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CHAPTER 412  
TAX EXEMPT BORROWING AUTHORITY FOR THE  
UNIVERSITY OF MAINE SYSTEM

- Section
- 10950. Legislative findings of fact.
- 10951. Definitions.
- 10952. Powers.
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- 10954. Other financing transactions.
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Cross References

Taxation, see § 1 et seq. of title 86.

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United States Code Annotated

Internal Revenue Code, see 26 U.S.C.A. § 1 et seq.

§ 10950. Legislative findings of fact

The purpose of this chapter is to promote the welfare and prosperity of the people of the State and the continuation and improvement of their educational opportunities through enabling the University of Maine System to borrow money and enter into financing transactions in its own name, on behalf of the State, to provide money for the financing of acquisition, construction, reconstruction, improvement and equipping of facilities, structures and related systems in

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furtherance of the purposes of the University of Maine System, all to the public benefit and good, and the exercise of the powers, to the extent and manner provided in this chapter, is declared to be for a public purpose and to be the exercise of an essential governmental function.  
1987, c. 785, § 14, eff. April 19, 1988.

Library References

American Digest System

Exemption from tax liability of persons and property; schools, colleges and universities, and property used for educational purposes, see Taxation ¶242 et seq.

Encyclopedias

Exemption from tax liability of persons and property; educational institutions, see C.J.S. Taxation § 283 et seq.

WESTLAW Research

Taxation cases: 871k[add key number].

§ 10951. Definitions

As used in this chapter, unless the context otherwise indicates, the following terms have the following meanings.

1. Assured revenues. "Assured revenues," as used in this chapter and in chapter 411,<sup>1</sup> means revenues to be received from grants, subsidies, contracts, leases or other, agreements made by or with the Federal Government, the State or any political subdivision, agency or instrumentality of the Federal Government or the State, or others; or revenues to be received from existing projects, from projects under construction or from projects for which the university has entered into a binding commitment for the acquisition, construction or accomplishment of the project, anticipated by the trustees to produce annual revenues in an amount not less than the anticipated annual cost of operation, maintenance and repair of such project, including aggregate annual debt service payments on any financing for the project, during the term of any financing effected under this chapter for the project, as determined by the trustees.

2. Cost. "Cost" as applied to a project or any portion of the project, includes, but is not limited to: The purchase price or acquisition cost of any such project; the cost of construction, building, alteration, enlargement, reconstruction, renovation, improvement, equipping and remodeling; the cost of all labor, materials, building systems, machinery and equipment; the cost of all lands, structures, real or personal property, rights, easements and franchises acquired; the cost of all utility extensions, access roads, site development, financing charges, premium for insurance, interest prior to and during construction and for 6 months thereafter; the cost of working capital related to the project; the cost of plans and specifications, surveys and estimates of cost and of revenues; the cost of engineering, feasibility studies, legal and other professional services; the cost of reserves for payment of future debt service related to the financing transaction and for improvements; the cost of all other expenses necessary or incident to determining the feasibility or practicability of such construction; and administra-

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tive and operating expenses and such other expenses as may be necessary or incident to the financing authorized.

3. Evidences of indebtedness. "Evidences of indebtedness" means any notes, long-term or short-term, or other evidences of indebtedness issued pursuant to this chapter.

4. Financing documents. "Financing documents" means any evidences of indebtedness, loan agreements, credit agreements, financing leases, lease-purchase agreements, trust agreements, indentures, resolutions, mortgages, security agreements, pledge agreements or other contracts, agreements or documents executed and delivered by the university in connection with a financing transaction under this chapter.

5. Financing transaction. "Financing transaction" means the borrowing of money by the university on behalf of the State pursuant to this chapter.

6. Project. "Project" means any structure designed for use as a dormitory or other housing facility, dining facility, student union, academic building, administrative facility, library, classroom building, research facility, faculty facility, office facility, athletic facility, health care facility, laboratory, maintenance, storage or utility facility or other building or structure essential, necessary or useful for instruction in a program of education provided by the university; or any multipurpose structure designed to combine 2 or more of the functions performed by the types of structures enumerated in this subsection, including, without limitation, improvements, reconstruction, additions and equipment acquired in connection with the project or in connection with operation of any such currently existing facilities. "Project" includes all real and personal property, lands, improvements, driveways, roads, approaches, pedestrian access roads, parking lots, parking facilities, rights-of-way, utilities, easements and other interests in land, machinery and equipment, and all appurtenances and facilities either on, above or under the ground that are used or usable in connection with any of the structures mentioned in this subsection. "Project" also includes landscaping, site preparation, furniture, machinery, equipment and other similar items necessary or convenient for the operation of a particular facility or structure in the manner for which its use is intended, but does not include such items as books, fuel, supplies or other items that are customarily considered as a current operating charge.

7. State. "State" means the State of Maine.

8. University. "University" means the body politic and corporate, established by Private and Special Law 1865, chapter 532, under the name of the "Trustees of the State College of Agriculture and Mechanic Arts," its name having been changed to the "University of Maine" by Private and Special Law 1897, chapter 551, and which is an instrumentality and agency of the State for the purpose for which it was established and for which it has been managed and maintained under Private and Special Law 1865, chapter 532, and supplementary legislation relating thereto, including section 10908; Private and Special Law

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1967, chapter 229; Private and Special Law 1969, chapter 238; and Public Law  
1985, chapter 779.

1987, c. 785, § 14, eff. April 19, 1988; R.R.1991, c. 2, § 66.

<sup>1</sup> Section 10901 et seq. of this title.

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**Historical and Statutory Notes**

**Codification**

Revisor's Report 1991, c. 2, § 66, in subsec. 6,  
substituted reference to "this subsection" for  
reference to "this paragraph" and made gram-  
matical changes.

**Library References**

**American Digest System**

Exemption from tax liability of persons and property, schools, colleges and universities, and property  
used for educational purposes, see Taxation ¶242 et seq.

**Encyclopedias**

Exemption from tax liability of persons and property, educational institutions, see C.J.S. Taxation  
§ 233 et seq.

**WESTLAW Research**

Taxation cases: §71k[add key number].

**Words and Phrases**

Words and Phrases (Perm.Ed.)

**§ 10952. Powers**

In order to carry out the purposes of this chapter, the university shall have the  
following powers, which shall be in addition to any other powers that the  
university may have pursuant to laws of the State:

1. Body politic and corporate. To have perpetual succession as a body  
politic and corporate and an instrumentality and agency of the State;
2. Sue and be sued. To sue and be sued in its own name;
3. Official seal. To adopt and have an official seal and alter it at pleasure;
4. Project ownership. To acquire, construct, reconstruct, improve, equip,  
own, operate and maintain any project or projects, or any combination of project;
5. Acquisition of property. To acquire by purchase, contract, lease, long-  
term lease or gift, and hold or dispose of, real or personal property or rights or  
interests in any such property;
6. Grants; subsidies; loans. To accept grants, subsidies or loans of money  
from the Federal Government or a federal agency or instrumentality, or others,  
upon such terms and conditions as may be imposed, and to pledge the proceeds  
of grants, subsidies or loans of money received or to be received from the  
Federal Government or any federal agency or instrumentality, or others, pursu-  
ant to agreements entered into between the university and the Federal Govern-  
ment or any federal agency or instrumentality, or others;
7. Borrow money. To borrow money pursuant to this chapter and issue  
evidences of indebtedness to finance the acquisition, construction, reconstruction,

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improvement or equipping of any one project, or more than one, or any combination of projects, or to refund evidences of indebtedness hereafter issued or to refund general obligation debt of the State previously issued to finance any project or projects, or to refund any such refunding evidences of indebtedness or for any one, or more than one, or all of those purposes, or any combination of those purposes, and to provide for the security and payment of those evidences of indebtedness and for the rights of the holders of them, except that any borrowing pursuant to this chapter, exclusive of borrowing to refund evidences of indebtedness, to refund general obligation debt of the State, or to fund issuance costs or necessary reserves, may not exceed in the aggregate principal amount outstanding at any time \$27,000,000, and except that no borrowing may be effected pursuant to this chapter unless the amount of the borrowing and the project or projects are submitted to the Legislative Office of Fiscal and Program Review for review by the Joint Standing Committee on Appropriations and Financial Affairs at least 60 days before closing on such borrowing for the project or projects is to be initiated;

8. Execute contracts. To make, enter into, execute, deliver and amend any and all contracts, agreements, leases, instruments and documents and perform all acts and do all things necessary or convenient to acquire, construct, reconstruct, improve, equip, finance, maintain and operate projects and to carry out the powers granted to this chapter, or reasonably implied from those powers; and

9. Maintain treasury. To retain in its treasury:

A. All money received from the sale of all evidences of indebtedness issued under this chapter;

B. All fees, tuitions, rentals and other charges from students, faculty, staff members and others using or being served by, or having the right to use or the right to be served by, or to operate, any project;

C. All fees for student activities, student services and all other fees, tuitions and charges collected from students matriculated, registered or otherwise enrolled at and attending the university, pledged under the terms of any resolution authorizing financing transactions pursuant to this chapter; and

D. All rentals from any facility or building leased to the Federal Government or any other 3rd party;

Any and all powers granted to the university under this chapter may be exercised by majority vote of the trustees and may be delegated to any officer, official or trustee of the university by majority vote of the trustees.

1987, c. 785, § 14, eff. April 19, 1988; 1989, c. 578, § 1, eff. July 11, 1989; 1991, c. 9, § E-13, eff. March 14, 1991.

Historical and Statutory Notes

Amendments

1989 Amendment. Laws 1989, c. 578, § 1, in subsec. 7, inserted the power to refund the general obligation debt of the State previously is-

sued, and added the proviso that any borrowing under this chapter shall not exceed \$18,000,000 in the aggregate and the amount of borrowing and the project must be submitted for review to

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Library References

American Digest System

Exemption from tax liability of persons and property; schools, colleges and universities, and property used for educational purposes, see Taxation ¶242 et seq.

Encyclopedias

Exemption from tax liability of persons and property; educational institutions, see C.J.S. Taxation § 283 et seq.

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Taxation cases: 371k[add key number].

§ 10954. Other financing transactions

In furtherance of the provisions of section 10906, subsection 1, paragraph C, subparagraph (1), the university may, when directed by vote of the trustees and duly and properly authorized by the Governor, borrow money and enter into financing transactions in the name of the university, on behalf of the State, and issue evidences of indebtedness in connection with such financing transactions:

1. Finance. To finance the cost of any one project, or more than one, or any combination of projects;
2. Refund evidences of indebtedness. To refund evidences of indebtedness issued and to finance the cost of any project or projects as provided in this chapter or to refund general obligation debt of the State issued previously to finance any project or projects; or
3. Refund refunding borrowings. To refund any such refunding borrowings.

All evidences of indebtedness issued in connection with financing transactions entered into pursuant to this section shall be authorized by majority vote of the trustees and approved by the Governor upon submission by the trustees of the vote so passed.

1987, c. 735, § 14, eff. April 19, 1988; 1989, c. 578, § 3, eff. July 11, 1989.

Historical and Statutory Notes

Amendments

1989 Amendment. Laws 1989, c. 578, § 3, in subsec. 2, added provision relating to the refund of general obligation debt of the State.

Library References

American Digest System

Exemption from tax liability of persons and property; schools, colleges and universities, and property used for educational purposes, see Taxation ¶242 et seq.

Encyclopedias

Exemption from tax liability of persons and property; educational institutions, see C.J.S. Taxation § 283 et seq.

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<sup>1</sup> Section 8-101 et seq. o



§ 10955. Terms of financing transactions

1. Form; terms; manner of sale. All evidences of indebtedness issued in connection with the financing transactions pursuant to this chapter may be in serial form; may bear such date or dates; may mature at such time or times, and in such amount or amounts; may bear interest at such rate or rates, including variable or adjustable; may be payable in such form and at such time or times and at such place or places; and may include such redemption and conversion privileges as those votes may provide. All evidences of indebtedness shall be issued and sold under such terms and conditions as the trustees determine. The votes shall provide that the treasurer shall manually sign evidences of indebtedness and other related financing documents and the votes may provide for counter-signature of those evidences of indebtedness and related documents by another officer, either manually or in facsimile form. All such evidences of indebtedness shall be deemed to be negotiable instruments under the Uniform Commercial Code, Title 11, Article 8.<sup>1</sup>

2. Presumption of lawful authorization. After issuance, all evidences of indebtedness of the university shall be conclusively presumed to be fully and duly authorized and issued under the laws of the State, and any person or governmental unit shall be stopped from questioning their authorization, sale, issuance, execution or delivery by the university.

3. Voting power. The power to fix the date of sale or issuance of any evidences of indebtedness, receive bids or proposals, award and sell any evidences of indebtedness to set the terms and provisions of any evidences of indebtedness and take all other action necessary to borrow money under this chapter and sell and deliver any evidences of indebtedness in connection with this chapter may be delegated to any officer, official or trustee of the university by a majority vote of the trustees.

4. No state debt. Money borrowed pursuant to this chapter and evidences of indebtedness issued in connection with this chapter shall not constitute any debt or liability of the State or of any municipality or any political subdivision of the State, but shall be payable solely from the revenues of the university or any project for which they are issued, and all such evidences of indebtedness shall contain on their face a statement to that effect. The borrowing of money pursuant to this chapter and the issuance of evidence of indebtedness under this chapter shall not directly or indirectly or contingently obligate the State or any municipality or political subdivision to levy or to pledge any form of taxation or to make any appropriation for their payment and such evidences of indebtedness shall not constitute an indebtedness within the meaning of any constitutional or statutory debt limitation or restriction.

1987, c. 785, § 14, eff. April 19, 1988; 1989, c. 502, § A, 57, eff. June 30, 1989.

<sup>1</sup> Section 8-101 et seq. of title 11.

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Historical and Statutory Notes

Amendments

1989 Amendment. Laws 1989, c. 502, § A, 57, in subsec. 1, substituted "Uniform Commercial Code" for "Uniform Commerical Code".

Library References

American Digest System

Exemption from tax liability of persons and property; schools, colleges and universities, and property used for educational purposes, see Taxation ¶242 et seq.

Encyclopedias

Exemption from tax liability of persons and property; educational institutions, see C.J.S. Taxation § 283 et seq.

WESTLAW Research

Taxation cases: 371k[add key number].

§ 10956. Security for indebtedness; trust agreement

To secure the payment of any indebtedness established or issued pursuant to this chapter, the university may mortgage any project or any part of the project and create a lien upon any or all of the revenues of any project or projects or upon any or all of the real or personal property constituting a part of any project or projects or upon any interests the university may hold in connection with a project or projects. Any trust agreement, financing document or resolution may contain such other provisions as the university determines reasonable and proper for the security of the holders of evidences of indebtedness.

A trust agreement or financing document containing a mortgage in respect of a project or projects or any part of a project, may authorize the trustee or mortgagee, as the case may be, in the event of a default as defined in respect to the evidences of indebtedness issued to provide for the costs of such project or projects or any part of a project, to take possession of all or any part of the mortgaged property constituting the project or projects or any part of a project, to hold, operate and manage the property and, with or without such taking of possession, to sell or from time to time to lease the property. Remedies provided under this chapter are not exclusive and these remedies shall be in addition to every other remedy existing at law, in equity. Upon satisfaction at any time of the obligations secured by the mortgage in respect of a project, projects or any part of a project, which shall be deemed to include all applicable fees and expenses, any surplus proceeds from such operation, sale or lease of such project or projects or any part of a project shall be paid to the mortgagor of a project, projects or any part of a project or to those claiming under the mortgagor, and subject to any sale or lease under this paragraph, the mortgaged property in respect of the project shall revert or be returned to the mortgagor or to those claiming under the mortgagor.

In the discretion of the university, any evidences of indebtedness issued under this chapter may be secured by a trust agreement by and between the university and a corporate trustee, which may be any trust company or bank having the

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powers of a trust company within or without the State. Such a trust agreement or the resolution providing for the issuance of such evidences of indebtedness may pledge or assign all or any portion of the revenues of any project or projects or any other assets of a project or projects and may contain such provisions for protecting and enforcing the rights and remedies of the holders of evidences of indebtedness as may be reasonable and proper and not in violation of law. The provisions may include covenants setting forth the duties of the university in relation to the acquisition of property and the construction, reconstruction, renewal, replacement and insurance of any project in connection with which such evidences of indebtedness shall have been authorized, the fees, tuition, charges, or rents to be charged or other payments to be made for the use of the property or payment for the property, and the custody, safeguarding and application of all money. Any such trust agreement may set forth the rights and remedies of the holders of evidences of indebtedness and of the trustee, and may restrict the individual right of action by holders of evidences of indebtedness.

All expenses incurred in carrying out the trust agreement, financing document or resolution may be treated as a part of the cost of the operation of a project. All pledges of revenues under this chapter shall be valid and binding from the time when the pledge is made. All such revenues so pledged and received by the university shall immediately be subject to the lien of the pledges without any physical delivery of them or further action under the Uniform Commercial Code, Title 11, or otherwise. The lien of those pledges shall be valid and binding as against all parties having claims of any kind in tort, contract or otherwise against the university irrespective of whether the parties have notice of the liens, and the liens shall automatically, without further action, be perfected and have the same status as a security interest perfected under the Uniform Commercial Code, Title 11, Article 9.  
1987, c. 735, § 14, eff. April 19, 1988; 1989, c. 502, § A, 58 eff. June 30, 1989.

Historical and Statutory Notes

Amendments

1989 Amendment. Laws 1989, c. 502, § A, 58, in the fourth par., substituted "Uniform Commercial Code" for "Uniform Commerical Code".

Library References

American Digest System

Exemption from tax liability of persons and property; schools, colleges and universities, and property used for educational purposes, see Taxation c. 242 et seq.

Encyclopedias

Exemption from tax liability of persons and property; educational institutions, see C.J.S. Taxation § 233 et seq.

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Taxation cases: 371k[add key number].

§ 10957. Trust funds

All money received pursuant to the authority of this chapter shall be deemed to be trust funds, to be held and applied solely as provided in this chapter and in



700 Louisiana  
Houston, TX 77002

Telephone 713 319 2000  
Fax 713 319 2041

January 4, 2000

Mr. B. Russell Smith  
Chief Financial Officer  
The University of Maine System  
197 Maine Ave  
Bangor, Maine 04401

Dear Mr. Smith:

At your request, KPMG has reviewed planning documents related to a new, bond financed, student housing project at the University of Southern Maine (USM or the University). KPMG LLP's review for this project was limited to examination of the following documents provided by the University:

- A document entitled "University of Southern Maine, Housing Expansion Report and Recommendations" prepared by Michael J. Coakely, Consultant, and dated December 1998.
- A summary of the project entitled "University of Maine System, University Bond Request - January 2000 for the University of Southern Maine, New Residence Hall Facility."

Based on the information provided to KPMG by the University and subject to the limitations detailed in the attached findings, we believe that the proposed residence hall project is feasible and represents a sound business decision as part of the USM overall housing plan. Our detailed findings supporting this conclusion are attached.

We are pleased to provide this evaluation for the University of Maine and appreciate the opportunity to be of service. If you have any questions regarding this evaluation, or if we can be of additional assistance, please call me at (504) 866-5383 or Roger Hardy at (310) 822-1028.

Very truly yours,

KPMG LLP

Robert Stickney  
Senior Consultant

Cc: David Hemingson, KPMG Houston  
John Moriarty, KPMG Hartford  
Roger Hardy, KPMG Los Angeles

Attachment: University of Southern Maine Student Housing Proposal Evaluation

KPMG LLP KPMG LLP is a U.S. limited liability partnership, a member of KPMG International, a Swiss association.



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January 4, 2000

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Very truly yours,

**KPMG LLP**

Robert Stickney  
Senior Consultant

Cc: David Hemingson, KPMG Houston  
John Moriarty, KPMG Hartford  
Roger Hardy, KPMG Los Angeles

Attachment: University of Southern Maine Student Housing Proposal Evaluation



KPMG LLP, KPMG LLP, a U.S. limited liability partnership, is a member of KPMG International, a Swiss association.

**UNIVERSITY OF MAINE SYSTEM**  
**UM STUDENT AUXILIARY SERVICES**  
**PRO FORMA OAK HALL FINANCING PLAN**

BUILDING COST	\$2,234,457.00
BOND REQUEST	\$1,862,048.00
RESERVE CONTRIBUTION	\$372,409.00
ANNUAL DEBT SERVICE AT 20 YEARS	<b>\$162,342.00</b>

REVENUE

82 STUDENTS AT \$3,927 PER YEAR	\$322,014.00
VENDING (Laundry)	\$1,000.00

<b>TOTAL REVENUE</b>	<b>\$323,014.00</b>
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EXPENSE

CUSTODIAL - 1 @ \$17,744*1.33	\$23,599.52
2 RA'S	\$10,936.68
GOODS AND SERVICES	\$5,555.56
UTILITIES (Phone, Heat, Elec., Water, Sewer)	\$50,000.00
MAINTENANCE	\$30,000.00
EQUIPMENT	\$7,000.00
INSURANCE	\$1,200.00
SERVICES (PUBLIC SAFETY, RUBBISH)	\$7,000.00
ADMINISTRATIVE OVERHEAD	\$25,380.24
DEBT SERVICE	\$162,342.00

<b>TOTAL EXPENSE</b>	<b>\$323,014.00</b>
<b>NET</b>	<b>\$0.00</b>

## UNIVERSITY OF MAINE SYSTEM

### UMF Debt Service For Auxiliary Enterprise Revenue Bond Issue

The following is an explanation of how UMF will fund the annual debt service on the \$1.3 million auxiliary enterprise revenue bond:

This is an upgrade of an existing UMF residence hall facility for the purpose of remaining competitive. The Scott Hall North (\$800,000) and South (\$500,000) projects have been identified as UMF's highest auxiliary enterprise priorities.

The estimated \$114,889 annual debt service cost will be financed by a combination of two primary sources. First, savings from recently retired (FY99) debt service and second, revenue from increased occupancy based on current high demand for on-campus housing.

#### Funding Detail

Savings from recently (FY99) retired debt service	\$ 91,049
Increased occupancy (10 residents)	<u>23,840</u>
TOTAL	\$114,889



# UNIVERSITY OF MAINE SYSTEM

OFFICE OF FINANCE AND TREASURER 107 Maine Avenue Bangor, Maine 04401-4380 207-973-3350 (Finance/Treasurer)  
TDD 207-973-3300/FAX 207-947-7556 -3310 (Accounting)

December 19, 2000

TO: Sam Andrews  
FROM: B. Russell Smith

In support of the planned Biosciences expansion/renovation of the Science Building on the Portland campus of the University of Southern Maine, funds have been provided as follows:

1. \$2.4 million from State of Maine R&D Bonds passed in 1998; and
2. \$5.0 million from University Revenue Bonds sold on August 15, 2000 with the debt service provided by a line item State appropriation.

BRS/jr

cc: President Pattenaude  
David Wilson

UNIVERSITY OF MAINE AT FARMINGTON      UNIVERSITY OF MAINE AT FORT KENT      UNIVERSITY OF MAINE AT MAGUIAS      UNIVERSITY OF MAINE AT PRESQUE ISLE

UNIVERSITY OF MAINE AT AUGUSTA



**SECTION 4**

**TECHNICAL ABILITY**

## SECTION 4

### TECHNICAL ABILITY

#### **4.0 Overview**

The design and permitting associated with the existing USM Portland Campus and proposed construction of the Bioscience Institute is being completed under the direction of the University of Maine System (UMS).

The following firms are acting as consultants to UMS for the project:

#### **OVERALL EXISTING CONDITIONS DATA, MASTER PLAN AND PERMITTING WORK**

<b>Firm</b>	<b>Address</b>	<b>Services</b>
DeLuca-Hoffman Associates, Inc.	778 Main Street, Suite 8 South Portland, ME 04106	Permit Preparation
Orcutt Associates	81 Bridge Street Yarmouth, ME 04096	Land Planning and Architect
Aerial Survey & Photo, Inc.	P.O. Box 659 546 Airport Road Norridgewock, ME 04957	Aerial Topographic and Existing Conditions Plan

#### **DESIGN OF PROPOSED BIOSCIENCE BUILDING INSTITUTE**

<b>Firm</b>	<b>Address</b>	<b>Services</b>
Symmes, Maini & McKee Associates	1000 Massachusetts Avenue Cambridge, MA 02138	Project Architect and Engineer
Owen Haskell, Inc.	16 Casco Street Portland, ME 04101	Existing Conditions Survey
Haley & Aldrich, Inc.	500 Southborough Drive Suite 10 South Portland, ME 04106	Geotechnical Engineer

USM has a full contingent of staff to manage and operate the facilities after construction.

#### **4.1 Experience of Project Team**

The team of consultants retained by the University has expertise and experience in the design of large facilities which require a Site Location of Development Permit. Wherein DeLuca-Hoffman Associates, Inc. has prepared the permit application, a list of relevant experience on projects which require a Site Location of Development Permit is attached.

Resumes of key personnel for the Master Plan/Permitting Team and Design Team for the Bioscience Institute are also attached. The select subconsultants retained for the project also have experience in permitting in the State of Maine. Resumes of individuals working for the subconsultants can be provided upon request.

#### 4.2 Ability of the Applicant

After construction, the new facilities will be operated and maintained by the university of Maine System. The University of Southern Maine has the ability and personnel to operate and maintain the new facilities as evident in the ongoing operation and maintenance of the current Portland campus.

#### 4.3 Attachments

Permit experience of DeLuca-Hoffman Associates, Inc. in Maine.

Resumes of Key Personnel for the Master Plan and Permitting Team from Symmes, Maini & McKee Associates.

- Cynthia Plank-Orcutt - Orcutt Associates
- John Russell Orcutt - Orcutt Associates
- Joseph A. Laverriere, P.E. – Sr. Engineer for DeLuca-Hoffman Associates, Inc.
- Randall Dunton, P.E. – Traffic Engineer for DeLuca-Hoffman Associates, Inc.

Resumes of Key Personnel for the Design Team from Symmes, Maini & McKee Associates.:

## DELUCA-HOFFMAN ASSOCIATES, INC.

### PERMIT EXPERIENCE IN MAINE

DeLuca-Hoffman Associates, Inc. has prepared numerous Site Location of Development Permits for projects throughout the State of Maine. In addition to the permit experience listed below DeLuca-Hoffman Associates, Inc. was retained by the Maine Department of Environmental Project to prepare a Master Site Location of Development Permit for 200 acres of multi-use development in Topsham, Maine in the area of the Topsham Fair Mall and near the new Bath Brunswick bypass.

- ***Shopping Center Projects with Wal\*Mart Anchor Stores.***

The following shopping centers were permitted for Wal\*Mart Stores in the State of Maine:

Location	Permit Number	Development Size	Description
Rockland	L-17640-26-A-N	93,000 ± s.f	Wal*Mart Store and outparcel
Houlton	L-18000-26-A-N	150,000 ± s.f.	Wal*Mart Store, Grocery Store, and outparcel
Waterville	L-18148-26-A-N	93,000 ± s.f	Wal*Mart Store
Skowhegan	L-18409-23-A-N	190,000 ± s.f	Retail strip center with Wal*Mart and Grocery Store
Calais	L-18408-23-A-N	170,000 ± s.f	Retail strip center with Wal*Mart and Grocery Store
Farmington	L-17771-26-A-N	76,000 ± s.f	Wal*Mart Store and outparcel
Lincoln	L-17849-26-A-N	50,000 ± s.f	Wal*Mart Store
Ellsworth	L-18070-26-A-N	93,000 ± s.f	Wal*Mart Store
Biddeford	L-18464-26-A-N	116,000 ± s.f	Wal*Mart Store with outparcel
Oxford	L-18508-26-A-N	102,000 ± s.f	Wal*Mart Store with outparcel
Windham	L-18560-26-A-N	102,000 ± s.f	Wal*Mart Store with outparcel

- ***Shopping Center Properties, South Portland, Maine***

The project includes an approximate 23,500 in-line building expansion and addition of a freestanding restaurant to an existing 96,000 s.f. retail facility. The project demanded unique design for relocating an existing stream and providing stormwater management facilities. Mary Beth Richardson was the staff analyst for the Site Location and NRPA permitting aspects of the project.

- **Shaw's Merrymeeting Plaza, Brunswick, Maine**

The project includes 150,000 square feet of retail space with 735 parking spaces, utility improvements, a major upgrade of Route 24, and railroad crossing, and best management practices for the storm water discharge, Linda Kokemuller was the Staff Analyst.

- **Biddeford Primary School, Biddeford, Maine**

The project consisted of a 750 student, 75,000 square foot building, parking, roadways, a softball and other playfields, utilities, and related improvements. An Army Corps of Engineers and a Natural Resource Permit for three crossings of an intermittent stream were required in addition to the Site Location of Development Permit. Tim Wright was the MeDEP Staff Analyst.

- **Winslow Common On The Lake, Falmouth, Maine**

This project consisted of the 29-lot subdivision of 64 acres of property within the Highland Lake Watershed. A road system 4,400 feet in length, water mains, phosphorus control ponds, a dry detention pond constructed across a perennial stream, jogging trails, and a lakefront park was included in this project. Stacey Beyer Ramsay was the Staff Analyst for the MeDEP.

- **Crescent Heights Subdivision, Casco, Maine**

This project consisted of the residential subdivision of 27 acres into 17 lots under the affordable housing provision of the Town of Casco. The project included 2,600 feet of roadway and two phosphorous/detention/fire ponds. A Natural Resource Permit was also issued for this project. Linda Kokemuller was the Staff Analyst for the MeDEP.

- **Madison Heights Subdivision, Naples, Maine**

This project included the residential subdivision of an 180-acre site into eight neighborhood clusters for a total of 87 lots. The project included 12,600 feet of roadway, two dry detention basins, and three phosphorous/fire/retention basins.

Special consideration of the thermal impacts to a trout stream which bisected the lower portion of the property was included in the design. Dave Studer was the Staff Analyst for the MeDEP.

- **Ambleside Subdivision, Cumberland, Maine**

This project consisted of the residential subdivision of 48 acres of land into 13 home sites, 2100 feet of roadway, a major detention facility, wet pond, and road crossing across a stream valley. A Natural Resource permit was obtained in addition to the Site Location permit. Tim Wright was the Staff Analyst for the MeDEP.

- **Stillwater Center At The Mall, Bangor, Maine**

This project is a large 200,000 square foot retail shopping center in Bangor, Maine. The project provides for over 1200 parking spaces and major utility improvements. The permit application included a Corps of Engineers and a Natural Resource Permit. Andrew Cobb was the Staff Analyst for the MeDEP.

- **Burnham Woods, Phase II Subdivision, Naples, Maine**

This project is in the Brandy Pond Watershed which is tributary to Sebago Lake. Control of phosphorus and water quality protection levels are the highest in the State. The project includes 15 lots, roadways, and phosphorus controls. Linda Kokemuller was the MeDEP Staff Analyst.

- **Long Lake Estates, Naples, Maine**

This project is the complete restructuring of a 72-unit condominium project to a high-income exclusive residential subdivision in Naples, Maine with frontage of Long Lake. The current owners purchased the failed project at a bankruptcy auction and immediately implemented the following steps:

- Demolition of the 18 condominiums erected by the time of bankruptcy.
- Implementation of an emergency restoration program to prevent continued environmental damage.

The project includes a Natural Resource Permit and a Corps of Engineers Permit to "post permit" approximately 3 acres of wetlands disturbed by the prior owners. John James was the MeDEP Staff Analyst.

- **Freeport Elementary School And Recreation Fields, Freeport, Maine**

This project includes the construction of a new elementary school and a Town recreation complex on about 27 acres of land in Freeport, Maine. The project includes filling of about 2 acres of wetlands and stream alteration requiring an Army Corps of Engineers and Natural Resource Permit Applications. Andrea Lapointe was the MeDEP Staff Analyst.

- **P & K Gravel Pit, Naples, Maine**

This project is a major gravel pit in Naples, Maine with borrow extraction estimated at over 750,000 cubic yards of material.

- **Quaker Ridge Estates, Durham, Maine**

This project is an expansion of a 7-lot subdivision to 21 lots which required post permitting of the original seven lots. The project included fills in Class II and Class III with a Natural Resource Protection Act Permit. Matt Hight was the Project Analyst for the DEP Office in Portland, Maine.

- **Municipal Facilities, Buxton, Maine**

This project includes a new municipal building, salt storage shed, and recreation fields on a 200-acre parcel of land owned by the municipality. Because of funding deadlines, MeDEP worked with DeLuca-Hoffman Associates, Inc. and the Town of Buxton to issue a permit in 2 weeks.

- **Lee Dodge Auto Dealership, Westbrook, Maine**

DeLuca-Hoffman Associates, Inc. prepared a post permit for activities which exceeded Site Location Law thresholds and permitted the long range expansion for a 5 year expansion program for this 20 acre facility in Westbrook, Maine.

- **Thornton Academy, Saco, Maine**

DeLuca-Hoffman Associates, Inc. prepared civil drawings and the MeDEP Permit Applications for work to include additional building space, reconfigured parking and access around the school.

- **Scarborough Middle School, Scarborough, Maine**

This project involves construction of a new school, parking lots with 318 spaces, 2 stormwater quality management ponds, and athletic fields. DeLuca-Hoffman Associates, Inc. prepared civil drawings and MeDEP Permit Application in coordination with Stephen Blatt Architects.

- **Oxford Hills High School Expansion, Norway and Paris, Maine**

The project involved a 22 million-dollar high school expansion. The upgrade required state and federal permits and has a separate athletic field complex.

- **University of Maine Systems, University of Southern Maine, Gorham Campus**

DeLuca-Hoffman Associates, Inc. prepared the Site Location of Development Permit Application for the Gorham Campus. The applications permitted post 1970 activity as well as the new Ice Arena and Fieldhouse. A detailed campus parking and traffic study was prepared as part of the permitting activity.

- **Poland Middle and High School, Maine**

DeLuca-Hoffman Associates, Inc. has prepared the Site Location Permits and completed the site design as a subconsultant to PDT Architects for a new middle and high school complex on 100 acres of land in Poland. The site contains a former borrow pit and is within a lake watershed requiring special water quality measures.

- **Piper Shores, Scarborough, Maine**

DeLuca-Hoffman Associates, Inc. prepared the civil/site design for the Maine Life Care Retirement Community in Scarborough, Maine. The project required a local contract zoning. The community will set a new standard and is anticipated to become Maine's premier and most desirable retirement community.

- **Topsham Demonstration Project and Business Park, Topsham, Maine**

DeLuca-Hoffman Associates, Inc. was retained by the Town of Topsham and MeDEP to conduct a pilot project to examine the viability of the Master Plan approach to Site Location permitting. Subsequently, DeLuca-Hoffman Associates, Inc. permitted the 25-acre Topsham Business Park which was within the pilot study area.

- **Saco Industrial Park, Saco, Maine**

DeLuca-Hoffman Associates, Inc prepared the Site Location of Development Permit, Tier 3 NRPA Wetlands Permit, and local Subdivision Permit Application for the Saco Industrial Park Expansion. The project includes the extension of infrastructure including rail lines to expand the City's Industrial Park to Route 1 and development of 24 industrial lots on a 120 acre parcel of land. Natural resource impacts require wetland fill permits for 1.81 acres of wetland.

- **York Commercial Park, York, Maine**

The project includes a five lot commercial subdivision with a Hannaford store in York, Maine.

- **Westbrook Power Plant, Westbrook, Maine**

DeLuca-Hoffman Associates, Inc. has prepared the Federal and State Site Permit Applications for the proposed 528mw gas fired generation facility in Westbrook, Maine. The permits for the project have been received. Construction of this \$250,000,000 facility is scheduled to commence in February 1999.

- **Shop 'n Save Grocery Store, Waterboro, Maine**

This project includes an onsite wastewater disposal system with pretreatment, two lined water quality ponds and a Shop 'n Save grocery store.

- **Interstate Bakeries**

DeLuca-Hoffman Associates, Inc. was retained by Florida Crown to amend the State and Federal site permits for the relocation of the former Nissen Bakery of Portland.

- **Lake Region Middle and High Schools, Naples, Maine**

DeLuca-Hoffman Associates, Inc. prepared the permit applications and site designs for the MSAD #61 athletic field upgrades at the high and middle school complex in Naples, Maine.

- **West Falmouth Crossing, Falmouth, Maine**

DeLuca-Hoffman Associates, Inc. has prepared the civil/site designs and permit applications for a large mixed use commercial development in Falmouth, Maine. Complex phasing is required to construct the project due to unstable clay soils.

- **Scarborough School – Wentworth Campus, Scarborough, Maine**

DeLuca-Hoffman Associates, Inc. has prepared permit applications to implement a master plan (permit pending) for the Wentworth School in Scarborough.

- **Biddeford Business Park, Biddeford, Maine**

DeLuca-Hoffman Associates, Inc. has prepared the permit applications for a 15-lot business park. The design includes all utilities, water quality measures, and extensive rock removal. (Permit pending.)



- *Lake Region School District; Bridgton Elementary School, Bridgton, Maine*

DeLuca-Hoffman Associates, Inc. has prepared site location permit applications for the Bridgton Elementary School. This project requires phosphorus controls, a new wastewater disposal field, and a new campus site design. (Permit pending.)

- *Bowdoin College – Office Building, Brunswick, Maine*

DeLuca-Hoffman Associates, Inc. has prepared the technical documents to support the College's Site Location of Development Permit for the office building at the Main Street Station Site. (Permit pending.)

- *Maine Criminal Justice Academy, Vassalboro, Maine*

DeLuca-Hoffman Associates, Inc. is completing permit applications for the Maine Criminal Justice Academy in Vassalboro.

- *Village School, Gorham, Maine*

DeLuca-Hoffman Associates, Inc. is completing feasibility studies for the upgrade of the Village School campus to enhance circulation, provide adequate areas for bus queuing, and reconstruct recreation fields. Permit applications are scheduled to be prepared in early 1999.

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**DeLuca-Hoffman Associates, Inc. Since 1988**

**SENIOR ENGINEER**

*University of Southern Maine, Gorham, Maine:*

Design, permitting, and construction administration services for the construction of new access roads and parking facilities to support the new athletic field house, ice hockey arena, and residence hall. The new access road services as a secondary connector road through the campus site from Route 25 to School Street. This was a requirement of the Town of Gorham to improve emergency access for the University Campus. The overall project included the construction of 4,500 l.f. of access roads, vehicular parking areas (845 spaces), two water quality retention ponds, and site improvements associated with the new facilities.

*Saco Industrial Park Expansion, Saco, Maine:*

Design, permitting, and construction administration services for the development of a 124-acre expansion to the Saco Industrial Park. The industrial park expansion consists of the creation of 24 industrial lots with the construction of 6,600 l.f. of access road with two water quality retention ponds. The new access road will eventually provide a direct connection between the existing Industrial Park Road and U.S. Route 1. The initial phase of the project is currently under construction.

*Route 1 Sewer Improvement Project, Saco, Maine:*

Design and construction administration of an 8.5 million dollar sewer improvement project. Included within this project were over 5,900 linear feet of force main, 19,800 linear feet of gravity sewer main, 5,500 linear feet of sewer and storm drain service leads, and 7,600 linear feet of storm drain.

*Bear Brook, Goosefare Brook and Mill Brook Sewage Pump Stations, Saco, Maine:*

Design and construction administration for three sewage pump stations with a maximum pump capacity ranging from 1,000 to 3,000 gpm. Included within this project were odor control systems, emergency standby generator, and instrumentation system containing a SCADA system.

*Colonial Road Sewage Pump Station, Westbrook, Maine:*

Design and construction administration for a duplex self-priming sewage pump station with a maximum pump capacity of 600 gpm. This project included the upgrade of an existing pump station to include new equipment, instrumentation, odor control system, and permanent emergency standby generator.

*Wastewater Treatment Plant Secondary Clarifier, Saco, Maine:*

Design and construction administration for a new 75' diameter secondary clarifier and flow split structure to replace existing clarifier tankage damaged during the October 1996 flood event. A key element in the design included maintaining of plant flows and treatment capacity throughout the construction process. The total cost of construction is 1.2 million dollars, which involved federal money through the FEMA Disaster Relief Program and SRF money through the Maine Bond Bank.

Mr. Laverriere is a Senior Engineer with DeLuca-Hoffman Associates, Inc.'s Land Design and Engineering Services Group and Environmental and Water Resources Group. He directs the preparation and review of preliminary and final design as well as permit applications for a variety of civil/site and environmental engineering projects. Mr. Laverriere's expertise includes civil/site design, hydraulic and hydrologic analysis.

*Thornton and Storer Streets, Saco, Maine:*

Design and construction administration for sanitary sewer replacement and storm drain separation along two urban streets. Included within this project were complete utility infrastructure and full depth road reconstruction.

*Lower Route 1 Infrastructure Improvements, Falmouth, Maine:*

Design and construction phase services for the replacement of sanitary sewer main and service leads along eight residential streets in Falmouth. The project also included the installation of a new storm drain system designed with service leads to receive illicit inflows from residential home foundation drains and sump pumps. The primary purpose of the project was to reduce the amount of infiltration and ground water inflow into the sanitary sewer system. The first three phases of the project were constructed from 1995 through 1997 with the final phase scheduled for the spring of 1998. The total cost of construction is 1.5 million dollars.

*Combined Sewer Overflow, 5-Year Plan, Portland, Maine:*

Participation in the preparation of the City's initial 5-year plan for abating CSOs throughout the City. The preparation of the 5-year plan required development of a ranking system for all CSO abatement projects in order to implement the most cost-effective projects during the initial years of the program. Re-evaluation of the financial requirements for the 5-year plan was performed in order that the City could assess the impacts on the sewer user rate necessary to fund the program.

*Fairgrounds Marketplace, Skowhegan, Maine:*

Design of a water quality retention pond to treat surface runoff for a 195,000 square foot retail center. The retail center is located within the Cold Brook watershed with a small six-acre manmade pond in proximate location to the retail center.

*Merry Meeting Plaza, Brunswick, Maine:*

Design of a storm water management detention pond facility to mitigate peak runoff rates from a 145,000 square foot retail center. The storm water management facility was also constructed with a permanent retention pond designed to provide long-term water quality treatment from the retail center surface runoff. The project is located within the Androscoggin River watershed.

*Water Quality Retention Pond, Route 302, Bridgton, Maine:*

Design of a phosphorus control retention pond facility to treat the surface runoff associated with the MDOT's recent widening of a 1.5-mile segment of Route 302 tributary to Highland Lake. Work included a three phase study to assess impacts of proposed project alternatives, review potential treatment measures, and develop a phosphorus control plan to meet the overall project water quality requirements.

*Thornton Academy, Saco, Maine:*

Design of a storm water management detention pond facility to mitigate increased runoff rates associated with the expansion of the school's campus facility. Work included the completion of an overall drainage master plan study to review drainage deficiencies throughout the school campus. The original school infrastructure system dated back to the late 1800's. Prioritization and sequence of drainage improvements was developed for implementation of the drainage master plan study.

*Water Quality Standards for LURC:*

Review of current LURC land development performance standards that affect water quality and development of recommendations to modify the performance standards to implement BMP's and minimum structural treatment measures to improve water quality protection and permit controlled development. Work included development of various algorithms to assess impacts of development on a lake water quality and predict means for increased development pressure for use by LURC.

*Sawyer Brook Watershed Study:*

Participation with the Soil Conservation Service and York County Soil and Water Conservation District to prepare a storm water analysis for a large 354-acre rural/urban watershed area in Saco, Maine. The Sawyer Brook watershed area is the primary water coarse draining the majority of the downtown business district. The study culminated in an assessment of recommendations to preserve the existing open channel systems and alternative for upgrading the drainage system to reduce chronic flooding problems resulting in the design and construction administrative services for the construction of a trunk storm drain (ranging from twin 72", 84", 96" and 5' by 10' box culvert pipeline) along Spring Street with outfall into the Saco River.

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JOSEPH A. LAVERRIERE, P.E.

Senior Engineer

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**EDUCATION:** BSCE - University of Maine, Orono, Maine (1986)

**REGISTRATION:** Registered Professional Engineer, Maine

**AFFILIATIONS:** Member, American Society of Civil Engineers  
Member, Water Environment Federation

**EXPERIENCE IN FIELD:** Active in Private Practice Since 1986

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DELUCA-HOFFMAN ASSOCIATES, INC. SINCE 1994

*TRAFFIC AND PARKING STUDY, USM GORHAM CAMPUS*

Responsible for annual data collection, analysis and technical memorandum of traffic and parking issues on the University of Southern Maine Gorham Campus. The purpose of the study was to qualify and quantify on Campus parking and traffic issues to be addressed by the University.

*TRAFFIC CALMING MEASURES, PORTLAND, MAINE:*

Responsible for all of the data collection, data reduction, and analysis of the results for an approximately 1.3-mile section of Stevens Avenue. The purpose of the monitoring was to determine the effect of traffic calming measures on pedestrian, bicycle and vehicle speeds along the corridor.

*STILLWATER AVENUE, BANGOR, MAINE*

Corridor evaluation of an approximately 1.2 mile section of Stillwater Avenue adjacent to the Bangor Mall. This study balanced the safety and needs for additional vehicle capacity and increased pedestrian and bicycle use.

*DOWNTOWN CIRCULATION, BANGOR, MAINE:*

Evaluated traffic operations, circulation, pedestrian and bicycle movement within the downtown area of Bangor which resulted in alternative traffic circulation options.

*SOUTH MAIN STREET STUDY, AUBURN, MAINE:*

Corridor evaluation of a 3 mile section of South Main Street. This study identified deficiencies and recommend prioritized alternatives to address the deficiencies which included pedestrian and bicycle safety, intersection and roadway reconstruction, shoulder widening, signage, and access management.

*TRAFFIC IMPACT STUDIES - VARIOUS LOCATIONS:*

Assisted in calculating and reviewing trip generation, distribution and capacity analysis for projects ranging in size from 50,000 to 300,000 square feet and their preparation for submittal to local and state agencies.

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PROJECT ENGINEER

Mr. Dunton is a Project Engineer with DeLuca-Hoffman Associates, Inc.'s Transportation Group. He directs data collection and performs capacity analysis to determine traffic impacts and proposed mitigation for a variety of site and traffic engineering projects. He then designs the mitigation measures to address the deficiencies. Mr. Dunton's expertise includes the knowledge of 12 computer data collection and traffic modeling programs which allows the flexibility of choosing the most appropriate modeling technique for the specific conditions and the ability to compare results with other appropriate programs.

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<b>EDUCATION:</b>	BSCE - University of Maine, Orono, Maine
<b>SEMINARS:</b>	<ul style="list-style-type: none"><li>• Intermediate CORSIM (Computer Simulation Program)</li><li>• Traffic Signals with TEAPAC, PASSER, TRANSYT, and NETSIM</li><li>• Advanced TEAPAC Application Techniques</li><li>• Traffic Signal System Timing Workshop</li></ul>
<b>REGISTRATION:</b>	Registered Professional Engineer: Maine
<b>AFFILIATIONS:</b>	Associate Member, Institute of Transportation Engineers  Past Treasurer of National Engineers Week Committee  Associate Member International Municipal Signal Association (IMSA)
<b>EXPERIENCE IN FIELD:</b>	7 Years in Private Practice

**Cynthia Plank Orcutt ASLA**

*President*

*Landscape Architect*

Cynthia Plank Orcutt is a principal and Director of Site Services for the firm Orcutt Associates, PA. Her site designs emphasize cultural context, historical continuity and environmental fit, often stressing the need for usable outdoor spaces. Prior to forming Orcutt Associates, Ms. Orcutt was a Senior Associate with the firm of Sasaki Associates. Ms. Orcutt has received numerous awards for outstanding site design of projects including Village-on-the-Green at Sugarloaf/USA Resort, the Basketball Hall of Fame, Kirk Hall and the Samoset Village in Rockport, Maine.

**EDUCATION**

Harvard University, Graduate School of Design, MLA with distinction 1982

ASLA Certificate of Merit 1982

Jacob Wiedeman Prize 1982

Penny White Grant 1981

State University of New York at Syracuse, College of Environmental Science and Forestry,  
Bachelor of Landscape Architecture 1978

State University of New York at Syracuse, College of Environmental Science and Forestry,  
Bachelor of Science / Environmental Studies 1977

**MASTER PLANNING EXPERIENCE**

- Central Maine Medical Center; Lewiston, ME - Master Plan and Medical Building
- Central Maine Techn. College; Auburn, ME - Master Plan and Allied Health Building
- Millennium Advisors Corporation; Boothbay, ME - Corporate Campus
- Merriconeag Waldorf School; Freeport, ME - Master Plan
- NorthBrook Business Campus; Gray, ME - Master Plan
- Okaloosa Walton Community College; Niceville, FL - Master Plan
- Peddocks Island; Boston Harbor, Boston, MA - Master Plan
- St. James Place Office Park; Scarborough, ME - Master Plan
- Samoset Resort; Rockport, ME - Master Plan
- Sugarbush Ski Resort; Warren, VT - Master Plan
- Sugarloaf/USA; Carrabassett Valley, ME - Master Plan
- University of Maine; Orono, ME - South Campus Master Plan
- University of Maine at Augusta; Augusta, ME - Master Plan
- University of Maine at Farmington; Farmington, ME - Master Plan
- University of Southern Maine; Portland, ME - Master Plan
- Washington County Technical College; Calais, ME - Master Plan



Cynthia Plank Orcutt ASLA

page 2

#### **INSTITUTIONAL AND CAMPUS SITE WORK EXPERIENCE**

- Bowdoin College; Brunswick, ME - ADA Renovations; War Memorial; Student Center-Schematic Design
- Kennebec Valley Technical College; Fairfield, ME - Carter Hall
- Central Maine Medical Center; Lewiston, ME - Streetscape Improvements
- College of the Holy Cross; Worcester, MA - Edith Stein Hall
- Connecticut General Insurance Co., CIGNA; Bloomfield, CT - Headquarters (with TAC)
- Hancock Shaker Village; Pittsfield, MA - ISTEA Visitor Access Enhancement Package
- Oak Hill High School; Sabattus, ME - Addition
- Okaloosa Walton Community College; Niceville, FL - Instructional Arts Center
- Irving Oil Corporation; Portland, ME - Petroleum Transfer Facility
- Ronald McDonald House; Portland, ME
- University of Maine; Orono, ME - Memorial Student Union
- Washington County Technical College; Calais, ME - St. Croix Hall

#### **RESIDENTIAL AND RESORT EXPERIENCE**

- Black Point Inn; Prouts Neck, ME - Master Plan and Guest Room Expansion
- Carota Residence; Wilmington, DE
- Derryfield Country Club; Manchester, NH - Master Plan; Golf Clubhouse Improvements
- Fry Residence; Freeport, ME
- Samoset Resort; Rockport, ME - Golf Clubhouse, Golf Course Improvements, Guest Rooms, Dining Facility Expansion and Pen Bay Gardens Master Plan
- Samoset Village; Rockport, ME
- Sugarloaf/USA; Carrabassett Valley, ME - Village on the Green, Sugarloaf Hotel, Golf Clubhouse, Riverside, Snowbrook, Base Village Site Improvements
- Villas at the Woodlands; Falmouth, ME

#### **RENOVATION, REUSE, AND HISTORIC SITE WORK EXPERIENCE**

- Arlington Memorial Park; City of Arlington, MA
- Basketball Hall of Fame; Springfield, MA (with Cambridge Seven)
- Bug Light Park and ISTEA Greenbelt Bikeway; City of South Portland, ME
- Glen Mill Office Renovation; Newton, MA
- Main Street and Vendor Cart Design Guidelines; Town of Freeport, ME
- Marlborough Heritage State Park; City of Marlborough, MA
- Mast Landing Square; Town of Freeport, ME
- Maxwell's Pottery Outlet / Mitchell House; Freeport, ME
- Peddocks Island; Metropolitan District Commission, Boston Harbor, MA - Master Plan
- Sparhawk Mill; Yarmouth, ME - Restoration
- Town of Topsham; Topsham, ME - Design Guidelines



Cynthia Plank Orcutt ASLA

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#### ATHLETIC SITE WORK EXPERIENCE

- Bates College; Lewiston, ME - Athletic Facility
- Black Point Inn; Prouts Neck, ME - Health Club
- Bowdoin College Farley Field House and Pool; Brunswick, ME
- Casco Bay Regional YMCA; Freeport, ME
- Colby College; Waterville, ME - Alford Athletic Complex; Athletic Facilities Master Plan
- Cornell University; Ithaca, NY - Athletic Facility
- Hebron Academy; Hebron, ME - Athletic Center
- Husson College; Bangor, ME - Field House
- Lima YMCA and LMH Wellness Center; Lima, OH
- Mount Desert Island YMCA; Bar Harbor, ME
- Twin Brook Recreational Complex; Cumberland, ME
- Wells Community and Wellness Center; Town of Wells, ME
- Westbrook College; Portland, ME - Recreation Center

#### MUNICIPAL EXPERIENCE

- Arlington Memorial Park; City of Arlington, MA
- Bug Light Park and ISTE Greenbelt Bikeway; City of South Portland, ME
- Cumberland Town Hall; Cumberland, ME
- Marlborough Heritage State Park; City of Marlborough, MA
- Peddocks Island Master Plan; Metropolitan District Commission, Boston Harbor, MA
- Topsham Design Guidelines; Town of Topsham, ME
- Town of Freeport, ME - Mast Landing Square; Main Street and Vendor Cart Design Guidelines
- Twin Brook Recreational Complex; Cumberland, ME
- Wells Community Center and Town Hall; Town of Wells, ME

#### MUSEUM SITE WORK EXPERIENCE

- Hancock Shaker Village; Pittsfield, MA - Center for Shaker Studies
- Mystic Seaport Museum; Mystic Seaport, CT
- Strawberry Banke Museum; Portsmouth, NH - Visitor's Center
- Nott House Museum; Kennebunkport, ME
- Basketball Hall of Fame; Springfield, MA





Cynthia Plank Orcutt ASLA

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#### PERMIT EXPERIENCE

- DEP, State of Maine, Site Plan, City of Auburn; Kirk Hall, CMTC
- DEP, State of Maine, Site Plan, City of Calais, Maine; St. Croix Hall, WCTC
- DEP, State of Maine, Site Plan, Town of Fairfield, Maine; Carter Hall, KVTC
- DEP, State of Maine, Army Corps of Engineers, Site Plan, City of Falmouth; Villas - Woodlands
- DEP, State of Maine, Army Corps of Engineers, Site Plan, Town of Freeport; Casco Bay Regional YMCA
- DEP, State of Maine, Oak Hill High School Addition, Sabattus, ME
- DEP, State of Maine, Site Plan, Town of Rockport, Maine; Samoset Village Housing
- DEP, State of Maine, Army Corps of Engineers, Golf Course Renovations, Samoset Resort
- DEP, State of Maine, Army Corps of Engineers, Site Plan, City of Scarborough, ME; St. James Place Office Park
- DEP, State of Maine, Army Corps of Engineers, Site Plan, City of South Portland; Irving Oil Petroleum Transfer Facility
- ISTEPA Review, Massachusetts Highway Department; Hancock Shaker Village
- L.U.R.C., State of Maine, Sugarloaf/USA, Kingfield, ME
- Other Municipal Permit Reviews: Towns and Cities of Boothbay, Falmouth, Freeport, Kennebunkport, Lewiston, Portland, Rockport, Scarborough, South Portland, Waldoboro, Yarmouth and Portsmouth, NH

#### REGISTRATIONS

Registered Landscape Architect: MA, ME

#### PROFESSIONAL AFFILIATIONS

American Society of Landscape Architects (ASLA)

Maine Section of the Boston Society of Landscape Architects (BSLA)

#### CIVIC AFFILIATIONS

Design Review Board, Vice Chairman, Freeport, Maine, 1992-94

Freeport Historical Society

Freeport Conservation Trust

#### ACADEMIC POSITIONS

Colby College, Advisor to the Overseers of the Physical Plant, 1994

State University of New York, College of

Environmental Science and Forestry, Guest

Lecturer and Teaching Assistant, 1992, 1977.

