property and the proposal expansion of the University onto the former Pike Industries property is not within a residential neighborhood.

- b. The proposed use will not cause significant displacement or conversion of residential uses existing as of June 1, 1983, or thereafter.

Staff Comment: Two of the buildings proposed to be demolished, 1 College Street and 746 Stevens Avenue, are offices and administrative support space as confirmed in the City's records. The third building at 750 Stevens

Avenue is a single family house, thus the College of Dental Medicine building and accompanying improvements will cause the displacement of one residential unit. This unit has been used by the University since the 1950's hence it has not been in the general housing stock. The loss of one unit does not cause significant displacement.

*Requires
a
Board
Finding*

- c. In the case of a use expansion which constitutes a combination of the above-listed uses with capacity for concurrent operations, the applicable minimum lot sizes shall be cumulative.

Staff Comment: The applicable lot sizes may be met but Ms. Schmuckal has requested more information from the applicant.

g. A college, university or trade school may build principal structures to a height of fifty-five (55) feet, not including the USM Overlay Zone, if the following standards can be met:

- (i) Lot size: 10 acres which may include adjacent land owned by the institution on both sides of a public street.

Staff Comment: The main campus site is 14 acres with a total of 61 acres for the entire campus with the recent acquisition of the pike Industries property.

- (ii) Minimum setback between buildings on-site: 20 feet.

Staff Comment: More information is requested of the applicant to determine if this is being met.

- (iii) Minimum setback from external property boundary: 30 feet, except that parking garages over 35 feet in height must be located 50 feet from external property boundaries when adjacent to an adjoining residential use.

Staff Comment: More information is requested of the applicant to determine if this is being met.

- (iv) The area between the structure and adjoining residential uses must be adequately screened with appropriate landscaping or other features to buffer the building and effects thereof (i.e. noise, light, etc.) from abutting properties.

Staff Comment: More information is requested of the applicant to determine if this is being met.

2. The following standards apply for all conditional uses, Section. 14-474(2), General Conditional Use Standards:

- a. There are unique or distinctive characteristics or effects associated with the proposed conditional use.

Staff Comment: There are no known unique or distinctive characteristics associated with the proposed use.

- b. There will be an adverse impact up on health, safety or welfare of the public or surrounding area.

Staff Comment: It does not appear that there will be any adverse impacts associated with the proposed project.

- c. Such impact differs substantially from the impact which would normally occur from such a use in that zone.

Staff Comment: The impacts of this site are similar as those normally expected from such a use in this zone.

VIII. DEVELOPMENT REVIEW

A. SITE PLAN SUBMISSION REQUIREMENTS (Section 14-527)

B. SITE PLAN STANDARDS (Section 14-526)

The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and applicable regulations. Staff comments are listed below.

1. Transportation Standards

The applicant is proposing two parking lots to meet the demand of the new students, faculty and patients for the Dental Medicine program. One lot is proposed on the main campus (adjacent to the new Dental Medicine Building) via an existing curb cut and a remote parking lot on the lower portion of the newly acquired Pike Industrial parcel (Attachment B.2 and B.9). The remote parking lot will be accessed by a driveway that would follow the alignment of the existing drive at the end of College Street. The remote parking will have a daily shuttle running between the proposed building and the Pike Industrial parcel.

In Section 14-332 (e) states that:

Schools providing instruction for students sixteen (16) years of age and over: One (1) parking space for each ten (10) seats or major fraction thereof, used for purposes of instruction; if no fixed seats, one (1) parking space for each one hundred (100) square feet or major fraction thereof used for purposes of instruction.

The parking demand for this new building is the following:

<i>Total New Students for the Dental Medicine Program</i>	<i>138 (46/ year over 3 years)</i>
Parking Demand for New Students	93 spaces
Parking Demand for IPE	10 spaces
Parking Demand for Dental Clinic	21 spaces
Total new parking demand	124 spaces

Of the 124 parking spaces required, thirty-three (33) parking spaces will be provided at the existing parking lot north of the proposed building permit. The twenty-one (21) spaces will be striped and these combined with the 12 leased spaces at the armory site will be utilized by patients. A note to the Board that the 20 parking spaces were part of the parking assessment for the College of Pharmacy and these spaces provide parking for existing users and will be displaced. In addition to the 124 spaces for the program, these 20 spaces will need to be considered. Tom Errico, Consultant Traffic Engineer, has reviewed the proposal and requests that the applicant provide information on the displacement of existing users. The applicant is also proposing a remote parking lot on the Pike's parcel that consists of nineteen (19) acres. UNE has undertaken preliminary studies and had several discussions with the Planning Division staff regarding the use of this property for parking. A shuttle service to the campus would be provided via the existing access road. All of the traffic associated with the proposed building will be on the campus. Mr. Errico had requested that

Further details need to be provided on the provision of the Pike satellite parking lot as it relates to shuttle

bus operations. My final conclusion will be presented in the future.

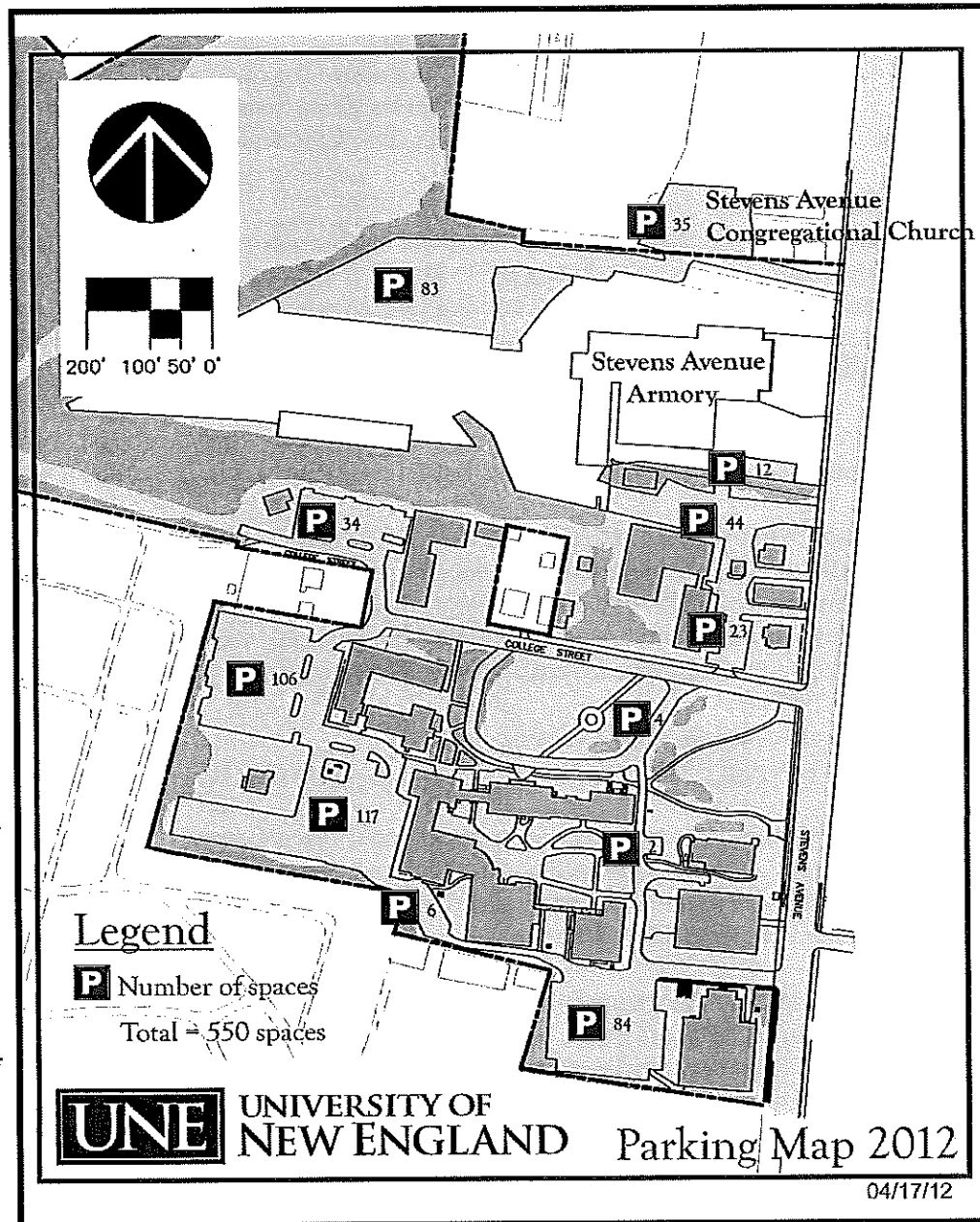
UNE has rental agreements with the State of Maine Armory for 12 spaces and with the Stevens Avenue Congregational Church for another 35 spaces, which is a total of 467 available for the current programs on site. In addition, UNE secured a lease from the Armory to lease additional parking space behind the building, which provides approximately 80 spaces that became available in January of 2012. UNE states that they may seek site plan approval for an expansion of the parking area (increasing the impervious area) to accommodate an additional 30 spaces.

Traffic Movement Permit

As part of the College of Pharmacy project, a Traffic Movement Permit was obtained. The total trips generated as part of this project is sixty-nine (69) trips. A Traffic Movement Permit (TMP) is required when the net new trips exceeds 100 trip in a peak hour. Since the total trips are less the required amount, an amendment to the TMP is not required. Mr. Errico concurs with the estimate of the new traffic volume generation and that the project will not require an amended Traffic Movement Plan.

Traffic Demand Management Plan

The UNE subsidizes the free use of the Metro to all its students and staff. According to the parking assessment, this program averages 232 rides per month in 2008, 462 rides per month in 2009 and 653 rides per month in 2011. Please refer to [Attachment A.15](#) for a complete review of the parking analysis for this project as well as the College of Pharmacy.



The following are mitigation options proposed by the applicant to decrease the parking demand on its Portland Campus:

1. Seek additional parking space rental opportunities proximal to the campus, and encourage UNE Community members to utilize these spaces;
2. Seek satellite parking options;
3. Construct new parking spaces on campus;
4. Purchase/acquisition of properties proximal to the campus and construction of parking spaces;
5. Encourage alternate transportation modes (mass transit, car-pooling, bicycling, etc.); and
6. Employ other Parking Demand Management Strategies.

Proposed Drop-Off Area

The applicant is proposing modifications along Stevens Avenue, at the entrance of the proposed building, to provide a drop off area (Attachment B.2). The drop-off area is intended for the use of private (patients) and commercial vehicles. This area will prohibit long term parking. According to David Margolis-Pineo, Deputy City Engineer, this drop-off area will eliminate parking and may need City Council approval. Mr. Errico continues to review this proposal and requests the following of the University:

...feedback on providing the drop-off space along the existing curb line and restrict usage. I would note that depending upon the outcome of this issue, changes to parking regulations may require City Council action.

Proposed Crosswalk on College Street

The applicant is proposing a new diagonal crosswalk on College Street. The City prefers a perpendicular alignment and Mr. Errico will need to review this more in detail. He has requested that a "summary of pedestrian origin and destination movement to and from the proposed building, and how these movements will be accommodated". The proposed crosswalk will need to be reviewed by the Crosswalk Committee.

2. Environmental Quality Standards

a. Preservation of Significant Natural Features/ Landscaping and Landscape Preservation

b. Water Quality, Storm Water Management and Erosion Control

Capisic Brook is also located on this parcel and runs through the lower campus. Capisic Brook has been designated by the MaineDEP as an urban impaired stream under Chapter 502 of the Maine Stormwater Management Rules. Runoff from the remainder of the site will enter the storm drain systems within College Street and Stevens Avenue.

The proposed dental medicine building is located on an existing developed lot. The Applicant proposes infiltration of rooftop runoff. Runoff from a portion of this site will enter the closed storm drain systems within College Street and Stevens Avenue enters an enclosed stormwater system via overland flow. These storm drain systems ultimately discharge to Back Cove. Therefore, runoff from the project site does not enter the Capisic Brook Watershed (an Urban Impaired Stream watershed). As such, this project is not subject to meeting the Urban Impaired Stream standard.

David Senus, Consultant Engineer with Woodard and Curran, has reviewed the proposal and a stormwater management plan pursuant to MaineDEP Chapter 500 guidance to address conformance with the Basic, General and Flooding Standards. Please refer to Mr. Senus memorandum, dated April 26, 2012 for a full review comments, Attachment 3.

3. Public Infrastructure and Community Safety Standards

a. Consistency with Master Plans

The City Sidewalk Materials Policy calls for concrete sidewalks along Stevens Avenue in front of the proposed building site and asphalt on college Street. David Margolis-Pineo, Deputy City Engineer, states in his memorandum, dated May 2, 2012 that "since it appears that less than half of the sidewalk will not be disturbed, the applicant may choose to retain Brick sidewalks.

b. Public Safety and Fire Prevention

Capt. Chris Pirone of Fire Prevention reviewed the application and the following are request of the University:

1. Hydrants are to be located within 100' of Fire Department Connections but no closer than 40'.
2. All construction shall comply with City Code Chapter 10.

c. Availability and Adequate Capacity of Public Utilities

The proposed building will connect to the existing sanitary sewer system in Stevens Avenue. Water service, as well as gas service will be via an existing service in Stevens Avenue. Electricity to the line will be via existing underground connections on Stevens Avenue. The applicant will need to submit the final capacity letters for the proposed utilities.

4. Site Design Standards

a. Massing, Ventilation, Wind Impact and Shadows

Please refer to Paragraph IV for the description of the building. The building's massing and material used will reflect the surrounding campus.

b. Snow and Ice Loading

The applicant will need to show the snow loading area on the site plan or submit how it is be handled if removed from the site.

c. Historic Resources

In order to aid the Board in its deliberations, historic preservation staff shall provide a written analysis of the proposed development. This information will be available for the public hearing.

d. Exterior Lighting

The applicant has not submitted a lighting and photometric plan. A plan needs to be submitted that shows the locations of proposed lighting fixtures. The catalogue cuts will also need to be submitted for review and approval by the Planning Division; these fixtures will be cut-off.

e. Noise and Vibration

Mechanical equipment, such as HVAC, is being proposed with the building. The applicant will need to submit catalogue cuts, as well as noise decibels for review. The applicant will be responsible for any kind of mitigation if the mechanical units do not meet the standards.

f. Signage and Wayfinding

The applicant will need to show all the traffic signage and wayfinding on the site plans.

g. Zoning Related Design Standards

Currently there are no design guidelines in the R-5 therefore this building is not subject to review under the City's design guidelines.

IX. NEXT STEPS

At the workshop on UNE's Master Plan, The Board requested a site walk. The staff is seeking clarification on whether a site walk should be scheduled prior to a public hearing for the Patient Care Center as part of the Master Plan discussion. If the Board seeks to have a site walk before a decision on the Patient Care Center, please come prepared to discuss possible dates for such a meeting.

1. Address any additional information requested by the Planning Board
2. Provide the updated information requested in this memorandum.
3. Address technical issues raised in this Planning Board memorandum and the comments received from staff reviewers.
4. Stormwater Management Plan needs be submitted

Attachments:

Planning Board Report Attachments

1. Marge Schmuckal Memorandum, Dated 04.26.2012
2. Tom Errico Memorandum, Dated 05.02.2012
3. David Sensus Memorandum, Dated 05.02.2012
4. David Margolis-Pineo Memorandum, Dated 04.26.2012
5. Capt. Chris Pirone memorandum, Dated 05.04.2012

A - Applicant's Submittal

- A.1 Cover Letter
- A.2 Application for Site Plan
- A.3 Application for Conditional Use
- A.4 Letter of Agent Authorization
- A.5 Project Description
- A.6 Deed Neighborhood Meeting Information
- A.7 Zoning Compliance and Applicable Design Standards Narrative
- A.8 Traffic
- A.9 Financial Capacity
- A.10 Utility Correspondence
- A.11 Fire Safety Summary and City Technical Manual, Chapter 3 Compliance
- A.12 Stormwater Management Plan
- A.13 Solid Waste
- A.14 Construction Management Plan
- A.15 Submittal for Conditions of Approval for College of Pharmacy
 - A.15 - Application Cover Letter
 - A.15.1 - Attachment A
 - A.15.2 - College of Dental Medicine Program

- A.15.3 - Attachment B Parking Demand Assessment
- A.15.4 - Stormwater Management Plan
- A.16 Parking Plan for Patient Care Center

B – Site Plan

- B.1 Existing Conditions Plan
- B.2 Site Plan
- B.3 Grading and Drainage Plan
- B.4 Utility Plan
- B.5 Landscaping Plan
- B.6 Erosion Control and Site Details
- B.7 Site Details
- B.8 Site Details
- B.9 Conceptual Parking Plan
- B.10 Street Renderings of North and South Elevation
- B.11 Street Renderings of East and West Elevation

Shukria Wiar - 1 College Ave

From: Marge Schmuckal
To: Shukria Wiar
Date: 4/26/2012 4:47 PM
Subject: 1 College Ave

One Solution does not have a place for me to zoning comments at this time.

1 College Avenue - 145-B-042
2012-483 R-5 Zone
4/26/2012

This project is in the R-5 Zone which allows university/college uses under a conditional use appeal to the Planning Board. The conditional use has separate requirements for university/colleges over 10 acres in size which I believe this project could be meeting. The site plan for the new building is covering the buildings to be demolished and is very difficult to read. Property lines are also a little difficult to read, although some setbacks are shown on the plan. It should be fleshed out more. I was also aware that some structural details may be encroaching on the setbacks. Those details may be allowable under zoning, but I would first like to see what those architectural features are before I make a final decision on this.

Parking requirements were not fully addressed within this application. I am aware that there is a proposal under the master plan to provide parking as part of an intermodal transportation facility on the old Pike Industries lot. No specific plans have been submitted.

I am expecting more information in the future to review before any final determinations on this project.

Marge Schmuckal
Zoning Administrator

Shukria Wiar - UNE - Patient Care Center

From: Tom Errico <thomas.errico@tylin.com>
To: "Wiar, Shukria" <SHUKRIAW@portlandmaine.gov>
Date: 5/2/2012 8:11 PM
Subject: UNE - Patient Care Center
CC: Katherine Earley <KAS@portlandmaine.gov>, David Margolis-Pineo <DMP@port...>

Shukria – I have reviewed the application materials for the project and offer the following preliminary comments.

1. I concur with the estimate of new traffic volume generation and the conclusion that the project will not require an amended Traffic Movement Permit.
2. Further details need to be provided on the provision of the Pike satellite parking lot as it relates to shuttle bus operations. My final conclusion will be presented in the future.
3. The existing parking lot abutting the proposed building currently provides parking for UNE. The applicant should provide information on the displacement of existing users.
4. My preliminary conclusion is that I concur with the parking generation estimate of needing an additional 124 parking spaces. I need to review this further before rendering my final conclusion.
5. I continue to review the need for the drop-off space on Stevens Avenue. The applicant should provide feedback on providing the drop-off space along the existing curb line and restrict usage. I would note that depending upon the outcome of this issue, changes to parking regulations may require City Council action.
6. The project illustrates a new crosswalk on College Street. The City prefers a perpendicular alignment and so I need to review this further. I would ask that the applicant provide a summary of pedestrian origin and destination movements to and from the proposed building and how these movements will be accommodated.

If you have any questions, please contact me.

Best regards,

Thomas A. Errico, PE
Senior Associate
Traffic Engineering Director
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TO: Shukria Wiar, Planner
FROM: Ashley Auger, E.I.T. & David Senus, P.E.
DATE: April 26, 2012
RE: UNE Patient Care Center, Level III Site Plan

Woodard & Curran has received the Final Level III Site Plan Application for the Patient Care Center project located at 1 College Street in Portland, Maine. The project includes a new 18,200 sf building, along with associated site improvements.

Documents Reviewed

Site Plan Application and attachments, dated April 17, 2012, prepared by Site Design Associates on behalf of the University of New England.

Comments

1. Basic Standards: The Applicant has provided sufficient erosion and sediment control notes and details; however, the site plan should show the location of sediment barriers, stabilized construction exit(s), and catch basin inlet protection. In addition, the Applicant must provide an Inspection and Maintenance Plan for their proposed stormwater management systems in accordance with Chapter 500 and City Code of Ordinances Chapter 32 guidance.
2. General Standards: In accordance with Section 5 of the City of Portland Technical Standards, the Applicant must submit a Stormwater Management Plan pursuant to MaineDEP Chapter 500 guidance to address conformance with the General Standards. The Applicant has not provided a Stormwater Management Plan that addresses runoff calculations; sizing of the infiltration system; soil data including soil classification at system subgrade depth, infiltration capacity at system subgrade depth, depth to groundwater, and presence of bedrock. We request that this additional information be provided so that we may perform a review of the stormwater management approach for the project. Additional detail must also be provided on the infiltration system, including sediment pretreatment, details showing the underdrain piping system between the drainage rings, and any overflow connections to the City's storm drain system.
3. Urban Impaired Stream Standard: The project site is situated at the intersection of Stevens Avenue and College Street. The Applicant proposes infiltration of rooftop runoff. Runoff from the remainder of the site will enter the storm drain systems within College Street and Stevens Avenue. These storm drain systems ultimately discharge to the Back Cove. Therefore, runoff from the project site does not enter the Capisic Brook Watershed (an Urban Impaired Stream watershed). As such, this project is not subject to meeting the Urban Impaired Stream standard.
4. Flooding Standard: The Applicant notes that a 2008 approval for the College of Pharmacy resulted in a net reduction of 6,000 sf of impervious area on the campus, and therefore the combination of the two projects will result in a net decrease of 1,600 sf of impervious area on the campus. A reduction of impervious area in 2008 does not count as credit toward meeting the stormwater requirements for this project. However, if the Applicant can show (through pre and post development hydrology and pipe flow calculations) that the net result of the two projects will decrease flow to the City's storm drain system, we would recommend a waiver from the Flooding Standard for this specific project (pending DPS review and approval).



5. In accordance with MaineDEP Chapter 500 guidance, infiltration from a stormwater infiltration system is considered *de minimis* and does not require an individual waste discharge license if the standards in Chapter 500, Appendix D are met. The applicant must provide a Stormwater Management Report that addresses how the proposed system will meet the Appendix D requirements, or the proposed system is required to be registered with and meet all other requirements of the MaineDEP's Underground Injection Control Program.
6. The Applicant proposes a private storm drain pipe crossing on College Street, a publicly owned right-of-way. The Applicant must receive approval from the City of Portland DPS and must obtain an easement from the City for this pipe crossing.
7. The Stormwater Management Plan should acknowledge Condition #2 of the December 1, 2011 UNE – Armory Parking Project Site Plan Conditions of Approval. This condition provides specific requirements for meeting the General Standards and Urban Impaired Stream Standard for the UNE – Armory Parking Project by providing quality treatment and mitigation of 7,343 sq. ft. of paved surface on campus or on the Armory Site.
8. City Standard Details should be added for all work proposed within the City Right-of-Way, including sidewalk, pipe trench, pavement repair, and curbing.

Shukria Wiar - UNE Review Comments

From: David Margolis-Pineo
To: Barbara Barhydt; Shukria Wiar
Date: 5/2/2012 10:41 AM
Subject: UNE Review Comments

May 2, 2012

TO: Shurkia Wiar
Barbara Barhydt
FROM: David Margolis-Pineo
RE: Review Comments: UNE Patient Care Center

The Department of Public Services has the following Preliminary comments.

1. Proposed "Drop Off" on Stevens Ave will eliminate parking and may need Council approval.
2. Note that the sidewalk material policy for Stevens Ave is concrete and College St. is asphalt. Since it appears that less than half of the sidewalk areas will not be disturbed, the applicant may chose stay with brick walks.
3. The proposed 12" drain line under College St. shall meet City of Portland's Technical Manual pipe material standards.

Shukria Wiar - Re: UNE -College of Dental Medicine

From: Chris Pirone
To: Shukria Wiar
Date: 5/4/2012 12:16 PM
Subject: Re: UNE -College of Dental Medicine

I am sorry if its too late.

Fire Comments:

- 1.Hydrants are to be located within 100' of Fire Department Connections but no closer than 40'.
- 2.All construction shall comply with City Code Chapter 10.

Captain Chris Pirone
Portland Fire Department
Fire Prevention Bureau
380 Congress Street
Portland, ME 04101
(t) 207.874.8405
(f) 207.874.8410

>>> Shukria Wiar 5/3/2012 9:37 AM >>>
Hello:

Could you please forward your comments to be for the UNE project at 1 College Street.

My report needs to go out Friday morning; I would like to wrap up the Planning Board memo by today.

Thanks.

Shukria

Applicant's Submittal

**Application for Site Plan
And Conditional Use Approvals**

Project:

Patient Care Center

*1 College Street
Portland, Maine*

Applicant:

University of New England
11 Hills Beach Road
Biddeford, Maine 04005

April 17, 2012

Prepared By:

Site Design Associates

**23 Whitney Way
Topsham Maine 04086**

A.1

Site Design Associates
Consulting Engineering and Land Planning

April 17, 2012

Ms. Barbara Barhydt
Development Review Services Manager
Planning Division
389 Congress St., Fourth Floor
Portland, ME 04101

**RE: University of New England - Westbrook College Campus
Site Plan & Conditional Use Application for: Patient Care Center**

Dear Barbara:

On behalf of University of New England (UNE), Site Design Associates (SDA) is pleased to submit one original paper copy and one CD containing the files of the Site Plan Application and Conditional Use Application for the referenced project. We have enclosed a check in the amount of \$600 for the following project review fees:

Site Plan Application

- Major Development under 50,000 s.f. - \$500

Conditional Use Application

- Conditional Use - \$100

This submission contains the following information:

- Site Plan Application and Check List (Attachment 1)
- Conditional Use Application and Narrative(Attachment 2)
- Letter of Agent Authorization (Attachment 3)
- Project Description (Attachment 4)
- Deed (Attachment 5)
- Zoning Compliance and Applicable Design Standards(Attachment 6)
- Traffic (Attachment 7)
- Financial Capacity (Attachment 8)
- Utility Letters (Attachment 9)
- Fire Safety Summary and City Technical Manual, Chapter 3, Compliance (Attachment 10)
- Stormwater Management Plan (Attachment 11)
- Solid Waste (Attachment 12)
- Construction Management Plan (Attachment 13)
- The following project drawings(one full size and 1 reduced set):
 - C-100 Existing Conditions & Demolition Plan
 - C-101 Site Plan

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- C-102 Grading, Drainage, and Erosion Control Plan
- C-103 Site Utilities Plan
- C-104 Landscape Plan
- C-300 Erosion & Sedimentation Control Notes and Site Details
- C-301 Site Details
- C-302 Site Details
- Architectural Elevations
- Boundary Survey

We realize that there is some outstanding information which includes:

- Exterior Lighting Plan
- HVAC equipment certification

As noted in the application, the lighting will conform with ordinance standards and the requirements outlined in the Technical Manual. HVAC equipment will be high efficiency roof mounted gas fired boilers conforming with all state and federal regulations. This information is currently being developed and will be submitted as soon as it is available.

We are in hopes that you can place these applications on the next available Planning Board agenda so that we may introduce the project to the Planning Board.

We look forward to meeting with the Planning Board to discuss the Patient Care Center project. Please contact me with any questions or comments concerning these

Sincerely,

Site Design Associates



Tom Saucier, P.E.
President

cc: Alan Thibeault, UNE

A.2

PROJECT NAME: Patient Care Center

PROPOSED DEVELOPMENT ADDRESS:

1 College Street

PROJECT DESCRIPTION:

See Attachment 4

CHART/BLOCK/LOT: 145B042001, 145B046001 **PRELIMINARY PLAN** _____ (date)
FINAL PLAN 04-17-12 (date)

CONTACT INFORMATION:	Applicant's Contact for electronic plans Name: Tom Saucier, Site Design Associates e-mail: tsaucier@sitedesignassociates.biz work #207-449-4275
Applicant – must be owner, Lessee or Buyer Name: Alan Thibeault Business Name, if applicable: University of New England Address: 11 Hills Beach Road City/State :Biddeford, ME Zip Code: 04005	Applicant Contact Information Work #207-602-2253 Home# Cell # Fax# e-mail:athibeault@une.edu
Owner – (if different from Applicant) Name:Applicant Address: City/State : Zip Code:	Owner Contact Information Work # Home# Cell # Fax# e-mail:
Agent/ Representative Name: Tom Saucier, Site Design Associates Address: 23 Whitney Way City/State :Topsham, ME Zip Code: 04086	Agent/Representative Contact information Work #207-449-4275 Cell #207-756-0068 e-mail: tsaucier@sitedesignassociates.biz
Billing Information Name: Bill applicant Address: City/State : Zip Code:	Billing Information Work # Cell # Fax# e-mail:

A.2a

Engineer Name: Tom Saucier, Site Design Associates Address: 23 Whitney Way City/State : Topsham, ME Zip Code: 04086	Engineer Contact Information Work #207-449-4275 Cell #207-756-0068 Fax# e-mail: : tsaucier@sitedesignassociates.biz
Surveyor Name: , Site Design Associates Address: City/State : Zip Code:	Surveyor Contact Information Work # Cell # Fax# e-mail:
Architect Name: Port City Architecture Address: 65 Newbury Street City/State :Portland, ME Zip Code: 04101	Architect Contact Information Work #207-761-9000 Cell # Fax# e-mail:
Attorney Name: Address: City/State : Zip Code:	Attorney Contact Information Work # Cell # Fax# e-mail:

A.2b

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

Level III Development (check applicable reviews)	Fees Paid (office use)	Other Reviews (check applicable reviews)	Fees Paid (office use)
<input checked="" type="checkbox"/> Less than 50,000 sq. ft. (\$500.00) <input type="checkbox"/> 50,000 - 100,000 sq. ft. (\$1,000) <input type="checkbox"/> 100,000 - 200,000 sq. ft. (\$2,000) <input type="checkbox"/> 200,000 - 300,000 sq. ft. (\$3,000) <input type="checkbox"/> over \$300,00 sq. ft. (\$5,000) <input type="checkbox"/> Parking lots over 11 spaces (\$1,000) <input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)	<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	<input type="checkbox"/> Traffic Movement (\$1,000) <input type="checkbox"/> Stormwater Quality (\$250) <input type="checkbox"/> Subdivisions (\$500 + \$25/lot) <input type="checkbox"/> # of Lots ___ x \$25/lot = _____ <input type="checkbox"/> Site Location (\$3,000, except for residential projects which shall be \$200/lot) <input type="checkbox"/> # of Lots ___ x \$200/lot = _____ <input type="checkbox"/> Other _____ <input type="checkbox"/> Change of Use <input type="checkbox"/> Flood Plain <input type="checkbox"/> Shoreland <input type="checkbox"/> Design Review <input type="checkbox"/> Housing Replacement <input type="checkbox"/> Historic Preservation	<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>
The City invoices separately for the following: - Notices (\$.75 each) - Legal Ad (% of total Ad) - Planning Review (\$40.00 hour) - Legal Review (\$75.00 hour) Third party review is assessed separately.			
Plan Amendments (check applicable reviews) <input type="checkbox"/> Planning Staff Review (\$250) <input type="checkbox"/> Planning Board Review (\$500)	Fees Paid (office use) <input type="checkbox"/> <input type="checkbox"/>		

A.2c

APPLICATION SUBMISSION

All site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Submissions shall include one (1) paper packet with folded plans containing the following materials:

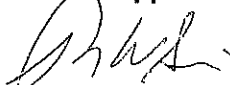
1. One (1) full size set of plans that must be folded.
2. One (1) copy of all written materials as follows, unless otherwise noted:
 - a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
3. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet.
4. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
5. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
6. One (1) set of plans reduced to 11 x 17.

Refer to the application checklist for a detailed list of submittal requirements.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level III Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: 	Date: 4-17-12
-------------------------------------------------------------------------------------------------------------------------	-------------------------

PROJECT DATA

A.2d

The following information is required where applicable, in order complete the application.

Total Area of Site	14 acres
Proposed Total Disturbed Area of the Site	37,000 sq. ft.
(If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)	
Impervious Surface Area	
Total Existing Impervious Area	310,300 (-6000 sf since COP in 2006) sq. ft.
Total Proposed Impervious/Paved Area	314,700 (-1600 sf since COP in 2006) sq. ft.
Net Change in Impervious Area	4,400 sq. ft.
Building Ground Floor Area and Total Floor Area	
Existing Building Footprint	4459 sq. ft.
Proposed Building Footprint	18,200 sq. ft.
Net Change in Building Footprint	13,741 sq. ft.
Existing Total Building Floor Area	10,000 +/- sq. ft.
Proposed Total Building Floor Area	39,500 +/- sq. ft.
Net Change in Building Floor Area	29,500 +/- sq. ft.
Zoning	
Existing	R-5
Proposed, if applicable	N/A
Land Use	
Existing	Institutional – College/University
Proposed	Same
Residential, if applicable	
Existing Number of Residential Units	N/A
Proposed Number of Residential Units	
Net Change in Residential Units	
Proposed Number of Lots	
Proposed Number of Affordable Housing Units	
Proposed Bedroom Mix:	
Number of Efficiency Units	
Number of One-Bedroom Units	
Number of Two-Bedroom Units	
Number of Three-Bedroom Units	
Parking Spaces	
Total Existing Number of Parking Spaces	See narratives
Total Proposed Number of Parking Spaces	
Net Change in Parking Spaces	
Number of Handicapped Spaces	
Bicycle Parking Spaces	
Total Existing Number of Bicycle Spaces	0
Total Proposed Number of Bicycle Spaces	9
Net Change	9
Estimated Cost of Project	\$9 million

**General Submittal Requirements – Preliminary Plan (Optional)
Level III Site Plan
Preliminary Plan Phase Check list (if elected by applicant)**

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirements
✓	<input type="checkbox"/>	1	Completed application form
✓	<input type="checkbox"/>	1	Application fees
✓	<input type="checkbox"/>	1	Written description of project
✓	<input type="checkbox"/>	1	Evidence of right, title and interest.
N/A	<input type="checkbox"/>	1	Copies of required State and/or Federal permits.
✓	<input type="checkbox"/>	1	Written assessment of proposed project's compliance with applicable zoning requirements.
N/A	<input type="checkbox"/>	1	Written description of existing and proposed easements or other burdens.
N/A	<input type="checkbox"/>	1	Written requests for waivers from individual site plan and/or technical standards, where applicable.
✓	<input type="checkbox"/>	1	Traffic analysis (may be preliminary, in nature, during the preliminary plan phase).
N/A	<input type="checkbox"/>	1	Written summary of significant natural features located on the site.
N/A	<input type="checkbox"/>	1	Written summary of project's consistency with related city master plans.
✓ pending	<input type="checkbox"/>	1	Neighborhood Meeting Material (refer to page 13 of this application.)
Applicant Checklist	Planner Checklist	Number of Copies	Site Plan Submittal Requirements
✓	<input type="checkbox"/>	1	Boundary Survey meeting the requirements of Section 13 of the City of Portland Technical Manual.
<input type="checkbox"/>	<input type="checkbox"/>	1	Preliminary Site Plan Including the following: (*information provided may be preliminary in nature during preliminary plan phase):
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed structures with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Location of adjacent streets and intersections and approximate location of structures on abutting properties.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed site access and circulation.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed grading and contours.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Preliminary landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed utilities (preliminary layout).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Preliminary infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, transit infrastructure, roadway improvements).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Preliminary stormwater management and erosion control plan.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b) 1. of the Land Use Code).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed alterations to and protection measures for significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-

N/A

526 (b)1. of the Land Use Code).

- Existing and proposed easements or public or private rights of way.

A.2F

General Submittal Requirements – Final Plan (Required)
Level III Site Plan
Final Plan Phase Check list (including items listed above in General Requirements for Preliminary Plan, if applicant did not elect to submit for a preliminary plan review)

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirement
✓	<input type="checkbox"/>	1	Evidence of financial and technical capacity.
✓ ongoing	<input type="checkbox"/>	1	Evidence of utilities' capacity to serve the development.
✓	<input type="checkbox"/>	1	Written summary of fire safety (referencing NFPA fire code and Section 3 of the City of Portland Technical Manual).
✓	<input type="checkbox"/>	1	Construction management plan.
N/A	<input type="checkbox"/>	1	Traffic Plan (if development will (1) generate 100 or more PCE or (2) generate 25 or more PCE and is located on an arterial, within 1/2 mile of a high crash location, and/or within ¼ mile of an intersection identified in a previous traffic study as a failing intersection).
✓	<input type="checkbox"/>	1	Stormwater management plan.
✓	<input type="checkbox"/>	1	Written summary of solid waste generation and proposed management of solid waste.
✓	<input type="checkbox"/>	1	Written assessment of conformity with applicable design standards.
✓ pending	<input type="checkbox"/>	1	Manufacturer's verification that HVAC and manufacturing equipment meets applicable state and federal emissions requirements.

Final Plan Phase		Number of Copies	Written Submittal Requirement
✓	<input type="checkbox"/>	1	Final Site Plan including the following
✓	<input type="checkbox"/>		▪ Existing and proposed structures on the site with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
✓	<input type="checkbox"/>		▪ Location of adjacent streets and intersections and approximate location of structures on abutting properties.
✓	<input type="checkbox"/>		▪ Proposed site access and circulation.
✓	<input type="checkbox"/>		▪ Proposed grading and contours.
✓	<input type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways. Proposed curb lines must be shown.
✓	<input type="checkbox"/>		▪ Proposed loading and servicing areas, including applicable turning templates for delivery vehicles
✓	<input type="checkbox"/>		▪ Proposed snow storage areas or snow removal plan.
✓	<input type="checkbox"/>		▪ Proposed trash and recycling facilities.
✓	<input type="checkbox"/>		▪ Landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
✓	<input type="checkbox"/>		▪ Existing and proposed utilities.
✓	<input type="checkbox"/>		▪ Location and details of proposed infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, public transit infrastructure, roadway improvements).
N/A	<input type="checkbox"/>		▪ Proposed septic system, if not connecting to municipal sewer. (Portland Waste Water Application included in this application)
✓	<input type="checkbox"/>		▪ Proposed finish floor elevation (FFE).
✓	<input type="checkbox"/>		▪ Exterior building elevation(s) (showing all 4 sides).