Westbrook College, Guest Lecturer, 1989.

Harvard University, Design Critic, Seminar

Instructor and Guest Lecturer, 1983-87.

Wentworth Institute of Technology, Guest Lecturer, 1986.

Boston Architectural Center, Guest Lecturer, 1984-86.

Harvard University, Teaching Assistant, 1982.



**Cynthia Plank Orcutt ASLA**

*President*

*Landscape Architect*

Cynthia Plank Orcutt is a principal and Director of Site Services for the firm Orcutt Associates, PA. Her site designs emphasize cultural context, historical continuity and environmental fit, often stressing the need for usable outdoor spaces. Prior to forming Orcutt Associates, Ms. Orcutt was a Senior Associate with the firm of Sasaki Associates. Ms. Orcutt has received numerous awards for outstanding site design of projects including Village-on-the-Green at Sugarloaf/USA Resort, the Basketball Hall of Fame, Kirk Hall and the Samoset Village in Rockport, Maine.

**EDUCATION**

Harvard University, Graduate School of Design, MLA with distinction 1982

ASLA Certificate of Merit 1982

Jacob Wiedeman Prize 1982

Penny White Grant 1981

State University of New York at Syracuse, College of Environmental Science and Forestry,  
Bachelor of Landscape Architecture 1978

State University of New York at Syracuse, College of Environmental Science and Forestry,  
Bachelor of Science / Environmental Studies 1977

**MASTER PLANNING EXPERIENCE**

- Central Maine Medical Center; Lewiston, ME - Master Plan and Medical Building
- Central Maine Techn. College; Auburn, ME - Master Plan and Allied Health Building
- Millennium Advisors Corporation; Boothbay, ME - Corporate Campus
- Merriconeag Waldorf School; Freeport, ME - Master Plan
- NorthBrook Business Campus; Gray, ME - Master Plan
- Okaloosa Walton Community College; Niceville, FL - Master Plan
- Peddocks Island; Boston Harbor, Boston, MA - Master Plan
- St. James Place Office Park; Scarborough, ME - Master Plan
- Samoset Resort; Rockport, ME - Master Plan
- Sugarbush Ski Resort; Warren, VT - Master Plan
- Sugarloaf/USA; Carrabassett Valley, ME - Master Plan
- University of Maine; Orono, ME - South Campus Master Plan
- University of Maine at Augusta; Augusta, ME - Master Plan
- University of Maine at Farmington; Farmington, ME - Master Plan
- University of Southern Maine; Portland, ME - Master Plan
- Washington County Technical College; Calais, ME - Master Plan



**John Russell Orcutt AIA**

*Principal  
Architect*

Mr. Orcutt is a principal of Orcutt Associates PA. Prior to establishing Orcutt Associates, Mr. Orcutt was the Principal-in-Charge of the Architectural Practice at Sasaki Associates for fifteen years at the firm's headquarters in Massachusetts. With over thirty years of experience, he has overseen the design, management and construction of college and university, corporate, commercial, institutional, theater, resort, athletic and residential projects. Mr. Orcutt's competition and award-winning architectural designs emphasize solutions which are sensitive to their local and regional sites.

**EDUCATION**

University of Colorado, School of Architecture; Bachelor of Architecture with Honors, 1967  
Alpha Rho Chi Medal 1967  
Dartmouth College; Bachelor of Arts in Geography and Economics, 1959

**INSTITUTIONAL AND CAMPUS PLANNING EXPERIENCE**

- Affiliated Hospitals; Boston, MA
- Governor Baxter School for the Deaf; Falmouth, ME
- Black Point Inn, Prout's Neck, ME
- Boston City Hospital; Boston, MA
- Boston College; Chestnut Hill, MA
- Bowdoin College; Brunswick, ME
- Central Maine Medical Center; Lewiston, ME
- Central Maine Technical College ; Auburn, ME
- Colby College Residence Hall Study; Waterville, ME
- College of the Holy Cross; Worcester, MA
- Cornell University; Ithaca, NY
- Governor Baxter School for the Deaf; Falmouth, ME
- Mayo Clinic; Rochester, MN
- Okaloosa-Walton Community College; Niceville, FL
- Samoset Resort; Rockport, ME
- St. James Place; Scarborough, ME
- Sugarloaf / USA, Carrabassett Valley, ME
- The Merriconeag Waldorf School; Freeport, ME
- University of Maine, Augusta, Master Plan; Augusta, ME
- University of Maine Farmington; Farmington, ME
- University of Maine, Orono, Arts District, Orono, ME
- University of Southern Maine; Portland, ME
- Washington County Technical College; Calais, ME
- Western Wyoming College; Rock Springs, WY



John Russell Orcutt AIA

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#### UNIVERSITY DESIGN EXPERIENCE

- Boston College; Chestnut Hill, MA - Conte Forum for Hockey & Basketball; Alumni Stadium Renovation; Parking Garage; Dormitory; Theater Arts Center
- Bowdoin College; Brunswick, ME - ADA Compliance Study; Campus Center Schematic Design; Dayton Ice Arena Renovation; Farley Field House and Pool; Bowdoin War Memorial
- Brandies University; Waltham, MA - Athletic Facility
- Bridgewater State College; Bridgewater, MA - Dormitory
- Brown University; Providence, RI - Performing Arts and Music Complex
- Bryant College; Smithfield, RI - Athletic Facility
- Central Maine Technical College; Auburn, ME - Kirk Hall
- Colby College; Waterville, ME - Athletic Facility Master Plan; Alford Athletic Center Expansion; Kotter Union Renovation; Residence Hall Study; ADA Compliance Study
- College of the Holy Cross; Worcester, MA - Edith Stein Hall; Pool and Rowing Tank
- Cornell University; Ithaca, NY - Athletic Facility, Schoelkopf Stadium Renovations, Press Box and Parking Garage
- Groton School; Groton, MA - Science Building Renovations and Additions
- Hebron Academy; Hebron, ME - Athletic Facility Master Plan; Dormitory Renovations
- Hobart and William Smith Colleges; Geneva, NY - Athletic Facility Field House; Master Plan
- Kennebec Valley Technical College; Fairfield, ME - Master Plan; Carter Hall
- Loomis Institute; Windsor, CT - Library
- McDonough School; McDonough, MD - Athletic Facility Consulting
- Maine Maritime Academy; Castine, ME - Locker Room Renovations
- Northeastern University; Boston, MA - Library Design Competition
- Okaloosa-Walton Community College; Niceville, FL - Instructional Arts Center, Museum and Theater; Learning Resource Center
- Rhodes College; Memphis, TN - Athletic Facility Master Plan
- St. Joseph's College; Windham, ME - Student Commons Competition,
- St. Mark's School; Southborough, MA - Library and Dormitory
- St. Vincent's Hospital; Worcester, MA - Teaching Amphitheater and Library
- State University of New York at Buffalo; Amherst, NY - Administration Building; Student Center; Three Libraries
- Tufts University; Medford, MA - Campus Center Schematic Design
- University of Central Florida; Orlando, FL - Arena
- University of Maine; Orono, ME - Campus Center
- University of New Hampshire; Durham, NH - Library Planning
- Washington County Technical College; Calais, ME - Master Plan; St. Croix Hall
- Westbrook College; Portland, ME - Beverly Burpee Finley Recreation Center
- Western Wyoming College; Rock Springs, WY - Classrooms; Student Commons; Athletic Facility; Theater
- Wheaton College; Norton, MA - Athletic Facility
- Williston Northampton School; Easthampton, MA - Pool and Field House



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#### CORPORATE AND COMMERCIAL EXPERIENCE

- Analogic Corporation Headquarters; Peabody, MA
- Bank of Boston; Framingham, MA
- Bay Colony Corporate Center; Waltham, MA
- Chase Mills Office Renovation; Watertown, MA
- Chubb Corporate Headquarters; Warren Township, NJ
- Data Terminal Systems Light Industrial Facility; San Juan, PR
- Doble Engineering Company; Watertown, MA
- Honeywell Electro-Optics Center; Marlborough, MA
- Northwestern Mutual Life Insurance Company; Milwaukee, WI
- One Portland Square; Portland, ME
- Salmon Brook Corporate Park; Glastonbury, CT
- Sparhawk Mills Office Renovation; Yarmouth, ME
- St. James Place; Scarborough, ME
- State of Florida Office Complex; Tallahassee, FL
- Tektronix Office Addition; Lexington, MA
- Union Warren Savings Bank; Boston, MA
- UNUM Home Office Building; Portland, ME
- Westford Corporate Center; Westford, MA

#### RESORT AND RESIDENTIAL EXPERIENCE

- Back Bay Tower; Portland, ME
- Billings Farm Museum; Woodstock, VT
- Black Point Inn; Prouts Neck, ME
- Carota Residence; Wilmington, DE
- Colonial Williamsburg Clubhouse; Williamsburg, VA
- Dorado Beach; Dorado Beach, PR
- Eastern Point Mixed-Use; Portland, ME
- Fry Residence; Freeport, ME
- Le Rivage; Key Biscayne, FL
- Little Dix Bay; Virgin Gorda, British Virgin Islands
- Mystic Seaport Museum; Mystic, CT
- Oceanwood Design Guidelines; Saco, ME
- Page/Webster Residence; Orr's Island, ME
- Patterson Wheel Track ; Freeport, ME
- Robin's Island; Long Island, NY
- Samoset Villages, Hotel, Restaurant and Lounge Renovations, Golf Clubhouse, Samoset Resort; Rockport, ME
- Stageneck Inn and Condominiums; York, ME
- Strawberry Banke Museum; Portsmouth, NH
- Tarantino Residence; Freeport, ME
- Village on the Green, Snowbrook Village, Sugarloaf Mountain Hotel, Golf Clubhouse, Ski Touring Center Master Plan, Sugarloaf/USA; Carrabassett Valley, ME



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#### INTERIOR DESIGN EXPERIENCE

- Analogic Corporation Headquarters; Peabody, MA
- Bank of Boston; Framingham, MA
- BayBanks; Boston, MA
- Digital Equipment Corporation Engineering Facility; Westford, MA
- Moon, Moss, McGill Law Office, Portland, ME
- Texas Instruments; Waltham, MA • University of Virginia; Charlottesville, VA

#### LIBRARY EXPERIENCE

- Loomis Institute; Windsor, CT
- Northeastern University; Boston, MA
- Okaloosa Walton Community College, Learning Resource Center; Niceville, FL
- Plymouth Public Library; Plymouth, MA
- Ramapo College of New Jersey; Mahwah, NJ
- St. Mark's School; Southborough, MA
- St. Vincent Hospital; Worcester, MA
- State University of New York at Buffalo; Amherst, NY
- Western Wyoming College; Rock Springs, WY

#### MUNICIPAL EXPERIENCE

- Affiliated Hospitals; City of Boston, MA
- Boston City Hospital Out-Patient Clinic, Mechanical Plant; City of Boston, MA
- Charleston Parking Garages; City of Charleston, SC
- Cooperstown Parking Garage; City of Cooperstown, NY
- Governor Baxter School for the Deaf; Town of Falmouth, ME
- Jackie Gleason Theater of the Performing Arts; City of Miami Beach, FL
- Lima Theater Arts Center; City of Lima, OH
- Wells Community Center and Town Hall; Town of Wells, ME

#### MUSEUM EXPERIENCE

- Billings Farm Visitor's Center and Museum; Woodstock, VT
- Hancock Shaker Village Visitor's Center and Center for Shaker Studies; Pittsfield, MA
- Kykiat Visitor's Center and Museum, Rockefeller Estate, Pocantico, NY
- Mystic Seaport Visitor's Center and Museum; Mystic, CT
- Nott House Visitor's Center; Kennebunkport, ME
- Strawberry Banke Visitor's Center and Museum, Portsmouth, NH



**John Russell Orcutt AIA**

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#### **PARKING FACILITIES EXPERIENCE**

- Boston College Parking Facility; Chestnut Hill, MA
- Central Maine Medical Center Parking Facility; Lewiston, ME
- Charleston Parking Garages; Charleston, SC
- Cumberland Avenue Parking Facility (design); Portland, ME
- Cooperstown Parking Garage; Cooperstown, NY
- Cornell University Parking Garage and Press Box; Ithaca, NY
- Bay Colony Corporate Center; Waltham, MA
- Chubb Corporate Headquarters; Warren Township, NJ
- Northwestern Mutual Life Insurance Company; Milwaukee, WI
- One Portland Square; Portland, ME

#### **RENOVATION REUSE AND HISTORIC EXPERIENCE**

- Billings Farm and Museum; Woodstock, VT
- Bowdoin College; Brunswick, ME - Dayton Ice Arena Renovation
- Charleston Parking Garages; Charleston, SC (National Historic District)
- Chase Mills Office Renovation; Watertown, MA
- Hancock Shaker Village; Pittsfield, MA (National Historic Site) - Master Plan; Center for Shaker Studies
- Jackie Gleason Theater of the Performing Arts; Miami Beach, FL (Art Deco District)
- Mitchell House (1797) Renovations; Freeport, ME (National Historic District)
- Northwestern Mutual Life Insurance Company Office Renovation; Milwaukee, WI
- Sparhawk Mill Renovation; Yarmouth, ME
- Strawberry Banke Museum; Portsmouth, NH - Museum Center
- Walker Art Museum Renovation (McKim, Mead and White), Bowdoin College; Brunswick, ME
- Wellesley Town Hall Renovation; Wellesley, MA

#### **RETAIL EXPERIENCE**

- Borealis Breads; Wells, ME
- Kittery Retail Center; Kittery, ME
- Maxwell's Pottery Outlet; Freeport, ME
- Maine Cottage Furniture; Yarmouth, ME
- Portland Public Market; Portland, ME
- The Big Tomato Restaurant; Freeport, ME
- The Portland Public Market; Portland, ME
- Village Candle; Topsham, ME



John Russell Orcutt AIA

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#### **THEATER ARTS AND AUDITORIUM FACILITIES**

- Boston College, Theater Arts Center; Chestnut Hill, MA
- Central Maine Technical College; Auburn, ME
- Jackie Gleason Theater of the Performing Arts; Miami Beach, FL
- Lima Theater Arts Center; Lima, OH
- Lima Memorial Hall Renovation; Lima, OH
- Okaloosa-Walton Community College, Theater, Art and Music Complex, Niceville, FL
- ST Vincent's Hospital, Teaching Amphitheater; Worcester, MA
- SUNY at Buffalo, Capen Hall and Auditorium; Amherst, NY
- University of Louisville, Humanities Building, Recital Hall; Louisville, KY
- Western Wyoming College, Student Commons and Theater; Rock Springs, WY

#### **URBAN REVITALIZATION EXPERIENCE**

- Charleston Parking Garages; Charleston, SC
- Cumberland Avenue Parking Facility and Portland Public Market; Portland, ME
- Cooperstown Parking Garage; Cooperstown, NY
- Jackie Gleason Theater of the Performing Arts; Miami Beach, FL
- Lima Theater Arts Center; Lima, OH
- Varrell's Wharf Feasibility Study; York, ME

#### **ATHLETIC FACILITY EXPERIENCE**

- Bar Harbor YMCA; Bar Harbor, ME - Schematic Design
- Bay Colony Corporate Center Health Club; Waltham, MA
- Black Point Inn Health Club; Prout's Neck, ME
- Boston College Conte Forum; Alumni Stadium; Chestnut Hill, MA
- Bowdoin College Farley Field House and Pool; Brunswick, ME
- Brandeis University Athletic Complex, Auerbach Arena; Waltham, MA
- Bryant College; Smithfield, RI
- Casco Bay Regional YMCA; Freeport, ME
- Colby College Alford Athletic Complex, Waterville, ME
- College of the Holy Cross Pool and Rowing Tank; Worcester, MA
- Cornell University Schoelkopf Stadium; Ithaca, NY
- Groton School; Groton, MA
- Hebron Academy Athletic Facility and Pool (design); Hebron, ME
- Hobart and William Smith Colleges Field House; Geneva, NY
- Lima YMCA and LMH Wellness Center; Lima, OH
- Maine Maritime Academy Renovations; Castine, ME
- McDonough School Athletic Facility; McDonough, MD
- Ramapo College; Mahwah, NJ
- Rhodes College; Memphis, TN
- Samoset Resort Recreation Center and Pool Complex; Rockport, ME
- State University of New York at Buffalo; Amherst, NY
- University of Central Florida Athletic Complex; Orlando, FL





John Russell Orcutt AIA

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**ATHLETIC FACILITY EXPERIENCE (continued)**

- Westbrook College Beverly Burpee Athletic Facility; Portland, ME
- Western Wyoming College Athletic Complex; Rock Springs, WY
- Wells Community Wellness Center; Wells, ME
- Wheaton College; Norton, MA
- Williston Northampton School Athletic Facility; Easthampton, MA

**REGISTRATIONS**

Registered Architect: CO, CT, FL, MA, ME, NH, NJ, NY, SC, VA, VT

Certificate: National Council of Architectural Registration Boards (NCARB)

**PROFESSIONAL AFFILIATIONS**

American Institute of Architects (AIA)

Maine Chapter, AIA, Board of Directors, 1992

**ACADEMIC POSITIONS**

Colby College, Advisor to the Board of Overseers, Physical Plant, 1994

Boston Architectural Center, Guest Lecturer

Harvard University Graduate School of Design, Design Critic, Energy Aspects of Atria

University of Cincinnati, Guest Lecturer

**CIVIC AND COMMUNITY ACTIVITIES**

Portland Landmarks, Board of Directors, 1991

Freeport Alternative Transportation Committee, 1993-4

Freeport Historical Society

Freeport Conservation Trust

Western Mountain Corp. Planning Committee, 1994

Freeport Public Schools Long Range Planning Committee, 1995

Casco Bay Regional YMCA, Programming Committee, 1996



## ABOUT THE FIRM

### *Background*

Symmes Maini & McKee Associates/SMMA is a multidisciplinary architectural, engineering, planning, and interior design firm, offering professional services to corporate/commercial, advanced technology/industrial, and institutional clients. Founded in 1955, the firm is headquartered in Cambridge, Massachusetts with a regional office in Minneapolis, Minnesota.

SMMA has been ranked by *Engineering News Record* as one of the Top 500 design firms in the United States and by *Building Design and Construction* as one of the Top 50 A/E firms. Repeat business from established clients has averaged nearly 70% for the last five years.

### *Approach*

SMMA's integrated approach to the delivery of architectural, engineering, interior design, and planning services offers substantial benefits to clients. Team members from all disciplines work together – from project initiation to completion – to develop uniquely tailored, coordinated, and appropriate solutions to the challenges of each client's program. This collaborative approach is the keystone of SMMA's ability to support the client's quality, schedule, and financial goals. Above all, there is respect for each client's unique image, market, and value requirements.

### *Services*

SMMA is committed to delivering the highest level of design and technical expertise. The diversity of the staff's project experience produces innovative planning, program, and design solutions for each client. Important components of SMMA's project development process include the integration of site and architecture, systems design, energy conservation, construction phasing/schedules, and value engineering and cost control.

SMMA's staff of 180 is guided by six principals, and Department Managers oversee the major architectural and engineering disciplines. The primary professional disciplines available to clients are:

- Architecture and architectural programming
- Campus planning, design, and educational programming
- Site design, landscape architecture, and urban planning
- Engineering – civil, structural, mechanical, electrical, plumbing, fire protection, and chemical
- Interior design and space planning

### *Technology*

Computer-based design, analysis, and communication tools are provided to every desktop, facilitating the sharing of information firm-wide and with clients, enhancing design creativity and productivity, and ensuring reliable information storage, retrieval, and publication. SMMA uses integrated Web and database technologies to disseminate internal information. Project extranet sites provide a secure, on-line meeting place for the exchange of information with all team members and the client.



**ARCHITECTURE**

- Programming
- Feasibility Studies
- Architectural Design
- Rehabilitation and Adaptive Re-Use
- Specifications
- Materials Research
- Code Analysis

**INTERIOR DESIGN**

- Programming
- Functional Planning
- Space Planning
- Furnishings and Finishes
- Specifications
- Installation Supervision
- Graphic Design
- Signage

**SITE DESIGN/  
CIVIL ENGINEERING**

- Site Evaluation and Selection
- Feasibility Studies
- Master Planning
- Landscape Architecture
- Site Program Analyses
- Planning and Zoning Approvals
- Site Engineering
- Environmental Impact Assessment
- Urban Planning

**STRUCTURAL ENGINEERING**

- Structural Analysis and Design
- Structural Feasibility Studies and Concept Design
- Restoration, Renovation, and Retrofit of Existing Structures
- Advanced Documents for Fast-Track Projects
- Structural Analysis and Investigation

- Forensic Engineering, Evaluation, and Remediation
- Structural Peer Review
- Structural Design of Building Components
- Administration of Structural Testing and Inspections during Construction
- Expert Testimony

**MECHANICAL ENGINEERING**

- HVAC Design
- Plumbing and Process Design
- Clean Room Systems Design
- Exhaust System Design
- Fire Protection and Life Safety Design
- Domestic Services Design
- Hydraulic Studies
- Central Plant Design
- Energy Conservation Studies
- Cost Benefit Analyses
- Existing System Evaluations
- Equipment Hook-up Design
- High Purity Water Systems
- Industrial Wastewater Systems
- Specialty Gas Systems

**ELECTRICAL ENGINEERING**

- Utilities Studies
- Lighting Design
- Power Distribution Design
- Computer Support Systems/UPS/APS Design
- Control and Security Systems Design
- Fire Protection and Life Safety Design
- Equipment Hook-up Design
- Telecommunications Design
- Utility Studies

**PROJECT MANAGEMENT/  
COST CONTROL**

- Feasibility Studies
- Detailed Cost Modeling
- Project Budget Analysis
- Cost Estimating
- Value Management/Life Cycle Costing
- Bid Evaluation
- Design/Construction Scheduling

**CONSTRUCTION ADMINISTRATION**

- Construction Contract Administration
- Field Coordination
- Submittals Review
- Systems “Shakedown”
- Record Drawings
- Operation and Maintenance Manuals



Adaptive Optics Associates	FSI International
Allied Domecq	GalaGen, Inc.
Analog Devices	GTE Laboratories
Andover Savings Bank	The Gutierrez Company
Bank of Boston	John Hancock
BayBank Middlesex	Harvard Community Health Plan
Bay Networks (now Nortel)	Harvard University
BioMedicus - Medtronics	IBM
Biotechnica International	Intersil (formerly Harris Semiconductor)
Blue Cross/Blue Shield of Maine	ITT
The Boston Company, Inc.	Jackson Laboratory
Boston Properties	The Leggatt McCall Companies
Boston University	L.L. Bean Inc.
Brown University	Lockheed Sanders Associates
Cambridge Savings Bank	MA/COM
Cambridgeport Savings Bank	Marriott Corporation
The Cambridge School of Weston	The Martignetti Company
Capital Crossing Bank (formerly Atlantic Bank)	Massachusetts Electric
The Chiofaro Company	Meadowbrook School, Weston
Christian Book Distributors	Medtronics
Combined Properties	Meredith & Grew, Inc.
Commercial Union Insurance Co.	Millipore/Waters
Compaq Computer Corporation	MIT
Concord Cooperative Bank	Nagog Development Company
Connecticut National Bank	National Amusements
Corning Inc.	National Development
Critical Care America	Noble & Greenough School
Data General	Nokia
Digital Equipment Corporation	Nordic Properties
DuPont/New England Nuclear Corp.	Nortel Networks
edu.com	Ocean Spray
EMC Corporation	Patriot Bank
Endicott College	The Pike School, Andover
Fallon Clinic	Pioneer Bank
Fidelity Properties, Inc.	Polariod Corporation
Fidelity Investments	Proteon
First Bank System	Quantum Corporation
FlexMedics	Radcliffe College
Forest City Development	Raytheon



RCA  
Saint Joseph's College of Maine  
St. Mark's School  
Serono Laboratories, Inc.  
Shawmut County Bank  
Siemens  
Southern Company  
Spaulding & Slye  
Stratus Computer  
Talbots  
Teradyne  
Tufts University  
University of Maine  
University of Massachusetts  
University of New England  
University of New Hampshire  
University of Southern Maine  
UnumProvident  
ViroMED Laboratories  
Waterville Valley Resort  
Wang Laboratories, Inc.



**Advanced Technology Manufacturing Center/ATMC – University of Massachusetts/Dartmouth**

Fall River, MA

- New Incubator Facility – Labs, Testing Facilities, and Classrooms

**Amherst College**

Amherst, MA

- Skating Arena

**Boston University**

Boston, MA

- Biology Laboratory Study, 53,000 sf
- Residence Hall Renovation, 70,000 sf
- Administrative Offices/Data Processing, 60,000 sf
- Administrative Offices/Student Health, 40,000 sf
- Hotel and Restaurant Teaching Facility, 15,000 sf

**Boston University, School of Medicine**

Boston, MA

- Medical Research Laboratory Renovation, 76,000 sf

**Central Lakes College**

Brainerd, MN

- New Master Plan for Co-location of Technical and Community Colleges
- Addition Linking Existing Community College, (190,000 sf)
- Child Care Facility
- Greenhouse

**Daniel Webster College**

Nashua, NH

- Campus Master Plan

**Endicott College**

Beverly, MA

- New Residence Hall, Design/Build, 124 beds

**Hamline University**

St. Paul, MN

- Addition to Law Library and Graduate School

**Harvard University**

Cambridge, MA

- Campus Buildings Audit

**Massachusetts Institute of Technology**

Cambridge, MA

- Alcator/Plasma Fusion Experiment, 50-foot concrete cell
- VLSI Teaching/Research, 74,000 sf
- Renovation of Warehouse for Office and R&D Laboratory, 31,400 sf

**Massachusetts Institute of Technology**

Middleton, MA

- Accelerator Laboratory Addition, 12,000 sf

**Massachusetts Institute of Technology**

Lexington, MA

- Submicron R&D Lab, 12,000 sf

**Northeastern University**

Boston, MA

- Cleanroom Study, 1,800 sf

**Radcliffe College**

Cambridge, MA

- Campus Buildings Audit

**St. Cloud State University – Lawrence Residence Hall**

St. Cloud, MN

- Restoration/Renovation  
Analysis of 1905 Residence Hall for ADA and Program Changes

**Tufts University**

Medford, MA

- Computer and Engineering Laboratories Expansion Studies  
26,000 sf

**University of Maine – Aubert Hall**

Orono, ME

- Renovation of Chemistry Building

**University of Massachusetts**

Worcester, MA

- Computer Center  
9,000 sf

**University of Minnesota**

St. Paul, MN

- Amundson Hall
- Morrill Hall Plaza – Garden, Garage, and Telecommunications Center,  
113,0900 sf
- Teaching and Research Center,  
120,000 sf
- Center for Transportation Studies & University Police Headquarters:  
Office/Teaching – 26,000 sf  
Police Facility – 13,000 sf  
Parking Garage – 690 cars

**University of New England – University Campus**

Biddeford, ME

- Campus Master Plan and Conference Center

**University of New England – Westbrook College Campus**

Portland, ME

- Campus Master Plan

**University of New Hampshire**

Durham, NH

- Biochemistry Research Laboratory
- Fume Hood/Safety Studies,  
100,000 sf

**University of Southern Maine – The Biosciences Institute and The Informational Sciences Institute**

Portland, ME

- Renovation and New Construction

**William Mitchell College of Law**

St. Paul, MN

- Law Library – 67,000 sf  
Campus Master Plan DocID:





**SECTION 5**

**SOLID WASTE**

**SECTION 5**  
**SOLID WASTE**

**5.0 Overview**

The University of Maine System has existing provisions for handling solid wastes at the University of Southern Maine Portland Campus, which will be extended to the proposed Bioscience Institute.

The University is classified as a low generator of hazardous and special wastes. The firms under contract to handle hazardous and special wastes are as follows:

<b>Firm</b>	<b>Type of Wastes</b>
Stericycle	Biomedical Waste
Phillips Services	Hazardous Wastes
Phillips Services	Printing Shop
TSI	PCB Ballast

The Bioscience Institute is not anticipated to add any hazardous or special wastes to the current waste stream. The new Bioscience Institute is anticipated to generate 375 lbs. of biomedical waste on a monthly basis.

Waste Management of Maine is under contract with the University for collection and disposal of solid waste. Solid waste is taken to Regional Waste Systems where it is incinerated.

Last year the Portland Campus generated 650,725 pounds of solid waste including 34,900 pounds of construction/demolition debris. Of the total solid waste, 341,419 pounds or 52% was recycled. Regional Waste Systems incinerated 297,440 pounds. The remaining, unrecycled solid waste was hauled by Waste Management for disposal at RWS landfill in Norridgewock.

The proposed project will increase solid waste generation by a small increment, as outlined in this section.

A dumpster will be provided at the service entrance to each facility for the collection of solid waste.

**5.1 Stumps and Grubbing Debris**

There are no forested areas within the proposed project site that are required to be cleared. There are select trees that will be required to be removed. The total volume of stumps and grubblings that will be generated by this project is estimated to be less than 50 cy. All stumps will be either chipped or hauled to a licensed processing or disposal facility.

## 5.2 Construction Debris

Construction debris will be placed in 30 cy. containers for transport and disposal at the Turnkey Landfill in Rochester. The debris has been estimated as follows:

Total floor area = 57,000 s.f.  
Estimated construction debris = 57,000 s.f. @ 10 cy/1,500 s.f. = 380 cy

Therefore, the total estimated construction debris as a result of this projects is 380 cy.

## 5.3 Demolition Debris

Demolition debris associated with the Bioscience Institute has been estimated by the project architect/engineer as follows:

Pavement/Sidewalk Removal	900 c.y.
Loading Dock Removal	30 c.y.
<b>Total</b>	<b>930 c.y.</b>

The proposed project does not require any demolition work, which will require special disposal. The existing bituminous pavement within the parking lot will be reclaimed for use on the project or within another suitable site. All other demolition debris numbers anticipated to be inert materials (concrete, soil, etc).

Any demolition debris that is encountered during the building construction or site work, which requires special disposal, will be placed in containers for transport and disposal at the Turnkey Landfill in New Hampshire.

## 5.4 Biomedical Waste

The generation of biomedical waste from the Bioscience Institute is estimated to average 375 lbs./month.

## 5.5 Recycling

The University of Maine System is aggressive in their recycling. As outlined in the overview to this section, it is anticipated the recycling of waste from the new facilities will meet or exceed the 52% recycling rate experienced elsewhere in the Campus.

## 5.6 Closure

Contracts for solid waste disposal are on record with the University of Maine System and can be made available for review by the Planning Department.

The computations of waste volumes are estimates only. Contractors should compute the waste volumes separately and not rely on estimates provided in this section.

**SECTION 6**

**ODORS**

## SECTION 6

### ODORS

#### 6.0 Summary

Odors will be generated during the construction phase of the development and during operation of the facilities.

Short-term odors or odors generated during construction include the following:

1. Organic odors from earth moving during construction.
2. Petroleum odors from construction equipment and vehicles used during construction for the project.

These odors will emanate from the site and may be detectable in the immediate vicinity of this development but will dissipate through the air into the atmosphere into negligible amounts. However, the generation of these odors will occur only during the construction phase of the project; therefore, the impact of these odors is considered negligible.

Long term odors or odors generated during day-to-day operation of a facility such as the proposed Bioscience Institute include:

1. Petroleum odors from vehicles traveling to and from the Campus, including faculty, students and visitors
2. Solid waste odors in the dumpster area.

These odors are not likely to be considered offensive, as they are of low intensity and the site is well within the property line of the Campus. Therefore, no provisions for odor control are planned.

## **SECTION 7**

### **WATER VAPOR EMISSIONS (Not Applicable to This Project)**

Large scale water vapor emissions are not anticipated as part of this project.

**SECTION 8**

**SUNLIGHT**

## SECTION 8

### SUNLIGHT

#### 8.0 Overview

The proposed Bioscience Institute is located on the north central portion of the campus as shown on Plate 2 contained in Section 1 of this application. The Bioscience building is located approximately 45 feet from the Falmouth Street right-of-way. The proposed Bioscience Institute is a six (6) story building with a maximum height of 92 feet above existing ground, which will be constructed immediately adjacent to the existing, three (3) story science building.

#### 8.1 Residence Hall Impacts to Adjacent Areas

The existing science building and Bioscience Institute are situated on the southeasterly side of Falmouth Street. Uses on the opposite side of Falmouth Street include multi-storied residential uses many of which are rental structures.

Due to the location of the Bioscience Institute with respect to Falmouth Street, the proposed building will cast shadows towards Falmouth Street during the early morning. These shadows are expected to be cast beyond the University's property lines and into the Falmouth Street right-of-way; however, these shadows will have minimal impact on adjacent buildings.

#### 8.2 Conclusions

Based upon a review of existing and proposed conditions, the proposed Bioscience Institute will result in shadows being created beyond the USM Campus boundary; however, the shadows are not anticipated to cause a significant impact on the surrounding properties.

#### 8.3 References

Site Layout Plan  
Grading and Drainage Plan

#### 8.4 Related Sections

Section 1 – Development Description



**SECTION 9**

**MAINTENANCE OF COMMON FACILITIES OR PROPERTY**

## SECTION 9

### MAINTENANCE OF COMMON FACILITIES OR PROPERTY

#### 9.0 Overview

The addition of the Bioscience Institute will not significantly expand the University of Maine System's infrastructure. The Bioscience institute is an expansion to the existing science building and will be serviced by existing utilities in proximate location of the site.

Maintenance of the school and its grounds will be the responsibility of the University of Southern Maine. This includes mowing and fertilizing the lawns and trimming/pruning of the trees and shrubs. Potholes and other obstructions will be promptly repaired. The parking lots will be kept clean through periodic sweeping, plowing and policing the area. Pavement markings shall be repainted periodically to maintain property traffic circulation and parking space delineation. Damaged curbing and islands will be repaired promptly. Paved areas shall be plowed and sanded as often as necessary to maintain public safety.

Maintenance of the stormwater drain inlets and tributary drainage systems will be the responsibility of University of Southern Maine.

#### 9.1 Related Sections

- Section 4 - Technical Ability
- Section 22 - Stormwater Management
- Section 24 - Erosion & Sediment Control

**SECTION 10**

**AIR EMISSIONS**

## SECTION 10

### AIR EMISSIONS

#### 10.0 Point Source and Non-Point Source

This project consists of construction of an approximate 57,000 sq. ft. total floor area Bioscience Institute split between six (6) floor levels, and reconfiguration of the adjacent parking lot area.

1. Emissions associated with construction vehicles and construction of each project.
2. Emissions associated with the vehicular travel of buses, students, professors, and visitors to the facility.
3. Emissions onsite associated with the heating and cooling system of the facility.

Although the application requests a summary of emissions compounds from both point and non-point sources, it appears this is generally required for "certain types of commercial and industrial developments and solid waste disposal facilities and non-point source emissions deriving from industrial, commercial and governmental developments."

No estimate of emissions for this project has been compiled since no unusual air emissions or high point sources are anticipated.

The USM Portland Campus has heat supplied by a central heat plant facility located on Durham Street, which has a separate MeDEP Air Emissions License.

**SECTION 11**

**NOISE**

## SECTION 11

### NOISE

#### 11.0 Summary

The proposed Bioscience Institute does not fall into the four categories of developments producing minor noise impacts; however, the applicants seek to have the development classified as one creating a minor sound impact. The following information is provided to support the classification as a minor sound impact.

#### 11.1 Type, Source and Location of Noise

The potential sources of noise at the project site will consist of noise generated during construction of the project and noise generated during the operation of the facilities.

- Construction Noise: Noise generated during construction will be limited to normal working hours; typically, 6 a.m. to 7 p.m.
- Facility Operation: The types and sources of facility operation noise would be from visitor and student vehicles entering and exiting the campus throughout the day and events or service vehicles accessing the new facilities.

#### 11.2 Uses, Zoning and Plans

The USM Portland Campus is located in the Residential (R-5) and the Business Community (B-2) zoning districts in the City of Portland. Figure 4 contained in Section 1, is an excerpt from the City of Portland Zoning Map, which shows the location of the project site and current zone districts.

Abutting land uses include residential buildings and other onsite university buildings or educational facilities.

#### 11.3 Noise Protected Location

There are no locations in this area that are considered noise protected locations.

#### 11.4 Minor Nature of Impact

The anticipated sound impact of the proposed project is expected to be compatible with the existing zoning districts and of similar nature to the noise and sound level of existing sound sources currently on the USM Portland Campus.

#### 11.5 Demonstration

A detailed noise assessment has not been conducted for this project. The project will be reviewed by the Portland Planning Board prior to issuance of the building permit. This municipal review will ensure that the project is in compliance with any municipal noise standards.

**SECTION 12**

**SOILS**

## SECTION 12

### SOILS

#### 12.0 Overview

The Bioscience Institute project requires detailed geotechnical evaluations for foundation design, the need for underdrain, the potential need for blasting, and the use of onsite and offsite material specifications.

A Class A High Intensity Soils survey would not add substantial data to the information being obtained through the detailed geotechnical information needed to overcome any onsite soils limitations. The watersheds evaluated for stormwater analysis were relatively small and largely contained onsite. The refinement of a high intensity soil survey would have limited use due to the urbanized nature of the USM Portland Campus; therefore, the applicant requests a waiver for the preparation of the high intensity soil survey for the site. The detailed geotechnical report is contained in Attachment A of this section.

There are no wetland areas located within the USM Portland Campus in its' present configuration.

#### 12.1 Overview of Mapped Soils

The USDA Medium Intensity Soils Map identifies the soils throughout the USM Portland Campus as Hinckley, Hollis, Windsor and Urban (cut and fill) land as shown on Figure 3 contained in Section 1. It should be noted that the Bioscience Institute project is located on the urban (cut and fill) land as mapped on the campus. The typical description of this soil series is as follows:

##### Hinckley Soils

The Hinckley series consists of deep, excessively drained, gently sloping to strongly sloping soils that are moderately coarse textured to coarse textured. These soils formed in glacial outwash deposits on terraces and eskers that commonly are known as "horsebacks". They are in central and northern parts of the county.

A representative profile of a Hinckley soil, in a formerly cultivated area that is now under a cover of white pine, has a layer of organic litter 4 inches thick over a surface layer of very dark grayish-brown gravelly sandy loam 7 inches thick. The upper 3 inches of the subsoil is dark yellowish-brown, very friable sandy loam, and the lower 14 inches is yellowish-brown, very friable to loose gravelly loamy sand to very gravelly sand. The substratum, at a depth of 24 inches, is light olive-brown, loose very gravelly loamy sand.

Depth to bedrock is 5 feet or more. Permeability is very rapid in this soil, and available water capacity is low.

Most areas of Hinckley soils were once cultivated, but many areas are now wooded. Common species are white pine, red pine, gray birch, and white birch.



### Hollis Soils

The Hollis series consists of shallow, somewhat excessively drained, gently sloping to steep, moderately coarse-textured soils that have a few to many outcrops. These soils formed in glacial till, and they are on uplands in the northern and central parts of the county and in the coastal areas.

A representative profile of a Hollis fine sandy loam in a cultivated area has a surface layer of dark-brown fine sandy loam 6 inches thick. The upper 2 inches of the subsoil is yellowish-brown, friable fine sandy loam, and the next 5 inches is dark yellowish-brown, friable fine sandy loam. The lower 5 inches of the subsoil is yellowish-brown, friable fine sandy loam. Schistose bedrock is at a depth of 18 inches.

Depth to bedrock ranges from 12 to 18 inches. Permeability is moderately rapid, and available water capacity is low.

Most areas of Hollis soils were formerly used for farming, but many areas are now wooded. Common species in coastal areas are spruce and balsam fir and in inland areas are oak, beech, maple, birch, eastern hemlock, white pine, and red pine.

### Windsor Soils

The Windsor series consists of deep, excessively drained, nearly level to strongly sloping, coarse-textured soils. These soils formed in glacial outwash deposits. They are on terraces adjacent to many streams and rivers throughout the county.

A representative profile of a Windsor soil in a cultivated area has a surface layer of dark-brown loamy sand 6 inches thick. The upper 9 inches of the subsoil is brown, very friable loamy sand, and the lower 11 inches of the subsoil is light olive-brown, loose loamy sand. The substratum, at a depth of 26 inches, is pale-yellow, loose medium sand.

Permeability is rapid or very rapid in these soils, and available water capacity is low. Depth to bedrock is 5 feet or more.

Many areas of Windsor soils were formerly cultivated, but they are now wooded. Common species are northern hardwoods, white pine, red pine, and eastern hemlock.

### Cut and Fill Land

Cut and fill land (Cu) consists of excavated soil material and bedrock at highway, airport, and building sites that have been redistributed in adjacent areas to depths of from 2 to 15 feet. The material consists of sandy, clayey, silty, cobbly, and gravelly sediment separately or in various combination.

Because of the variability of the material of cut and fill land at any one location, onsite investigation is needed to determine the suitability of this land type for a particular use. Capability unit, unclassified; woodland group, needs onsite investigation; wildlife group, needs onsite investigation.

## 12.2 General Limitations and Methods Used to Overcome the Limitations

The geotechnical limitations of the site will be addressed by the following practices:

- Planning of the project layout.
- Engineering solutions.

The Bioscience Institute was planned to minimize the impact on the existing University campus. The building site plans depict the careful effort undertaken for the facility siting to accommodate the needs of the facility while providing a physical connection with the existing science building and maintaining pedestrian and vehicular access to the site.

The Bioscience Institute is a carefully planned multi-story facility to reduce the building footprint size and impacts on the existing campus facilities.

The following engineering solutions will be utilized to deal with the geotechnical limitations of the site:

- Limitation of Slope: The development area will be graded to meet the relatively flat requirements of this type of project. Containments of fills or excavation around the perimeter of the development will be accomplished by using stable earth slopes and retaining walls. The building will have a ground level finish front elevation of 55.00. The parking lot and service area ranges from elevation 45 to 42. The loading/service area is approximately 6' below existing grade and a retaining wall is proposed along Falmouth Street to contain the cut requirements.
- Wetness and Potential Frost Action: These limitations will be overcome by a properly designed pavement section. The onsite soils are susceptible to frost action. Exposure of the footing and slab subgrade to frost during construction will be avoided. If materials are exposed, the affected soils will be removed, replaced and recompacted. Positive drainage away from areas of construction will be provided during and after construction. Limiting the amount of exposed area and scope compaction of onsite soils are also construction requirements for this project.
- Materials for Pavement Subbase and Base Aggregates: Onsite soils are suitable for general embankment fills and parking area subgrades, given proper moisture conditions, compaction and limited disturbance. Aggregate material used for the subbase and base gravels of the pavement section and along foundation walls will be imported from offsite.

## 12.3 Blasting

See Section 16 of this application for blasting requirements.

## 12.4 Wetlands

There are no wetland areas located within the USM Portland Campus in its' present configuration.

## 12.5 Attachments

Attachment A: Geotechnical Report prepared by Haley & Aldrich for the Residence Hall

**ATTACHMENT A**

**Geotechnical Report**

**REPORT ON  
BIOSCIENCES AND INFOSCIENCES INSTITUTE  
ADDITION TO SCIENCE BUILDING  
UNIVERSITY OF SOUTHERN MAINE  
FALMOUTH STREET  
PORTLAND, MAINE**

**by**

**Haley & Aldrich, Inc.  
South Portland, Maine**

**for**

**Department of Facilities Management  
University of Southern Maine  
Portland, Maine**

**File No. 27021-000  
December 2000**



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15 December 2000  
File No. 27021-000

Department of Facilities Management  
University of Southern Maine  
P.O. Box 9300  
Portland, Maine 04104-9300

Attention: Mr. David Barbour  
Director of Facilities Management

Subject: Biosciences and Infosciences Institute  
Addition to Science Building  
University of Southern Maine  
Falmouth Street  
Portland, Maine

Ladies and Gentlemen:

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Boston  
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San Diego  
California

San Francisco  
California

Washington  
District of Columbia

This report presents the results of our subsurface and foundation investigation for the proposed addition to the science building for the Biosciences and Infosciences Institute at the University of Southern Maine in Portland, Maine. This work was undertaken in accordance with our proposal dated 8 November 2000.

In summary, it is our opinion that the building addition may be supported on spread footings bearing on undisturbed, naturally deposited, inorganic soil, or on compacted structural fill placed after removal of unsuitable soil. In addition, an earth-supported slab-on-grade may be used for the ground floor. Specific recommendations for foundation design and construction considerations are presented below.

**INTRODUCTION**

The Science Building is located on Falmouth Street near the intersection with Oakdale Street as shown on Figure 1, Project Locus. The addition will be on the east side of the Science Building, extending into the present paved parking lot. We understand that the addition will be designed for six stories with a daylight basement. However, you anticipate constructing the basement and two to three stories under an initial phase. The remaining portion will be constructed when funds are available.

## PROPOSED CONSTRUCTION

The addition will have plan dimensions of approximately 65 ft. by 120 ft. with the lowest (ground floor) level at approximately El. 43. Columns will be spaced at approximately 24 ft. on center. Maximum column loads will be on the order of 750 kips. We understand that the existing Science Building is supported on spread footings with a slab-on-grade lowest floor. Site development will include lowering the grade in the present parking lot by up to 5 ft. to accommodate the daylight basement entrance to the addition. There will be a low retaining wall along the Falmouth Street side of the parking lot. Elevations in this report are in feet and referenced to National Geodetic Vertical Datum (NGVD).

## SUBSURFACE EXPLORATIONS

On 13 and 14 November 2000, Maine Test Borings, Inc. drilled seven borings, B1 to B7 at the site at locations shown on Figure 2, Site and Subsurface Exploration Plan. Borings were drilled to depths below ground surface varying from 8.5 ft. to 21.0 ft. All borings, except B4, terminated at refusal. A groundwater observation well was installed in completed boring B1. Haley & Aldrich monitored the borings and prepared the logs included in Appendix A.

Borings were drilled using 2.5-in. I.D. hollow stem augers. Samples were recovered at 5-ft. intervals. Standard Penetration Resistance (N) was measured at each sample interval in the overburden soil in accordance with ASTM test designation D1586.

Haley & Aldrich determined the locations of test borings by taping from existing site features. Haley & Aldrich estimated ground surface elevations at borings by interpolating between ground surface contours shown on the site plan.

The boring logs and related information depict subsurface conditions and water levels at their specific location at the time of completion. Soil conditions at other locations may differ from conditions at these locations. Also, the passage of time may result in a change in groundwater conditions at the exploration locations.

## SUBSURFACE CONDITIONS

The borings encountered three principal soil units overlying bedrock at the site: topsoil, fill, and glacial till. A 0.2 to 0.3-ft. thick layer of asphalt pavement was encountered in borings in the parking area. Encountered thickness and generalized descriptions of the soil units are presented below in order of increasing depth below ground surface.

**Topsoil** – Topsoil consists of dark brown, loamy Organic Soil with roots and sand. Encountered thickness varied from 0.1 ft. to 0.5 ft.

**Fill** – Fill consists of medium dense to dense, brown, medium to fine SAND; to stiff, brown-gray, mottled, clayey SILT, with varying amounts of coarse sand, gravel and silt. Encountered thickness varied from 0.2 ft. to 6.9 ft.

**Glacial Till** – Glacial till consists of dense to very dense, brown, silty SAND with cobbles and boulders. Encountered thickness varied from 4.8 ft. to 19.0 ft.

Refusal in the borings, anticipated to be bedrock, was encountered at depths below ground surface varying from 8.5 ft. to 20.2 ft. However, glacial till typically contains boulders, which could result in boring refusals

A 3.4-ft. thick pocket of dark brown to black Organic Soil with roots and wood was encountered below the fill in boring B6.

Water was encountered in the borings at depths below ground surface varying from 18.8 ft. to 20.6 ft. Water was encountered in the observation well installed in B1 at a depth of 11.1 ft. below ground surface approximately 20 hours after installation. Observations of water level were made over a relatively short period of time and may not reflect the stabilized groundwater level. In addition, water levels at the site will vary with season, precipitation, temperature and construction activity in the area. Therefore, water levels during and following construction may vary from those measured in the observation well and borings.

## **RECOMMENDATIONS FOR FOUNDATION DESIGN**

### **Recommended Foundation Type and Design Criteria**

The topsoil, fill and bituminous concrete (asphalt) are not considered suitable for support of the building addition. All topsoil, fill and bituminous concrete should be removed from within the addition area.

We recommend that the building addition be supported on spread footings bearing on the naturally deposited, inorganic soil (glacial till), or on compacted structural fill placed after removal of unsuitable soil.

We recommend that footings be proportioned for an allowable bearing stress equal to 2,600 lbs. per sq. ft. multiplied by the least lateral dimension of the footing in feet, up to a maximum of 8,000 lbs. per sq. ft. All foundations should be at least 2 ft. wide.