- ✓
- ✓ pending
- ✓
- N/A

- N/A

- ✓
- ✓
- N/A
- N/A

<ul style="list-style-type: none"> ▪ Proposed stormwater management and erosion controls. 	A.2g
<ul style="list-style-type: none"> ▪ Exterior lighting plan, including street lighting improvements.. 	
<ul style="list-style-type: none"> ▪ Proposed signage. 	
<ul style="list-style-type: none"> ▪ Identification of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code). Wetlands must be delineated. 	
<ul style="list-style-type: none"> ▪ Proposed alterations to and protection measures for of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code). 	
<ul style="list-style-type: none"> ▪ Total area and limits of proposed land disturbance. 	
<ul style="list-style-type: none"> ▪ Soil type and location of test pits and borings. 	
<ul style="list-style-type: none"> ▪ Details of proposed pier rehabilitation (Shoreland areas only). 	
<ul style="list-style-type: none"> ▪ Existing and proposed easements or public or private rights of way. 	

A.3



**Conditional Use Application
Development Review Application
Portland, Maine**

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Conditional Use Review. General information pertaining to the thresholds of review for site plans, public noticing procedures and the fee structure is contained in site plan applications and within the Developer's Packet.

(Please submit the Conditional Use Application in addition to an applicable site plan application.)

PROJECT NAME: Patient Care Center

CHART/BLOCK/LOT: 145 B042001, 145 B046001

RIGHT, TITLE OR INTEREST: (Please identify the status of the applicant's right, title, or interest in the subject property.)

Owner See Deed Attachment 5

(Please provide documentary evidence, attached to this application, of the applicant's right, title, or interest in the property. For Example – a deed, option or contract to purchase or least the subject property.)

VICINITY MAP: (Please attach a map showing the subject parcel and abutting parcels, labeled as to ownership and/or current use.)

EXISTING USE: Describe the existing use of the subject property.

Institutional – College/University Use

TYPE OF CONDITIONAL USE PROPOSED:

College/University Use in the R-5 Zone, and Building over 35 feet in height

SITE PLAN: Provide a site plan of the property, showing existing and proposed improvements, which meets the submission requirements of the applicable level of site plan review.

CONDITIONAL USE AUTHORIZED BY: SECTION 14- 118 (b) 6.

Address any specific conditional use standards for the specific use contained in the zoning code in the written submission.

STANDARDS – CRITERIA FOR CONDITIONAL USE APPEAL – Section 14-474

Address the following criteria in your written application and any applicable conditional use standards contained in the zoning code for the specific use.

Upon showing that a proposed use is a conditional use under this article, a conditional use permit shall be granted unless the Board determines that:

- a. There are unique or distinctive characteristics or effects associated with the proposed conditional use;
- b. There will be an adverse impact upon the health, safety, or welfare of the public or the surrounding areas;
- c. Such impact differs substantially from the impact which would normally occur from such a use in that zone.

CONTACT INFORMATION:

		Applicant's Contact for electronic plans	
		Name: Tom Saucier – Site Design Associates e-mail: tsaucier@sitedesignassociates.biz work #:	
Applicant – must be owner, Lessee or Buyer		Applicant Contact Information	
Name: Alan Thibeault		Work # 207-602-2253	
Business Name, if applicable: University of New England		Home#	
Address: 11 Hills Beach Road		Cell #	Fax#
City/State : Biddeford, ME	Zip Code: 04005	e-mail: athibeault@une.edu	
Owner – (if different from Applicant)		Owner Contact Information	
Name: Same as applicant		Work #	
Address:		Home#	
City/State :	Zip Code:	Cell #	Fax#
		e-mail:	
Billing Information		Billing Information	
Name: Bill applicant		Work #	
Address:		Home#	
City/State :	Zip Code:	Cell #	Fax#
		e-mail:	

A.3b

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

<p>Conditional Use</p> <p><input checked="" type="checkbox"/> Conditional Use \$100</p> <hr/> <p>The City invoices separately for the following:</p> <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) <p>Third party review is assessed separately.</p>	
<p>Site Plan Application</p> <p>Please submit a separate application for the applicable site plan review. Fees and charges are listed within the site plan application</p>	<p>Site Plan Application submitted and Fees Paid (office use) _____</p>

APPLICATION SUBMISSION

All site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

The application for a conditional use review must contain the following items:


- Conditional Use Application form that is completed and signed.
- Cover letter stating the nature of the project.
- Evidence of right, title and interest.
- Written Submittals that address the conditional use standards of Section 14-474 and any applicable standards of review contained in the zoning code for the specific use.
- One set of the paper plans and maps based upon the boundary survey at a scale not less than one (1) inch to fifty (50) feet and containing the information required for the applicable level of site plan review. The plan requirements and submission requirements are listed in the Site Plan Ordinance (Section 14 -527) of the Land Use Code. Refer to the application checklist for a detailed list of submittal requirements.
- One set of the plans at 11X17.
- The conditional use application fee and all other applicable fees as determined on the site plan application.

A.3c

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521) and the Conditional Use Standards (Section 14-474). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Conditional Use review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

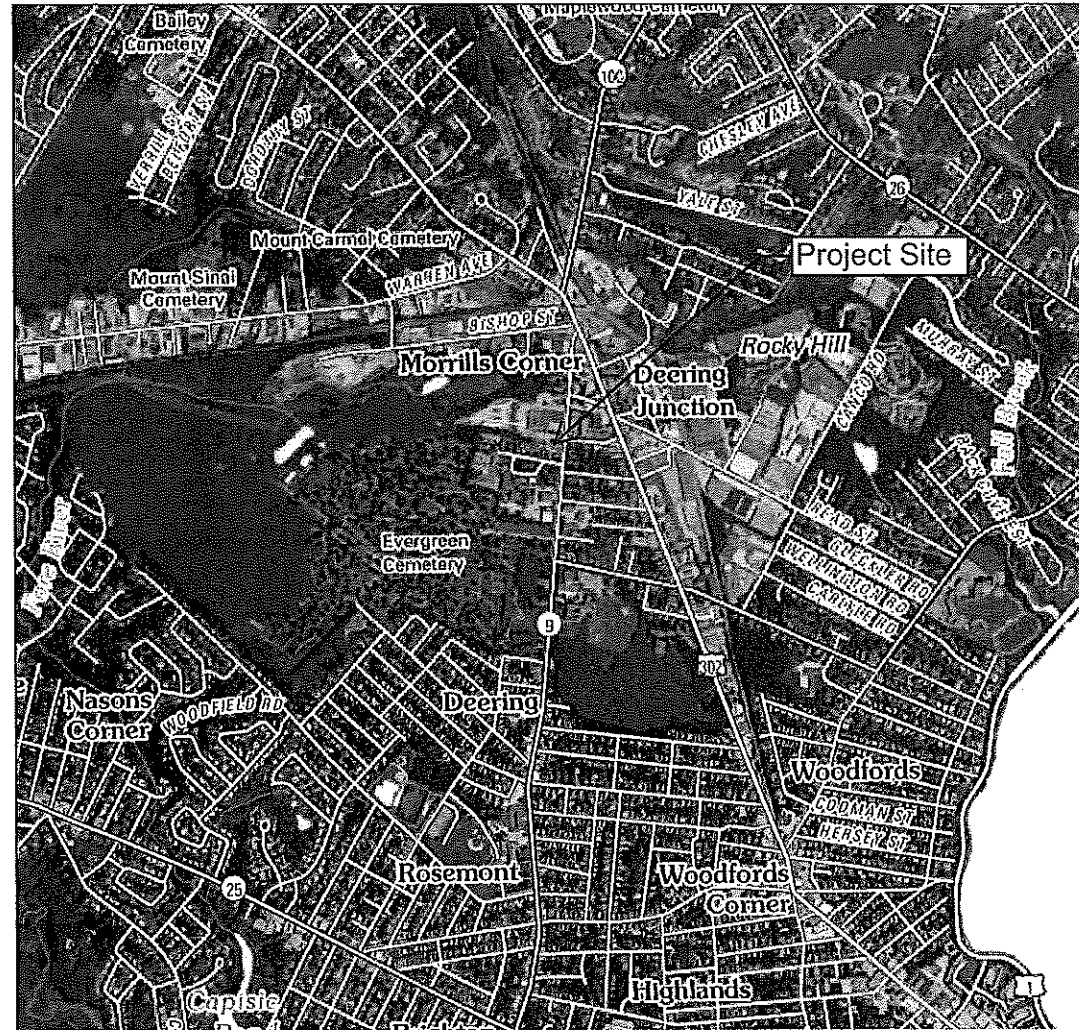
Signature of Applicant: 	Date: 4-17-12
----------------------------------------------------------------------------------------------------------------	------------------

The Portland Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division Fourth Floor, City Hall 389 Congress Street (207) 874-8721 or 874-8719	Office Hours Monday thru Friday 8:00 a.m. – 4:30 p.m.
----------------------------------------------------------------------------------------------------------	--------------------------------------------------------------------

A-3d

**University of New England
Proposed Patient Care Center**
Stevens Avenue, Portland, Maine



USGS Map 2011
7.5x7.5 Quadrangle

Conditional Use Narrative

Overview

As an institutional college/university use in the city's R-5 zone, the Patient Care Center at University of New England's Portland campus requires conditional use approval. Additionally, structures over 35 feet in height but less than 55 feet are permitted as a conditional use in R-5 under the city code, 14-118(b), provided certain standards are met.

The project, located just north of the recently completed College of Pharmacy on Stevens Avenue, will introduce a new Dental Medicine curriculum to the University. The program will, as part of the curriculum, incorporate a low-cost Dental Teaching Clinic (including Urgent Care) that is open to the public, thus fulfilling the increasing need for dental care in the State of Maine. A future build-out within the proposed structure will provide a low-cost health clinic which will focus on interprofessional care and treatment that is open to the public as well as UNE faculty, students, and staff.

Conditional Use Standards – Section 114.118(b)6.

Section 114.118 (b) 6.a. The proposed use will be located in the lot on which the principal use is located.

Section 114.118 (b) 6.b. The proposed use will not cause significant displacement or conversion of residential uses. Two of the buildings being displaced, at 1 College Street and 746 Stevens Avenue are offices and administrative support space, while the building at 750 Stevens Avenue is a single family home.

Section 114.118 (b) 6.c. Minimum lot size standards are met.

Section 114.118 (b) 6.d.,e.,f. Not applicable.

Section 114.118 (b) 6.g. This section of the ordinance outlines standards which must be met if a building is greater than 35 feet but less than 55 feet in height. The minimum lot size is 10 acres, minimum setback between buildings on the site must be 20 feet, and the setback from external property boundaries must be 30 feet. The project as proposed conforms with these standards. The site is abutted to the north by the armory, to the south by College Street and university property, to the east by Stevens Avenue, and to the west by existing UNE structures.

Conclusion

Section 14-474. Upon a showing that a proposed use is a conditional use under this article, a conditional use permit shall be granted unless the board determines that:

a. There are unique or distinctive characteristics or effects associated with the proposed conditional use;

A.3f

- b. There will be an adverse impact upon the health, safety, or welfare of the public or the surrounding area; and
- c. Such impact differs substantially from the impact which would normally occur from such a use in that zone.

The previous project approved as a conditional use on the campus was the College of Pharmacy. There are no unique or distinctive characteristics or effects associated with this use, and, in fact the use is similar to the College of Pharmacy, and is consistent with the intent of the Land Use Code.

Further, the use complements and is similar to the existing dental hygiene training and clinic which currently exist on the campus, as a part of the college/university use.

There will be no adverse impact upon public health, safety, or welfare, which is demonstrated by the information contained in the attached Level III review application and supporting documentation.

Any impacts would not differ substantially from impacts which would normally occur from such a use in the R-5 zone. In fact, given that the facility will be located on an existing college campus, any impacts due to the proposed use at this site would be much less than those which could occur in other R-5 zones.

It is apparent that the proposed use meets the applicable conditional use standards outlined in the ordinance.



UNIVERSITY OF
NEW ENGLAND

A.4

Biddeford Campus
11 Hills Beach Road
Biddeford, ME 04005
(207) 283-0171

Portland Campus
716 Stevens Avenue
Portland, ME 04103
(207) 797-7261

March 1, 2012

Mr. Tom Saucier
Site Design Associates
23 Whitney Way
Topsham, Maine 04086

TO WHOM IT MAY CONCERN:

This letter authorizes Tom Saucier to serve as an agent for University of New England for the purpose of permitting a proposed Parking Facility and Road improvements on the University of New England's Portland campus.

Sincerely,

A handwritten signature in cursive script that reads "Alan Thibeault".

Alan Thibeault
Assistant Vice President of Planning

PROJECT DESCRIPTION

Overview

The Patient Care Center at University of New England's Portland campus will be a significant asset to the existing UNE Portland campus, and the City of Portland. The project, located just north of the recently completed College of Pharmacy on Stevens Avenue, will introduce a new Dental Medicine curriculum to the University. The program will, as part of the curriculum, incorporate a low-cost Dental Teaching Clinic (including Urgent Care) that is open to the public, thus fulfilling the increasing need for dental care in the State of Maine. A future build-out within the proposed structure will provide a low-cost health clinic which will focus on interprofessional care and treatment that is open to the public as well as UNE faculty, students, and staff.

Site and Context

Due to its location on Stevens Avenue, and the demographics of its program, it is important this project address its connection to the existing campus, and a connection to its public use. The building's size, massing and materials will reflect the collegiate vernacular of the surrounding campus, while addressing the pedestrian nature of its urban neighborhood.

Students, faculty and resident dentists will be accessing the building from the existing campus to the south. This entrance addresses the pedestrian nature of the campus.

While the connection to the existing campus is important, the connection to public access is equally important. This facility will address its public entrance on the North side with the use of an inviting, light-filled entrance atrium and main building signage. This entrance will be both vehicular and pedestrian focused, with the adjacent public parking lot to serve the Clinical portion of the program.

Building Use

This new development will consist of a two-story building, with each story having 18,200+/- square feet each, and a small Mechanical basement of 3,100 +/- square feet, for a total building area of 39,500+/- square feet. This new Dental Medicine curriculum comprises several main components including simulation areas and comprehensive care areas. The IPE Clinic serves as a teaching clinic and is a primary use of the first floor of the building. Also included on the first floor are the patient simulation and teaching areas, which are mainly for the use of the first and second-year students.

The second level of the building contains the comprehensive care areas. These areas are for upper-level students working on live patients rather than simulators. A specialty care area is also on the second floor for post-graduation students and resident dentists. In addition to the exam areas, it is the specialty care area that includes the Imaging (X-ray) and the urgent care facilities.

Each entrance to the building (the campus entrance and the public entrance), will have stairwells and elevators, with the public entrance having an open railed two-story lobby for clear wayfinding. It is anticipated to have the mechanical units hidden in the center of the flat portion

of the roof, with the remainder of the mechanical equipment located in the basement.

Building Construction

A steel structure with composite metal decks for the floor structures will be utilized for this building. Insulated exterior panels will also be used to aid in expediting construction times. A combination of brick, precast concrete, and cementitious panels will be installed over the insulated panels, to maintain the collegiate vernacular of the university. Several large window elements will be used bring in natural light further into the interior of the building and to add definition to the elevations. Pitched roofs around the perimeter will assist in enhancing the vernacular of the surrounding campus and neighborhood. The pitched roofs will also aid in hiding the rooftop HVAC units. An entrance atrium area will be used to signify the main entrance and distinguish the public from the campus entrance.

Conclusion

It is the desire of the University that, along with the College of Pharmacy, this new Patient Care Center will provide the “bookends” to the beautiful historic green in the center of the campus. The pitched roofs, along with the brick exterior will maintain the classic campus atmosphere. The size, massing, and building elements will fit both within the existing campus and the surrounding neighborhood. The landscaping will provide additional outdoor student areas and enhance Stevens Avenue. This new Dental Medicine curriculum will be a welcomed addition to the University and will help to provide needed dental care in the state of Maine, as well as bringing many students and faculty to Portland to live and work. We believe that this development will affect the site, the University, and the City of Portland in an extremely positive way.

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Abstract of Title to the following described land:

A certain lot or parcel of land with the building thereon, situated number 374 Stevens Avenue in that part of said Portland formerly Deering, and being bounded and described as follows:

Beginning at a stone post standing on the southerly side of College Street, formerly Seminary Street, one hundred and fifty-eight (158) feet more or less from the northwesterly corner of said College Street and Stevens Avenue, and running north, twenty-four and one-fourth ($24 \frac{1}{4}$) degrees east, one hundred and forty-eight and one-half ($148 \frac{1}{2}$) feet to land now or late of Captain Sargent; thence south, seventy (70) degrees east by said Sargent's land one hundred and forty-one and one half ($141 \frac{1}{2}$) feet more or less to said Stevens Avenue; thence southerly by said Stevens Avenue seventy-four (74) feet to land now or late of Arabella W. Roberts; thence westerly by land now or late of said Roberts in a line parallel with said College Street one hundred (100) feet; thence southerly again by land now or late of said Roberts in a line parallel with said Stevens Avenue ninety (90) feet to said College Street; and thence westerly by said College Street to point of beginning; be any or all of said measurements more or less, or however otherwise bounded, measured or described.

Being the same premises conveyed by Lydia J. Snow to Thalia L. Clement, formerly Thalia L. Scamman, by warranty deed dated April 22, 1902, and recorded in Cumberland County Registry of Deeds, Book 716, Page 326.

A-6a

bounded, measured or described.

Being the same premises conveyed by Lydia J. Snow to Thalia L. Clement, formerly Thalia L. Scamman, by warranty deed dated April 22, 1902, and recorded in Cumberland County Registry of Deeds, Book 716, Page 326.

Excepting that certain lot or parcel of land conveyed by the said Thalia L. Clement to Victor Brewster by warranty deed dated October 19, 1908, and recorded in Cumberland County Registry of Deeds, Book 831, Page 38.

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Y 0011711 FOR THE RECORD THE FIRST...

377-518.

(Elizur)

Eleazer B. Forbes to Joseph C. Snow;

Warranty; July 29, 1870;

Tract embracing lot described.

478-428.

Joseph C. Snow to Lydia J. Snow, wife;

Warranty; December 13, 1880;

Same tract as 377-518.

716-326.

Lydia J. Snow to Thalia L. Scamman;

Warranty, April 22, 1902.

721-20.

Thalia L. Scamman to Portland Savings Bank;

Mortgage; September 30, 1902; Discharged on margin

Mar. 2, 1905.

Thalia L. Clement to Portland Savings Bank, June 10, 1910.

Mortgage;

Discharged on margin September 22, 1914.

At the present time there are no encumbrances of record.

Dated at Portland, Maine, this fifth day of April,

A.D. 1919.

Wheeler

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Quit-Claim Deed

CORPORATION — WITH COVENANT

FROM

ALL SOUS UNIVERSALIST CHURCH OF
PORTLAND, MAINE
(ALL SOUS CHURCH PARISH)

TO

WESTBROOK SEMINARY AND JUNIOR COLLEGE

Dated February 9 1940

State of Maine,

Cumberland ss. Registry of Deeds

Received FEB 9 1940

at B. H. & Co., M., and

recorded in Book 1598 Page 276

Wm. M. Linnell
REGISTER

FROM THE OFFICE OF

Bradley, Linnell, Nulty & Brown
192 Middle Street
Portland, Maine

MASTON, PORTLAND, MAINE — LAW OFFICE SUPPLIES AND EQUIPMENT
MASTON'S FORM NO. 9

46

A. 6d

To have and to hold the same, together with all the privileges and appurtenances thereunto belonging, to it, the said Westbrook Seminary and Junior College, its successors

and Assigns forever.

And the said Grantor Corporation does warrant with the said Westbrook Seminary and Junior College, its successors

and Assigns, that it will warrant and defend the premises to it, the said Grantee, its successors

and Assigns forever, against the lawful claims and demands of all persons claiming by, through, or under it.

In Witness Whereof, the said All Souls Universalist Church of Portland, Maine (All Souls Church Parish)

has caused this instrument to be sealed with its corporate seal and signed in its corporate name by Herbert B. Seal, President of its Board of Trustees, thereunto duly authorized, this 9th day of February in the year one thousand nine hundred and forty.

Signed, Sealed and Delivered in presence of

E. F. Perkins

ALL SOULS UNIVERSALIST CHURCH OF PORTLAND, MAINE (ALL SOULS CHURCH PARISH)

By Herbert B. Seal Its President of Board of Trustees



CORPORATE SEAL

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State of Miss.
CUMBERLAND,

February 9, 1940.

Personally appeared the above named Herbert B. Seal, President
of the Board of Trustees of said Grantor Corporation
as aforesaid, and acknowledged the foregoing instrument to be his
free act and deed in his said capacity, and the free act and deed of
said corporation.



Before me, Elliott P. Perkins
Justice of the Peace

A.6f

*Walter G. Whitman -
192 Middle Street -
Portland, Maine - 5/12/30*

Warranty Deed

FROM

WALTER G. WHITMAN

TO

WESTBROOK SEMINARY AND JUNIOR COLLEGE

Dated _____, 1940

State of ~~Maine~~

County of ~~Maine~~ ss. Registry of Deeds

Received MAR 28 1940

at 4 H. 55 M. P M., and

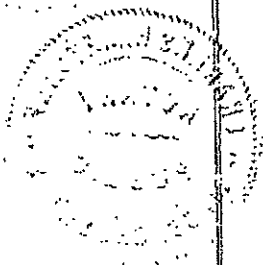
recorded in Book 1603 Page 98

Wm. C. Penick
Register

FROM THE OFFICE OF

Bradley, Innell, Nitty & Brown
192 Middle Street
Portland, Maine

MAINTENANCE PORTLAND, MAINE - OFFICE REPAIRING AND REPAIRING
TYPEWRITERS - SHORTHAND MACHINES - PILING CASES - COPIES - ETC.
MARTIN'S FORM NO. 1 - STANDARD REGISTRY FORM



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Ludake

Know all Men by these Presents

That ALL SOULS UNIVERSALIST CHURCH OF PORTLAND, MAINE, a religious society duly organized under the laws of the State of Maine and located at Portland, in the County of Cumberland and State of Maine, formerly known as and called ALL SOULS CHURCH PARISH,

~~and corporation organized and existing under the laws of the State of Maine and located at Portland, in the County of Cumberland and State of Maine, formerly known as and called ALL SOULS CHURCH PARISH,~~

in consideration of one dollar and other valuable consideration

paid by WESTBROOK SEMINARY AND JUNIOR COLLEGE, a corporation organized and existing under the laws of the State of Maine and located at said Portland,

the receipt whereof it does hereby acknowledge, does hereby remise, release, bargain, sell and convey, and forever quit-claim unto the said

Westbrook Seminary and Junior College, its successors

and assigns forever,

the following described premises situated in said Portland:

1. A certain lot or parcel of land situated in said Portland and bounded and described as follows: Beginning at an iron stake at the northwesterly corner of the lot of land conveyed by Ella Louise Fernald to Susie Whitman by deed dated May 11, 1908 and recorded in Cumberland County Registry of Deeds in Book 826, Page 72; thence by land conveyed to said Whitman as aforesaid and by land conveyed to said Whitman by Cordelia S. Pierce by deed dated August 3, 1892 and recorded in said Registry of Deeds, Book 595, Page 255, on a course of South sixty-four degrees forty-two minutes East (S. 64° 42' E.) two hundred thirty-three (233) feet to an iron stake; thence by other land of said Grantor on a course of North twenty-one degrees fifty-six minutes East (N. 21° 56' E.) sixty-eight and fifty-five hundredths (68.55) feet to the Southeasterly corner of the lot of land next hereinafter described and conveyed; thence by said lot of land next hereinafter described and conveyed on a course of North sixty-four degrees fifty-five minutes West (N. 64° 55' W.) two hundred twenty-one and three tenths (221.3) feet to other land of said Grantee; thence by land of said Grantee on a course of South thirty-one degrees forty-five minutes West (S. 31° 45' W.) sixty-eight and one tenth (68.1) feet to the point of beginning.

Said above described lot is subject to the life lease to Ella L. Perry as granted to her by All Souls Church Parish by lease dated October 17, 1924 and recorded in said Registry in Book 1204, Page 85.

~~2. A certain other lot or parcel of land in said Portland was purchased and land which Cordelia S. Pierce devised to All Souls Universalist Church of Portland, Maine under the name of Society of All Souls Church by her last will and testament~~

Westbrook Seminary and Junior College, its successors

A.6h

holds and assigns forever,

the following described premises situated in said Portland:

1. A certain lot or parcel of land situated in said Portland and bounded and described as follows: Beginning at an iron stake at the northwesterly corner of the lot of land conveyed by Ella Louise Fernald to Susie Whitman by deed dated May 11, 1908 and recorded in Cumberland County Registry of Deeds in Book 826, Page 72; thence by land conveyed to said Whitman as aforesaid and by land conveyed to said Whitman by Cordelia S. Pierce by deed dated August 3, 1892 and recorded in said Registry of Deeds, Book 593, Page 236, on a course of South sixty-four degrees forty-two minutes East (S. 64° 42' E.) two hundred thirty-three (233) feet to an iron stake; thence by other land of said Grantor on a course of North twenty-one degrees fifty-six minutes East (N. 21° 56' E.) sixty-eight and fifty-five hundredths (68.55) feet to the Southeastern corner of the lot of land next hereinafter described and conveyed; thence by said lot of land next hereinafter described and conveyed on a course of North sixty-four degrees fifty-five minutes West (N. 64° 55' W.) two hundred twenty-one and three tenths (221.3) feet to other land of said Grantee; thence by land of said Grantee on a course of South thirty-one degrees forty-five minutes West (S. 31° 45' W.) sixty-eight and one tenth (68.1) feet to the point of beginning.

Said above described lot is subject to the life lease to Ella L. Perry as granted to her by All Souls Church Parish by lease dated October 17, 1924 and recorded in said Registry in Book 1204, Page 66.

2. A certain other lot or parcel of land in said Portland and bounded and described as follows: Beginning at a stake on the northerly line of the lot of land which Cordelia S. Pierce devised to All Souls Universalist Church of Portland, Maine under the name of Society of All Souls Church by her last will and testament an abstract of which is recorded in Cumberland County Registry of Deeds in Book 757, Page 136, said stake being distant one hundred forty-five (145) feet westerly of the westerly line of Stevens Avenue as established by deed from Westbrook Seminary to the City of Portland dated May 29, 1917 and recorded in said Registry in Book 1086, Page 449; thence by other land of said Grantor on a course of South twenty-one degrees fifty-six minutes West (S. 21° 56' W.) sixty-eight and fifty-five hundredths (68.55) feet to the northerly line of the lot of land next hereinafter described and conveyed; thence by said lot of land next hereinafter described and conveyed on a course of North sixty-four degrees fifty-five minutes West (N. 64° 55' W.) two hundred twenty-one and three tenths (221.3) feet to land of said Grantee; thence by land of said Grantee on a course of North thirty-one degrees forty-five minutes East (N. 31° 45' E.) sixty-eight and one tenth (68.1) feet to a stake; thence by land of said Grantee on a course of South sixty-five degrees eight minutes East (S. 65° 08' E.) two hundred nine and sixty-four hundredths (209.64) feet to the point of beginning.

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Said aforementioned courses are magnetic and of the date of 1955.

Being a part of the property devised to All Souls Universalist Church of Portland, Maine under the name of Society of All Souls Church by the last will and testament of Cordelia S. Pierce, an abstract thereof being recorded in said Registry of Deeds in Book 757, Page 156.

A-6j

On ~~and in~~ ~~with~~ the aforegranted and bargained premises with all the privileges and appurtenances thereof, to the said

Westbrook Seminary and Junior College, its successors

~~and~~ and assigns, to its and their use and behoof forever.

And I do covenant with the said Grantee, its ^{successors} ~~and~~ and assigns,

that I am lawfully seized in fee of the premises, that they are free of all incumbrances; except as aforesaid;

that I have good right to sell and convey the same to the said

Grantee to hold as aforesaid; and that I and my heirs

shall and will warrant and defend the same to the said Grantee, its

^{successors} ~~and~~ and assigns forever, against the lawful claims and demands of

all persons except as aforesaid,

In Witness Whereof, I, the said Walter G. Whitman,

and I, Grace B. Whitman,

wife of the said

Walter G. Whitman,

joining in this deed as Grantor, and relinquishing and conveying

all right by descent and all other rights in the above described

premises, have hereunto set our hands and seals this 26th

day of *March* in the year of our Lord one thousand nine

hundred and forty.

Signed, Sealed and Delivered
in presence of

Sumner Wheeler

Walter G. Whitman

Grace B. Whitman



premises, have ~~not~~ ^{made} day of March in the year of our Lord one thousand nine hundred and forty.

A. L. K

Signed, Sealed and Delivered in presence of

Summer J. Wheeler

Walter G. Whitman

Grace B. Whitman



COMMONWEALTH OF MASSACHUSETTS;

Essex, ss.

March 26, 1940.

Personally appeared the above named

Walter G. Whitman

and acknowledged

the foregoing instrument to be his

free act and deed.

Before me,

Charles A. Metcalf

Justice of the Peace
Notary Public

My commission expires Apr. 27, 1940

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KNOW ALL MEN BY THESE PRESENTS, That the CITY OF PORTLAND, a body politic and corporate located in the County of Cumberland and State of Maine, in consideration of One Dollar (\$1.00) and other good and valuable considerations, paid by Westbrook College, a corporation organized and existing under the laws of the State of Maine and located at 716 Stevens Avenue in said Portland in said County and State, the receipt whereof it does hereby acknowledge, does hereby give, grant, bargain, sell and convey, unto the said Westbrook College, its successors and assigns forever, certain real estate located in said Portland, lying Westerly of College Street and Southerly of Bishop Street, bounded and described as follows:

Beginning at a point on the Northerly line of the land of Portland Railroad Company where the same intersects with land hereby conveyed and land formerly of George H. Smardon, commonly known as the Smardon Gravel Pit, formerly Read's Gravel Pit; thence from said point North 19° 30' East 28.68 rods; thence North 60° West Seven and thirty-two hundredths (7.32) rods to a point; thence South 74° 45' West Thirty-six and sixty-four hundredths (36.64) rods to a point; thence North 5° West Two and fifty-six hundredths (2.56) rods; thence North 30° West Twenty-four and four tenths (24.4) rods; thence South 65° West Forty-seven and twelve hundredths (47.12) rods to center of ditch; thence up the center of said ditch towards what is known as pasture Two Hundred Ninety-eight (298) feet; thence in a straight line to a point in line of Evergreen Cemetery indicated by a spruce stump, said stump being on the intersection between land of said Cemetery and land conveyed by Luther B. Roberts to the said City of Portland; thence from said stump North 80° 21' East Three Hundred Seventy-one and two tenths (372.2) feet to an angle; thence from said angle North 76° 8' East, Five Hundred Ninety-six and twenty-two hundredths (596.22) feet to an angle; thence from said last named angle at a right angle South 12° 52' East Seventy-five (75) feet to land of said Portland Railroad Company; thence about North 76° 8' East by said Portland Railroad Company's land to the point of beginning.

Also a certain other parcel of land, abutting the above described premises and bounded and described as follows: Beginning at the center of the ditch

Two Hundred Ninety-eight (298) feet; thence in a straight line to a point in line of Evergreen Cemetery ~~indicated by a stump in said Cemetery and land~~ conveyed by Luther B. Roberts to the said City of Portland; thence from said stump North 80° 21' East Three Hundred Seventy-one and two tenths (372.2) feet to an angle; thence from said angle North 76° 8' East, Five Hundred Ninety-six and twenty-two hundredths (596.22) feet to an angle; thence from said last named angle at a right angle South 12° 52' East Seventy-five (75) feet to land of said Portland Railroad Company; thence about North 76° 8' East by said Portland Railroad Company's land to the point of beginning.

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Also a certain other parcel of land, abutting the above described premises and bounded and described as follows: Beginning at the center of the ditch between the hereinabove described premises and the land hereby conveyed and in line of land now or formerly owned by Adam Wilson and running Southerly by land of said Wilson to the Westerly corner of land of Evergreen Cemetery; thence South Twenty-four (24°) degrees East by land of said Cemetery One hundred and seventy-five (175) feet to a point; thence by land formerly of Dennis F. Gulliver, and being the premises hereinabove described, in a straight line to a point in the center of said ditch; said point being Two Hundred Ninety-eight (298) feet from the point of beginning; thence down the center of said ditch to the point of beginning.

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Excepting and reserving from the above described premises that portion thereof conveyed by the Grantor to Central Maine Power Company by deed dated February 26, 1967, recorded in said Registry of Deeds in Book 2989, Page 13.

This conveyance is made subject to the right-of-way or easement granted by the Grantor to Central Maine Power Company by deed dated December 13, 1951, recorded in said Registry of Deeds in Book 2055, Page 463.

Reserving to the Grantor, its successors and assigns, the right perpetually to enter at any and all times upon a strip of land fifty (50) feet in width lying equally on either side of the storm drain and upon a strip of land fifty (50) feet in width lying equally on either side of the brook presently located on the premises herein conveyed and the right perpetually to maintain, repair, rebuild, relocate, or remove said storm drain and brook with all necessary fixtures and appurtenances within said strips of land, granting to the Grantee, its successors and assigns, the use and enjoyment of said strips of land for such purposes only as will in no way interfere with the perpetual use thereof by the Grantor, its successors and assigns, for the purposes above mentioned, except that no buildings or structures may be erected within said strips of land. Also reserving to the Grantor, its successors and assigns, the right perpetually to enter at any and all times upon the premises herein conveyed for the purpose of access and egress by itself, its servants and/or agents to and from said strips of land.

Being the same premises conveyed to the Grantor by deed of James Gulliver, Administrator, d.b.n.e.t.a., of the Estate of Stephen Gulliver by deed dated August 23, 1935, recorded in said Registry of Deeds in Book 1476, Page 441.

Also a certain lot of land lying in the rear of College Street, in the City of Portland, bounded and described as follows, to wit:

Beginning at a point on the Northerly side line of College Street and at the Westerly corner of land belonging to Francis L. Stone; thence North 25° 22' east by land of said Stone eighty-seven and nineteen one hundredths (87.19) feet to land of the Portland Railroad Company; thence North 62° 26' west by land of said Portland Railroad Company four hundred and thirty-one and ninety three one hundredths (431.93) feet

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Seventy-one and two tenths (372.2) feet to an angle; thence from said angle North 76° 8' East, Five Hundred Ninety-six and twenty-two hundredths (596.22) feet to an angle; thence from said last named angle at a right angle South 12° 52' East Seventy-five (75) feet to land of said Portland Railroad Company; thence about North 76° 8' East by said Portland Railroad Company's land to the point of beginning.

Also a certain other parcel of land, abutting the above described premises and bounded and described as follows: Beginning at the center of the ditch between the hereinabove described premises and the land hereby conveyed and in line of land now or formerly owned by Adam Wilson and running Southerly by land of said Wilson to the Westerly corner of land of Evergreen Cemetery; thence South Twenty-four (24°) degrees East by land of said Cemetery One hundred and seventy-five (175) feet to a point; thence by land formerly of Dennis F. Gulliver, and being the premises hereinabove described, in a straight line to a point in the center of said ditch; said point being Two Hundred Ninety-eight (298) feet from the point of beginning; thence down the center of said ditch to the point of beginning.

Zoning Compliance and Applicable Design Standards

The project, located just north of the recently completed College of Pharmacy on Stevens Avenue, will introduce a new Dental Medicine curriculum to the University. The program will, as part of the curriculum, incorporate a low-cost Dental Teaching Clinic (including Urgent Care) that is open to the public, thus fulfilling the increasing need for dental care in the State of Maine. A future build-out within the proposed structure will provide a low-cost health clinic which will focus on interprofessional care and treatment that is open to the public as well as UNE faculty, students, and staff.

As noted in the previous Conditional Use section, the project is allowed in the R-5 zone as a conditional use. The project meets the minimum lot size (10 acres for a building over 35 feet in height), setbacks (30 feet from external boundaries), and height restrictions (less than 55 feet total height).

Section 14-526 of the Land Use Ordinance outlines the standards for Site Plan Approval. The following discussion will outline how this project will comply with the site plan standards.

14-526. (a) Transportation – As noted in the trip generation analysis included as Attachment 7, the project is expected to generate 69 new trips (24 entering and 45 exiting) the site during the peak hour. The report also indicates that the peak hour will occur some time after 12:00 noon, but before the peak hour of the adjacent street, Stevens Avenue. For these reasons, it appears there will be minimal impact on the levels of service at any intersection in the vicinity of the project.

As noted in the revised master plan submitted to the city, UNE has recently acquired the 19 acre former Pike property at the end of Bishop Street. This property directly abuts UNE's property, and can be accessed through the campus by an existing roadway to Bishop Street. UNE has undertaken preliminary studies and had several discussions with city staff regarding the use of this property for parking. A shuttle service to the campus would be provided via the existing access road.

It is UNE's understanding that the zoning will allow for the use of this existing impervious surface as parking. On a concurrent track with the site plan review process for the PCC, UNE is continuing to work with city staff to develop a plan for parking on this parcel. An added benefit from a traffic standpoint is that parking at the parcel and riding the shuttle into the campus will remove traffic from the congested Morrill's Corner intersection.

The existing curb cut from Stevens Avenue will serve as the public access to the site. No new curb cuts will be created. Sight distances at the access point conform with local and state standards. Two existing curb cuts will be removed along Stevens Avenue.

Existing sidewalks are located along both the Stevens Avenue and College Street frontages. As shown on the drawings, UNE is proposing modifications along Stevens Avenue to provide a drop-off area along Stevens Avenue at the main public entrance. The sidewalks in this area will

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be improved as required. The drop-off will be signed to prohibit parking, and will provide a drop area for both private and commercial vehicles. Stevens Avenue will be widened in this area to accommodate the drop-off.

Bus stops are located at two locations along the campus frontage and provide adequate public transit access.

In regards to parking, the public entrance of the proposed building will be located on the north end. As noted in the trip generation analysis of Attachment 7, the parking demand for the clinical use will be 31 spaces. It is our understanding the city's consulting traffic engineer has reviewed the parking generation analysis and generally concurs with the results. As noted on the site plan, 21 spaces will be striped on the north end of the building. These, when combined with the 12 leased spaces at the armory site which directly abut this parking, will provide for a total of 33 spaces for the public parking.

As previously noted, UNE is currently developing a plan to provide parking at the recently acquired Pike property on Bishop Street, in order to eliminate the parking deficits projected in their long term parking analysis. This will offer a number of benefits, including reduced traffic at Morrill's corner, and the opportunity to park on existing impervious surfaces. Preliminary studies indicate that there is adequate space available to eliminate the parking deficit projections. We will continue to work with city staff on this alternative to parking on Gulliver's Field.

A bike rack pad is provided at the southerly building entrance, where the primarily non-public or student/faculty entrance is located. This rack will accommodate up to 9 bikes.

A Transportation Demand Management plan for the campus is currently under review by the staff and planning board. Reference is made to that plan with this submission.

14-526. (b) Environmental Quality Standards – The plan is to preserve existing trees on site to the extent practicable. Any trees designated to remain which are damaged during construction will be replaced.

Given the developed nature of the site in an urban setting, there are no threatened or endangered plant or animal species or habitat located on the project site. There are no wetlands or vernal pools on or proximate to the site.

This project involves the demolition of several wood frame structures and existing parking areas on the campus in order to construct an approximately 18,200 sf footprint building. A portion of runoff from the building site at the corner of College Street and Stevens Avenue drains to the existing city systems in those streets via overland flow. At this point in time, it appears the building will result in a net increase of approximately 4400 sf of impervious area on the development site.

However, when the College of Pharmacy was permitted in 2008, the city approved stormwater management plan demonstrated a reduction in impervious area of 6000 sf on the campus. With this project, the extent of impervious area on the campus will continue to be 1600 sf less than prior to the College of Pharmacy project. Both sites are tributary to the same system in Stevens

A.7b

Avenue.

As a further benefit of this project, an infiltration system will be constructed to replace existing infiltration systems displaced by the construction, and to collect runoff from the roof of the proposed building. This will serve to further reduce runoff from the UNE property on the existing storm and sanitary sewers in Stevens Avenue.

Therefore, construction of this project should not adversely affect the receiving storm drain systems, as runoff rates and quantities will be less than existing rates.

14-526. (c) Public Infrastructure and Community Safety Standards – The development as proposed is consistent with the master plan recently submitted to the city by UNE. Attachment 10 includes a narrative regarding the community safety standards.

See Attachment 9 for utility letters. The proposed building will connect with the existing sanitary sewer system in the westerly side of Stevens Avenue. Water service will also be provided from Stevens Avenue, while gas service will be via an existing service extending into the site. Power will be provided from an existing underground connection to Stevens Avenue. A new transformer and backup generator will be installed in an enclosure as noted on the project drawings.

14-526. (d) Site Design Standards – The bulk, height, and location of the building will not result in health or safety problems from a reduction in ventilation to abutting structures, nor will it result in changes to the existing wind climate. There will be no adverse impact of shadows on abutting public open space. All neighboring structures are owned by UNE. No view corridors will be impacted.

A historic district, the campus green, is located southerly of College Street. The building design is currently being reviewed by the Maine Historic Preservation Commission and will be subject to input from the city's historic preservation staff and board.

All exterior lighting will meet city Technical Manual requirements. Lighting levels in the parking area and along walks will be in accordance with IESNA standards for security and safety. All fixtures will be cutoff and shielded as required to avoid spill over into the adjacent streets.

All HVAC equipment will meet applicable state and federal emissions standards. The HVAC units will be high efficiency gas fired condensing units. The HVAC and air handling units will be located on the roof, and will be screened from public view.

Noise standards will not be exceeded.

In regards to signage, the existing sign at the intersection of College Street and Stevens Avenue will remain. Signage will be located on the building canopy, similar to the signage at the College of Pharmacy. Regulatory signage will comply with the applicable MUTCD standards.

A.8

UNE Portland Campus – College of Dental Medicine
04/01/12

Parking Demand:

Total New Students: 138 (46/year over 3 years)
 Parking Demand for new students (@ 0.67 sp/student): 93 spaces
 Parking Demand for IPE (5000sf – 50% student/50% outside) @ 3.9 sp/ksf): 10 spaces
 Parking Demand for Dental Clinic (5,492sf @ 3.9 sp/ksf) 21 spaces

Total new parking demand: 124 spaces

Trip Generation:

Trips from new students:	AM Peak Hour	29 trips	23 in/ 6 out
	PM Peak Hour	29 trips	8 in/ 21 out
	PM Peak (Gen)	33 trips	10 in/23 out
Trips from IPE (New)	AM Peak Hour	6 trips	5in/ 1 out
	PM Peak Hour	9 trips	2in/ 7 out
	PM Peak (Gen)	11 trips	4 in/7 out
Trips from Dental Clinic	AM Peak Hour	15 trips	12in/ 3 out
	PM Peak Hour	21 trips	6 in/ 15 out
	PM Peak (Gen)	25 trips	10 in/15 out

Total Trips (Using PM peak hour of the generator – the highest level of trips which occurs sometime after 12:00 Noon but not in the range of 4:00 to 6:00 PM): 69 trips

A Traffic Movement Permit was obtained for the UNE Pharmacy School, thus another TMP is required only when net new trips exceeds 100 trips in a peak hour – this condition is not met and no TMP is required.

A.9



Office of Fiscal Affairs

Biddeford Campus
11 Hills Beach Road
Biddeford, ME 04005
(207) 283-0171 T
(207) 602-5907 F

Portland Campus
716 Stevens Avenue
Portland, ME 04103
(207) 797-7261 T
(207) 221-4890 F

March 30, 2012

City of Portland
Planning Board
389 Congress St.
Portland, ME 04101

Dear Planning Board Members:

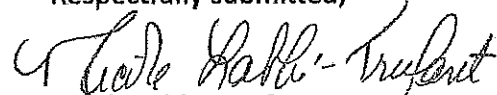
Please accept this letter as assurance that the University of New England (UNE) has the financial capacity to construct, equip, and make the proposed Patient Care Center a state of the art facility.

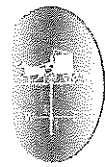
From 2008 to 2011 UNE has had a total surplus from operations of over \$35 million. These surpluses have dramatically improved the University's liquidity and financial capacity. Even during the stock market decline in fiscal year 2009, the University maintained an overall surplus of \$3.7 million. Total net assets during that time have gone from \$76 million to over \$108 million. This demonstrates UNE's capability to rapidly increase its net worth and financial viability.

To date, UNE raised over \$9 million to construct and equip the proposed Patient Care Center. These fund raising efforts are continuing. Internal budgets and cash flow projections provide further proof that the University can readily afford the proposed expansion.

Taking into consideration the \$9 million raised, along with the financial strength of the University, the administration is confident it has more than enough resources to complete the project. The completed Patient Care Center will be a great benefit for the residents of Portland as well as the surrounding area.

Respectfully submitted,


Nicole Labbe-Trufant
Vice President for Fiscal Affairs
University of New England



Portland Water District
FROM SEBAGO LAKE TO CASCO BAY

A-10

March 21, 2012

Site Design Associates
23 Whitney Way
Topsham, ME 04086

MAR 23 2012

Attn: Tom Saucier, P.E.
Re: University of New England – Patient Care Center; Stevens Avenue, Portland
Ability to Serve with PWD Water

Dear: Mr. Saucier

The Portland Water District has received your request for an Ability to Serve determination for the noted site submitted on March 2, 2012. Based on the information provided, we can confirm that the District will be able to serve the proposed project as further described in this letter.

Please note that this letter does not constitute approval of this project from the District. Please review this letter for any special conditions specified by the District and to determine the appropriate next steps to take to move your project through the submittal and approval process.

Existing Site Service

According to District records, the project site does currently have existing water service. Three 3/4-inch diameter copper water service lines, located as shown on the attached water service cards, provide water service to 1 College Street, 746 Stevens Avenue and 750 Stevens Avenue. Please refer to the "Conditions of Service" section of this letter for requirements related to the use of these services.

Water System Characteristics

According to District records, there is a 6-inch diameter cast iron water main on the north side of College Street and an 8-inch cast and a 12-inch cast iron cement lined water main on the east and west sides of Stevens avenue as well as two public fire hydrants located across the street from the site.

The current data from the nearest hydrant with flow test information is as follows:

Hydrant Location: Stevens Avenue opposite College Street
Hydrant Number: POD-HYD01290
Last Tested: 7/20/1994
Static Pressure: 58 psi
Residual Pressure: Not Measured
Flow: 1,074 GPM



Public Fire Protection

It is anticipated that this project will not include the installation of new public hydrants to be accepted into the District water system. The decision to require new hydrants and to determine their locations is solely that of the local fire department. It is your responsibility to contact your local fire department to ensure that this project is adequately served by existing and/or proposed hydrants.

Domestic Water Needs

The ability to serve request noted that the peak domestic water demand of the proposed facility would be 125 gallons per minute and a daily consumption of 8,000 to 10,000 gallons. The data noted above indicates there should be adequate pressure and volume of water to serve the domestic water needs of your proposed project.

Private Fire Protection Water Needs

You have indicated that this project will require water service to provide private fire protection to the site. Please note that the District does not guarantee any quantity of water or pressure through a fire protection service. Please share these results with your sprinkler system designer so that they can design the fire protection system to best fit the noted conditions. If the data is out of date or insufficient for their needs, please contact the MEANS Division to request a hydrant flow test and we will work with you to get more complete data.

Conditions of Service

It is anticipated that a new fire protection service and a new domestic water service will be installed to serve the new building. A condition of allowing these new services is that each of the three existing service lines must be retired by shutting the corporation valve cutting the copper from the main. Please submit the design plan to our office for review as your project progresses.

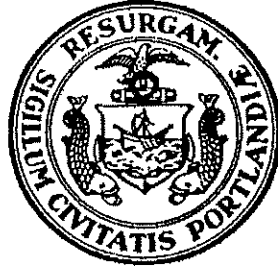
If the District can be of further assistance in this matter, please let us know.

Sincerely,
Portland Water District


Rico Spugnardi, P.E.
Business Development Engineer

CITY OF PORTLAND WASTEWATER CAPACITY APPLICATION

A.10b



Department of Public Services,
55 Portland Street,
Portland, Maine 04101-2991

Mr. Frank J. Brancely,
Senior Engineering Technician,
Phone #: (207) 874-8832,
Fax #: (207) 874-8852,
E-mail: fjb@portlandmaine.gov

Date: 4/17/12

1. Please, Submit Utility, Site, and Locus Plans.

Site Address: 1 COLLEGE STREET 195 B046001
 (Regarding addressing, please contact Leslie Kaynor, either at 756-8346, or at LMK@portlandmaine.gov) Chart Block Lot Number: 195B042001
 Proposed Use: COLLEGE OF NATURAL MEDICINE
 Previous Use: OFFICE ADMINISTRATION
 Existing Sanitary Flows: _____ GPD
 Existing Process Flows: _____ GPD
 Description and location of City sewer, at proposed building sewer lateral connection: _____

Site Category:
 Commercial _____
 Industrial (complete part 4 below) _____
 Governmental _____
 Residential _____
 Other (specify) COLLEGE/UNIVERSITY

Clearly, indicate the proposed connection, on the submitted plans.

2. Please, Submit Domestic Wastewater Design Flow Calculations.

Estimated Domestic Wastewater Flow Generated: 8000-10000 GPD
 Peaking Factor/ Peak Times: PEAK FLOW 125 gpm -
 Specify the source of design guidelines: (i.e., "Handbook of Subsurface Wastewater Disposal in Maine," "Plumbers and Pipe Fitters Calculation Manual," "Portland Water District Records," Other (specify) ENGINEER)

Note: Please submit calculations showing the derivation of your design flows, either on the following page, in the space provided, or attached, as a separate sheet.

3. Please, Submit Contact Information.

Owner/Developer Name: ALAN THIBEault
 Owner/Developer Address: UNIVERSITY OF NEW ENGLAND
11 HILLS BEACH ROAD - BIDDINGTON, ME 04005
 Phone: 602-2253 Fax: _____ E-mail: ATHIBEAULT@UNM.EU
 Engineering Consultant Name: SITE DESIGN ASSOCIATES - TIM SAULIER
 Engineering Consultant Address: 23 WHITNEY WAY, TORSHAM, ME 04096
 Phone: 449-4275 Fax: _____ E-mail: TSAILIER@SITEDESIGNASSOCIATES.COM
 City Planner's Name: _____ Phone: 812

Note: Consultants and Developers should allow +/- 15 days, for capacity status, prior to Planning Board Review.

4. Please, Submit Industrial Process Wastewater Flow Calculations

Estimated Industrial Process Wastewater Flows Generated: _____ GPD
 Do you currently hold Federal or State discharge permits? Yes _____ No _____
 Is the process wastewater termed categorical under CFR 40? Yes _____ No _____
 OSHA Standard Industrial Code (SIC): _____
 Peaking Factor/Peak Process Times: _____

(<http://www.osha.gov/oshstats/sicser.html>)

Note: On the submitted plans, please show the locations, where the building's sanitary, and process water sewer laterals, exit the facility, where they enter the city's sewer, the location of any control manholes, wet wells, or other access points, and the locations of any filters, strainers, or grease traps.

Code Review – NFPA 2009

Chapter 6 – Classification of Occupancy and Hazard of Contents

Section 6.1.2.1 – The Simulation space on the second level has an occupant load of 54 people and is thereby defined as Assembly.

Section 6.1.11.1 – Per the commentary, the remainder of this building (except for mechanical and storage areas) is defined as a Business occupancy.

Section 6.1.14.2.3 – Due to the Simulation space having an Occupant Load of over 50 people, it is required to be separated by a 1-hour rated assembly per Table 6.1.14.4.1(b).

Chapter 7 – Means of Egress

Section 7.1.3.1 – Corridors used as exit access are required to be 1-hour rated per this section. However, per subsection (2) of this section, they are not required to be rated per Section 38.3.6(3).

Section 7.1.3.2.1 – The two exits that serve this building are required to be 1-hour rated (connecting three stories or less).

Section 7.1.3.2.2 – The exit enclosures provide continuous protected path of travel to an exit discharge.

Section 7.2.1.2.4 – Door openings in the Means of Egress shall be 36” wide (32” min. clear width). Also, per Section 7.3.3 (Table 7.3.3.1), the door components shall provide for 0.2 inches per occupant (345 total load / 173 per floor / 87 per exit door): $87 \text{ people} \times .2" = 17.4"$ (use 32” clear min.).

Note: Door hardware (panic devices, closers ect...) are as yet to be determined, but will adhere to all code requirements.

Section 7.2.2.2.1.2(B) – Due to the occupant loads being greater than 50, the minimum stair widths are 44” per Table 7.2.2.2.1.2(B) for occupant loads less than 2,000 people.

Note: Maximum risers shall be 7”, minimum tread depths are 11”, and there will be less than 12'-0” between landings (per Table 7.2.2.2.1.1(a)).

Section 7.2.2.2.3.2.4 – Landings shall be 48” wide (in the direction of travel).

Section 7.2.2.4.1.1 – Handrails will be provided on both sides of the stairs and conform to Section 7.2.2.4.4.

Section 7.2.2.4.5.2 – Guards shall be 42” high and conform to Section 7.2.2.4.5.3.

Section 7.2.2.5.1.1 – Stairs shall be enclosed as required (also see Section 7.1.3.2).

Section 7.2.2.5.2.2 – Construction of the stair enclosures shall extend to the roof line.

Section 7.2.2.5.4.2 – Signage for the lower level, showing the direction to the level of exit discharge will comply with this section.

Section 7.2.5 – Exterior ramps shall comply with all dimensional requirements of this section.

Section 7.2.12.2 – An area of refuge is provided in one of the stairwells (also see Section 7.5.4.4).

Section 7.2.13 – This section does not apply since the elevators in this building are not considered means of egress.

Section 7.3.1.2 – The occupant load is as follows (per Table 7.3.1.2):

Basement Level: 2,423 s.f. / 500 s.f. per person = 5 persons

First Level:

Business Use: 11,965 s.f. / 100 s.f. per person = 120 persons

Utility/Storage Use: 1,463 s.f. / 500 s.f. per person = 3 persons

Assembly Use: 2,431 s.f. (designed for 54 persons) = 54 persons

Second Level:

Business Use: 15,679 s.f. / 100 s.f. per person = 157 persons

Utility/Storage Use: 160 s.f. / 500 s.f. per person = 1 person

Total Occupant Load: 340 persons

Section 7.3.3.1 – The egress capacity shall be .3” per occupant for the stairs. Each stair to serve a maximum of 79 persons (158 total occupant load on the second floor, divided by 2). Therefore, 79 persons x .3” per person = 23.7” (use minimum width of 44” per Section 7.2.2.2.1.2(B)).

Section 7.4.1.1 – There are two means of egress provided from the first level and second level (2 means of egress are required).

Section 7.4.1.1(1) – There is a single exit out of the basement level per 38.2.4.3.

Section 7.5.1.3.3 – The diagonal distance of the building is 205’. Because the building is sprinkled, the minimum distance between exits shall be 68’-4”. The actual distance between exits is 161’-4”.

Section 7.5.4.1.1 – Only one accessible means of egress from the second floor is provided. This exit stairwell is accessible from all areas of the second floor within the allowable travel distance per Section 38.2.6.1 (300’).

A.11b

Section 7.5.4.4 – The exit stair that is used as an accessible means of egress is provided with an area of refuge, and is compliant with Section 7.2.12.3.

Section 7.7.1 – Both stairwells terminate directly to the exterior.

Note: Per Section 7.7.2, the discharge from the second floor is permitted to go through areas on the first floor (50% of occupants and 50% of exits). Exit is through an open lobby space and the building is sprinkled (Sections 7.7.2.3 and 7.7.2.4).

Section 7.12.1(1)(a) – The common path of travel out of the Mechanical basement is be less than 100' (89'-8").

Section 7.12.2 – The Mechanical basement is allowed to have one exit.

Chapter 8 – Features of Fire Protection

Section 8.3.4 – Opening protectives in fire resistant rated assemblies shall comply with Section 8.3.4.2.

Section 8.5 – The smoke barriers that enclose the Communicating Space (see Section 8.6.6) shall comply with this section.

Section 8.6.5(2) – The fire resistant rating of floor opening enclosures (shafts) shall be 1-hour as they are connecting less than four stories.

Section 8.6.6 – At the North entrance to the building, a Communicating Space is located. This space is not prohibited by Chapter 38 of this code, and will be compliant with this Section.

- (1) The communicating space connects two stories.
- (2) The lowest floor area of the connecting space is a street floor (and the level of exit discharge).
- (3) The floor area is open and unobstructed.
- (4) Per subsection (a), the building is sprinkled and therefore the communicating space is required to be separated by smoke barriers (not 1-hour rated walls).
- (5) The hazards within the space are ordinary hazards.
- (6) The occupant load of the entire communicating floor area is: 2,167 sf on the 1st floor + 1,539 sf on the 2nd floor = 3706 sf / 100 sf per person (Business) = 38 persons. All of the 38 occupants can exit out of the stair at once as the stair has the required egress capacity. Persons on the second floor communicating space can egress by entering the stair and then going directly outside. Persons on the first floor communicating space can egress through the front door, or if they have to...into the stair and directly out.
- (7) Each occupant of the communicating space has access to the exit without having to go back through the communicating space (see note 6 above).
- (8) Each occupant not in the communicating space has access to an exit without entering the communicating space. All of the occupants not in the communicating space have access

to the other exit. From the furthest point (not in the communicating space) to that exit is 185' (the maximum travel distance to an exit is 300' per Section 38.2.6.1).

Section 8.6.10.1 – Draftstopping of concealed spaces are as follows:

- (1) Walls and partitions at each floor level, top floor ceiling level, and level of roof support.
- (2) The unoccupied space formed by the sloped roof at 3,000 s.f. (maximum).
- (3) Concealed spaces between the ceiling and the floor above at 1,000 s.f. maximum, and the concealed space between the second floor ceiling and roof at 3,000 s.f. maximum.

Chapter 9 – Building Service and Fire Protection Equipment

Section 9.4 – The elevators shall comply with this section and the requirements of ASME A17.1.

Section 9.4.5 – The elevators do not have travel distances that exceed 50' above the level of exit discharge or 30' below the level of exit discharge. Therefore, the independent ventilation requirements for the machine rooms are not required.

Section 9.6 – The Fire Detection and alarm systems shall be installed in this building and comply with this section.

Section 9.7 – The automatic sprinkler system shall be installed in this building and comply with this section.

Section 9.7.4 – It is anticipated that two fire extinguishers will be installed on each floor of the building. All fire extinguishers shall comply with this section of the code and with NFPA 10.

Chapter 10 – Interior Finish, Contents and Furnishings

Section 10.2.8 – Automatic sprinklers will be installed in this building. Class A, B, and C finishes will be installed accordingly and comply with this section of the code.

Chapter 38 – New Business Occupancies

Section 38.2.3.2 – The clear width of all corridors and passageways serving occupant loads over 50 persons shall be greater than 44" in width.

Section 38.2.4.3 – A single exit will be provided out of the basement level.

Section 38.2.5.2.1 – Dead-end corridors shall not exceed 50' (building is sprinkled).

Section 38.2.5.3.1 – Common path of travel shall not exceed 100' (building is sprinkled).

Section 38.2.6.1 – The travel distance to an exit shall not exceed 300' (building is sprinkled).

A.11d

Section 38.2.9.1 – Emergency lighting shall be provided (building has more than 50 occupants above the level of discharge).

Section 38.3.1.1 – Vertical openings (except for the Communicating Space per Section 8.6.6) shall be protected as required by Section 8.6.

Section 38.3.1.2 – The basement floor area does not have unprotected openings to the floor above.

Section 38.3.4.1(2) – This building will have more than 50 occupants above the level of exit discharge and will be required to install a fire alarm system that is compliant with this code.

Section 38.3.5 – Fire extinguishers shall be installed (also see Section 9.7.4).

Section 38.3.6.1(3) – This building is fully sprinkled and is not required to have fire-rated corridors.

Section 38.7.1 – This building will have an occupant load greater than 100 above the level of exit discharge and will be required to have scheduled fire drills in accordance with Section 4.7 of this code.

Section 38.7.2 – Fire extinguishers will be installed in this building and periodic training for designated employees is required.

City of Portland Technical Manual – Chapter 3

3.1 Crime Prevention Through Environmental Design Principles

3.1.1 – Due to its location on Stevens Avenue, the UNE Patient Care Center will be a visible building. Safety is of paramount importance to the University, its faculty, students, staff, and patients. It is the intent of this development to be as open and accessible as possible. Three sides of the building are located adjacent to a public way, College Street to the South, Stevens Avenue to the East and a public parking lot to the North. A way for pedestrians and UNE service vehicles is proposed between Alexander/Linnell and the Patient Care Center.

3.1.1(a) – This development will have two separate and distinct entrances/exits to the building. There is a campus entrance to the South along College Street, and a public entrance to the North, directly adjacent to the public parking. The campus entrance to the South will utilize an open plaza with low landscaping, seatwalls and lighting as required to maintain high visibility. The public entrance to the North will be located within an entrance atrium to assist the public in orientation of the development. Both entrances will use a moderate amount of glazing to insure visibility into the building from the exterior. At this time, no walls or fences are contemplated within this development. The area to the West of the building (adjacent to the existing campus buildings) is slightly removed from the public way. This area will be kept with wide pedestrian pathways and site lighting to ensure safety.

3.1.1(b) – The University is aware of the importance of site lighting with regards to campus safety. Due to its proximity to Stevens Avenue, this development will be able to utilize large expanses of glazing on the building to provide much of the site lighting. Additional pathway lighting will also be used as required. Both entrances (as described above) will use canopies to increase lighting at those locations, and to direct the lighting downwards to the paths of travel, without light trespass off-site.

3.1.1(c) – It is not anticipated to have video surveillance systems. However, the University of New England employs an emergency campus security intercom system.

3.1.2 – The University has 24-hour security officers that regularly patrol the campus. All pathways on the campus are designed to be highly visible. The campus is not closed off, but is an open campus.

3.1.2(a) – This building will have the public entrance located directly adjacent to the parking. A welcoming atrium element and building signage will assist the public with orientation.

3.1.2(b) – Because this is a public building, it will be open during normal business hours. However, it is a University building and will be required to be accessed during off-hours. The University will install their standard key-card access system.

A-11F

3.2 Fire Hydrant Standards

3.2.1 – This development is in the Fire District #1. There are three existing public fire hydrants located within 500' of the development. On the east side of Stevens Avenue, there is a hydrant located approximately 115' southeast of the southeast corner of the proposed development. The farthest point of the building is located 310' away from this fire hydrant. On the South side of College Street, there is a hydrant located 190' from the Southwest corner of the building, with the farthest point of the building located 355' from the hydrant. The last hydrant is located on the East side of Stevens Avenue, approximately 95' due East of the Northeast corner of the building, with the farthest point of the building located 275' from this hydrant.

3.2.2 – The hydrants are public hydrants which we assume meet all standards of city code Chapter 10 and of the Portland Water District.

3.2.3 – There are no private fire hydrants located within this development.

3.3 Single and Two Family Residential Development

3.3.3 – Not applicable.

3.4 Site Access Standards

3.4.1 – There are no dead-end roadways located in this development.

3.4.2 – This development will provide Fire Department access on three sides of the building. The building will have access from the North side via the parking lot. Access on the East side is gained by Stevens Avenue, and on the South side by College Street. There is a drop-off zone on Stevens Avenue that will be accessible to the Fire Department.

3.4.3 – Per zoning requirements, this building has a 30' setback from the property lines to the South, and East, and greater than 75 feet to the north. It is also compliant with the required 20' setback from the existing UNE buildings to the West.

3.3.4 – There are no fire department access roads proposed for this development. However, the main entrance is located approximately 40' from fire department access in the North parking lot. The campus entrance is located approximately 45' from fire department access to the South via College Street.

3.4.5 – The clearance height to the site is unrestricted.

3.4.6 – The elevator will be sized to accommodate an 80" x 24" stretcher.

3.4.7 – The assigned street number will be visible from the public right-of-way on the northeast

corner of the building.

3.5 Standards for Emergency Access Lanes and Gates

3.5.1 – 3.5.12 – There are no emergency access lanes or gate proposed for this project.

3.6 Subdivision Standards

3.6.1 – 3.6.4 – This development is not a subdivision.

3.7 Standards for Blasting and Regulation of Explosives

3.7.1 – 3.7.16 – From our experience in recent construction in the vicinity (UNE College of Pharmacy) and the Geotechnical Report by Summit Engineering, no blasting is anticipated.

3.8 Model Blasting Plan

Not applicable.

3.9 Model Notification Letter

Not applicable.

3.10 Model Offer of Pre-Blast Survey Notice

Not applicable.

3.11 Door Hanger Model Language

Not applicable.

3.12 Model Emergency Access Lane Maintenance Agreement

Not applicable.

A.12

Stormwater Management

This project involves the demolition of several wood frame structures and existing parking areas on the campus in order to construct an approximately 18,200 sf footprint building. A portion of runoff from the building site at the corner of College Street and Stevens Avenue drains to the existing city systems in those streets via overland flow. At this point in time, it appears the building will result in a net increase of approximately 4400 sf of impervious area on the development site.

However, when the College of Pharmacy was permitted in 2008, the city approved stormwater management plan demonstrated a reduction in impervious area of 6000 sf on the campus. With this project, the extent of impervious area on the campus will continue to be 1600 sf less than prior to the College of Pharmacy project. Both sites are tributary to the same system in Stevens Avenue.

As a further benefit of this project, an infiltration system will be constructed to replace existing infiltration systems displaced by the construction, and to collect runoff from the roof of the proposed building. This will serve to further reduce runoff from the UNE property to the existing storm and sanitary sewers in Stevens Avenue.

Therefore, construction of this project should not adversely affect the receiving stormdrain systems, as runoff rates and quantities will remain less than existing rates.

A.13

Solid Waste

Estimates

20 c. y waste per week

4 c.y. single sort recyclables per week

4 c.y. cardboard recycled per week

All transported to Ecomaine, via Waste Management

A.14

Construction Management Plan

UNE recently retained Allied Cook Construction to serve as construction managers for this project. Allied Cook also constructed the College of Pharmacy building and is very familiar with city of Portland requirements regarding safety, site security, and housekeeping.

The construction site will be fenced with a perimeter chain link fence. The sidewalks along Stevens Avenue and College Street will remain in service to the extent practical. When the sidewalks must be closed, a clearly defined alternate safe route for pedestrians will be provided.

When it is necessary to undertake excavation within the city right of way, city staff will be notified in accordance with the city notification procedures.

Prior to any site work, a pre-construction meeting will be scheduled on site. Attendees will include representatives for the owner, site contractor, general contractor, and city staff as appropriate.

Construction will take place within the time frames permitted by the city ordinances.



Biddeford Campus
11 Hills Beach Road
Biddeford, ME 04005
(207) 283-0171 T

Portland Campus
716 Stevens Avenue
Portland, ME 04103
(207) 797-7261 T

April 20, 2012

Alex Jaegerman
Director of Planning
City of Portland
389 Congress Street
Portland, ME 04101

RE: Site Plan Conditions of Approval
University of New England College of Pharmacy
714 Stevens Avenue
CBL 144A-005-001

Dear Mr. Jaegerman;

This submission is intended to fulfill the requirements of the February 28, 2008 *Site Plan Conditions of Approval* and the May 6, 2010 *Extension of Conditions of Approval* included in the Site Plan and Building Permit for the University's College of Pharmacy Building, on its Portland Campus. In addition, the submittal addresses the Planning Board Comments that UNE received at the April 10, 2012 workshop.

A Site Locale Plan, a UNE Land Holdings Plan 2012, and an Existing Conditions Plan are attached, immediately following this cover letter, to allow for an easier interpretation of the information presented within.

We respond to these conditions of approval and subsequent Planning Board comments as follows:

1. *The Planning Board grants an extension to February 24, 2012 for meeting the condition of approval regarding the Campus Master Plan.*

The original condition of approval, dated February 28, 2008, required that UNE submit to the Planning Board a Master Planning process for review.

The plan has been revised per the discussion with and comments received from the Planning Board on April 10, 2012. This revised plan, dated April 20, 2012, is attached.

See **Attachment A**, dated 04/20/12.

2. *The Planning Board grants an extension to February 24, 2012 for meeting the condition of approval regarding the Long-term Parking Strategy Plan.*

The original condition of approval required that UNE submit to the Planning Board for review and approval a long-term parking strategy plan which shall address, among other items, the deficit parking that is depicted on page 8 of the Planning board Report #12-08. The plan has been revised per the discussion with and comments received from the Planning Board on April 10, 2012. This revised plan, dated April 20, 2012, is attached.

See **Attachment B**, dated 04/20/12.

3. *The Planning Board grants an extension to February 24, 2012 for meeting the condition of approval regarding the Stormwater Management Plan.*

The original condition of approval required that UNE submit to the Planning Board for review and approval a campus stormwater management plan. The plan submitted on April 1, 2012 is attached.

See **Attachment C**, dated 04/01/12.

4. *The Planning Board conditions the foregoing granting of the extensions that a Traffic Demand Management Plan (TDM) shall be incorporated as an integral part of the long term parking management strategy and campus master plan. The TDM plan submitted 04/01/12, is attached.*

See **Attachment D**, dated 04/01/12.



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5. *The Planning Board amends the site plan condition #1 to October and April and shall continually monitor the long term parking strategy plan excluding April 2010.*

The University will continue to monitor the parking situation on the campus and surrounding areas, including continuation of the parking counts in October and April each year. These counts will be utilized to evaluate the overall effectiveness of the long-term parking strategy plan.

The University has initiated neighborhood meetings to discuss the parking situation in the surrounding areas. With these ongoing discussions, the University can better understand and subsequently mitigate the impacts of the University's parking demand on the greater community.

In response to comments received from the Planning Board on April 10, 2012, the University offers the following clarifications and/or responses.

We trust that you will find this information sufficient. If you require any additional information in this regards, please feel free to contact me. I can be reached at 207.602.2253.

I look forward to reviewing this information with the Planning Board at a Public Hearing in May 2012.

Respectfully,

A handwritten signature in cursive script that reads "Alan Thibeault". The signature is written in black ink and is positioned above the typed name.

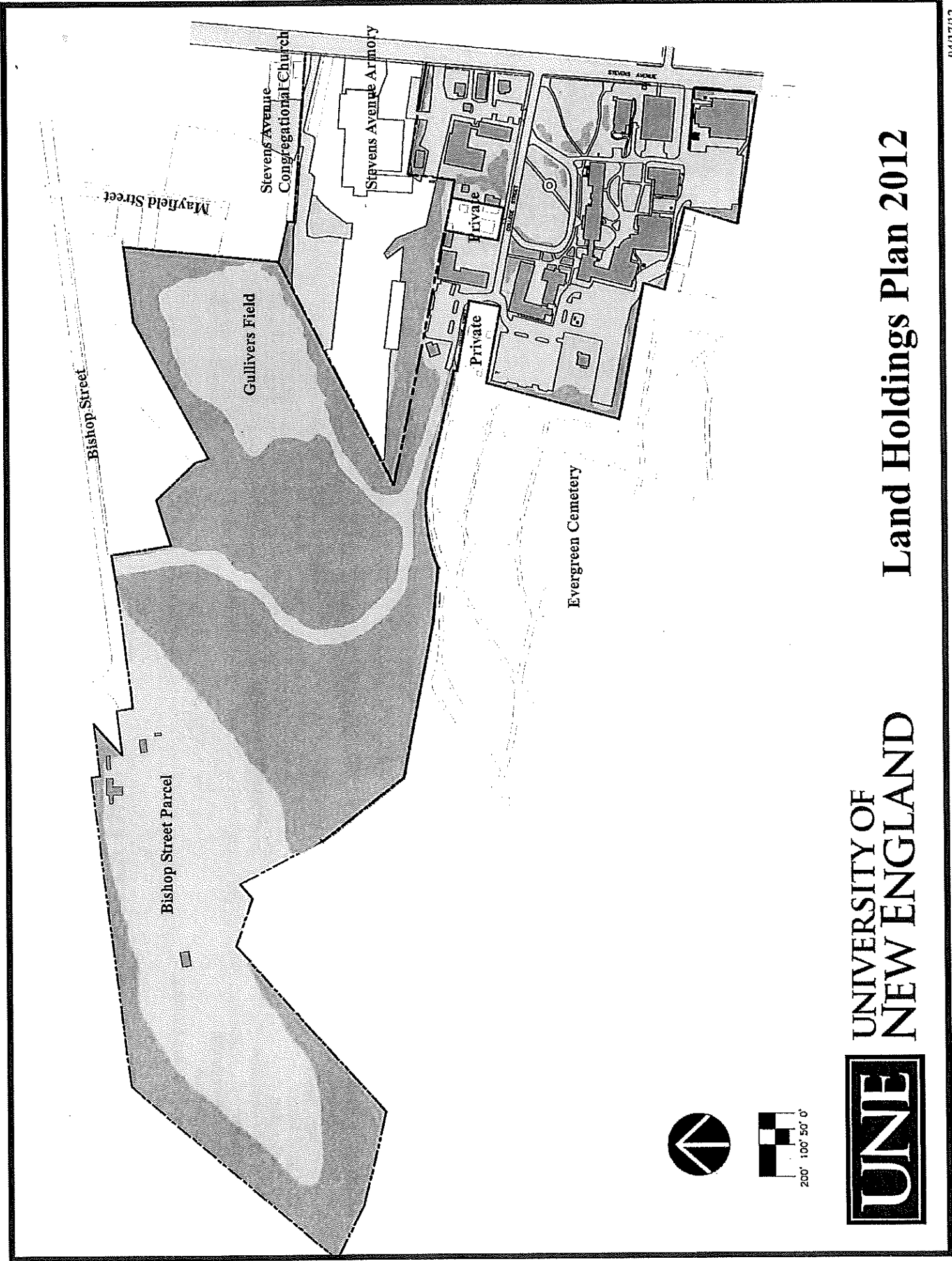
Alan Thibeault
Assistant Vice President of Planning

Cc William J. Bola, Vice President of Operations



UNIVERSITY OF
NEW ENGLAND

Campus Locale Plan 2012



UNE UNIVERSITY OF
NEW ENGLAND

Land Holdings Plan 2012

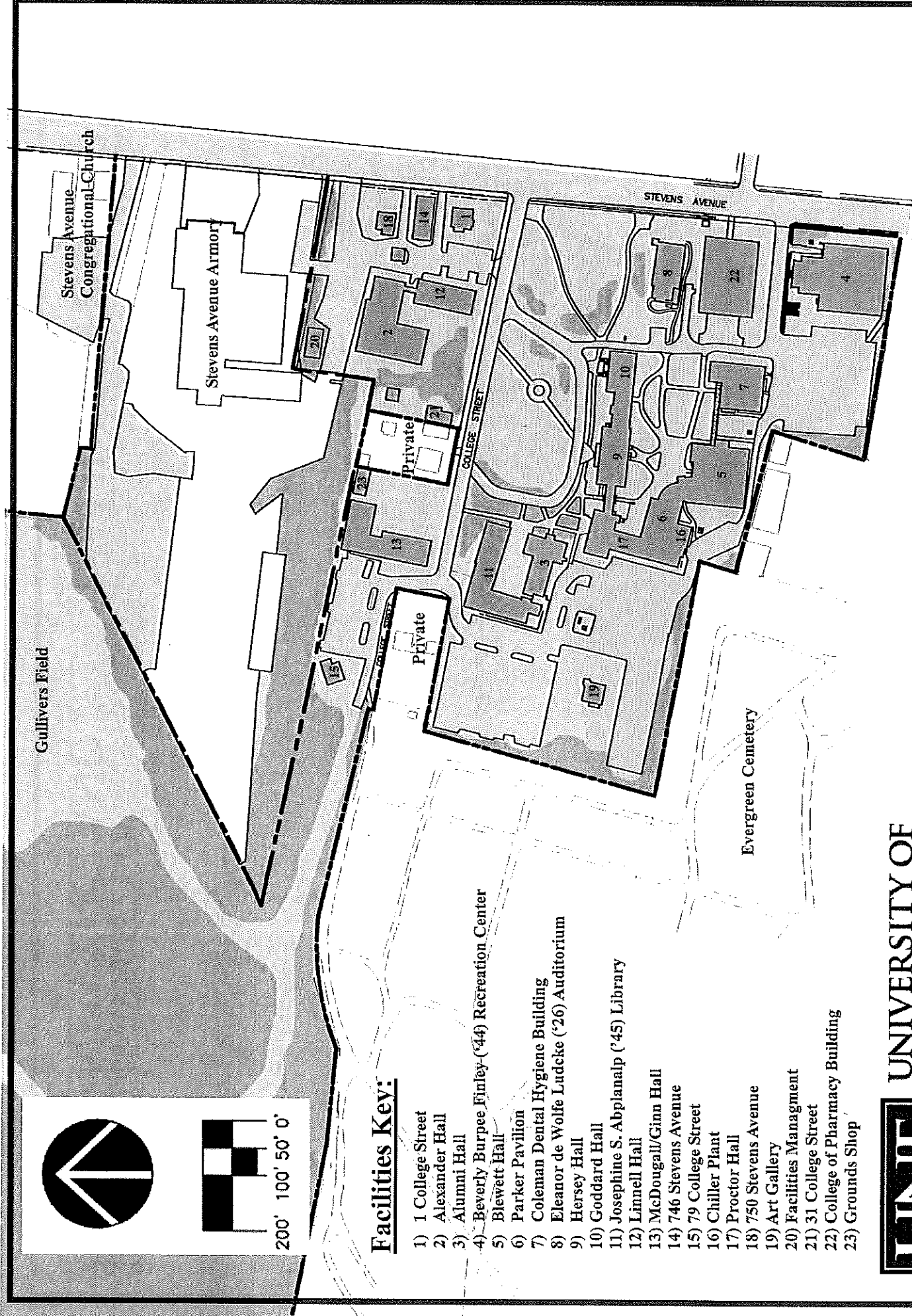
04/17/12



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Facilities Key:

- 1) College Street
- 2) Alexander Hall
- 3) Alumni Hall
- 4) Beverly Burpee Finley ('44) Recreation Center
- 5) Blewett Hall
- 6) Parker Pavilion
- 7) Coleman Dental Hygiene Building
- 8) Eleanor de Wolfe Ludcke ('26) Auditorium
- 9) Hersey Hall
- 10) Goddard Hall
- 11) Josephine S. Abplanalp ('45) Library
- 12) Linnell Hall
- 13) McDougall/Ginn Hall
- 14) 746 Stevens Avenue
- 15) 79 College Street
- 16) Chiller Plant
- 17) Proctor Hall
- 18) 750 Stevens Avenue
- 19) Art Gallery
- 20) Facilities Management
- 21) 31 College Street
- 22) College of Pharmacy Building
- 23) Grounds Shop



UNIVERSITY OF
NEW ENGLAND

Existing Conditions Plan 2012

Attachment A

04/20/12

The Planning Board grants an extension to February 24, 2012 for meeting the condition of approval regarding the Campus Master Plan.