Exterior footings should be founded at least 4.5 ft. below the lowest adjacent ground surface exposed to freezing. Interior footings should be founded a minimum of 1.5 ft. below the floor slab.

Footings adjacent to the existing building should bear at the same elevation as the existing footings.

Compacted structural fill supporting footings should extend laterally from the footings to at least the limits defined by 1 horizontal to 1 vertical lines sloped outward and downward from points located at least 2 ft. horizontally beyond the bottom edges of the footings.

At the recommended bearing stress, we anticipate that foundation settlement will be less than 1 in. We anticipate that more than half of this settlement will occur during the construction period. We anticipate that settlement of this magnitude is acceptable. However, Symmes, Maini & McKee Associates, Inc., should determine final acceptability of settlement.

#### **Ground Floor Slab**

We recommend that lowest level floor slab be designed as an earth-supported slab-on-grade bearing on a minimum of 6 in. of compacted structural fill. All topsoil, fill and bituminous pavement should be removed from within the building limits prior to placing fill. All fill placed below the floor slab for raise-in-grade should consist of compacted structural fill.

Although groundwater was not observed above the proposed lowest floor level, we recommend that a perimeter foundation drain and an underslab drain be constructed on the outside of the foundation walls and below the slab of the lower level of the addition. Typical drain details are shown in Figure 3.

The perimeter drain and underslab drain may be designed for gravity discharge, if available. If gravity discharge is used, provisions should be made to prevent reversal of flow and backup of discharge in case of a severe storm or other event. Alternatively, discharge from the system may be accomplished by pumping. In order to provide for backup discharge, the system should be designed to pump from at least two sumps, one at opposite ends of the basement. The pumps should have emergency electric power available in the event of a power failure. We recommend that the discharge from each sump be designed for a flow of 25 gal. per min. Normal damp-proofing measures should be provided for basement walls and slab.

#### **Seismic Design Considerations**

We recommend that the building be designed according to the seismic requirements of the latest edition of the BOCA National Building Code. The site coefficient,  $S$ , is 1.0; the effective peak velocity-related acceleration coefficient,  $A_v$ , is 0.10; and the effective peak acceleration,  $A_a$ , is 0.10.



### **Lateral Foundation Loads**

We recommend that lateral loads be resisted by bottom friction on footings and that a coefficient of friction equal to 0.35 be used for footings bearing on soil. If this does not

provide sufficient lateral resistance, we will consider the problem in more detail, to take into account other factors.

### **Lateral Soil Pressure**

We recommend that the lower level foundation walls which are restrained at the top and backfilled on the outside be designed to resist a lateral earth pressure calculated on the basis of an equivalent fluid unit weight of 55 lbs. per cu. ft. This fluid unit weight assumes an at-rest earth pressure coefficient of 0.45, a free-draining granular backfill and an effective drainage system.

### **Backfill Materials**

Structural fill used below foundations and floor slabs and for backfill adjacent to walls should consist of sandy gravel to gravelly sand. It should be free of organic material, loam, trash, snow, ice, frozen soil and other objectionable material, and should conform to the following gradation:

<u>Sieve Size</u>	<u>Percent Finer by Weight</u>
3 in.	100
No. 4	30 to 90
No. 40	10 to 50
No. 200	0 to 8

Compacted structural fill should be placed in layers not exceeding 8 in. in loose measure and compacted by self-propelled vibratory equipment at the approximate optimum moisture content to a dry density of at least 95 percent of the maximum dry density, as determined in accordance with ASTM Test Designation D1557. In confined areas, the loose layer thickness should be reduced to 6 in. and compaction performed by hand-guided equipment.

Compacted structural fill on the outside of the foundation walls should extend laterally a minimum of 2 ft. from the wall. Backfill beyond this limit on the outside of the building may consist of common fill. The top 12-in. of fill on the exterior of the building should consist of low permeability material to minimize water infiltration next to the building. Grading should provide for runoff away from the building.

Common fill may consist of inorganic mineral soil that can be placed in layers not exceeding 12 in. in thickness and compacted with a minimum of two systematic passes of the equipment placing the fill.

### **Pavement Section**

The recommended pavement sections for roads and parking areas are as follows:

#### Roads and Automobile Parking Areas

- 3 in. bituminous concrete, placed in two layers
- 15 in. sand or gravel subbase course

Subbase course materials should conform to the following gradations:

Sand or Gravel (Maine DOT Standard Specification, Highways and Bridges; Section 703.06b, Type E)

<u>Sieve Size</u>	<u>Percent Finer by Weight</u>
6 in.	100
¼ in.	25-100
No. 40	0-50
No. 200	0-7

Note: Compacted structural fill may be substituted for gravel subbase course)

Fill required below the pavement section may consist of common fill. Common fill should be placed in layers not exceeding 12 in. in thickness and compacted to a dry density of at least 92 percent of maximum dry density, as determined in accordance with ASTM Test Designation D1557. Subbase course material should be placed in maximum 8-in. thick loose lifts and compacted at approximately optimum moisture content to a dry density of at least 92 percent of maximum dry density, as determined in accordance with ASTM Test Designation D1557.

It should be noted that the subgrade soils are considered frost-susceptible. Therefore, pavement roughness due to non-uniform frost movement may occur. To eliminate such non-uniform frost movement would require approximately 4.5 ft. of structural fill subbase. However, it is common practice to tolerate seasonal movement to avoid the high cost of the added thickness of subbase.

## **CONSTRUCTION CONSIDERATIONS**

### **General**

The primary purpose of this section of the report is to comment on items related to excavation, earthwork, and related geotechnical aspects of proposed construction. It is written primarily for the engineer having responsibility for preparation of plans and specifications. Since it identifies potential construction problems related to foundations and earthwork, it will also aid personnel who monitor the construction activity. Prospective contractors for this project must evaluate the construction problems on the basis of their own knowledge and experience in the Portland, Maine area, and on the basis of similar projects in other localities, taking into account their proposed construction methods, procedures, equipment and personnel.

### **Excavation, Lateral Support, and Control of Water**

We anticipate that foundation excavation can be accomplished with sloped open excavation through the overburden soils. Some sloughing and raveling should be anticipated in temporary slopes. Temporary excavations should be made in accordance with all OSHA and other applicable regulatory agency requirements.

We anticipate that perched groundwater may be encountered at proposed bearing level of the footings. We anticipate that groundwater can be controlled by open pumping from sumps. In general, groundwater and water from runoff and other sources should be controlled by methods, which prevent disturbance of bearing surfaces or adjacent soils and allow construction in-the-dry.

### **Subgrade Preparation**

The subgrade soil is susceptible to disturbance from construction traffic. Equipment and personnel should not be permitted to travel across exposed footing bearing surfaces or exposed slab subgrades. Any subgrade areas that are disturbed should be recompacted or excavated and replaced with compacted structural fill prior to placing of concrete. Subgrades should be protected against freezing temperatures if exposed during construction. Final excavation to subgrade should be performed using equipment with smooth-edge buckets.

### **Construction Monitoring**

The foundation recommendations contained herein are based on the known and predictable behavior of a properly engineered and constructed foundation. Monitoring of the foundation construction by personnel qualified by training and experience is recommended to enable the geotechnical engineer to keep in contact with procedures and techniques used in construction.

Department of Facilities Management  
University of Southern Maine  
15 December 2000  
Page 8

## LIMITATIONS OF RECOMMENDATIONS

This report has been prepared for specific application to the subject project in accordance with generally accepted geotechnical engineering practices. In the event that any changes in the nature, design or location of the addition are planned, the conclusions and recommendations contained in this report should not be considered valid, unless the changes are reviewed and the conclusions of this report modified or verified in writing.

The recommendations presented herein are based in part upon the data obtained from the referenced test borings. The nature and extent of variations from that disclosed by the explorations may not become evident until construction. If variations then appear evident, it will be necessary to reevaluate the recommendations of this report.

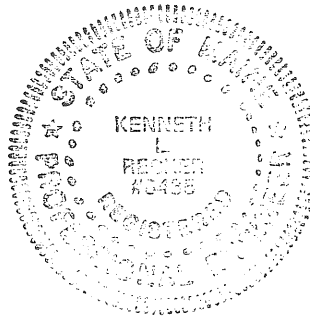
We recommend that we be provided with the opportunity for a general review of final design and specifications in order to determine that our earthwork and foundation recommendations have been interpreted and implemented in the design and specifications as they were intended.

It has been a pleasure to work with you on this project. Please do not hesitate to contact us if you have any questions or require additional information.

Sincerely yours,  
HALEY & ALDRICH, INC.



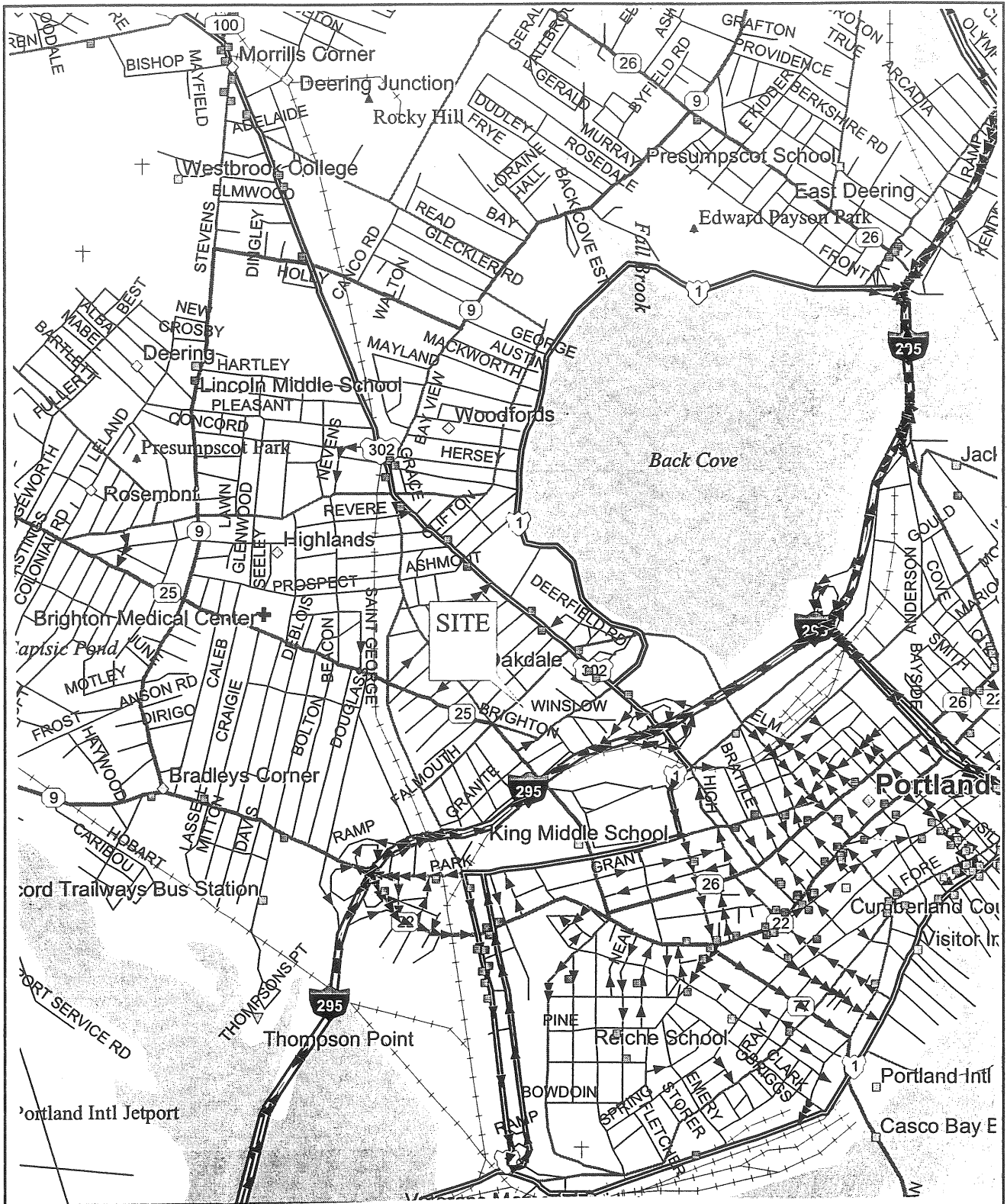
Kenneth L. Recker, P.E.  
Vice President



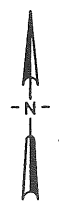
### Enclosures:

- Figure 1 - Project Locus
- Figure 2 - Site and Subsurface Exploration Plan
- Figure 3 - Typical Foundation Drain Details
- Appendix A - Logs of Test Borings

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FILE NO. 27021-000



SITE COORDINATES: N43°39'45", W70°16'39"

"MAP FROM DELORME'S STREET ATLAS USA, FREEPORT, MAINE"



UNDERGROUND  
ENGINEERING &  
ENVIRONMENTAL  
SOLUTIONS

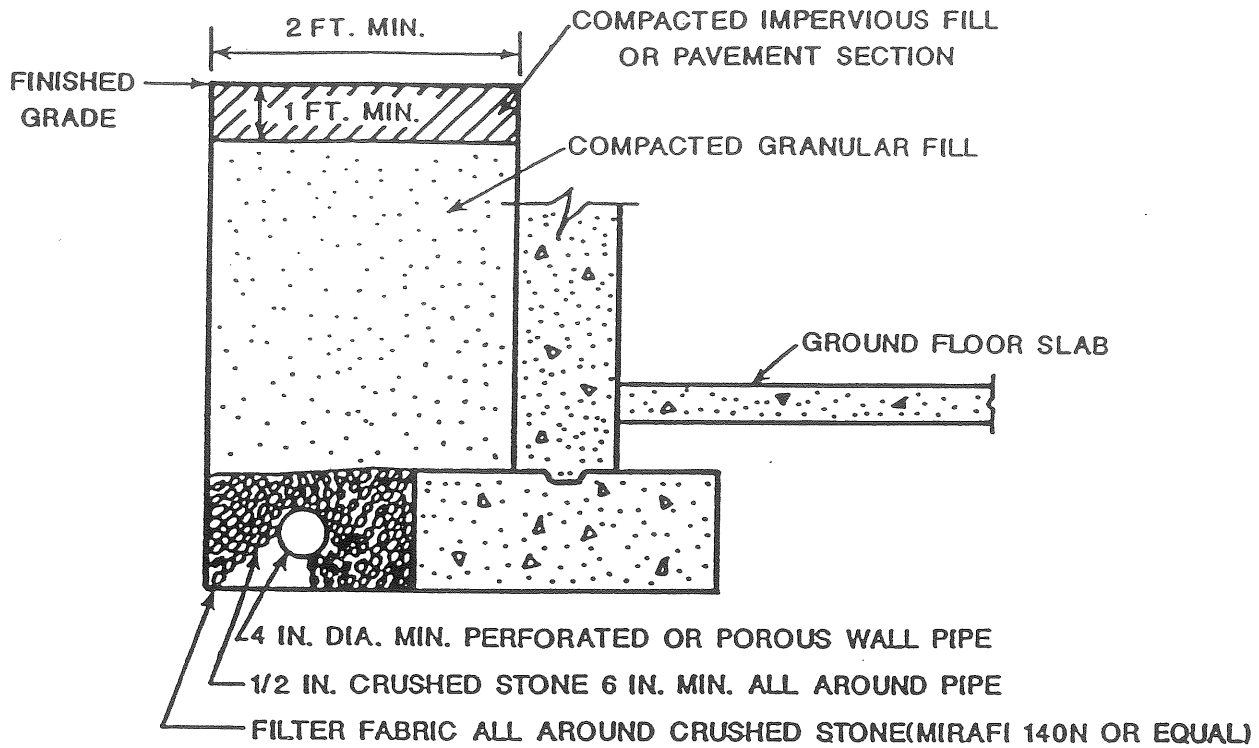
BIOSCIENCES AND INFOSCIENCES INSTITUTE  
ADDITION TO SCIENCE BUILDING  
UNIVERSITY OF SOUTHERN MAINE  
PORTLAND, MAINE

PROJECT LOCUS

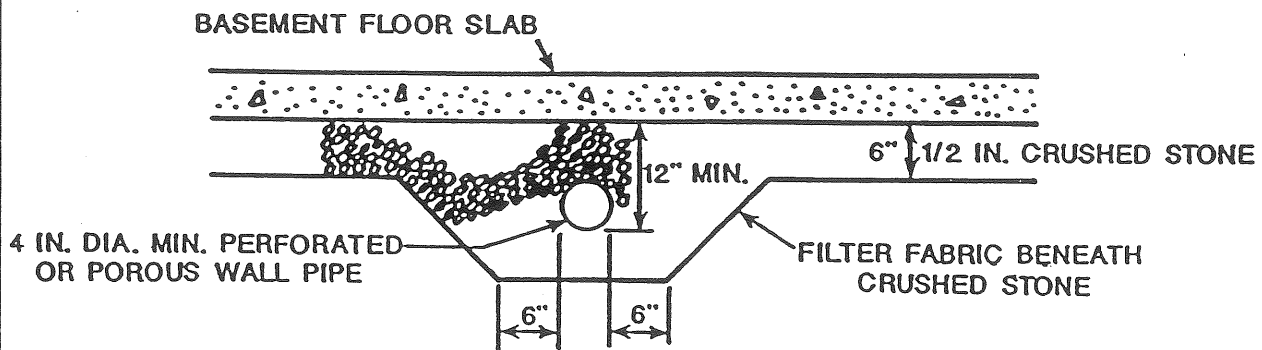
APPROX. SCALE: 1:25,000

DECEMBER 2000

FIGURE 1



**TYPICAL PERIMETER DRAIN DETAIL**



**TYPICAL UNDERSLAB DRAIN DETAIL**

**NOTES:**

1. PIPE INDICATED SHALL BE POROUS WALL CONCRETE OR SCHEDULE 40 PVC MIN.

FILE NO. '021-000



BIOSCIENCES AND INFOSCIENCES INSTITUTE  
 ADDITION TO SCIENCE BUILDING  
 UNIVERSITY OF SOUTHERN MAINE  
 PORTLAND, MAINE

UNDERGROUND  
 ENGINEERING &  
 ENVIRONMENTAL  
 SOLUTIONS

TYPICAL FOUNDATION DRAIN DETAILS

APPROX. SCALE: 1:25,000

DECEMBER 2000

FIGURE 3

**APPENDIX A**  
**Logs of Test Borings**



# TEST BORING REPORT

Boring No. **B1**

Project Addition to Physical Science Building-USM Portland, Maine  
 Client University of Southern Maine  
 Contractor Maine Test Borings, Inc.

File No. 27021-000  
 Sheet No. 1 of 1  
 Start 13 November 2000  
 Finish 13 November 2000

Type	Casing	Sampler	Barrel	Drilling Equipment and Procedures
Inside Diameter (in.)	HSA	S		Rig Make & Model: Mobile B-53
Hammer Weight (lb.)	2.5	1 3/8		Bit Type: Cutting Head
Hammer Fall (in.)		140	-	Drill Mud:
		30	-	Casing:
				Hoist/Hammer: Winch Automatic Hammer

Driller M. Coffin  
 H&A Rep. M. Snow  
 Elevation 49.9  
 Datum NGVD  
 Location See Plan

Depth (ft.)	SPT*	Sample No. & Rec. (in.)	Sample Depth (ft.)	Well Diagram	Elev./Depth (ft.)	USCS Symbol	Visual-Manual Identification and Description (Density/consistency, color, GROUP NAME, max. particle size**, structure, odor, moisture, optional descriptions, geologic interpretation)	Gravel		Sand			Field Test						
								% Coarse	% Fine	% Coarse	% Medium	% Fine	% Fines	Dilatancy	Toughness	Plasticity	Strength		
0					49.6		-ASPHALT-												
	8	S1	0.5		0.3		Medium dense, brown coarse to fine SAND, trace gravel, trace silt, dry												
	12	14"	2.5		49.1														
	7				0.8		-FILL-		5	5	50	40							
	9						Medium dense, brown silty fine SAND, trace coarse sand, trace fine gravel, dry												
					46.9														
					3.0														
5	13	S2	5.0			SM	Very dense, brown silty SAND, mps 3/4", damp, well bonded	5	5	5	5	50	30						
	23	24"	7.0																
	29																		
	26						-GLACIAL TILL-												
10	13	S3	10.0			SM	Very dense, rust-brown to brown silty SAND, Weathered rock from 11.4-11.8 ft., wet, mps 3/4"	5	5	5	5	50	30						
	28	14"	12.0																
	45																		
	27																		
					37.2														
					12.7		-BEDROCK-												
					37.1		Auger refusal at 12.8 ft.												
					12.8		Bottom of Exploration at 12.8 ft.												

Water Level Data					Sample Identification		Well Diagram			Summary					
Date	Time	Elapsed Time (hr.)	Depth (ft.) to:			O Open End Rod	Riser Pipe	Screen	Filter Sand	Cuttings	Grout	Concrete	Bentonite Seal		
			Bottom of Casing	Bottom of Hole	Water									Overburden (lin. ft.)	Rock Cored (lin. ft.)
11/14/00	0850	20	12.0		11.1	T Thin Wall Tube							12.8		3S
		IN WELL				U Undisturbed Sample									
						S Split Spoon									
						G Geoprobe									

Field Tests: Dilatancy: R-Rapid, S-Slow, N-None  
 Toughness: L-Low, M-Medium, H-High  
 Plasticity: N-Nonplastic, L-Low, M-Medium, H-High  
 Dry Strength: N-None, L-Low, M-Medium, H-High, V-Very High

\*SPT = Sampler blows per 6 in. \*\*Maximum particle size is determined by direct observation within the limitations of sampler size.

Note: Soil identification based on visual-manual methods of the USCS as practiced by Haley & Aldrich, Inc.

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# TEST BORING REPORT

Boring No. B2

Project Addition to Physical Science Building-USM Portland, Maine  
 Client University of Southern Maine  
 Contractor Maine Test Borings, Inc.

File No. 27021-000  
 Sheet No. 1 of 2  
 Start 13 November 2000  
 Finish 13 November 2000

	Casing	Sampler	Barrel	Drilling Equipment and Procedures	
Type	HSA	S		Rig Make & Model: Mobile B-53	
Inside Diameter (in.)	2.5	1 3/8		Bit Type: Cutting Head	
Hammer Weight (lb.)		140	-	Drill Mud:	
Hammer Fall (in.)		30	-	Casing:	
				Hoist/Hammer: Winch Automatic Hammer	

H&A Rep. M. Snow  
 Elevation 52.6  
 Datum NGVD  
 Location See Plan

Depth (ft.)	SPT*	Sample No. & Rec. (in.)	Sample Depth (ft.)	Well Diagram	Elev./Depth (ft.)	USCS Symbol	Visual-Manual Identification and Description (Density/consistency, color, GROUP NAME, max. particle size**, structure, odor, moisture, optional descriptions, geologic interpretation)	Gravel		Sand			Field Test						
								% Coarse	% Fine	% Coarse	% Medium	% Fine	% Fines	Dilatancy	Toughness	Plasticity	Strength		
0	3	S1	0.0	NO WELL INSTALLED	52.1	OL/OH	Dark brown loamy Organic soil with sand					5	95						
10	10	22"	2.0		0.5		-TOPSOIL-	5	5	20	40	25	5						
23	23							Dense, brown medium to fine SAND, some coarse sand, little gravel, trace silt, dry											
13	13							-FILL-											
						49.6													
						3.0													
5	13	S2	5.0					Dense, brown silty SAND, mps 3/4", dry			5	5	65	25					
20	20	24"	7.0					-GLACIAL TILL-											
23	23																		
30	30																		
10	10	S3	10.0				SM	Dense, brown silty SAND, mps 3/4", wet			5	10	10	50	25				
20	20	18"	12.0																
20	20																		
25	25																		

Water Level Data						Sample Identification		Well Diagram		Summary												
Date	Time	Elapsed Time (hr.)	Depth (ft.) to:			O Open End Rod	T Thin Wall Tube	U Undisturbed Sample	S Split Spoon	G Geoprobe	Riser Pipe	Screen	Filter Sand	Cuttings	Grout	Concrete	Bentonite Seal	Overburden (lin. ft.)	Rock Cored (lin. ft.)	Samples	5S	
			Bottom of Casing	Bottom of Hole	Water																	
11/13/00	1500	0	20.0	20.2	20.2																	

Field Tests: Dilatancy: R-Rapid, S-Slow, N-None Plasticity: N-Nonplastic, L-Low, M-Medium, H-High  
 Toughness: L-Low, M-Medium, H-High Dry Strength: N-None, L-Low, M-Medium, H-High, V-Very High

\*SPT = Sampler blows per 6 in. \*\*Maximum particle size is determined by direct observation within the limitations of sampler size.

Note: Soil identification based on visual-manual methods of the USCS as practiced by Haley & Aldrich, Inc.

**TEST BORING REPORT**

**Boring No. B2**  
 File No. 27021-000  
 Sheet No. 2 of 2

Depth (ft.)	SPT*	Sample No. & Rec. (in.)	Sample Depth (ft.)	Well Diagram	Elev./Depth (ft.)	USCS Symbol	Visual-Manual Identification and Description <small>(Density/consistency, color, GROUP NAME, max. particle size**, structure, odor, moisture, optional descriptions, geologic interpretation)</small>	Gravel		Sand			Field Test				
								% Coarse	% Fine	% Coarse	% Medium	% Fine	% Fines	Dilatancy	Toughness	Plasticity	Strength
15	10 25 24 27	S4 18"	15.0 17.0				Dense, brown silty SAND, mps 3/4", wet, well bonded	10	5	30	30	25					
20	<del>5/2</del>	S5 1"	20.0 20.2		32.4 20.2	SM	Dense, brown silty SAND, wet  -GLACIAL TILL- Bottom of Exploration at 20.2 ft. Split Spoon Refusal at 20.2 ft.	10	5	30	30	25					

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\*SPT = Sampler blows per 6 in. \*\*Maximum particle size is determined by direct observation within the limitations of sampler size.

NOTE: Soil Identification based on visual-manual methods of the USCS as practiced by Haley & Aldrich, Inc.



**TEST BORING REPORT**

Boring No. B3  
 File No. 27021-000  
 Sheet No. 2 of 2

Depth (ft.)	SPT*	Sample No. & Rec. (in.)	Sample Depth (ft.)	Well Diagram	Elev./Depth (ft.)	USCS Symbol	Visual-Manual Identification and Description <small>(Density/consistency, color, GROUP NAME, max. particle size**, structure, odor, moisture, optional descriptions, geologic interpretation)</small>	Gravel		Sand			Field Test					
								% Coarse	% Fine	% Coarse	% Medium	% Fine	% Fines	Dilatancy	Toughness	Plasticity	Strength	
15	15 40 48 65	S4 24"	15.0 17.0			SM	Very dense, brown-gray silty SAND, mps 3/4", wet  -GLACIAL TILL-	10	10	5	55	20						
					34.5 19.2		Auger refusal at 19.2 ft. on probable Bedrock Bottom of Exploration											

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\*SPT = Sampler blows per 6 in. \*\*Maximum particle size is determined by direct observation within the limitations of sampler size.  
 NOTE: Soil identification based on visual-manual methods of the USCS as practiced by Haley & Aldrich, Inc.



**TEST BORING REPORT**

**Boring No. B4**  
 File No. 27021-000  
 Sheet No. 2 of 2

Depth (ft.)	SPT*	Sample No. & Rec. (in.)	Sample Depth (ft.)	Well Diagram	Elev./Depth (ft.)	USCS Symbol	Visual-Manual Identification and Description  (Density/consistency, color, GROUP NAME, max. particle size**, structure, odor, moisture, optional descriptions, geologic interpretation)	Gravel		Sand			Field Test			
								% Coarse	% Fine	% Coarse	% Medium	% Fine	% Fines	Dilatancy	Toughness	Plasticity
15	28 35 45 78	S4 18"	15.0 17.0			SM SM	Very dense, silty SAND, wet  -GLACIAL TILL-	10	5	10	45	30				
20	48 90	S5 10"	20.0 22.0		32.0 21.0	SM	Very dense, gray-brown silty SAND, mps 3/4", damp	10	5	10	45	30				
							Bottom of Exploration at 21.0 ft. No refusal									

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\*SPT = Sampler blows per 6 in. \*\*Maximum particle size is determined by direct observation within the limitations of sampler size.  
 NOTE: Soil identification based on visual-manual methods of the USCS as practiced by Haley & Aldrich, Inc.



# TEST BORING REPORT

Boring No. **B5**

Project Addition to Physical Science Building-USM Portland, Maine  
 Client University of Southern Maine  
 Contractor Maine Test Borings, Inc.

File No. 27021-000  
 Sheet No. 1 of 2  
 Start 14 November 2000  
 Finish 14 November 2000

	Casing	Sampler	Barrel	Drilling Equipment and Procedures	
Type	HSA	S		Rig Make & Model: Mobile B-53	
Inside Diameter (in.)	2.5	1 3/8		Bit Type: Cutting Head	
Hammer Weight (lb.)		140	-	Drill Mud:	
Hammer Fall (in.)		30	-	Casing:	
				Hoist/Hammer: Winch Automatic Hammer	
				H&A Rep. M. Snow	
				Elevation 51.8	
				Datum NGVD	
				Location See Plan	

Depth (ft.)	SPT*	Sample No. & Rec. (in.)	Sample Depth (ft.)	Well Diagram	Elev./Depth (ft.)	USCS Symbol	Visual-Manual Identification and Description (Density/consistency, color, GROUP NAME, max. particle size**, structure, odor, moisture, optional descriptions, geologic interpretation)	Gravel		Sand			Field Test						
								% Coarse	% Fine	% Coarse	% Medium	% Fine	% Fines	Dilatancy	Toughness	Plasticity	Strength		
0	2	S1	0.0	NO WELL INSTALLED	51.7		-GRASS-												
	2	10"	2.0		0.1		Loose, brown medium to fine SAND, little coarse sand, silt, trace gravel												
	4						-FILL-												
	6																		
						49.5													
						2.3		Brown silty medium to fine SAND, little coarse sand, trace gravel, slightly bonded											
						49.3													
						2.5													
5	2	S2	5.0					Loose, brown medium to fine SAND, little coarse sand, little gravel, trace silt											
	3	12"	7.0					-FILL-											
	2																		
	3																		
						44.8													
						7.0													
10	2	S3	10.0				SM	Loose, brown silty SAND, mps 3/4", damp, bonded	5	10	30	30	25						
	3	15"	12.0				-GLACIAL TILL-												
	2																		
	2																		
					37.8														
					14.0														

Water Level Data						Sample Identification			Well Diagram			Summary							
Date	Time	Elapsed Time (hr.)	Depth (ft.) to:			O	T	U	S	G	Riser Pipe	Screen	Filter Sand	Cuttings	Grout	Concrete	Bentonite Seal	Overburden (lin. ft.)	Rock Cored (lin. ft.)
			Bottom of Casing	Bottom of Hole	Water														
11/14/00	0715	0	19.5	19.5	19.2														4S

Field Tests: Dilatancy: R-Rapid, S-Slow, N-None Plasticity: N-Nonplastic, L-Low, M-Medium, H-High  
 Toughness: L-Low, M-Medium, H-High Dry Strength: N-None, L-Low, M-Medium, H-High, V-Very High

\*SPT = Sampler blows per 6 in. \*\*Maximum particle size is determined by direct observation within the limitations of sampler size.

Note: Soil identification based on visual-manual methods of the USCS as practiced by Haley & Aldrich, Inc.

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**TEST BORING REPORT**

**Boring No. B5**  
 File No. 27021-000  
 Sheet No. 2 of 2

Depth (ft.)	SPT*	Sample No. & Rec. (in.)	Sample Depth (ft.)	Well Diagram	Elev./Depth (ft.)	USCS Symbol	Visual-Manual Identification and Description <small>(Density/consistency, color, GROUP NAME, max. particle size**, structure, odor, moisture, optional descriptions, geologic interpretation)</small>	Gravel		Sand			Field Test			
								% Coarse	% Fine	% Coarse	% Medium	% Fine	% Fines	Dilatancy	Toughness	Plasticity
15	18 20 60 93	S4 14"	15.0 17.0			SM	Very dense, brown silty SAND, mps 3/4", wet, bonded  -GLACIAL TILL-	5	10	30	30	25				
					32.3 19.5		Auger refusal at 19.5 ft. on probable Bedrock Bottom of Exploration									

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\*SPT = Sampler blows per 6 in. \*\*Maximum particle size is determined by direct observation within the limitations of sampler size.  
 NOTE: Soil identification based on visual-manual methods of the USCS as practiced by Haley & Aldrich, Inc.





# TEST BORING REPORT

Boring No. **B6**

Project Addition to Physical Science Building-USM Portland, Maine  
Client University of Southern Maine  
Contractor Maine Test Borings, Inc.

File No. 27021-000  
Sheet No. 1 of 1  
Start 13 November 2000  
Finish 13 November 2000  
Driller M. Coffin  
H&A Rep. M. Snow

	Casing	Sampler	Barrel	Drilling Equipment and Procedures
Type	HSA	S		Rig Make & Model: Mobile B-53
Inside Diameter (in.)	2.5	1 3/8		Bit Type: Cutting Head
Hammer Weight (lb.)		140	-	Drill Mud:
Hammer Fall (in.)		30	-	Casing:
				Hoist/Hammer: Winch Automatic Hammer

Elevation 48.4  
Datum NGVD  
Location See Plan

Depth (ft.)	SPT*	Sample No. & Rec. (in.)	Sample Depth (ft.)	Well Diagram	Elev./Depth (ft.)	USCS Symbol	Visual-Manual Identification and Description (Density/consistency, color, GROUP NAME, max. particle size**, structure, odor, moisture, optional descriptions, geologic interpretation)	Gravel					Sand			Field Test					
								% Coarse	% Fine	% Coarse	% Medium	% Fine	% Fines	Dilatancy	Toughness	Plasticity	Strength				
0				NO WELL INSTALLED	48.2		-ASPHALT-														
					0.2			Medium dense, brown medium to fine SAND, little coarse sand, trace gravel, no odor													
	12	S1	0.5		47.6																
	11	16"	2.5		0.8																
	11							-FILL-													
	10							Stiff, brown-gray mottled clayey SILT, trace coarse to medium sand, and fine gravel (Reworked)													
								-FILL-													
5	2	S2	5.0		43.3			Soft, dark brown-black Organic soil, rootlets, wood, organic odor						10	90	S	N/A	N	N		
	1	24"	7.0		5.1																
	2							-ORGANIC DEPOSIT-													
	2																				
					39.9		Auger encountered resistance at ~8.5 ft., probable cobbles														
					8.5																
10	16	S3	10.0			SM	Dense, brown silty SAND, mps 3/4", bonded	5	10	10	55	20									
	16	16"	12.0																		
	19						-GLACIAL TILL-														
	28																				
					36.0		-Probable BEDROCK-														
					12.4																
					35.8		Auger refusal at 12.6 ft.														
					12.6		Split spoon refusal at 12.6 ft. Bottom of Exploration														

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Water Level Data				Sample Identification			Well Diagram			Summary										
Date	Time	Elapsed Time (hr.)	Depth (ft.) to:			O	T	U	S	G	Riser Pipe	Screen	Filter Sand	Cuttings	Grout	Concrete	Bentonite Seal	Overburden (lin. ft.)		Samples
			Bottom of Casing	Bottom of Hole	Water													12.6	12.6	
11/13/00	1130	0	12.6	12.6	DRY													12.6	Rock Cored (lin. ft.)	3S
																	<b>Boring No. B6</b>			

Field Tests: Dilatancy: R-Rapid, S-Slow, N-None      Plasticity: N-Nonplastic, L-Low, M-Medium, H-High  
 Toughness: L-Low, M-Medium, H-High      Dry Strength: N-None, L-Low, M-Medium, H-High, V-Very High

\*SPT = Sampler blows per 6 in.      \*\*Maximum particle size is determined by direct observation within the limitations of sampler size.

**Note: Soil identification based on visual-manual methods of the USCS as practiced by Haley & Aldrich, Inc.**

**TEST BORING REPORT**

**Boring No. B7**

Project Addition to Physical Science Building-USM Portland, Maine  
 Client University of Southern Maine  
 Contractor Maine Test Borings, Inc.

File No. 27021-000  
 Sheet No. 1 of 1  
 Start 13 November 2000  
 Finish 13 November 2000  
 Driller M. Coffin  
 H&A Rep. M. Snow

	Casing	Sampler	Barrel	Drilling Equipment and Procedures
Type	HSA	S		Rig Make & Model: Mobile B-53
Inside Diameter (in.)	2.5	1 3/8		Bit Type: Cutting Head
Hammer Weight (lb.)		140	-	Drill Mud:
Hammer Fall (in.)		30	-	Casing:
				Hoist/Hammer: Winch Automatic Hammer

Elevation 45.1  
 Datum NGVD  
 Location See Plan

Depth (ft.)	SPT*	Sample No. & Rec. (in.)	Sample Depth (ft.)	Well Diagram	Elev./Depth (ft.)	USCS Symbol	Visual-Manual Identification and Description <small>(Density/consistency, color, GROUP NAME, max. particle size**, structure, odor, moisture, optional descriptions, geologic interpretation)</small>	Gravel					Sand			Field Test					
								% Coarse	% Fine	% Coarse	% Medium	% Fine	% Fines	Dilatancy	Toughness	Plasticity	Strength				
0				NO WELL INSTALLED	44.8		-ASPHALT-														
	15	S1	0.5			0.3		Dense, brown medium to fine SAND, some coarse sand, little coarse to fine gravel													
	24	14"	2.5					-FILL-													
	31																				
	22																				
						42.1															
						3.0															
5	13	S2	5.0				SM	Very dense, brown silty SAND, well bonded, damp, mps 3/4"	5	5	10	5	50	25							
	15	14"	7.0																		
	50/.2																				
					39.1		-GLACIAL TILL-														
					6.0		-WEATHERED ROCK-														
					38.9		-GLACIAL TILL-														
					6.2																
					37.3																
					7.8		-WEATHERED BEDROCK-														
					36.6																
					8.5		Bottom of Exploration at 8.5 ft. Auger Refusal on Bedrock														

Water Level Data				Sample Identification			Well Diagram			Summary									
Date	Time	Elapsed Time (hr.)	Depth (ft.) to:			O	T	U	S	G	Riser Pipe	Screen	Filter Sand	Cuttings	Grout	Concrete	Bentonite Seal	Summary	
			Bottom of Casing	Bottom of Hole	Water													Overburden (lin. ft.)	Samples
11/13/00					DRY													8.5	2S

Field Tests: Dilatancy: R-Rapid, S-Slow, N-None      Plasticity: N-Nonplastic, L-Low, M-Medium, H-High  
 Toughness: L-Low, M-Medium, H-High      Dry Strength: N-None, L-Low, M-Medium, H-High, V-Very High

\*SPT = Sampler blows per 6 in.      \*\*Maximum particle size is determined by direct observation within the limitations of sampler size.

**Note: Soil identification based on visual-manual methods of the USCS as practiced by Haley & Aldrich, Inc.**

**SECTION 13**

**WATER SUPPLY**

## SECTION 13

### WATER SUPPLY

#### 13.0 Overview

The University of Southern Maine Portland Campus has public water supplied by the Portland Water District. Water mains ranging from 6" to 20" in diameter are located along the public right-of-way surrounding the USM campus. The water main within Falmouth Street is 6" in diameter according to the distribution maps available from the Portland Water District. This public main is intended to be used for both fire protection and domestic water supply for the new Bioscience Institute.

The average daily flow for the proposed Bioscience Institute is expected to be approximately 42,000 gallons per day. This information was provided by Symmes, Maini & McKee Associates, architects/engineers for the Bioscience Institute project.

The Bioscience Institute building will be fully sprinklered with external sprinkler connections for fire protection. The final building sprinkler plans will be submitted for approval to the City of Portland Fire Department and Portland Water District prior to construction of the Bioscience Institute building. The available fire flows for the site will be based upon field studies conducted by the Portland Water District and Symmes, Maini & McKee Associates in accordance with NFPA Standards.

#### 13.1 Utility Review

The Portland Water District has been contacted regarding the projected water usage requirements for the Bioscience Institute building, service connections to the watermain and request for ability to serve the new facility. A letter from the Portland Water District has not yet been received and will be forwarded to the City of Portland Planning Department upon receipt.

#### 13.2 Attachments

Letter from Symmes, Maini & McKee Associates to Portland Water District, dated December 20, 2000.

#### 13.3 Cross Reference

Section 14 - Wastewater Disposal

**ATTACHMENT A**

**Letter from Symmes, Maini & McKee Associates to Portland Water District, dated December 20, 2000.**



Symmes Maini & McKee Associates

CAMBRIDGE, MA • MINNEAPOLIS, MN

December 20, 2000

Mr. Dave Coffin, PLS  
Portland Water District  
225 Douglass Street  
P.O. Box 3553  
Portland, Maine 04104-3553

Re: University of Maine Portland Campus – Water Supply

Request for "Ability to Serve" letter

SMMA No. 00013.00

Dear Mr. Coffin:

The University of Maine is proposing to construct a six-story building with basement as an expansion to the existing science building located on Falmouth Street. The new expansion will connect to the east side of the existing building and will occupy approximately 9000 S.F. of area.

The proposed building expansion will require an 8-inch fire line service and a 4-inch domestic water line service. Both lines will connect to an existing 6-inch line in Falmouth Street. The estimated average daily water usage for the proposed building expansion is 42,000 GPD.

Our office is requesting an "Ability to Serve" letter from the Portland Water District. This letter should not address the sprinkler system or hydrant locations. The letter will be included with local and state permit applications.

Very truly yours,

Symmes Maini & McKee Associates

A handwritten signature in black ink, appearing to read "J. P. Burke", written in a cursive style.

Joseph P. Burke, PE  
Project Engineer

cc: JAS, BWL, JB(MF)

**SECTION 14**

**WASTEWATER DISPOSAL**

## SECTION 14

### WASTEWATER DISPOSAL

#### 14.0 Overview

The proposed Bioscience Institute building will be served by the City of Portland's combined sewer system. Presently there are existing sewers located within the various street right-of-ways abutting the USM Portland Campus. The sanitary sewer main along Falmouth Street ranges from 18" to 24" in diameter. A new service connection is proposed for the Bioscience Institute which will connect to the Falmouth Street sanitary sewer main.

The average daily flow requirements have been estimated by the project architect/engineer (Symmes, Maini & McKee Associates) for the proposed Bioscience Institute. The total project is estimated to generate 38,000 gpd.

#### 14.1 Municipal Review

The City of Portland Engineering Department and the Portland Water District have been contacted regarding the sanitary sewer system's and Portland wastewater treatment plant ability to serve the proposed development. Letters from the City of Portland Engineering Department and the Portland Water District have not yet been received and will be submitted to the City of Portland Planning Department upon receipt.

#### 14.2 Design Standards

The applicant will require the sewer work to meet infiltration standards as set forth by the American Society of Civil Engineers (ASCE) Manual of Practice No. 9 and FD-5. Construction will be specified to be in accordance with ASCE MOP37 and 60. All design and construction will be in adherence to specifications and standards as set forth by the rules and regulations of the City of Portland and the Portland Water District.

#### 14.3 Attachments

Letter from Symmes, Maini & McKee Associates to City of Portland Engineering Department dated December 20, 2000.

Letter from Symmes, Maini & McKee Associates to Portland Water District dated December 20, 2000.



**ATTACHMENT A**

**Letter from Symmes, Maini & McKee Associates to City of Portland Engineering Department dated December 20, 2000.**

**Letter from Symmes, Maini & McKee Associates to Portland Water District dated December 20, 2000.**



Symmes Maini & McKee Associates

CAMBRIDGE, MA • MINNEAPOLIS, MN

December 20, 2000

Ms. Catherine Staples  
City Engineer  
City of Portland  
55 Portland Street  
City Portland, Maine 04104-3553

Re: University of Maine Portland Campus – Wastewater Supply

---

Request for "Ability to Serve" letter

SMMA No. 00013.00

Dear Ms. Staples:

The University of Maine is proposing to construct a six-story building with basement as an expansion to the existing science building located on Falmouth Street. The new expansion will connect to the east side of the existing building and will occupy approximately 9000 S.F. of area.

Wastewater discharge generated from the existing building ties into a combined sewer line located on campus property adjacent to Falmouth Street. The combined sewer connects to another combined sewer line in Falmouth Street. A new service line, which will collect flows from both the existing building and new expansion, will be extended from underneath the new building and connected to the combined sewer just outside the proposed building footprint on campus property.

The estimated average daily water usage for the proposed building expansion is 42,000 GPD. Based upon this water usage rate, the proposed building expansion is estimated to generate 38,000 GPD of additional sewage.

Our office would like to request a letter from you stating that the town's sewerage system can accommodate additional sewerage flows anticipated from this development. This letter will be included with local and state permit applications.

If you have any questions regarding this project, please feel free to contact me at 617-520-9451. Thank you.

Ms. Catherine Staples  
December 20, 2000

Very truly yours,

**Symmes Maini & McKee Associates**

A handwritten signature in black ink, appearing to read 'J.P. Burke', written over the company name.

Joseph P. Burke, PE  
Project Engineer

cc: JAS, BWL, JB(MF)



Symmes Maini &amp; McKee Associates

CAMBRIDGE, MA • MINNEAPOLIS, MN

December 20, 2000

Mr. Charles McDowell  
Portland Water District  
225 Douglass Street  
P.O. Box 3553  
Portland, Maine 04104-3553

Re: University of Maine Portland Campus – Wastewater Supply

Request for "Ability to Serve" letter

SMMA No. 00013.00

Dear Mr. McDowell:

The University of Maine is proposing to construct a six-story building with basement as an expansion to the existing science building located on Falmouth Street. The new expansion will connect to the east side of the existing building and will occupy approximately 9000 S.F. of area.

Wastewater discharge generated from the existing building ties into a combined sewer line located on campus property adjacent to Falmouth Street. The combined sewer connects to another combined sewer line in Falmouth Street. A new service line, which will collect flows from both the existing building and new expansion, will be extended from underneath the new building and connected to the combined sewer just outside the proposed building footprint on campus property.

The estimated average daily water usage for the proposed building expansion is 42,000 GPD. Based upon this water usage rate, the proposed building expansion is estimated to generate 38,000 GPD of additional sewage.

Our office would like to request a letter from you stating that the city of Portland treatment plant has the capacity to treat the additional sewerage flows anticipated from this development. This letter will be included with local and state permit applications.

If you have any questions regarding this project, please feel free to contact me at 617-520-9451. Thank you.

Mr. Charles McDowell  
December 20, 2000

Very truly yours,

**Symmes Maini & McKee Associates**

A handwritten signature in black ink, appearing to read "Joseph P. Burke". The signature is fluid and cursive, with a long horizontal stroke at the end.

Joseph P. Burke, PE  
Project Engineer

cc: JAS, BWL, JB(MF)

**SECTION 15**  
**GROUNDWATER**

**SECTION 15**  
**GROUNDWATER**

**15.0 Overview**

The University of Southern Maine Portland Campus is not located on a sand and gravel aquifer as mapped by the Maine Geological Survey Department of Conservation Sand and Gravel Aquifer Map 4. An excerpt of the Sand and Gravel Aquifer Map with the project site superimposed is contained as Figure 9 in Section 1 of this application.

The proposed Bioscience Institute's domestic and fire protection water requirements will be supplied by connections to existing water mains on the Campus or adjacent roadway systems. The project will not require the use of groundwater as a source of water supply.

The proposed Bioscience Institute will be served by the existing sewer system and will not require onsite wastewater disposal.

**15.1 Cross References**

Section 13 - Water Supply  
Section 14 - Wastewater Disposal

**SECTION 16**

**BLASTING**



## SECTION 16

### **BLASTING**

#### **16.0 Overview**

The proposed Bioscience Institute building, access drives and parking areas is located on the southerly side of Falmouth Street. Blasting of rock is not anticipated for the construction of the parking field, access drives, utility lines, and building foundation system.

In the event that shallow bedrock is encountered during the construction, a blasting plan identifying all buildings or residences within 500 feet of the blast limits will be prepared and a preblast survey will be performed in accordance with the requirements of Section 16.1.

#### **16.1 Preblast Survey**

The University of Maine System will bid the project to general contractors. In the event that blasting is required for the project, the contractor will be required to prepare a blasting plan and preblast survey. A written report of the preblast survey will be provided to the University of Maine System by the contractor and will be available for review by the City of Portland and MeDEP. A copy of the blasting plan will be submitted to the City of Portland, MeDEP and UMS for review and approval prior to the initiation of the site preparation work. The scope of the blasting plan and preblast survey will be required to conform to the following specifications:

- All structures within a minimum distance of 500 feet from any blasting activity shall be surveyed as part of the preblast survey. The extent beyond the 500 foot minimum shall be determined by the Contractor.
- A blasting plan shall be prepared which addresses:
  - Airblast Limits
  - Ground Vibrations
  - Maximum Peak Particle Velocity
- The blasting plan shall meet criteria established in Chapter 3 (Control of Adverse Effects) in the Blasting Guidance Manual of the United States Department of the Interior Office of Surface Mining Reclamation and Enforcement.
- Provisions and measures to monitor and assure compliance with the blasting plan.
- The blasting plan and preblast survey shall conform to all recommendations of the project geotechnical report and supplemental geotechnical evaluations included in the Contract Documents.

#### **16.2 Blasting**

- A. Blasting, if required, shall be performed only after approval has been given by the Owner for such operations. A preblast survey must be completed prior to any blasting. A copy of the preblast survey and blasting plan prepared by the Contractor

shall be submitted to the Owner prior to the Owner's authorization for blasting. A copy of the blasting plan shall be submitted to the City of Portland and MeDEP for review and approval prior to the start of site preparation.

- B. All blasting shall be performed in accordance with all pertinent provisions of the "Manual of Accident Prevention in Construction," issued by the Associated General Contractors of America, Inc., of the "Construction Safety Rules and Regulations," as adopted by the State Board of Construction Safety, Augusta, Maine, and the Maine Department of Transportation "Standard Specifications" Section 107.12, Use of Explosives. Blasting through the over burden will not be allowed.
- C. The Contractor shall provide the Owner with a blasting log for the work. The blasting log shall contain the following information:
  - 1. Location
  - 2. Time and Date
  - 3. Number of Holes
  - 4. Amount and Type of Explosives Used Per Hole
  - 5. The names of persons, companies, corporations and public utilities that own, lease, or occupy property structures in proximity to the site of the work, who have been contacted and notified of the Contractor's intention to use explosives.
- D. Drilling equipment will be equipped with suitable dust control apparatus which must be kept in repair and used during all drilling operations.

The applicant will require the general contractor or his blasting contractor conduct this work. By requiring the preblast survey to be conducted in this manner, the applicant does not become involved in liability for errors and omissions in the survey.

### 16.3 Related Section

Section 1 – Development Description

Section 12 – Soils

**SECTION 17**

**VISUAL QUALITY**

## SECTION 17

### VISUAL QUALITY

#### **17.0 Existing Conditions**

The project is located on the north central portion of the 26-acre USM Portland Campus in the vicinity of the science building and gymnasium along Falmouth Street. The area between the science building and gymnasium is currently comprised of a small parking area, which serves the transient student population. The Bioscience Institute will be constructed adjacent to the existing science building within a portion of the existing vehicular parking lot. The construction of the Bioscience Institute building will result in the loss of 36 spaces from this parking lot.

#### **17.1 Adjacent and Nearby Land Use**

Figures 1, 2 and 4, contained in Section 1, show the location of the project site. The areas surrounding the site are comprised of university buildings, parking areas, and residential properties along Falmouth Street, opposite the project site.

#### **17.2 Proposed Project**

The proposed Bioscience Institute will consist of a 9,500 s.f. footprint building with a total floor area of approximately 57,000 sq. ft. split between six (6) levels, a new service loading area, and reconstruction of the vehicular parking lot adjacent to Falmouth Street. In addition, the project includes the construction of pedestrian sidewalks to link the new Bioscience Institute to the surrounding University Campus.

The extent and type of development associated with the new Bioscience Institute is consistent with the historical development, which has occurred at the University campus and surrounding areas.

#### **17.2 Mitigative Measures**

The new parking lot will be lowered from the present parking lot elevations to conceal the loading dock area from Falmouth Street and landscaping will be planted along the street frontage to help screen the parking lot from the street. Additional landscaping will be installed around the new building expansion and around electrical equipment.