The University is pleased to submit to the Planning Board its Master Plan. Please see the *UNE Land Holding Plan 2012*, and the *Existing Conditions Plan*, located immediately following the cover letter, and the *Campus Master Plan 2012 – 2017*, which is attached at the rear of this section. These have been revised to address the comments received from the Planning Board at the April 10, 2012 workshop.

The *Existing Conditions Plan* illustrates the current development on the main campus. It identifies the location of the existing buildings, structures and facilities, parking, walkways, service areas, and other impervious surfaces.

The *Campus Master Plan 2012-2017* illustrates the potential development that UNE envisions in the next five years. This plan includes the newly acquired parcel on Bishop Street. This parcel of approximately 19 acres was formerly owned by Pike Industries and utilized as an asphalt plant, and currently has approximately 12.5 acres of impervious surface.

In addition to this purchase, the University continues to actively pursue other acquisition options proximal to the campus, and is engaged in on-going discussions with another land owner.

University of New England Mission Statement

“The University of New England provides a highly integrated learning experience that promotes excellence through interdisciplinary collaboration and innovation in education, research, and service.”

The University is a private, non-profit corporation with recognized tax exempt status under IRS Code Section 501(c)(3), and must abide by the rules of tax exempt status in order to maintain that standing. Accordingly, only those uses permitted by code would be pursued.

The institution’s objectives with respect to growth and changes on its Portland Campus over the next five (5) years include:

1. In keeping with the University of New England’s role as a regional leader in health professions education, the UNE Board of Trustees voted in 2010 to approve the launching of Maine’s first College of Dental Medicine. The University plans to construct the required facilities to support this new college. (Please see the attached *University of New England College of Dental Medicine Progress Report - Winter 2012.*)

2. Add a cadre of new faculty and staff to adequately support academic programs and support units.
3. The University of New England will be nationally recognized with programs of excellence and innovation in health professions and the arts and sciences. The University will have well developed centers of excellence in research, and will have integrated educational, research, and science programs that are based on collaborative, interdisciplinary partnerships.
4. Cocurricular programs are central to the mission of the University for educating students and supporting the personal growth of all members of the university community.
5. Increase diversity in national and international populations.
6. UNE will have a strong program of service at all levels of our society and be a leader in environmental stewardship.
7. The planning and integration of our academic and support facilities will be viewed as a model for building interdisciplinary, team based learning and scholarly communities.

Existing Conditions

**University of New England Facilities
Portland Campus**

Abplanalp Library- Originally constructed in 1900, as McArthur Gymnasium, this three level wood frame and masonry building was renovated and expanded into a 26,636 SF Library in 1986. A new central HVAC system was completed in 2001.

Alexander Hall- This 13,912 SF two story masonry building was constructed in 1962 and serves as the Student Union complex, housing the Dining Hall and Campus Bookstore.

Alumni Hall- This 9,809 SF three story brick bearing wall and wood frame masonry building with a wood addition, was constructed in 1834 and is capped by a bell tower that was relocated from Portland City Hall at the time of its construction. It serves as an academic facility.

Art Gallery- Constructed in 1976, this three level, 2,809 SF masonry building is used to exhibit works of art.

Blewett Hall- This 31,650 SF academic facility contains classrooms and laboratories. This three story masonry facility was constructed in 1967, and was partially renovated in 1998 and 2001.

Coleman Hall- This wood/masonry split level facility is home to the Dental Hygiene Program and contains a Dental Hygiene Clinic, which is open to the public. It was constructed in 1961 and added onto in 1968 and 1976.

College of Pharmacy Building - This 46,380 SF academic/research facility was completed in 2009.

Finley Recreation Center- Constructed in 1990, this 20,000 SF steel frame masonry veneer facility is used for recreational purposes. It contains a gymnasium and weight training and fitness rooms.

Ginn and McDougall Halls- This four story masonry complex of 34,091 SF, constructed in 1964, is used for support offices. The central campus heating facility, completely updated in 2001, is located in its basement.

Goddard Hall- This 1857 masonry brick wood framed building was completed renovated in 2011 and is used for academic support.

Grounds Shop- This one story wood structure, built circa 1900, is used to house Grounds equipment and supplies.

Hersey Hall- This 22,170 SF four story masonry structure with wood framing, originally built in 1869, was completely renovated to academic offices in 2001.

Linnell Hall- This two story, 12,762 SF brick building, is used for support offices, and was constructed in 1962.

Ludcke Auditorium- Built in 1867, this former 9,676 SF wooden church, was renovated in 1986, and is currently used as an auditorium facility.

Maintenance Shop- This 2 ½ story wooden structure is used to house the maintenance shops. It was built approximately 100 years ago and is 2,300 SF.

Parker Pavilion- The 6,600 SF masonry/steel structure building was constructed in 2001, and is used for academic support.

Proctor Hall- Constructed in 1951, this 31,074 SF four story masonry brick building is used for academic purposes. Renovations were completed in the late 1990's.

750 Stevens Avenue - This single family style residential property of 2,250 SF, was renovated in 2005.

Residential Style Properties- These properties (1 College Street – 2,250 SF, 31 College Street – 2129 SF, 79 College Street – 720 SF, 746 Stevens Avenue – 5,056 SF, and 750 Stevens Avenue – 2,250 SF) are used as support spaces by the University.

Bishop Street Parcel- This property has several outbuildings that could be utilized to support the University’s programmatic needs. These could include the adaptive re-use for Facilities Support Services; for example Maintenance, Grounds and or Fleet services.

Population Projections

In the Fall of 2010, the University re-assigned all its first and second year undergraduate students to its Biddeford Campus to provide for a more complete undergraduate experience and to minimize the duplication of similar academic environments. This resulted in the reallocation of approximately 100 students a year. These students will return to the Portland Campus beginning the Fall of 2012, to complete the “professional” component of their degree. In the Fall of 2013, the second cohort will return to the Portland Campus, in conjunction with the arrival of the first year of the College of Dental Medicine students. The College of Dental Medicine will enroll up to 46 students per year. During this four year program, the students will be on campus for the first three years and out on rotations during the four year. The following illustrates the anticipated student population (FTE) on campus over the next five years.

	2011-12	2012-13	2013-14	2014-15	2015-16	2016-17
Students Projections	818	941	1097	1143	1189	1189

Proposed Development

In support of these objectives, the University will initiate the development as illustrated on the *Campus Master Plan 2012-2017* drawing.

This development plan includes potential renovations to improve academic support spaces in Alumni Hall (S1) and Finley Recreational Center (S2). These renovations are to be conducted in support of our academic mission, and should result in no additional classroom square footage. In support of the College of Dental Medicine, the University will construct the new Patient Care Center (A1). This facility is anticipated as being a two story 36,000sf facility, and will result in additional classroom and patient care center square footages. This new facility and its additional student population was included in the *Traffic*

Demand Management Plan (TDM), located in Attachment D and accounted for in the *Parking Assessment/Long-term Parking Strategy Plan*, located in Attachment B.

The development of the Patient Care Center will require the displacement of three of the existing University owned facilities. These include two administrative/support facilities located at 1 College Street and 746 Stevens Avenue. These three story buildings would be impractical to relocate due to their size, and thus are anticipated to be removed. The facility at 750 Stevens Avenue is a two story single family residence that has only been rented to UNE students since 2005. Prior to 2005, this facility was used to house the offices of the Women's Literary Union, for several decades. The University has offered the building to Habitat for Humanity, but they are unable to relocate the facility. The University continues to pursue the option of relocating this facility with a real estate broker. Should the University be unable to find an opportunity to relocate this facility, then it will need to be removed from the site. While this facility is considered a dwelling unit per the City code, it is the University opinion that "*the proposed use will not cause significant displacement or conversion of residential uses*". This opinion was discussed at a March 30, 2012 meeting with Greg Mitchell, Alex Jaegerman, and Barbara Barhydt, and was understood by the University to be supported by City Staff.

The development presented in the *Campus Master Plan 2012-2017*, results in a projected parking deficit on the campus. This deficit can be mitigated by the utilization of the newly acquired parcel and its existing impervious surface, as a parking area, as noted with a "P", on the *Campus Master Plan 2012-2017*. This plan includes the creation of an Intermodal Transportation Facility at the newly acquired property on Bishop Street. In utilizing the 12.5 acres of impervious surface currently existing at the site, the University now has the ability to meet its entire long-term projected parking deficit on this parcel, without creation of any additional impervious surface. The UNE community members could park their vehicles at the facility, wait in a heated and secure shelter, and then board a shuttle that would bring them to the main campus. The shuttle service would make continuous trips, during peak academic hours, to insure minimal wait times. The University intends to develop plans for the site and submit them to the Planning Office for review in the near future, in order to begin utilization of the facility in the Fall of 2012.

There is an existing road connecting the main campus to Bishop Street. The University will look to maintain and or improve this road in order that service and shuttle service vehicles can utilize this as an access route between the different segments of the campus.

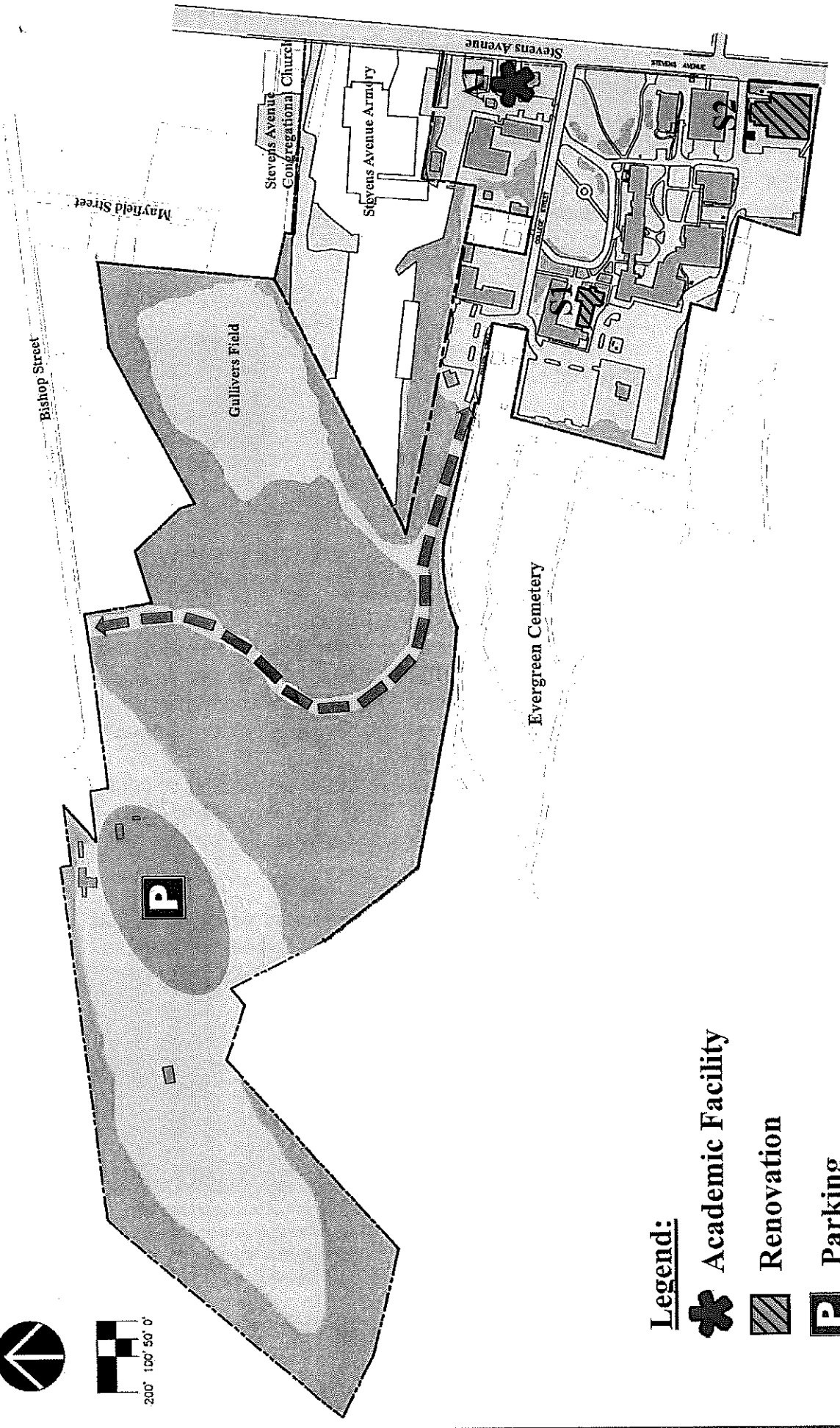
With the acquisition of the Bishop Street parcel, the vision for the future of the University of New England's Portland Campus is now subject to a new perspective. The University's land holdings are spread across four distinct zoning areas. This complicates any future development and has stimulated conversations with City Officials at all levels. It has been recommended by City Staff that the University work with the City to create an overlay zone

for the Portland Campus, similar to those in place at USM, MMC and Waynflete. The University is committed towards this goal.

It has become increasingly obvious over the past few years that the University's transition from an undergraduate to a graduate campus and the subsequent development has had an increasing impact on the community. The recent neighborhood meetings related to parking in the vicinity of the campus have given the University some good insight into the results of this impact. The University has made a commitment to the neighbors to continue with these meetings in an effort to enhance the relationship with our neighbors and to help shape the future development in a manner that minimizes the adverse impacts to the community. It will be through these ongoing dialogues that the University, City and neighbors can hope to achieve their mutual goals.




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Legend:

 Academic Facility

 Renovation

 Parking

 Vehicular Circulation



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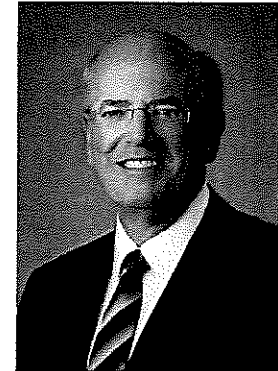
Campus Master Plan 2012 - 2017

University of New England
College of Dental Medicine
Progress Report | Winter 2012

INTRODUCTION

The University will enroll the first entering class for the UNE College of Dental Medicine in Fall 2013.

The founding dean of the College of Dental Medicine, James J. Koelbl, DDS, MS, MJ joined the University in March 2011 to oversee the College's Initial Application for Accreditation. His duties also included reviewing and evaluating the substantive planning that had been accomplished prior to his arrival.



The College of Dental Medicine team continues to work closely with the President, the Provost, other members of the University administration, and the deans and faculty of the other colleges within UNE to bring the plans for the new college to fruition.

With the help of Institutional Advancement, UNE applied for a Maine State Dental Bond for \$3.5 million towards the construction of a dental school teaching clinic. The voters of Maine passed this bond referendum in November 2010 and the money was awarded to UNE in August 2011. An additional \$1.5 million from the bond will be awarded to help expand the capability of community clinics to accept fourth-year dental students as part of the planned dental education program

Initially, the administration explored building the dental teaching clinic/simulation center in downtown Portland. Upon further review, in summer 2011 the University Board of Trustees approved plans to begin the process of building a clinical facility on an ideal site on the University's Portland campus.

The College of Dental Medicine continues to move confidently toward admitting its first entering class in fall 2013. The 2013 date clearly shows that UNE places higher value on long-term success than potential short term results. The advantages of this timing include:

- Better alignment of student recruitment, admissions and enrollment with the process/timeline for accreditation
- Further development of unique aspects of innovative curriculum, including the potential development of online graduate curriculum resources



Goddard Hall renovations were completed in late 2011, making this historic building the administrative home for the College of Dental Medicine.

- Creation of synergistic partnerships with other colleges and programs throughout the university for the sharing of academic resources
- Development of innovative, efficient patient care model, including the ability to utilize the expertise of new VP for Clinical Affairs
- Opportunities to create potential clinical partnerships to enhance the ability of the university to deliver high quality, comprehensive health care
- Efficiencies of scale by having simulation center, patient care center, and faculty offices on one campus
- Further development of true academic health science center (education and patient care) in Portland
- Establishes a stronger foundation for long-term success of UNE as leader in health care education, service and patient care
- Will increase the confidence of various constituencies (e.g., higher education; dental education; dental practice; accrediting bodies; potential faculty, staff students, etc.) in UNE's commitment to outstanding quality and enduring value to benefit the community and region

AMERICAN DENTAL EDUCATION ASSOCIATION MEMBERSHIP

At its March 2011 Annual Session, the House of Delegates of the American Dental Education Association (ADEA) formally approved the University of New England College of Dental Medicine's membership in that organization.

STUDENT ADMISSIONS

The College is finalizing plans for the recruitment and admissions process, and will participate in the national dental application service administered by the American Dental Education Association (AADSAS). It is expected that AADSAS will begin accepting online applications for the fall 2013 entering class on or about June 1, 2012.

The College is committed to a comprehensive review of student applications, and seeks a diverse and balanced student population and considers academic and non-academic criteria in selecting applicants who have the potential for success.

Admission to the DMD program is competitive and based on a "whole file review" process. Applicants are evaluated on academic record and scholastic ability; performance on the DAT; maturity; perseverance; motivation; letter of recommendation; type, scope and duration of community service; leadership potential; career goals; ethics; state of residence; and interpersonal skills. To ascertain these factors, an on-campus interview is required prior to a final admission decision. All interviews are by invitation only.



ACCREDITATION

The College submitted its initial application for accreditation to the Commission on Dental Accreditation on March 31, 2011. A team of Commission consultants reviewed the application. Subsequently, the Commission requested additional information on several issues. A written report is being finalized and will be submitted on or about February 1, 2012. The Accreditation site visit has been scheduled for April 3 - 4, 2012. At that time, the site visit team will review our overall plans and assess our progress in implementing those plans. In approximately thirty days after the site visit, UNE will receive a draft site visit report. We will then have an additional thirty days to respond and submit additional information. CODA's Pre-doctoral Review Committee at its regularly scheduled meeting in on July 16-17, 2012 will review a copy of the site visit report and any response from UNE. The full Commission at its regularly scheduled meeting on August 9 - 10, 2012 will then consider that committee's recommendation. The Commission on Dental Accreditation makes the final decision on the program's status, and that decision will be communicated to UNE shortly after the August meeting.

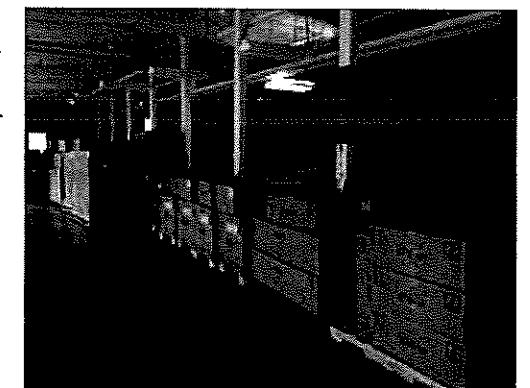


ADMINISTRATIVE SPACE

Work has been completed on the renovation of Goddard Hall on the Portland campus. This historic building will house administrative and faculty offices, a small lecture facility, some conference rooms, and student space. The College moved into its new facilities in early November 2011. An Open House for Westbrook College Alumni was held in Goddard in December 2011, and we received many favorable comments regarding the renovation of this 154-year old building.

CLINICAL & SIMULATION FACILITIES

After the decision by the Board of Trustees to house the college's clinical and simulation facilities on the Portland campus, requests for proposals from architectural firms were sent out in December of 2011. An architectural firm was chosen, and meetings with the architects began during the week of January 16, 2012. A schedule for the programming, design and construction of the building has been established, and it is anticipated that the facilities will be ready in August 2013. As plans for the Simulation Center and Oral Health Center continue, comprehensive lists of equipment and supplies are also being developed.



Dental operatories purchased through American Recovery and Reinvestment Act funding

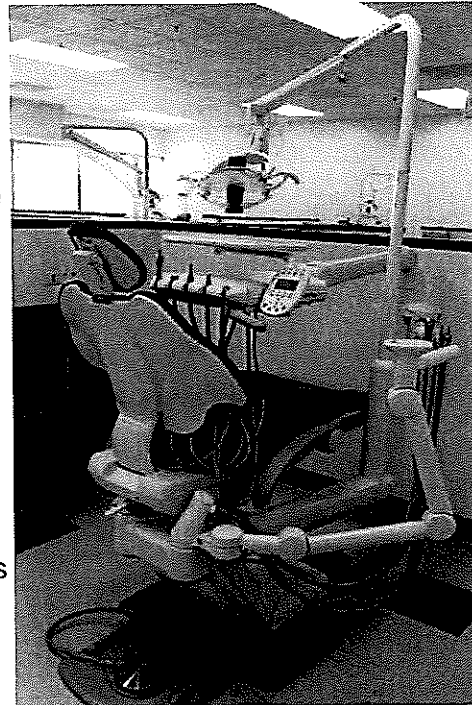
The University obtained an American Recovery and Reinvestment Act grant through the State of Maine in the amount of \$291,525. These funds had to be expended no later than summer 2011, so the College has already purchased its first sixteen dental operatories, which are currently being stored in a climate-controlled facility in Saco.

DENTAL SIMULATION

The College of Dental Medicine plans to provide state-of-the-art simulation facilities for our students. The students will begin learning and practicing the technical aspects of dentistry on simulated patients, who will have realistic oral and general health histories, and who will be treated according to a realistic phased plan of treatment.

The advantages of this dental simulation environment include:

- Accelerated student learning
- Prepare students for earlier entry into patient care
- Improved health and production through better ergonomics
- Improve student satisfaction and confidence
- Supplement students' clinical experience



Example of UNE dental unit

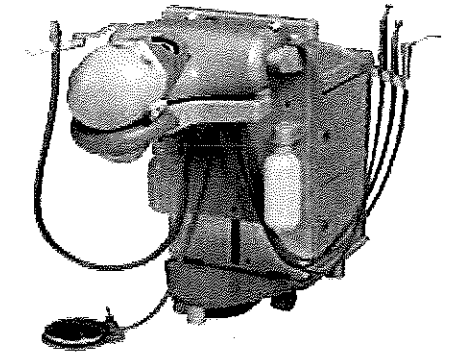
ARCHITECTURAL TEAM

The Oral Health Center planning is based on an integrated Group Practice Model with four 'practices' overseen by Group Practice Leaders and Patient Care Coordinators. This preliminary planning process was enhanced by the various experiences the team has had relative to administrative and teaching experiences in integrated group models, renovation of existing clinical spaces and/or new construction of both simulation and clinical spaces and the fact that each team member has visited many pre-clinical and clinical settings in dental schools across the country.

The next steps were to begin the space planning with the architectural team. An RFP was sent to eight architectural firms from around the country making it very clear that experience with larger dental clinic settings would be a high priority. After a thorough interviewing process, Port City Architecture, partnering with Kahler-Slater, was chosen for the UNE Oral Health Center building project. Port City Architecture brings a wealth of experience in constructing new buildings and was the architect responsible for the new UNE College of Pharmacy building on the Portland Campus. Their teaming with Kahler-Slater is a key factor as their dental architect has been involved in numerous dental simulation and clinic projects across the country.

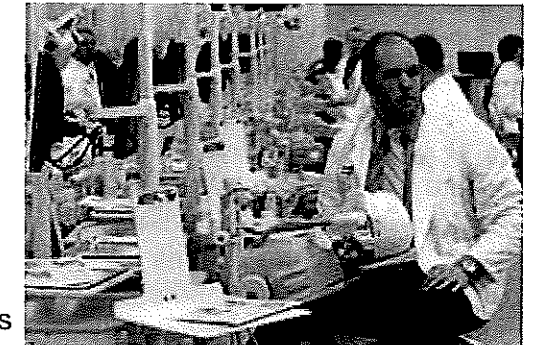
CURRICULUM DEVELOPMENT

During the feasibility phase for the development of the College of Dental Medicine, representatives from the University visited twelve dental schools across the United States and Canada to assess various aspects of the programs—including the curriculum. During this time, UNE decided to develop a dental school based on a model that uses extensive community-based clinical education. Specifically, UNE's administrative team developed a proposal that was approved by the UNE Board of Trustees in November 2008 with the following characteristics:



Dental simulator

- **Portland:** Two years on campus focused on an integrated basic science, preclinical dental science including dental simulation and early clinical experience curriculum.
- **Portland:** A third year focusing on intensive clinical experience in the University-based Oral Health Center in Portland, Maine with continued didactic and seminar studies in dental sciences, dental practice and integration of biomedical sciences into clinical practice.
- **Distributed Clinical Network:** A fourth year focusing on extensive clinical experience in a distributed, community-based clinical learning environment across Northern New England (Maine, New Hampshire and Vermont). Specifically, students would participate in externships across the region for the majority of the fourth year of dental school. Clinical sites would include federally qualified community health centers, Veteran's Administration dental clinics, and other non-profit dental clinics. Faculty oversight would be assured for all clinical experiences.



Dental Simulation Clinic

Initially, four advisory groups were invited to provide guidance in the further development of the predoctoral program. One of the groups, "The Success Group" (Northern New England Dentists for the Success of a College of Dental Medicine at the University of New England) included nine dentists who were each appointed by the dental association/societies of the three target states: Maine, New Hampshire and Vermont. This group focused on the needs and practices of dentists in Northern New England.

In 2010, several "Blue Ribbon Advisors" Groups were convened. The first group focused on the clinical education portion of the curriculum including preclinical education and

clinical education both on and off-campus. The second Blue Ribbon Advisors' Group focused on a research program for the College. The discussions included student research opportunities and evidence-based dentistry as part of the curriculum. The third Blue Ribbon Advisors' Group focused on the curriculum as a whole.

A final advisory group was convened in June 2011. This group, The DMD Curriculum Design Group, was charged with serving as the Curriculum Committee for the College of Dental Medicine for three days. The participants reviewed and made recommendations to the draft Competencies and the overall student outcomes assessment plan and then focused on the first two years of the curriculum. The participants broke into groups across the three days and drafted specific learning objectives for various aspects of the curriculum. This final advisory group provided detailed recommendations and work products to the College's Curriculum Design Group.

Each of the advisory groups, prior to their meeting times, were provided background information regarding the plans for the College and the basic premises that had been established in the feasibility study. They also reviewed the work that had been developed by the previous Advisory Groups. As a result, the work of each group built upon the work of the previous individuals and groups.

A total of almost forty external advisors from approximately twenty institutions of higher education including fifteen dental schools in the United States and Canada were engaged in the development of the College of Dental Medicine's DMD program. This included individuals representing the biomedical sciences, the preclinical/clinical sciences, private practice and the dental industry.

Additionally during the summer of 2011, UNE engaged two of the DMD Curriculum Design Group members to further develop learning objectives for the entire Biomedical Science and Dental Science portions of the curriculum. The work included multiple conference calls, work via email, and four face-to-face meetings.

The educational philosophy and resulting curriculum are not derived from any one school but are an amalgamation of best and promising practices utilized in a variety of settings across North America. As a summary, the following are but a few of the resources the College has used to establish its curricular philosophy and the DMD curriculum:

- UNE Administration visits to 12 US and Canadian dental schools
- Work of three advisory groups
- Work of the DMD Curriculum Design Group
- White papers of the American Dental Education Association's Commission on Change and Innovation
- Core Competencies for Inter-professional Collaborative Practice by the Inter-professional Education Collaborative

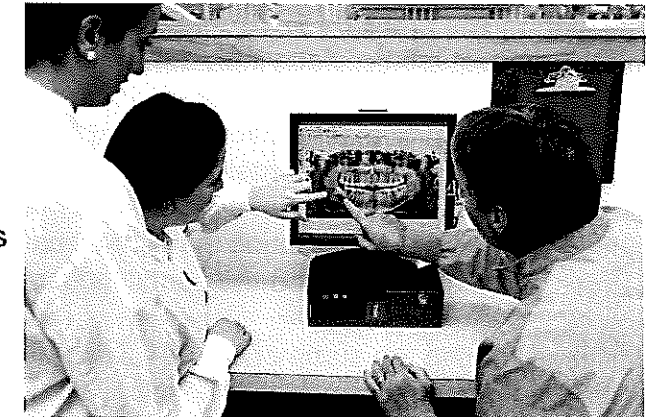
RESEARCH

Research will also be central to UNE's mission, and a dental medicine program will provide rich opportunities to study oral health issues facing people in northern New England. The anticipated research agenda will focus on population research, educational research and pain research.

The College has also developed a robust research plan. Interested students will receive direct hands-on experience on a clearly defined research project with attainable goals. The research projects will be performed by the students related to ongoing projects or newly developed projects under a faculty mentor's supervision. The students will also have the opportunity to participate in campus-wide, seminars, and workshops. Although the major training program activities are during the summer, students may continue related activities during the fall and spring semesters, contingent on mentor and College of Dental Medicine discretion. Students will be encouraged to submit an abstract and present their research findings at a scientific meeting. The program will culminate with a final research report.

FACULTY AND STAFF

A five to seven year hiring plan is also being developed to set the stage for the future recruitment of faculty and staff. Faculty and staff hires will be phased as the program develops over the four years of the curriculum, and it is anticipated that initial hires will occur on or about the spring of 2013.



FUND RAISING

To date, over thirty individual, corporate and foundation donors have contributed \$ 8,511,382 to support the College of Dental Medicine. Some of the major gifts include:

\$3,500,000	Maine Dental Bond
\$2,300,000	NE Delta Dental (ME, NH, VT)
\$ 611,820	U.S. Dept. of Health & Human Services
\$ 500,000	Elmina B. Sewall Foundation
\$ 291,525	American Recovery and Reinvestment Act (ARRA)
\$ 250,000	UNUM
\$ 220,000	Betterment Fund (first endowed dental scholarship)
\$ 200,000	Vincent B. and Barbara G. Welch Foundation

In addition, many individuals, including dental practitioners have made donations in amounts ranging from \$5,000 to \$100,000 each.

COMMUNICATIONS

The College is continuing to meet with dental groups and others to continue to inform people about our plans, and to continue to solicit support for the short-term and long-term financial needs of the developing educational program.

The following is a partial list of meetings/discussions that have been held over this past year:

- Northeast Delta Dental
- UNUM
- American Dental Education Association Annual Session
- Yankee Dental Congress
- New England Dental Leadership Conference
- Rhode Island Dental Association Executive Board
- York County Dental Society
- Greater Portland Dental Society
- Vincent B. and Barbara G. Welch Foundation Board
- UNE College of Dental Medicine "Meet the Dean" reception
- UNE Dental Hygiene Program Advisory Committee
- Mattina R. Proctor Foundation
- Harry and Sadie Davis Foundation
- New Hampshire Dental Society Annual Session
- Nobel Biocare
- Maine Dental Association Executive Board
- Penobscot Community Health Care Center
- Maine Board of Dental Examiners
- University of Maine Augusta
- Senator Chris Rector
- Maine Dental Association Annual Session
- Penobscot Community Health Center "Growing Our Own" Conference
- Mid-Coast Dental Society
- Merry Meeting Bay Dental Society
- Community Dental Care
- American Dental Education Association Curricular Change and Innovation Conference
- Dalhousie University College of Dentistry
- Westbrook College Alumni Event
- Maine Medical Center

Overall, our team continues to work on all fronts to make the dream of a UNE College of Dental Medicine a reality. The University remains exceptionally grateful for your continued support and we look forward to continuing to share with you the progress of northern New England's first College of Dental Medicine.

Attachment B

04/20/12

2. The Planning Board grants an extension to February 24, 2012 for meeting the condition of approval regarding the Long-term Parking Strategy Plan.

The February 28, 2008 College of Pharmacy Site Plan Condition of Approval stated, "UNE shall submit to the Planning Board for review and approval within two years of February 26, 2008 a long term parking strategy which shall address, among other items, the deficit parking that is depicted on page 8 of the Planning Board Report #12-08." This Planning Board report #12-08 is attached at the rear of this section for reference purposes.

Please see the attached *Parking Assessment/ Long-term Parking Strategy Plan*, April 2012 and *Campus Master Plan 2012-2017 Parking Demand Projections* documents, dated 04/01/12.

At the April 10, 2012 workshop, the Planning Board clarified that it required a clearer understanding of how UNE had specifically addressed the deficit parking that was depicted on page 8 of the Planning Report #12-08. The parking deficit projected in 2007, for the 2011-12 academic year was 134 spaces. The University has addressed this deficit.

At the conclusion of the Pharmacy construction there were 413 spaces available on the Portland Campus. (Per the *Planning Board report #12-08, Section II. Summary of Findings* the 413 parking spaces available on the campus at the completion of the College of Pharmacy project, exceeded the City of Portland Code requirement of 231 spaces.) Since the original Planning Board Site Plan Approval of the College of Pharmacy, the University has looked to maximize the efficiency of its existing parking areas. Reconfiguration of stall layouts in a few parking areas has allowed for the addition/creation of 7 parking spaces, resulting in a total of 420 spaces available on the campus. (This parking space count has been confirmed by the independent audit of Robert Fuller, on December 10, 2011, as noted in Attachment 5 of the April 5, 2012 *College of Pharmacy Conditions of Approval*, memorandum to the Planning Board, from Shukria Wiar.) See attached *Parking Map 2012* for locations of the specific parking areas on campus.

The University of New England offers free Metro passes to its students and staff. Attached at the rear of this section is a report illustrating the monthly ridership since the inception of this program. At the time for the original College of Pharmacy site plan submission, 2007, there was an average of 232 rides per month. This program has grown significantly and in 2011 the ridership has increased to an average of approximately 643 rides per month. (September 2011 counts were excluded from this count as the Metro offered all college students free passes for the first half of the month, thus decreasing the total rides attributed to UNE.) This increase of 411 rides per month equates to approximately 20 rides per academic day, or the equivalent of 10 daily round trips to/from the campus. This has eliminated the need for 10 parking spaces on the campus over this time. The university continues to encourage the use of the Metro as an alternative to commuting by car, and anticipates this program to continue to grow in its popularity and effectiveness.

The University has also continued to seek additional parking spaces to lease, in lieu of creating new parking spaces. Since 2007 the University has successfully negotiated with the Stevens Avenue Congregational Church on a long term, renewable lease for 35 parking spaces and with the Stevens Avenue Armory for two additional spaces. Additionally, the University has partnered with the Stevens Avenue Armory to consolidate their secured parking to one side of the lower Armory parking area, and the University was able to re-orient an existing access road to allow for the use of the vacated parking area. The plan approved by the Planning Office allowed for the 77 spaces that were created in 2011, and indicated that in a Phase 2 project another 30 could be added upon successfully completing a Level II Site Plan Review and increasing the total impervious area at the site. If the additional spaces are not constructed, the parking as indicated within the original plan can be increased to 83 spaces by reconfiguring the spaces into what would have been aisle space in Phase 2 of the plan. Please see attached drawings at the rear of this section. Both the approved Phase 1 and revised Phase 1 plans are attached.

These actions have addressed the 134 space deficit projected in the Planning Board Report #12-08.

Summary:

Maximize efficiency of existing parking	7
Increased Metro Utilization	10
Increased Rentals	
Stevens Ave. Congregational Church	35
Stevens Avenue Armory	
Upper	2
Lower	<u>83</u>
TOTAL	137

While the University has addressed the deficit as projected, the utilization of the newer, more remote parking areas is not as high as the University would like to see. UNE community vehicles continue to park in the public spaces proximal to the campus. This utilization of neighboring side street public parking has resulted in increasingly more concern expressed by the neighbors of the campus. In response to this, the City has initiated a survey to seek opinion on the creation of a Residential Parking Zone adjacent to the campus. The University supports this plan and feels that once it is implemented, the UNE community vehicles currently utilizing the more proximal, on-street public parking, would then relocate to the UNE provided parking spaces.

The University's *Campus Master Plan 2012-2017*, dated April 17, 2012, is attached. This plan reflects the anticipated future growth and development on the campus. This plan includes the creation of an Intermodal Transportation Facility at the newly acquired property on Bishop Street. In utilizing the 12.5 acres of impervious surface currently existing at the site, the University now has the ability to meet its entire long-term projected parking deficit on this parcel, without creation of any additional impervious surface. The UNE community

members could park their vehicles at the facility, wait in a heated and secure shelter, and then board a shuttle that would bring them to the main campus. The shuttle service would make continuous trips, during peak academic hours, to insure minimal wait times. The University intends to develop plans for the site and submit them to the Planning Office for review in the near future, in order to begin utilization of the facility in the Fall of 2012.

PLANNING BOARD REPORT # 12-08

**COLLEGE OF PHARMACY
VICINITY OF 716 STEVENS AVENUE**

CONDITIONAL USE AND SITE PLAN REVIEW

UNIVERSITY OF NEW ENGLAND, APPLICANT

Submitted to:

Portland Planning Board
Portland, Maine

February 26, 2008

Submitted by:

Shukria Wiar
February 20, 2008

I. INTRODUCTION

University of New England (UNE) is seeking to establish a new College of Pharmacy on its Westbrook College Campus on Stevens Avenue in Portland. The program would be a professional six-year degree program with years one and two on the University Campus in Biddeford and years three, four and five on the Westbrook College Campus. The sixth year is off-site. The program will accommodate 100 students per class with approximately 40 full time faculty members. The site is located in the Residential R-5 zone.

The establishment of the pharmacy program will bring 300 additional students (100 students per year over a three year period) and faculty to the Westbrook College Campus. Originally the applicant was proposing a parking lot to be located on the lower campus and accessed by a driveway that would follow the alignment of the existing gravel drive at the end of College Street. This parking lot and the access driveway are no longer being proposed as part of this project. The existing parking lots on campus and the ten spaces leased from the Armory are adequate to meet the zoning ordinance provisions.

The site plan proposal will be reviewed by the Planning Board under the conditional use regulations of the Residential R-5 zone, the site plan standards, and Traffic Movement Permit requirements. Notices were sent to 279 area residents and interested citizens along with a notice in the Portland Press Herald. A Neighborhood Meeting was held on January 15, 2008; two people attended. See Attachment 7 for the minutes.

II. SUMMARY OF FINDINGS

Zoning:	Residential R-5
Proposal:	UNE is proposing a four-story building on their Westbrook College Campus on Stevens Avenue.
Parcel Size:	42 acres; only 25,000 sq ft of existing parking lot will be redeveloped into the proposed building, service drive, entry courtyard, and landscaped areas
Parking Spaces Required:	56 spaces are required for the proposed building (staff estimate calculated from all classroom and teaching lab space) and 175 for existing facilities. A total of 231 parking spaces are required for the entire campus under the City's zoning code.
Parking Spaces Provided:	A total of 413 spaces will be available on the campus at the completion of the project.
Building Floor Area:	The footprint for the building is 11,595 sq ft and total gross area is 46,380 sq ft
Building Height:	50.5 feet
Use of the Project:	College of Pharmacy. Please look at Paragraph III for specifics.

III. PROPOSED DEVELOPMENT

The proposed project includes the construction of a four-story building, see Attachment 5. This new building will house the educational components necessary for UNE's new pharmacy program. The program components consist of research labs, teaching labs, drug information library, vivarium and other supporting spaces. The proposed College of Pharmacy building has 46,380 square feet with a footprint of 11,595 sq ft.

Lower Level	11,595 sf
Mechanical / Support / Unfinished Space (Future Vivarium)	11,595 sf
First Floor	11,595 sf
Classrooms / Lecture Hall	2,342 sf
Teaching Labs (Not included as classrooms)	1,808 sf
Office / Administration	1,193 sf
Support / Circulation	6,252 sf
Second Floor	11,595 sf
Classrooms / Lecture Hall	0 sf
Teaching Labs (Not included as classrooms)	1,444 sf
Library	969 sf
Office / Administration	4,420 sf
Support / Circulation / Grossing	4,762 sf
Third Floor	11,595 sf
Classrooms / Lecture Hall	0 sf
Research Labs (Not included as classrooms)	6,860 sf
Office / Administration	1,902 sf
Support / Circulation / Grossing	2,833 sf

There is a total of 42 acres but only 25,000 sq ft of an existing parking lot will be redeveloped into the proposed building, service drive, entry courtyard, and landscaped areas.

IV. STAFF REVIEW

The proposal will be reviewed for compliance with the conditional use regulations of the Residential R-5 zone, the site plan standards, and Traffic Movement Permit. The site plan has been reviewed by Zoning, Traffic, Fire and Department of Public Works.

V. INSTITUTIONAL CONDITIONAL USE REVIEW

1. The following standards apply for review of an institutional expansion in the R-5 zone.

Sec. 14- 118:

- a. In the case of expansion of existing such uses [onto] land other than the lot on which the principal use is located, it shall be demonstrated that the proposed use cannot reasonable be accommodated on the existing site through more efficient utilization of land or building, and will not cause significant physical encroachment into established residential areas.

The placement of the addition will be within the school's property

- b. The proposed use will not cause significant displacement or conversion of residential uses existing as of June 1, 1983, or thereafter.

The College of Pharmacy building and accompanying improvements will not cause the displacement or conversion of any residential units.

- c. In the case of a use expansion which constitutes a combination of the above-listed uses with capacity for concurrent operations, the applicable minimum lot sizes shall be cumulative.

The applicable lot sizes have been met.

- d. A college, university or trade school may build principal structures to a height of fifty-five (55) feet, not including the USM Overlay Zone, if the following standards can be met:

(i) Lot size: 10 acres which may include adjacent land owned by the institution on both sides of a public street.

The site is larger than 10 acres with a total of 42 acres.

(ii) Minimum setback between buildings on-site: 20 feet.

The minimum setback between buildings is being met.

(iii) Minimum setback from external property boundary: 30 feet, except that parking garages over 35 feet in height must be located 50 feet from external property boundaries when adjacent to an adjoining residential use.

The minimum setback from external property boundary is being met.

(iv) The area between the structure and adjoining residential uses must be adequately screened with appropriate landscaping or other features to buffer the building and effects thereof (i.e. noise, light, etc) from abutting properties.

The applicant is adequately screening the proposed building from

abutting properties.

2. The following standards apply for all conditional uses:

Sec. 14-474(2):

- a. There are unique or distinctive characteristics or effects associated with the proposed conditional use.

There are no known unique or distinctive characteristics associated with the proposed use.

- b. There will be an adverse impact up on health, safety or welfare of the public or surrounding area.

It does not appear that there will be any adverse impacts associated with the proposed project.

- c. Such impact differs substantially from the impact which would normally occur from such a use in that zone.

The impacts of this site are similar as those normally expected from such a use in this zone.

VI. SITE PLAN REVIEW

1/2. Traffic and Parking

A. Traffic

The traffic portion of this project was reviewed under the State Traffic Movement Permit (TMP). The City has delegated authority to review this permit. The applicant has had a scoping meeting with the City's Traffic Engineer on January 4th to discuss the TMP application (Attachment 2). Tom Errico, Consulting Traffic Engineer has reviewed the TMP application along with the site plan application, and finds that the revised traffic assessment to be acceptable and agrees with the Summary of Findings. The intersections of Stevens Avenue with College Avenue, the UNE South Entrance, and Walton Street will operate at satisfactory levels of service following project completion. Mr. Errico offers the following comments:

- a. On-street parking on the westerly side of Stevens Avenue should be prohibited near the intersections of College Avenue and the UNE South Entrance such that sight distance is improved. I concur with the specifics noted in the Tom Saucier's email dated January 16, 2008. In addition the nearest parking space on College Street (west of Steven Avenue) should be eliminated. I would note that the applicant will be responsible for all costs associated with the suggested parking prohibitions.

- b. The proposed site plan notes the internal roadway west of the proposed building to be approximately 14 feet wide. This narrow width is unacceptable for two-way flow. According to an email from Tom Saucier dated January 17, 2008; UNE is recommending that this road be restricted to one-way flow from north to south. I concur with this recommendation and the applicant shall submit a traffic control plan for review and approval by the City.
- c. The City is undergoing a long-term planning process for the next generation of improvements at Morrill's Corner. Accordingly, the City will be collecting monetary contributions from projects that impact Morrill's Corner. Based upon traffic generation and assignment information provided by the applicant, the applicant shall contribute \$29,000.00 towards improvements at Morrill's Corner.

Potential Conditions of Approval are listed below to address the above comments:

1. *The applicant will be responsible for all costs associated with the suggested parking prohibitions contained in Mr. Errico's review, dated January 18, 2008.*
2. *The applicant shall make a contribution of \$29,000.00 towards the improvements at the intersection at Morrill's Corner.*

B. Parking Requirements per Zoning Ordinance

In Section 14-332 (e) states that:

Schools providing instruction for students sixteen (16) years of age and over: One (1) parking space for each ten (10) seats or major fraction thereof, used for purposes of instruction; if no fixed seats, one (1) parking space for each one hundred (100) square feet or major fraction thereof used for purposes of instruction.

Based on the numbers presented in the applicant's submission of December 20, 2007, the required parking for the entire campus and the proposed pharmacy building is broken down as following:

Existing Conditions as Submitted by UNE

- 14,077 sq ft of classroom space without fixed seating- 560 non-fixed
- Two rooms with 222 fixed seats
- Total classroom seating = 782 seats
- Total classroom sq ft = 17,492 sq ft

Staff Calculations for Required Parking-- College of Pharmacy Building

- Two classrooms space without fixed seating- 68 non-fixed (includes teaching labs)
- One classroom with 121 fixed seats
- Total classroom seating = 189 seats
- Total classroom sq ft = 5,594 sq ft

Totals after construction

- 14,077 sq ft of classroom space without fixed seating
- Five rooms with 393 fixed seats
- Total classroom seating = 953 seats
- Total classroom sq ft = 22,751 sq ft

Therefore according to above information, the total number of parking spaces required for the existing classrooms and the proposed pharmacy building is 231 spaces (175 for existing facilities and 56 for the proposed development). The proposed Pharmacy building will be located in an existing parking lot and will permanently displace 63 parking spaces. Therefore, a total of 413 (476 minus 63) off-street parking spaces are provided on the campus, including the ten spaces being leased from the Armory. Please see Attachment 12 for overview of the parking spaces on the Westbrook College campus.

Marge Schmuckal, the Zoning Administrator has reviewed the submitted information and had been unable to reproduce some of the figures given for the calculations. However, she states in her memo dated 01.18.2008 that there are many more parking spaces provided for the college above the minimum requirements listed in the parking division, see Attachment 18a.

C. Parking Assessment per UNE's Needs/Demand

The applicant prepared a parking assessment, which is based on the number of existing parking spaces and the current number of enrolled students. Based upon the most recent available enrollment data, the student population is approximately 735 students. Hourly counts were taken between October 22, 2007 and October 26, 2007. There is a total capacity of 466 spaces on site, with a rental agreement with the State of Maine Armory for an additional ten (10) spaces, resulting in 476 available parking spaces. According to the parking assessment, average of 106 spaces were vacant at any specific time; therefore 370 spaces are occupied. Based upon this data, there is an existing ratio of 0.50 (370 divided by 735) occupied spaces per student.

Understanding that there are on-street parking spaces available and utilized by students and staff, these spaces were considered when projecting future parking demand as a result of the proposed development. Off-street parking will not be considered in mitigating potential parking demand. Observations from the week of December 3 to December 7, 2007 of on-street parking along Stevens Avenue and College Street indicated that a maximum of approximately 64 spaces were occupied adjacent to the campus. When these numbers are factored into the occupied spaces to student ratio calculation, the ratio is increased to 0.59. (This is a conservative estimate in that it assumes all on street parkers in the vicinity are college related and the other uses in the vicinity include St. Patrick's church, an elementary school, day care center and the Armory.)

The University began offering free use of the METRO to all students and staff in September of 2006. Since the launch of the program, ridership averages 310 rides per academic month. The University is looking at other ways to increase this usage.

The establishment of the pharmacy program will bring 300 additional students (100 students per year over a three year period) to the Westbrook College Campus, starting in the fall of 2009. Using the 0.59

occupied spaces to student ratio calculated in the previous paragraphs, the yearly anticipated parking demand is presented in the Table below by the applicant. The following Table was obtained from Attachment 4a.

Year	Total Spaces	Available	New Students	New Demands	Deficit
2007-08	476	06	0	0	-106
2008-09	413	43	0	0	-43
2009-10	413	43	100	59	16
2010-11	413	-16	100	59	75
2011-12	413	-75	100	59	134

As it can be observed from the table that there will be a shortage of sixteen (16) spaces when the College of Pharmacy building is first occupied in the fall of 2009. According to the applicant, this shortage number could be less with an increase in the METRO ridership or if other parking management mitigation measures are proposed.

UNE has been in contact with the Maine State Amory to explore the possibility of extending its parking space rental agreement. According to the applicant, the Amory has verbally committed to extend the agreement to include a total of 30 parking spaces. With this agreement, the parking will offset the projected parking deficit anticipated prior to September 2010.

Eventually, a parking structure may be needed to meet the growth needs of UNE, and we have discussed with the applicant the need for a medium to long term campus master plan to address future growth and associated campus development. Because the College of Pharmacy is a start-up program, the maximum parking demand generated by this facility will not be fully realized until 2011 when all three classes will be present on campus. Therefore it may be reasonable in this case to impose a condition of approval on parking that includes monitoring and locating supplemental parking in the years to come.

John Peverada, Parking Manager, has reviewed the 2007 Parking Assessment and is satisfied provided that the applicant completes a Campus Master Plan (Condition of Approval for the Conditional Use Permit), see Attachment 15.

Tom Errico, Consulting Traffic Engineer has also reviewed the 2007 Parking Assessment (Attachment 4a) and concurs with the methods and conclusions contained in the report. There will be a parking deficit that will occur at the opening of the project (16 spaces), with the deficit rising as enrollment increases. For the short term condition, leasing of spaces from surrounding locations is satisfactory. As for the long-term parking deficit requirements, Mr. Errico recommends that a condition of approval requiring completion of a campus master plan and implementation of required parking facilities or parking strategies should be included. In the proceeding paragraphs, a condition of approval language is proposed.

The Traffic Control Plan has also been reviewed by Tom Errico and offers the following comments:

- a. The specific signing noted on Stevens Avenue will need to be reviewed by the City parking and traffic divisions for sign location and sign details. Accordingly the signs noted may need to be revised. Exact sign type and location can be determined in the future.

- b. Because of the narrow roadway width (14 feet), the roadway behind the proposed COP building will be restricted to one-way flow in the southerly direction. Additional signs will be required to reinforce this restriction.
- c. Based upon the above two items, the applicant shall submit a roadway signage plan for both Stevens Avenue and on-site for review and approval before Certificate of Occupancy.

Potential conditions of approval:

1. *The applicant shall submit a traffic control plan and a roadway signage plan for both Stevens Avenue and the campus for review and approval by the City Parking and Traffic Division prior to the issuance of a certificate of occupancy.*
2. *UNE shall conduct a parking analysis/ monitoring study every six months from the issuance of a certificate of occupancy to ensure that sufficient parking spaces are available for the proposed use. The monitoring shall continue until a master plan and long-term parking strategies are approved by the Planning Board. If during that time, the City determines the improvements are not working as intended, the applicant shall be required to mitigate parking impacts as directed by the City.*

2. Bulk, Location, Health, Safety Air and Height of Proposed Buildings

Carrie Marsh, Urban Designer reviewed the conceptual elevations for the UNE College of Pharmacy building. Currently there are no design guidelines in the R-5 therefore this building is not subject to review under the City's design guidelines. According to Carrie Marsh, the architectural plans present a handsome building that appears to be well articulated and appropriate to its context

The City Sidewalk Materials Policy calls for concrete sidewalks along Stevens Avenue in front of the proposed building site. Based on the Sidewalk Materials Policy and the condition of the existing sidewalk, DPW recommends replacing the existing asphalt sidewalk along Stevens Avenue in front of the proposed building with a new concrete sidewalk, constructed in accordance with our design standards. The applicant has agreed to replace the bituminous concrete walk from the campus entrance drive (in front of Finley) to the bus stop shelter with a concrete walk.

DPW recommends that the existing curb along Stevens Avenue be replaced with new granite curbing. The applicant agrees to only replace the granite curbing along the sidewalk section that will be replaced with concrete. The applicant will be providing a handicapped accessible ramp at the campus entrance near the proposed building.

A potential condition of approval:

The site plan shall be revised to show the replacement of the sidewalk and curb along Stevens Avenue that meet the City's standards.

3. Stormwater, Stormdrains, Soils and Drainage

The proposed pharmacy building is located entirely on an existing parking lot. Currently the runoff from

this area enters an enclosed stormwater system which drains the campus and connects to the City system at Stevens Avenue. This development will result in the conversion of approximately 12,000 sq ft of existing impervious pavement to impervious roof area and about 6,200 sq ft of existing pavement will be converted to pervious landscaped area. The conversion of impervious pavement to landscaping will increase the potential for groundwater recharge through infiltration to the underlying sandy soils.

UNE is proposing to discharge foundation drainage into the City's combined sewer system. UNE agrees to install an hour-metering apparatus (one of the options suggested in the memo dated 02.01.2008 by Mike Farmer) and report the results of any groundwater flows and groundwater elevations to the city on a monthly basis so that an appropriate sewer user fee could be charged, if the discharge is significant. If the groundwater flows exist and are estimated to have a significant impact upon the functioning of the City system, UNE would address the flows through development of a mitigation plan as part of their master plan process, prior to City approval of other site development projects on the campus. Mitigation options may include impact fees, construction of dry well systems, or participation with the City in installation of new drainage in College Street at such time in the future when the City has scheduled that reconstruction work.

Mike Farmer, Project Engineer for Department of Public Works, recommends two conditions of approval in his memo dated February 20, 2008, see Attachment 14a, for the project to address the concern about the discharge of foundation drainage and surface runoff to the City's combined sewer system. The monitoring would last as long as the City thinks the discharge is significant.

The volume of foundation drainage discharged to the City's combined sewer system shall be continuously metered, and recorded and reported on a monthly, or more frequent, basis to the Engineering Division of Public Works. If the Engineering Division determines that the volume of such discharge is significant, then the City shall charge UNE, and UNE shall pay the City, the normal sewer use fee based on the volume discharged. UNE shall be responsible for owning and maintaining the facilities necessary to accurately meter the discharge of foundation drainage to the City's combined sewer system. The requirement to meter, record, and report the volume of foundation drainage discharged shall be suspended by the City if records indicate that the volume of such discharge is insignificant.

UNE shall develop a campus stormwater management plan as part of its Campus Master Plan. The stormwater management plan shall address the treatment and discharge of stormwater runoff from the campus with the goal of minimizing impacts on the City's sewer system and natural drainage system.

4. Wetlands and Wild Life

The initial proposal included a parking lot in the lower campus, also known as Gulliver's Field. This portion of the parcel is not being developed as part of this proposal. Broadwater Environmental, Inc. has conducted a wetland delineation and vernal pool survey on this parcel; fieldwork was initially conducted in February 2007 and a review of the wetland boundaries was completed in field in May 2007. There were a number of wetlands areas that were identified on the lower part of parcel: five wetlands and two open water areas. According to the environmental report, wetland area 1 and wetland 2 are of special significance under Maine Department of Protection (MDEP) Chapter 310. Wetlands and Waterbodies

Protection. Wetland 3 is classified as small Palustrine Forested Wetlands (PFO) wetland; these types of wetland only occur in undisturbed areas and often associated with streams. Wetland 4 and wetland 5 may be considered a stream by the MDEP. Please refer to the Wetland Delineation Report (Attachment 3f) for details.

Capisic Brook is also located on this parcel and runs through the lower campus. Capisic Brook is listed as an Urban Impaired Stream.

Third Party Review of the Wildlife Habitat

Upon the request of the Planning Board, a third review of the wildlife habitat of the lower campus was conducted by an independent entity. Diane Boretos, P.W.S of Call of the Wild Consulting conducted a site visit of the parcel on December 5, 2007. This third-party report is presented only as background information to the project. According to the report, this site provides very significant migratory songbird stopover habitat for both the spring and fall migrations and provides habitat for red fox, red and gray squirrels. The report of these findings is included as Attachment 11.

5. Landscaping and Existing Vegetation

Jeff Tarling, the City Arborist has reviewed the site plan for the UNE College of Pharmacy off Stevens Avenue. According to Mr. Tarling, the landscape plan and tree protection measures are satisfactory and will improve the proposed project. He recommends that the tree-protection fencing shall include the area within the drip-line of the tree as shown on the landscaping plan.

A potential condition of approval:

The tree-protection fencing shall include the area within the drip-line of the tree as shown on the landscaping plan.

6. Exterior Lighting

The applicant has submitted locations and catalog cuts of proposed lighting fixtures for the pharmacy building. The applicant is proposing three fixtures as seen in Attachment 3g; the applicant stated that these fixtures will be cut-off.

7. Sewer and Water Capacity Letters

UNE has submitted a capacity to serve letter from Portland Water District. A sewer capacity letter shall be submitted as soon it is available.

A potential condition of approval:

The applicant shall obtain and provide to the Planning Authority a sewer capacity to serve letter.

8. Fire

Captain Greg Cass of the Fire Department has reviewed the site plans and all of his concerns have been addressed.

9. Financial Capability

The applicant has provided a letter from Bernard G. Chretien, Vice President for Business and Finance at University of New England attesting that the University has sufficient funds to pay for the construction of the pharmacy building and parking lot. See Attachment 1f.

10. Easements

There are two fifty (50) feet easements on the lower field mentioned in the deed between the applicant and the City of Portland (Attachment 1e) which are depicted on the Boundary Survey Plan.

VI. STAFF COMMENTS

At the outset of this project, UNE sat down with the City development review team to lay out their needed time frames and the issues at stake for the University. Those present recognized that a comprehensive approach based on a campus master plan, (similar to the USM Overlay Zone that was created for that institution), is warranted for UNE. However, the constraints of time for this project compelled a two stage process whereby stage one would amend the zoning specifically for the College of Pharmacy to open the way forward for this time critical project, and stage two will involve a more comprehensive master planned approach, including an overlay zone to accommodate UNE's future growth.

Other institutions have struggled with similar growth pressures, and have agreed to undertake periodic master plans to help guide their growth. It is recommended that the conditional use for this project be approved, with the condition that UNE proceed with master planning prior to returning for any further major expansion projects. This master plan should affirmatively address stormwater management, transportation and parking issues, including consideration of structured parking, Transportation Demand Management.

A potential condition of approval:

That UNE commit to a campus master planning process and report back to the Planning Board within two years, and prior to any further site development requiring major site plan review by the Planning Board. This master plan should affirmatively address stormwater management, transportation and parking issues, including consideration of structured parking and/or long-term parking strategies

VIII. MOTIONS FOR THE BOARD TO CONSIDER

I. Conditional Use:

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report #12-08, relevant to Portland's Conditional Use Standards and other regulations, and the testimony presented at the Planning Board hearing:

The Planning Board finds the proposed conditional use for an expansion of institutional use (does or does not) meet the standards of Section 14-118 and 14-474(2) subject to the following condition(s) of approval.

- a. That UNE commit to a campus master planning process and report back to the Planning Board within two years, and prior to any further site development requiring major site plan review by the Planning Board. This master plan should affirmatively address stormwater management, transportation and parking issues, including consideration of structured parking and/or long-term parking strategies;

2. Site Plan Ordinance

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report #12-08, relevant to the Site Plan Ordinance and other regulations, and the testimony presented at the Planning Board hearing, the Planning Board finds that the plan (is/is not) in conformance with the site plan standards of the land use code, subject to the following conditions of approval:

- a. UNE shall conduct a parking analysis/ monitoring study every six months from the issuance of a certificate of occupancy to ensure that sufficient parking spaces are available for the proposed use. The monitoring shall continue until a master plan and long-term parking strategies are approved by the Planning Board. If during that time, the City determines the improvements are not working as intended, the applicant shall be required to mitigate parking impacts as directed by the City.
- b. UNE shall develop a campus stormwater management plan as part of its Campus Master Plan. The stormwater management plan shall address the treatment and discharge of stormwater runoff from the campus with the goal of minimizing impacts on the City's sewer system and natural drainage system.
- c. The volume of foundation drainage discharged to the City's combined sewer system shall be continuously metered, and recorded and reported on a monthly, or more frequent, basis to the Engineering Division of Public Works. If the Engineering Division determines that the volume of such discharge is significant, then the City shall charge UNE, and UNE shall pay the City, the normal sewer use fee based on the volume discharged. UNE shall be responsible for owning and maintaining the facilities

necessary to accurately meter the discharge of foundation drainage to the City's combined sewer system. The requirement to meter, record, and report the volume of foundation drainage discharged shall be suspended by the City if records indicate that the volume of such discharge is insignificant;

- d. The site plan shall be revised to show the replacement of the sidewalk and curb along Stevens Avenue that meet the City's standards;
- e. The tree-protection fencing shall include the area within the drip-line of the tree as shown on the landscaping plan;
- f. The applicant shall obtain and provide to the Planning Authority a sewer capacity to serve letter.

3. Traffic Movement Permit

On the basis of the University Commons Application, plans, reports, and other information submitted by the applicant, and on findings and recommendations contained in Planning Report # 8-08, and other applicable regulations, and testimony presented at the Planning Board Hearing, the Planning Board finds that the plan (is/ is not) in conformance with the standards of a traffic movement permit, subject to the following conditions of approval:

- a. The applicant will be responsible for all costs associated with the suggested parking prohibitions contained in Mr. Errico's review, dated January 18, 2008;
- b. The applicant shall submit a traffic control plan and a roadway signage plan for both Stevens Avenue and the campus for review and approval by the City Parking and Traffic Division prior to the issuance of a certificate of occupancy;
- c. The applicant shall make a contribution of \$29,000.00 towards the improvements at the intersection at Morrill's Corner.

ATTACHMENT:

1. Application

Dated: 09.05.2007

- a. Coverletter
- b. Site Plan and Conditional Use Applications
- c. Project Description
- d. Location Map
- e. Evidence of Right, Title and Interest
- f. Financial and Technical Capacity
- g. Sanitary Wastewater Disposal
- h. Water Supply
- i. Traffic Impacts

- j. Stormwater Management Plan
- k. Wetlands
- l. Solid Waste Disposal and Recycling
- 2. Application for Traffic Movement Permit Dated: 12.21.2007
- 3. Additional Information from the Applicant Dated: 10.12.2007
 - a. Coverletter
 - b. Capacity to serve letter from Portland Water District
 - c. Letter from UNE expanding on Financial Capacity
 - d. Construction Management Plan prepared by Allied/Cook Construction
 - e. Occupant loading break-down prepared by Port City Architecture
 - f. Wetland Delineation Report
 - g. Lighting Catalog Cut Sheets
- 4. Supplementary Information from SYTDesign Consultants
 - a. Revised Parking Assessment Report Dated: 02.05.2008
 - b. Supplemental Information from the Applicant Dated: 12.20.2007
- 5. Site Plan Dated: 10.12.2007
 - a. Campus Vicinity Plan- December 2007
 - b. Traffic Control Plan Dated: 02.2008
 - c. Existing Conditions and Demolition Plan C-100
 - d. Site Plan- Building C-101
 - e. Grading, Drainage, and Erosion Plan C-102
 - f. Site Utility Plan C-103
 - g. Landscape Plan C-104
 - h. Erosion and Sedimentation Control Notes and Details C-300
 - i. Site Details C-301
 - j. Site Details C-302
 - k. Site Details C-303
 - l. Lower Level Plan A1.1
 - m. First Floor Plan A1.2
 - n. Second Floor Plan A1.3
 - o. Third Floor Plan A1.4
 - p. Elevations A3.1
 - q. Elevations A3.2
- 6. Letter from SYTDesign Consultants Dated: 11.27.2007
- 7. Neighborhood Meeting minutes Dated: 01.15.2008
- 8. Letters from Planner to Applicant
 - a. Dated: 12.13.2007
 - b. Dated: 10.02.2007
- 9. Foundation Drainage System Letter from Haley & Aldrich Dated: 01.10.2008
- 10. Geotechnical Report from Haley & Aldrich Dated: 01.16.2008
- 11. Call of the Wild Consulting- Wildlife Report Dated: 12.07.2007
- 12. Armory Lease Agreement Dated: 09.05.2003
- 13. Dan Goyette memorandum
 - a. Dated: 01.17.2008
 - b. Dated: 01.02.2008

- 14. Mike Farmer memorandum
 - a. Revised memorandum Dated: 02.21.2008
 - b. Memorandum Dated: 02.01.2008
- 15. John Peverada memorandum Dated: 02.05.2008
- 16. Greg Cass email Dated: 01.09.2008
- 17. Tom Errico
 - a. Email Dated: 02.22.2008
 - b. Email Dated: 01.18.2008
- 18. Marge Schmuckal memorandum
 - a. Dated: 01.30.2008
 - b. Dated: 01.18.2008
- 19. Jeff Tarling email Dated: 02.22.2008
- 20. Email Correspondences from Tom Saucier
 - a. Dated: 02.19.2008
 - b. Dated: 02.15.2008
- 21. William Hancock, Maine Audubon Society
- 22. Jerry Hankinson, Abutter Dated: 01.17.2008

University of New England
Portland Campus
Parking Assessment/Long-term Parking Strategy Plan
April 2012

Existing Conditions:

As of the start of the Fall 2011 semester, UNE had 420 on site parking spaces on its Portland campus and had rental agreements with the State of Maine Armory for an additional 12 spaces, and with the Stevens Avenue Congregational Church for another 35 spaces, resulting in a total of 467 spaces available to serve its Portland Campus. This supply exceeds the City's code required parking count.

An assessment of available parking was conducted during the week of October 24-28, 2011, during which counts of available spaces were conducted hourly from 8am to 5pm each day. (See attached *Campus Master Plan 2012-2017 Parking Demand Projections* document.) A minimum of 21 available spaces was noted at 11am on October 25, 2011.

The Fall 2011 Full Time Equivalent (FTE), student enrollment determined that there are approximately 818 FTE students on the Portland Campus. This includes the third and final year of on-campus College of Pharmacy students.

Using the Fall 2011 parking and student population information it was determined that the ratio of occupied spaces to FTE students is 0.67. This ratio was calculated utilizing the number of occupied spaces on campus, as well as including the on street parking that was attributed to UNE. This is the same method previously utilized and approved by the City Traffic Engineer. The on street parking assumed to be utilized by UNE community vehicles includes all of the cars parked on College Street and 50% of the cars parked on Stevens Avenue in the vicinity of the campus as well as 50% of the cars parked on the neighboring side streets. On street parked vehicle count data was supplied to UNE from John Peverada, Manager of the Portland Parking Division. Using this ratio, the annual projected parking demand is presented in the attached *Portland Campus Master Plan 2012-2017 Parking Demand Projections* document.

The projected demand illustrates a deficit of 274 parking spaces in 2017.

The University subsidizes the free use of the METRO to all its students and staff. The program ridership averaged 232 rides per month in 2008 and has increased to 643 rides per month in 2011. Efforts are ongoing to increase this usage.

This parking deficit could be reduced further with a continued increase in Metro ridership and if other parking demand management options are successfully employed.

Parking Options

The University has continued to explore many options to increase the parking capacity and to mitigate the parking demand for its Portland Campus. These include the following:

- 1) **Seeking additional parking space rental opportunities proximal to the campus.** The University has successfully negotiated with the State of Maine Armory to increase the rental spaces at the Armory by approximately 80 spaces. These spaces were made available in January 2012.
- 2) **Acquisition of properties proximal to the campus and construction of parking spaces.** The University has identified, toured and considered several properties for purchase, which may provide proximal parking to the campus. The University has successfully negotiated the purchase of an additional 19 acres of land, contiguous to the campus. This parcel was formerly owned by Pike Industries and was utilized as an asphalt manufacturing plant. The parcel contains approximately 12.5 acres of impervious surface and can be readily utilized for surface parking and could easily accommodate the entire 2017 projected parking demand with minimal additional development.

The University's Master Plan includes the creation of an Intermodal Transportation Facility at the newly acquired property on Bishop Street. This option was reviewed on April 12, 2012 with the City Planning staff, including Alex Jaegerman, Barbara Barhydt and Marge Schmuckal, and it is understood by the University to be supported by City Staff. The University intends to firm up the plans for the site and submit them to the Planning Office for review in the near future, in order to begin utilization of the facility in the Fall of 2012.

The Bishop Street parcel would require some additional lighting, emergency call boxes, some minor asphalt repair, and striping. In addition, the University would look to retrofit one of the existing structures to accommodate a heated and secure shelter for persons utilizing the lot, while waiting for the shuttle service.

As the parcel is not directly adjacent to the main campus, the University will need to initiate the operation of a shuttle service to/from the main campus. The shuttle service is envisioned as operating during the main academic hours and offer continuous round trips to insure minimal wait times. After hour transportation would be arranged through the University's Security Office, via the existing "escort" service program. This program currently offers rides to persons "uncomfortable" or unable to walk to their parked vehicles or destinations.

In addition to this purchase, the University continues to actively pursue other acquisition options proximal to the campus, and is engaged in on-going discussions with one other land owner.

- 3) **Encouraging alternate transportation modes (mass transit, car pooling, bicycling, etc.)** The University continues to subsidize the free use of the METRO to all its students and staff. The program ridership averaged 232 rides per month in 2008 and has increased to 643 rides per month in 2011. This represents a 277% increase and is extremely encouraging. Efforts to increase the usage are on-going.

The University strongly encourages and supports carpool/rideshare opportunities, and makes use of the GoMaine website for both its Biddeford and Portland campuses.

- 4) **Other Parking Demand Management Strategies.** The University's most remote lots continually are the most underutilized. In an effort to encourage their use, the University continually advertizes their location and availability of off street parking. The Department of Safety and Security has conducted several raffles where they announce the raffle taking place during a certain week and then randomly select a vehicle parked in the remote lot to receive a prize. The winner is notified after the end of the raffle period, thus encouraging users to continue the use during the entire period. The prizes have included bicycles from LL Bean. The selection of a high quality bike as a prize was done in order to hopefully encourage the more frequent use of bicycles as a mode of transportation to the campus. In addition, free coffee and donuts have been provided to the users of these remote lots on several occasions. These incentive programs have proven successful in diverting vehicles to these lots, on the days the incentives are "active". On days when the incentives are not "active", the community members often opt to utilize the available on street parking closer to the campus core.

In response to the ongoing concern expressed from neighboring side street residents, the City has initiated a pilot program for Residential Parking on the adjacent side streets. This program should reduce the ability of the UNE community vehicles from parking in these areas. In addition to addressing the neighbor concerns, this will incentivize UNE community vehicles to park in the more remote lots, or face ticketing. It is anticipated that the remote lot usage will increase dramatically as a result.

- 5) **Constructing new parking spaces on campus.** The University has reviewed options for adding additional spaces on campus and has been unable to squeeze any additional spaces onto the main campus. There are no undeveloped and/or unregulated areas on the main campus that could support any significant additional surface parking areas.

The University owned vehicles have all been consolidated to the parking area on campus where parking is the most underutilized, thus “freeing” up several spaces in a more heavily used parking area, in an effort to encourage additional on-site parking. With the purchase of the parcel of land on Bishop Street, the University is looking to relocate the University service vehicles to this location, thus freeing up additional spaces on the main campus.

The University has also utilized the services of a structural engineer to review the viability of the construction of a parking garage on campus. The engineer was asked to look at the available areas on the main campus and determine the feasibility of constructing a parking garage. The largest areas on the main campus are currently parking lots, thus any structured parking construction would need to be developed in these areas, thus increasing the size of the parking structure in order to compensate for these displaced spaces. This review resulted in determining that there are no locations on the main campus that a parking garage, large enough to accommodate the projected parking demand, could be constructed that is compliant with the code established setbacks and height restrictions.

The University owns additional land behind the main campus. This property has an existing field, Gulliver’s Field, where a parking lot could be constructed that would accommodate the entire Portland Campus Master Plan 2012-2017 projected demand. While this field is located in an area proximal to wetlands, the University is confident that a parking area could be designed and constructed in compliance with all pertinent regulations. However, the City Planning Office has expressed concern that permitting this facility would be difficult and has suggested that the University avoid this option.