Existing conditions photographs are contained in Section 1 of this application.

#### **17.3 Related Sections**

Section 1 – Development Description

**SECTION 18**

**BUFFERS**

## SECTION 18

### **BUFFERS**

#### **18.0 Introduction**

There are no wetlands or state regulated areas which require buffers under Site Location Permit Standards.

#### **18.1 Summary**

The University of Southern Maine Portland Campus is located within a densely developed region of the City bounded by I-295 to the south, Forest Avenue and commercial development to the east, and dense residential development along Falmouth Street, Exeter Street, and Chamberlain Avenue to the north and west of the campus. Due to the current configuration of the campus, there are two arterial roadways which essentially traverse through the campus. These roadways include Brighton Avenue/Bedford Street and Deering Avenue. The proposed Bioscience Institute is set within the University's 26 acre campus and will be located adjacent to the existing science building along Falmouth Street.

The proposed Bioscience Institute will be setback approximately 45 feet from Falmouth Street, which is further away from Falmouth Street than the existing science building. No buffer areas are proposed.

#### **18.2 Cross References**

- Section 23 - Visual Quality

**SECTION 19**

**UNUSUAL NATURAL AREAS**

## SECTION 19

### UNUSUAL NATURAL AREAS

#### 19.0 Summary

No unusual areas are known to exist on the project site. In addition, there are no wetland or sensitive resource areas that exist within the USM Portland Campus.

The Maine Natural Areas Program was contacted and informed of the proposed Bioscience Institute. DeLuca-Hoffman Associates, Inc. sought to obtain information on locations of rare, endangered or registered critical areas which the project may impact. A letter dated December 5, 2000 was received from the Maine Natural Areas Program which states that they are not aware of any potential unusual areas that may be impacted. A list of rare plants in the Portland area was provided, although none are known to exist on the site. This letter is attached at the end of this section.

#### 19.1 Attachments

Attached to this section are the following:

- Letter from DeLuca-Hoffman Associates, Inc. to Maine Natural Areas Program, dated December 1, 2000, requesting their assistance in identifying rare and unusual areas.
- Letter of response from Maine Natural Areas Program to DeLuca-Hoffman Associates, Inc. dated December 5, 2000.

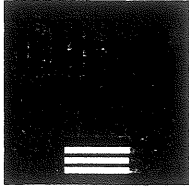
#### 19.2 Related Sections

- Section 15 – Soils
- Section 20 – Wildlife and Fisheries



## **ATTACHMENT A**

- Letter dated December 1, 2000 from DeLuca-Hoffman Associates, Inc. to the Maine Natural Areas Program.
- Response letter from Maine Natural Areas Program, dated December 5, 2000.



DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

778 MAIN STREET  
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SOUTH PORTLAND, MAINE 04106  
TEL. 207 775 1121  
FAX 207 879 0896

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- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

December 1, 2000

Ms. Sarah Holbrook, Information Manager  
Maine Natural Areas Program  
Department of Economic and community Development  
State House Station 59  
Augusta, ME 04333

**Subject: University of Maine System  
USM Portland Campus**

Dear Ms. Holbrook:

The University of Maine System has retained our office to prepare permit applications associated with the construction of the Bioscience Institute at the USM Portland Campus. This project includes a six-story expansion to the existing science building located on Falmouth Street. The building expansion includes an approximate foot print area of 9,500 s.f., which will be constructed primarily within an existing parking area located on the northeast side of the science building.

A DeLorme location map, USGS location map, and USDA Soils map are enclosed for your use.

We are requesting your review to determine if your Department has information concerning possible locations of rare, endangered, or registered critical areas, which may be impacted by this project. DeLuca-Hoffman Associates, Inc. is aware of the fee structure used by the Natural Heritage Program and asks that you invoice our office with your response.

If you have any questions concerning this request, please contact me.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.

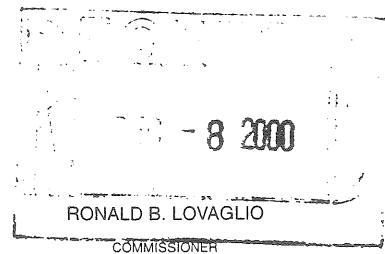
Joseph A. Laverriere, P.E.  
Senior Engineer

JAL/mb/JN2074/Holbrook11-30

Enclosure



STATE OF MAINE  
DEPARTMENT OF CONSERVATION  
159 HOSPITAL STREET  
93 STATE HOUSE STATION  
AUGUSTA, MAINE 04333-0093



ANGUS S. KING, JR.  
GOVERNOR

December 5, 2000

Joseph A. Laverriere  
DeLuca-Hoffman  
778 Main Street, Suite 8  
South Portland, ME 04106

Re: Rare and exemplary botanical features, USM Portland Science Building Expansion, Portland

Dear Mr. Laverriere:

I have searched the Natural Areas Program's Biological and Conservation Data System files in response to your request of December 1, 2000 for information on the presence of rare or unique botanical features documented from the vicinity of the project site in the town of Portland, Maine. Rare and unique botanical features include the habitat of rare, threatened, or endangered plant species and unique or exemplary natural communities. Our review involves examining maps, manual and computerized records, other sources of information such as scientific articles or published references, and the personal knowledge of staff or cooperating experts.

Our official response covers only botanical features. For authoritative information and official response for zoological features you must make a similar request to the Maine Department of Inland Fisheries and Wildlife, 284 State Street, Augusta, Maine 04333.

According to the information currently in our Biological and Conservation Data System files, there are no rare botanical features documented specifically within the project area. This lack of data may indicate minimal survey efforts rather than confirm the absence of rare botanical features. You may want to have the site inventoried by a qualified field biologist to ensure that no undocumented rare features are inadvertently harmed.

If a field survey of the project area is conducted, please refer to the enclosed supplemental information regarding rare and exemplary botanical features documented to occur in the vicinity of the project site. The list may include information on features that have been known to occur historically in the area as well as recently field-verified information. While historic records have not been documented in several years, they



may persist in the area if suitable habitat exists. The enclosed list identifies features with potential to occur in the area, and it should be considered if you choose to conduct field surveys.


This finding is available and appropriate for preparation and review of environmental assessments, but it is not a substitute for on-site surveys. Comprehensive field surveys do not exist for all natural areas in Maine, and in the absence of a specific field investigation, the Maine Natural Areas Program cannot provide a definitive statement on the presence or absence of unusual natural features at this site.

The Natural Areas Program is continuously working to achieve a more comprehensive database of exemplary natural features in Maine. We would appreciate the contribution of any information obtained should you decide to do field work. The Natural Areas Program welcomes coordination with individuals or organizations proposing environmental alteration, or conducting environmental assessments. If, however, data provided by the Natural Areas Program are to be published in any form, the Program should be informed at the outset and credited as the source.

The Natural Areas Program has instituted a fee structure of \$75.00 an hour to recover the actual cost of processing your request for information. You will receive an invoice for \$75.00 for our services.

Thank you for using the Natural Areas Program in the environmental review process. Please do not hesitate to contact me if you have further questions about the Natural Areas Program or about rare or unique botanical features on this site.

Sincerely,



Emily C. Pinkham  
Information Specialist

Enclosures

# Rare or Endangered Botanical Features in the Project Vicinity

Documented within a four mile radius of the USM Portland Science Building Expansion, Portland.

<u>Scientific Name</u> Common Name	Last Seen	State Rarity	Global Rarity	State Legal Status	Federal Legal Status	Habitat Description
ADLUMIA FUNGOSA ALLEGHENY VINE	1860	S1	G4	T		Wet or recently burned woods, rocky wooded slopes.
ALLIUM CANADENSE WILD GARLIC	1918	S2	G5	SC		Alluvial woods, thickets, and meadows.
ALLIUM TRICOCCUM WILD LEEK	1991	S2	G5	SC		Rich hardwood forests, usually alluvial.
CAREX POLYMORPHA VARIABLE SEDGE	1911	S1	G3	E		In Maine, habitat is between downslope seeps (with horsetails and wetland sedges) and upslope mixed oak/huckleberry forest. Preferred soil type is Deerfield Loamy Sand. All Maine occurrences are from coastal towns where climate is moderated by the ocean.
CHIMAPHILA MACULATA SPOTTED WINTERGREEN	1991	S1	G5	E		Dry woods.
ELYMUS HYSTRIX BOTTLEBRUSH GRASS	1905	S2	G5	T		Rich, rocky, or alluvial deciduous forests.
LONICERA DIOICA MOUNTAIN HONEYSUCKLE	1981	S1	G5	E		Rocky banks, dry woods and thickets.
POTAMOGETON VASEYI VASEY'S PONDWEED	1901	S1	G4	T		Quiet muddy or calcareous waters.
SUAEDA CALCEOLIFORMIS AMERICAN SEA-BLITE	1932	S1	G5	T		Rocky or gravelly saltmarshes and sea-strands.

## STATE RARITY RANKS

- S1** Critically imperiled in Maine because of extreme rarity (five or fewer occurrences or very few remaining individuals or acres) or because some aspect of its biology makes it especially vulnerable to extirpation from the State of Maine.
- S2** Imperiled in Maine because of rarity (6-20 occurrences or few remaining individuals or acres) or because of other factors making it vulnerable to further decline.
- S3** Rare in Maine (on the order of 20-100 occurrences).
- S4** Apparently secure in Maine.
- S5** Demonstrably secure in Maine.
- SH** Occurred historically in Maine, and could be rediscovered; not known to have been extirpated.
- SU** Possibly in peril in Maine, but status uncertain; need more information.
- SX** Apparently extirpated in Maine (historically occurring species for which habitat no longer exists in Maine).

Note: State Ranks determined by the Maine Natural Areas Program.

## GLOBAL RARITY RANKS

- G1** Critically imperiled globally because of extreme rarity (five or fewer occurrences or very few remaining individuals or acres) or because some aspect of its biology makes it especially vulnerable to extirpation from the State of Maine.
- G2** Globally imperiled because of rarity (6-20 occurrences or few remaining individuals or acres) or because of other factors making it vulnerable to further decline.
- G3** Globally rare (on the order of 20-100 occurrences).
- G4** Apparently secure globally.
- G5** Demonstrably secure globally.

Note: Global Ranks are determined by The Nature Conservancy.  
T indicates subspecies rank, Q indicates questionable rank, HYB indicates hybrid species.

## STATE LEGAL STATUS

Note: State legal status is according to 5 M.R.S.A. § 13076-13079, which mandates the Department of Conservation to produce and biennially update the official list of Maine's endangered and threatened plants. The list is derived by a technical advisory committee of botanists who use data in the Natural Areas Program's database to recommend status changes to the Department of Conservation.

- E** ENDANGERED; Rare and in danger of being lost from the state in the foreseeable future, or federally listed as Endangered.
- T** THREATENED; Rare and, with further decline, could become endangered; or federally listed as Threatened.
- SC** SPECIAL CONCERN; Rare in Maine, based on available information, but not sufficiently rare to be considered Threatened or Endangered.
- PE** POSSIBLY EXTIRPATED; Not known to currently exist in Maine; not field-verified (or documented) in Maine over the past 20 years.

## FEDERAL STATUS

- LE** Listed as Endangered at the national level.
- LT** Listed as Threatened at the national level.

---

Please note that species names follow Flora of Maine: A Manual for Identification of Native and Naturalized Vascular Plants of Maine, Arthur Haines and Thomas F. Vining, 1998, V.F. Thomas Co., P.O. Box 281, Bar Harbor, Maine 04069-0281.

Where entries appear as binomials, all representatives (subspecies and varieties) of the species are rare in Maine; where names appear as trinomials, only that particular variety or subspecies is rare in Maine, not the species as a whole.

Visit our web site for more information on rare, threatened and endangered species!  
<http://www.state.me.us/doc/nrimc/mnap/factsheets/mnapfact.htm>

**SECTION 20**

**WILDLIFE AND FISHERIES HABITAT**

## SECTION 20

### WILDLIFE AND FISHERIES HABITAT

#### 20.0 Summary

DeLuca-Hoffman Associates, Inc. contacted the Maine Department of Inland Fisheries and Wildlife, the U.S. Department of the Interior, and the Natural Areas Program in an effort to obtain information on potential impacts to wildlife and fisheries habitat within the project area.

At this point, response letters from the resource agencies have not yet been received and will be submitted to the City of Portland Planning Department upon receipt. Based upon the urbanized nature of the Portland Campus, it is not anticipated the resource agencies will respond with any specific concerns.

#### 20.1 Attachments

Attached to this section are the following:

- Letter from DeLuca-Hoffman Associates, Inc. to the Maine Department of Inland Fisheries and Wildlife, dated December 1, 2000.
- Letter from DeLuca-Hoffman Associates, Inc. to the U.S. Department of the Interior, U.S. Fish and Wildlife Service, dated December 1, 2000.

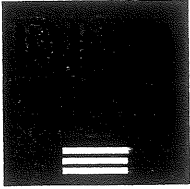
#### 20.2 Related Sections

- Section 15 – Soils
- Section 19 – Unusual Natural Areas



## ATTACHMENT A

- Letter from DeLuca-Hoffman Associates, Inc. to the Maine Department of Inland Fisheries and Wildlife, dated December 1, 2000.
- Letter from DeLuca-Hoffman Associates, Inc. to the U.S. Department of the Interior, U.S. Fish and Wildlife Service, dated December 1, 2000.



DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

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- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

December 1, 2000

Mr. Michael J. Bartlett, Supervisor  
U.S. Department of the Interior, NE Field Office  
Fish & Wildlife Service – Unit 1  
22 Bridge Street  
Concord, NH 03301-4986

**Subject: University of Maine System  
USM Portland Campus**

Dear Mr. Bartlett:

The University of Maine System has retained our office to prepare permit applications associated with the construction of the Bioscience Institute at the USM Portland Campus. This project includes a six-story expansion to the existing science building located on Falmouth Street. The building expansion includes an approximate foot print area of 9,500 s.f., which will be constructed primarily within an existing parking area located on the northeast side of the science building.

A DeLorme location map, USGS location map, and USDA Soils map are enclosed for your use.

We are requesting your review to determine if your office has information concerning the presence of any federally listed (or proposed to be listed) endangered or threatened species which might be impacted by these projects.

If you have any questions concerning this request, please contact me.

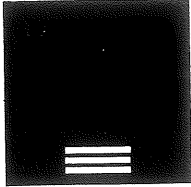
Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.

Joseph A. Laverriere, P.E.  
Senior Engineer

JAL/mb/JN2074/Bartlett11-30

Enclosure



DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

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- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

December 1, 2000

Mr. Norman Trask, Deputy Commissioner  
Department of inland Fisheries & Wildlife  
State House Station 41  
Augusta, ME 04333

**Subject: University of Maine System  
USM Portland Campus**

Dear Mr. Trask:

The University of Maine System has retained our office to prepare permit applications associated with the construction of the Bioscience Institute at the USM Portland Campus. This project includes a six-story expansion to the existing science building located on Falmouth Street. The building expansion includes an approximate foot print area of 9,500 s.f., which will be constructed primarily within an existing parking area located on the northeast side of the science building.

A DeLorme location map, USGS location map, and USDA Soils map are enclosed for your use.

We are requesting your review to determine if your Department has information concerning threatened, endangered, or special status wildlife or fisheries habitat, which may be impacted by this project. We would also appreciate your routing this request to the Department Heads who can aid in this review. If a fee for your review is required, please invoice our office.

If you have any questions concerning this request, please contact me.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.

Joseph A. Laverriere, P.E.  
Senior Engineer

JAL/mb/JN2074/Trask11-30

Enclosure

**SECTION 21**  
**HISTORIC SITES**

## SECTION 21

### HISTORIC SITES

#### 21.0 Overview

The Maine Historic Preservation Commission was contacted by DeLuca-Hoffman Associates, Inc. and asked to determine if any properties of historic, architectural or archaeological significance as defined by the Natural Historic Preservation Act of 1966 at the proposed site. The Maine Historic Preservation Commission stated that there are no properties in the project area of any archeological significance. The request letter from DeLuca-Hoffman Associates, Inc., and the Maine Historic Preservation Commission's response letter, are attached to this section.

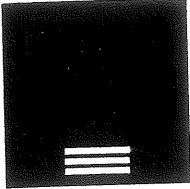
#### 21.1 Attachments

Attached to this section are the following

- Letter dated December 1, 2000 from DeLuca-Hoffman Associates, Inc. to the Maine Historic Preservation Commission
- Response letter from the Maine Historic Preservation Commission dated December 13, 2000.

## **ATTACHMENT A**

- Letter dated December 1, 2000 from DeLuca-Hoffman Associates, Inc. to the Maine Historic Preservation Commission
- Response letter from the Maine Historic Preservation Commission dated December 13, 2000



DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

778 MAIN STREET  
SUITE 8  
SOUTH PORTLAND, MAINE 04106  
TEL. 207 775 1121  
FAX 207 879 0896

- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

December 1, 2000

Mr. Earl G. Shettleworth, Jr.  
Maine Historic Preservation Commission  
State House Station 65  
Augusta, ME 04333

**Subject: University of Maine System  
USM Portland Campus**

Dear Mr. Shettleworth:

The University of Maine System has retained our office to prepare permit applications associated with the construction of the Bioscience Institute at the USM Portland Campus. This project includes a six-story expansion to the existing science building located on Falmouth Street. The building expansion includes an approximate foot print area of 9,500 s.f., which will be constructed primarily within an existing parking area located on the northeast side of the science building.

A DeLorme location map, USGS location map, and USDA Soils map are enclosed for your use.

We are requesting your review to determine if there are any structures or areas on the site with historic, architectural, or archaeological significance as defined by the Natural Historic Preservation Act of 1966.

If you have any questions concerning this request, please contact me.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.

Joseph A. Laverriere, P.E.  
Senior Engineer

JAL/mb/JN2074/Shettleworth11-30



MAINE HISTORIC PRESERVATION COMMISSION  
55 CAPITOL STREET  
65 STATE HOUSE STATION  
AUGUSTA, MAINE  
04333

ANGUS S. KING, JR.  
GOVERNOR

EARLE G. SHETTLEWORTH, JR.  
DIRECTOR

December 13, 2000

Joseph A. Laverriere  
DeLuca-Hoffman Associates, Inc.  
778 Main Street, Suite 8  
South Portland, Maine 04106

Project: MHPC #2554 - Construct Bioscience Institute at USM  
Location: Portland, Maine

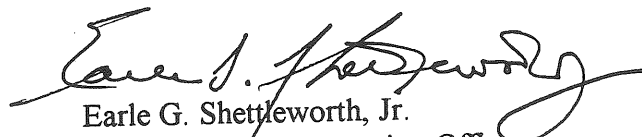
Dear Mr. Laverriere:

In response to your recent request, I have reviewed the information received December 5, 2000 to initiate consultation on the above referenced project.

Based upon the proposed scope of work for this project and the project location, no additional identification efforts are warranted at this time as there is adequate documentation for a finding on historic properties. Our office finds that the subject property and area of potential effects does not contain resources eligible for listing in the National Register of Historic Places. Therefore, I find no historic properties [historic, architectural or archaeological] affected by this project.

Please contact Dana R. Vaillancourt of my staff if you require further assistance in this matter.

Sincerely,

  
Earle G. Shettleworth, Jr.  
State Historic Preservation Officer

EGS/drv



**SECTION 22**

**STORMWATER MANAGEMENT  
PREPARED BY SYMMES, MAINI & McKEE ASSOCIATES**

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## SECTION 22 STORMWATER MANAGEMENT

The project site has an existing stormwater management system, which assimilates runoff generated within the project site and discharges it to a combined sewer and storm drain system in Falmouth Street. The existing onsite system also has a combined sewer and storm drain and consists of a detention basin, storm drains and inlet structures. The onsite system collects runoff generated from building roof drainage, parking areas, landscape areas and sanitary sewer discharge generated within the buildings on campus.

Over time, the drainage system has been changed to accommodate the continuing expansion of the campus. The proposed science building expansion will require further changes to the drainage system. An existing storm drain will be relocated around the new building expansion and additional catch basins will be installed. No changes will be made to the existing detention basin. The parking lot will be lowered to accommodate a new loading dock and basement. Limits of drainage areas will generally remain the same. Impervious area will be reduced, which will reduce the volume of runoff generated within the development. The total impervious area within the limit of work is estimated as follows:

	Existing Conditions	Proposed Conditions
Impervious area	1.04 acres	0.91 acres

Ultimately, the proposed development is expected to reduce the total volume of runoff that discharges to the Falmouth Street combined sewer system.

**SECTION 23**

**FLOODING**

## SECTION 23

### FLOODING

#### 23.0 Overview

The project site is located on the north central portion of the USM Portland Campus on Falmouth Street. The site is characterized by slopes ranging from 3 to 8 percent across the project site. Existing onsite runoff is controlled by an onsite storm drain collection system which discharges to the municipal combined sewer system in Falmouth Street.

There are no existing open waterbodies existing on the site associated with this project. The Federal Emergency Management Agency (FEMA) Flood Zone Map indicates the entire development site is not located within the 100 year flood plain. An excerpt of the FEMA Flood Zone Map showing the project site boundaries is contained as Figure 7 in Section 1 of this application.

A stormwater management study has been prepared for this project which indicates the development will not cause or increase flooding, nor cause an unreasonable flood hazard to any structure.

#### 23.1 Cross Reference

Section 1 - Development Description  
Section 22 - Stormwater Management

**SECTION 24**

**EROSION & SEDIMENT CONTROL  
PREPARED BY SYMMES, MAINI & McKEE ASSOCIATES**

---

**SECTION 24**  
**EROSION AND SEDIMENT CONTROL**

The project site will require minimal erosion and sediment control measures. The site shall be graded such that stormwater runoff generated within the limit of work shall not discharge directly off the property. Instead, it will discharge to onsite catch basins. Haybales will be installed around existing catch basins to limit discharge of sediment into the existing sewer system. When new drainage structures are installed, proper measures shall be taken to also limit discharge of sediment into the existing sewer system.

New catch basins will be equipped with sumps to collect sediment generated within the parking area. Sufficient landscaping shall reduce erosion. Ultimately, project improvements should improve the quality of stormwater runoff being discharged into the Falmouth Street sewer.

**SECTION 25**

**NOTICES**

## SECTION 25

### NOTICES

#### 25.0 Introduction

The abutters to the project were identified based upon tax maps and assessors' records. A list of abutters is enclosed as Attachment A.

Abutters were notified of the University's intent to file a MeDEP Site Location Permit Application for the USM Portland Campus. This notice to the abutters was sent via certified mail. These notices were sent on December 18, 2000. The notice of intent to file a Site Location Permit Application was published in the Portland Press Herald on December 21, 2000. Copies of the notices sent to the abutters and the copy of the published notice is contained in Attachment B.

#### 25.1 Attachments

Attachment A - List of Abutters

Attachment B - Published Advertisement for Notice of Intent to File  
Copies of the Notices Sent to Abutters

Attachment C - Form D of MeDEP Application



**ATTACHMENT A**

**LIST OF ABUTTERS**

**USM PORTLAND, MAINE  
ABUTTER LIST**

<u>Tax Map</u>	<u>Block</u>	<u>Lot</u>	<u>Owner</u>
051	A	1	MARION & ROBERT STEVENS 160 FALMOUTH ST PORTLAND, ME 04102
051	A	2	ELIZABETH SALDANHA 68 EXETER ST PORTLAND, ME 04102
051	A	3	ANNE D CHILDS 64 EXETER ST PORTLAND, ME 04102
051	A	4	VINCENT M CONNOLLY 20 OLD BLACKWOOD WAY FALMOUTH, ME 04105
051	A	5	GAIL M MERILL & GRETCHEN E HOREYSECK 54 EXETER ST PORTLAND, ME 04102
051	A	6	BETTY J & RAPHAEL WUESTHOFF 13 MORNING ST PORTLAND, ME 04101
051	A	7	BEVERLENE B & FESTUS JOYCE 48 EXETER ST PORTLAND, ME 04102
051	A	8	BESSIE B FRAGER 42 EXETER ST PORTLAND, ME 04102

<u>Tax Map</u>	<u>Block</u>	<u>Lot</u>	<u>Owner</u>
051	A	9	JENNIFER L & SEAN L MORRISSEY 38 EXETER ST PORTLAND, ME 04102
051	A	10	MARY ANN DELSANDRO 34 EXETER ST PORTLAND, ME 04102
051	A	11	KATHRYN DAVIS 30 EXETER ST PORTLAND, ME 04102
051	A	12	KELLY C PRUCNAL & GREGG R SHAPIRO 26 EXETER ST PORTLAND, ME 04102
051	D	1	EVELYN J PETERS 208 DEERING AVE PORTLAND, ME 04102
051	D	2	MICHELLE LYNN STONE & IAN C DUNN 204 DEERING AVE PORTLAND, ME 04102
051	E	8	ELIZABETH G CLOUTIER 216 DEERING AVE PORTLAND, ME 04102
051	E	11	JAMES W LINDVALL & CAROL MITCHELL 74 CONCORD ST PORTLAND, ME 04103
051	E	20	CATHERINE & WALTER J CARLSON 461 CAPISIC ST PORTLAND, ME 04102
051	E	21	25 GRANITE STREET REALTY 23 BEACH ST KENNEBUNK, ME 04043

<u>Tax Map</u>	<u>Block</u>	<u>Lot</u>	<u>Owner</u>
051	E	24	PETER L DARVIN & LINDA STIMPSON 29 EXETER ST PORTLAND, ME 04102
081 115	C A	6 1	CITY OF PORTLAND 389 CONGRESS ST PORTLAND, ME 04101
081	D	4	BLAKE W & SUSAN C WHEELER 133 FALMOUTH ST PORTLAND, ME 04102
081	D	7	ELLEN & MICHAEL C MONAGHAN 141 FALMOUTH ST PORTLAND, ME 04102
081	D	8	KAREN E & MATTHEW J MONAGHAN 149 FALMOUTH ST PORTLAND, ME 04102
081	D	10	ELLEN & MICHAEL C MONAGHAN 153 FALMOUTH ST PORTLAND, ME 04102
114 114 114	C D D	1 21 22	FREDERICK J OLSEN PO BOX 3907 PORTLAND, ME 04104
114	D	1	EASTERN ELECTRICAL CORP PO BOX 346 PORTLAND, ME 04112
114A 114A	F F	1 4	OAKHURST DAIRY 364 FOREST AVE PORTLAND, ME 04101
114A 114A 114A	F F F	3 7 8	RACHEL B & WYATT JR GARFIELD 16 FARMS EDGE RD CAPE ELIZABETH, ME 04107
115	B	1	ELIZABETH J O'CONNELL 215 DEERING AVE PORTLAND, ME 04102

<u>Tax Map</u>	<u>Block</u>	<u>Lot</u>	<u>Owner</u>
115	B	5	MARY H & THOMAS H III AYLING 106 BEDFORD ST PORTLAND, ME 04102
115	B	15	JOAN L DOW 23 CHAMBERLAIN AVE PORTLAND, ME 04101
115	B	16	TODD I GETCHELL 29 CHAMBERLAIN AVE PORTLAND, ME 04101
115	B	17	GERTRUDE I & MICHAEL MOSCOWITZ 33 CHAMBERLAIN AVE PORTLAND, ME 04101
115	B	18	GAIL A GELLER 39 CHAMBERLAIN AVE PORTLAND, ME 04101
115	C	1	BRENDA S NICHOLAS 42 CHAMBERLAIN AVE PORTLAND, ME 04101
115	C	2	MARGARET R DONOVAN PO BOX 10165 PORTLAND, ME 04101
115	C	3	ROBERT J RIDGE 34 CHAMBERLAIN AVE PORTLAND, ME 04101
115	C	4	LINDA M POWELL 348 THOMPSON CREEK MALL #PMB101 STEVENSVILLE, MD 21666
115	C	5	MARY L & THEODORE W SOTTERY 24 CHAMBERLAIN AVE PORTLAND, ME 04101

<u>Tax Map</u>	<u>Block</u>	<u>Lot</u>	<u>Owner</u>
115	C	6	DARRYL E HAGAR 20 CHAMBERLAIN AVE PORTLAND, ME 04101
115	C	7	SHIRELY COZENS COZENS 16 CHAMBERLAIN AVE PORTLAND, ME 04101
115	C	8	RUTH E & EDGAR M BAILEY 12 CHAMBERLAIN AVE PORTLAND, ME 04102
115	C	9	BETH H DAGGETT 8 CHAMBERLAIN AVE PORTLAND, ME 04101
115	C	10	THE FAMILY CRISIS SHELTER P O BOX 704 PORTLAND, ME 04104
115	C	11	MICHAEL H & DONNA C GIBBS 3 WASHBURN AVE PORTLAND, ME 04101
116	D	3	KEY BANK OF MAINE ONE CANAL PLAZA PORTLAND, ME 04112
116	D	12	LOON WATCH DEVELOPERS 38 CHAMBERLAIN AVE PORTLAND, ME 04102
116	D	14	NEAL M GERBER PO BOX 6694 PORTLAND, ME 04103
116	D	15	RALPH W E & KATHLEEN C GILES 98 JOY LN GROTON, MA 01450
116	D	17	STEPHEN P & SUSAN C HILTON 19 OLD IRONSIDE RD SCARBOROUGH, ME 04074

<u>Tax Map</u>	<u>Block</u>	<u>Lot</u>	<u>Owner</u>
116	D	19	DORIS M CARMAN RIVER ROAD EAST LIMINGTON, ME 04049
116	D	22	LISBETH FAULKNER 61 FALMOUTH ST PORTLAND, ME 04103
116	D	25	RICHARD N KNUDSEN 21 CASCO TER FALMOUTH, ME 04105
116	D	28	GLADYS JUC 25 FALMOUTH ST PORTLAND, ME 04103
116	D	29	ZARTARINA & ANTRANIK ARTINYAN 141 NOYES ST PORTLAND, ME 04103
117	A	1	3-D REALTY PARTNERS 257 DEERING AVE PORTLAND, ME 04103
117	A	3	SULAN CHAU 98 CODMAN ST PORTLAND, ME 04103
117	A	5	PETER J MANNING 108 BRIGHTON AVE PORTLAND, ME 04102
117	A	7	PAUL B DAILEY 93 FALMOUTH ST PORTLAND, ME 04103
117	A	10	PATRICIA G & PAGE H BURNHAM PO BOX 34 SCARBOROUGH, ME 04070
117	A	14	CHARLTON S SMITH TRUSTEE PO BOX 355 HOLLIS, ME 04042

<u>Tax Map</u>	<u>Block</u>	<u>Lot</u>	<u>Owner</u>
117	A	15	MOIRA E & STEPHEN M LEIGHTON 69 FALMOUTH ST PORTLAND, ME 04103
117	A	21	DORIS M CARMAN RIVER ROAD EAST LIMINGTON, ME 04049



**ATTACHMENT B**

**PUBLISHED ADVERTISEMENT FOR  
NOTICE OF INTENT TO FILE**

**COPIES OF THE NOTICES SENT TO ABUTTERS**

NOTICE OF INTENT TO FILE

Please take notice that University of Maine System, 107 Maine Avenue, Bangor, Maine 04401, Attn: David Early (207) 780-4656.

is intending to file a Site Location of Development permit application with the Maine Department of Environmental Protection pursuant to the provisions of 38 M.R.S.A. §§ 481-490 on or about December 22, 2000

The application is for after-the-fact post 1975 structure area for the existing University Campus and construction of a new six (6) story Bioscience Institute with associated access drive, parking and service area reconstruction adjacent to Falmouth Street.

at the following location: University of Southern Maine Portland Campus on Bedford, Falmouth, Forest and Deering Avenue in Portland, Maine.

A request for a public hearing or a request that the Board of Environmental Protection assume jurisdiction over this application must be received by the Department, in writing, no later than 20 days after the application is found by the Department to be complete and is accepted for processing. Public comment on the application will be accepted throughout the processing of the application.

The application will be filed for public inspection at the Department of Environmental Protection's office in Portland during normal working hours. A copy of the application may also be seen at the municipal offices in Portland, Maine.

Written public comments may be sent to the Department of Environmental Protection, Bureau of Land & Water Quality, 17 State House Station, Augusta, Maine 04333.

**ATTACHMENT C**

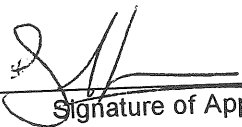
**FORM D OF MeDEP APPLICATION**

**NOTICE CERTIFICATION**

By signing below, the applicant (or authorized agent) certifies that he or she has

1. Published a Notice of Intent to File once in a newspaper circulated in the area where the project site is located within thirty days prior to the filing of the application;
2. Sent by certified mail a completed copy of the Notice of Intent to File to the owners of the property abutting the land upon which the project site is located within thirty days prior to the filing of the application;
3. Sent by certified mail a completed copy of the Notice of Intent to File and filed a duplicate of this application with the town clerk or city clerk of the municipality(ies) where the project is located; and
4. The public informational meeting will be noticed by the City of Portland as part of the delegated review process. The public informational meeting will be held concurrently with the Planning Board public hearing process for local site plan approval. The Planning Board meeting schedule is to be determined by the City and notification will be provided to all abutters in accordance with City Planning established protocol and notification requirements.

Approximately N/A members of the public attended the Public Informational Meeting.

  
\_\_\_\_\_  
Signature of Applicant

December 22, 2000  
Date

Joseph A. Laverriere, P.E., Authorized Agent  
Print name and title of Applicant

If signature is other than that of the applicant, attach letter of agent authorization signed by applicant.

April 21, 2003

Ms. Sarah Hopkins  
City of Portland  
389 Congress Street  
Portland, Maine 04101

Subject: USM Biosciences Research Addition CBL: 114A-A-001

Dear Sarah:

I am enclosing a revised signed landscape and lighting plans for the Biosciences Research Addition which reflects the design developed by Catherine and Duncan Hewitt to create a work of art for the Biosciences Addition as part of the State's Percent for Art program. As I indicated to you last year, the University is required to provide artwork for the new facility as part of the construction of the Addition because State funding has been provided to construct it. After an extensive search, Catherine and Duncan Hewitt of Hollis, Maine, were selected by the Percent for Art Search Committee to create a combination sculpture and landscape to be located to the south of the glass curtain wall of the Addition.

President Patternaude has reviewed and approved the concept and design and the Maine Arts Commission has agreed that the proposed artwork is an appropriate form of art to be funded by the program. A description of the artwork is attached along with the landscape and lighting plan, and light distribution pattern plan. The plan includes all required landscaping to complete the project. Working with Catherine, Duncan, and Leslie Glynn of Symmes Maini McKee Associates, the project architect, we have developed a plan that we feel is appropriate for the Addition and USM campus. The University is excited about this part of the project and feels that it will create an interesting space through which USM employees and visitors can pass on their way to and from classes. If they are so inclined, they can stop and linger in a small courtyard with a stone sitting wall and sculpture or sit on five Victor Stanley benches along the sidewalk in front of the Addition.

When we last discussed this proposed plan change, you indicated that you felt it could be reviewed and approved through a Planning Office staff review process. We have reviewed the proposed plan with Jeff Tarling, the City Arborist, and have incorporated his recommendations and suggestions into the enclosed. Since the planting season is upon us, I would ask that you review the enclosed plans and approve it as soon as possible. I look forward to hearing from you.

I can be reached at 780-4751 if you have questions or need more information.

Sincerely,



David N. Barbour  
Director of Facilities Management

Cc: D. Early  
D. Hewitt  
C. Hewitt  
L. Glynn  
B. Hodgkin  
B. Holmes  
J. Waters  
D. Krichels

Proposal for The University of Southern Maine/ BioScience Institute Percent for Art  
May 14, 2002

It is in the nature of our collaboration to introduce art that may be experienced in varied ways, including sight, touch and movement. We design knowing that relationships change through the day and with the seasons, but provided a strong overall gesture, the space will remain beautiful. At best we feel our work can and should be sensitive to the full implications of the site as it provides a place for the imagination.

The identity of the new Bio Science Research Institute will be dominated by the overriding visual strength of the curved reflective facade of the building's south side. On a non-residential campus that has struggled to establish links between buildings and disciplines, the facade may provide identity, but may neither anchor the building to the campus nor be expressive of the attitude of the people whose passion is at work within the building. Not only is this located on a promontory, but it is also the juncture of major pedestrian traffic. It is an area that is panoramic and exposed to the elements. Pushing into space, the new wing is met by a ground plane that curves and descends towards it creating a naturally defined area. It is here that we see an opportunity.

The site itself would be sculpted as if physically alive. First, we have relocated the sidewalks and sitting wall to allow for a more gracious space. The ground plane has been substantially raised on the parking lot side in order to create a buffer. On the west side the walls will hold back the ground to create a cove-gathering place. Within the cove there is sculpture suspended from shafts that come up through the paved surface. Visually accessible to the entire exterior room and distant sight lines, a tone would be brought to the site. New plantings would respond to existing landscape, at the same time be very different. Flowering crabs extend across the top of the ground plane on one side and larches flank the sidewalk ascending from the student center on the other. Between these there is a bluestem meadow surrounded by a group of deciduous and evergreen plantings creating a backbone that wraps around the cove. The cove could be entered through a grove of trees. Seating would be provided by the stone walls and a row of benches aligned with the the curve of the sidewalk and building.

The parameters of the sculpture will come from an existing wood carving ( see enclosed photograph) which itself had as it's source a child's folded paper snow flake. It is an image that hints at the familiar and the unknowable and at the complex relationship between story telling( fiction) and observation of the natural process. Like a diatom, it is about growth patterns which are geometric, organic, and one of a kind. Deceptively simple,( laid flat this one would be nearly circular,

but folded it provides endlessly complex permutations) it has in it the kind of feeling of being alive which comes when one senses that the parts and the whole have unfolded at the same time. There would be one or two bronze objects either adjacent to each other or one above the other. The largest could be 4' tall by 28" wide by 24" deep. A wrought iron pole would serve to support-suspend- the images. In the end it would honor a state of being which sees the extra-ordinary within the ordinary.

Respectfully submitted,

Catherine and Duncan Hewitt



Dear David,  
 Here is a proposal for our contract. I deleted the lighting budget of 10,000. The October payment secures a price with the foundry. January covers contracts with hardscape contractors. I would like to leave the completion date October 15th, although we fully anticipate completion in the spring....just in case there is a plant that doesn't make the spring dig. Our plant order will go in by December at the latest. Thanks, Catherine

**BUDGET**

<b>TUMBLE COBBLE PAVERS</b>	20,040
<i>*ADDITIONAL PAVERS UNDER BENCHES/PLAZA</i>	3,114
<b>STONE WALLS</b>	9,045
<b>TREE GRATES - 3 4X4'</b>	3,000
<i>* TRASH RECEPTACLES - 2@700</i>	1,400
<b>BENCHES- 5 6' BENCHES</b>	4,000
<b>LANDSCAPE - LOAM, MULCH, PLANTS, LABOR</b> (AMENDMENTS AND EXCAVATION FROM FACILITIES MANAGEMENT)	27,000
<b>SCULPTURE</b>	<u>20,000</u>
	87,599

**PAYMENT SCHEDULE:**

OCTOBER 15, 2002	6,500/signing
JANUARY 15, 2003	10,000/approve installation methods
APRIL 1, 2003	31,169/installation
MARCH 1, 2003	31,170/installation
UPON COMPLETION	8,760/final approval

**SUBCONTRACTOR'S/SOURCES**

- JOZEF CUSTOM BRONZE AND IRON WORKS- SCULPTURE BRIDGEPORT  
CT
- O'DONAL'S NURSURIES- TREES, SHRUBS GORHAM, ME
- VAN BERKUM NURSERY- PERENNIALS DEERFIELD, NH
- NEENAH -TREE GRATES NEENAH, WI
- VICTOR STANLEY INC. -BENCHES, TRASH RECEPTACLES DUNKIRK, MD
- HARDSCAPE- TBD

# CITY OF PORTLAND, MAINE PLANNING BOARD

---

Jaimey Caron, Chair  
Mark Malone, Vice Chair  
Orlando E. Delogu  
Kevin Beal  
Lee Lowry III  
Michael Patterson  
Janice E. Tevanian

February 4, 2004

Dave Early, Director  
Facilities Management  
University of Southern Maine  
96 Falmouth Street  
Portland, ME 04103

Re: Bioscience Phase II, 98 Falmouth Street  
CBL: 114A A001001

Dear Mr. Early:

On December 9, 2003, the Portland Planning Board voted (5-0; Malone absent, Delogu abstaining) to approve the conditional use for the Bioscience Phase II project. The Board also voted (5-0; Malone absent, Delogu abstaining) to approve the site plan and amendment to the Site Location of Development permit subject to the following condition:

- that the applicant submit a revised landscape plan showing the screening of the utility structure on Falmouth St. for City staff review and approval.

The approval includes the addition of three floors to the existing Bioscience building.

The approval is based on the submitted site plan and the findings related to site plan review standards as contained in Planning Report #59-03, which is attached.

Please note the following provisions and requirements for all site plan approvals:

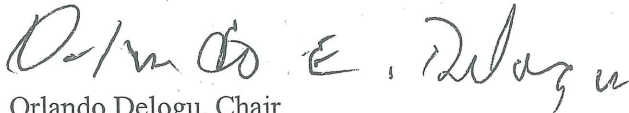
1. Where submission drawings are available in electronic form, the applicant shall submit any available electronic CADD.DXF files with seven (7) sets of the final plans.
2. A performance guarantee covering the site improvements as well as an inspection fee payment of 2.0% of the guarantee amount and 7 final sets of plans must be submitted to and approved by the Planning Division and Public Works prior to the release of the building permit. If you need to make any modifications to the approved site plan, you must submit a revised site plan for staff review and approval.
3. The site plan approval will be deemed to have expired unless work in the development has commenced within one (1) year of the approval or within a time period agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the expiration date.

4. A defect guarantee, consisting of 10% of the performance guarantee, must be posted before the performance guarantee will be released.
5. Prior to construction, a preconstruction meeting shall be held at the project site with the contractor, development review coordinator, Public Work's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the preconstruction meeting.
6. If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)

The Development Review Coordinator must be notified five (5) working days prior to date required for final site inspection. The Development Review Coordinator can be reached at the Planning Department at 874-8632. Please make allowances for completion of site plan requirements determined to be incomplete or defective during the inspection. This is essential as all site plan requirements must be completed and approved by the Development Review Coordinator prior to issuance of a Certificate of Occupancy. Please schedule any property closing with these requirements in mind.

If there are any questions, please contact Sarah Hopkins at 874-8720.

Sincerely,



Orlando Delogu, Chair  
Portland Planning Board

cc: Lee D. Urban, Planning and Development Department Director  
Alexander Jaegerman, Planning Division Director  
Sarah Hopkins, Development Review Services Manager  
Jay Reynolds, Development Review Coordinator  
Marge Schmuckal, Zoning Administrator  
Karen Dunfey, Inspections  
Michael Bobinsky, Public Works Director  
Traffic Division  
Tony Lombardo, Project Engineer  
Eric Labelle, City Engineer  
Jeff Tarling, City Arborist  
Penny Littell, Associate Corporation Counsel  
Lt. Gaylen McDougall, Fire Prevention  
Don Hall, Appraiser, Assessor's Office  
Approval Letter File



UNIVERSITY OF  
**SOUTHERN MAINE**  
Facilities Management

June 24, 2004

Ms. Sarah Hopkins  
City of Portland  
389 Congress Street  
Portland, Maine 04101

Subject: Biosciences Phase II, 98 Falmouth Street

CBL: 114A A001001

Dear Sarah:

As required by paragraph 2 of the Planning Board approval letter dated February 4, 2004, for this project and in response to the Planning Board condition of approval to provide screening of the utility structure on Falmouth Street, I am enclosing a completed Cost Estimate of Improvements to be Covered by Performance Guarantee form and three (3) copies of the landscape drawing for your review. The 7 copies of the final set of plans were delivered to your office April 13, 2004. Also attached is a breakdown of plant materials, quantities, and unit costs. The cost estimate is for the landscaping work along the Falmouth Street side of the building. The only site work anticipated for this project is the landscaping along Falmouth Street. All other work was completed as part of Phase I.

I have asked our architect SMMA to provide me with a disk of the project drawings which I will deliver to your office upon receipt as required in paragraph 1 of the approval letter.

Since this project is being funded by State bond funds, the University is required to expend up to 1% of the construction cost to have an artist create a piece of art work for the building. The Administration has indicated its preference to have the Phase II art work be placed on the Falmouth Street side of the building. It is possible that this might impact the landscape plan as it did in Phase I. At this point we haven't started the Percent for Art process, so I don't know what if any impact the art work might have on the current plan. If it does modify the plan, I will resubmit it for your review and approval.

The landscaping will be done in the spring of 2005 after the construction of the additional floors on the Research Wing has been completed. Please let me know the amount of the inspection fee so I can have a check cut for it and the amount of the performance guarantee so I can arrange to have the letter of credit sent to you.

Give me a call at 780-4751 if you have any questions.

Sincerely,



David N. Barbour  
Director of Facilities Management

Cc: S. Andrews  
D. Early  
D. Schurman



6. SITE LIGHTING	_____	_____	_____	_____	_____	_____
7. EROSION CONTROL						
Silt Fence	_____	_____	_____	_____	_____	_____
Check Dams	_____	_____	_____	_____	_____	_____
Pipe Inlet/Outlet Protection	_____	_____	_____	_____	_____	_____
Level Lip Spreader	_____	_____	_____	_____	_____	_____
Slope Stabilization	_____	_____	_____	_____	_____	_____
Geotextile	_____	_____	_____	_____	_____	_____
Hay Bale Barriers	_____	_____	_____	_____	_____	_____
Catch Basin Inlet Protection	_____	_____	_____	_____	_____	_____
8. RECREATION AND OPEN SPACE AMENITIES	_____	_____	_____	_____	_____	_____
9. LANDSCAPING (Attach breakdown of plant materials, quantities, and unit costs)	_____	_____	_____	_____	_____	\$25,000
10. MISCELLANEOUS	_____	_____	_____	_____	_____	_____
TOTAL:	_____	_____	_____	_____	_____	\$25,000
GRAND TOTAL:	_____	_____	_____	_____	_____	\$25,000

**INSPECTION FEE (to be filled out by the City)**

	<u>PUBLIC</u>	<u>PRIVATE</u>	<u>TOTAL</u>
A: 2.0% of totals:	_____	_____	_____
<u>or</u>			
B: Alternative Assessment:	_____	_____	_____
Assessed by:	_____	_____	_____
	(name)	(name)	

UNIVERSITY OF SOUTHERN MAINE			
CITY OF PORTLAND SITE PLAN APPROVAL			
PLANT SCHEDULE FOR BIOSCIENCE PHASE II, 98 FALMOUTH STREET			
CBL: 114A A001001			
<u>Plant Materials</u>	<u>Botanical Name</u>	<u>Quantity</u>	<u>Unit Cost</u>
<b><u>Deciduous Trees</u></b>			
-Paperbark Maple	Acer Griseum	4	\$500
-Three-Flower Maple	Acer Trifoleum	2	\$266
-Katsura Tree	Cercidiphyllum Japonicum	1	\$275
-Pink Spires Crab	Malus 'Pink Spires'	3	\$200
-'Emerald Pillar' Pin Oak	Quercus Palustris 'Green Pilar'	3	\$246
<b><u>Evergreen Trees</u></b>			
-Oriental Spruce	Picea Orientalis	3	\$425
-Swiss Stone Pine	Pinus Cembra	4	\$264
<b><u>Shrubs</u></b>			
-Shasta Viburnum	Viburnum Placatum Var. Tomentosum ' Shasta'	6	\$34
<b><u>Ground Cover</u></b>			
-Bearberry	Arctostaphylos Uva-Ursi	127	\$14
-Myrtle	Vinca Minor	8 flats	\$36
-Northern Lingonberry	Vaccinium Vitis-Idaea Minus	40	\$40





# UNIVERSITY OF SOUTHERN MAINE

Office of Finance and Administration

DATE: January 18, 2001

TO: Mr. David Early, Executive Director, Facilities Management

FROM: Samuel G. Andrews, Office of Finance and Administration

RE: Biosciences Research Institute Addition

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4484  
FAX (207) 780-4549  
TTY (207) 780-5646

As I understand it, the City of Portland Planning Board and Planning Staff requested some additional information relative to future enrollments at the Portland campus as well as information on USM's parking mitigation efforts. In regard to the Biosciences Research Institute Addition to the Portland Science Building, it should be noted that this is a research facility only and will not have any impact at all on student enrollment.

The Maine Legislature has made two targeted additions to the University of Maine System base budget designed to substantially increase research and development (R&D) activity at the University of Maine and the University of Southern Maine, in particular to strengthen the capacity of both Institutions to compete for Federal grants and contracts. The annual base budget operating increase is \$10 million plus \$2.5 million to cover debt service payments on a \$25 million internal bond issue for a period of 15 years. The general goal is that together the Universities will generate an additional \$40-\$50 million annually in external research funding from the investment base of \$10 million. For USM the additional per year operation investment will be \$2 million directed at research faculty and staff, equipment and materials in the biosciences, information sciences, and advanced materials, primarily electronics. In addition, USM has a total of \$7.7 to build and renovate research laboratories (\$2.7 M from a bond referendum approved by voters in 1998 and \$5M from the campus share of the internal bonding capacity).

Exhibit I presents student headcount enrollment for the Portland campus for the fall, 1998 through the fall, 2000 semester with a projection through the fall, 2020 semester. The table shows Weekend College enrollment (classes offered at 4:00 p.m. and after on Friday through Saturday and Sunday) and Regular Enrollment (classes offered from 8 a.m. on Monday through 4:00 p.m. on Friday). About 20% of the students attending the Portland campus are present on Friday, Saturday and Sunday and not during the week days. The introduction of Weekend College was to provide access to adult learners and to relieve pressure on classrooms, laboratories, and parking resources during the week. The enrollment projection takes into account the projected population trends in southern Maine over the next twenty years; projected high school senior population in Maine as a whole, and the high school senior population in southern Maine, specifically; the projected rate of college attendance among high school seniors; and the post-secondary educational participation rate among adults. The projected rates of increase in student headcount enrollment are noted at the bottom of Exhibit I.

The parking and traffic study carried-out in connection with the permitting of the Science Building Addition was carried out on a Tuesday. Exhibit II shows the number of students registered for Portland campus classes at one hour intervals throughout the week. For example, at 8:00 a.m. on Monday morning there were 248 students registered to take classes at the Portland campus while at 2:00 p.m. on Monday, there were 1,116 students registered to be in class at that time. The

**UNIVERSITY OF SOUTHERN MAINE  
STUDENT ENROLLMENT BY HOUR  
PORTLAND CAMPUS - FALL, 2000**

	<u>Monday</u>	<u>Tuesday</u>	<u>Wednesday</u>	<u>Thursday</u>	<u>Friday</u>	<u>Saturday</u>
8 a.m.	248	11	218	2	41	20
9 a.m.	548	842	534	768	218	436
10 a.m.	595	1,239	568	1,180	287	441
11 a.m.	498	1,270	462	1,194	270	448
Noon	905	1,006	874	914	203	108
1 p.m.	941	1,090	785	976	273	106
2 p.m.	1,116	1,153	839	1,064	273	111
3 p.m.	888	887	689	840	121	73
4 p.m.	1,235	1,182	1,127	1,092	352	36
5 p.m.	1,091	1,083	1,040	1,019	344	0
6 p.m.	1,056	988	1,051	909	345	0
7 p.m.	774	767	798	648	40	0
8 p.m.	736	666	689	527	35	0
9 p.m.	<u>710</u>	<u>617</u>	<u>666</u>	<u>503</u>	<u>16</u>	<u>0</u>
<b>TOTAL</b>	<b><u>11,341</u></b>	<b><u>12,801</u></b>	<b><u>10,340</u></b>	<b><u>11,636</u></b>	<b><u>2,818</u></b>	<b><u>1,779</u></b>

SHUTTLE BUS SCHEDULE  
EFFECTIVE JANUARY 16, 2001  
MONDAY THROUGH FRIDAY

There will be one bus per run in each direction. If all seats are filled, standing will be allowed.