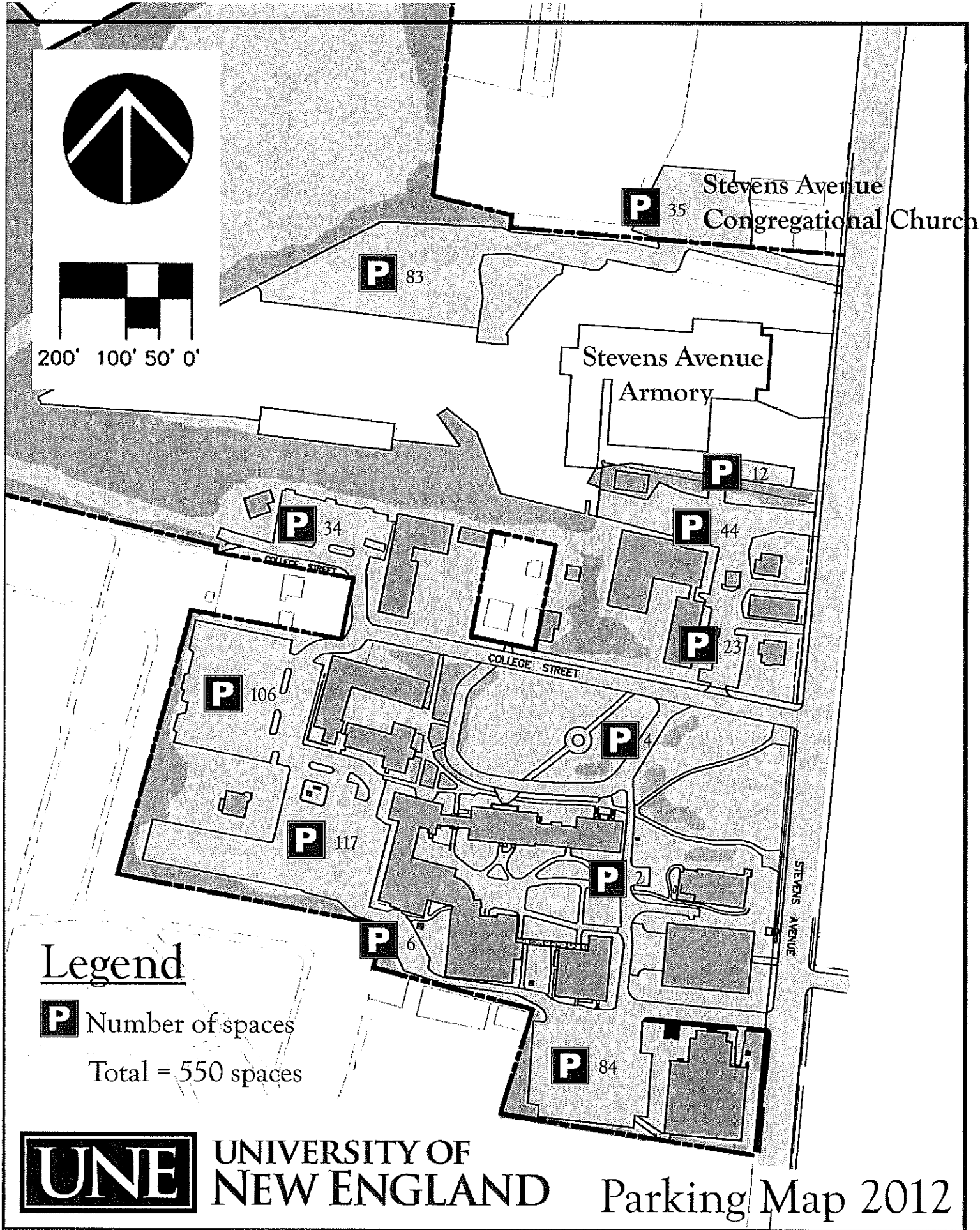


4/1/2012

Portland Campus Master Plan
2012-17
Parking Demand Projections

	2011-12	2012-13 ⁽³⁾	2013-14 ⁽⁴⁾	2014-15	2015-16	2016-17
Students ⁽¹⁾	818	941	1097	1143	1189	1189
Parking required ⁽²⁾ (0.67 spaces/stud)	589	630	766	797	828	828
Parking Supply	467	577	554	554	554	554
Surplus/Deficit	-122	-53	-212	-243	-274	-274

- (1) Assumes WCHP growth as previously outlined and CDM growth of 46 students per year for three years, starting in 2013
- (2) Utilizes parking information provided by John Peverada on 02/22/12
- (2) Accounts for all of the cars parked on College St., and 50% of the vehicles parked proximally on Stevens and side streets from Stevens
- (3) Includes 110 spaces in Armory Lower Level
- (4) Accounts for new CDM Patient Care Clinic facility



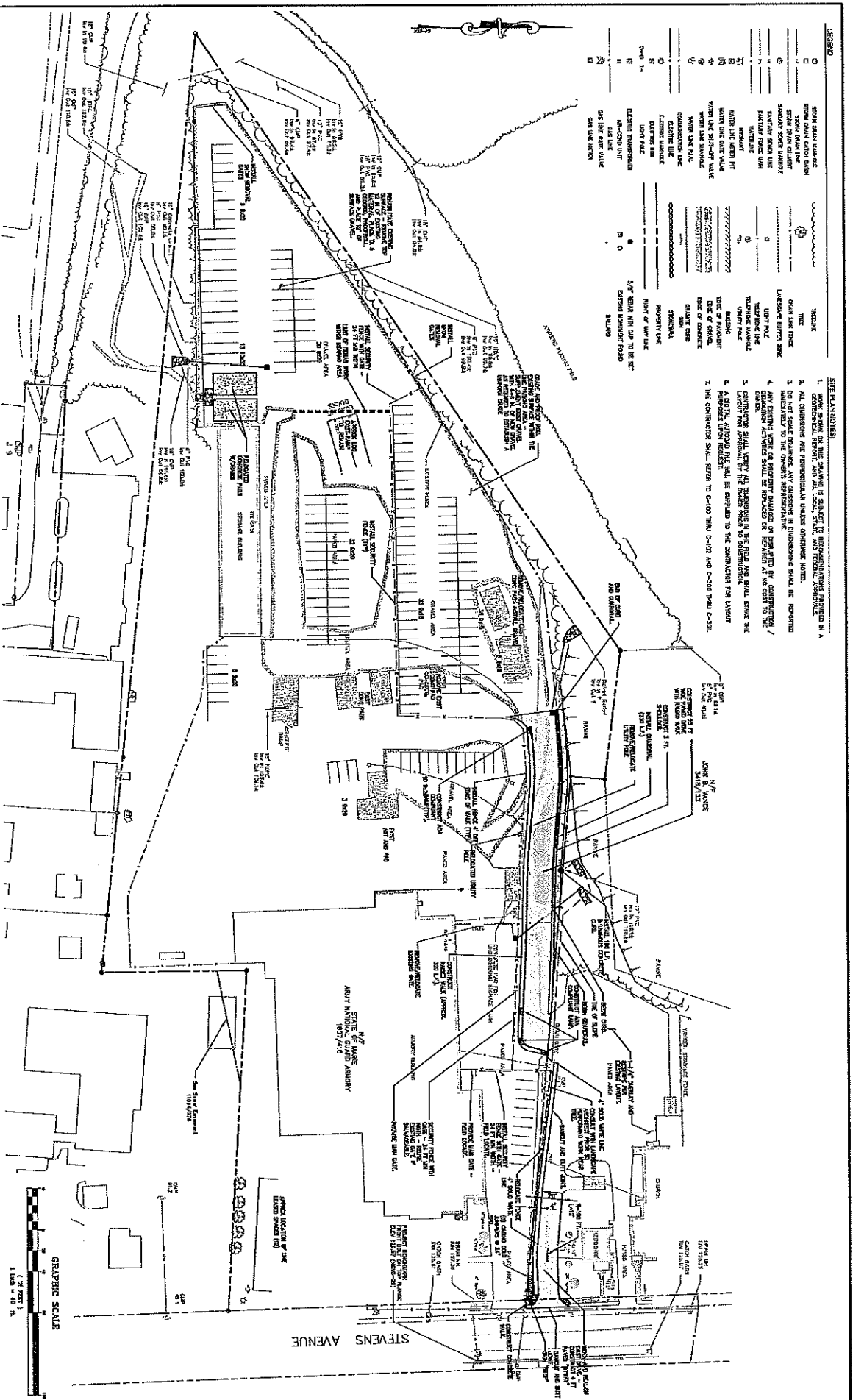


Portland Metro Bus Ridership

	2013	2012	2011	2010	2009	2008	2007	2006
January	0	700	501	371	608	218	242	
February	0		624	339	543	161	192	
March	0		768	370	506	200	592	
April	0		719	442	515	217	565	
May	0		652	330	309	179	531	
June	0		707	350	448	175	356	
July	0		495	360	405	95	24	
August	0		422	398	463	115	19	
September	0		350	656	709	333	362	136
October	0		870	486	414	415	252	179
November	0		740	467	333	340	212	219
December	0		580	370	293	338	153	246
AVERAGE	0	700	619	412	462	232	292	195

During September 2011, the Metro provided free bus services to College Students for two weeks, thus monthly total is not reflective of actual use.

Starting September 2011, passes were issued in increments of ten



- LEGEND**
- | | | | |
|---|----------------------|------|---------------|
| ○ | STORM SEWER MANHOLE | ———— | POUR CONCRETE |
| ○ | STORM SEWER CLEANOUT | ———— | CONCRETE |
| ○ | STORM SEWER | ———— | CONCRETE |
| ○ | STORM SEWER LINE | ———— | CONCRETE |
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- STEP PLAN NOTES:**
1. WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE PROVISIONS OF THE MASSACHUSETTS STATE PLANNING BOARD REGULATIONS, CHAPTER 86C, PART 2A.
 2. ALL DIMENSIONS ARE HORIZONTAL UNLESS OTHERWISE NOTED.
 3. DO NOT SCALE DRAWING. ANY DIMENSIONS IN DIMENSIONS SHALL BE REPORTED AS SHOWN.
 4. ANY DIMENSIONS ARE HORIZONTAL UNLESS OTHERWISE NOTED.
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<p>CLIENT UNIVERSITY OF NEW ENGLAND 11 HILLS BEACH ROAD, BANGOR, MAINE 04403</p> <p>DESIGNER SHE DESIGN ASSOCIATES CONSULTING ENGINEERING & LAND PLANNING 1101 HILLS BEACH ROAD, BANGOR, MAINE 04403 TEL: (207) 688-1924</p> <p>DATE 08/27/24</p>		<p>PROJ. NO. C-101</p> <p>DATE 08/27/24</p> <p>SCALE 1" = 40'</p>																														



AMORY PARKING
STEVENS AVENUE, PORTLAND, MAINE
SITE PLAN

Attachment C

04/01/12

1. *The Planning Board grants an extension to February 24, 2012 for meeting the condition of approval regarding the Stormwater Management Plan.*

Stormwater Management

04/01/12

Existing Conditions

The February 26, 2008 condition of approval for the College of Pharmacy stated “the stormwater management plan shall address the treatment and discharge of stormwater runoff from the campus with the goal of minimizing impacts on the city sewer system and natural drainage system.”

We have reviewed the existing storm drain conditions on the 14 acre portion of the campus located northerly and southerly of College Street, and bounded on the east by Stevens Avenue, referred to herein as the campus proper.

The existing stormdrain system on the campus proper consists of a combination of dry wells sited at strategic locations and stormdrains tied into the city system. As best we can tell from our investigations, the majority of the campus is tributary to existing dry well systems.

The larger existing parking lots on the westerly portion of the campus are tributary to existing dry well systems. A recent inspection indicated that they are functioning adequately.

Runoff from the green located southerly of Goddard and Hersey Halls and from the loop drive and green space northerly of Hersey Hall is tributary to existing dry well systems.

The area of the previously approved College of Pharmacy and Finley recreation center is tributary to an existing enclosed system tied into the city system in Stevens Avenue. The majority of stormwater runoff from the proposed College of Dental Medicine site also flows to Stevens Avenue and College Street in the form of overland flow to that system, while approximately 12,000 sf drains to an existing drywell on the site.

Given the highly permeable nature of the surficial soils on the site, we believe that runoff from open space areas is minimal. Also, based on a review of plans for the older buildings on campus, it is evident that roof drains from these buildings were tied into dry well systems.

Based upon our findings it appears runoff from approximately 80% of the campus is tributary to dry wells. From this one can conclude that 80% of the entire volume of runoff from the campus is being detained and treated on site, which is likely a higher treatment and detention level than most developments in the city.

Since the submission of the original master plan on February 24, 2012, UNE closed on the purchase of an abutting parcel of land to the northwest of campus; the former Pike paving plant. This 19 acre parcel consists of approximately 60-70% impervious surface.

Proposed Conditions

Based on a review of the UNE master plan for the Westbrook College Campus, it appears that one master plan project would require a significant amount of site work and require city staff and planning board review from a stormwater perspective.

This project is the proposed Patient Care Center. This project involves the demolition of several wood frame structures and existing parking areas on the campus in order to construct an approximately 18,600 sf footprint building. The majority of runoff from the building site at the corner of College Street and Stevens Avenue drains to the existing city systems in those streets via overland flow. At this point in time, it appears the building will result in a net increase of approximately 4800 sf of impervious area on the development site.

However, when the College of Pharmacy was permitted in 2008, the city approved stormwater management plan demonstrated a reduction in impervious area of 6000 sf on the campus. With this project, the extent of impervious area on the campus will continue to be 1200 sf less than prior to the College of Pharmacy project. Both sites are tributary to the same system in Stevens Avenue. Also, infiltration systems will be constructed to replace existing infiltration systems displaced by the construction, and to collect runoff from the roof of the proposed building.

Therefore, construction of this project should not adversely affect the receiving stormdrain systems, as runoff rates and quantities will remain less than those existing in 2007-2008.

As noted in the master plan discussion, the purchase of the Pike Industries property will also provide for expansion of the campus. This property will be available for parking, with no increase in impervious area necessary, or proposed, as a part of this master plan.

Conclusions

- Stormwater runoff from approximately 80% of the campus is tributary to dry wells, which detain and provide treatment for 100% of the volume of runoff from these areas. These measures currently minimize any impacts on the city sewer system and natural drainage

system, and likely exceed the treatment and detention provided on most development sites in the city.

- To the best of UNE's knowledge, the existing campus is currently compliant with local and state stormwater regulations.
- The proposed Patient Care Center will result in approximately 4800 sf of new impervious area on the development site. However, when combined with the 2008 College of Pharmacy project, the net result is a reduction in impervious area on the campus when compared with conditions prior to 2008. Additionally, a portion of the runoff from the new building will be routed to dry wells. This minimizes any impacts on the city sewer system and natural drainage system.
- UNE acknowledges condition #2 of the February 1, 2012 UNE - UNE Armory Parking Project Amended Approval Letter which requires mitigation for the additional impervious surface created on the armory site, either on the armory site or UNE owned property. This mitigation will be included in the Site Plan application for the Phase II Armory Parking.

Attachment D

04/01/12

1. *The Planning Board conditions the foregoing granting of the extensions that a Traffic Demand Management Plan (TDM) shall be incorporated as an integral part of the long term parking management strategy and campus master plan.*

The University has contracted Eaton Traffic Engineering to produce a Traffic Demand Management Plan (TDM). The plan was formatted to be consistent with the City of Portland's Technical Manual, and will be utilized in implementing and evaluating the effectiveness of the University's Long-term Parking Strategy plan. Please see the attached TDM, dated 04/01/12.



EATON TRAFFIC ENGINEERING

67 Winter Street Suite 5•Topsham•Maine•04086
Tel 207.725.9805 • Fax 207.373.9400

**University of New England Portland Campus – Transportation Demand Management (TDM) Plan
04/01/12**

Introduction

The University of New England (UNE) Portland Campus is located at 716 Stevens Avenue, in Portland, Maine, just northerly of Evergreen Cemetery. The Portland Campus was formerly Westbrook College, and includes all of the buildings associated with that institution plus a new building housing the College of Pharmacy, and a proposed new building to house the College of Dentistry.

The Portland Campus of UNE is well located in terms of access to multiple modes of transportation. Stevens Avenue is an arterial street in the City of Portland with good access in all directions from the Campus, including the Maine Turnpike (I-95) via Riverside Street, Warren Avenue and Bishop Street, and I-295 access from nearby Forest Avenue. Mass transit service is provided via the Portland Metro Route 3 which connects downtown Portland with North Deering via Stevens Avenue, and Route 2 which connects downtown Portland to the Riverton area and Westbrook via Forest Avenue. There 2 bus stops on the UNE campus (one with a shelter) for Route 3. Bus stops on Forest Avenue in the vicinity of UNE are generally less than 1000 feet from the Campus. (UNE subsidizes the free use of Metro for all students and staff.) Pedestrian travel is facilitated by an extensive systems of sidewalks in the area, which are important not only to UNE, but to other nearby educational facilities such as McAuley High School, Longfellow Elementary School and Deering High School. Finally, Stevens Avenue is a designated bicycle route in the City of Portland, and the UNE Campus provides bicycle racks and access to lockers and showers in the recreational center.

As a university, UNE transportation demand is quite typical of other institutions of higher education. Faculty, staff and a majority of students begin arriving at the site during the morning

peak period (7:00 – 9:00 AM) and departing the site during the evening peak period (4:00 – 6:00 PM). Because of various class schedules these peak periods are not as concentrated as would be associated with other land uses such as offices or retail, where work hours are more rigid. While there are options available to access the campus (bus, bicycle, walking) the majority of students and staff arrive by private vehicle. Parking demand on the UNE campus usually peaks at about 10:00 AM and begins to subside at approximately 2:00 – 3:00 PM. UNE has taken actions to reduce parking demand through use of free Metro tickets, encouragement of carpools and bicycling, but the primary focus of this TDM plan will be clearly to address the issue of parking in terms of reducing parking demand where possible and providing adequate supplies as necessary.

TDM Coordinator

The TDM Coordinator for UNE will be Alan Thibeault, Director of Campus Planning. The Coordinator will be responsible for directing the efforts of staff and outside consultants to implement and monitor efforts to reduce parking (and traffic) demand, as well as provided a campus planning framework to expand parking supply through direct construction of new facilities on or adjacent to the campus, use of possible satellite parking facilities (with some type of vanpool or shuttle service) and/or rental of surplus available spaces in the vicinity of the campus.

Student/Staff/Faculty Survey

UNE will develop a student/staff/faculty survey to monitor modal choice and parking usage. The survey will be prepared so that it is consistent with the surveys used by other educational institutions such as the University of Southern Maine, and perhaps with other large employers in Portland. This survey will take advantage of near universal on-line accessibility to facilitate conduct and evaluations of the results of the survey. With regard to carpool/rideshare opportunities, UNE already makes extensive use of the GoMaine website for both its Biddeford and Portland campuses. This program is overseen by Alethea Cariddi, Sustainability Coordinator for UNE.

Parking/Trip Reduction Target

UNE recently obtained a Traffic Movement Permit from the City of Portland as part of the expansion of the Campus for the College of Pharmacy. No significant traffic impacts were projected for the immediate vicinity of UNE and no direct mitigation action was required. UNE did make a contribution (as part of a city impact fee program) to improvements to Morrill's Corner (Forest Avenue @ Allen Avenue/Stevens Avenue/Bishop Street) as a condition of the City's approval of the expansion. If the University's plan to develop a parking lot on the Bishop Street parcel is implemented and a secondary access is provided from Bishop Street, this would have a mitigating impact on the vehicular traffic commuting through Morrill's Corner.

As noted in previous sections, UNE has encouraged the use of Metro and carpool/rideshare for its population, but most students/staff/faculty currently travel to/from the campus by private automobile. Accordingly, UNE primary emphasis in terms of TDM actions will be to expand parking supply and try to reduce demand while addressing needs to minimize parking on residential streets in the neighborhoods in the vicinity of the site.

Current parking demand projections for the UNE campus (including the proposed College of Dental Medicine) indicate that for the 2016-17 academic year (at which time the proposed College of Dentistry will be at full enrollment) the anticipated deficit in parking supply will be 274 spaces. This figure may be modified or refined over time based upon actual experience and observation in the field, but at this time the target for the TDM is to provide sufficient parking supply to meet this demand and/or reduce this demand by shifting private automobile trips to other modes of travel.

Education

The TDM Coordinator shall provide provisions for informing students/staff/faculty of the following:

- Links to Portland Metro maps and schedules as well as hard copies in areas convenient to students/staff/faculty
- On-line access to information regarding carpooling/rideshare and vanpooling
- Educational and promotional services via e-mail to students/staff/faculty regarding the advantages of use of alternative modes of transportation to/from the campus
- Employee recognition for efforts to reduce traffic impact and/or parking demand via e-mail and web posting
- Providing information regarding new parking strategies, alternative modes, etc.

Monitoring

The TDM Coordinator shall monitor the effectiveness of the TDM plan through providing for continuous on-line contact with students/staff/faculty as well as through more traditional means such as direct comments to staff, correspondence, etc. In addition the periodic surveys of parking usage and use of alternative modes of travel (typically on an annual basis) supplemented by direct observation (for example the counting of empty parking spaces on campus during peak parking demand periods, review of Portland metro ticket usage, use of bicycle racks, etc.). These studies, surveys and observations will be utilized to continually monitor progress and make adjustments and refinements as deemed appropriate.

Project Specific Standards

Infrastructure

The primary focus of UNE, as noted earlier in this document, is upon both increasing parking supply in the vicinity of the campus, and reducing parking demand through increased use of carpool/rideshare opportunities through GoMaine. UNE staff are continually reviewing alternatives to locate accessible potential parking areas in the vicinity of the site, while continuing to encourage carpool/vanpool usage through communications with their students/staff and faculty on the UNE website. UNE did recently obtain additional leased parking supply and will construct additional parking behind the Maine State National Guard Facility. Recent communications with

students/staff and faculty via the UNE website has resulted in increased usage of the new supply with corresponding reduction of on-street parking in residential areas in the vicinity of the UNE campus. The University will be seeking permits to construct additional parking on campus to meet the current and future demands as projected in their Masterplanning activities.

Incentives

UNE has a parking permit/fee program for parking lots on the primary campus; however, in order to encourage use of satellite lots, such as located behind the Maine State National Guard Armory, no fee is charged. In inaugurating this new facility UNE notified students/staff and faculty of its availability, security (patrolled by UNE Safety and Security, call boxes and lighting) and periodically raffled off new bicycles, and other prizes, at the new lot as an incentive to use this parking area. As noted earlier in this document, UNE fully pays the cost of Metro bus tickets for UNE students/staff and faculty. Average monthly transit ridership from 2007 is as follows:

2007	292
2008	232
2009	462
2010	412
2011	653 ¹

UNE will continue this policy of providing free transit service to students/staff and faculty and evaluate other incentives to increase usage of alternative travel modes to hopefully reduce parking demand.

¹ Data from January through April only



UNE Portland Campus – College of Dental Medicine
04/01/12

Parking Demand:

Total New Students: 138 (46/year over 3 years)
 Parking Demand for new students (@ 0.67 sp/student): 93 spaces
 Parking Demand for IPE (5000sf – 50% student/50% outside) @ 3.9 sp/ksf): 10 spaces
 Parking Demand for Dental Clinic (5,492sf @ 3.9 sp/ksf) 21 spaces

Total new parking demand: 124 spaces

Trip Generation:

Trips from new students:	AM Peak Hour	29 trips	23 in/ 6 out
	PM Peak Hour	29 trips	8 in/ 21 out
	PM Peak (Gen)	33 trips	10 in/23 out
Trips from IPE (New)	AM Peak Hour	6 trips	5in/ 1 out
	PM Peak Hour	9 trips	2in/ 7 out
	PM Peak (Gen)	11 trips	4 in/7 out
Trips from Dental Clinic	AM Peak Hour	15 trips	12in/ 3 out
	PM Peak Hour	21 trips	6 in/ 15 out
	PM Peak (Gen)	25 trips	10 in/15 out

Total Trips (Using PM peak hour of the generator – the highest level of trips which occurs sometime after 12:00 Noon but not in the range of 4:00 to 6:00 PM): 69 trips

A Traffic Movement Permit was obtained for the UNE Pharmacy School, thus another TMP is required only when net new trips exceeds 100 trips in a peak hour – this condition is not met and no TMP is required.

University of New England
Patient Care Center
Parking Plan
May 2012

The Site Plan Application contains a Parking Demand Calculation performed by Eaton Traffic Engineering that indicates the proposed Patient Care Center would require a total of 124 parking spaces. The proposed facility would be sited in a location that would displace another 23 spaces, increasing the total demand to 147.

The parking demand for the patients was estimated at 31 spaces, 21 for the College of Dental Medicine Clinic and 10 for the Interprofessional Education Health Clinic (IPE). There are 38 existing parking spaces, at the north end of the site which would be would be dedicated for patient parking.

The University's Master Plan includes the creation of an Intermodal Transportation Facility at the newly acquired property on Bishop Street. This option was reviewed on April 12, 2012 with the City Planning staff, including Alex Jaegerman, Barbara Barhydt and Marge Schmuckal, and it is understood by the University to be supported by City Staff. The University has developed conceptual plans for the site and a copy of this plan is attached.

The additional 109 spaces required for the Patient Care Center, to accommodate the faculty, staff and students would be provided for at the Bishop Street Intermodal Transportation Facility.

The Bishop Street parcel contains approximately 12.5 acres of impervious surface and can be readily utilized for surface parking and could easily accommodate the remaining projected demand for the Patient Care Center with minimal additional development. There is a previously approved stormwater management plan for this site that has been implemented. The conceptual plan provides for additional parking over the projected demand for the Patient Care Center and will accommodate the entire projected long term parking deficit as illustrated in the University's Master Plan.

Pike Industries, the former owner, is in the process of removing from the site all material stockpiles and processing equipment previously utilized in their asphalt manufacturing process. This includes removal of the actual equipment as well as the support structures, and once the equipment is removed they will then return the site to its original grades. It is anticipated that the removal of the equipment will be completed by the end of May 2012, at which time the University's civil engineer can confirm the grades and finalize the design of the Intermodal Transportation Facility (ITF). These plans will then be submitted to staff for review.

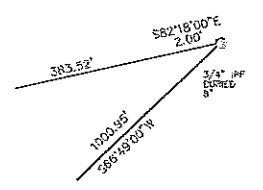
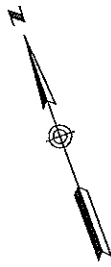
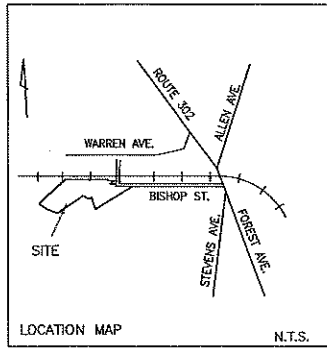
The Bishop Street ITF will require some additional lighting, emergency call boxes, some minor asphalt repair, and striping. In addition, the University would retrofit one of the existing structures, as noted on the plan, to accommodate a heated and secure shelter for persons utilizing the lot, while waiting for the shuttle service.

As the parcel is not directly adjacent to the main campus, the University will need to initiate the operation of a shuttle service to/from the main campus. The University is looking to secure a small bus for this service. The shuttle service is envisioned as operating during the peak academic hours (approximately 9am to 7pm) and offer continuous round trips (3-4 per hour) to insure minimal wait times. After hour transportation to the ITF would be arranged through the University's Security Office, via the existing "escort" service program. This program currently offers rides to persons "uncomfortable" or unable to walk to their parked vehicles or destinations. It is envisioned that the shuttle service would depart the ITF and connect to the campus via an existing service road, and would drop off the riders at Proctor Hall. Distance portal to portal is approximately 2700 feet.

There are several academic schedules that are followed on the UNE's Portland Campus. These include the traditional schedule that allows the individual student to select their classes, which allows for a great diversity in the comings and goings of the students. Another schedule starts the entire program at 8am and concludes at 5pm. These students tend to arrive before the main campus lots are full and then depart the campus after the peak parking demand times. There is also a smaller evening population that begins to arrive on campus in the late afternoon/early evening, which is after the peak parking demand has subsided. These various schedules result in no significant times of arrival and departure that would overwhelm the shuttle service, nor significantly impact the traffic flow in the community.

The University is exploring options for incentivizing the UNE Community to utilize the ITF. Options include free parking permits, or the potentially a rebate program to reimburse the parking permit fee, if the UNE community member utilizes the ITF a predetermined amount of times.

In a continued effort to provide additional parking on the main campus, the University is planning to relocate all service and fleet vehicles to the Bishop Street parcel. The staff utilizing these service vehicles would report to work at the Bishop Street parcel and leave their personal vehicle at that location. This would in effect "free up" approximately 16 additional parking spaces on the main campus, which could then be used by other UNE community members.



N/F
PORTLAND TERMINAL CO. TM 291-A-2
TM 291-A-2

N/F
PORTLAND TERMINAL CO.
RAILROAD LOCATION

N/F
CENTRAL MAINE POWER CO.

N/F
WESTROCK COLLEGE
TM 291-A - LOT 5

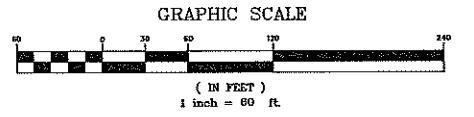
- LEGEND:**
- IRON PIPE OR ROD FOUND
 - GAS VALVE
 - WATER VALVE
 - HYDRANT
 - UTILITY POLE
 - LIGHT POLE
 - MANHOLE
 - CATCH BASIN
 - SIGN
 - FENCE
 - CURB
 - OVERHEAD UTILITIES
 - WATER MAIN
 - GAS MAIN
 - SANITARY SEWER
 - STORM DRAIN
 - EXISTING 1' CONTOUR

TOTAL PARKING 270 SPACES

R-3

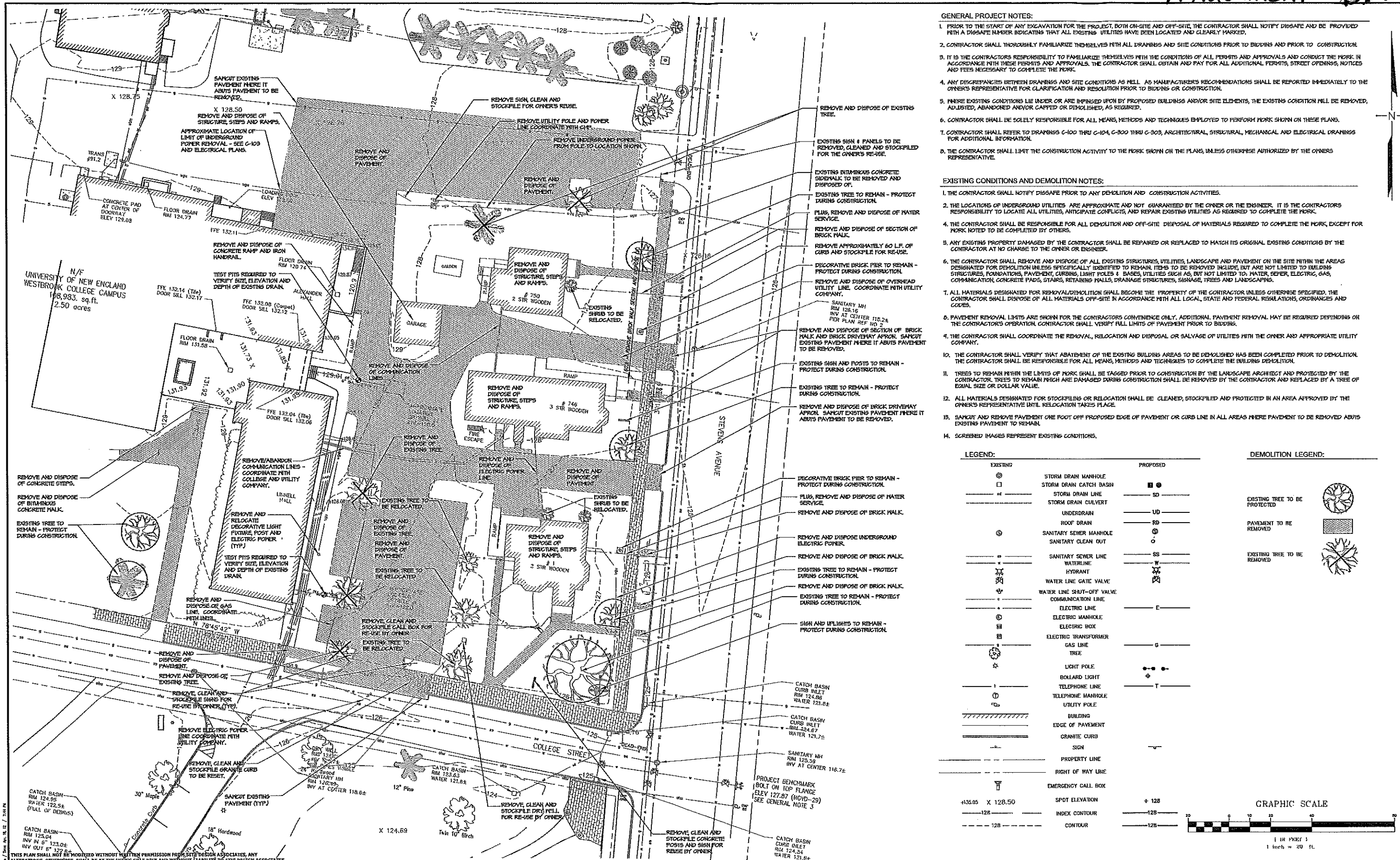
LINE TABLE

No.	Bearing	Distance
L1	S82°34'00\"/>	

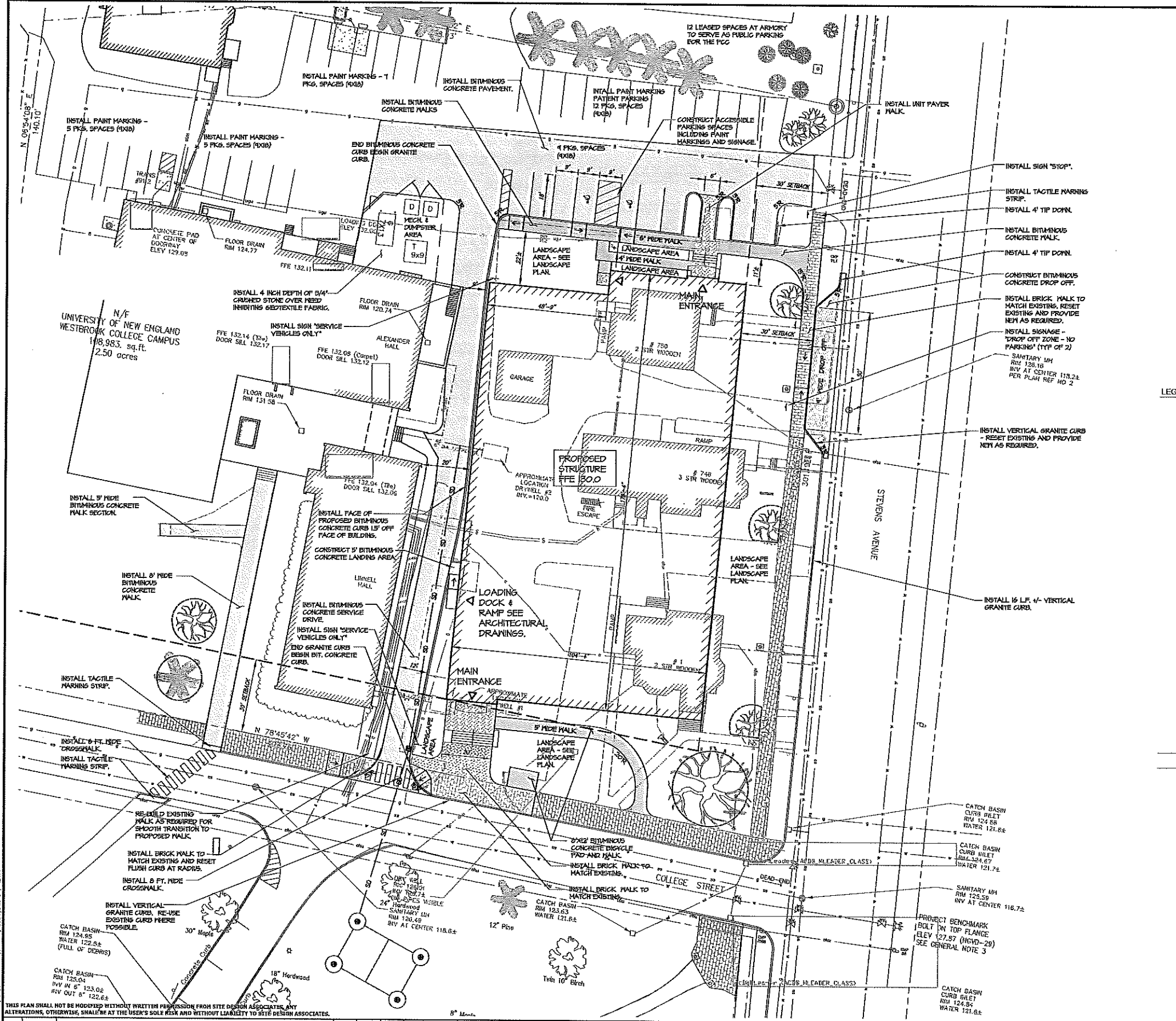


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A 05/02/12 FOR CITY CONCEPTUAL REVIEW DEPT TWS TWS BY CHKD APPD REV DATE STATUS BY CHKD APPD				Site Design Associates Consulting Engineering & Land Planning 23 Whitney Way Topsham, Maine 04086 Tel: (207) 449-4275		DESIGN: TWS DRAWN: DEPT. CHKD: TWS		PROJECT: INTERMODAL FACILITY 102 BISHOP STREET, PORTLAND, MAINE	
						CLIENT: UNIVERSITY OF NEW ENGLAND 11 HILLS BEACH ROAD, BIDDEFORD, MAINE 04005		CONCEPTUAL PARKING PLAN	
REV. DATE STATUS				DATE STATUS		DATE: APRIL 2012 SCALE: 1"=60'		PROJ. NO. DWG. NO. C-100	



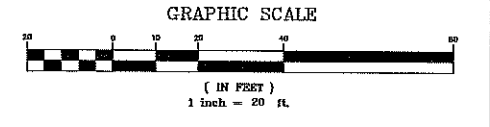
<p>THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM SITE DESIGN ASSOCIATES. ANY ALTERATIONS, OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SITE DESIGN ASSOCIATES.</p>				<p>Site Design Associates Consulting Engineering & Land Planning 23 Whitney Way Topsham, Maine 04086 Tel: (207) 449-4275</p>		<p>DESIGN: DEPT. DRAWN: DEPT. CHKD: TWS</p>		<p>PROJECT: PATIENT CARE CENTER UNIVERSITY OF NEW ENGLAND STEVENS AVENUE, PORTLAND, MAINE</p>			
<p>CLIENT: PORT CITY ARCHITECTURE 65 NEWBURY STREET, PORTLAND, MAINE 04101</p>		<p>DATE: JAN. 2012 SCALE: 1" = 20'</p>		<p>PROJ. NO. DWS. NO.</p>		<p>EXISTING CONDITIONS & DEMOLITION PLAN</p>		<p>REV. C-100</p>			
REV.	DATE	STATUS	BY	CHKD.	APPD.	REV.	DATE	STATUS	BY	CHKD.	APPD.
B	4/17/12	SUBMITTED TO THE CITY OF PORTLAND FOR LEVEL 3 REVIEW	TWS	TWS							
A	4/13/12	ISSUED FOR DESIGN DEVELOPMENT	TWS	TWS							



- SITE PLAN NOTES:**
1. ALL DIMENSIONS ARE PERPENDICULAR UNLESS OTHERWISE NOTED.
 2. DO NOT SCALE DRAWINGS, ANY OMISSIONS IN DIMENSIONING SHALL BE REPORTED IMMEDIATELY TO THE OWNER'S REPRESENTATIVE.
 3. SIDEWALKS AND CURBS SHALL BE DESIGNED AND BUILT WITH TIP DOWN RAMPS AT ALL STREET CORNERS, CROSSWALKS AND DRIVEWAYS IN CONFORMANCE WITH THE CITY OF PORTLAND'S TECHNICAL DESIGN STANDARDS AND GUIDELINES.
 4. THE CITY OF PORTLAND STANDARD DETAILS SHALL BE USED FOR ALL WORK PERFORMED IN THE TOWN OF PORTLAND.
 5. ALL SIGNAGE AND PAVEMENT MARKINGS TO BE IN ACCORDANCE WITH THE UNIVERSITY OF NEW ENGLAND, MITCO STANDARDS AND THE CITY OF PORTLAND ORDINANCES.
 6. CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD AND SHALL STAKE THE LAYOUT FOR APPROVAL BY THE OWNER PRIOR TO CONSTRUCTION.
 7. THE CONTRACTOR'S LICENSED SURVEYOR SHALL ISSUE INTERIM CERTIFICATIONS TO THE OWNER'S REPRESENTATIVE THAT THE SITE IMPROVEMENTS ARE LAYED OUT ACCORDING TO THE PLANS. THE CERTIFICATION SCHEDULE SHALL BE AS FOLLOWS:
 - A. INITIAL BUILDING LAYOUT,
 - B. FOUNDATION INSTALLATION COMPLETE,
 - C. RECORD PLANS OF BUILDING, PAVEMENT & UNDERGROUND UTILITIES.
 8. A DIGITAL AUTOCAD FILE WILL BE SUPPLIED TO THE CONTRACTOR FOR LAYOUT PURPOSES UPON REQUEST.
 9. THE CONTRACTOR SHALL REFER TO C-100 THRU C-104, C-300, THRU C-303, ARCHITECTURAL, MECHANICAL, STRUCTURAL AND ELECTRICAL DRAWINGS FOR ADDITIONAL INFORMATION.
 10. REFER TO ARCHITECTURAL AND STRUCTURAL DRAWINGS FOR BUILDING LAYOUT INFORMATION.