LEAVES PORTLAND

7:25 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4.10  
 4:50  
 5:35  
 6:15  
 7:00  
 8 :30  
 8:45(via Mall Mon-Fri)  
 10:00 (Mon-Thurs)

LEAVES GORHAM

7:15 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4.10  
 4:50  
 5:35  
 6:15  
 7:00  
 8:15 (via Mall Mon-Fri)  
 9:35 (Mon-Thurs)  
 10:20 (Mon-Thurs)

A shuttle bus will run continually from the Marginal Way lot, in Portland, with stops at the Campus Center, Payson Smith Hall and other on-campus locations as needed, and return to the Marginal Way lot at 7:15a.m., Monday through Thursday. **At 8pm, the shuttle that serves Portland Hall will also serve as the Marginal Way shuttle.** The last bus will return to the Marginal Way lot at 10:00pm, Monday through Thursday. **There will be no Marginal Way bus on Fridays and weekends.** **NOTE: VIP driver shift change occurs at approximately 3:30pm.**

**BUS SCHEDULES ARE SUBJECT TO CHANGE EACH SEMESTER AND DURING FINAL EXAMS.**

Dave Early, cont'd

page 3

January 18, 2001

Marginal Way shuttle bus stops are located on the campus and about the location of a shuttle bus stop being instituted at Baxter Boulevard. Also, there will be a more clearly delineated time schedule for the Marginal Way/Baxter Boulevard shuttle and better maintenance of the time schedule to instill confidence in the service on the part of users. Once the construction phase of the Biosciences Addition is completed, the lot between the Science Building and the Sullivan Gymnasium will be returned to use, although there will be a loss of 44 parking spaces due to the Addition.

Over the past two years, the University Community has discussed changes in the scheduling of classes in order to spread the scheduling of classes more evenly over the Monday through Friday period. Through this revamping of the course scheduling, it is hoped that time blocks can be more effectively utilized and will even-out some of the peaks and valleys in student attendance. Exhibit IV, Pages 1 & 2, are the announcement from Provost Joseph S. Wood to USM Faculty and Staff concerning the new class schedule to be implemented effective with the fall, 2001 semester. As indicated in the memo, "implementation should open-up classrooms and also help relieve some parking stress on Tuesdays and Thursdays", the two days that have the highest parking demand.

The use of the three University of Southern Maine shuttle bus services, the invigoration of the Marginal Way parking alternative, the addition of parking spaces at 15 Baxter Boulevard and the implementation of the new class schedule should provide sufficient results to cope with any new parking demand resulting from the addition of the Biosciences Research Institute to the Portland campus. As a purely research oriented facility, the new Addition will not generate any new students for the campus; only research personnel who will be working in the new faculty.

In addition to the items outlined, the University of Southern Maine is and has been an active partner with the Metro to create awareness and use of the Metro as an alternative to bringing a vehicle to the Portland campus. Exhibit V is material shared with the USM Parking and Transportation Review Committee concerning USM's effort with the Metro since March of 2000. From March through August, 2000, the free Metro ride program involved 11,346 riders, and the "50-Cent Ride" program generated 4,517 Metro riders during the September through November period. Through the \$11,000 in financial support being provided to the Metro by USM for the "50-Cent Ride Anywhere, Anytime" program, hopefully, some current drivers to campus will switch and use public transportation.

SGA/c

attachments

Beginning with the Fall 2001 semester the normal scheduling periods will be changed.

All periods on Monday thru Thursday before 4:00 p.m., will be 75 minutes with a 15 minute break between periods. This schedule will be the same for each day.

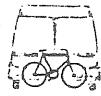
Periods after 4:00 p.m. will accommodate either 75 minute periods or 150 minute (2.5 hours) periods.

Friday will normally consist of two 2.5 hour periods.

PORTLAND		GORHAM	
MTWR	F	MTWR	F
8:45 - 10:00	8:45 - 11:15	8:00 - 9:15	8:00 - 10:30
10:15 - 11:30	11:45 - 2:15	9:30 - 10:45	11:00 - 1:30
11:45 - 1:00		11:00 - 12:15	
1:15 - 2:30		12:30 - 1:45	
2:45 - 4:00		2:00 - 3:15	
4:10 - 5:25		4:10 - 5:25	
5:35 - 6:50		5:35 - 6:50	
(4:10 - 6:40)		(4:10 - 6:40)	
7:00 - 9:30		7:00 - 9:30	

Notes

- 1) A 3 credit class offered in a 75 minute period will be offered either M/W or T/R.
- 2) A 3 credit class offered 3 times a week will be offered M W F during 50 minutes of a single period time block.
- 3) Classes that are 4 credit hours can be offered in a single period time block on M W F, with one meeting per week being 50 minutes.



August 3, 2000

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO Ridership Program

The following is an update on the METRO-USM pilot program:

Background - USM/Free METRO Fare Pilot Program

Since March 2000, we have been working with Craig Hutchinson, Helen Gorgas, and Judy Ryan on creating awareness and trial usage of public transportation via a free METRO ride program. By displaying a USM photo ID card, any member of the USM community rides for free. The METRO has spent over \$1,000 in display advertising in the USM Free Press in promoting the free service. We have received very positive support from Craig and Helen via USM on-campus promotions. In June, our FAST (Forest Avenue Smart Travel) or 15-minute frequency service was introduced. METRO FAST has been a strong complement to the free USM fare program. The accessibility of the METRO bus on Forest Avenue near the campus and higher service frequency have proven to be highly attractive to USM riders.

The pilot program's ridership to date is:

USM METRO trips

March	April	May	June	July	August	Cumulative
1,234	2,024	2,227	1,893	1,909		9,287

## 50¢ Fares for USM. Anytime. Anywhere.



- Show a **USM Photo ID** or a current **USM ID registration card** to a METRO driver and ride for 50¢ (regular fare is \$1). No other USM ID will be accepted.
- Get a free USM Photo ID card at Payson Smith Hall in Room 4.
- Ride any route – anytime, anywhere.
- Use the METRO for your commute to school, work, intown Portland, shopping, medical appointments and for other daily trips.
- Use the METRO Smart KIOSK at the Woodbury Campus Center and at the METRO Pulse for bus time schedule and information.



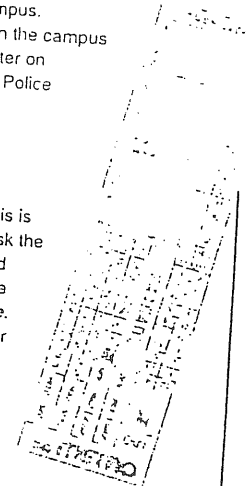
### METRO to USM – #2 or #4 Bus

The #2 bus (Forest Avenue) stops by Back Bay Bicycle on Forest Avenue. It is a short walk up Bedford Street to the campus. The #2 bus stops in front of Pier One for the return intown trip.

The #4 bus (Westbrook/Exit 8) also takes you to the campus. Traveling to the campus, the bus stops on Bedford Street on the campus side. Leaving the campus, the #4 bus stops at the bus shelter on Bedford Street opposite the Campus Center near the USM Police Department building.

### How do I change or transfer to another METRO bus?

You may need to change buses to reach your location. This is called a *transfer*. If you need to change buses, please ask the bus driver for a *transfer* when you first get on the bus and pay for your ride. When you change your bus, please give the transfer to the second bus driver. The *transfer* is free. You do not need to pay when you change buses. On your ride home, you will need to pay another fare. (See picture of transfer at the right.) You can change buses at various METRO bus stops along Congress Street which runs through downtown Portland. You can also change buses at the METRO PULSE located at the Elm Street Garage.

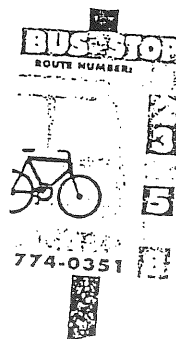


### Where do I wait for the bus?

Please wait for a METRO bus where you see a *bus stop sign* (see left). There are also structures that are METRO bus stops called *bus shelters*.

### Are there special bus fare plans?

- \$30 Monthly Pass: unlimited rides for month purchased
- \$9 TenRide Ticket: 10 rides with no time limit



For information — 774-0351

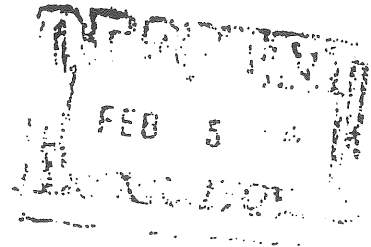


February 1, 2001

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program



For your posting the following is ridership for the program:

Year 2000

March 1,234

April 2,024

May 2,227

June 1,893

July 1,909

August 2,059

**Cumulative Free Rides: 11,346**

September 1,313

October 1,616

November 1,588

December 1,504

January '01 1,734

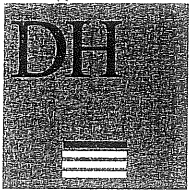
**Cumulative 50-Cent Rides: 7,755**

**Total All Rides (March-January): 19,101**

In the upcoming weeks, we will be actively working with Rodney Mondor to introduce students on the Gorham campus to the METRO, as well as, Chris O'Connor at Portland Hall with Portland campus students..



Attachment



DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

778 MAIN STREET  
SUITE 8  
SOUTH PORTLAND, MAINE 04106  
TEL. 207 775 1121  
FAX 207 879 0896

- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

April 11, 2001.

Ms. Sarah Hopkins, Senior Planner  
Planning and Urban Development  
389 Congress Street  
Portland, ME 04101

**Subject: USM Portland Campus  
Bioscience Building Expansion  
Response to Engineering Review Comments**

Dear Sarah:

Our office along with Symmes, Maini & McKee Associates and the University of Southern Maine have received and reviewed the engineering review comments prepared by Anthony Lombardo (dated February 5, 2001) from the City's Engineering Department and peer review comments prepared by Jeffrey Prebble of Dufresne-Henry (dated January 5, 2001). A summary of each comment and response is provided below:

A. Anthony Lombardo Comment Memo:

Comment 1:

*Public Works is requesting the applicant install a new drain manhole structure in the location where the on site storm drain system connects in the combined sewer system, within the Falmouth Street right of way. Currently, a 10" diameter pipe outfalls the site and connects directly into the combined sewer. A manhole structure will enhance the ability of the City to maintain and repair the system in Falmouth Street.*

Response:

The site plans for the Bioscience Building Expansion have been revised to include the installation of a drain manhole structure as requested (refer to Sheet C4 attached).

Comment 2:

*Public Works is also requesting the applicant specify, as part of the "erosion and sediment control plan", that the site contractor shall clean the entire on site storm drain system, including the sumps of all catch basins. The timing of this work should coincide with the paving of the parking lot.*

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 2

Response:

The erosion and sedimentation control narrative has been revised to specify the cleaning of the onsite storm drain system within the Bioscience Building Expansion project area to be completed as requested. In addition, a special construction note has also been added to Sheet C4.

Comment 3:

*The applicant should contact Carol Merritt of Public Works, prior to beginning of construction, to determine all of the mandatory permits and fees associated with construction activity within the City right of way.*

Response:

The applicant or assigned agent will contact the City prior to construction to ensure all permits and fees are obtained for construction work within the City's right-of-way.

B. Dufresne-Henry Comment Letter

Comment 1:

*Erosion and sedimentation control measures are limited on this project and involve primarily hay bale barriers around catch basins. The parking area adjacent to the building addition will be significantly altered, with a significant amount of materials being moved into and out of the site and onto Falmouth Street. There are no measures planned for a stabilized construction entrance to the site, therefore materials could be transported onto Falmouth Street. Additional safeguards against erosion and sedimentation should be provided around the site. Seeding and mulching should be specified for disturbed areas and a schedule for stabilizing areas once they reach final grades. An implementation schedule should also be provided.*

Response:

The erosion and sedimentation control plan and narrative contained in Section 24 of the MeDEP Site Location Permit Application has been revised to include the following:

- Temporary erosion control measures including hay bales, catch basin inlet sediment sump, stabilized construction entrance, water quality treatment unit, etc.
- Permanent erosion control measures including seeding, landscape vegetation/bark mix, etc. (Refer to Sheet C5).
- Timing and sequence of construction.
- Implementation schedule of erosion control measures.

Refer to Attachment A and revised site plans.

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 3

Comment 2:

*We agree that the proposed development will not significantly alter storm water runoff from the site. However, storm water quality issues have not been addressed as part of the application. There does not appear to be any storm water quality measures presently around this site, and there are none proposed.*

Response:

The storm drain system associated with the Bioscience Institute project has been revised to include a Stormceptor water quality treatment unit designed to achieve 63% TSS removal efficiency for the project area. It should be noted, that the Stormceptor unit will treat the 0.7 acre watershed area associated with the Bioscience Institute parking area, as well as, an additional 1.0 acre watershed tributary to the storm drain system from the Masterton Hall area upgradient of the site.

The Stormwater Management Report, as prepared by Symmes Maini & McKee Associates, contained in Section 22 of the MeDEP Site Location Permit Application has been revised to summarize the water quality measures associated with this project. A copy of the revised Stormwater Management Report is contained in Attachment B of this letter.

Comment 3:

*Several utilities need to be modified to make room for the proposed building expansion. Test pits should be called for in key utility crossings to avoid potential conflicts during construction, and verify clearances. The proposed 6-inch sewer service is shown tying directly into an existing manhole, but no invert data is given. I believe the Public Works Department prefers that services be tied into the sewer main rather than manholes. A new 12-inch CPP is shown being tied into DHM-1 and runs underneath the new concrete seat. The concrete seat detail does not provide an overall depth. Clearances in this area need to be verified. Finally, confirmation letters are needed from Public Works on the sewer capacity and Portland Water District for the water services.*

Response:

The Grading and Utilities Plan has been revised to include a note requiring the contractor to conduct testpits at all utility crossings. In addition, the new sanitary sewer service connection has been revised to tie into the sewer main rather than the manhole.

The detail cross-referencing has been revised on Sheet C3 to correspond with the detail numbering on Sheet C6.

The depth of the concrete seat wall has been defined on Sheet C6. Inverts and elevations at these crossings have been identified on Sheet C4.

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 4

Copies of the revised site plans are appended to this letter. In addition, copies of the ability to serve letter from the Portland Water District is contained in Attachment C. At this point, the ability to serve letter from the City of Portland has not yet been received and will be provided separately upon receipt.

Comment 4:

*A solid waste dumpster is called for in Section 5 of the application. The location of the dumpster is not shown on the plans.*

Response:

There are individual containers within the building for disposal of solid waste/recycling items that are collected by the USM Department of Facilities and consolidated for hauling and disposal offsite.

An exterior dumpster is not proposed for this project. The reference in Section 5 to a dumpster has been removed and an updated copy of Section 5 is contained in Attachment D.

Comment 5:

*No information has been provided on lighting fixtures proposed for the site lighting on this project.*

Response:

Details on the exterior site lighting fixtures are contained in Attachment E.

Comment 6:

*A detail has not been provided for the proposed stonewall retaining walls. The granite curbing detail calls for a 6-inch reveal, City of Portland standard is 7 inches. Details 14 and 18, Concrete Retaining Wall, are redundant.*

Response:

A detail section through the stone retaining wall along Falmouth Street has been added to Sheet C6. The granite curb detail on Sheet C6 has been revised to reflect a 7-inch reveal in accordance with the City of Portland standards. In addition, the redundant concrete retaining wall (detail 16) on Sheet C6 has been eliminated. Also, a bituminous curb detail has been added to Sheet C6.

Comment 7:

*The cover letter states that the construction sequence is provided in Sections 1 and 24 of the application. There is a very brief discussion on Construction Plan in Section 1. The construction sequencing should be further defined.*

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 5

Response:

The construction sequencing has been provided in the revised Section 24 – Erosion and Sedimentation Control Report contained in Attachment A of this letter.

Our office along with representatives from Symmes, Maini & McKee Associates and the University of Southern Maine look forward to meeting with you and the members of staff and the Planning Board to discuss the project further.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.



Joseph A. Laverriere, P.E.  
Senior Engineer

JAL/mb/JN2074/Hopkins2-28

Enclosures

- Attachment A - Revised Section 24 – Erosion and Sedimentation Control Report
  - B - Revised Section 22 – Stormwater Management Report
  - C - Ability to Serve Letters from the Portland Water District
  - D - Revised Section 5 – Solid Waste
  - E - Exterior Site Lighting Details
- Revised Site Plans

c: Dave Early, P.E. – USM  
Joe Burke, P.E. - SMMA

Last winter, the City had notified USM that it would be ending the lease of the Marginal Way parking lot for use as a train station. Since that time, the most recent plans for the train station will not require the use of the parking lot, and the lot is once again available for USM's use.

USM has submitted a parking calculation (see Attachment 2). The Zoning Administrator has determined that they are low by 117 parking spaces. The applicant will therefore be requesting from the Board a shared use permit for those parking spaces.

Also included with the applicant's submission is information regarding the enrollment of the campus, Metro ridership data, and parking locations.

### 3. Building Design

Included as Attachment P-5 are the proposed building elevations for the Bio-Science addition. The applicant's architect will be available at the workshop to discuss the building concepts and floor plans.

### 4. Landscaping

Included as Attachment P-4 is the landscaping plan for the site. Street trees will be placed along the Falmouth Street façade of the building and a more intensive landscaping treatment will be provided along the south elevation of the building, where the building is connected by pathways to the rest of the campus.

### 5. Grading/Utilities

The existing Science Building parking lot will be lowered as part of this proposal, providing a more attractive streetscape along Falmouth Street and access to the east side of the building.

Run off within the parking lot will be collected in a new catchbasin and routed via manholes and stormdrains to the internal stormwater system of the campus.

## Attachments

1. Enrollment Information
2. Parking Requirements per City Ordinance
3. Neighborhood Meeting Information
4. Engineering Response to Comments Raised
5. Memo from Public Works
6. Comments from the Zoning Administrator

## Plans:

1. Schematic Landscaping Plan
2. Existing Conditions
3. Grading/Utility Plans
4. Landscaping Plans
5. Building Elevations



# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4200 FAX (207) 780-4800  
TTY (207) 780-5646

September 10 , 2001

Dear USM Neighbor:

We'd like to invite you to a neighborhood meeting at 7 p.m., Thursday, September 27 in the fourth floor conference room of the Glickman Family Library, Portland.

USM President Richard L. Pattenaude will update you on the Bioscience Research Institute and the upcoming bond issue for a community education center on the Portland campus. He also will allow plenty of time to address any questions or concerns.

As you may recall, this meeting is part of our commitment to build a better relationship with you and other Portland neighbors. During the series of meetings last winter and spring, we made a commitment to offer neighborhood representation on university building committees; to host an annual fall meeting with President Pattenaude; and to schedule other meetings related to specific projects so that we can identify concerns and gain your input.

The meeting on September 27 is open to any member of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP at 780-4007.

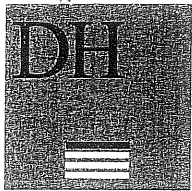
If you have questions related to the meeting or any other issue, please feel free to contact me directly at 780-4200, or send me an e-mail ([caswell@usm.maine.edu](mailto:caswell@usm.maine.edu)).

We hope to see you on the 27th.

Sincerely,

Bob Caswell  
Executive Director, Media & Community Relations

RSC/jao



DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

778 MAIN STREET  
SUITE 8  
SOUTH PORTLAND, MAINE 04106  
TEL. 207 775 1121  
FAX 207 879 0896

- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

April 11, 2001.

Ms. Sarah Hopkins, Senior Planner  
Planning and Urban Development  
389 Congress Street  
Portland, ME 04101

**Subject: USM Portland Campus  
Bioscience Building Expansion  
Response to Engineering Review Comments**

Dear Sarah:

Our office along with Symmes, Maini & McKee Associates and the University of Southern Maine have received and reviewed the engineering review comments prepared by Anthony Lombardo (dated February 5, 2001) from the City's Engineering Department and peer review comments prepared by Jeffrey Prebble of Dufresne-Henry (dated January 5, 2001). A summary of each comment and response is provided below:

A. Anthony Lombardo Comment Memo:

Comment 1:

*Public Works is requesting the applicant install a new drain manhole structure in the location where the on site storm drain system connects in the combined sewer system, within the Falmouth Street right of way. Currently, a 10" diameter pipe outfalls the site and connects directly into the combined sewer. A manhole structure will enhance the ability of the City to maintain and repair the system in Falmouth Street.*

Response:

The site plans for the Bioscience Building Expansion have been revised to include the installation of a drain manhole structure as requested (refer to Sheet C4 attached).

Comment 2:

*Public Works is also requesting the applicant specify, as part of the "erosion and sediment control plan", that the site contractor shall clean the entire on site storm drain system, including the sumps of all catch basins. The timing of this work should coincide with the paving of the parking lot.*



Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 2

Response:

The erosion and sedimentation control narrative has been revised to specify the cleaning of the onsite storm drain system within the Bioscience Building Expansion project area to be completed as requested. In addition, a special construction note has also been added to Sheet C4.

Comment 3:

*The applicant should contact Carol Merrit of Public Works, prior to beginning of construction, to determine all of the mandatory permits and fees associated with construction activity within the City right of way.*

Response:

The applicant or assigned agent will contact the City prior to construction to ensure all permits and fees are obtained for construction work within the City's right-of-way.

B. Dufresne-Henry Comment Letter

Comment 1:

*Erosion and sedimentation control measures are limited on this project and involve primarily hay bale barriers around catch basins. The parking area adjacent to the building addition will be significantly altered, with a significant amount of materials being moved into and out of the site and onto Falmouth Street. There are no measures planned for a stabilized construction entrance to the site, therefore materials could be transported onto Falmouth Street. Additional safeguards against erosion and sedimentation should be provided around the site. Seeding and mulching should be specified for disturbed areas and a schedule for stabilizing areas once they reach final grades. An implementation schedule should also be provided.*

Response:

The erosion and sedimentation control plan and narrative contained in Section 24 of the MeDEP Site Location Permit Application has been revised to include the following:

- Temporary erosion control measures including hay bales, catch basin inlet sediment sump, stabilized construction entrance, water quality treatment unit, etc.
- Permanent erosion control measures including seeding, landscape vegetation/bark mix, etc. (Refer to Sheet C5).
- Timing and sequence of construction.
- Implementation schedule of erosion control measures.

Refer to Attachment A and revised site plans.

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 3

Comment 2:

*We agree that the proposed development will not significantly alter storm water runoff from the site. However, storm water quality issues have not been addressed as part of the application. There does not appear to be any storm water quality measures presently around this site, and there are none proposed.*

Response:

The storm drain system associated with the Bioscience Institute project has been revised to include a Stormceptor water quality treatment unit designed to achieve 63% TSS removal efficiency for the project area. It should be noted, that the Stormceptor unit will treat the 0.7 acre watershed area associated with the Bioscience Institute parking area, as well as, an additional 1.0 acre watershed tributary to the storm drain system from the Masterton Hall area upgradient of the site.

The Stormwater Management Report, as prepared by Symmes Maini & McKee Associates, contained in Section 22 of the MeDEP Site Location Permit Application has been revised to summarize the water quality measures associated with this project. A copy of the revised Stormwater Management Report is contained in Attachment B of this letter.

Comment 3:

*Several utilities need to be modified to make room for the proposed building expansion. Test pits should be called for in key utility crossings to avoid potential conflicts during construction, and verify clearances. The proposed 6-inch sewer service is shown tying directly into an existing manhole, but no invert data is given. I believe the Public Works Department prefers that services be tied into the sewer main rather than manholes. A new 12-inch CPP is shown being tied into DHM-1 and runs underneath the new concrete seat. The concrete seat detail does not provide an overall depth. Clearances in this area need to be verified. Finally, confirmation letters are needed from Public Works on the sewer capacity and Portland Water District for the water services.*

Response:

The Grading and Utilities Plan has been revised to include a note requiring the contractor to conduct testpits at all utility crossings. In addition, the new sanitary sewer service connection has been revised to tie into the sewer main rather than the manhole.

The detail cross-referencing has been revised on Sheet C3 to correspond with the detail numbering on Sheet C6.

The depth of the concrete seat wall has been defined on Sheet C6. Inverts and elevations at these crossings have been identified on Sheet C4.

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 4

Copies of the revised site plans are appended to this letter. In addition, copies of the ability to serve letter from the Portland Water District is contained in Attachment C. At this point, the ability to serve letter from the City of Portland has not yet been received and will be provided separately upon receipt.

Comment 4:

*A solid waste dumpster is called for in Section 5 of the application. The location of the dumpster is not shown on the plans.*

Response:

There are individual containers within the building for disposal of solid waste/recycling items that are collected by the USM Department of Facilities and consolidated for hauling and disposal offsite.

An exterior dumpster is not proposed for this project. The reference in Section 5 to a dumpster has been removed and an updated copy of Section 5 is contained in Attachment D.

Comment 5:

*No information has been provided on lighting fixtures proposed for the site lighting on this project.*

Response:

Details on the exterior site lighting fixtures are contained in Attachment E.

Comment 6:

*A detail has not been provided for the proposed stonewall retaining walls. The granite curbing detail calls for a 6-inch reveal, City of Portland standard is 7 inches. Details 14 and 18, Concrete Retaining Wall, are redundant.*

Response:

A detail section through the stone retaining wall along Falmouth Street has been added to Sheet C6. The granite curb detail on Sheet C6 has been revised to reflect a 7-inch reveal in accordance with the City of Portland standards. In addition, the redundant concrete retaining wall (detail 16) on Sheet C6 has been eliminated. Also, a bituminous curb detail has been added to Sheet C6.

Comment 7:

*The cover letter states that the construction sequence is provided in Sections 1 and 24 of the application. There is a very brief discussion on Construction Plan in Section 1. The construction sequencing should be further defined.*

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 5

Response:

The construction sequencing has been provided in the revised Section 24 – Erosion and Sedimentation Control Report contained in Attachment A of this letter.

Our office along with representatives from Symmes, Maini & McKee Associates and the University of Southern Maine look forward to meeting with you and the members of staff and the Planning Board to discuss the project further.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.



Joseph A. Laverriere, P.E.  
Senior Engineer

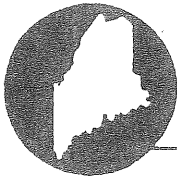
JAL/mb/JN2074/Hopkins2-28

Enclosures

- Attachment A - Revised Section 24 – Erosion and Sedimentation Control Report
  - B - Revised Section 22 – Stormwater Management Report
  - C - Ability to Serve Letters from the Portland Water District
  - D - Revised Section 5 – Solid Waste
  - E - Exterior Site Lighting Details
- Revised Site Plans

c: Dave Early, P.E. – USM  
Joe Burke, P.E. - SMMA

Attachment 3



# UNIVERSITY OF SOUTHERN MAINE

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P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4200 FAX (207) 780-4860  
TTY (207) 780-5646

August 22, 2001

Ms. Sarah Hopkins  
Development Review Program Manager  
City of Portland  
389 Congress St.  
Portland, ME 04101

Dear Sarah:

As requested, I'm forwarding summaries of our meetings with neighbors who live in the vicinity of USM's Portland campus. USM representatives met with neighbors three times between February and April of this year.

For your reference, I've also included letters of invitation to each of the meetings, and USM's responses to the concerns raised at the meeting of April 5. To help you identify which pieces of information go with which meeting, I've numbered the pages as follows:

1. Invitation to the meeting of February 9.
2. Summary of the meeting of February 9.
3. Invitation to the meeting of March 16.
- 4, 5, 6. Summary of the meeting of March 16.
7. Invitation to meeting of April 5.
- 8, 9. Summary of meeting of April 5.
- 10, 11, 12. USM response to concerns raised at meeting of April 5.

To continue our dialog with neighbors, we will offer neighborhood representation on all future building committees; hold informational meetings each fall; and host meetings related to specific building projects to identify any concerns.

Please feel free to give me a call (780-4200) or an e-mail ([caswell@usm.maine.edu](mailto:caswell@usm.maine.edu)) if you have any questions regarding this information.

Sincerely,

A handwritten signature in cursive script that reads "Bob Caswell".

Bob Caswell  
USM Media & Community Relations

RSC/jao

Enclosures

1



# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4200 FAX (207) 780-4860  
TTY (207) 780-5646

January 25, 2001

Dear USM Neighbor:

We'd like to invite you to a neighborhood meeting to find out more about building plans on the Portland campus of the University of Southern Maine.

The meeting will be held at 7 p.m., Friday, February 9, in the cafeteria of the Woodbury Campus Center, Bedford St., Portland. There should be adequate parking in the Bedford St. lots, located a short distance from the intersection of Bedford and Forest Ave.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP to 780-4200.

USM officials will be outlining the plans for the Biosciences Research Institute. This facility, which will be financed with state research and development funds, is planned as an addition to the Science Building on Falmouth St.

We also want to share with you our master plan for the development of the Portland campus. For the most part, the plan represents a 20 to 30-year vision as to how our Portland campus might develop, depending on state funds, private gifts, enrollment trends and other factors. There are only three projects under consideration during the next five years: the Biosciences Research Institute; renovation of the former Steego Building on Bedford St. to house our Muskie School; and construction of a Community Education Center/parking garage on the current Bedford St. parking lot.

USM's mission is to enhance the region's quality of life. Our success in doing that depends on thoughtful partnerships with the community. We value your input and hope to see you on the 9th.

Sincerely,

Bob Caswell  
Executive Director, Media & Community Relations

Dave Early  
Executive Director, Facilities Management

RSC/jao

2



# UNIVERSITY OF SOUTHERN MAINE

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## REVIEW OF USM NEIGHBORHOOD MEETING

The following is an outline of the discussions from the neighborhood meeting, held at 7 p.m., Friday, February 9, in the cafeteria at the University of Southern Maine's Woodbury Campus Center, Portland.

Thanks to the cooperation of the city's Planning Department, we were able to mail 1,219 letters of invitation to residents in an area roughly bounded by Back Cove, the Parkside neighborhood, St. John St. and through the Oakdale section to Douglass St.

Thirty-five people attended, and shared with us concerns ranging from parking to the type of research to be conducted in the proposed Biosciences Institute.

I've grouped those concerns under the following headings, and under each heading have summarized issues that were addressed.

### **PARKING/TRAFFIC:**

Much of the 3+ hour meeting was devoted to this issue. In the short term, many residents felt that our plan to compensate for loss of parking during construction of the Biosciences Institute was flawed in that most people would not use the Marginal Way lot. We responded that an aggressive campaign to inform people of the Marginal Way lot, coupled with a beefed up shuttle bus service, would serve us and the neighbors well during the construction period.

It was further explained that the University is implementing a revised class schedule for the fall, 2001. The schedule is designed to help distribute uniformly space demands throughout the week as opposed to the current schedule that causes peak demands on Tuesdays and Thursdays.

Several questioned why we are planning to construct a research building before a parking garage. We explained that dedicated funding that must be used or be forfeited is in place for the research facility. We also noted that the facility will be dedicated exclusively to research and will not include classrooms and other high-traffic areas. Guests questioned the number of researchers that would be housed in the building.

Neighbors also voiced frustration at students' cars that are taking up on-street parking in and around their neighborhoods. A USM/city collaborative effort to address the problem ought to be initiated, they said. As part of this discussion, people asked why the long-range campus master plan did not contain any provisions for developing on-campus student housing. We explained that our plans call for continuing to emphasize Gorham as a traditional campus environment. A new 224-bed residence hall will open in Gorham this summer. Portland Hall on Congress St. seems to satisfy student demand for Portland-based housing. We have had informal discussions about the possibility of a cooperative agreement with private developers to provide housing but there are no plans to do so.

Guests also expressed concerns over the lack of studies detailing how the closing of Bedford St., for example, would affect traffic flows in the neighborhoods. We explained that the Bedford St. proposal is one of several long-range visions included as part of the campus master plan. As such, it is so far in the future that it does not yet warrant detailed studies.

//more on back

3

USM

# UNIVERSITY OF Southern Maine

Office of Media and Community Relations  
96 Falmouth Street  
P.O. Box 9300  
Portland, ME 04104-9300  
(207) 780-4200  
TTY (207) 780-5646  
FAX (207) 780-4860

March 2, 2001

Dear USM Neighbor:

We'd like to invite you to a neighborhood meeting at 7 p.m., Friday, March 16, in the cafeteria of the Woodbury Campus Center, Bedford St., Portland.

Parking Manager John Peverada and Traffic Engineer Larry Ash of the City of Portland will join us so that we can respond to your questions and concerns about traffic and parking in the campus neighborhoods. A special thanks goes to City Councilor Tom Kane for his help in making these arrangements.

We also want you to know that we heard the concerns expressed at the meeting of February 9, especially those related to a lack of neighborhood input into the development of campus plans.

As a start toward building a better relationship, we want to offer neighborhood representation on university building committees, and host meetings related to specific projects so that we can identify concerns and gain your input. We can discuss these and other ideas in more detail on the 16th.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP at 780-4200.

We hope to see you on the 16th.

Sincerely,



Bob Caswell  
Executive Director, Media & Community Relations



Dave Early,  
Executive Director, Facilities Management

RSC/jao



SUMMARY OF FLIP CHARTS AT  
USM/NEIGHBORHOOD/CITY MEETING ON

MARCH 16, 2001

Consensus of meeting was that the University, neighborhood and City need to join together as partners to develop a common vision.

Phase I of the Biotech Facility (2 stories) is about to begin the city planning board process. The University must spend \$2, 400,000 of the construction monies authorized for this building by November 2001 or lose it. The group agreed that this project will be the first test of the commitment by all parties to the partnership.

The group identified the following issues that need to be addressed regarding the Biotech Facility (Phase 1)

- \*Parking
  - During Construction
  - will it make a bad situation even worse?
- \*Traffic
  - more people, more cars
  - (USM said that there will be 12 new employees and no new classrooms)
- \*Construction Management
  - Time, dust, blasting
- \*Safety of Activities
  - What materials used in labs, infectious viruses, etc.?
- \*Process
  - Adversaries or Partners

5

SUMMARY OF 3/16 MEETING

The group also addressed the following parking/traffic issues and potential solutions:

ISSUES

"Arrogant" parkers who:

- block driveways
- park too near driveways
- park too close to corners
- park in front of fire hydrants
- dump trash in the neighborhood

\*USM mail and security vehicles don't use USM building driveways

\*Need more enforcement/ticketing

\*Tickets are just a slap on the wrist

\*Falmouth Street too narrow because of snow

\*Takes an hour to tow a car that's blocking a driveway

\*USM advertises/encourages neighborhood parking in Lifeline brochure

\*Parking on Brighton Avenue causes safety problems (too narrow in winter and sight problems coming out of William and Pitt Streets)

\*No parking control coverage after 5:00 p.m.

6  
SUMMARY OF MARCH 16 MEETING

POTENTIAL SOLUTIONS

USM and City need to work together on parking/traffic issues

- \*Enforce existing laws (1 & 2 hour limits)
- \*Designate parking control officer specifically for USM area (pay for itself or USM pay?)
- \*Discourage students parking in Portland and taking shuttle bus to Gorham
- \*Resident parking only streets
- \*Resident only on one side of street
- \*Ban parking on Brighton Avenue from Falmouth to William
- \*Reduce parking from two hours to one hour
- \*Improve people moving from satellite parking areas
- \*Chamberlain Street needs time limit to on street parking
- \*More "No Parking Between Signs" (near driveway) further apart
- \*Meter parking on Falmouth Street (USM side)
- \*Stiffer fine for ticket on blocked driveway
- \*Join forces to get bond for parking structure
- \*USM staff use USM driveway
- \*USM offer incentive for staff to park at satellite lots
- \*During construction require contractor to park at satellite lot – and enforce!
- \*Improve Forest Avenue I-295 exit/entrance ramps



# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

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Portland, Maine 04104-9300  
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TTY (207) 780-5636

March 27, 2001

Dear USM Neighbor:

Our next neighborhood meeting will be held at **6:30 p.m., Thursday, April 5**, in Room 1 (basement level) of Payson Smith Hall, Falmouth Street, Portland.

As we agreed, the next meeting will be a facilitated discussion on USM's plans for a Bioscience Research Institute. We have retained the services of Susanna Liller of Barton & Gingold to lead the discussion. USM President Rich Pattenaude also will be joining us.

Notes from our meeting of March 16th are attached, courtesy of City Councilor Tom Kane.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP at 780-4200.

We hope to see you on the 5th.

Sincerely,

Bob Caswell  
Executive Director, Media & Community Relations

RSC/jao

(8)  
USM Neighborhood Meeting #3: April 5, 2001

**Meeting Purpose:** To provide an opportunity for the neighbors of USM to give feedback and ask questions about the Biosciences Research Institute and for USM to understand their concerns and answer their questions.

**The following ideas / issues were raised at the meeting:**

**HOW TO MAKE THE BUILDINGS MORE ATTRACTIVE?**

- Shift building as far away from Falmouth Street to minimize shadow effect — look at shadow study
- Have pictures of what homes across the street will look like — 3D models

**PHASE II ISSUE — ADDITIONAL STORIES**

- 1988 — Building always been planned
- As a concept, we like the project — it's the location — find way to work together
- Don't want to take over more parking in that location — larger footprint — park within two blocks
- Issue is with phase II — alternatives to phase II?
- Options to flow out? Make it larger floor plate and then take up parking/green space
- Less focused on height — not sure getting funding anyway
- Need to consider functionality and cost
- Don't know what trade offs are to remove another floor
- Willing to entertain question
- Until see shadow lines — don't know how much impact
- Issue — lose view of city

**PARKING (MAJOR ISSUE)**

- Find a way to identify contractor vehicles to make sure they are parking in Marginal Way lot and no where else!
- Teeth in their contract (contractor's contract) to make sure significant penalty if they don't follow through
- Love seeing growth at the University — cultural/economic focus it brings to our area is very positive
- Could USM contract with city for 1 full time parking officer until building is built (at least)? Though we know, enforcement is not only answer. USM hoping by construction begins — we add 161 spaces (Green Street — incorporating existing lot) (27 spaces near Chamberlain)
- Our staff/students no right to park incorrectly — stiffer fines
- During construction — requirement to park in Marginal Way lot — contract — put workers there
- Bus employees from remote shuttle lot? Contract problems
- Come up with incentives to get students to park in shuttle lot?
  - \$5 vs. \$25 to park in Marginal Way is current incentive
  - Union issue to enforce faculty
  - Maybe make it free?
  - Certificates?

9

## USM Neighborhood Meeting #3: April 5, 2001

- Why can't campus police ask students to move car? Federal law USM can't. We can't release info — Buckley Amendment
- Have you looked at scheduling of courses? Yes, next year, change in schedule to reduce peaks
- Coffee in Marginal Way lot/Dunkin' Donuts (media campaign) — more incentive for students to park there
- Get people to walk across Forest Ave
- What have other schools done — have you looked into this? What they've done with parking.
  - \$250/yr average urban parking — we have \$25
- Utilize parking garage within city — get cut rates then bus students
- Consistent, focused enforcement (now spotty at best) \$10 not deterrent for ticket — boots?

### CONSTRUCTION MANAGEMENT

- Hot line
- Communicate rules in advance
- Will do renovations in building this summer — construction manager can meet with anyone

### WHAT WILL BE GOING ON IN THE LAB?

- Hiring new people — don't completely know at this point
- Safety will be a major focus
- Probably people working on infectious diseases
- Variety
- Researchers will do some teaching — not about enrollment growth
- Nothing that isn't already going on now at USM

### NEXT STEPS

- Another meeting?
  - Give USM time to review input from tonight and develop response
  - Yes — USM will let people know when the next meeting will be
- Master plan involvement?
  - Comprehensive plan — always in evolution
  - Over next couple years — should involve community
- Add this attendance list to planning board meetings announcements so we can attend planning board meetings — change venue?
- List of local legislators — let them know how you feel about the funding — USM will provide list

(10)  
(text of e-mail sent to neighbors, response to the meeting of April 5.)

Dear Neighbors:

As a follow up to our last meeting, we're responding to your concerns. USM's commencement and related on-campus events, coupled with discussions with City Hall to clarify some zoning issues, have contributed to the delay in responding to you.

Many thanks for your patience. And please, don't hesitate to give us a ring (780-4200) or e-mail (caswell@usm.maine.edu) if you want some university updates.

Below, we've addressed one-by-one the Bioscience building project and related concerns.

#### BUILDING LOCATION:

This is a fundamental issue. Some of you have asked us to reconsider the location of the Bioscience Research Institute. To cut right to the chase, we still plan to build the institute as an addition to the current Science Building on Falmouth St. For the sake of operating efficiency and cost effectiveness, a building dedicated to scientific research has to be attached to our current science facility. In fact, there were several sites considered. The location was selected based on availability of needed services in the current Science Building; cost; and the fact that constructing the institute as an attachment to the Science Building will allow us to improve the appearance of the cement "block house" section of the Science Building.

We started working with the city on this project in August of 2000, but unfortunately did not start working with you during the project's early stages. We regret that. All we can do is make a genuine commitment to giving you input into the early planning of all future projects.

#### BUILDING HEIGHT:

The funding picture for the Bioscience Research Institute has become clearer since our last meeting. We now are certain that available funding only will allow us to construct a two-story building. (Incidentally, you may hear or read references to a three-story building. Those three-story references include a basement level, plus two levels above ground.)

Granted, we hope in the future to add up to three additional stories, for a total of five above ground. But in recent discussions with the city, we have learned that any additional stories will require an entirely new application and permitting process.

As promised at our last meeting, we will share with you architectural studies of the building's shadow effects. Frankly, the timing of that isn't as critical since, as noted above, there's no money for more than two stories, plus any additional stories in the future will necessitate a completely new application and permitting process.

(11)  
(text of e-mail sent to neighbors, response to the meeting of April 5.)

#### CONSTRUCTION PHASE:

In all likelihood, we will not be breaking ground until late fall, at the earliest. We will make sure that the contract requires construction workers to park at Marginal Way. We also will ask the on-site construction managers to attend a neighborhood meeting so that they are aware, firsthand, of your concerns. Finally, Dave Barbour, USM director of facilities management, will set up a special phone line in his office to field any construction-related questions and concerns.

PLEASE NOTE: Beginning in mid-June, you will notice some construction activity around the existing Science Building on Falmouth St. This work is NOT associated with the proposed Bioscience addition. The work is previously planned renovations to the existing building that include: installation of sprinkler/fire alarm systems; renovations of bathrooms; creation of a high-tech classroom in the basement; connection of sewer/water lines to the city line; and upgrades to the road entrance off Falmouth Street. The contractor will be using the loading dock as his staging area and for deliveries. The construction manager for Pizzagalli Construction also has been instructed that all personal vehicles of employees are to be parked in the Marginal Way lot.

#### PARKING:

It's clear from your feedback that we must address the parking issue, through stricter enforcement and/or creation of additional parking.

Several of you suggested that we enter into an agreement with the city to fund an additional parking officer to patrol the streets surrounding the Portland campus. After getting some feedback from Portland Parking Manager John Peverada, we've decided that it would be more effective to create new spaces on campus. More officers on neighborhood streets, in our opinion, will only serve to spread the problem further into the neighborhoods.

Consequently, we are working on creating well over 100 new parking spaces on campus. Those spots will be realized through expansion of the current Alumni House lot and the lot at the Deering Ave. end of Payson Smith Hall; conversion of the university-owned Grand Street into new spaces; and construction of additional spaces in the driveways behind university-owned houses along Bedford St. We will have to seek city approval for these spaces, but we plan to have them in place by the time construction starts on the Bioscience building. According to a city formula, the Bioscience project requires 33 new parking spaces, plus replacing the 38 spaces that will be lost in the lot between the Science Building and the Sullivan Gym due to construction.

You also asked us to consider ways to promote parking at Marginal Way, the 300-space lot that is underutilized by USM faculty, staff and students. Beginning this fall, we will offer free parking to any USM student or employee who wishes to use Marginal Way. We'll be promoting the offer on campus, and also reminding people of the free shuttle



12

(text of e-mail sent to neighbors, response to the meeting of April 5.)

service between Marginal Way and campus locations. We'll also continue to tweak the class schedule to reduce peak demands.

Our long-range plans call for construction of a community education center/parking garage on the current Bedford St. lot that will net us 600 spaces. That will be adequate to meet projected parking demands associated with the proposed Muskie Building (the former Steego Building), the Biosciences project and the community education building. Those plans are contingent upon legislative approval of a university bond issue and voter approval of the bond in the November elections. If you'd like to advocate for passage of the bond issue, contact your local legislator and ask him or her to support L.D. 1378, a bill to authorize a general fund bond issue of \$65.2 million for the University of Maine System and Maine Maritime Academy.

Hopefully, this addresses your major concerns. There undoubtedly are additional concerns that will need to be addressed before we receive final approval. We'll work those out in collaboration with you, either during the Planning Board sessions, or in campus meetings with you.

As we've said at previous meetings, we are committed to making sure that we gain your input on all future projects.

We will:

1. Offer neighborhood representation on all our building committees that are formed to develop campus-based projects.
2. Hold an informational meeting each fall to update neighbors on our plans and priorities for the coming academic year.
3. Host neighborhood meetings related to specific building projects to identify any concerns and gain neighbors' input.
4. Add neighbors to our mailing lists for calendars of events/internal newsletter.

NEXT STEP:

We probably won't have any new issues to discuss until late summer or early fall, once the Planning Board schedule is finalized and we have a clearer picture of the proposed construction schedule. Nevertheless, if you'd like to meet with us this summer, we'd be happy to do so. Please call 780-4200 or drop an e-mail to [caswell@usm.maine.edu](mailto:caswell@usm.maine.edu)

We will keep you posted on any new information related to the above issues.

Sincerely,  
Bob Caswell  
USM Media & Community Relations

**CITY OF PORTLAND, MAINE  
MEMORANDUM**

**TO:** Chair Caron and Members of the Portland Planning Board  
**FROM:** Sarah Hopkins, Development Review Program Manager  
**DATE:** September 11, 2001  
**SUBJECT:** USM Bio-Science Building

**Introduction**

The University of Southern Maine is returning to the Planning Board for a continuation of its presentation of the Bio-Science building on Falmouth Street within the Portland campus. The Bio-Science building will be reviewed for conformance with the Site Plan Ordinance and Conditional Use standards for institutional expansion in a residential zone.

Since the last workshop in January with USM, there have been some clarifications and changes in regards to zoning requirements and funding. Also, the University has continued its work with the neighborhood on the updating of its Master Plan..

The University of Southern Maine campus is 26 acres and is bounded by Forest Avenue, Falmouth, Exeter, Bedford Streets and I-295. The campus is zoned B-2 and R-5. The campus has expanded since 1970 by acquisitions of smaller parcels around the campus perimeter by approximately 9.9 acres.

**Biology Science Building**

The applicant has applied for major site plan and conditional use review for the Bio-Science building and parking lot reconfiguration. The project will also be reviewed for compliance with Site Location of Development under the City's delegated authority.

The Bio-Science building addition will be two stories above ground and one basement level. The footprint of the building is approximately 9,500 sq. ft. and the total square footage of the building will be 27,000sq.ft.

1. Phasing

As the Board may recall, when the applicant was last before the Board, there was an expectation of a second phase of the building, resulting in six total additional stories. The second phase option of the proposal is no longer viable, given the lack of funding, and the proposal has been scaled back accordingly.

2. Parking

Since the last workshop on this item, a determination was made that USM is not exempt from zoning and would therefore be required to meet the parking requirements of the Land Use Code.

Last winter, the City had notified USM that it would be ending the lease of the Marginal Way parking lot for use as a train station. Since that time, the most recent plans for the train station will not require the use of the parking lot, and the lot is once again available for USM's use.

USM has submitted a parking calculation (see Attachment 2). The Zoning Administrator has determined that they are low by 117 parking spaces. The applicant will therefore be requesting from the Board a shared use permit for those parking spaces.

Also included with the applicant's submission is information regarding the enrollment of the campus, Metro ridership data, and parking locations.

### 3. Building Design

Included as Attachment P-5 are the proposed building elevations for the Bio-Science addition. The applicant's architect will be available at the workshop to discuss the building concepts and floor plans.

### 4. Landscaping

Included as Attachment P-4 is the landscaping plan for the site. Street trees will be placed along the Falmouth Street façade of the building and a more intensive landscaping treatment will be provided along the south elevation of the building, where the building is connected by pathways to the rest of the campus.

### 5. Grading/Utilities

The existing Science Building parking lot will be lowered as part of this proposal, providing a more attractive streetscape along Falmouth Street and access to the east side of the building.

Run off within the parking lot will be collected in a new catchbasin and routed via manholes and stormdrains to the internal stormwater system of the campus.

## Attachments

1. Enrollment Information
2. Parking Requirements per City Ordinance
3. Neighborhood Meeting Information
4. Engineering Response to Comments Raised
5. Memo from Public Works
6. Comments from the Zoning Administrator

## Plans:

1. Schematic Landscaping Plan
2. Existing Conditions
3. Grading/Utility Plans
4. Landscaping Plans
5. Building Elevations

**CITY OF PORTLAND, MAINE  
MEMORANDUM**

**TO:** Chair Caron and Members of the Portland Planning Board

**FROM:** Sarah Hopkins, Development Review Program Manager

**DATE:** January 23, 2001

**SUBJECT:** USM Master Plan and Bio-Science Building

The University of Southern Maine is returning to the Planning Board for a continuation of its presentation of the updated master plan for the USM Portland campus. The master plan includes several phases that will be implemented over the next 10 years. The first phase will include a Biology Science building addition to be reviewed concurrently by the Planning Board for compliance with Site Plan and Site Location of Development.

The University of Southern Maine campus is 26 acres and is bounded by Forest Avenue, Falmouth, Exeter, Bedford Streets and I-295. The campus is zoned B-2 and R-5. The campus has expanded since 1970 by acquisitions of smaller parcels around the campus perimeter by approximately 9.9 acres.

At the Planning Board's request, the applicant will present traffic and parking information, as well as enrollment calculations for the campus. Information regarding the Marginal Way shuttle bus and parking management plan are also included

A copy of the January 9, 2001 Planning Board memorandum is attached with the Master Plan booklet, as well as a Parking and Traffic Study and report on USM's enrollment and parking mitigation efforts.

Attachments

1. Letters from Neighbors
2. Enrollment/Parking Mitigation Plan
3. Parking and Traffic Study

Also, January 9, 2001 memorandum with Master Plan Booklet

January 1, 2001

Joseph E. Gray  
Director of Planning and Urban Development  
City of Portland  
389 Congress Street  
Portland, ME 04101

Dear Mr. Gray and Members of the Committee:

I strongly object to the University of Maine adding buildings as proposed to the Portland Campus. I am a resident of Falmouth Street and find their plan highly destructive to the well being of the abutting neighborhood.

There are several issues that enter into my reason for objection.

1. Falmouth Street has a highly problematic traffic and parking situation as it is. Falmouth Street is used as a connector by fire, police and the community for prompt access across town. Further, it is a highly trafficked area by the University community attending classes there. It is highly congested and currently requires almost constant monitoring by the city's traffic control to reduce the amount of illegal and problematic parking. Neighbors in this community currently cannot find their own parking during the day. Further the city has limitations on landlords as to the amount of apartments they may have in their buildings based on parking. I do not understand why this is different for the University.
2. The University in Portland is a small commuter school in general. I do believe that it has reached its limit on the amount of traffic it can handle. New growth should be concentrated on its Gorham Campus which is more rural and larger with more potential for expansion and with its ability to meet the needs of students that reside there. The only area that could possibly tolerate some expansion is the land abutting the interstate and commercial buildings.
3. The University and the City must realize that Falmouth Street has two sides. One side the university, and the other side turn of the century three story wooden buildings that have an odd but nice architectural appearance. Slowly these buildings are being owner occupied and improved – something that is encouraged and applauded in other parts of the city. They are attractive inside and have the capacity to keep families in the city. Part of the street, including my building, have a panorama view of the city that makes one appreciate the morning sunrise, the clouds over the bay and the moon rising. With the proposed plan this area will be totally encroached with brick and cement.

4. The University currently has tremendous amounts of lighting which lights up the neighborhood at night when the campus is empty, making it unnecessary for personal lighting to see inside one's home. They run enough equipment at night to be a nuisance past the hours that most neighbor's respectfully stop ruckuses. Additional buildings will only add to that problem and further deteriorate the community rather than provide an opportunity to compliment each other.
5. A science building and additional parking bring environmental and safety concerns to the neighborhood. Even with a parking garage in this area car emissions from cars idling will increase effecting my and the community's health. A science building abutting buildings with families and children could lead to health hazards from dangerous chemicals.
6. I also have concerns about the damage to my building while setting the foundation of such a large building across the street. I think it would be impossible for the University to set a foundation without the machinery damaging the integrity of my building. I have seen such building crack foundations and walls in neighboring buildings. The effect of noise, dust, dirt and ongoing machinery during such a close and overwhelmingly large construction is mind boggling.

I am but one member of the community. I have worked hard, paid high taxes and have taken great pride in living in and working on this house. I have been a strong member of the community and care about its future. I request a public hearing regarding this matter.

Again, I highly oppose the building of a science building and additional parking in the Falmouth Street area.

Sincerely,



Lisbeth Faulkner  
Owner and resident of  
61 Falmouth Street  
Portland, ME 04103

**From:** Alex Jaegerman  
**To:** "MHAYling@unum.com"@Portland.gwgwia; Sarah Hopkin...  
**Date:** Fri, Jan 5, 2001 2:21 PM  
**Subject:** Re: USM Master Plan

Thank you for your comment. I will see that it gets included in the Planning Board Packet.

Alex Jaegerman, Chief Planner.

>>> "Ayling, Mary H" <MHAYling@unum.com> 01/05 12:18 PM >>>  
To: Alexander Jaegerman, Chief Planner  
Joseph E. Gray, Jr., Director of Planning and Urban Development

We, Thomas and Mary Ayling, received notification in the mail yesterday about a workshop session scheduled for January 9, 2001 being held for the University of Southern Maine to present their plan to the Planning Board for the future of the USM campus. I understand that this is not a forum for public comments, but want to express our interest/concern about this matter. I do plan on attending this session.

We reside at 106 Bedford St., which is the only dwelling on Bedford St. that the USM does not currently own. Our son, Thomas, also works, temporarily, for the University in the facilities department. He recently had the opportunity to view the proposed expansion. It appears that the longterm plan (within the next 10 years), is to take down all the dwellings on Bedford St., in order to accomplish this proposed project. Obviously, our concern is that we live in one of those dwellings.

To date, we have not been approached by any officials of USM to discuss this. I will attend this meeting next Tuesday to hear for myself what their plans, and those of the City of Portland, are in regard to this proposed expansion.

Respectfully,  
Mary H. Ayling  
Thomas H. Ayling III



# UNIVERSITY OF SOUTHERN MAINE

Office of Finance and Administration

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4484  
FAX (207) 780-4549  
TTY (207) 780-5646

DATE: January 18, 2001

TO: Mr. David Early, Executive Director, Facilities Management

FROM: Samuel G. Andrews, Office of Finance and Administration

RE: Biosciences Research Institute Addition

As I understand it, the City of Portland Planning Board and Planning Staff requested some additional information relative to future enrollments at the Portland campus as well as information on USM's parking mitigation efforts. In regard to the Biosciences Research Institute Addition to the Portland Science Building, it should be noted that this is a research facility only and will not have any impact at all on student enrollment.

The Maine Legislature has made two targeted additions to the University of Maine System base budget designed to substantially increase research and development (R&D) activity at the University of Maine and the University of Southern Maine, in particular to strengthen the capacity of both Institutions to compete for Federal grants and contracts. The annual base budget operating increase is \$10 million plus \$2.5 million to cover debt service payments on a \$25 million internal bond issue for a period of 15 years. The general goal is that together the Universities will generate an additional \$40-\$50 million annually in external research funding from the investment base of \$10 million. For USM the additional per year operation investment will be \$2 million directed at research faculty and staff, equipment and materials in the biosciences, information sciences, and advanced materials, primarily electronics. In addition, USM has a total of \$7.7 to build and renovate research laboratories (\$2.7 M from a bond referendum approved by voters in 1998 and \$5M from the campus share of the internal bonding capacity).

Exhibit I presents student headcount enrollment for the Portland campus for the fall, 1998 through the fall, 2000 semester with a projection through the fall, 2020 semester. The table shows Weekend College enrollment (classes offered at 4:00 p.m. and after on Friday through Saturday and Sunday) and Regular Enrollment (classes offered from 8 a.m. on Monday through 4:00 p.m. on Friday). About 20% of the students attending the Portland campus are present on Friday, Saturday and Sunday and not during the week days. The introduction of Weekend College was to provide access to adult learners and to relieve pressure on classrooms, laboratories, and parking resources during the week. The enrollment projection takes into account the projected population trends in southern Maine over the next twenty years; projected high school senior population in Maine as a whole, and the high school senior population in southern Maine, specifically; the projected rate of college attendance among high school seniors; and the post-secondary educational participation rate among adults. The projected rates of increase in student headcount enrollment are noted at the bottom of Exhibit I.

The parking and traffic study carried-out in connection with the permitting of the Science Building Addition was carried out on a Tuesday. Exhibit II shows the number of students registered for Portland campus classes at one hour intervals throughout the week. For example, at 8:00 a.m. on Monday morning there were 248 students registered to take classes at the Portland campus while at 2:00 p.m. on Monday, there were 1,116 students registered to be in class at that time. The



**UNIVERSITY OF SOUTHERN MAINE  
ACTUAL & PROJECTED STUDENT HEADCOUNT  
FALL, 1998 to FALL, 2020**

<u>Actual Student Headcount</u>	<u>Weekend College*</u>	<u>Regular Enrollment**</u>	<u>TOTAL</u>
Fall, 1998	940	4,786	5,726
Fall, 1999	1,015	4,500	5,515
Fall, 2000	1,077	4,386	5,463
<u>Projected Student Headcount</u>			
Fall, 2001	1,109	4,474	5,583
Fall, 2002	1,142	4,563	5,705
Fall, 2003	1,176	4,654	5,830
Fall, 2004	1,211	4,747	5,958
Fall, 2005	1,247	4,842	6,089
Fall, 2006	1,259	4,890	6,149
Fall, 2007	1,272	4,939	6,211
Fall, 2008	1,284	4,989	6,273
Fall, 2009	1,297	5,039	6,336
Fall, 2010	1,310	5,089	6,399
Fall, 2011	1,317	5,113	6,430
Fall, 2012	1,324	5,137	6,461
Fall, 2013	1,331	5,161	6,492
Fall, 2014	1,338	5,185	6,523
Fall, 2015	1,345	5,209	6,554
Fall, 2016	1,352	5,233	6,585
Fall, 2017	1,359	5,258	6,617
Fall, 2018	1,366	5,283	6,649
Fall, 2019	1,373	5,308	6,681
Fall, 2020	1,380	5,333	6,713

Enrollment projection for Fall, 2001 to Fall, 2005 includes a 2% annual increase for the School of Law, a 3% annual increase for Weekend College and a 2% annual increase for Regular Enrollment. Enrollment projection for Fall, 2006 to Fall, 2010 includes a 1% annual increase for all categories and the projection for Fall, 2011 to Fall, 2020 includes a 1/2% annual increase for Weekend College and Regular Enrollment and no increase for School of Law

\* Includes classes offered at 4:00 p.m. and after on Friday and those offered on Saturday and Sunday.

\*\* Includes classes offered from 8 a.m. on Monday through 4:00 p.m. on Friday.

**UNIVERSITY OF SOUTHERN MAINE  
STUDENT ENROLLMENT BY HOUR  
PORTLAND CAMPUS - FALL, 2000**

	<u>Monday</u>	<u>Tuesday</u>	<u>Wednesday</u>	<u>Thursday</u>	<u>Friday</u>	<u>Saturday</u>
8 a.m.	248	11	218	2	41	20
9 a.m.	548	842	534	768	218	436
10 a.m.	595	1,239	568	1,180	287	441
11 a.m.	498	1,270	462	1,194	270	448
Noon	905	1,006	874	914	203	108
1 p.m.	941	1,090	785	976	273	106
2 p.m.	1,116	1,153	839	1,064	273	111
3 p.m.	888	887	689	840	121	73
4 p.m.	1,235	1,182	1,127	1,092	352	36
5 p.m.	1,091	1,083	1,040	1,019	344	0
6 p.m.	1,056	988	1,051	909	345	0
7 p.m.	774	767	798	648	40	0
8 p.m.	736	666	689	527	35	0
9 p.m.	<u>710</u>	<u>617</u>	<u>666</u>	<u>503</u>	<u>16</u>	<u>0</u>
<b>TOTAL</b>	<b><u>11,341</u></b>	<b><u>12,801</u></b>	<b><u>10,340</u></b>	<b><u>11,636</u></b>	<b><u>2,818</u></b>	<b><u>1,779</u></b>

Dave Early cont'd  
page 2  
January 18, 2001

enrollment by hour data clearly indicates that Tuesday is the busiest day of the week and Tuesday attendance exceeds Monday attendance by 12.9%. Tuesday enrollment is 23.8% greater than that on Wednesday; Tuesday has 10.0% more students than on Thursday; and Tuesday has enrollment that is 354.3% greater than on Friday.

The University of Southern Maine provides three shuttle bus services. One such service runs between the Portland and Gorham campuses, with one bus leaving each campus at 30 to 45 minute intervals from about 7:00 a.m. to 10:00 p.m. During the 1999/00 academic year, this shuttle service provided 92,936 riders with transportation between the two campuses. Exhibit III, Page 1 shows time schedule for this shuttle service during the spring, 2001 semester and Exhibit III, Page 2 indicates the service times on Saturday for the Gorham and Portland shuttle, and the Monday through Friday service times for the separate and distinct shuttle service provided between Portland Hall and the Portland campus. During the 1999/00 academic year, there were 23,234 riders who took the Portland Hall/Portland campus shuttle service.

Exhibit III, Page 1 also indicates that a shuttle bus runs continually from the Marginal Way parking lot to the Portland campus, starting at 7:15 a.m. with the last shuttle returning to the Marginal Way lot at 10:00 p.m. For University staff or students wishing to use a University parking lot, there is a parking decal fee of \$15 per semester or \$25 per year. Those who purchase a Marginal Way decal (322 were sold in 1999/00), the cost is \$5 per year. The Marginal Way shuttle service operates Monday through Thursday when classes are in session. When the Marginal Way shuttle service is not being provided, those with Marginal Way parking decals are able to park in campus lots. During the 1999/00 academic year, the Marginal Way shuttle service to the Portland campus had 7,397 riders with 60% of the ridership occurring during the spring semester.

During the 1999/00 fiscal year, the University took a lease for office space at 15 Baxter Boulevard, just across from the University Library and a short walk from campus. This was for a three year period with renewal options and provided office space for several Muskie School of Public Service grant/contract activities. This property included 52 additional parking spaces that were available for use by University of Southern Maine staff and students with University parking decals.

Construction of the Biosciences Research Institute Addition to the Portland Science Building will cause much of the current parking lot between the Science Building and the Sullivan Gym (approximately 104 parking spaces) to be used for construction and staging. Currently, this is a parking lot designated for faculty/staff parking. The lot in front of the Woodbury Campus Center which is currently designated for students only will be opened to faculty and staff during the construction period. Also, during this construction period, Marginal Way and Baxter Boulevard will be available as alternative parking sites for the academic year. In order to have people use Marginal Way or Baxter Boulevard as alternatives, there will be better designation and publicity about where the

SHUTTLE BUS SCHEDULE  
EFFECTIVE JANUARY 16, 2001  
MONDAY THROUGH FRIDAY

There will be one bus per run in each direction. If all seats are filled, standing will be allowed.

LEAVES PORTLAND

7:25 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4:10  
 4:50  
 5:35  
 6:15  
 7:00  
 8:30  
 8:45(via Mall Mon-Fri)  
 10:00 (Mon-Thurs)

LEAVES GORHAM

7:15 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4:10  
 4:50  
 5:35  
 6:15  
 7:00  
 8:15 (via Mall Mon-Fri)  
 9:35 (Mon-Thurs)  
 10:20 (Mon-Thurs)

A shuttle bus will run continually from the Marginal Way lot, in Portland, with stops at the Campus Center, Payson Smith Hall and other on-campus locations as needed, and return to the Marginal Way lot at 7:15a.m., Monday through Thursday. **At 8pm, the shuttle that serves Portland Hall will also serve as the Marginal Way shuttle.** The last bus will return to the Marginal Way lot at 10:00pm, Monday through Thursday. **There will be no Marginal Way bus on Fridays and weekends.** **NOTE: VIP driver shift change occurs at approximately 3:30pm.**

**BUS SCHEDULES ARE SUBJECT TO CHANGE EACH SEMESTER AND DURING FINAL EXAMS.**

PORTLAND HALL SHUTTLE BUS SCHEDULE  
PORTLAND HALL RESIDENTS  
EFFECTIVE JANUARY 16, 2001

LEAVES PORTLAND HALL

7:00a.m.  
 7:45  
 8:15  
 8:45  
 9:15  
 9:45  
 10:15  
 10:45  
 11:45  
 12:15p.m.  
 12:45  
 1:15  
 1:45 (last run on Fri)  
 2:15  
 3:45  
 4:15  
 4:45  
 5:15  
 5:45  
 6:15  
 6:45  
 7:15  
 7:45  
 8:15 \*also serves Marginal Way  
 8:45 \*  
 9:15 \*  
 9:45 \*

LEAVES CAMPUS CENTER

7:30a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 12:00noon  
 12:30p.m.  
 1:00  
 1:30 (last run on Fri)  
 2:00  
 2:30  
 4:00  
 4:30  
 5:00  
 5:30  
 6:00  
 6:30  
 7:00  
 7:30  
 8:00 \*also serves Marginal Way  
 8:30 \*  
 9:00 \*  
 9:30 \*  
 10:05 \*

No Runs on Saturday and Sunday. No Runs to 68 High St.

NOTE: If the shuttle is full, there will not be another shuttle until the next scheduled departure.

SATURDAY MINIBUS SCHEDULE  
EFFECTIVE JANUARY 16, 2001

LEAVES GORHAM

8:30am (no Mall Stop)  
 9:30  
 11:30  
 2:00pm  
 4:00  
 6:00

LEAVES PORTLAND

9:00am (no Mall Stop)  
 10:30  
 12:30pm  
 3:00  
 5:00  
 7:00

NOTE: All Saturday Trips, except 8:30am & 9:00am, Stop at the Maine Mall.

Bus service is provided during the fall and spring semesters when classes are in session. All bus schedules are reviewed by Student Life and the Registrar's Office. Copies are available at the USM Police Department. Please call USM Parking and Transportation at 780-5254, with any questions or concerns regarding transportation services.

Dave Early, cont'd  
page 3  
January 18, 2001

Marginal Way shuttle bus stops are located on the campus and about the location of a shuttle bus stop being instituted at Baxter Boulevard. Also, there will be a more clearly delineated time schedule for the Marginal Way/Baxter Boulevard shuttle and better maintenance of the time schedule to instill confidence in the service on the part of users. Once the construction phase of the Biosciences Addition is completed, the lot between the Science Building and the Sullivan Gymnasium will be returned to use, although there will be a loss of 44 parking spaces due to the Addition.

Over the past two years, the University Community has discussed changes in the scheduling of classes in order to spread the scheduling of classes more evenly over the Monday through Friday period. Through this revamping of the course scheduling, it is hoped that time blocks can be more effectively utilized and will even-out some of the peaks and valleys in student attendance. Exhibit IV, Pages 1 & 2, are the announcement from Provost Joseph S. Wood to USM Faculty and Staff concerning the new class schedule to be implemented effective with the fall, 2001 semester. As indicated in the memo, "implementation should open-up classrooms and also help relieve some parking stress on Tuesdays and Thursdays", the two days that have the highest parking demand.


The use of the three University of Southern Maine shuttle bus services, the invigoration of the Marginal Way parking alternative, the addition of parking spaces at 15 Baxter Boulevard and the implementation of the new class schedule should provide sufficient results to cope with any new parking demand resulting from the addition of the Biosciences Research Institute to the Portland campus. As a purely research oriented facility, the new Addition will not generate any new students for the campus; only research personnel who will be working in the new faculty.

In addition to the items outlined, the University of Southern Maine is and has been an active partner with the Metro to create awareness and use of the Metro as an alternative to bringing a vehicle to the Portland campus. Exhibit V is material shared with the USM Parking and Transportation Review Committee concerning USM's effort with the Metro since March of 2000. From March through August, 2000, the free Metro ride program involved 11,346 riders, and the "50-Cent Ride" program generated 4,517 Metro riders during the September through November period. Through the \$11,000 in financial support being provided to the Metro by USM for the "50-Cent Ride Anywhere, Anytime" program, hopefully, some current drivers to campus will switch and use public transportation.

SGA/c

attachments

December 14, 2000

TO: USM Faculty and Staff  
FROM: Joseph S. Wood, Provost   
RE: New Class Schedule for Fall 2001

With full administrative support and endorsement of the Faculty Senate, we will implement a new class schedule effective in the fall semester of 2001. Perhaps the least problematic of various options we have considered over the last few years, the new schedule builds on 90-minute morning time blocks on Monday-Wednesday-Friday, as well as on Tuesday-Thursday. See the attached complete schedule prepared by the Registrar.

Within the new 90-minute morning time blocks for Monday-Wednesday-Friday, scheduling priority will be given to 50-minute classes (3 credit hours) or 75-minute classes (4 credit hours) running over all three days of the week. Friday mornings also provide opportunity for innovative pedagogies that do not correspond to conventional scheduling.

We will adhere strictly to time blocks due to the availability of classroom spaces. The schedule does not affect offering of studio and laboratory classes, except insofar as they may necessarily have to adapt to new starting times.

The new schedule will incorporate afternoon changes established for spring semester 2001. To allow for a 45-minute bus trip between campuses all day long, Portland classes will begin at 8:45 a.m. all days. Implementation should open up classrooms and also help relieve some parking stress on Tuesdays and Thursdays.

As always, Deans are responsible to work with faculty to develop schedules that most effectively allow students to meet degree requirements. Registrar Steve Rand will work closely with Deans to reduce or eliminate scheduling conflicts.

I recognize that we will encounter unforeseen difficulties in implementing this schedule change, and I ask your support and patience as we proceed.

JSW/es

Beginning with the Fall 2001 semester the normal scheduling periods will be changed.

All periods on Monday thru Thursday before 4:00 p.m., will be 75 minutes with a 15 minute break between periods. This schedule will be the same for each day.

Periods after 4:00 p.m. will accommodate either 75 minute periods or 150 minute (2.5 hours) periods.

Friday will normally consist of two 2.5 hour periods.

PORTLAND		GORHAM	
MTWR	F	MTWR	F
8:45 - 10:00	8:45 - 11:15	8:00 - 9:15	8:00 - 10:30
10:15 - 11:30	11:45 - 2:15	9:30 - 10:45	11:00 - 1:30
11:45 - 1:00		11:00 - 12:15	
1:15 - 2:30		12:30 - 1:45	
2:45 - 4:00		2:00 - 3:15	
4:10 - 5:25		4:10 - 5:25	
5:35 - 6:50		5:35 - 6:50	
(4:10 - 6:40)		(4:10 - 6:40)	
7:00 - 9:30		7:00 - 9:30	

Notes

- 1) A 3 credit class offered in a 75 minute period will be offered either M/W or T/R.
- 2) A 3 credit class offered 3 times a week will be offered M W F during 50 minutes of a single period time block.
- 3) Classes that are 4 credit hours can be offered in a single period time block on M W F, with one meeting per week being 50 minutes.





## UNIVERSITY OF SOUTHERN MAINE

Office of Finance and Administration

96 Falmouth Street  
 P.O. Box 9300  
 Portland, Maine 04104-9300  
 (207) 780-4484  
 FAX (207) 780-4549  
 TTY (207) 780-5646

DATE: September 21, 2000  
 TO: University of Southern Maine Parking/Transportation Committee  
 FROM: Mr. Samuel G. Andrews, Office of Finance and Administration / C  
 RE: USM/METRO Cooperative Efforts

At the September 19 meeting of the USM Parking/Transportation Review Committee, a Committee member noted the advertisements that have been appearing in the FREE PRESS about the "50¢ Fares, Any where, Anytime" and the METRO "FAST Service on Forest Avenue". (I have enclosed a copy of advertisements from the 9/5/00 and 9/18/00 issues.) Beginning last spring, USM began cooperating with METRO to create awareness and travel usage of the METRO via a free ride program. As indicated in the attached memo from Philip Chin, Director of Marketing for METRO, by displaying a USM photo ID card, any member of the USM Community (faculty, staff or student) was eligible to ride free on the METRO. Mr. Chin's memo to me outlined the ridership that resulted from the pilot program. We also cooperated with the Greater Portland Council of Governments (COG) and METRO in having an electronic touchscreen-Smart Kiosk installed in the Woodbury Campus Center in Portland with the kiosk providing bus, area transportation and tourism information.

In late summer the METRO began the "FAST Service on Forest Avenue" which provides METRO service every fifteen minutes along Forest Avenue from Pride's Corner in Westbrook to Downtown Portland. This "FAST Service" significantly improved the ability of folks living near or along bus route #2 (as well as those who might use the Park and Ride lot at Prides' Corner) to get to the Portland campus. The University was involved with the group that helped put together the funding proposal for this service. In addition, for the 2000/01 academic year, we are cooperating with METRO on the "50¢ Fares for USM, Anytime, Anywhere" program. In this program, anyone with a USM photo ID or USM ID card can ride the METRO for 50¢, or half the regular fare.

To promote these programs, the University has provided up to \$5,000 to METRO to be used to purchase display advertising in the FREE PRESS during the academic year. USM has also provided \$6,000 toward the cost of printed materials including informational brochures, posters, and other educational outreach materials about the reduced fare and Forest Avenue programs and about the benefits of using public transportation rather than ones own vehicle. Throughout the academic year, METRO will provide us with usage data. Hopefully the efforts from METRO will help make the USM community aware of METRO services that can facilitate its commute to Portland campus. As the academic year progresses, I will keep you informed as to the on-going results.

SGA/c

pc: Ms. Judith Ryan; Mr. Craig Hutchinson;  
 Ms. Helen Gorgas-Goulding

August 3, 2000  
To: Sam Andrews, USM  
Fr: Philip Chin, Director of Marketing  
Re: USM/METRO Ridership Program

The following is an update on the METRO-USM pilot program:

Background - USM/Free METRO Fare Pilot Program

Since March 2000, we have been working with Craig Hutchinson, Helen Gorgas, and Judy Ryan on **creating awareness and trial usage** of public transportation via a **free METRO ride** program. By displaying a USM photo ID card, any member of the USM community rides for free. The METRO has spent over \$1,000 in display advertising in the USM *Free Press* in promoting the free service. We have received very positive support from Craig and Helen via USM on-campus promotions. In June, our *FAST* (Forest Avenue Smart Travel) or 15-minute frequency service was introduced. METRO *FAST* has been a strong complement to the free USM fare program. The accessibility of the METRO bus on Forest Avenue near the campus and higher service frequency have proven to be highly attractive to USM riders.

The pilot program's ridership to date is:

USM METRO trips						
March	April	May	June	July	August	Cumulative
1,234	2,024	2,227	1,893	1909		9,287

# FAST Service on Forest Avenue

## 50¢ Fare

*Anytime. Anywhere.*

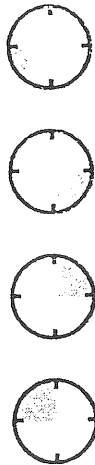
If you attend USM at the Portland Campus or at 68 High Street, we'll make your weekday commute between Forest Avenue and downtown Portland a little easier.

METRO #2 bus will provide service **every 15 minutes** along Forest Avenue, Monday thru Friday, for your school and work commute, shopping trips, short errands, medical appointments, and other intown and crosstown trips.

**With a USM Photo ID card and/or a current USM Registration card, the fare is only 50¢!**

Ride in brand new buses. Use the bicycle racks on each bus to combine bike and bus commuting. Visit our exciting, electronic touchscreen SMART Kiosks at the METRO PULSE (Elm Street Garage) and at the Woodbury Campus Center (Bedford Street) for bus, area transportation, and tourism information.

## METRO Bus #2

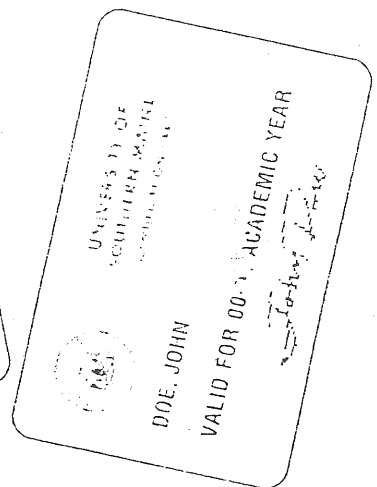
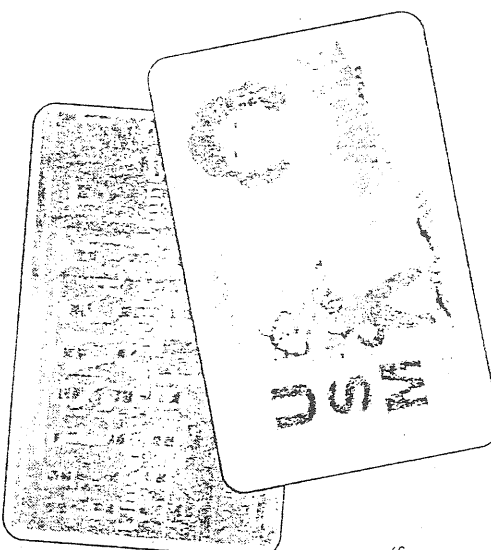
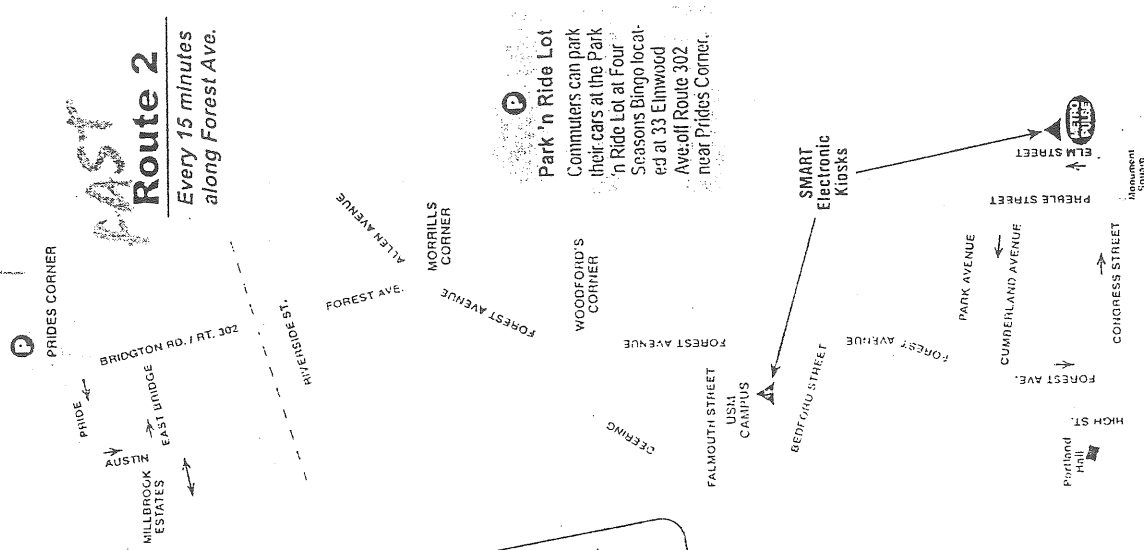


*Every 15 minutes daily along Forest Avenue!*

# METRO

*Your City Bus*

**FAST**  
**Route 2**  
Every 15 minutes  
along Forest Ave.



## 50¢ Fares for USM.

Anytime. Anywhere.



- Show a **USM Photo ID** or a current **USM ID registration card** to a METRO driver and ride for 50¢ (regular fare is \$1). No other USM ID will be accepted.
- Get a free USM Photo ID card at Payson Smith Hall in Room 4.
- Ride any route – anytime, anywhere.
- Use the METRO for your commute to school, work, intown Portland, shopping, medical appointments and for other daily trips.
- Use the METRO Smart KIOSK at the Woodbury Campus Center and at the METRO Pulse for bus time schedule and information.



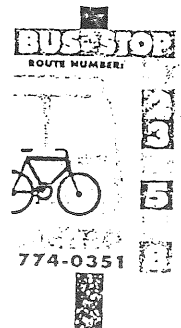
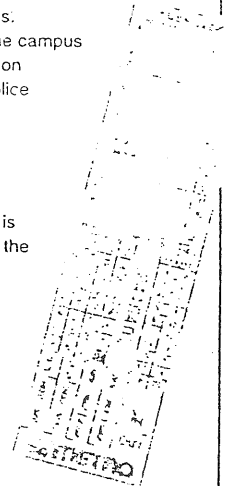
### METRO to USM – #2 or #4 Bus

The #2 bus (Forest Avenue) stops by Back Bay Bicycle on Forest Avenue. It is a short walk up Bedford Street to the campus. The #2 bus stops in front of Pier One for the return intown trip.

The #4 bus (Westbrook/Exit 8) also takes you to the campus. Traveling to the campus, the bus stops on Bedford Street on the campus side. Leaving the campus, the #4 bus stops at the bus shelter on Bedford Street opposite the Campus Center near the USM Police Department building.

### How do I change or transfer to another METRO bus?

You may need to change buses to reach your location. This is called a *transfer*. If you need to change buses, please ask the bus driver for a *transfer* when you first get on the bus and pay for your ride. When you change your bus, please give the transfer to the second bus driver. **The transfer is free.** You do not need to pay when you change buses. On your ride home, you will need to pay another fare. (See picture of transfer at the right.) You can change buses at various METRO bus stops along Congress Street which runs through downtown Portland. You can also change buses at the METRO PULSE located at the Elm Street Garage.



### Where do I wait for the bus?

Please wait for a METRO bus where you see a *bus stop sign* (see left). There are also glass structures that are METRO bus stops called *bus shelters*.

### Are there special bus fare plans?

- \$30 Monthly Pass: unlimited rides for month purchased
- \$9 TenRide Ticket: 10 rides with no time limit

For information — 774-0351



December 8, 2000

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program

For your posting the following is ridership for the program:

March	1,234	
April	2,024	
May	2,227	
June	1,893	
July	1,909	
August	2,059	
<b>Cumulative Free Rides:</b>		<b>11,346</b>

September	1,313	
October	1,616	
November	1,588	
<b>Cumulative 50-Cent Rides:</b>		<b>4,517</b>

The baseline of ridership provides optimism that we can continue to build interest in public transit within the USM community. In the months ahead, we will focus on USM faculty and staff, and on the Gorham campus. Thank you for your continued support and I will provide future updates.

**CITY OF PORTLAND, MAINE  
MEMORANDUM**

**TO:** Chair Caron and Members of the Portland Planning Board  
**FROM:** Sarah Hopkins, Development Review Program Manager  
**DATE:** September 11, 2001  
**SUBJECT:** USM Bio-Science Building

**Introduction**

The University of Southern Maine is returning to the Planning Board for a continuation of its presentation of the Bio-Science building on Falmouth Street within the Portland campus. The Bio-Science building will be reviewed for conformance with the Site Plan Ordinance and Conditional Use standards for institutional expansion in a residential zone.

Since the last workshop in January with USM, there have been some clarifications and changes in regards to zoning requirements and funding. Also, the University has continued its work with the neighborhood on the updating of its Master Plan..

The University of Southern Maine campus is 26 acres and is bounded by Forest Avenue, Falmouth, Exeter, Bedford Streets and I-295. The campus is zoned B-2 and R-5. The campus has expanded since 1970 by acquisitions of smaller parcels around the campus perimeter by approximately 9.9 acres.

**Biology Science Building**

The applicant has applied for major site plan and conditional use review for the Bio-Science building and parking lot reconfiguration. The project will also be reviewed for compliance with Site Location of Development under the City's delegated authority.

The Bio-Science building addition will be two stories above ground and one basement level. The footprint of the building is approximately 9,500 sq. ft. and the total square footage of the building will be 27,000sq.ft.

1. Phasing

As the Board may recall, when the applicant was last before the Board, there was an expectation of a second phase of the building, resulting in six total additional stories. The second phase option of the proposal is no longer viable, given the lack of funding, and the proposal has been scaled back accordingly.

2. Parking

Since the last workshop on this item, a determination was made that USM is not exempt from zoning and would therefore be required to meet the parking requirements of the Land Use Code.

Sarah Hopkins - Re: Fwd: USM

**From:** "Dave Early" <dearly@usm.maine.edu>  
**To:** "Sarah Hopkins " <SH@ci.portland.me.us>  
**Date:** Thu, Sep 27, 2001 9:04 AM  
**Subject:** Re: Fwd: USM

Sarah,

Thank you for forwarding me a copy of the letter from Michael and Donna Gibbs. I do not know why they never received their invitations to our 3 pervious neighborhood meetings. I do hope they come tonight. As you are aware we have had Larry Ash and John Peverada attend and speak at our earlier meetings. A number of issues were discussed including Parking Enforcement and University financial support of a parking enforcement officer. I have a call in to John Peverada now and am looking into what it would cost the University to have the snow hauled away after each snow storm. We do have a lot to accomplish between now and October 23rd, but I think we can do it. Thank you Sarah for placing us on the tentative schedule, and all your assistance.

Dave E.

Sarah Hopkins - USM

**From:** Michael Gibbs <MGibbs@mail.maine.rr.com>  
**To:** Portland.CityHall(SH)  
**Date:** Wed, Sep 26, 2001 2:46 PM  
**Subject:** USM

First of all, thank you for taking the time to up date me on the status of USM's application for the addition to the Science Building. I also appreciated the opportunity to address the Planning Board during the workshop and look forward to attending the Public Hearing in October.

I regret that I was not better prepared to address the Board and to present my arguments against the application. I would like to take this opportunity to expand and restate my position and I hope they will be provided to the Board members and the applicant.

I live at 3 Washburn Avenue and my home is located at the corner of Washburn and Surrenden, dierctly across from the Bedford Street Parking lot. My driveway which exists onto Surrenden is a narrow single car drive that is easily and frequently blocked. In the thirteen years we have lived here we have seen conditions worsen and the residential nature of the neighborhood compromised by the noise, loss of privacy, litter, safety hazards, and inconvenience that the failure of the University has created. Until now the University has ignored the condition and has become the worst neighbor possible. They pass their failures and problems off to the neighborhood to solve or cope with with little or no regard to our rights. And with the high growth of the student body they are exporting their problems to other neighborhoods every day. The University has acted with arrogance and indifference toward the neighborhood and the students who rely on them to provide basic services like adequate parking and I have no faith that they will improve once they gain approval for the new building.

While I cannot dispute the figures provided by the University documenting their compliance with off street parking, I would simply ask how they can explain the amount of on street parking that exists and how they plan to allieviate that condition. They know they have inadequate off street parking and their lack of planning and questionable policies has created this situation. As to whether the University relies on on street parking for its students needs, of course they do. In fact they have recently managed to get parking on both sides of Surrenden Street for additional parking for students and staff.

Last winter the large amount of snow created many problems. Narrowed street, safety problems, and the loss of hundreds of off street parking spots due to snow storage in the parking lots. I am very concerned about the safety os Surrenden Street this winter and so should the city.

I invite the Board Members and staff to visit my home between 10am and 2:00pm Monday through Wednesday to witness the difficulties of everyone, students, commuters, and neighbors, face. I do not feel should be allowed to proceed with any expansion until they have shown that they can manage their growth.

Respectfully Submitted;

Michael and Donna Gibbs

--





24 January - 7 March

**Duncan Hewitt**  
Visiting Artist Lecture

**January 31 3:30<sub>pm</sub> Thomas Auditorium**  
Opening Reception to follow at the UMF ART Gallery

**University of Maine at Farmington**

Sponsored by: UMF ART Gallery + Visual and Performance Art Dept. Q: (207) 778-7077

02/14/03

# PLANTING LIST FOR BIOSCIENCE PARKING/STREE AREA

2	ABICON 6'	CONCOLOR FIR 6-7'
2	CORKOU 6'	KOREAN DOGWOOD 6-7'
6	GINBMA200	MAGYAR GINKGO 2"
3	FRXACH200	CHICAGO REDAL WHT ASH 2"
1	PAPER2.5"	PARROTIA 2.5"
3	PINDEM4'	SWISSTONE PINE 4-5'
6	PRUMAA200	AMUR CHOKECHERRY 2"
50	AROUVA#1	BEARBERRY #1
8	AESPAR#3	BOTTLEBRUSH BUCKEYE #3
8	CORSWF15"	WINTER FLAME DOGWOOD 15"
5	MALPRO200	PROFUSION CRABAPPLE 2"
8	ENKCAM3'	REDVEIN ENKIANTHUS 3-3.5'
8	HEPMIC#5	HEPTACODIUM MICONIUIDES#5
12	ILXBLD#3	COMPACT INKBERRY #3
1	JUNROB4'	ROBUSTA GREEN JUN 4-5'
10	KALLAT2'	MT LAUREL 2-2.5'
3	THUTEC5'	TECHNY ARBORVITAE 5-6'
3	VIBTOS4'	SHASTA VIBURNUM 4-5'
10	7ASUSSAR#1	SPRITE ASTILBE #1
15	7ANESYLV#1	SNOW DROP ANEMONE
7	7GRMIS6ST#2	STRICTUS EULALIA GRASS
5	7GRHEL6S#1	SAPPHIRE BLUE OAT GRASS#1
5	7HEUMINT#1	MINT FROST CORAL BELL
9	7PERATRI#1	RUSSIAN SAGE #1
31	7HOSPATR	PATRIOT HOSTA HOST
2	GINBMA250	MAGYAR GINKGO 2.5"

Set: 1    Reg: 41    Draw: 41

# PLANT LIST FOR 90 FOR ART BIOSCIENCE

02/14/03

02/14/03

3	AMEAUTC06'	AUTUMN BRILL AME CLUMP 6'
1	BETNISCI0	RIVER BIRCH 10-12' CL
3	MAAAMU150	AMUR MAACKIA 150
1	CERJAPC00	KATSURA TREE 0' CL
0	LARKEM 5'	JAPANESE LARCH 5-6'
3	WALGOL200	GOLDEN RAINDROPS CRAB 2"
2	WALPIS200	PINKSPIRE CRABAPPLE 2"
1	FAGSFA200	FASTIGIATE BEECH 2.00"
3	PINHEL4'	BOSNIAN PINE 4-5'
1	CRYCOL200	TURKISH FILBERT 2"
1	CHMDBC3'	COMPACT HINOKI FAL 3-4'
3	FOTMAJ#2	BOTTLEBRUSH BUSH #2
5	PIEBRO#3	BROUWERS STY ANDRM #3
5	JUNSI#2	SIERRA SPREADER JUN #2
6	JUNPCD#3	COMPACT PFITZR JUN #3
5	JUNPCD#3	COMPACT PFITZR JUN #3
6	VIBLAM3' HVY	MOHICAN VIBURNUM 3-4' HVY
9	ILXOLC#3	COMPACT INKBERRY #3
22	MYRPE#2'	NORTHERN BAYBERRY 2-2.5'
5	BUDBLK#3	BLACK KNIGHT BUDDLEIA #3
14	SPIBDA15"	DARTS RED SPIREA 15-18"
3	SALPUR#2	DWF ARCTIC WILLOW #2
3	SYRTIN#2	TINKERBELLE LILAC #2
1	JUNROB4'	ROBUSTA GREEN JUN 4-5'
40	ARCUVA#1	BEARBERRY #1
3	VIBDAY2.5	CAYUGA VIBURNUM 2.5-3'
2	HAMPAL#3	PALLIDA WITCHHAZEL #3
0	STEINC#1	STEPHANANDRA CUTLEAF #1

**From:** Michael Gibbs <MGibbs@mail.maine.rr.com>  
**To:** Portland.CityHall(SH)  
**Date:** Tue, Oct 9, 2001 2:25 PM  
**Subject:** usm

Thank you for allowing me to expand and clarify my comments to the Planning Board at their September 25, workshop.

I have owned the house at 3 Washburn Avenue since 1988. It is located at the corner of Washburn and Surrenden directly across from the USM parking lot. In the thirteen years I have lived there this is the first time the University has shown any interest in the impact they have had on the neighborhood. I suspect that needing City approval for their expansion plans may have something to do with their new found interest. Over that period we have seen the noise, congestion, loss of privacy, litter, and inconsiderate on street parkers lower the residential nature of our home. Now due to high growth, the University is exporting their negative impact to new neighborhoods. I believe that before they are allowed to expand they must address their parking policy.

The University claims that they do not rely on on street parking to provide needed parking for their staff and students. You only have to drive around the area to know that is not the case. Their use of Marginal Way is ineffective, and their policy regarding parking fees and access to the existing lots, forces people to use the neighborhood street for parking. The new Biology Center will only aggravate the problem. By their count they state that they are 179 parking spots short of compliance. If that were all it would be too much. During peak periods the situation is much worse the the static figures they rely on, and during the winter when they use parking lots for snow storage it is much worse. The new two sides of the street parking on Surrenden, approved by Mr. Ash in the City Traffic Office, is a big mistake and an example of how the University looks to the neighborhood to solve its problems.

It may be too late for my neighborhood, a fact that is forcing me to consider selling because of the changes. However the city can insure that other areas do not suffer the same fate by forcing the University to take responsibility and develop a new, and workable policy before they expand further.

Respectfully

Michael Gibbs

--

**CITY OF PORTLAND, MAINE  
MEMORANDUM**

**TO:** Chair Caron and Members of the Portland Planning Board

**FROM:** Sarah Hopkins, Development Review Program Manager

**DATE:** January 9, 2001

**SUBJECT:** USM Master Plan and Bio-Science Building

**Introduction**

The University of Southern Maine has requested review of its updated Master Plan for the USM Portland campus. The master plan includes several phases which will be implemented over the next 10 years. The first phase will include a Biology Science building addition which will be reviewed by the Planning Board for compliance with Site Plan and Site Location of Development this winter.

The University of Southern Maine campus is 26 acres and is bounded by Forest Avenue, Falmouth, Exeter, Bedford Streets and I-295. The campus is zoned B-2 and R-5. The campus has expanded since 1970 by acquisitions of smaller parcels around the campus perimeter by approximately 9.9 acres.

**Master Plan**

The goals for the Master Plan, as generated by USM's Master Plan Committee, are listed in the Master Plan Book and may be summarized as: strengthen an urban campus; create a sense of community; create identities of buildings; improve the pedestrian environment; establish edges and gateways; strengthen the relationship between the campus and the City of Portland; improve the integration of the library into the campus; improve green space; and consolidate services to improve efficiency and function.

As part of its Master Plan, the University conducted a site analysis and studied existing building uses, vehicular circulation, parking availability and demand, pedestrian circulation and open space.

**Master Plan Phases**

The Master Plan incorporates five major elements that will be implemented over several phases during the next 10+ years. These elements include:

1. creation of a pedestrian boulevard along Bedford Street.
2. create a new campus entrance along Forest Avenue with campus buildings at the entrance.
3. relocate parking out of the middle of the campus to a parking garage.
4. create a campus open space by eliminating the central parking lot.
5. construction of new buildings similar to Payson Smith.

### Phase A1: Biology Science Building – Immediate

The first phase of the Master Plan is the construction of the Bio-Science addition and reconfiguration of the existing parking lot. The Bio-Science building addition will be six stories with a footprint of 9,500 sq. ft. and total square footage of 51,756.

The applicant has applied for major site plan review for the Bio-Science building and parking lot reconfiguration. The project will also be reviewed for compliance with Site Location of Development under the City's delegated authority.

Once the Planning Board has reviewed the Master Plan, USM is prepared to present its application for the Bio-Science building.

### Phase I: Community Education Facility/Parking Garage & Muskie Institute

The Community Education Facility will be located between Bedford Street and I-295 and will include a 500 seat auditorium, campus bookstore, small business development and family owned business services and other classrooms and office space.

The garage will have three levels with 2 vehicle access points off Winslow and Chamberlain Streets.

The Steego building will be rehabilitated and expanded to provide 80,000 sq. ft. of new space for the Muskie School of Public Policy.

### Phase 2: 5-10 years

1. new office/administrative space (3 buildings at 25,000 each) along Falmouth, Bedford, Winslow, and Law School
2. open Bedford Street as a pedestrian promenade, restrict and reroute traffic
3. create a gateway to the University from Forest Avenue

### Phase 3: 10+ years

1. skybridges connecting the major activity centers
2. academic buildings (3 buildings at approximately 25,000 sq. ft. each) along Deering, Bedford, and Forest
3. gymnasium expansion, construction of arcade

### **Planning Board Review**

During Tuesday's workshop, the applicant will present the Master Plan for the USM campus and introduce the Bio-Science building within that context.

The issues to be addressed by the applicant include:

- Parking supply during and after construction
- Construction phasing related to parking and circulation
- Traffic Circulation, impacts on Forest Ave, implications of Bedford Street as a pedestrian promenade, street vacation, etc
- Design, massing of the Bio Science addition
- Neighborhood Impacts of overall master plan

The applicant has submitted an extremely thorough Master Plan and Site Plan application. We have attached the Master Plan booklet to this memo and will work to forward additional information to the Planning Board during subsequent workshops.

#### Attachments

1. Existing Conditions
2. Bio-Science Institute Location Plan
3. Bio-Science Site Layout
4. Bio-Science Perspective Drawings

Master Plan Book

**PLANNING BOARD REPORT #56-01**

**BIOSCIENCE BUILDING ADDITION  
VICINITY OF 96 FALMOUTH STREET  
SITE PLAN, CONDITIONAL USE, SHARED PARKING,  
AND SITE LOCATION OF DEVELOPMENT REVIEW  
UNIVERSITY OF MAINE, APPLICANT**

Submitted to:

Portland Planning Board  
Portland, Maine

November 13, 2001



## **I. INTRODUCTION**

The University of Maine has requested approval by the Planning Board for the construction of a Bio-Science building on Falmouth Street within the Portland campus. The Bio-Science building will be reviewed for conformance with the Site Plan Ordinance and Conditional Use standards for institutional expansion in a residential zone, and standards for MEDEP Site Location of Development review.

The University of Southern Maine campus is 26 acres and is bounded by Forest Avenue, Falmouth, Exeter, Bedford Streets and I-295. The campus is zoned B-2 and R-5. The campus has expanded since 1970 by acquisitions of smaller parcels around the campus perimeter by approximately 9.9 acres.

The Bio-Science building addition will be two stories above ground and one basement level. The footprint of the building is approximately 9,500 sq. ft. and the total square footage of the building will be 27,000sq.ft.

## **II. FINDINGS**

Zoning:	R-5
Land Area:	26 acres
Number of Parking Spaces:	
Required:	1,625
Proposed:	1,554
Shared Parking Application	179

## **III. STAFF REVIEW**

The proposal has been reviewed for compliance with the Site Plan Ordinance and conditional use standards of the Land Use Code, As well as Site Location of Development Review. The plan has been reviewed by the Inspections, Traffic, Fire, Public Works, and Planning Departments. Dufresne Henry has provided the engineering review for the project.

## **IV. CONDITIONAL USE REVIEW**

The following conditions must be met by a proposed conditional use:

### Section 14-118

- i. In the case of expansion of existing such uses onto land other than the lot on which the principal use is located, it shall be demonstrated that the proposed use cannot reasonably be accommodated on the existing site through more efficient utilization of land or buildings, and will not cause significant physical encroachment into established areas; and

The placement of the addition will be within the university's property

- ii. The proposed use will not cause significant displacement or conversion of any residential uses as of June 1, 1983, or thereafter; and

The bio-science building and accompanying improvements will not cause the displacement or conversion of any residential units.

- iii. In the case of a use or use expansion which constitutes a combination of an above-listed use with capacity for concurrent operations, the applicable minimum lot sizes shall be cumulative.

#### Section 14-474

- i. There are unique or distinctive characteristics or effects associated with the proposed conditional use;

There are no known unique or distinctive characteristics associated with the proposed use.

- ii. There will be an adverse impact upon the health, safety, or welfare of the public or the surrounding area; and

It does not appear that there will be any adverse impacts associated the proposed project.

- iii. Such impact differs substantially from the impact which would normally occur from such a use in that zone.

The impacts of this site are similar as those normally expected from such a use in this zone.

## V. SITE PLAN REVIEW

### 1. Traffic/Circulation/Parking

Vehicular traffic will continue to enter the Falmouth Street parking lot adjacent to the proposed Bio-Science building. The lot will be lowered and regraded to allow for a loading dock off the street, below the sidewalk level. The building addition will replace approximately 35 parking spaces.

At the Planning Board's request, the applicant has submitted a comprehensive parking plan to answer questions of the Board and neighbors regarding the availability and management of parking on the campus. The applicant has worked with the City's Traffic Engineer and Parking Manager to create and test the plan. Both Larry Ash, the City's Traffic Engineer, and John Peverada, the Parking Manager, will be present at the public hearing to discuss the plan and describe its correlation to the City's parking and Traffic management efforts in the USM neighborhood.

The applicant has conducted an analysis of existing overall parking space availability. The parking inventory includes 300 parking spaces at Marginal Way, in addition to parking lots campus-wide, for a total of 1517 spaces

The parking space calculation, when compared to the parking required by ordinance, indicate a deficit of 179 parking spaces.

The applicant is therefore requesting shared parking approval for the 179 parking spaces. The applicant argues that several of the uses on campus that require parking are uses that are being used in conjunction with another service or use on the campus. These uses for which parking requirements may be redundant include the library, lounge, food facility, and merchandising services. Random surveys conducted recently on the campus support this argument.

### Shared Parking

Under Section 14-343 of the Land Use Code, the Planning Board is given the authority to approve shared parking in cases where the following condition has been met:

“...where it is clearly demonstrated that the parking facility will substantially meet the intent of the requirements by reason of variation in the probable time of maximum use by patrons or employees among such establishments.”

### 2. Bulk, Location, Height of Building and Uses Thereof

The Bio-Science building will be approximately 34 ft high and will connect internally to the existing science building and planetarium on Falmouth Street. Due to the type of research to be conducted in the building and the absence of teaching facilities, the applicant has not proposed a public entrance to the building from the quad or from Falmouth Street.

In response to questions raised by Planning Board members regarding the façade of the building along Falmouth Street, the applicant has added windows to the north elevation and has updated the building elevation perspectives to reflect the changes. Also, the applicant has proposed further landscaping improvements to the existing entrance of the science building in order to reinforce its role as the central, primary access to the entire building.

The applicant’s architect will be available at the workshop to discuss the building concepts and floor plans.

### 3. Utilities

The applicant will maintain its existing utility connections in Falmouth Street.

### 4. Landscaping

Linden trees are proposed along the Falmouth Street sidewalk, to be planted 40 feet on center. Red Oaks and Summit Ash trees will be planted between the lot and the quad with a substantial landscape redesign including several shrubs, perennials and groundcovers to transition the building into the campus.

## VII. MOTIONS FOR THE BOARD TO CONSIDER

On the basis of plans and materials submitted by the applicant and on the basis of information provided in Planning Board Report #56-01, the Board finds:

- i. That the plan is/is not in conformance with the Conditional Use Standards of the Land Use Code.
- ii. That the plan is/is not in conformance with the Joint Use Standards of 14-343 of the Land Use Code.
- iii. That the plan is/is not in conformance with the Site Plan Standards of the Land Use Code.
  - That the applicant provide a detailed lighting plan with associated photometrics for Planning staff review and approval.
- iv. That the plan is/is not in conformance with the standards for Site Location of Development review.

### Attachments

1. Neighborhood Meeting Information
2. Memo from Public Works
3. Comments from the Zoning Administrator
4. Comments from Reviewing Engineer
5. Comments from the Parking Manager

### Plans:

1. Perspective Drawing
2. Existing Conditions
3. Grading/Utility Plans
4. Landscaping Plans
5. Building Elevations

### Parking Management Plan



# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4200 FAX (207) 780-4860  
TTY (207) 780-5646

August 22, 2001

Ms. Sarah Hopkins  
Development Review Program Manager  
City of Portland  
389 Congress St.  
Portland, ME 04101

Dear Sarah:

As requested, I'm forwarding summaries of our meetings with neighbors who live in the vicinity of USM's Portland campus. USM representatives met with neighbors three times between February and April of this year.

For your reference, I've also included letters of invitation to each of the meetings, and USM's responses to the concerns raised at the meeting of April 5. To help you identify which pieces of information go with which meeting, I've numbered the pages as follows:

1. Invitation to the meeting of February 9.
2. Summary of the meeting of February 9.
3. Invitation to the meeting of March 16.
- 4, 5, 6. Summary of the meeting of March 16.
7. Invitation to meeting of April 5.
- 8, 9. Summary of meeting of April 5.
- 10, 11, 12. USM response to concerns raised at meeting of April 5.