LEGEND:

EXISTING	PROPOSED		
(Symbol)	STORM DRAIN MANHOLE	(Symbol)	STORM DRAIN CATCH BASIN
(Symbol)	STORM DRAIN LINE	(Symbol)	STORM DRAIN CULVERT
(Symbol)	UNDERDRAIN	(Symbol)	ROOF DRAIN
(Symbol)	SANITARY SEWER MANHOLE	(Symbol)	SANITARY CLEAN OUT
(Symbol)	SANITARY SEWER LINE	(Symbol)	WATERLINE
(Symbol)	HYDRANT	(Symbol)	WATER LINE GATE VALVE
(Symbol)	WATER LINE SHUT-OFF VALVE	(Symbol)	COMMUNICATION LINE
(Symbol)	ELECTRIC LINE	(Symbol)	ELECTRIC MANHOLE
(Symbol)	ELECTRIC BOX	(Symbol)	ELECTRIC TRANSFORMER
(Symbol)	GAS LINE	(Symbol)	TREE
(Symbol)	LIGHT POLE	(Symbol)	BOLLARD LIGHT
(Symbol)	TELEPHONE LINE	(Symbol)	TELEPHONE MANHOLE
(Symbol)	UTILITY POLE	(Symbol)	BUILDING
(Symbol)	EDGE OF PAVEMENT	(Symbol)	GRANITE CURB
(Symbol)	SIGN	(Symbol)	PROPERTY LINE
(Symbol)	RIGHT OF WAY LINE	(Symbol)	EMERGENCY CALL BOX
(Symbol)	SPOT ELEVATION	(Symbol)	INDEX CONTOUR
(Symbol)	CONTOUR	(Symbol)	



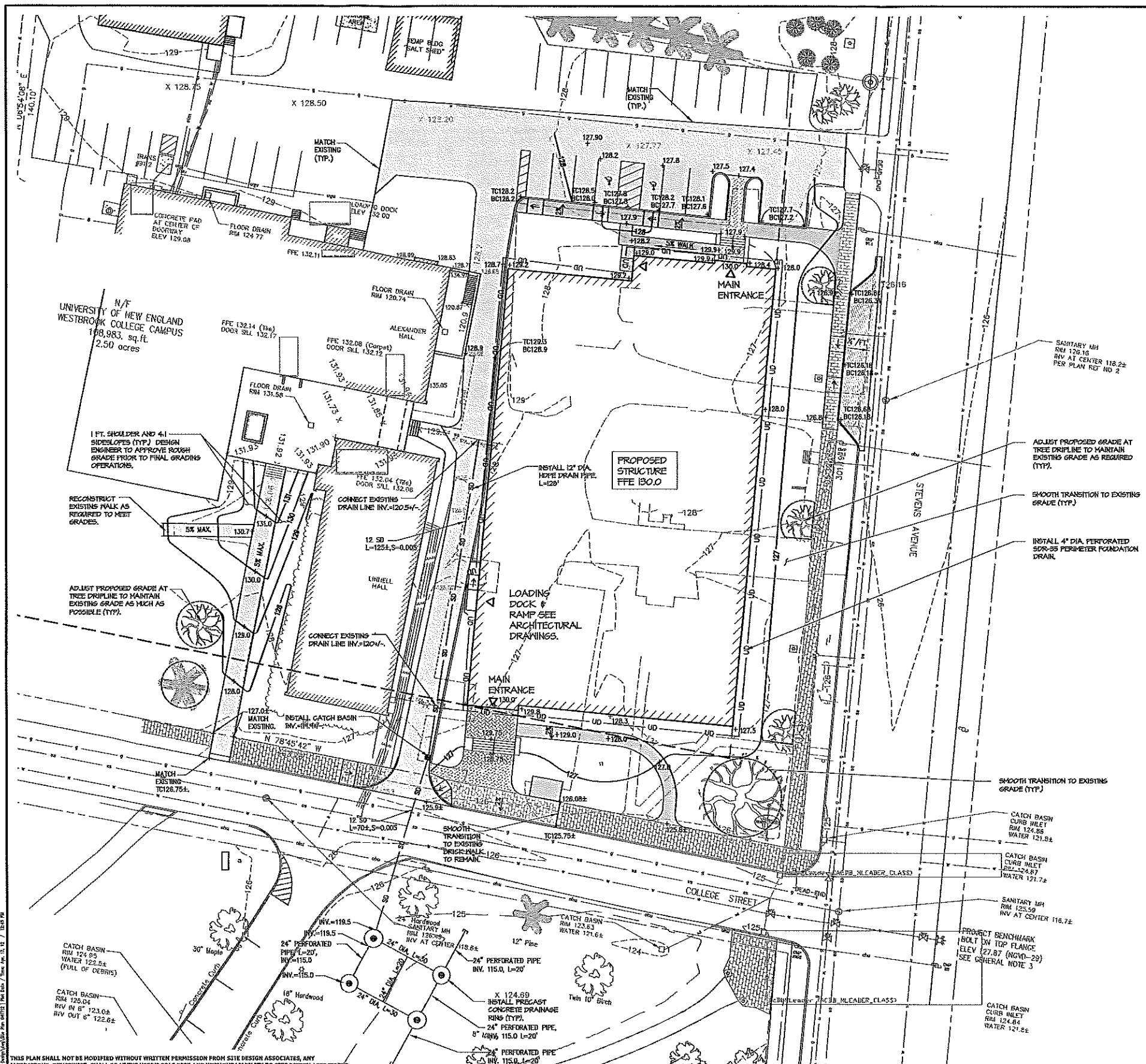
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A	4/13/12	ISSUED FOR DESIGN DEVELOPMENT	TWS	TWS							

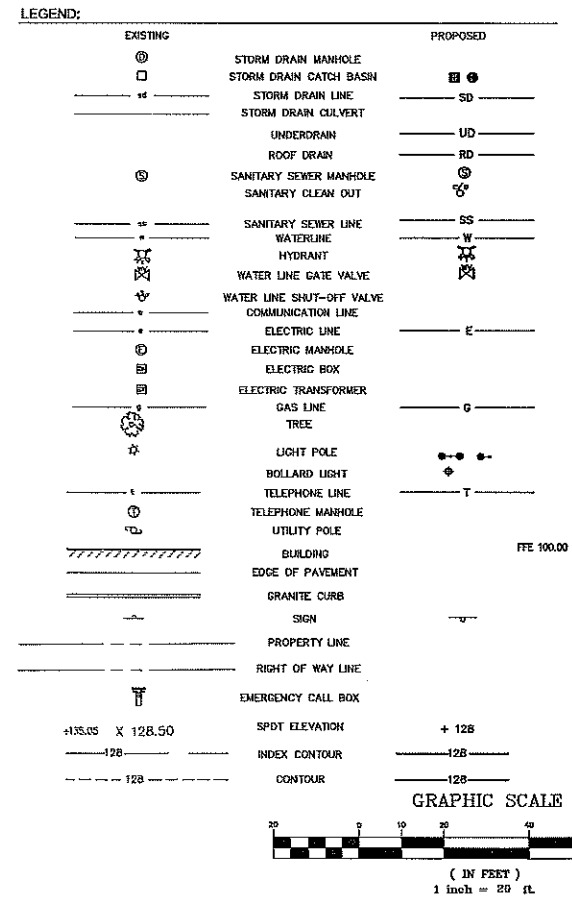


Site Design Associates
 Consulting Engineering & Land Planning
 23 Whitney Way
 Topsham, Maine 04086
 Tel: (207) 449-4275
CLIENT: PORT CITY ARCHITECTURE
 65 NEWBURY STREET, PORTLAND, MAINE 04101

DESIGN: DEPT.	PROJECT:	PATIENT CARE CENTER
DRAWN: DEPT.		UNIVERSITY OF NEW ENGLAND
CHKD: TWS		STEVENS AVENUE, PORTLAND, MAINE
DATE: JAN. 2012	PROJ. NO.	SITE PLAN
SCALE: 1" = 20'	DWG. NO.	C-101



- GRADING & DRAINAGE NOTES:**
- PRIOR TO THE START OF ANY EXCAVATION FOR THE PROJECT BOTH ON AND OFF THE SITE, THE CONTRACTOR SHALL NOTIFY DISSAFE AND BE PROVIDED WITH A DISSAFE NUMBER INDICATING THAT ALL EXISTING UTILITIES HAVE BEEN LOCATED AND MARKED.
 - CONTRACTOR SHALL VERIFY EXISTING GRADES AND NOTIFY THE OWNER'S REPRESENTATIVE OF ANY DISCREPANCIES.
 - CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AWAY FROM ALL STRUCTURES AND PLANTING BEDS.
 - CONTRACTOR TO ADJUST EXISTING AND PROPOSED UTILITY ELEMENT MEANT TO BE FLUSH WITH GRADE (CLEAN-OUTS, UTILITY MANHOLES, CATCH BASINS, INLETS, ETC) THAT ARE AFFECTED BY SITE WORK OR GRADE CHANGES, WHETHER SPECIFICALLY NOTED ON PLANS OR NOT.
 - WHERE PROPOSED GRADES MEET EXISTING GRADES, CONTRACTOR SHALL BLEND GRADES TO PROVIDE A SMOOTH TRANSITION BETWEEN EXISTING AND NEW WORK. PONDING AT TRANSITION AREAS WILL NOT BE ALLOWED.
 - CONTRACTOR SHALL PROVIDE A FINISHED PAVEMENT SURFACE FREE OF LOW SPOTS AND PONDING AREAS.
 - BITUMINOUS CONCRETE ELEVATIONS AT CATCH BASINS TO BE 1/4 INCH ABOVE RIM ELEVATION SHOWN FOR CATCH BASINS.
 - ALL WALKS SHALL PITCH TO DRAIN WITH A MAXIMUM CROSS SLOPE OF 2%.
 - OWNER'S REPRESENTATIVE SHALL APPROVE LAYOUT OF ALL DRAINAGE STRUCTURES PRIOR TO INSTALLATION.
 - ALL DRAINAGE PIPE NOT CALLED OUT ON THE DRAWINGS SHALL BE SMOOTH INTERIOR, CORRUGATED EXTERIOR, HOPE, 11-1/2 AS MANUFACTURED BY ADS.
 - THE CONTRACTOR SHALL INSTALL CATCH BASIN INLET PROTECTION AT ALL CATCH BASIN INLETS WITHIN THE DISTURBED AREA AND BE RESPONSIBLE FOR TAKING ALL NECESSARY EROSION CONTROL MEASURES. SEE SHEET C-300.
 - ALL UNSUITABLE AND UNUSED MATERIALS WHICH CAN NOT BE DISPOSED OF ON SITE SHALL BE REMOVED AND DISPOSED OF OFF SITE BY THE CONTRACTOR IN ACCORDANCE WITH ALL STATE AND LOCAL LAWS.
 - SEE SHEET C-300 FOR EROSION CONTROL NOTES, SEQUENCE AND DETAILS.
 - REFER TO DRAWINGS C-100 THRU C-104 AND C-300 THRU 303 AND THE ARCHITECTURAL, STRUCTURAL, MECHANICAL, AND ELECTRICAL DRAWINGS FOR ADDITIONAL INFORMATION.



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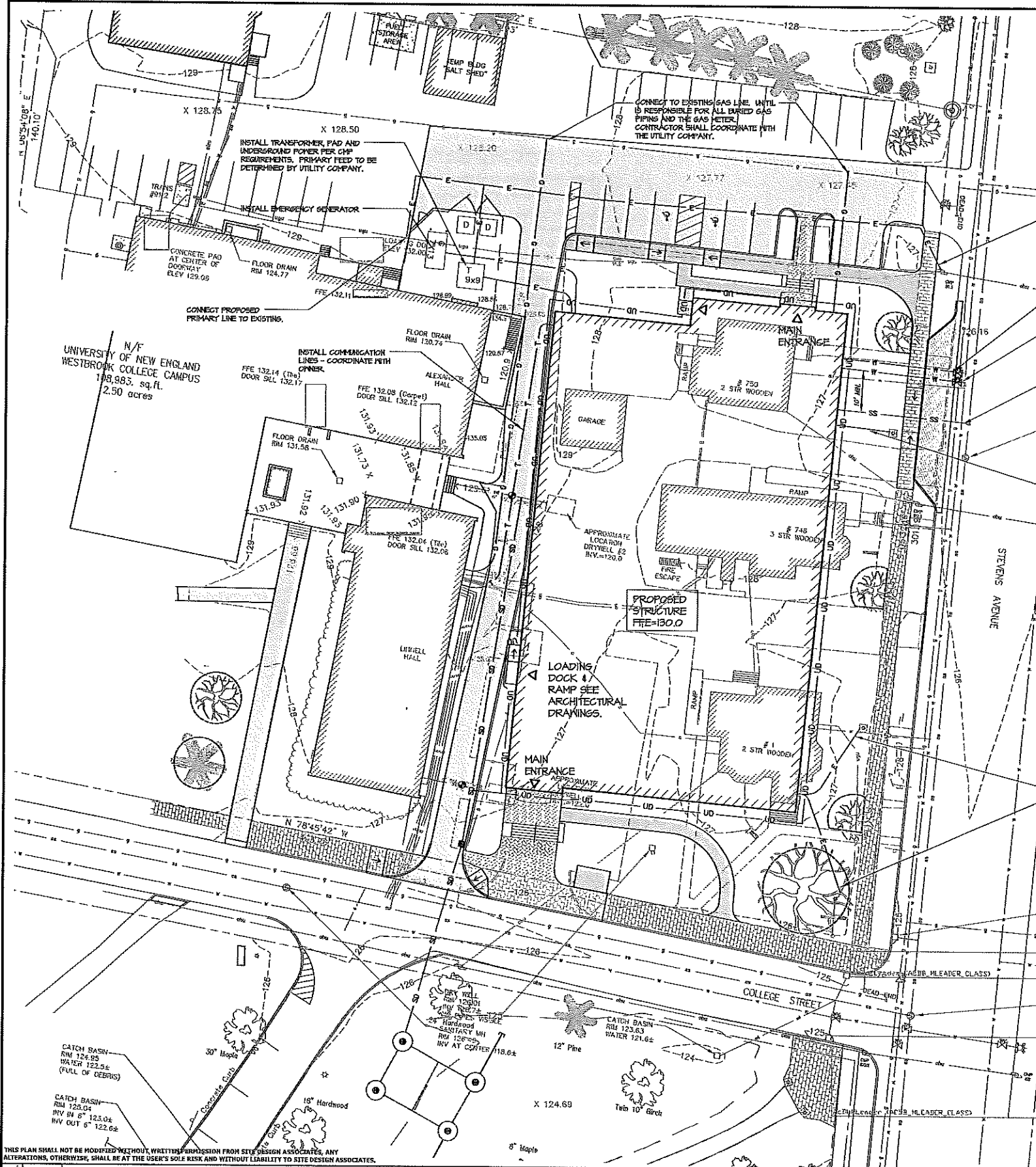
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B	4/17/12	SUBMITTED TO THE CITY OF PORTLAND FOR LEVEL 3 REVIEW	DEPT.	TWS	TWS						
A	4/13/12	ISSUED FOR DESIGN DEVELOPMENT	DEPT.	TWS	TWS						



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DESIGN: DEPT.	PROJECT:	PATIENT CARE CENTER UNIVERSITY OF NEW ENGLAND STEVENS AVENUE, PORTLAND, MAINE
DRAWN: DEPT.		
CHKD: TWS		GRADING, DRAINAGE & EROSION CONTROL PLAN
DATE: JAN. 2012	PROJ. NO.	
SCALE: 1" = 20'	DWG. NO.	
	REV.	
		C-102

B.1c

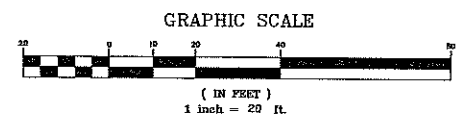


UTILITY NOTES:

1. THE CONTRACTOR SHALL COORDINATE THE INSTALLATION OF NEW UTILITIES WITH THE APPROPRIATE UTILITY COMPANY.
2. THE CONTRACTOR SHALL VERIFY ACTUAL FIELD LOCATION OF UTILITIES PRIOR TO CONSTRUCTION.
3. ALL UTILITY WORK SHALL BE IN CONFORMANCE WITH THE CITY OF PORTLAND AND INDIVIDUAL UTILITY COMPANY STANDARDS RESPECTIVELY.
4. ALL WATER SERVICE SHALL BE COORDINATED WITH THE PORTLAND WATER DISTRICT. CONSTRUCTION MATERIALS, TESTING AND COLORINATION SHALL BE IN ACCORDANCE WITH DISTRICT STANDARDS. WATER VALVES TO BE LEFT OPEN.
5. CONTRACTOR SHALL EXCAVATE TEST PITS AT ALL POINTS WHERE PROPOSED UTILITIES ARE TO CROSS EXISTING UTILITIES. THIS INFORMATION SHALL BE PROVIDED TO THE OWNER'S REPRESENTATIVE PRIOR TO CONSTRUCTION.
6. CONTRACTOR SHALL PROVIDE CONDUIT OF SUITABLE SIZE WITH PULL LINES FOR ALL UNDERGROUND WIRING. SEE ELECTRICAL DRAWINGS FOR CONDUIT REQUIREMENTS.
7. LIGHT POLE BASES TO BE LOCATED 36" MIN. FROM EDGE OF WALKS, DRIVES AND PARKING AREAS. LIGHT POLES SHOWN ON THIS PLAN FOR LOCATION ONLY REFER TO ELECTRICAL DRAWINGS FOR ALL OTHER LIGHTING AND ELECTRICAL INFORMATION.
8. ALL MANHOLES AND CATCH BASINS SHALL BE PROVIDED WITH FLEXIBLE BOOT CONNECTIONS FOR PIPES.
9. PROPOSED CONNECTIONS INTO EXISTING MANHOLES AND CATCH BASINS SHALL BE CORE DRILLED AND A FLEXIBLE BOOT INSTALLED WITH A PRESS WEDGE SYSTEM FOR WATER TIGHTNESS.
10. PIPE MATERIAL FOR 4" AND 6" WATER LINES SHALL BE CLASS 52 DOUBLE CEMENT LINED DUCTILE IRON.
11. MINIMUM COVER OVER WATER LINE SHALL BE 5'-6".
12. GAS LINE SHALL BE INSTALLED PER UTILITY COMPANY (NITEL) STANDARDS.
13. PIPE MATERIAL FOR SANITARY SEWER SHALL BE SDR-35PVC.
14. WHERE THERE IS LESS THAN 6"-6" COVER OVER SEWER LINES, PLACE 2 LAYERS OF 2" THICK RIGID INSULATION OVER PIPE FOR FULL WIDTH OF TRENCH. INSULATION JOINTS SHALL BE STAGGERED.
15. THE OWNER SHALL SUBMIT AS-BUILT RECORD DRAWINGS TO THE CITY FOR ANY UTILITY SERVICE THAT IS UPGRADED OR REPLACED DURING CONSTRUCTION.
16. REFER TO DRAWING G-102 FOR STORM DRAIN INFORMATION.
17. CONTRACTOR SHALL REFER TO MECHANICAL AND ELECTRICAL DRAWINGS FOR EXACT LOCATION, CONFIGURATION, AND DETAILS OF UTILITIES AND WHERE THEY ENTER THE BUILDING.
18. CONTRACTOR SHALL REFER TO G-100 THRU G-104, G-300 THRU G-302, ARCHITECTURAL, STRUCTURAL, MECHANICAL, ELECTRICAL DRAWINGS FOR ADDITIONAL INFORMATION.

LEGEND:

EXISTING		PROPOSED
⊙	STORM DRAIN MANHOLE	⊙
⊞	STORM DRAIN CATCH BASIN	⊞
—	STORM DRAIN LINE	SD
—	STORM DRAIN CULVERT	—
—	UNDERDRAIN	UD
—	ROOF DRAIN	RD
⊙	SANITARY SEWER MANHOLE	⊙
—	SANITARY CLEAN OUT	—
—	SANITARY SEWER LINE	SS
—	WATERLINE	W
—	HYDRANT	—
—	WATER LINE GATE VALVE	—
—	WATER LINE SHUT-OFF VALVE	—
—	COMMUNICATION LINE	—
—	ELECTRIC LINE	E
⊙	ELECTRIC MANHOLE	—
⊞	ELECTRIC BOX	—
⊞	ELECTRIC TRANSFORMER	—
—	GAS LINE	G
—	TREE	—
—	LIGHT POLE	—
—	BOLLARD LIGHT	—
—	TELEPHONE LINE	T
⊙	TELEPHONE MANHOLE	—
—	UTILITY POLE	—
—	BUILDING	—
—	EDGE OF PAVEMENT	—
—	GRANITE CURB	—
—	SIGN	—
—	PROPERTY LINE	—
—	RIGHT OF WAY LINE	—
—	EMERGENCY CALL BOX	—
+128.05 X 128.50	SPOT ELEVATION	+ 128
—128	INDEX CONTOUR	—128
—128	CONTOUR	—128



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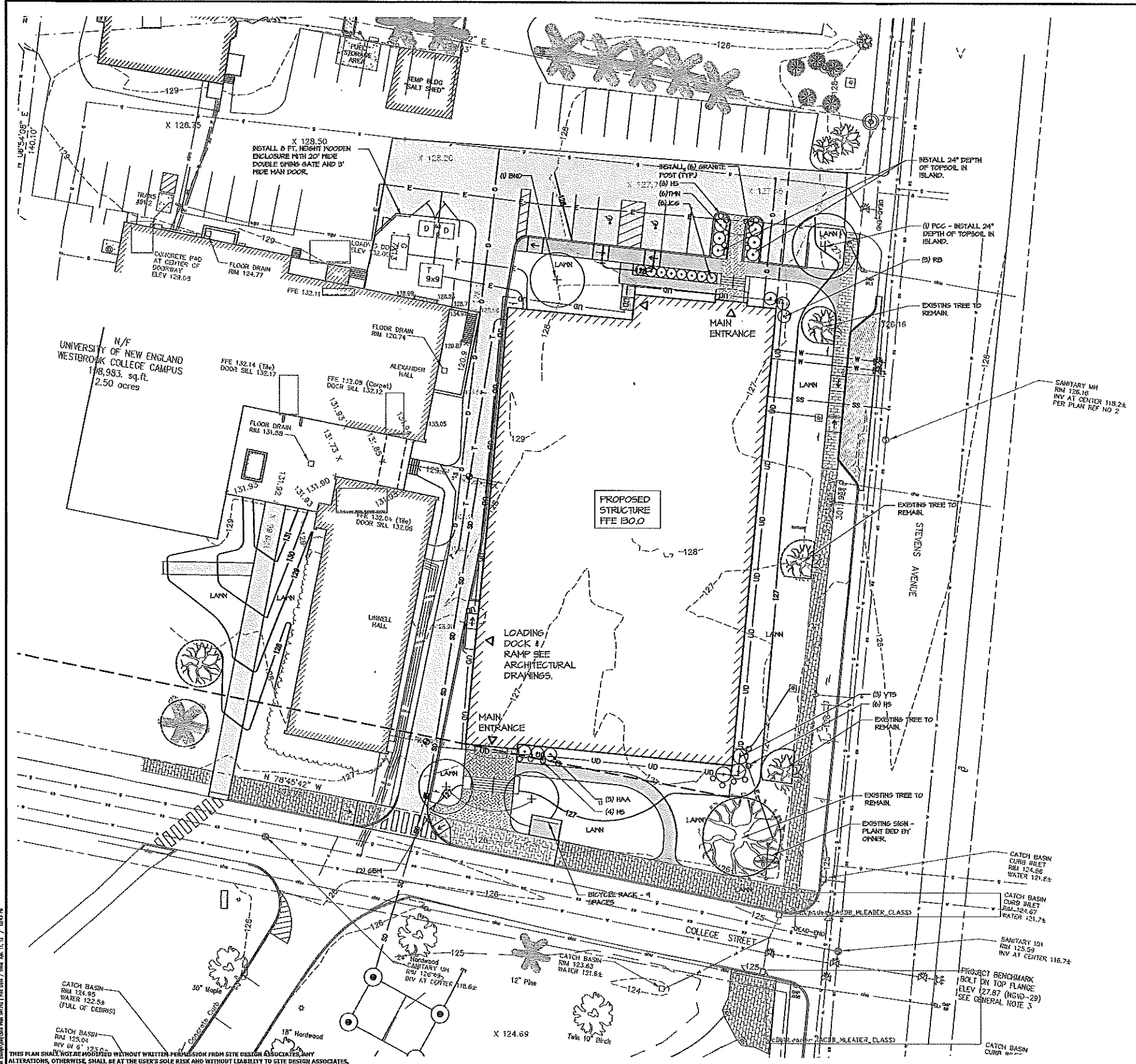
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DESIGN: DEPT.	PROJECT:	PATIENT CARE CENTER UNIVERSITY OF NEW ENGLAND STEVENS AVENUE, PORTLAND, MAINE
DRAWN: DEPT.	CHKD: TWS	SITE UTILITIES PLAN
DATE: JAN. 2012	PROJ. NO.	
SCALE: 1" = 20'	DWG. NO.	C-103

B.1d



LANDSCAPE NOTES:

- CONTRACTOR SHALL VERIFY ALL TREE REMOVALS WITH LANDSCAPE ARCHITECT PRIOR TO START OF CONSTRUCTION. ANY TREES MARKED TO REMAIN THAT ARE DAMAGED OR REMOVED DURING CONSTRUCTION SHALL BE REPLACED WITH TREES EQUALING THE SPECIES AND CALIPER LOGS.
- LANDSCAPE CONTRACTOR IS ENCOURAGED TO PROVIDE THE LANDSCAPE ARCHITECT WITH CONCERNS AND/OR SUGGESTIONS WITH REGARDS TO PROPOSED PLANT MATERIAL SELECTION PRIOR TO PLACING A PURCHASE ORDER.
- THE LANDSCAPE CONTRACTOR SHALL SUPPLY ALL PLANT MATERIALS IN QUANTITIES SUFFICIENT TO COMPLETE ALL PLANTINGS SHOWN GRAPHICALLY ON THIS DRAWING. CLARIFY ANY DISCREPANCIES WITH THE LANDSCAPE ARCHITECT PRIOR TO PRICING ANY PLANT MATERIAL.
- ALL PLANT MATERIALS SHALL CONFORM TO THE GUIDELINES ESTABLISHED BY THE LATEST EDITION OF THE AMERICAN ASSOCIATION OF NURSERMEN'S "AMERICAN STANDARD OF NURSERY STOCK".
- ALL PLANT MATERIALS ARE SUBJECT TO THE APPROVAL OF THE OWNER'S REPRESENTATIVE AT THE SITE. PLANTS WHICH ARE REJECTED SHALL BE REMOVED FROM THE SITE IMMEDIATELY AND REPLACED AT NO ADDITIONAL COST TO THE OWNER.
- ALL TREES AND SHRUBS TO BE BALLED IN BURLAP OR CONTAINERIZED.
- MULCH FOR PLANTED AREAS TO BE AGED SPRUCE AND FIR BARK, PARTIALLY DECOMPOSED, DARK BROWN IN COLOR AND FREE OF MOOD CHIPS THICKER THAN 1/4 INCH.
- CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AWAY FROM ALL BUILDING FOUNDATIONS, STRUCTURES AND PLANTING BEDS.
- NO PLANTS SHALL BE PLANTED BEFORE ACCEPTANCE OF ROUGH GRADING AND BEFORE CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- ALL SHRUB GROUPINGS SHALL BE INCORPORATED INTO BEDS. WHERE MULCHED PLANT BED ADJUTS LAWN, CONTRACTOR SHALL PROVIDE A TURF CUT EDGE.
- ALL TREES ALONG WALK AND PARKING AREAS SHALL BEGIN BRANCHING AT 6' HT. MIN.
- ALL PLANT MATERIAL OR REPRESENTATIVE SAMPLES SHALL BE LEGIBLY TAGGED WITH PROPER COMMON AND BOTANICAL NAMES. TAGS SHALL REMAIN ON THE PLANTS UNTIL FINAL ACCEPTANCE.
- CONTRACTOR SHALL LOADED DISTURBED AREAS AS FOLLOWS:
- LAWN AREAS 6" DEPTH OF TOPSOIL.
- SEED MIXTURES FOR AREAS TO BE SEEDED SHALL BE AS FOLLOWS:
- LAWN AREAS SHALL BE SEEDED WITH: SEED TYPE & BY HEIGHT CREEPING RED FESCUE (MIN. 2 VARIETIES) 55%, KENTUCKY BLUEGRASS (MIN. 2 VARIETIES) 30%, PERENNIAL RYE GRASS 15%. SEED AT A RATE OF 4 LBS PER 1000 SQ.FT.
- CONTRACTOR SHALL BEGIN MAINTENANCE IMMEDIATELY AFTER PLANTING AND WILL CONTINUE UNTIL FINAL ACCEPTANCE. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MEANS AND METHODS OF WATERING AND MAINTENANCE.
- THE LANDSCAPE CONTRACTOR SHALL GUARANTEE ALL PLANT MATERIALS FOR ONE (1) FULL YEAR FROM DATE OF FINAL ACCEPTANCE.
- SCREENED IMAGES SHOW EXISTING CONDITIONS. WHERE EXISTING CONDITIONS LIE UNDER OR ARE IMPINGED UPON BY PROPOSED BUILDINGS AND OR SITE ELEMENTS, THE EXISTING CONDITION WILL BE REMOVED, ABANDONED AND OR CAPPED OR DEMOLISHED AS REQUIRED.
- SEE DRAWINGS C-100 THRU C-104 AND C-300 THRU C-302 FOR ADDITIONAL INFORMATION.

PLANT LIST:

SYMBOL	BOTANICAL NAME	COMMON NAME	QTY	SIZE	COMMENTS
TREES					
BND	BETULA NIGRA 'DURAEAT'	DURAEAT RIVER BIRCH	1	12" HT.	CLUMP, BIB
GM	GINKGO BILoba MASYAR	MASYAR GINKGO	2	3" GAL.	BIB
PGC	PYRUS CALLERIANA 'CLEVELAND'	CLEVELAND FLOWERING PEAR	1	3" GAL.	BIB
SHRUBS, GROUNDCOVERS & HERBACEOUS MATERIALS					
HAA	HYDRANGEA ARBORESCENS 'ANNABELLE'	ANNABELLE HYDRANGEA	3	36" HT.	FULL & BUSHY
HS	HEMEROCALLIS STELLA D'ORO	STELLA D'ORO DAYLILLY	16	1 GAL.	FULL & BUSHY
JCG	JANIPEROUS CHINENSIS 'CASINO GOLD'	CASINO GOLD CHINESE JUNIPER	8	24" HT.	FULL & BUSHY
RB	RHODODENDRON BOULE DE NEIGE	BOULE DE NEIGE RHODODENDRON	3	36" HT.	FULL & BUSHY
THN	TAXUS X MEDIA 'NIGRA'	DARK SPREADING YEW	6	30" HT.	FULL & BUSHY
YTS	VIBURNUM P. TOMENTOSUM 'SHASTA'	SHASTA DOUBLEFILE VIBURNUM	3	40" HT.	FULL & BUSHY

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DESIGN: DEPT.	PROJECT:	PATIENT CARE CENTER
DRAWN: DEPT.	UNIVERSITY OF NEW ENGLAND	STEVENS AVENUE, PORTLAND, MAINE
CHKD: TWS	LANDSCAPE PLAN	
DATE: JAN. 2012	PROJ. NO.	-
SCALE: 1" = 20'	DWG. NO.	C-104

Site Design Associates
 Consulting Engineering & Land Planning
 23 Whitney Way Topsham, Maine 04086 Tel: (207) 449-4275
 CLIENT: **PORT CITY ARCHITECTURE**
 65 NEWBURY STREET, PORTLAND, MAINE 04101

BY	CHKD	APPD	REV.	DATE	STATUS	BY	CHKD	APPD	REV.	DATE	STATUS
B				4/17/12	SUBMITTED TO THE CITY OF PORTLAND FOR LEVEL 3 REVIEW	PBB					
A				4/13/12	ISSUED FOR DESIGN DEVELOPMENT	TWS					

EROSION AND SEDIMENTATION CONTROL NOTES & DETAILS

TEMPORARY EROSION AND SEDIMENTATION CONTROL MEASURES INCLUDE THE USE OF STABILIZED CONSTRUCTION ENTRANCE, SEDIMENT BARRIER, EROSION CONTROL MIX, STONE CHECK DAMS, HAY BALE BARRIERS, CATCH BASIN INLET BARRIERS, CATCH BASIN SEDIMENT COLLECTION BAGS, EROSION CONTROL BLANKET, AND TEMPORARY SEEDING AND MULCHING AS REQUIRED...

- A. GENERAL
1. IT IS ANTICIPATED THAT CONSTRUCTION WILL BEGIN AS SOON AS POSSIBLE FOLLOWING RECEIPT OF NECESSARY PERMITS.
2. ALL SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MAINE EROSION AND SEDIMENT CONTROL BMPs...

B. EROSION AND SEDIMENTATION CONTROL MEASURES

- 1. PRIOR TO THE BEGINNING OF CONSTRUCTION, A STABILIZED CONSTRUCTION ENTRANCE AND TEMPORARY SILT FENCE SHALL BE INSTALLED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE OWNER'S REPRESENTATIVE.
2. ALL CATCH BASINS, NEW OR EXISTING, THAT MAY RECEIVE RUNOFF FROM DISTURBED AREAS MUST BE PROTECTED DURING CONSTRUCTION, INSPECT & CLEAN OUT AS NECESSARY...

FOR THE LIGHT, MOISTURE, AND SOIL CONDITIONS, AMEND AREAS OF DISTURBED SUBSOIL WITH TOP SOIL OR OTHER ORGANIC AMENDMENTS, PROTECT SEEDBED AREAS WITH MULCH OR, IF NECESSARY, EROSION CONTROL BLANKETS, AND SCHEDULE SOEDING, PLANTING, AND SEEDING SO TO AVOID DIE-OFF FROM SUMMER DROUGHT AND FALL FROSTS...

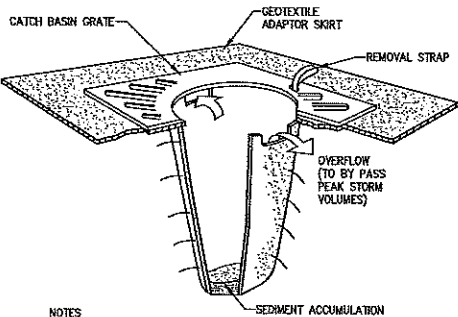
- 11. PERMANENT SEEDING SPECIFICATION. IF A LANDSCAPE PLAN HAS BEEN PREPARED FOR THE PROJECT, SOIL PREPARATION AND SEEDING OF THAT PLAN SHALL SUPERSEDE THESE GENERAL PERMANENT SEEDING SPECIFICATIONS.
A. APPLY TOPSOIL TO A MINIMUM DEPTH OF 6 INCHES, MIX TOPSOIL WITH THE SUBSOIL TO A MINIMUM DEPTH OF 2 INCHES.

- 1. "WINTER CONSTRUCTION" IS CONSTRUCTION ACTIVITY PERFORMED DURING THE PERIOD FROM NOVEMBER 1 THROUGH APRIL 15. IF AREAS WITHIN THE CONSTRUCTION ACTIVITY ARE NOT STABILIZED WITH TEMPORARY OR PERMANENT MEASURES OUTLINED ABOVE BY NOVEMBER 15, THEN THE SITE MUST BE PROTECTED WITH ADDITIONAL STABILIZATION MEASURES THAT ARE SPECIFIC TO WINTER CONDITIONS.
2. AREAS WITHIN 100 FEET OF A PROTECTED NATURAL RESOURCE MUST BE PROTECTED WITH A DOUBLE ROW OF SEDIMENT BARRIERS.

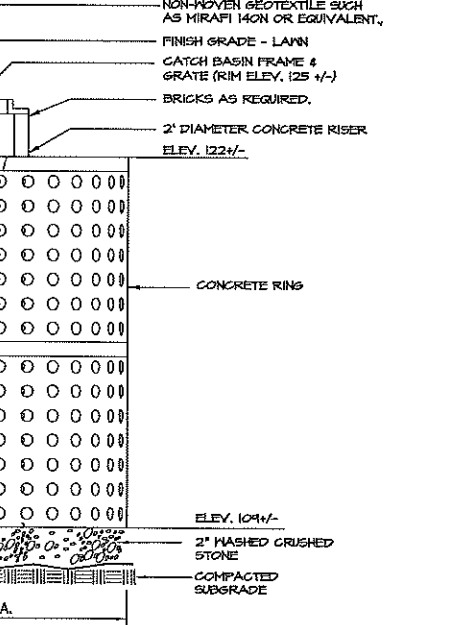
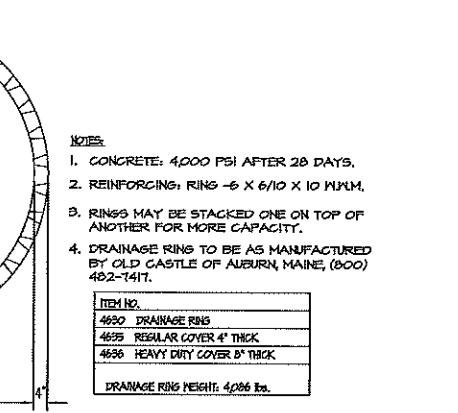
- D. HOUSEKEEPING
1. SPILL PREVENTION CONTROLS MUST BE USED TO PREVENT POLLUTANTS FROM BEING DISCHARGED FROM MATERIALS ON SITE, INCLUDING STORAGE PRACTICES TO MINIMIZE EXPOSURE OF THE MATERIALS TO STORMWATER, AND APPROPRIATE SPILL PREVENTION, CONTAINMENT, AND RESPONSE PLANNING AND IMPLEMENTATION.

- 4. DEBRIS AND OTHER MATERIAL LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION CHEMICALS EXPOSED TO STORM WATER, MUST BE PREVENTED FROM BECOMING A POLLUTANT SOURCE.
5. TRENCH OR FOUNDATION DE-WATERING, TRENCH DE-WATERING IS THE REMOVAL OF WATER FROM TRENCHES, FOUNDATIONS, COFFER DAMS, PONDS AND OTHER AREAS WITHIN THE CONSTRUCTION AREA THAT RETAIN WATER AFTER EXCAVATION...

- E. INSPECTION AND MAINTENANCE
1. INSPECT DISTURBED AND IMPERVIOUS AREAS, EROSION AND STORMWATER CONTROL MEASURES, AREAS USED FOR STORAGE THAT ARE EXPOSED TO PRECIPITATION, AND LOCATIONS WHERE VEHICLES ENTER OR EXIT THE SITE AT LEAST ONCE A WEEK AS WELL AS BEFORE AND AFTER STORM EVENTS...
2. AN INSPECTION AND MAINTENANCE LOG MUST BE KEPT SUMMARIZING THE SCOPE OF THE INSPECTION, NAME AND QUALIFICATIONS OF THE PERSON PERFORMING THE INSPECTION...