To continue our dialog with neighbors, we will offer neighborhood representation on all future building committees; hold informational meetings each fall; and host meetings related to specific building projects to identify any concerns.

Please feel free to give me a call (780-4200) or an e-mail ([caswell@usm.maine.edu](mailto:caswell@usm.maine.edu)) if you have any questions regarding this information.

Sincerely,

A handwritten signature in black ink that reads "Bob Caswell".

Bob Caswell  
USM Media & Community Relations

RSC/jao

Enclosures

①



# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

96 Falmouth Street  
P.O. Box 9300  
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January 25, 2001

Dear USM Neighbor:

We'd like to invite you to a neighborhood meeting to find out more about building plans on the Portland campus of the University of Southern Maine.

The meeting will be held at 7 p.m., Friday, February 9, in the cafeteria of the Woodbury Campus Center, Bedford St., Portland. There should be adequate parking in the Bedford St. lots, located a short distance from the intersection of Bedford and Forest Ave.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP to 780-4200.

USM officials will be outlining the plans for the Biosciences Research Institute. This facility, which will be financed with state research and development funds, is planned as an addition to the Science Building on Falmouth St.

We also want to share with you our master plan for the development of the Portland campus. For the most part, the plan represents a 20 to 30-year vision as to how our Portland campus might develop, depending on state funds, private gifts, enrollment trends and other factors. There are only three projects under consideration during the next five years: the Biosciences Research Institute; renovation of the former Steego Building on Bedford St. to house our Muskie School; and construction of a Community Education Center/parking garage on the current Bedford St. parking lot.

USM's mission is to enhance the region's quality of life. Our success in doing that depends on thoughtful partnerships with the community. We value your input and hope to see you on the 9th.

Sincerely,

Bob Caswell  
Executive Director, Media & Community Relations

Dave Early  
Executive Director, Facilities Management

RSC/jao

2



# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

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## REVIEW OF USM NEIGHBORHOOD MEETING

The following is an outline of the discussions from the neighborhood meeting, held at 7 p.m., Friday, February 9, in the cafeteria at the University of Southern Maine's Woodbury Campus Center, Portland.

Thanks to the cooperation of the city's Planning Department, we were able to mail 1,219 letters of invitation to residents in an area roughly bounded by Back Cove, the Parkside neighborhood, St. John St. and through the Oakdale section to Douglass St.

Thirty-five people attended, and shared with us concerns ranging from parking to the type of research to be conducted in the proposed Biosciences Institute.

I've grouped those concerns under the following headings, and under each heading have summarized issues that were addressed.

### **PARKING/TRAFFIC:**

Much of the 3+ hour meeting was devoted to this issue. In the short term, many residents felt that our plan to compensate for loss of parking during construction of the Biosciences Institute was flawed in that most people would not use the Marginal Way lot. We responded that an aggressive campaign to inform people of the Marginal Way lot, coupled with a beefed up shuttle bus service, would serve us and the neighbors well during the construction period.

It was further explained that the University is implementing a revised class schedule for the fall, 2001. The schedule is designed to help distribute uniformly space demands throughout the week as opposed to the current schedule that causes peak demands on Tuesdays and Thursdays.

Several questioned why we are planning to construct a research building before a parking garage. We explained that dedicated funding that must be used or be forfeited is in place for the research facility. We also noted that the facility will be dedicated exclusively to research and will not include classrooms and other high-traffic areas. Guests questioned the number of researchers that would be housed in the building.

Neighbors also voiced frustration at students' cars that are taking up on-street parking in and around their neighborhoods. A USM/city collaborative effort to address the problem ought to be initiated, they said. As part of this discussion, people asked why the long-range campus master plan did not contain any provisions for developing on-campus student housing. We explained that our plans call for continuing to emphasize Gorham as a traditional campus environment. A new 224-bed residence hall will open in Gorham this summer. Portland Hall on Congress St. seems to satisfy student demand for Portland-based housing. We have had informal discussions about the possibility of a cooperative agreement with private developers to provide housing but there are no plans to do so.

Guests also expressed concerns over the lack of studies detailing how the closing of Bedford St., for example, would affect traffic flows in the neighborhoods. We explained that the Bedford St. proposal is one of several long-range visions included as part of the campus master plan. As such, it is so far in the future that it does not yet warrant detailed studies.

//more on back

March 2, 2001

Office of Media and Community Relations  
96 Falmouth Street  
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Dear USM Neighbor:

We'd like to invite you to a neighborhood meeting at 7 p.m., Friday, March 16, in the cafeteria of the Woodbury Campus Center, Bedford St., Portland.

Parking Manager John Peverada and Traffic Engineer Larry Ash of the City of Portland will join us so that we can respond to your questions and concerns about traffic and parking in the campus neighborhoods. A special thanks goes to City Councilor Tom Kane for his help in making these arrangements.

We also want you to know that we heard the concerns expressed at the meeting of February 9, especially those related to a lack of neighborhood input into the development of campus plans.

As a start toward building a better relationship, we want to offer neighborhood representation on university building committees, and host meetings related to specific projects so that we can identify concerns and gain your input. We can discuss these and other ideas in more detail on the 16th.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP at 780-4200.

We hope to see you on the 16th.

Sincerely,



Bob Caswell  
Executive Director, Media & Community Relations



Dave Early,  
Executive Director, Facilities Management

RSC/jao



February 27, 2001

To: Sarah Hopkins, Development Review Program Manager  
City of Portland

From:  David Early, P.E. Executive Director

Subject: Proposed U.S.M. Bio-Science Institute

The following information has been enclosed as it may aid the Planing Board in their review of U.S.M.'s proposed Bio-Science Institute.

1. **Parking Inventory:**

U.S.M.'s Portland Campus presently has a parking space inventory of 1,605 parking spaces. Our peak demand per DeLuca-Hoffman Associates Parking and Traffic Study is 1,321. This leaves 284 available parking spaces. These available spaces are typically located at 15 Baxter Boulevard and the Marginal Way Parking Lot.

During the construction of the Bio-Science Institute, 98 cars will be displaced from U.S.M.'s P-9 parking lot. Ultimately the capacity for these 98 parking spaces will end up at 15 Baxter Boulevard and Marginal Way.

Our Construction Contract will specify that Construction Workers park off campus. Space will be made available for these workers only at the Marginal Way Parking Lot.

An inventory of U.S.M. Parking is attached. Our Enrollment/Parking Mitigation Plan was distributed to the Board in January. Another copy is attached for convenience. I have also included the most recent "Metro" report on the U.S.M./METRO 50-CENT FARE program. The report identifies 19,101 rides on this program since March of 2000.

## **2. Neighborhood Meeting**

On Friday, February 9th a meeting was held at U.S.M.'s Woodbury Campus Center with Neighbors. 1,219 invitations were sent to our neighbors. We had 35 people in attendance. A copy of our letter of invitation and a record of the meeting is attached. Our next neighborhood meeting is scheduled for Friday, March 16th. This meeting will be on Parking & Traffic. The DeLuca-Hoffman Associates Report will be reviewed with our neighbors in detail. City, as well as University Officials, will be present to meet with U.S.M. neighbors.

## **3. Building Elevations and Landscape Plan**

Leslie Glynn, AIA, Associate Principal of Symmes Maine & McKee Associates is prepared to discuss in detail the appearance and presentation of the Building from both the Falmouth Street (north) as well as Campus (south) elevations. The proposed addition to the existing Science Building results in the re-location of the present loading dock facing Falmouth Street. This loading dock is relocated to the lower, basement level, and turned to face the Sullivan Gym. It will no longer be in plain site on Falmouth Street, as the present one is. No additional entrance / exits are added to the Science Building Complex as a result of this addition, particularly for reasons of building security. The addition itself connects to existing circulation corridors, and building entrances/exits. Additional work is proposed to improve the appearance of the existing 3 story precast structure. Ms. Glynn will be prepared to provide a full explanation of the landscape plan as well.

### **Attachments:**

1. USM Parking Inventory, December-2000
2. USM Parking Space Availability at Peak Demand
3. January 18, 2001 Letter from Sam Andrews  
on Enrollment and Parking Mitigation
4. January 25, 2001 Letter to USM Neighbors
5. Notes - Review of USM Neighborhood Meeting
6. Colored Site Plan
7. Colored Building Elevations

**UNIVERSITY OF SOUTHERN MAINE  
PARKING INVENTORY - PORTLAND CAMPUS  
DECEMBER, 2000**

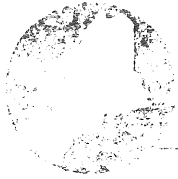
LOT	LOCATION	DESIGNATION	PARKING SPACES
P1	Luther Bonney	Faculty/Staff Handicapped	28 8
P2	Campus Center	Student Commuter Univ. Health Services Handicapped	182 1 7
P3	Bedford Street	Student Commuter Meters Handicapped	536 2 1
P4	Alumni House	Faculty/Staff	30
P5	Bedford Street	Faculty/Staff	7
P6	Law Building	Faculty/Staff Meters Handicapped	137 7 6
P7	Bookstore DFM	Faculty/Staff Handicapped	41 1
P8	Child Care	Faculty/Staff 15 Min Drop-Offs Handicapped	12 4 2
P9	Science/Gym	Faculty/Staff Motorcycles Handicapped	109 6 2
P10	Payson Smith	Meters Handicapped	6 4
P11	Science Front	Meters	4
P12	Police & Safety	Student Commuter Police Parking 15 Min Police Business Handicapped	12 1 2 1
P13	Library	Faculty/Staff Meters Handicapped	28 9 4
P14	Marginal Way	Student Commuter Handicapped	293 7
	15 Baxter Boulevard	Student Commuter Handicapped	53 2
	Grand Street	Meters	12
	Outbuildings	Faculty/Staff	38
		<b>TOTAL</b>	<b><u>1,605</u></b>

**University of Southern Maine  
Portland Campus  
Master Plan**

**Existing Overall Parking Space Availability**

Location	Available Spaces	Demand*	Unoccupied Spaces
P 1	36	37	-1
P 2	190	189	1
P 3	539	567	-28
P 4	30	32	-2
P 5	7	8	-1
P 6	150	136	14
P 7	42	45	-3
P 8	18	18	0
P 9	117	119	-2
P 10	10	9	1
P 11	4	1	3
P 12	16	13	3
P 13	41	40	1
Marginal Way	300	52	248
15 Baxter Blvd.	55	13	42
Grand Street	12	12	0
Outbuildings	38	30	8
<b>Total</b>	<b>1605</b>	<b>1321</b>	<b>284</b>

\* Demand = Peak 10:00 AM Demand on a Tuesday. Tuesday was identified by the University as the peak weekday for enrollment.



# UNIVERSITY OF SOUTHERN MAINE

Office of Finance and Administration

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P.O. Box 9300  
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(207) 780-4484  
FAX (207) 780-4549  
TTY (207) 780-5646

DATE: January 18, 2001

TO: Mr. David Early, Executive Director, Facilities Management

FROM: Samuel G. Andrews, Office of Finance and Administration

RE: Biosciences Research Institute Addition

As I understand it, the City of Portland Planning Board and Planning Staff requested some additional information relative to future enrollments at the Portland campus as well as information on USM's parking mitigation efforts. In regard to the Biosciences Research Institute Addition to the Portland Science Building, it should be noted that this is a research facility only and will not have any impact at all on student enrollment.

The Maine Legislature has made two targeted additions to the University of Maine System base budget designed to substantially increase research and development (R&D) activity at the University of Maine and the University of Southern Maine, in particular to strengthen the capacity of both Institutions to compete for Federal grants and contracts. The annual base budget operating increase is \$10 million plus \$2.5 million to cover debt service payments on a \$25 million internal bond issue for a period of 15 years. The general goal is that together the Universities will generate an additional \$40-\$50 million annually in external research funding from the investment base of \$10 million. For USM the additional per year operation investment will be \$2 million directed at research faculty and staff, equipment and materials in the biosciences, information sciences, and advanced materials, primarily electronics. In addition, USM has a total of \$7.7 to build and renovate research laboratories (\$2.7 M from a bond referendum approved by voters in 1998 and \$5M from the campus share of the internal bonding capacity).

Exhibit I presents student headcount enrollment for the Portland campus for the fall, 1998 through the fall, 2000 semester with a projection through the fall, 2020 semester. The table shows Weekend College enrollment (classes offered at 4:00 p.m. and after on Friday through Saturday and Sunday) and Regular Enrollment (classes offered from 8 a.m. on Monday through 4:00 p.m. on Friday). About 20% of the students attending the Portland campus are present on Friday, Saturday and Sunday and not during the week days. The introduction of Weekend College was to provide access to adult learners and to relieve pressure on classrooms, laboratories, and parking resources during the week. The enrollment projection takes into account the projected population trends in southern Maine over the next twenty years; projected high school senior population in Maine as a whole, and the high school senior population in southern Maine, specifically; the projected rate of college attendance among high school seniors; and the post-secondary educational participation rate among adults. The projected rates of increase in student headcount enrollment are noted at the bottom of Exhibit I.

The parking and traffic study carried-out in connection with the permitting of the Science Building Addition was carried out on a Tuesday. Exhibit II shows the number of students registered for Portland campus classes at one hour intervals throughout the week. For example, at 8:00 a.m. on Monday morning there were 248 students registered to take classes at the Portland campus while at 2:00 p.m. on Monday, there were 1,116 students registered to be in class at that time. The

**UNIVERSITY OF SOUTHERN MAINE  
ACTUAL & PROJECTED STUDENT HEADCOUNT  
FALL, 1998 to FALL, 2020**

<u>Actual Student Headcount</u>	<u>Weekend College*</u>	<u>Regular Enrollment**</u>	<u>TOTAL</u>
Fall, 1998	940	4,786	5,726
Fall, 1999	1,015	4,500	5,515
Fall, 2000	1,077	4,386	5,463
 <u>Projected Student Headcount</u>			
Fall, 2001	1,109	4,474	5,583
Fall, 2002	1,142	4,563	5,705
Fall, 2003	1,176	4,654	5,830
Fall, 2004	1,211	4,747	5,958
Fall, 2005	1,247	4,842	6,089
Fall, 2006	1,259	4,890	6,149
Fall, 2007	1,272	4,939	6,211
Fall, 2008	1,284	4,989	6,273
Fall, 2009	1,297	5,039	6,336
Fall, 2010	1,310	5,089	6,399
Fall, 2011	1,317	5,113	6,430
Fall, 2012	1,324	5,137	6,461
Fall, 2013	1,331	5,161	6,492
Fall, 2014	1,338	5,185	6,523
Fall, 2015	1,345	5,209	6,554
Fall, 2016	1,352	5,233	6,585
Fall, 2017	1,359	5,258	6,617
Fall, 2018	1,366	5,283	6,649
Fall, 2019	1,373	5,308	6,681
Fall, 2020	1,380	5,333	6,713

Enrollment projection for Fall, 2001 to Fall, 2005 includes a 2% annual increase for the School of Law, a 3% annual increase for Weekend College and a 2% annual increase for Regular Enrollment. Enrollment projection for Fall, 2006 to Fall, 2010 includes a 1% annual increase for all categories and the projection for Fall, 2011 to Fall, 2020 includes a 1/2% annual increase for Weekend College and Regular Enrollment and no increase for School of Law

\* Includes classes offered at 4:00 p.m. and after on Friday and those offered on Saturday and Sunday.

\*\* Includes classes offered from 8 a.m. on Monday through 4:00 p.m. on Friday.

**UNIVERSITY OF SOUTHERN MAINE  
STUDENT ENROLLMENT BY HOUR  
PORTLAND CAMPUS - FALL, 2000**

	<u>Monday</u>	<u>Tuesday</u>	<u>Wednesday</u>	<u>Thursday</u>	<u>Friday</u>	<u>Saturday</u>
8 a.m.	248	11	218	2	41	20
9 a.m.	548	842	534	768	218	436
10 a.m.	595	1,239	568	1,180	287	441
11 a.m.	498	1,270	462	1,194	270	448
Noon	905	1,006	874	914	203	108
1 p.m.	941	1,090	785	976	273	106
2 p.m.	1,116	1,153	839	1,064	273	111
3 p.m.	888	887	689	840	121	73
4 p.m.	1,235	1,182	1,127	1,092	352	36
5 p.m.	1,091	1,083	1,040	1,019	344	0
6 p.m.	1,056	988	1,051	909	345	0
7 p.m.	774	767	798	648	40	0
8 p.m.	736	666	689	527	35	0
9 p.m.	<u>710</u>	<u>617</u>	<u>666</u>	<u>503</u>	<u>16</u>	<u>0</u>
<b>TOTAL</b>	<b><u>11,341</u></b>	<b><u>12,801</u></b>	<b><u>10,340</u></b>	<b><u>11,636</u></b>	<b><u>2,818</u></b>	<b><u>1,779</u></b>

Dave Early cont'd  
page 2  
January 18, 2001

enrollment by hour data clearly indicates that Tuesday is the busiest day of the week and Tuesday attendance exceeds Monday attendance by 12.9%. Tuesday enrollment is 23.8% greater than that on Wednesday; Tuesday has 10.0% more students than on Thursday; and Tuesday has enrollment that is 354.3% greater than on Friday.

The University of Southern Maine provides three shuttle bus services. One such service runs between the Portland and Gorham campuses, with one bus leaving each campus at 30 to 45 minute intervals from about 7:00 a.m. to 10:00 p.m. During the 1999/00 academic year, this shuttle service provided 92,936 riders with transportation between the two campuses. Exhibit III, Page 1 shows time schedule for this shuttle service during the spring, 2001 semester and Exhibit III, Page 2 indicates the service times on Saturday for the Gorham and Portland shuttle, and the Monday through Friday service times for the separate and distinct shuttle service provided between Portland Hall and the Portland campus. During the 1999/00 academic year, there were 23,234 riders who took the Portland Hall/Portland campus shuttle service.

Exhibit III, Page 1 also indicates that a shuttle bus runs continually from the Marginal Way parking lot to the Portland campus, starting at 7:15 a.m. with the last shuttle returning to the Marginal Way lot at 10:00 p.m. For University staff or students wishing to use a University parking lot, there is a parking decal fee of \$15 per semester or \$25 per year. Those who purchase a Marginal Way decal (322 were sold in 1999/00), the cost is \$5 per year. The Marginal Way shuttle service operates Monday through Thursday when classes are in session. When the Marginal Way shuttle service is not being provided, those with Marginal Way parking decals are able to park in campus lots. During the 1999/00 academic year, the Marginal Way shuttle service to the Portland campus had 7,397 riders with 60% of the ridership occurring during the spring semester.

During the 1999/00 fiscal year, the University took a lease for office space at 15 Baxter Boulevard, just across from the University Library and a short walk from campus. This was for a three year period with renewal options and provided office space for several Muskie School of Public Service grant/contract activities. This property included 52 additional parking spaces that were available for use by University of Southern Maine staff and students with University parking decals.

Construction of the Biosciences Research Institute Addition to the Portland Science Building will cause much of the current parking lot between the Science Building and the Sullivan Gym (approximately 104 parking spaces) to be used for construction and staging. Currently, this is a parking lot designated for faculty/staff parking. The lot in front of the Woodbury Campus Center which is currently designated for students only will be opened to faculty and staff during the construction period. Also, during this construction period, Marginal Way and Baxter Boulevard will be available as alternative parking sites for the academic year. In order to have people use Marginal Way or Baxter Boulevard as alternatives, there will be better designation and publicity about where the



SHUTTLE BUS SCHEDULE  
EFFECTIVE JANUARY 16, 2001  
MONDAY THROUGH FRIDAY

There will be one bus per run in each direction. If all seats are filled, standing will be allowed.

LEAVES PORTLAND

7:25 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4:10  
 4:50  
 5:35  
 6:15  
 7:00  
 8:30  
 8:45(via Mall Mon-Fri)  
 10:00 (Mon-Thurs)

LEAVES GORHAM

7:15 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4:10  
 4:50  
 5:35  
 6:15  
 7:00  
 8:15 (via Mall Mon-Fri)  
 9:35 (Mon-Thurs)  
 10:20 (Mon-Thurs)

A shuttle bus will run continually from the Marginal Way lot, in Portland, with stops at the Campus Center, Payson Smith Hall and other on-campus locations as needed, and return to the Marginal Way lot at 7:15a.m., Monday through Thursday. **At 8pm, the shuttle that serves Portland Hall will also serve as the Marginal Way shuttle.** The last bus will return to the Marginal Way lot at 10:00pm, Monday through Thursday. **There will be no Marginal Way bus on Fridays and weekends.** **NOTE: VIP driver shift change occurs at approximately 3:30pm.**

**BUS SCHEDULES ARE SUBJECT TO CHANGE EACH SEMESTER AND DURING FINAL EXAMS.**

PORTLAND HALL SHUTTLE BUS SCHEDULE  
PORTLAND HALL RESIDENTS  
EFFECTIVE JANUARY 16, 2001

LEAVES PORTLAND HALL

7:00a.m.  
 7:45  
 8:15  
 8:45  
 9:15  
 9:45  
 10:15  
 10:45  
 11:45  
 12:15p.m.  
 12:45  
 1:15  
 1:45 (last run on Fri)  
 2:15  
 3:45  
 4:15  
 4:45  
 5:15  
 5:45  
 6:15  
 6:45  
 7:15  
 7:45  
 8:15 \*also serves Marginal Way  
 8:45 \*  
 9:15 \*  
 9:45 \*

LEAVES CAMPUS CENTER

7:30a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 12:00noon  
 12:30p.m.  
 1:00  
 1:30 (last run on Fri)  
 2:00  
 2:30  
 4:00  
 4:30  
 5:00  
 5:30  
 6:00  
 6:30  
 7:00  
 7:30  
 8:00 \*also serves Marginal Way  
 8:30 \*  
 9:00 \*  
 9:30 \*  
 10:05 \*

No Runs on Saturday and Sunday. No Runs to 68 High St.

NOTE: If the shuttle is full, there will not be another shuttle until the next scheduled departure.

SATURDAY MINIBUS SCHEDULE  
EFFECTIVE JANUARY 16, 2001

LEAVES GORHAM

8:30am (no Mall Stop)  
 9:30  
 11:30  
 2:00pm  
 4:00  
 6:00

LEAVES PORTLAND

9:00am (no Mall Stop)  
 10:30  
 12:30pm  
 3:00  
 5:00  
 7:00

NOTE: All Saturday Trips, except 8:30am & 9:00am, Stop at the Maine Mall.

Bus service is provided during the fall and spring semesters when classes are in session. All bus schedules are reviewed by Student Life and the Registrar's Office. Copies are available at the USM Police Department. Please call USM Parking and Transportation at 780-5254, with any questions or concerns regarding transportation services.

Dave Early, cont'd

page 3

January 18, 2001

Marginal Way shuttle bus stops are located on the campus and about the location of a shuttle bus stop being instituted at Baxter Boulevard. Also, there will be a more clearly delineated time schedule for the Marginal Way/Baxter Boulevard shuttle and better maintenance of the time schedule to instill confidence in the service on the part of users. Once the construction phase of the Biosciences Addition is completed, the lot between the Science Building and the Sullivan Gymnasium will be returned to use, although there will be a loss of 44 parking spaces due to the Addition.

Over the past two years, the University Community has discussed changes in the scheduling of classes in order to spread the scheduling of classes more evenly over the Monday through Friday period. Through this revamping of the course scheduling, it is hoped that time blocks can be more effectively utilized and will even-out some of the peaks and valleys in student attendance. Exhibit IV, Pages 1 & 2, are the announcement from Provost Joseph S. Wood to USM Faculty and Staff concerning the new class schedule to be implemented effective with the fall, 2001 semester. As indicated in the memo, "implementation should open-up classrooms and also help relieve some parking stress on Tuesdays and Thursdays", the two days that have the highest parking demand.

The use of the three University of Southern Maine shuttle bus services, the invigoration of the Marginal Way parking alternative, the addition of parking spaces at 15 Baxter Boulevard and the implementation of the new class schedule should provide sufficient results to cope with any new parking demand resulting from the addition of the Biosciences Research Institute to the Portland campus. As a purely research oriented facility, the new Addition will not generate any new students for the campus; only research personnel who will be working in the new faculty.

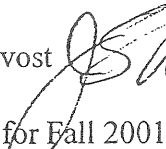
In addition to the items outlined, the University of Southern Maine is and has been an active partner with the Metro to create awareness and use of the Metro as an alternative to bringing a vehicle to the Portland campus. Exhibit V is material shared with the USM Parking and Transportation Review Committee concerning USM's effort with the Metro since March of 2000. From March through August, 2000, the free Metro ride program involved 11,346 riders, and the "50-Cent Ride" program generated 4,517 Metro riders during the September through November period. Through the \$11,000 in financial support being provided to the Metro by USM for the "50-Cent Ride Anywhere, Anytime" program, hopefully, some current drivers to campus will switch and use public transportation.

SGA/c

attachments

December 14, 2000

Office of the Provost  
96 Falmouth Street  
P.O. Box 9300  
Portland, ME 04104-9300  
(207) 780-4485  
TTY (207) 780-5646  
FAX (207) 780-4549

TO: USM Faculty and Staff  
FROM: Joseph S. Wood, Provost   
RE: New Class Schedule for Fall 2001

With full administrative support and endorsement of the Faculty Senate, we will implement a new class schedule effective in the fall semester of 2001. Perhaps the least problematic of various options we have considered over the last few years, the new schedule builds on 90-minute morning time blocks on Monday-Wednesday-Friday, as well as on Tuesday-Thursday. See the attached complete schedule prepared by the Registrar.

Within the new 90-minute morning time blocks for Monday-Wednesday-Friday, scheduling priority will be given to 50-minute classes (3 credit hours) or 75-minute classes (4 credit hours) running over all three days of the week. Friday mornings also provide opportunity for innovative pedagogies that do not correspond to conventional scheduling.

We will adhere strictly to time blocks due to the availability of classroom spaces. The schedule does not affect offering of studio and laboratory classes, except insofar as they may necessarily have to adapt to new starting times.

The new schedule will incorporate afternoon changes established for spring semester 2001. To allow for a 45-minute bus trip between campuses all day long, Portland classes will begin at 8:45 a.m. all days. Implementation should open up classrooms and also help relieve some parking stress on Tuesdays and Thursdays.

As always, Deans are responsible to work with faculty to develop schedules that most effectively allow students to meet degree requirements. Registrar Steve Rand will work closely with Deans to reduce or eliminate scheduling conflicts.

I recognize that we will encounter unforeseen difficulties in implementing this schedule change, and I ask your support and patience as we proceed.

JSW/es

Beginning with the Fall 2001 semester the normal scheduling periods will be changed.

All periods on Monday thru Thursday before 4:00 p.m., will be 75 minutes with a 15 minute break between periods. This schedule will be the same for each day.

Periods after 4:00 p.m. will accommodate either 75 minute periods or 150 minute (2.5 hours) periods.

Friday will normally consist of two 2.5 hour periods.

PORTLAND		GORHAM	
MTWR	F	MTWR	F
8:45 - 10:00	8:45 - 11:15	8:00 - 9:15	8:00 - 10:30
10:15 - 11:30	11:45 - 2:15	9:30 - 10:45	11:00 - 1:30
11:45 - 1:00		11:00 - 12:15	
1:15 - 2:30		12:30 - 1:45	
2:45 - 4:00		2:00 - 3:15	
4:10 - 5:25		4:10 - 5:25	
5:35 - 6:50		5:35 - 6:50	
(4:10 - 6:40)		(4:10 - 6:40)	
7:00 - 9:30		7:00 - 9:30	

Notes

- 1) A 3 credit class offered in a 75 minute period will be offered either M/W or T/R.
- 2) A 3 credit class offered 3 times a week will be offered M W F during 50 minutes of a single period time block.
- 3) Classes that are 4 credit hours can be offered in a single period time block on M W F, with one meeting per week being 50 minutes.



## UNIVERSITY OF SOUTHERN MAINE

Office of Finance and Administration

 96 Falmouth Street  
 P.O. Box 9300  
 Portland, Maine 04104-9300  
 (207) 780-4484  
 FAX (207) 780-4549  
 TTY (207) 780-5646

DATE: September 21, 2000

TO: University of Southern Maine Parking/Transportation Committee

FROM: Mr. Samuel G. Andrews, Office of Finance and Administration / C

RE: USM/METRO Cooperative Efforts

At the September 19 meeting of the USM Parking/Transportation Review Committee, a Committee member noted the advertisements that have been appearing in the FREE PRESS about the "50¢ Fares, Any where, Anytime" and the METRO "FAST Service on Forest Avenue". (I have enclosed a copy of advertisements from the 9/5/00 and 9/18/00 issues.) Beginning last spring, USM began cooperating with METRO to create awareness and travel usage of the METRO via a free ride program. As indicated in the attached memo from Philip Chin, Director of Marketing for METRO, by displaying a USM photo ID card, any member of the USM Community (faculty, staff or student) was eligible to ride free on the METRO. Mr. Chin's memo to me outlined the ridership that resulted from the pilot program. We also cooperated with the Greater Portland Council of Governments (COG) and METRO in having an electronic touchscreen-Smart Kiosk installed in the Woodbury Campus Center in Portland with the kiosk providing bus, area transportation and tourism information.

In late summer the METRO began the "FAST Service on Forest Avenue" which provides METRO service every fifteen minutes along Forest Avenue from Pride's Corner in Westbrook to Downtown Portland. This "FAST Service" significantly improved the ability of folks living near or along bus route #2 (as well as those who might use the Park and Ride lot at Prides' Corner) to get to the Portland campus. The University was involved with the group that helped put together the funding proposal for this service. In addition, for the 2000/01 academic year, we are cooperating with METRO on the "50¢ Fares for USM, Anytime, Anywhere" program. In this program, anyone with a USM photo ID or USM ID card can ride the METRO for 50¢, or half the regular fare.

To promote these programs, the University has provided up to \$5,000 to METRO to be used to purchase display advertising in the FREE PRESS during the academic year. USM has also provided \$6,000 toward the cost of printed materials including informational brochures, posters, and other educational outreach materials about the reduced fare and Forest Avenue programs and about the benefits of using public transportation rather than ones own vehicle. Throughout the academic year, METRO will provide us with usage data. Hopefully the efforts from METRO will help make the USM community aware of METRO services that can facilitate its commute to Portland campus. As the academic year progresses, I will keep you informed as to the on-going results.

SGA/c

pc: Ms. Judith Ryan; Mr. Craig Hutchinson;  
 Ms. Helen Gorgas-Goulding

August 3, 2000

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO Ridership Program

The following is an update on the METRO-USM pilot program:

Background - USM/Free METRO Fare Pilot Program

Since March 2000, we have been working with Craig Hutchinson, Helen Gorgas, and Judy Ryan on creating awareness and trial usage of public transportation via a free METRO ride program. By displaying a USM photo ID card, any member of the USM community rides for free. The METRO has spent over \$1,000 in display advertising in the USM *Free Press* in promoting the free service. We have received very positive support from Craig and Helen via USM on-campus promotions. In June, our *FAST* (Forest Avenue Smart Travel) or 15-minute frequency service was introduced. METRO *FAST* has been a strong complement to the free USM fare program. The accessibility of the METRO bus on Forest Avenue near the campus and higher service frequency have proven to be highly attractive to USM riders.

The pilot program's ridership to date is:

USM METRO trips

<u>March</u>	<u>April</u>	<u>May</u>	<u>June</u>	<u>July</u>	<u>August</u>	<u>Cumulative</u>
1,234	2,024	2,227	1,893	1909		9,287

# FAST Service on Forest Avenue

## 50¢ Fare

*Anytime. Anywhere.*

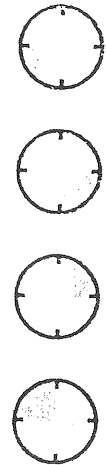
If you attend USM at the Portland Campus or at 68 High Street, we'll make your weekday commute between Forest Avenue and downtown Portland a little easier.

METRO #2 bus will provide service **every 15 minutes** along Forest Avenue, Monday thru Friday, for your school and work commute, shopping trips, short errands, medical appointments, and other intown and crosstown trips.

**With a USM Photo ID card and/or a current USM Registration card, the fare is only 50¢!**

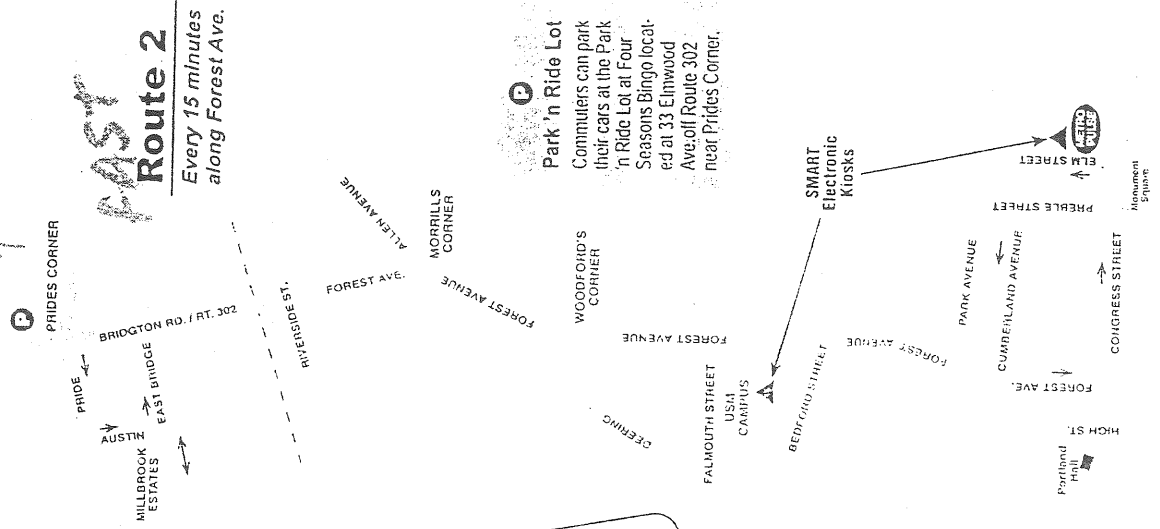
Ride in brand new buses. Use the bicycle racks on each bus to combine bike and bus commuting. Visit our exciting, electronic touchscreen SMART Kiosks at the METRO PULSE (Elm Street Garage) and at the Woodbury Campus Center (Bedford Street) for bus, area transportation, and tourism information.

## METRO Bus #2



Every 15 minutes daily along Forest Avenue!

**FAST**  
**Route 2**  
Every 15 minutes along Forest Ave.



# METRO

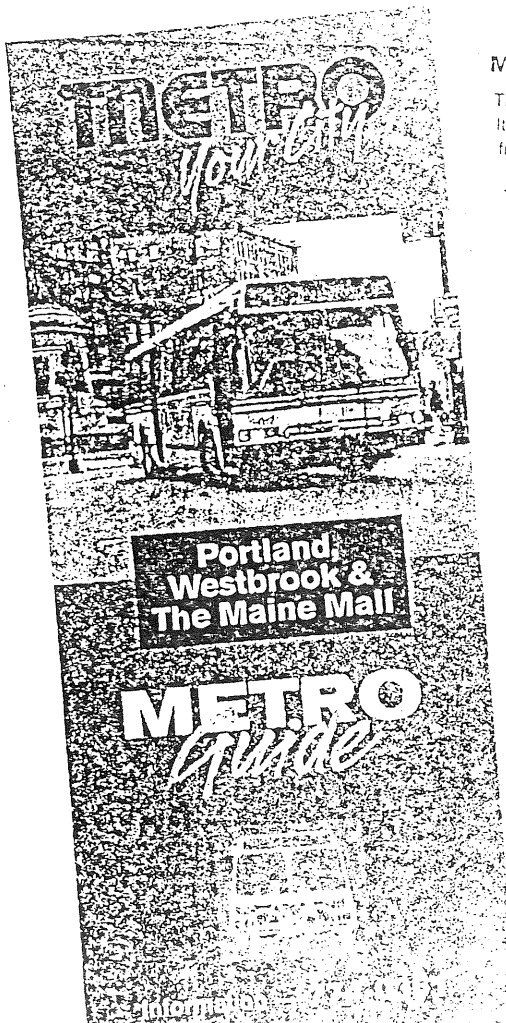
*Your City Buses*



## 50¢ Fares for USM. Anytime. Anywhere.



- Show a **USM Photo ID** or a current **USM ID registration card** to a METRO driver and ride for 50¢ (regular fare is \$1). No other USM ID will be accepted.
- Get a free USM Photo ID card at Payson Smith Hall in Room 4.
- Ride any route – anytime, anywhere.
- Use the METRO for your commute to school, work, intown Portland, shopping, medical appointments and for other daily trips.
- Use the METRO Smart KIOSK at the Woodbury Campus Center and at the METRO Pulse for bus time schedule and information.



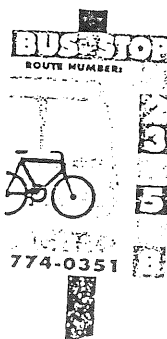
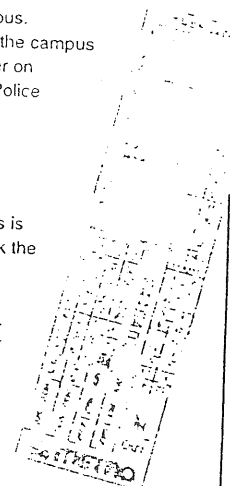
### METRO to USM – #2 or #4 Bus

The #2 bus (Forest Avenue) stops by Back Bay Bicycle on Forest Avenue. It is a short walk up Bedford Street to the campus. The #2 bus stops in front of Pier One for the return intown trip.

The #4 bus (Westbrook/Exit 8) also takes you to the campus. Traveling to the campus, the bus stops on Bedford Street on the campus side. Leaving the campus, the #4 bus stops at the bus shelter on Bedford Street opposite the Campus Center near the USM Police Department building.

### How do I change or transfer to another METRO bus?

You may need to change buses to reach your location. This is called a *transfer*. If you need to change buses, please ask the bus driver for a *transfer* when you first get on the bus and pay for your ride. When you change your bus, please give the transfer to the second bus driver. The *transfer* is *free*. You do not need to pay when you change buses. On your ride home, you will need to pay another fare. (See picture of transfer at the right.) You can change buses at various METRO bus stops along Congress Street which runs through downtown Portland. You can also change buses at the METRO PULSE located at the Elm Street Garage.



### Where do I wait for the bus?

Please wait for a METRO bus where you see a *bus stop sign* (see left). There are also glass structures that are METRO bus stops called *bus shelters*.

### Are there special bus fare plans?

- \$30 Monthly Pass: unlimited rides for month purchased
- \$9 TenRide Ticket: 10 rides with no time limit

For information — 774-0351



December 8, 2000

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program

For your posting the following is ridership for the program:

March	1,234
April	2,024
May	2,227
June	1,893
July	1,909
August	2,059
Cumulative Free Rides:	11,346

September	1,313
October	1,616
November	1,588
Cumulative 50-Cent Rides:	4,517

The baseline of ridership provides optimism that we can continue to build interest in public transit within the USM community. In the months ahead, we will focus on USM faculty and staff, and on the Gorham campus. Thank you for your continued support and I will provide future updates.

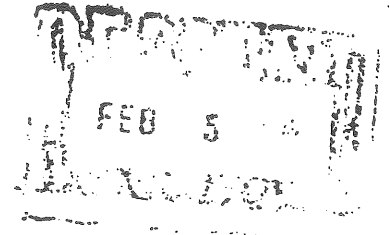


February 1, 2001

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program



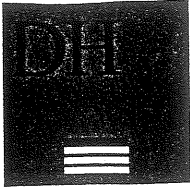
For your posting the following is ridership for the program:

Year 2000  
March 1,234  
April 2,024  
May 2,227  
June 1,893  
July 1,909  
August 2,059  
**Cumulative Free Rides: 11,346**

September 1,313  
October 1,616  
November 1,588  
December 1,504  
January '01 1,734  
**Cumulative 50-Cent Rides: 7,755**

**Total All Rides (March-January): 19,101**

In the upcoming weeks, we will be actively working with Rodney Mondor to introduce students on the Gorham campus to the METRO, as well as, Chris O'Connor at Portland Hall with Portland campus students..



DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

778 MAIN STREET  
SUITE 8  
SOUTH PORTLAND, MAINE 04106  
TEL. 207 775 1121  
FAX 207 879 0896

*Attachment 2*

- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

June 20, 2001

*Received  
6/25/01*

Ms. Marge Schmuckal  
Inspection Services/Planning  
City of Portland  
389 Congress Street  
Portland, Maine 04101

**Re: Parking Requirements Per City Ordinance  
USM Portland Campus**

Dear Marge:

Attached please find a revised breakdown of the University buildings based on type of use and square footage. These revisions should reflect the comments and questions you raised at our previous meeting in your office and subsequent telephone conversations. The attached sheets include a single total sheet titled "Portland Parking Requirements Per Square Feet 2001", with supplemental breakdown sheets of the areas you and I discussed. More specifically, those areas included the following:

- Recreation
- Merchandising
- Food Facility
- Lounge
- Conference
- Storage
- Office
- Shop
- Grounds

### ADJUSTMENTS

Per our conversation, several uses were adjusted including the removal of vehicle storage and the attic and basement spaces of the outbuildings. The new square footages also reflect a very thorough review of each of the University buildings and their uses. The previous submittal relied on existing data which was determined to require updating.

Many of the requirements for spaces are based on a previous study performed by DeLuca-Hoffman Associates, Inc. for the University titled "Parking Study for the University of Southern Maine at the Portland Campus" dated November 8, 1990. Based on a recent review by DeLuca-Hoffman Associates, Inc. and the University of both the existing ordinance requirements and those utilized in the previous study, we request your review and consideration of the following

items in determining what the number of available parking spaces should be for the campus after the construction of the proposed Bio Science building.

- The required number of parking spaces (58) for general storage (1/400 s.f.) appears high. We suggest a revised rate of (1.0/1,000 s.f.), which is the rate utilized for "stack area" within a library. It is expected that the general storage areas on campus generate very little parking demand and the rate of 1.0/1,000 would be more accurate.
- DeLuca-Hoffman Associates, Inc. performed a survey of random students entering/leaving the library and Luther Bonney Hall on May 3, 2001 from approximately 9:00 AM to 5:00 PM (summary sheet provided). This survey indicates that a significant number (approximately 66%) of students entering/leaving those buildings were going to visit another use on campus on that day. This supports my discussions with you regarding "shared" trips between the uses, as opposed to adding the demand for each use individually as if they were isolated uses.

#### PARKING STUDY vs. CITY ORDINANCE

Based on the information provided herein, DeLuca-Hoffman Associates, Inc. evaluated the parking demand for the campus based on both the study recently performed by DeLuca-Hoffman Associates, Inc. and the ordinance requirements. The determination by ordinance does not take credit for the two areas discussed above regarding a revised parking rate and "shared" trips which would decrease the required parking spaces determined by the ordinance. The following is a summary of the parking evaluation.

<b>USM Portland Campus, Parking Summary</b>					
<b>Parking Requirements Based On:</b>	<b>Parking Req. to Meet Existing Demand</b>	<b>Parking Req. for Proposed Bio Science**</b>	<b>Total Required Parking</b>	<b>Existing Supply***</b>	<b>Req. New Parking Spaces</b>
<b>DeLuca-Hoffman Associates, Inc. Parking Study</b>	1,517* (Includes 179 On Street)	77	1,594	1,777	None required (surplus of 183)
<b>City Ordinance</b>	1,625	90	1,715	1,777	None required (surplus of 62)

\* This is the demand for the busiest hour of the day on the busiest day of the week.

\*\* Study: 33 spaces for Bio Science plus loss of 44 spaces = 77 spaces

Ordinance:  $25,878 \text{ gross s.f.} \times 0.70 = 18,115 \text{ net s.f.}$  at 1/400 = 46 spaces plus loss of 44 existing spaces = 90 spaces

\*\*\* Includes on campus (1,200), outbuildings (38), Grand Street (12), Baxter Blvd. (48), Marginal Way (300) On Street (179)

Ms. Marge Schmuckal  
June 20, 2001  
Page 3

As the table shows, based on the DeLuca-Hoffman Associates, Inc. study, there is a surplus of 183 spaces with no additional parking spaces required. This does include the full use of the Marginal Way parking area. The University is currently evaluating how to improve the usership of the parking area, as well as evaluating its class schedules to distribute parking demand more evenly throughout the week. Starting in the Fall, the University will be providing free Marginal Way parking stickers to students, faculty and staff, which will supplement the free shuttle service to/from the campus. These stickers will be applicable Monday through Thursday on the Marginal Way lot and will allow the bearer to park on campus Fridays and weekends when parking demand is less.

Based on the ordinance requirements, there is a surplus of 62 spaces with no additional parking spaces required to accommodate the campus demands, including the proposed Bio Science building. This does not consider the suggested reductions discussed previously regarding change in parking rates or "shared" trips, which would further increase the estimated available surplus.

### **SUPPLEMENTAL MODES OF TRANSPORTATION**

In addition to the University evaluating its class schedules and usership of Marginal Way, they also currently utilize both their own shuttle service as well as the City's Metro service. The University's shuttle program includes both a service between Portland Hall (student residence) and the Portland Campus, as well as a service between the Gorham and Portland Campuses.

Contributing to the reduction of parking demand for the University is the usership of the City's Metro service which started with free rides in March of 2000 and changed to 50-cent rides in September of 2000. Based on information provided by the University (attached), 25,239 students rode the Metro from March 2000 through May 2001. This is in addition to the shuttle service described previously. The figures show that even during the summer months, the University students, faculty and staff continue to utilize the service. Based on my conversations with Philip Chin, Director of Marketing for METRO, they expect a baseline ridership from the University of approximately 1,500 per month minimum and expect that will significantly increase as word travels.

It is not stated in the ordinance whether or not the availability of other forms of transportation, such as the University's shuttle service and the metro, are considered in determining parking demand. We suggest that it has an impact and should reduce the ordinance requirements based on square footage as reported previously.

### **PREVIOUS REPORTS**

The previous study performed by DeLuca-Hoffman Associates, Inc. in 1990 estimated an existing demand at that time for both on- and off-street parking of 1,452 spaces. It also estimated a projected demand of 147 spaces for a proposed library, offices and law library addition for a total demand of 1,599 spaces. This corresponds closely to the 1,594 spaces identified in this study which was performed independently of the previous 1990 study.

Ms. Marge Schmuckal  
June 20, 2001  
Page 4

There have been some additional buildings constructed/obtained since 1990 which were not considered in the 1990 study. This further validates the idea of "shared" trips, since the 1990 projected parking space demand is still accurate despite additional square footage.

### PROPOSED PARKING

The University is currently evaluating areas for potential additional on-campus parking. The University would like to construct these spaces this summer so they may be utilized by vehicles displaced due to the construction of the Bio Science building. The exact number and location of these spaces will be refined once you have identified the number of parking spaces the University is required to provide, if any, in order to meet both the existing parking demand as well as that required for the proposed Bio Science building.

### CONCLUSION

Based on the information provided herein, DeLuca-Hoffman Associates, Inc. and the University suggest the results of the parking study recently performed and submitted to the City yield a more accurate representation of the University's parking demand than that based on the ordinance. In addition, we conclude that both the study and the ordinance indicate no additional parking spaces are required to meet either the existing demand or the increased demand generated by the Bio Science building. If you have any questions or would like to discuss this further, please feel free to contact me or Dave Early at the University. The University is more than willing to show you any of the areas on campus with which you may have questions. We look forward to your response so we may proceed with this project.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.



Randall E. Dunton, P.E.  
Project Engineer

RED/sq/JN2074/Schmuckal6-6

Enclosure

c: Sam Andrews, University of Southern Maine  
Dave Early, University of Southern Maine



Portland Parking Requirement  
per Square Footage  
2001

6/6/01  
10:09 AM

Required  
Spaces

<u>Type of Facility</u>	<u>Area (s.f.) 2001</u>	<u>Parking Factor</u>	
General purpose Classroom	25,059	1.0/100 s.f.	251
Lecture room	11,945	1.0/100 s.f.	119
Seminar	3,541	1.0/100 s.f.	35
Labs	2 employees	1.0/room	2
Class Labs	26,095	1.0/100 s.f.	261
Class Labs svc	2,554	1.0/400 s.f.	6
Day Care	3,203	20 employees	20
Indivial Study Lab	2,087	1.0/100 s.f.	21
Office	100,350	1.0/400 s.f.	251
Office services	6,579	1.0/400 s.f.	16
Conference Room	1,523	1.0/400 s.f.	4
Library Reading Study	33,232	1.0/400 s.f.	83
Library Open Stack Reading Rm.	5,249	1.0/400 s.f.	13
Stack area	74,511	1.0/1000 s.f.	75
Library Process Room	11,419	1.0/400 s.f.	29
Physical Education	19,971	1.0/160 s.f.	125
ITV High tech class	2,472	1.0/400 s.f.	6
Lounge	6,341	1.0/160 s.f.	40
Amphitheater	40 (seats)	1.0/5 seats	8
Food Facilities	6,717	1.0/100 s.f.	67
Merchandising	5,364	1.0/150 s.f.	36
Recreation	15,930	1.0/400 s.f.	40
Student Health	1,480	1.0/500 s.f.	3
Computer Room	2,015	1.0/400 s.f.	5
Computer Center	7,249	1.0/400 s.f.	18
Shop	10,036	1.0/400 s.f.	25
General Storage	23,026	1.0/400 s.f.	58
Grounds Department	9,006	1.0/1000 s.f.	9
<b>Totals</b>	<b>416,954</b>	<b>(Net SF)</b>	<b>1,625</b>





DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

778 MAIN STREET  
SUITE 8  
SOUTH PORTLAND, MAINE 04106  
TEL. 207 775 1121  
FAX 207 879 0896

- Attachment 2*
- ROADWAY DESIGN
  - ENVIRONMENTAL ENGINEERING
  - TRAFFIC STUDIES AND MANAGEMENT
  - PERMITTING
  - AIRPORT ENGINEERING
  - SITE PLANNING
  - CONSTRUCTION ADMINISTRATION

June 20, 2001

*Received  
6/25/01*

Ms. Marge Schmuckal  
Inspection Services/Planning  
City of Portland  
389 Congress Street  
Portland, Maine 04101

**Re: Parking Requirements Per City Ordinance  
USM Portland Campus**

Dear Marge:

Attached please find a revised breakdown of the University buildings based on type of use and square footage. These revisions should reflect the comments and questions you raised at our previous meeting in your office and subsequent telephone conversations. The attached sheets include a single total sheet titled "Portland Parking Requirements Per Square Feet 2001", with supplemental breakdown sheets of the areas you and I discussed. More specifically, those areas included the following:

- Recreation
- Merchandising
- Food Facility
- Lounge
- Conference
- Storage
- Office
- Shop
- Grounds

### ADJUSTMENTS

Per our conversation, several uses were adjusted including the removal of vehicle storage and the attic and basement spaces of the outbuildings. The new square footages also reflect a very thorough review of each of the University buildings and their uses. The previous submittal relied on existing data which was determined to require updating.

Many of the requirements for spaces are based on a previous study performed by DeLuca-Hoffman Associates, Inc. for the University titled "Parking Study for the University of Southern Maine at the Portland Campus" dated November 8, 1990. Based on a recent review by DeLuca-Hoffman Associates, Inc. and the University of both the existing ordinance requirements and those utilized in the previous study, we request your review and consideration of the following

items in determining what the number of available parking spaces should be for the campus after the construction of the proposed Bio Science building.

- The required number of parking spaces (58) for general storage (1/400 s.f.) appears high. We suggest a revised rate of (1.0/1,000 s.f.), which is the rate utilized for “stack area” within a library. It is expected that the general storage areas on campus generate very little parking demand and the rate of 1.0/1,000 would be more accurate.
- DeLuca-Hoffman Associates, Inc. performed a survey of random students entering/leaving the library and Luther Bonney Hall on May 3, 2001 from approximately 9:00 AM to 5:00 PM (summary sheet provided). This survey indicates that a significant number (approximately 66%) of students entering/leaving those buildings were going to visit another use on campus on that day. This supports my discussions with you regarding “shared” trips between the uses, as opposed to adding the demand for each use individually as if they were isolated uses.