- NOTES
1. CATCH BASIN PROTECTION TO BE "SLITSTACK" (BY ACF ENVIRONMENTAL) OR "STREAM GUARD" (BY FOSS ENVIRONMENTAL SERVICES).
2. INSPECT INSERT AFTER ALL RAINFALL EVENTS, REPAIR AND MAINTAIN AS REQUIRED.



TRENCH BACKFILL SCHEDULE table with columns for PIPE MATERIAL, PIPE BEDDING, INITIAL BACKFILL, and BACKFILL.

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Revision table with columns: REV., DATE, STATUS, BY, CHKD., APPD., REV., DATE, STATUS, BY, CHKD., APPD.

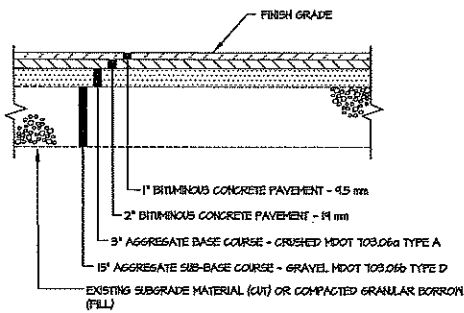
Project information block including Site Design Associates logo, address (23 Whitley Way, Topsham, Maine 04086), project name (PATIENT CARE CENTER), and drawing number (C-300).

B.l.e.

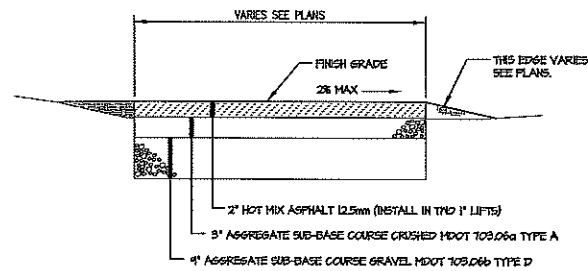
B.1f

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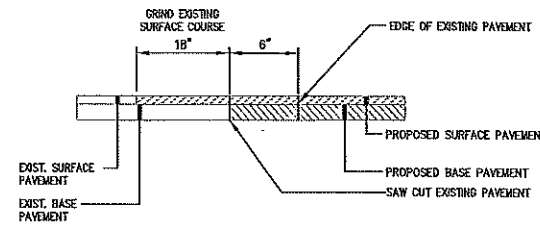
- ADDITIONAL SUBBASE GRAVEL AND/OR GEOTEXTILES MAY BE REQUIRED DURING CONSTRUCTION IF SOIL CONDITIONS WARRANT.
- BUILD-UP RECOMMENDATION IS PRELIMINARY REFER TO GEOTECHNICAL REPORT.



1 PARKING AREAS, SERVICE DRIVE & DROP OFF BUILD-UP
SCALE: N.T.S.



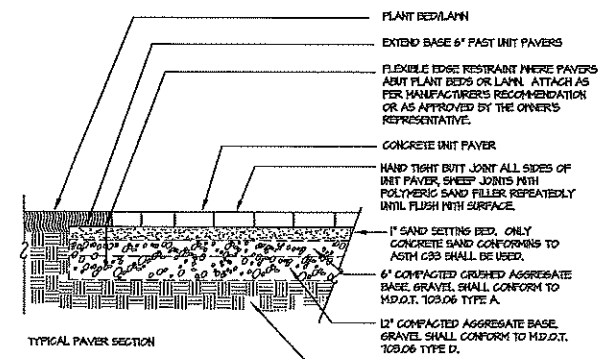
2 WALKWAY BUILD UP - BITUMINOUS CONCRETE (ALEXANDER AND LINNELL PLAZA)
SCALE: N.T.S.



3 HMA PAVEMENT BUTT JOINT
SCALE: N.T.S.

NOTES:

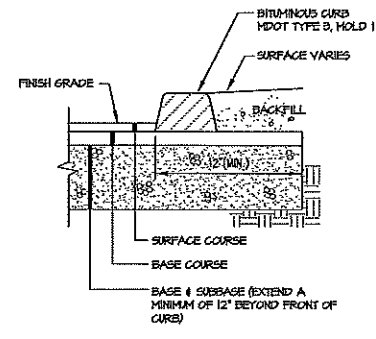
- ALL PAVERS SHALL BE SAW CUT ONLY. NO FRAGMENT SHALL BE SMALLER THAN 2\"/>



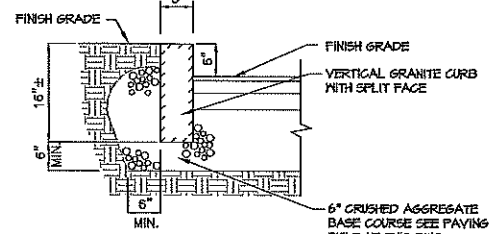
4 CONCRETE UNIT PAVER
SCALE: N.T.S.

NOTES:

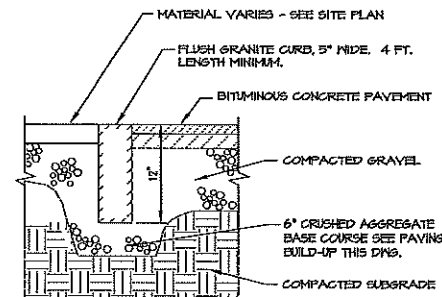
- NO STRAIGHT CURB STONES SHALL BE USED ON RADII. SUBMIT SHOP DRAWINGS TO THE OWNER'S REPRESENTATIVE OF CURB STONE LENGTHS TO BE USED.
- MINIMUM LENGTH OF STRAIGHT CURB STONES SHALL BE 4'.
- MAXIMUM LENGTH OF STRAIGHT CURB STONES SHALL BE 8'.
- JOINTS BETWEEN CURB STONES SHALL BE NO LESS THAN 1/8\"/>



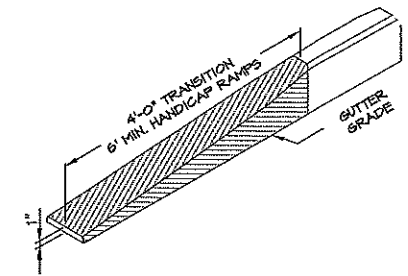
5 BITUMINOUS CURB (ADJACENT TO SIDEWALK)
SCALE: N.T.S.



6 VERTICAL GRANITE CURB (RESET & NEW)
SCALE: N.T.S.



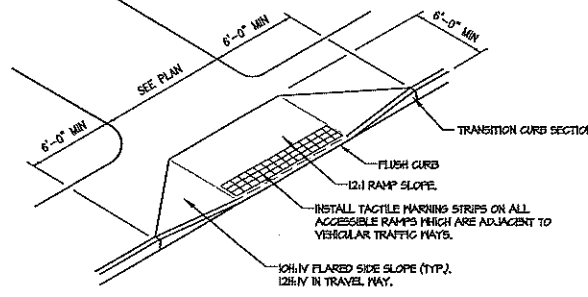
7 GRANITE CURB - FLUSH
SCALE: N.T.S.



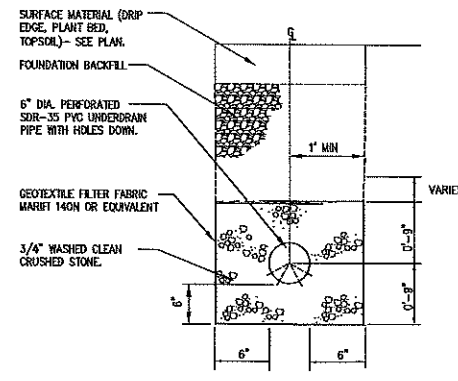
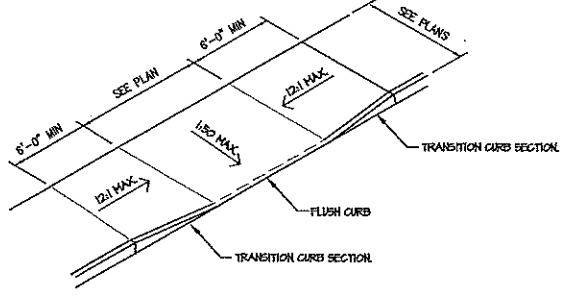
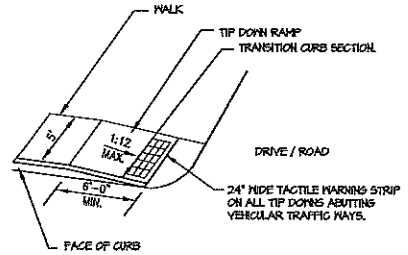
8 TRANSITION CURB
SCALE: N.T.S.

NOTES:

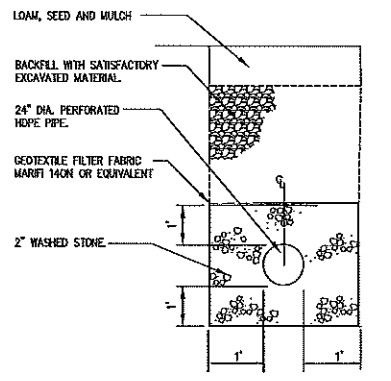
- DIMENSIONS SHOWN AT EDGE OF ROAD ARE FIXED.
- RAMP CROSS SECTION TO BE THE SAME AS ADJACENT SIDEWALK, I.E. DEPTH OF SURFACE AND FOUNDATION.
- DIMENSIONS ARE SUBJECT TO CHANGE IN FIELD. ALL SLOPES AND DIMENSIONS TO COMPLY WITH A.D.A. REQUIREMENTS.
- 24\"/>



9 ACCESSIBLE CURB CUT RAMPS
SCALE: N.T.S.



10 FOUNDATION PERIMETER DRAIN
SCALE: N.T.S.



11 DRAINAGE RING PIPE
SCALE: N.T.S.

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REV	DATE	STATUS	BY	CHKD	APPD	REV	DATE	STATUS	BY	CHKD	APPD
B	4/17/12	SUBMITTED TO THE CITY OF PORTLAND FOR LEVEL 3 REVIEW	DEPT.	TWS	TWS						
A	4/13/12	ISSUED FOR DESIGN DEVELOPMENT	DEPT.	TWS	TWS						



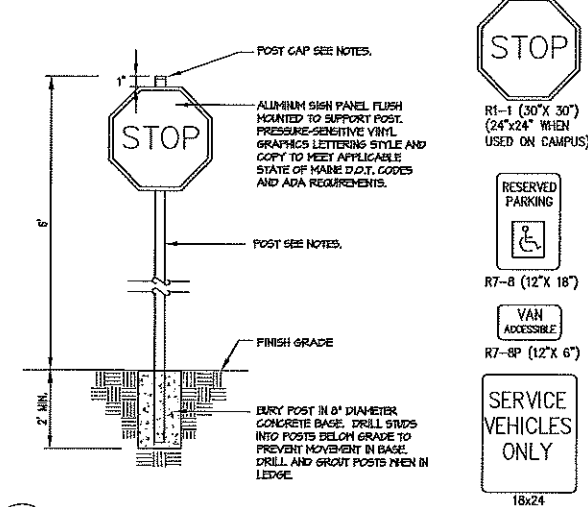
Site Design Associates
Consulting Engineering & Land Planning
23 Whitney Way
Topsham, Maine 04086
Tel: (207) 449-4275
CLIENT: **PORT CITY ARCHITECTURE**
65 NEWBURY STREET, PORTLAND, MAINE 04101

DESIGN: DEPT.
DRAWN: DEPT.
CHKD: TWS
DATE: JAN. 2012
SCALE: AS NOTED

PROJECT: **PATIENT CARE CENTER**
UNIVERSITY OF NEW ENGLAND
STEVENS AVENUE, PORTLAND, MAINE
SITE DETAILS
PROJ. NO. _____
DWG. NO. _____
REV. _____
C-301 B

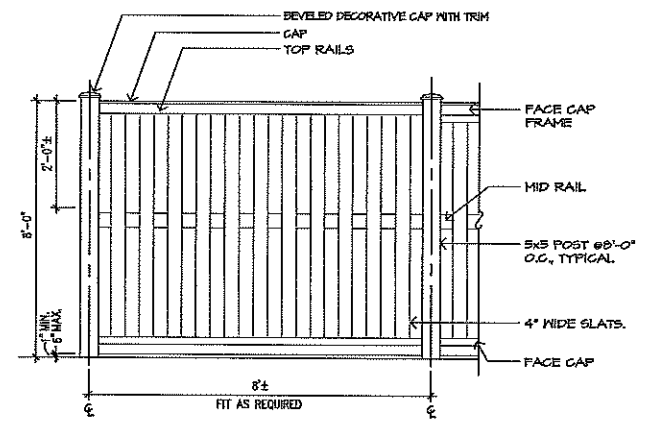
B.1g

- NOTES:**
1. PROVIDE SHOP DRAWINGS OF ALL SIGNS TO THE OWNER FOR APPROVAL PRIOR TO PURCHASING.
 2. OWNER TO APPROVE ALL SIGN LOCATIONS IN THE FIELD PRIOR TO INSTALLATION.
 3. CONTRACTOR TO REVIEW SIGN LOCATIONS FOR CONFLICTS WITH UNDERGROUND UTILITIES AND UNDERDRAN PRIOR TO INSTALLING POSTS.
 4. ALL SIGNAGE TO COMPLY WITH ALL ADA, FEDERAL, LOCAL CODES AND MUTCD STANDARDS.
 5. SIGN POSTS - FOR POSTS WITH INDIVIDUAL SIGNS, POSTS TO BE 2"x2"x6' POWDERED ALUMINUM, 0.06" THICKNESS, SQUARE PLASTIC CAP IN TOP AS MANUFACTURED BY NEWCO OF PROVIDENCE RHODE ISLAND. FOR POSTS WITH MULTIPLE SIGNS, POST TO BE 2" DIAMETER SCHEDULE 40 GALVANIZED STEEL, 6' LONG WITH CONICAL GALVANIZED STEEL CAP.
 6. POSTS TO BE PAINTED WITH 2 COATS OF PAINT, COLOR TO BE PANTONE 294. GALVANIZED POSTS MUST BE CLEANED, PROTRUSIONS REMOVED, AND SURFACE ROUGHED PRIOR TO PAINTING.



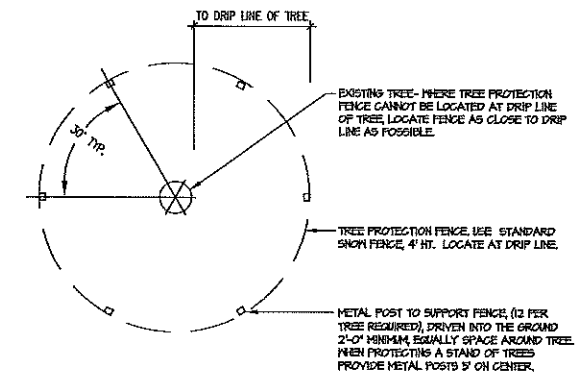
1 SIGNAGE
SCALE: N.T.S.

- NOTE:**
1. FENCE TO BE NORTHERN WHITE CEDAR. CONTRACTOR SHALL APPLY 2 COATS OF CASOT COMPANY'S EXTERIOR CLEAR WOOD SEALER / PRESERVATIVE.
 2. PROVIDE HEAVY DUTY HARDWARE FOR DOUBLE SWING GATE.
 3. PROVIDE FENCE AND GATE SHOP DRAWING FOR OWNER'S REPRESENTATIVE'S APPROVAL.



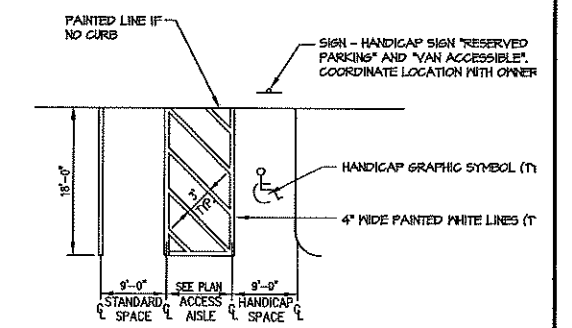
2 WOODEN SHADOWBOX FENCE MECHANICAL PAD ENCLOSURE
SCALE: N.T.S.

- NOTES:**
1. ALL DEAD AND DISEASED LIMBS SHALL BE PRUNED PRIOR TO BEGINNING CONSTRUCTION. ALL PRUNING AND TREE PROTECTION SHALL BE DONE BY A MAINE LICENSED ARBORIST SUCH AS BARKLETT TREE (885-9940).
 2. A DEEP SOIL INJECTION OF FERTILIZER AND MYCORRHIZA SHALL BE GIVEN UNDER THE TREE CANOPY TO PROMOTE ROOT GROWTH. INJECTION AMOUNTS AND TIMING SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT.
 3. NO EQUIPMENT SHALL BE PARKED OR STORED UNDER THE EXISTING TREE CANOPY.
 4. IN AREAS WHERE THE PROPOSED EXCAVATION WILL IMPACT THE EXISTING TREE ROOTS THE ROOTS SHALL BE FRAMED BY A LICENSED ARBORIST.



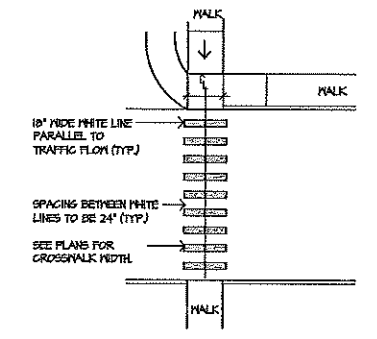
3 EXISTING TREE PROTECTION PLAN
SCALE: N.T.S.

- NOTES:**
1. SYMBOLS AND PARKING STALLS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA).
 2. ALL PAINT SHALL BE LEAD FREE, FAST DRYING, 100% ACRYLIC WATERBORNE TRAFFIC PAINT WITH SILICA SAND FOR SKID RESISTANCE, MEETING THE REQUIREMENTS OF TT-P-1832E TYPE I & II. PAINT SHALL BE APPLIED AS SPECIFIED BY THE MANUFACTURER.

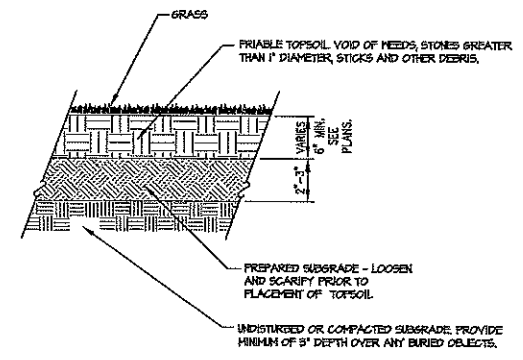


4 HANDICAP PARKING SPACE LAYOUT
SCALE: N.T.S.

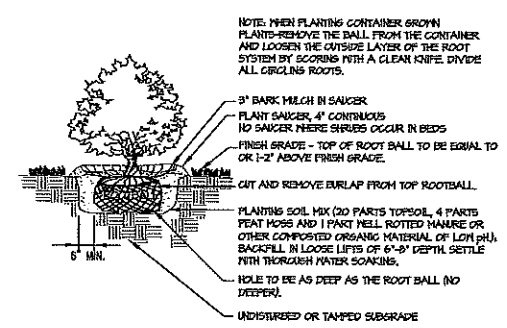
- NOTES:**
1. CROSSWALKS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT AND ALL LOCAL CODES AND MUTCD STANDARDS.
 2. ALL PAINT SHALL BE LEAD FREE, FAST DRYING, 100% ACRYLIC WATERBORNE TRAFFIC PAINT WITH SILICA SAND FOR SKID RESISTANCE, MEETING THE REQUIREMENTS OF TT-P-1832E TYPE I & II. PAINT SHALL BE APPLIED AS SPECIFIED BY THE MANUFACTURER.
 3. OWNER TO APPROVE THE LAYOUT OF CROSSWALKS PRIOR TO PAINTING.



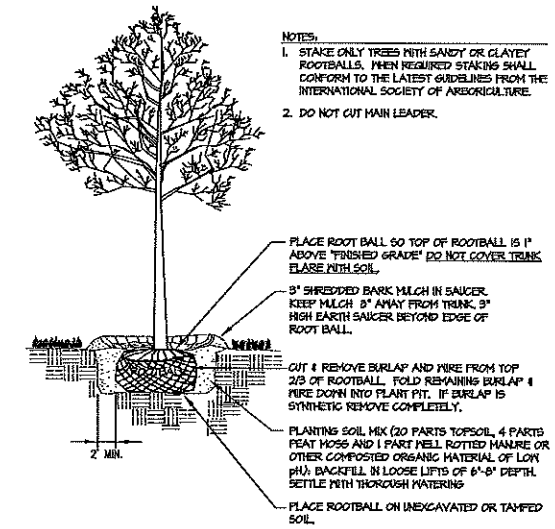
5 PAINTED CROSSWALK
SCALE: N.T.S.



6 LAWN INSTALLATION (SEED OR SOD)
SCALE: N.T.S.




7 SHRUB PLANTING DETAIL
SCALE: N.T.S.



8 DECIDUOUS TREE PLANTING
SCALE: N.T.S.

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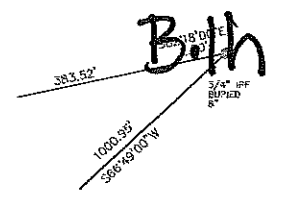
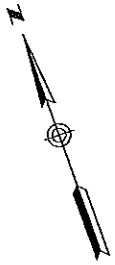
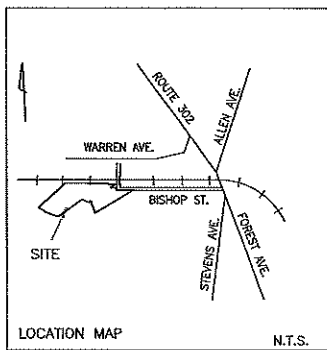
PORT CITY ARCHITECTURE
65 NEWBURY STREET, PORTLAND, MAINE 04101

PATIENT CARE CENTER
UNIVERSITY OF NEW ENGLAND
STEVENS AVENUE, PORTLAND, MAINE

SITE DETAILS

DESIGN: DEPT.	PROJECT:	REV.
DRAWN: DEPT.		
CHKD: TWS		
DATE: JAN. 2012	PROJ. NO.	
SCALE: AS NOTED	DWS. NO.	

C-302 B



IM

N/F PORTLAND TERMINAL CO. RAILROAD LOCATION

11/F PORTLAND TERMINAL CO. TM 291-A-2 TM 291-A-2

R-3

N/F CENTRAL MAINE POWER CO.

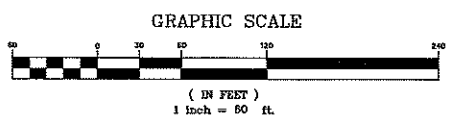
N/F WESTBROOK COLLEGE TM 291-A - LOT 5

- LEGEND:
- IRON PIPE OR ROD FOUND
 - ⊙ GAS VALVE
 - ⊙ WATER VALVE
 - ⊙ HYDRANT
 - ⊙ UTILITY POLE
 - ⊙ LIGHT POLE
 - ⊙ MANHOLE
 - ⊙ CATCH BASIN
 - ⊙ SIGN
 - FENCE
 - CURB
 - OU OVERHEAD UTILITIES
 - W WATER MAIN
 - G GAS MAIN
 - SS SANITARY SEWER
 - SD STORM DRAIN
 - 70 EXISTING 1' CONTOUR

TOTAL PARKING 270 SPACES

LINE TABLE

No.	Bearing	Distance
L1	S62°54'00" W	58.60'
L2	S07°38'40" W	28.00'
L3	S82°21'20" E	49.03'



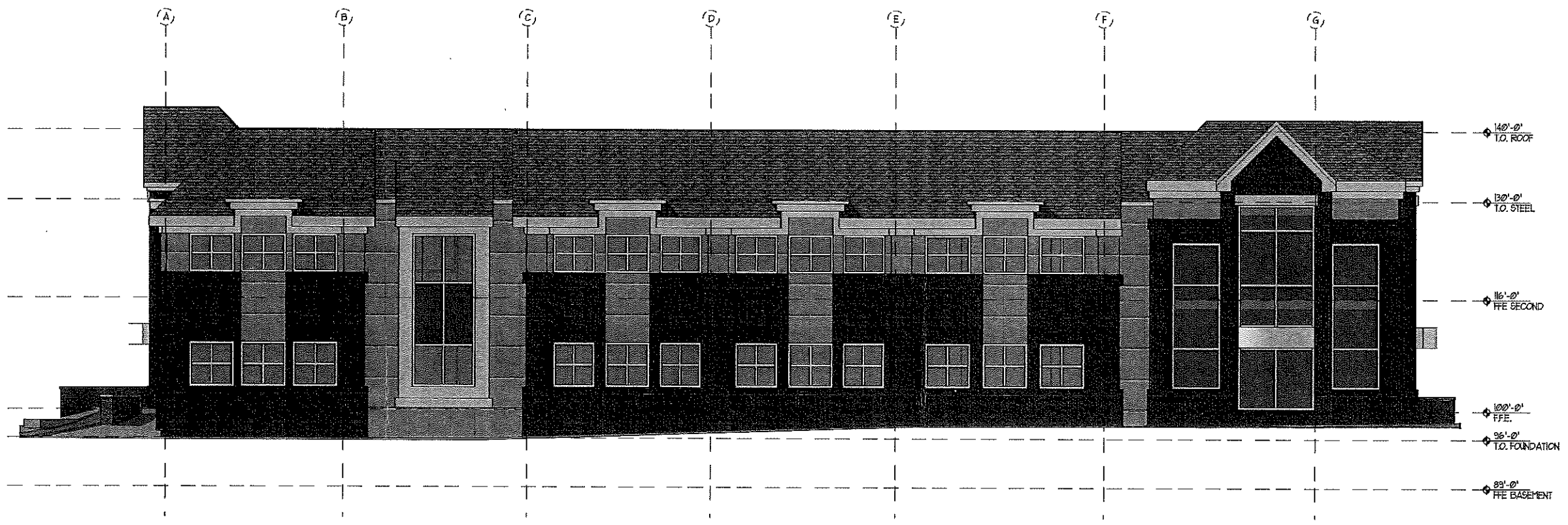
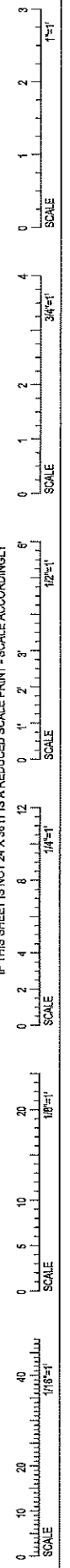
THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM SITE DESIGN ASSOCIATES, ANY ALTERATIONS, OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SITE DESIGN ASSOCIATES.

REV.	DATE	STATUS	BY	CHKD.	APPD.	REV.	DATE	STATUS	BY	CHKD.	APPD.
A	05/02/12	FOR CITY CONCEPTUAL REVIEW	DEPT	TWS	TWS						

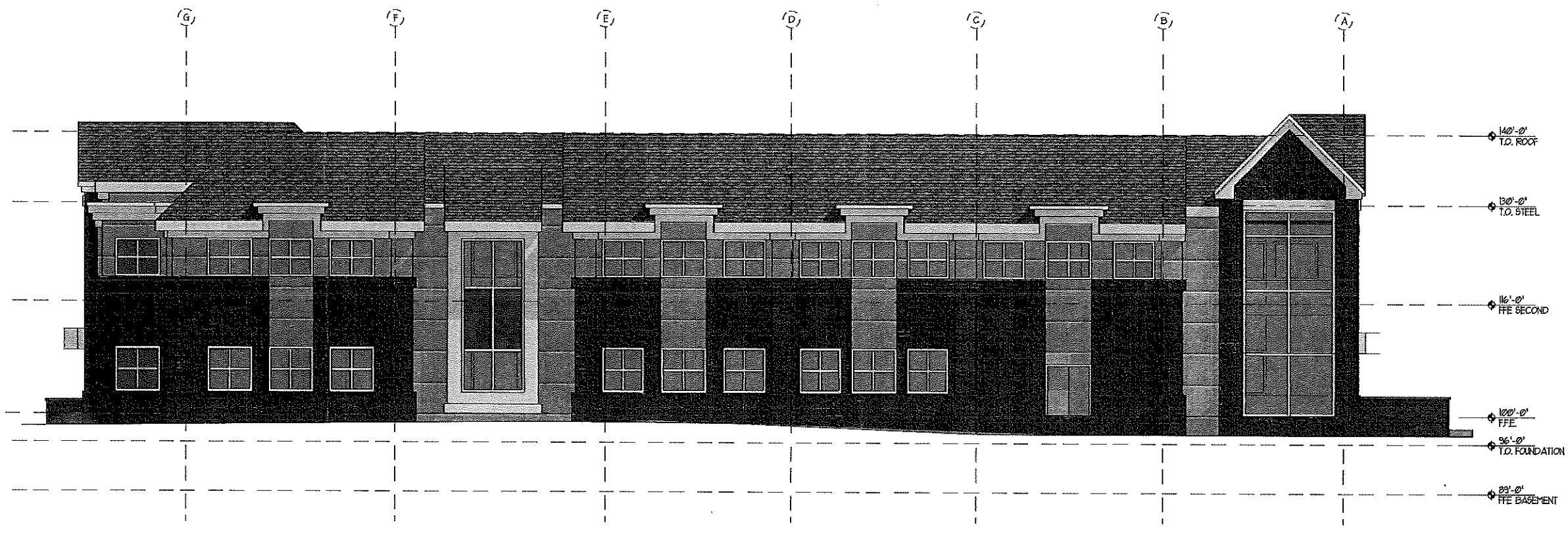
<p>Site Design Associates Consulting Engineering & Land Planning</p> <p>23 Whitney Way Topsham, Maine 04086 Tel: (207) 449-4275</p>		DESIGN: TWS		PROJECT: INTERMODAL FACILITY	
		DRAWN: DEPT.		102 BISHOP STREET, PORTLAND, MAINE	
CLIENT: UNIVERSITY OF NEW ENGLAND		CHKD: TWS		CONCEPTUAL PARKING PLAN	
11 HILLS BEACH ROAD, BIDDEFORD, MAINE 04005		DATE: APRIL 2012		PROJ. NO.	
		SCALE: 1"=60'		DWG. NO.	
				REV. A	

B.li

IF THIS SHEET IS NOT 24 X 36 IT IS A REDUCED SCALE PRINT - SCALE ACCORDINGLY



2 WEST ELEVATION
SCALE: 1/8" = 1'-0"



1 EAST ELEVATION
SCALE: 1/8" = 1'-0"



PORT CITY ARCHITECTURE

65 NEWBURY STREET
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207.761.9000
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- Kahler Slater
experience design
 - BECKER
EXTERIOR ARCHITECTS
 - VA
Vincent A. DiIorio
INC.
 - E.D.E. INC.
ARCHITECTURAL ENGINEERING
PLANNING INTERIORS
SCULPTURE
- Site Design Associates
23 Whitney Way
Topsham, Maine 04086
207-449-4275

REVISIONS

No.	Description	Date

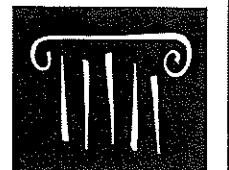
DESIGN DOCUMENT

UNIVERSITY OF NEW ENGLAND
PORTLAND, MAINE
PATIENT CARE CENTER
EXTERIOR SHELL
EAST/WEST ELEVATIONS

Project Number 12502
Date March 16, 2012
Drawn by DRW
Checked by CHK

A2.01

B.lj



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Vincent A. Diorio Inc.

**ED
HE** EDE INC.
Site Design Associates

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Topsham, Maine 04086
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REVISIONS

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DESIGN DOCUMENT

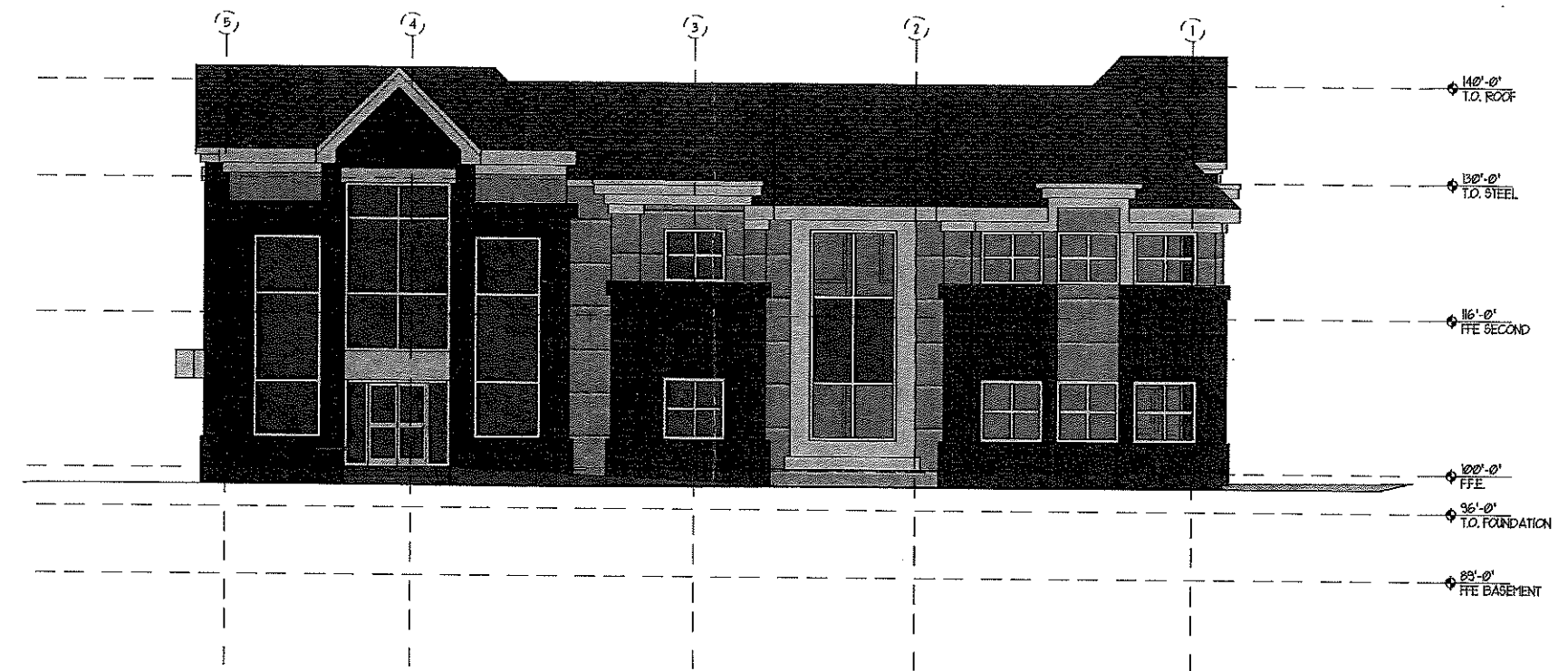
UNIVERSITY OF NEW ENGLAND
PORTLAND, MAINE
PATIENT CARE CENTER
EXTERIOR SHELL

NORTH/SOUTH ELEVATIONS

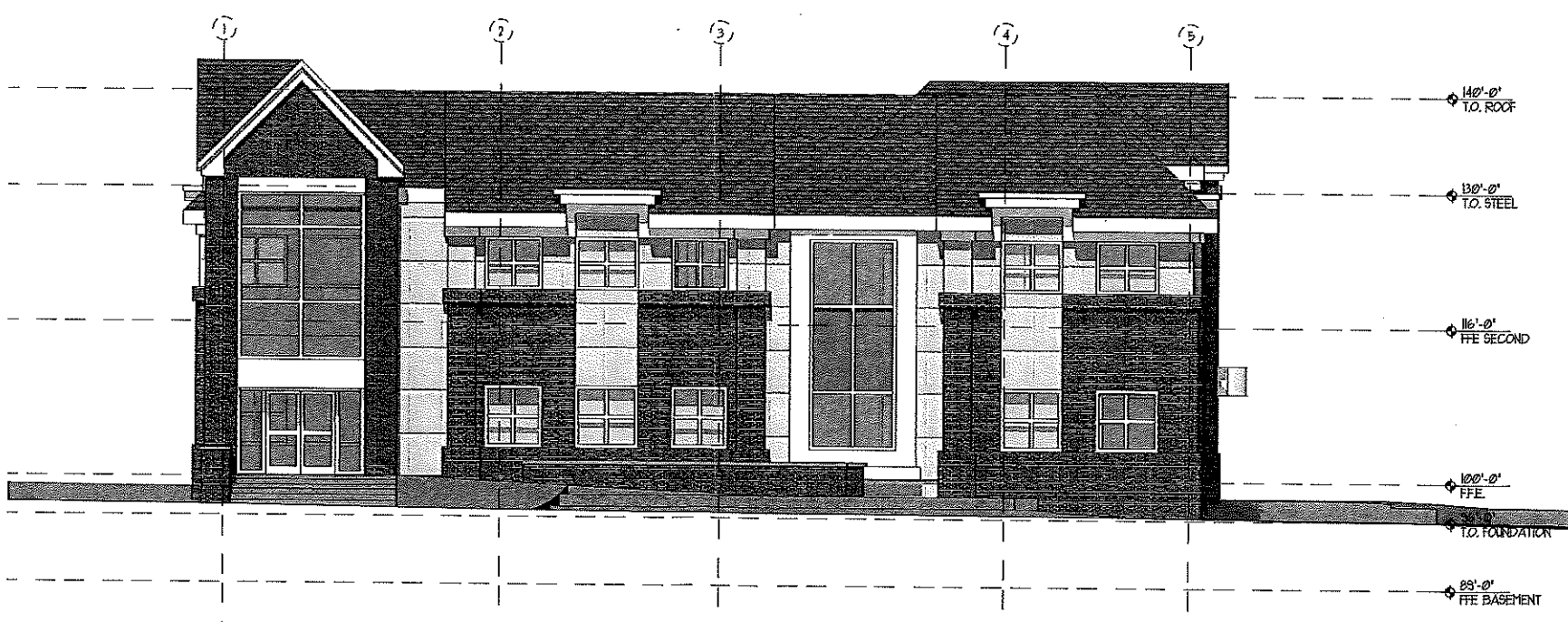
Project Number 12502
Date March 16, 2012
Drawn by DRW
Checked by CHK

A2.00

Scale



2 NORTH ELEVATION
SCALE: 1/8" = 1'-0"



1 SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

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