**PARKING STUDY vs. CITY ORDINANCE**

Based on the information provided herein, DeLuca-Hoffman Associates, Inc. evaluated the parking demand for the campus based on both the study recently performed by DeLuca-Hoffman Associates, Inc. and the ordinance requirements. The determination by ordinance does not take credit for the two areas discussed above regarding a revised parking rate and “shared” trips which would decrease the required parking spaces determined by the ordinance. The following is a summary of the parking evaluation.

<b>USM Portland Campus, Parking Summary</b>					
<b>Parking Requirements Based On:</b>	<b>Parking Req. to Meet Existing Demand</b>	<b>Parking Req. for Proposed Bio Science**</b>	<b>Total Required Parking</b>	<b>Existing Supply***</b>	<b>Req. New Parking Spaces</b>
<b>DeLuca-Hoffman Associates, Inc. Parking Study</b>	1,517* (Includes 179 On Street)	77	1,594	1,777	None required (surplus of 183)
<b>City Ordinance</b>	1,625	90	1,715	1,777	None required (surplus of 62)

\* This is the demand for the busiest hour of the day on the busiest day of the week.

\*\* Study: 33 spaces for Bio Science plus loss of 44 spaces = 77 spaces

Ordinance: 25,878 gross s.f. x 0.70 = 18,115 net s.f. at 1/400 = 46 spaces plus loss of 44 existing spaces = 90 spaces

\*\*\* Includes on campus (1,200), outbuildings (38), Grand Street (12), Baxter Blvd. (48), Marginal Way (300) On Street (179)

As the table shows, based on the DeLuca-Hoffman Associates, Inc. study, there is a surplus of 183 spaces with no additional parking spaces required. This does include the full use of the Marginal Way parking area. The University is currently evaluating how to improve the usership of the parking area, as well as evaluating its class schedules to distribute parking demand more evenly throughout the week. Starting in the Fall, the University will be providing free Marginal Way parking stickers to students, faculty and staff, which will supplement the free shuttle service to/from the campus. These stickers will be applicable Monday through Thursday on the Marginal Way lot and will allow the bearer to park on campus Fridays and weekends when parking demand is less.

Based on the ordinance requirements, there is a surplus of 62 spaces with no additional parking spaces required to accommodate the campus demands, including the proposed Bio Science building. This does not consider the suggested reductions discussed previously regarding change in parking rates or "shared" trips, which would further increase the estimated available surplus.

#### **SUPPLEMENTAL MODES OF TRANSPORTATION**

In addition to the University evaluating its class schedules and usership of Marginal Way, they also currently utilize both their own shuttle service as well as the City's Metro service. The University's shuttle program includes both a service between Portland Hall (student residence) and the Portland Campus, as well as a service between the Gorham and Portland Campuses.

Contributing to the reduction of parking demand for the University is the usership of the City's Metro service which started with free rides in March of 2000 and changed to 50-cent rides in September of 2000. Based on information provided by the University (attached), 25,239 students rode the Metro from March 2000 through May 2001. This is in addition to the shuttle service described previously. The figures show that even during the summer months, the University students, faculty and staff continue to utilize the service. Based on my conversations with Philip Chin, Director of Marketing for METRO, they expect a baseline ridership from the University of approximately 1,500 per month minimum and expect that will significantly increase as word travels.

It is not stated in the ordinance whether or not the availability of other forms of transportation, such as the University's shuttle service and the metro, are considered in determining parking demand. We suggest that it has an impact and should reduce the ordinance requirements based on square footage as reported previously.

#### **PREVIOUS REPORTS**

The previous study performed by DeLuca-Hoffman Associates, Inc. in 1990 estimated an existing demand at that time for both on- and off-street parking of 1,452 spaces. It also estimated a projected demand of 147 spaces for a proposed library, offices and law library addition for a total demand of 1,599 spaces. This corresponds closely to the 1,594 spaces identified in this study which was performed independently of the previous 1990 study.

Ms. Marge Schmuckal  
June 20, 2001  
Page 4

There have been some additional buildings constructed/obtained since 1990 which were not considered in the 1990 study. This further validates the idea of "shared" trips, since the 1990 projected parking space demand is still accurate despite additional square footage.

### PROPOSED PARKING

The University is currently evaluating areas for potential additional on-campus parking. The University would like to construct these spaces this summer so they may be utilized by vehicles displaced due to the construction of the Bio Science building. The exact number and location of these spaces will be refined once you have identified the number of parking spaces the University is required to provide, if any, in order to meet both the existing parking demand as well as that required for the proposed Bio Science building.

### CONCLUSION

Based on the information provided herein, DeLuca-Hoffman Associates, Inc. and the University suggest the results of the parking study recently performed and submitted to the City yield a more accurate representation of the University's parking demand than that based on the ordinance. In addition, we conclude that both the study and the ordinance indicate no additional parking spaces are required to meet either the existing demand or the increased demand generated by the Bio Science building. If you have any questions or would like to discuss this further, please feel free to contact me or Dave Early at the University. The University is more than willing to show you any of the areas on campus with which you may have questions. We look forward to your response so we may proceed with this project.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.

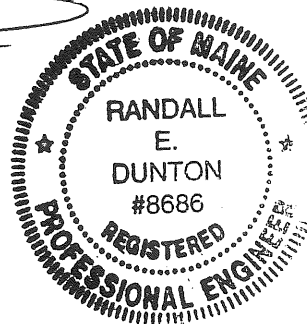


Randall E. Dunton, P.E.  
Project Engineer

RED/sq/JN2074/Schmuckal6-6

Enclosure

c: Sam Andrews, University of Southern Maine  
Dave Early, University of Southern Maine



Portland Parking Requirement  
per Square Footage  
2001

6/6/01  
10:09 AM

Required  
Spaces

<u>Type of Facility</u>	<u>Area (s.f.) 2001</u>	<u>Parking Factor</u>	
General purpose Classroom	25,059	1.0/100 s.f.	251
Lecture room	11,945	1.0/100 s.f.	119
Seminar	3,541	1.0/100 s.f.	35
Labs	2 employees	1.0/room	2
Class Labs	26,095	1.0/100 s.f.	261
Class Labs svc	2,554	1.0/400 s.f.	6
Day Care	3,203	20 employees	20
Indivial Study Lab	2,087	1.0/100 s.f.	21
Office	100,350	1.0/400 s.f.	251
Office services	6,579	1.0/400 s.f.	16
Conference Room	1,523	1.0/400 s.f.	4
Library Reading Study	33,232	1.0/400 s.f.	83
Library Open Stack Reading Rm.	5,249	1.0/400 s.f.	13
Stack area	74,511	1.0/1000 s.f.	75
Library Process Room	11,419	1.0/400 s.f.	29
Physical Education	19,971	1.0/160 s.f.	125
ITV High tech class	2,472	1.0/400 s.f.	6
Lounge	6,341	1.0/160 s.f.	40
Amphitheater	40 (seats)	1.0/5 seats	8
Food Facilities	6,717	1.0/100 s.f.	67
Merchandising	5,364	1.0/150 s.f.	36
Recreation	15,930	1.0/400 s.f.	40
Student Health	1,480	1.0/500 s.f.	3
Computer Room	2,015	1.0/400 s.f.	5
Computer Center	7,249	1.0/400 s.f.	18
Shop	10,036	1.0/400 s.f.	25
General Storage	23,026	1.0/400 s.f.	58
Grounds Department	9,006	1.0/1000 s.f.	9
<b>Totals</b>	<b>416,954</b>	<b>(Net SF)</b>	<b>1,625</b>

Space identification

NAME	Floor	RM.-N SQ. FT.	TYPE
SULLIVAN GYM	G	115	recreation
		Total ✓ 15,930	recreation
LUTHER-BONNEY HALL	G	136	MERCHANDISE
WOODBURY CAMPUS CTR	G	110	MERCHANDISING
		Total ✓ 5,364	MERCHANDISING
LUTHER-BONNEY HALL	G	127	232 food facility
WOODBURY CAMPUS CTR	G	163	5,517 FOOD FACILITY
WOODBURY CAMPUS CTR	G		804
WOODBURY CAMPUS CTR	G		164
		Total ✓ 6,717	FOOD FACILITY
GLICKMAN LIBRARY	4	414	182 LOUNGE
LAW BUILDING	B	001	888 LOUNGE
LAW BUILDING	B	018	841 LOUNGE
LAW BUILDING	2	219	360 LOUNGE
LAW BUILDING	4	430	212 LOUNGE
PAYSON-SMITH HALL	B	032	262 LOUNGE
SULLIVAN GYM	G	137	636 LOUNGE
WOODBURY CAMPUS CTR	G	132	160 LOUNGE
WOODBURY CAMPUS CTR	2	200	680 LOUNGE
WOODBURY CAMPUS CTR	2	201	767 LOUNGE
WOODBURY CAMPUS CTR	G	131	291 LOUNGE
WOODBURY CAMPUS CTR	G	134	287 LOUNGE
WOODBURY CAMPUS CTR	G	130	775 LOUNGE
		Total ✓ 6,341	LOUNGE
WOODBURY CAMPUS CTR	G	143C	507 CONFERENCE
WOODBURY CAMPUS CTR	G	143B	674 CONFERENCE
WOODBURY CAMPUS CTR	G	143A	342 CONFERENCE
		Total ✓ 1,523	CONFERENCE
46 Bedford	1	101	6,511 storage
CENTRAL HEAT PLANT-P	2	201	420 storage
GLICKMAN LIBRARY	B	012S	9,736 storage
LAW BUILDING	3	303	161 storage
LUTHER-BONNEY HALL	B	002	848 storage
LUTHER-BONNEY HALL	4	409	204 storage

Space identification

LUTHER-BONNEY HALL	G	135	1,394	storage
MASTERTON HALL	2	207A	75	storage
MASTERTON HALL	G	020	127	storage
PAYSON-SMITH HALL	B	018	436	storage
PAYSON-SMITH HALL	B	030	196	storage
PAYSON-SMITH HALL	B	033	47	storage
PAYSON-SMITH HALL	B	031	359	storage
SCIENCE BLDG	3	361A	172	storage
SCIENCE BLDG	3	361	517	storage
SCIENCE BLDG	G	115	152	storage
SCIENCE BLDG	B	012	502	storage
SCIENCE BLDG	B	066	264	storage
SCIENCE BLDG	B	067	52	storage
SULLIVAN GYM	B	062A	72	storage
SULLIVAN GYM	B	062B	180	storage
SULLIVAN GYM	B	063	49	storage
SULLIVAN GYM	G	117	26	storage
SULLIVAN GYM	B	010	81	storage
SULLIVAN GYM	G	162	180	storage
SULLIVAN GYM	2	233	265	storage
LUTHER-BONNEY HALL		Total	23,026	storage
Bedford-118	G	106	180	office
Bedford-120	G	107	224	office
Exter St 47	G	106	187	office
Exter St 47	2	206	217	office
LUTHER-BONNEY HALL	3	311	694	office
ALUMNI HOUSE	G	103	120	OFFICE
ALUMNI HOUSE	G	108	230	OFFICE
ALUMNI HOUSE	G	107	184	OFFICE
ALUMNI HOUSE	G	102	189	OFFICE
ALUMNI HOUSE	G	101	119	OFFICE
ALUMNI HOUSE	G	104	124	OFFICE
BEDFORD ST- 25-FACMGT	G	102	121	OFFICE
BEDFORD ST- 25-FACMGT	G	100	830	OFFICE
BEDFORD ST- 25-FACMGT	G	101	98	OFFICE
BEDFORD ST- 25-FACMGT	G	101A	228	OFFICE
BEDFORD ST- 46-STEEGO	G	101	420	OFFICE
Bedford-92	G	102	108	office
Bedford-92	G	104	240	office
Bedford-92	2	201	251	office
Bedford-92	G	101	241	office

Space identification

BEDFORD ST-92	G	106	145	OFFICE
BEDFORD ST-92	G	109	96	OFFICE
BEDFORD ST-92	G	107	208	OFFICE
BEDFORD ST-92	2	207	186	OFFICE
BEDFORD ST-92	2	203	100	OFFICE
BEDFORD ST-92	2	204	146	OFFICE
Bedford-94	G	101	234	office
Bedford-94	2	201	209	office
BEDFORD ST-94	2	202	124	OFFICE
BEDFORD ST-94	2	204	100	OFFICE
BEDFORD ST-94	G	104	159	OFFICE
BEDFORD ST-94	G	102	134	OFFICE
BEDFORD ST-102	2	201	220	OFFICE
BEDFORD ST-102	G	101	230	OFFICE
BEDFORD ST-118	3	302	168	OFFICE
BEDFORD ST-118	G	102	252	OFFICE
BEDFORD ST-118	G	103	165	OFFICE
BEDFORD ST-118	G	105	222	OFFICE
BEDFORD ST-118	2	204	128	OFFICE
BEDFORD ST-118	2	203	240	OFFICE
BEDFORD ST-118	2	202	238	OFFICE
BEDFORD ST-118	2	201	149	OFFICE
BEDFORD ST-120	2	203	202	OFFICE
BEDFORD ST-120	G	105	202	OFFICE
BEDFORD ST-120	G	106	140	OFFICE
BEDFORD ST-120	G	110	143	OFFICE
BEDFORD ST-120	G	104	147	OFFICE
BEDFORD ST-120	2	204	141	OFFICE
BEDFORD ST-120	2	205	161	OFFICE
BEDFORD ST-120	2	202	133	OFFICE
BEDFORD ST-120	G	103	234	OFFICE
BEDFORD ST-120	G	101	147	OFFICE
BEDFORD ST-120	2	206	140	OFFICE
BEDFORD ST-120	2	210	143	OFFICE
BEDFORD ST-120	2	201	149	OFFICE
BEDFORD ST-126	G	109	196	OFFICE
BEDFORD ST-126	G	102	76	OFFICE
BEDFORD ST-126	G	110	257	OFFICE
BEDFORD ST-126	2	209	92	OFFICE
BEDFORD ST-126	2	206	68	OFFICE
BEDFORD ST-126	2	205	169	OFFICE
BEDFORD ST-126	2	204	154	OFFICE



Space identification

BEDFORD ST-126	2	202	55	OFFICE
CHAMBERLAIN AVE- 1	6	101	165	OFFICE
CHAMBERLAIN AVE- 1	6	102	236	OFFICE
CHAMBERLAIN AVE- 1	6	103	105	OFFICE
CHAMBERLAIN AVE- 1	6	104	145	OFFICE
CHAMBERLAIN AVE- 1	6	105	328	OFFICE
CHAMBERLAIN AVE- 7	2	204	140	OFFICE
CHAMBERLAIN AVE- 7	6	102B	103	OFFICE
CHAMBERLAIN AVE- 7	6	102A	147	OFFICE
CHAMBERLAIN AVE- 7	6	103	312	OFFICE
CHAMBERLAIN AVE- 7	2	201	90	OFFICE
CHAMBERLAIN AVE- 7	2	202	165	OFFICE
CHAMBERLAIN AVE- 7	2	204A	140	OFFICE
CHAMBERLAIN AVE-11	2	202	165	OFFICE
CHAMBERLAIN AVE-11	6	101	280	OFFICE
CHAMBERLAIN AVE-11	6	102	135	OFFICE
CHAMBERLAIN AVE-11	6	103	217	OFFICE
CHAMBERLAIN AVE-11	2	201	198	OFFICE
CHAMBERLAIN AVE-11	2	203	139	OFFICE
CHAMBERLAIN AVE-15	2	204	168	OFFICE
CHAMBERLAIN AVE-15	2	201	89	OFFICE
CHAMBERLAIN AVE-15	2	203	148	OFFICE
CHAMBERLAIN AVE-15	2	202	174	OFFICE
CHAMBERLAIN AVE-15	6	101	258	OFFICE
CHAMBERLAIN AVE-15	6	105	157	OFFICE
CHAMBERLAIN AVE-15	6	104	158	OFFICE
BEDFORD ST- 98	1	100	293	OFFICE
BEDFORD ST- 98	1	102	101	OFFICE
BEDFORD ST- 98	1	103	146	OFFICE
BEDFORD ST- 98	1	104	90	OFFICE
BEDFORD ST- 98	1	105	83	OFFICE
BEDFORD ST- 98	1	201	124	OFFICE
BEDFORD ST- 98	1	202	119	OFFICE
BEDFORD ST- 98	1	203	129	OFFICE
CHAMBERLAIN AVE-19	1	102	260	OFFICE
CHAMBERLAIN AVE-19	1	105	134	OFFICE
DEERING AVE-209	2	202	150	OFFICE
DEERING AVE-209	2	200	117	OFFICE
DEERING AVE-209	2	205	195	OFFICE
DEERING AVE-209	6	102	157	OFFICE
DEERING AVE-209	6	106	222	OFFICE
DEERING AVE-209	6	108	126	OFFICE

Space identification

DEERING AVE-228		2	204	185	OFFICE
DEERING AVE-228		2	201	176	OFFICE
DEERING AVE-228		2	203	163	OFFICE
DEERING AVE-228		2	205	126	OFFICE
DEERING AVE-228		6	106	212	OFFICE
DEERING AVE-228		2	202	96	OFFICE
DEERING AVE-228		6	103	192	OFFICE
DEERING AVE-228		6	102	137	OFFICE
DEERING AVE-228		6	101	162	OFFICE
EXETER ST- 39		2	203	137	OFFICE
EXETER ST- 39		2	202	94	OFFICE
EXETER ST- 39		2	204	154	OFFICE
EXETER ST- 39		6	106	136	OFFICE
EXETER ST- 39		6	102	187	OFFICE
EXETER ST- 45		6	103	184	OFFICE
EXETER ST- 45		6	101	228	OFFICE
EXETER ST- 45		3	301	700	OFFICE
EXETER ST- 45		2	204	99	OFFICE
EXETER ST- 45		2	203	145	OFFICE
EXETER ST- 45		2	201	183	OFFICE
EXETER ST- 45		B	001	350	OFFICE
EXETER ST- 45		B	002	320	OFFICE
EXETER ST- 45		6	102	59	OFFICE
EXETER ST- 45		2	202	123	OFFICE
EXETER ST- 47		6	102	180	OFFICE
EXETER ST- 47		6	104	130	OFFICE
EXETER ST- 47		6	103	126	OFFICE
EXETER ST- 47		6	101	128	OFFICE
EXETER ST- 47		2	201	72	OFFICE
EXETER ST- 47		6	107	100	OFFICE
EXETER ST- 47		2	204	130	OFFICE
EXETER ST- 47		2	202	180	OFFICE
EXETER ST- 47		2	203	126	OFFICE
EXETER ST- 49-51		6	109	180	OFFICE
EXETER ST- 49-51		6	108	145	OFFICE
EXETER ST- 49-51		6	107	113	OFFICE
EXETER ST- 49-51		6	105	116	OFFICE
EXETER ST- 49-51		6	102	142	OFFICE
EXETER ST- 49-51		2	219	180	OFFICE
EXETER ST- 49-51		2	218	145	OFFICE
EXETER ST- 49-51		2	217	113	OFFICE
EXETER ST- 49-51		2	214	115	OFFICE

Space identification

EXETER ST- 49-51		2	211	142	OFFICE
EXETER ST- 55		2	204	154	OFFICE
EXETER ST- 55		6	101	189	OFFICE
EXETER ST- 55		3	306	181	OFFICE
EXETER ST- 55		6	103	100	OFFICE
EXETER ST- 55		6	106	101	OFFICE
EXETER ST- 55		6	108	150	OFFICE
EXETER ST- 55		6	110	151	OFFICE
EXETER ST- 55		2	201	192	OFFICE
EXETER ST- 55		6	107	172	OFFICE
EXETER ST- 55		2	203	102	OFFICE
EXETER ST- 55		2	206	152	OFFICE
EXETER ST- 55		2	207	181	OFFICE
EXETER ST- 55		2	208	152	OFFICE
EXETER ST- 59-61		2	208	135	OFFICE
EXETER ST- 59-61		2	209	130	OFFICE
EXETER ST- 59-61		2	206	176	OFFICE
EXETER ST- 59-61		2	204	206	OFFICE
EXETER ST- 59-61		6	104	205	OFFICE
EXETER ST- 59-61		6	109	130	OFFICE
EXETER ST- 59-61		6	108	135	OFFICE
EXETER ST- 63-65		2	203	135	OFFICE
EXETER ST- 63-65		2	208	105	OFFICE
EXETER ST- 63-65		3	301	161	OFFICE
EXETER ST- 63-65		6	101	84	OFFICE
EXETER ST- 63-65		2	201	156	OFFICE
EXETER ST- 63-65		6	134	180	OFFICE
GLICKMAN LIBRARY		6	136	226	OFFICE
GLICKMAN LIBRARY		6	127	100	OFFICE
GLICKMAN LIBRARY		3	313	120	OFFICE
GLICKMAN LIBRARY		4	428	120	OFFICE
GLICKMAN LIBRARY		4	427	204	OFFICE
GLICKMAN LIBRARY		4	426	266	OFFICE
GLICKMAN LIBRARY		2	217	308	OFFICE
GLICKMAN LIBRARY		2	214	630	OFFICE
GLICKMAN LIBRARY		2	213	150	OFFICE
GLICKMAN LIBRARY		2	212	180	OFFICE
GLICKMAN LIBRARY		3	312	390	OFFICE
GRANITE ST- 11		2	201	165	OFFICE
GRANITE ST- 11		2	203	96	OFFICE
GRANITE ST- 11		6	101	183	OFFICE
GRANITE ST- 11		6	103	96	OFFICE

Space identification

LAW BUILDING	7	711	286	OFFICE
LAW BUILDING	7	726	214	OFFICE
LAW BUILDING	7	724	400	OFFICE
LAW BUILDING	7	723	325	OFFICE
LAW BUILDING	7	722	210	OFFICE
LAW BUILDING	7	721	225	OFFICE
LAW BUILDING	7	728	200	OFFICE
LAW BUILDING	7	714	144	OFFICE
LAW BUILDING	4	417	205	OFFICE
LAW BUILDING	6	607A	140	OFFICE
LAW BUILDING	7	710	270	OFFICE
LAW BUILDING	8	011	762	OFFICE
LAW BUILDING	7	732	130	OFFICE
LAW BUILDING	7	731	200	OFFICE
LAW BUILDING	6	615	122	OFFICE
LAW BUILDING	6	622	180	OFFICE
LAW BUILDING	6	621	126	OFFICE
LAW BUILDING	6	619	256	OFFICE
LAW BUILDING	6	618	115	OFFICE
LAW BUILDING	6	616	105	OFFICE
LAW BUILDING	6	629B	215	OFFICE
LAW BUILDING	6	614	138	OFFICE
LAW BUILDING	6	613	120	OFFICE
LAW BUILDING	6	608	92	OFFICE
LAW BUILDING	6	608A	36	OFFICE
LAW BUILDING	6	607S	31	OFFICE
LAW BUILDING	7	709	240	OFFICE
LAW BUILDING	7	708	104	OFFICE
LAW BUILDING	7	707	270	OFFICE
LAW BUILDING	7	705	575	OFFICE
LAW BUILDING	6	626	248	OFFICE
LAW BUILDING	6	111	135	OFFICE
LAW BUILDING	6	122	131	OFFICE
LAW BUILDING	6	121	145	OFFICE
LAW BUILDING	6	120	131	OFFICE
LAW BUILDING	6	123	131	OFFICE
LAW BUILDING	6	119	131	OFFICE
LAW BUILDING	6	124	145	OFFICE
LAW BUILDING	6	115	180	OFFICE
LAW BUILDING	6	114	150	OFFICE
LAW BUILDING	6	113	150	OFFICE
LAW BUILDING	6	126	218	OFFICE

Space identification

LAW BUILDING	G	125	145	OFFICE	
LAW BUILDING	G	112	150	OFFICE	
LAW BUILDING	B	003	170	OFFICE	
LAW BUILDING	G	103	162	OFFICE	
LAW BUILDING	G	110	150	OFFICE	
LAW BUILDING	G	109	150	OFFICE	
LAW BUILDING	G	108	218	OFFICE	
LAW BUILDING	G	105	283	OFFICE	
LAW BUILDING	G	104	162	OFFICE	
LAW BUILDING	B	011H	100	OFFICE	
LAW BUILDING	G	102	210	OFFICE	
LAW BUILDING	G	101	156	OFFICE	
LAW BUILDING	G	105S	31	OFFICE	
LAW BUILDING	2	225	171	OFFICE	
LAW BUILDING	4	407	131	OFFICE	
LAW BUILDING	4	406	109	OFFICE	
LAW BUILDING	4	403	100	OFFICE	
LAW BUILDING	4	402	153	OFFICE	
LAW BUILDING	4	401	186	OFFICE	
LAW BUILDING	2	200	102	OFFICE	
LAW BUILDING	6	607	126	OFFICE	
LAW BUILDING	2	215S	25	OFFICE	
LAW BUILDING	2	222	150	OFFICE	
LAW BUILDING	2	217	135	OFFICE	
LAW BUILDING	2	216	135	OFFICE	
LAW BUILDING	2	215	221	OFFICE	
LAW BUILDING	2	235	144	OFFICE	
LAW BUILDING	2	214	193	OFFICE	
LAW BUILDING	2	214S	21	OFFICE	
LAW BUILDING	2	213	135	OFFICE	
LAW BUILDING	2	212	200	OFFICE	
LAW BUILDING	5	521	291	OFFICE	
LAW BUILDING	6	602	140	OFFICE	
LAW BUILDING	6	601	232	OFFICE	
LAW BUILDING	5	523A	208	OFFICE	
LAW BUILDING	5	522	500	OFFICE	
LAW BUILDING	5	520	355	OFFICE	
LAW BUILDING	5	515	141	OFFICE	
LAW BUILDING	5	514	142	OFFICE	
LAW BUILDING	5	513	121	OFFICE	
LAW BUILDING	5	524	245	OFFICE	
LAW BUILDING	2	232	160	OFFICE	

Space identification

LAW BUILDING		6	605	159	OFFICE
LAW BUILDING		6	604	105	OFFICE
LAW BUILDING		6	603	105	OFFICE
LAW BUILDING		6	602A	160	OFFICE
LAW BUILDING		6	629	215	OFFICE
LAW BUILDING		6	628	430	OFFICE
LAW BUILDING		4	426	205	OFFICE
LAW BUILDING		4	424	131	OFFICE
LAW BUILDING		4	423	220	OFFICE
LAW BUILDING		4	415	196	OFFICE
LAW BUILDING		5	512	155	OFFICE
LAW BUILDING		4	409	316	OFFICE
LAW BUILDING		4	414	158	OFFICE
LAW BUILDING		5	511A	152	OFFICE
LAW BUILDING		5	505	291	OFFICE
LAW BUILDING		5	504	156	OFFICE
LAW BUILDING		5	503	145	OFFICE
LAW BUILDING		5	502	193	OFFICE
LAW BUILDING		5	501	167	OFFICE
LUTHER-BONNEY HALL		2	231	743	OFFICE
LUTHER-BONNEY HALL		2	231A	774	OFFICE
LUTHER-BONNEY HALL		2	237	230	OFFICE
LUTHER-BONNEY HALL		2	222	122	OFFICE
LUTHER-BONNEY HALL		3	303	733	OFFICE
LUTHER-BONNEY HALL		2	212	103	OFFICE
LUTHER-BONNEY HALL		2	213	103	OFFICE
LUTHER-BONNEY HALL		2	214	103	OFFICE
LUTHER-BONNEY HALL		2	215	103	OFFICE
LUTHER-BONNEY HALL		2	216	103	OFFICE
LUTHER-BONNEY HALL		2	217	103	OFFICE
LUTHER-BONNEY HALL		2	219	157	OFFICE
LUTHER-BONNEY HALL		2	221	122	OFFICE
LUTHER-BONNEY HALL		3	314	210	OFFICE
LUTHER-BONNEY HALL		B	004S	24	OFFICE
LUTHER-BONNEY HALL		B	003I	80	OFFICE
LUTHER-BONNEY HALL		B	010S	14	OFFICE
LUTHER-BONNEY HALL		B	003F	90	OFFICE
LUTHER-BONNEY HALL		B	003D	97	OFFICE
LUTHER-BONNEY HALL		B	003	396	OFFICE
LUTHER-BONNEY HALL		3	315	103	OFFICE
LUTHER-BONNEY HALL		3	316	103	OFFICE
LUTHER-BONNEY HALL		G	113	267	OFFICE

Space identification

LUTHER-BONNEY HALL	G	109	324	OFFICE
LUTHER-BONNEY HALL	B	003K	90	OFFICE
LUTHER-BONNEY HALL	4	417	103	OFFICE
LUTHER-BONNEY HALL	3	321	104	OFFICE
LUTHER-BONNEY HALL	G	115	596	OFFICE
LUTHER-BONNEY HALL	G	117	119	OFFICE
LUTHER-BONNEY HALL	4	414	103	OFFICE
LUTHER-BONNEY HALL	4	411	168	OFFICE
LUTHER-BONNEY HALL	4	416	103	OFFICE
LUTHER-BONNEY HALL	4	423	105	OFFICE
LUTHER-BONNEY HALL	4	422	103	OFFICE
LUTHER-BONNEY HALL	4	421	98	OFFICE
LUTHER-BONNEY HALL	4	419	103	OFFICE
LUTHER-BONNEY HALL	G	120	150	OFFICE
LUTHER-BONNEY HALL	G	142	208	OFFICE
LUTHER-BONNEY HALL	G	142A	227	OFFICE
LUTHER-BONNEY HALL	G	144A	162	OFFICE
LUTHER-BONNEY HALL	3	325	179	OFFICE
LUTHER-BONNEY HALL	3	322	106	OFFICE
LUTHER-BONNEY HALL	3	320	82	OFFICE
LUTHER-BONNEY HALL	3	318	103	OFFICE
LUTHER-BONNEY HALL	3	317	103	OFFICE
LUTHER-BONNEY HALL	G	114	168	OFFICE
LUTHER-BONNEY HALL	G	121	123	OFFICE
LUTHER-BONNEY HALL	G	122	108	OFFICE
LUTHER-BONNEY HALL	G	123	127	OFFICE
LUTHER-BONNEY HALL	G	141	150	OFFICE
LUTHER-BONNEY HALL	G	129	124	OFFICE
LUTHER-BONNEY HALL	5	522	105	OFFICE
LUTHER-BONNEY HALL	5	521	103	OFFICE
LUTHER-BONNEY HALL	5	520	108	OFFICE
LUTHER-BONNEY HALL	5	518	103	OFFICE
LUTHER-BONNEY HALL	5	517	103	OFFICE
LUTHER-BONNEY HALL	5	514	103	OFFICE
LUTHER-BONNEY HALL	5	515	103	OFFICE
LUTHER-BONNEY HALL	5	516	103	OFFICE
MASTERTON HALL	2	229	114	OFFICE
MASTERTON HALL	2	230	114	OFFICE
MASTERTON HALL	3	304	96	OFFICE
MASTERTON HALL	3	307	103	OFFICE
MASTERTON HALL	3	306	103	OFFICE
MASTERTON HALL	2	225	504	OFFICE

Space identification

MASTERTON HALL	2	224	152	OFFICE
MASTERTON HALL	3	308	103	OFFICE
MASTERTON HALL	2	219	72	OFFICE
MASTERTON HALL	2	218	72	OFFICE
MASTERTON HALL	2	223	72	OFFICE
MASTERTON HALL	3	328	95	OFFICE
MASTERTON HALL	3	329	150	OFFICE
MASTERTON HALL	3	332	71	OFFICE
MASTERTON HALL	3	326	71	OFFICE
MASTERTON HALL	3	334	95	OFFICE
MASTERTON HALL	3	335	95	OFFICE
MASTERTON HALL	3	337	90	OFFICE
MASTERTON HALL	3	338	86	OFFICE
MASTERTON HALL	3	320	99	OFFICE
MASTERTON HALL	3	316	177	OFFICE
MASTERTON HALL	3	319	99	OFFICE
MASTERTON HALL	3	322	95	OFFICE
MASTERTON HALL	3	323	95	OFFICE
MASTERTON HALL	3	324	95	OFFICE
MASTERTON HALL	3	325	95	OFFICE
MASTERTON HALL	1	126	114	OFFICE
MASTERTON HALL	1	125	234	OFFICE
MASTERTON HALL	1	123	89	OFFICE
MASTERTON HALL	3	339	114	OFFICE
MASTERTON HALL	1	122	112	OFFICE
MASTERTON HALL	6	028	124	OFFICE
MASTERTON HALL	6	031	208	OFFICE
MASTERTON HALL	6	007	320	OFFICE
MASTERTON HALL	6	039	152	OFFICE
MASTERTON HALL	6	042	144	OFFICE
MASTERTON HALL	6	026	158	OFFICE
MASTERTON HALL	6	025	119	OFFICE
MASTERTON HALL	6	024	158	OFFICE
MASTERTON HALL	3	331	95	OFFICE
PAYSON-SMITH HALL	3	301A	916	OFFICE
PAYSON-SMITH HALL	3	301	922	OFFICE
PAYSON-SMITH HALL	6	105	460	OFFICE
PAYSON-SMITH HALL	6	119B	90	OFFICE
PAYSON-SMITH HALL	6	119	912	OFFICE
PAYSON-SMITH HALL	6	117	188	OFFICE
PAYSON-SMITH HALL	6	116	98	OFFICE
PAYSON-SMITH HALL	6	121	143	OFFICE



Space identification

PAYSON-SMITH HALL	G	111	110	OFFICE
PAYSON-SMITH HALL	G	109	132	OFFICE
PAYSON-SMITH HALL	G	115	90	OFFICE
PAYSON-SMITH HALL	G	120	108	OFFICE
PAYSON-SMITH HALL	G	110	118	OFFICE
PAYSON-SMITH HALL	G	128A	144	OFFICE
PAYSON-SMITH HALL	G	128	648	OFFICE
PAYSON-SMITH HALL	G	127	139	OFFICE
PAYSON-SMITH HALL	G	125	111	OFFICE
PAYSON-SMITH HALL	G	124	103	OFFICE
PAYSON-SMITH HALL	G	123	101	OFFICE
PAYSON-SMITH HALL	B	004F	320	OFFICE
PAYSON-SMITH HALL	B	004E	102	OFFICE
PAYSON-SMITH HALL	G	112	100	OFFICE
PAYSON-SMITH HALL	B	004B	148	OFFICE
PAYSON-SMITH HALL	B	048	102	OFFICE
PAYSON-SMITH HALL	B	047	152	OFFICE
PAYSON-SMITH HALL	B	046	157	OFFICE
PAYSON-SMITH HALL	B	045	136	OFFICE
PAYSON-SMITH HALL	G	103A	132	OFFICE
PAYSON-SMITH HALL	G	103	300	OFFICE
PAYSON-SMITH HALL	G	101E	89	OFFICE
PAYSON-SMITH HALL	G	101D	89	OFFICE
PAYSON-SMITH HALL	G	101C	127	OFFICE
PAYSON-SMITH HALL	G	101B	91	OFFICE
PAYSON-SMITH HALL	G	103B	150	OFFICE
PAYSON-SMITH HALL	G	101	578	OFFICE
PAYSON-SMITH HALL	G	101A	297	OFFICE
PAYSON-SMITH HALL	G	100E	120	OFFICE
PAYSON-SMITH HALL	G	100D	94	OFFICE
PAYSON-SMITH HALL	G	100C	89	OFFICE
PAYSON-SMITH HALL	G	100B	115	OFFICE
PAYSON-SMITH HALL	G	100A	260	OFFICE
PAYSON-SMITH HALL	G	208	90	OFFICE
PAYSON-SMITH HALL	G	205	90	OFFICE
PAYSON-SMITH HALL	G	203	108	OFFICE
PAYSON-SMITH HALL	G	202	160	OFFICE
PAYSON-SMITH HALL	G	108	125	OFFICE
PAYSON-SMITH HALL	G	209	125	OFFICE
PAYSON-SMITH HALL	G	107	90	OFFICE
PAYSON-SMITH HALL	G	104	90	OFFICE
PAYSON-SMITH HALL	G	101	160	OFFICE
POWERS HS- 86 WINSLOW	2			
POWERS HS- 86 WINSLOW	2			
POWERS HS- 86 WINSLOW	2			
POWERS HS- 86 WINSLOW	2			
POWERS HS- 86 WINSLOW	6			
POWERS HS- 86 WINSLOW	2			
POWERS HS- 86 WINSLOW	6			
POWERS HS- 86 WINSLOW	6			
POWERS HS- 86 WINSLOW	6			

Space identification

SCIENCE BLDG	3	370	193	OFFICE
SCIENCE BLDG	3	306	96	OFFICE
SCIENCE BLDG	3	314	97	OFFICE
SCIENCE BLDG	3	363A	108	OFFICE
SCIENCE BLDG	3	350	241	OFFICE
SCIENCE BLDG	5	510	170	OFFICE
SCIENCE BLDG	3	350	80	OFFICE
SCIENCE BLDG	5	512	117	OFFICE
SCIENCE BLDG	5	514	92	OFFICE
SCIENCE BLDG	5	516	117	OFFICE
SCIENCE BLDG	5	520	80	OFFICE
SCIENCE BLDG	5	518	204	OFFICE
SCIENCE BLDG	5	506	124	OFFICE
SCIENCE BLDG	2	235	248	OFFICE
SCIENCE BLDG	2	231	136	OFFICE
SCIENCE BLDG	2	221	130	OFFICE
SCIENCE BLDG	2	223	141	OFFICE
SCIENCE BLDG	2	225	141	OFFICE
SCIENCE BLDG	2	226	143	OFFICE
SCIENCE BLDG	2	227	141	OFFICE
SCIENCE BLDG	2	229	141	OFFICE
SCIENCE BLDG	2	233	115	OFFICE
SCIENCE BLDG	2	224	143	OFFICE
SCIENCE BLDG	2	270	281	OFFICE
SCIENCE BLDG	2	206	105	OFFICE
SCIENCE BLDG	2	212	97	OFFICE
SCIENCE BLDG	2	220	130	OFFICE
SCIENCE BLDG	2	222	96	OFFICE
SCIENCE BLDG	6	105	190	OFFICE
SCIENCE BLDG	6	116	107	OFFICE
SCIENCE BLDG	6	114	124	OFFICE
SCIENCE BLDG	6	112	96	OFFICE
SCIENCE BLDG	6	110	124	OFFICE
SCIENCE BLDG	6	106	312	OFFICE
SCIENCE BLDG	6	105C	220	OFFICE
SCIENCE BLDG	6	163	113	OFFICE
SCIENCE BLDG	6	161A	152	OFFICE
SCIENCE BLDG	6	161	152	OFFICE
SCIENCE BLDG	6	153	278	OFFICE
SCIENCE BLDG	6	151	113	OFFICE
SCIENCE BLDG	6	150	117	OFFICE
SCIENCE BLDG	6	151A	120	OFFICE

Space identification

SCIENCE BLDG	B	006D	118	OFFICE
SCIENCE BLDG	B	006C	95	OFFICE
SCIENCE BLDG	B	006B	54	OFFICE
SULLIVAN GYM	G	104	171	OFFICE
SULLIVAN GYM	G	109A	93	OFFICE
SULLIVAN GYM	G	110	108	OFFICE
SULLIVAN GYM	G	112A	90	OFFICE
SULLIVAN GYM	G	111A	90	OFFICE
SULLIVAN GYM	G	110A	90	OFFICE
SULLIVAN GYM	G	109	92	OFFICE
SULLIVAN GYM	G	112	144	OFFICE
SULLIVAN GYM	G	129	97	OFFICE
SULLIVAN GYM	B	012	362	OFFICE
SULLIVAN GYM	2	220	243	OFFICE
SULLIVAN GYM	2	219	183	OFFICE
SULLIVAN GYM	B	013	144	OFFICE
SULLIVAN GYM	B	014	134	OFFICE
SULLIVAN GYM	B	015	132	OFFICE
SULLIVAN GYM	2	210	72	OFFICE
SULLIVAN GYM	2	211	72	OFFICE
SULLIVAN GYM	2	213	70	OFFICE
SULLIVAN GYM	2	215	70	OFFICE
SULLIVAN GYM	2	212	70	OFFICE
SULLIVAN GYM	2	214	70	OFFICE
SURRENDEN ST- 15	G	108	132	OFFICE
SURRENDEN ST- 15	3	307	165	OFFICE
SURRENDEN ST- 15	G	103	140	OFFICE
SURRENDEN ST- 15	G	104	172	OFFICE
SURRENDEN ST- 15	3	306	172	OFFICE
SURRENDEN ST- 15	2	204	96	OFFICE
SURRENDEN ST- 15	2	201	98	OFFICE
SURRENDEN ST- 15	2	202	83	OFFICE
SURRENDEN ST- 15	2	206	172	OFFICE
SURRENDEN ST- 15	3	301	98	OFFICE
SURRENDEN ST- 15	3	302	83	OFFICE
SURRENDEN ST- 15	3	304	96	OFFICE
SURRENDEN ST- 15	2	207	155	OFFICE
WOODBURY CAMPUS CTR	G	035C	84	OFFICE
WOODBURY CAMPUS CTR	G	019	76	OFFICE
WOODBURY CAMPUS CTR	G	020	86	OFFICE
WOODBURY CAMPUS CTR	G	035E	190	OFFICE
WOODBURY CAMPUS CTR	G	01354	109	OFFICE

Space identification

5/29/01 9:15 AM

WOODBURY CAMPUS CTR	G	035B	84 OFFICE
WOODBURY CAMPUS CTR	G	035D	88 OFFICE
WOODBURY CAMPUS CTR	G	035F	92 OFFICE
WOODBURY CAMPUS CTR	G	035G	69 OFFICE
WOODBURY CAMPUS CTR	G	035H	69 OFFICE
WOODBURY CAMPUS CTR	G	035J	93 OFFICE
WOODBURY CAMPUS CTR	G	035K	113 OFFICE
CHAMBERLAIN AVE- 19	1	104	148 OFFICE
CHAMBERLAIN AVE- 19	1	103	90 OFFICE
CHAMBERLAIN AVE- 19	2	201	144 OFFICE
CHAMBERLAIN AVE- 19	2	202	139 OFFICE
CHAMBERLAIN AVE- 19	2	203	151 OFFICE
DEERING AVE-222	1	103	348 OFFICE
DEERING AVE-222	1	104	127 OFFICE
LAW BUILDING	5	511B	154 OFFICE
LUTHER-BONNEY HALL	2	232A	104 OFFICE
LUTHER-BONNEY HALL	2	232	898 OFFICE
LUTHER-BONNEY HALL	2	242	783 OFFICE
LUTHER-BONNEY HALL	G	144B	116 OFFICE
LUTHER-BONNEY HALL	G	144C	130 OFFICE
SCIENCE BLDG	B	006A	101 OFFICE
DEERING AVE-222	1	107	102 OFFICE
SCIENCE BLDG	B	006E	123 OFFICE
SCIENCE BLDG	B	006F	123 OFFICE
SCIENCE BLDG	B	006G	205 OFFICE
LUTHER-BONNEY HALL	G	144D	351 OFFICE
LUTHER-BONNEY HALL	G	144E	1,474 OFFICE
LUTHER-BONNEY HALL	G	144F	924 OFFICE
LUTHER-BONNEY HALL	2	243	125 OFFICE
LUTHER-BONNEY HALL	2	244	120 OFFICE
LUTHER-BONNEY HALL	2	245	120 OFFICE
LUTHER-BONNEY HALL	2	246	120 OFFICE
LUTHER-BONNEY HALL	2	247	120 OFFICE
LUTHER-BONNEY HALL	2	248	119 OFFICE
LUTHER-BONNEY HALL	2	249	111 OFFICE
LUTHER-BONNEY HALL	2	250	178 OFFICE
LUTHER-BONNEY HALL	2	251	137 OFFICE
LUTHER-BONNEY HALL	2	252	140 OFFICE
LUTHER-BONNEY HALL	2	253	230 OFFICE
LUTHER-BONNEY HALL	2	237A	343 OFFICE
SCIENCE BLDG	3	305C	104 Office
SCIENCE BLDG	3	305D	80 Office

Space identification

98 Bedford		1	102	100	Office	
98 Bedford		1	103	103	Office	
98 Bedford		1	104	90	Office	
98 Bedford		1	105	80	Office	
98 Bedford		2	201	130	office	
98 Bedford		2	202	119	office	
98 Bedford		2	203	118	office	
98 Bedford		2	204	140	office	
Deering 222		1	107	104	Office	
Deering 222		1	103	350	Office	
Deering 222		1	104	127	Office	
Chamberlain 19		1	103	92	Office	
Chamberlain 19		1	104	148	Office	
Chamberlain 19		1	105	130	Office	
Chamberlain 19		2	201	144	Office	
Chamberlain 19		2	202	138	Office	
Chamberlain 19		2	203	106	Office	
			Total	✓ 100,350	OFFICE	
6P040	BEDFORD ST- 25-FACMG T	G	107	545	SHOP	
6P040	BEDFORD ST- 25-FACMG T	G	116	194	SHOP	
6P047	BEDFORD ST- 46-STEEOG	G	199	7,976	SHOP	
6P006	CENTRAL HEAT PLANT-P	G	103	420	SHOP	
6P002	LUTHER-BONNEY HALL	3	309	211	SHOP	
6P013	SCIENCE BLDG	B	009	224	SHOP	
6P013	SCIENCE BLDG	B	054	466	SHOP	
			Total	✓ 10,036	SHOP	
46 Bedford	Ground Department	G		9,006	Grounds	
			Total	✓ 9,006	Grounds	



**SUMMARY OF UNIVERSITY SURVEY  
UNIVERSITY OF SOUTHERN MAINE  
PORTLAND CAMPUS**

Are you currently an enrolled student?

Yes		No	
Library	Luther Bonney Hall	Library	Luther Bonney Hall
59	175	6	25
<b>234 (88%)</b>		<b>31 (12%)</b>	

Total

Have you been or will you be visiting any other building on campus today?

Yes		No	
Library	Luther Bonney Hall	Library	Luther Bonney Hall
54	121	11	79
<b>175 (66%)</b>		<b>90 (34%)</b>	

Total

Did you park in a campus lot, along the road, get dropped-off, take the bus, walk or bike?

Campus Lot		Along the Road	
Library	Luther Bonney Hall	Library	Luther Bonney Hall
42	142	9	33
<b>184 (69%)</b>		<b>42 (16%)</b>	
Drop-offs or Bus		Walk or Bicycle	
Library	Luther Bonney Hall	Library	Luther Bonney Hall
4	12	10	13
<b>16 (6%)</b>		<b>23 (9%)</b>	

Total

Total

Date conducted: May 3, 2001  
Weather: Clear and Sunny  
Survey ongoing from 9:00 AM to 5:00 PM

**UNIVERSITY OF SOUTHERN MAINE  
SHUTTLE BUS RIDERSHIP  
1988/89 TO 2000/01**

	<u>Fall Semester</u>	<u>Spring Semester</u>	<u>Total # of Riders</u>
<b><u>Portland - Gorham Shuttle</u></b>			
1988/89	92,113	85,259	177,372
1989/90	112,185	96,077	208,262
1990/91	130,316	137,294	267,610
1991/92	122,425	113,944	236,369
1992/93	97,623	95,681	193,304
1993/94	90,211	87,527	177,738
1994/95	85,371	87,111	172,482
1995/96	100,096	86,207	186,303
1996/97	76,865	66,675	143,540
1997/98	73,275	66,399	139,674
1998/99	62,744	50,451	113,195
1999/00	52,697	40,239	92,936
2000/01	43,477	34,859	78,336
<b><u>Portland Hall to Portland Campus</u></b>			
1998/99	12,252	14,034	26,286
1999/00	11,662	11,572	23,234
2000/01	13,447	11,347	24,794
<b><u>Marginal Way to Portland Campus</u></b>			
1998/99	4,422	4,359	8,781
1999/00	2,964	4,433	7,397
2000/01	2,381	2,846	5,227





JUN-12-01 TUE 2:26 PM

USM FACILITIES MGMT.

FAX NO. 2077805520

P. 1

JUN-10-01 TUE 11:06

USM PRESIDENT'S OFFICE

FAX NO. 2077804549

P. 01



June 11, 2001

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program

JUN 12 2001

For your posting the following is ridership for the program:

Year 2000

March 1,234  
 April 2,024  
 May 2,227  
 June 1,893  
 July 1,909  
 August 2,059

Cumulative Free Rides: 11,346

September	1,313	February '01	1,459
October	1,616	March '01	1,567
November	1,588	April '01	1,587
December	1,504	May '01	1,525
January '01	1,734		

Cumulative 50-Cent Rides: 13,893

Total All Rides (March '00-May '01): 25,239

Thank you for the support that you have given to the METRO and to this program. We have already begun the planning for incoming students in the fall. We will extend our advertising in the FREE PRESS in September and continue to submit the invoices and updates to you.

Post-it  
 Fax Note R78735  
 To Randy Quenton  
 Date 6/12  
 # of pages 2  
 From Dave Farley  
 Phone#

Date	# of pages 1
From: Sam Andrews	
Co.	
Phone #	
Fax #	

GREATER PORTLAND  
 114 Valley Street • PO Box 1097 • Portland, ME

**From:** Larry Ash  
**To:** Alex Jaegerman , Sarah Hopkins  
**Date:** Fri, Feb 9, 2001 7:27 AM  
**Subject:** USM expansion

Sarah:

As regards the traffic report prepared by DeLuca-Hoffman Associates, I have the following comments:

1. The report states that the Forest Ave/Bedford St intersection operates at an acceptable Level of Service(LOS). I disagree. Lane approaches for the eastbound left-turn, northbound left-turn and the westbound left-turn operate at LOS E, E and F respectfully and therefore the intersection LOS as a whole cannot be acceptable. The latest Highway Capacity Manual (HCM) defines delay and LOS in terms of "control delay" rather than "stopped delay". Thus, the latest HCM defines the "D" range LOS to be 35 to 55 seconds instead of the previous 25 to 40 seconds. It is my opinion that any lane approach with a **stopped delay** in excess of 40 seconds operates at an **unacceptable** LOS and therefore the **intersection** requires mitigation. AM LOS analysis also needs to be provided.
2. The intersections of Forest Ave/Dartmouth and Brighton/Falmouth/Deering need to be included and AM analysis also provided.
3. Assuming that Bedford Street would be closed some assumptions and a comprehensive analysis needs to be made **and** justified on vehicular dispersions or assignment at each of the above referenced intersections and a capacity/LOS analysis reiterated. Unacceptable Levels of Service need to be addressed and mitigation proposals/alternatives reviewed.
4. The proposed exit/entrance for the parking garage on Chamberlain Street is unacceptable. Bill Bray, Public Works Director had made this clear very early in the discussions of the expansion.
5. If the Marginal Way parking lot is lost due to Amtrak coming to Portland, how will the parking be affected and what adjustments would USM have to make?
6. How will parking be handled during the construction phases, e.g. the Bioscience Bldg and the parking ramp?
7. If the Bioscience Bldg is built before the parking ramp where will these users park?
8. I think more information needs to be provided for the Community Center and auditorium, when will these be used at their peaks, by how many, and where will users be parking, etc.???

**CC:** Penny Littell , William Bray

On Monday and Tuesday November 26 and 27 I counted the number of vehicles parked in the immediate vicinity of the USM Portland campus and at the USM commuter parking lot at Marginal Way. The counts were conducted between 10 and 11 am. While they were done in an unscientific and admittedly biased manner they are representative of what the neighbors of USM face each and every day when USM is in session.

	11/26	11/27
Washburn (Surrenden to Deering)	16	20
Deering/Brighton	52/33	62/35
Falmouth (Brighton to Forest)	76	70
Surrenden	24	26
Total	201	213
Marginal Way	14	9

I only counted the major streets but it can be assumed that there are another 50 or so vehicles associated with USM parked along Oakdale, Pitt, Chamberlain, Fessenden, and Falmouth streets at the same time. I also counted the number of vehicles parked along those streets on Sunday the 25 to get a baseline of resident parking ...3 on Falmouth 1 on Surrenden, none on any of the other streets.

Respectfully Submitted

Michael Gibbs  
3 Washburn Avenue  
Portland, Maine

**From:** "Dave Early" <dearly@usm.maine.edu>  
**To:** "Sarah Hopkins " <SH@ci.portland.me.us>  
**Date:** Thu, Oct 4, 2001 1:32 PM  
**Subject:** USM Parking Management Plan

Sarah,

We had a good meeting with John and Larry this am. Thanks for allowing us to use your conference room. They will be sending you a letter addressed that summarizes our collective work and many of the accomplishments to date. Most of the focus here is neighbourhood on street parking. We also discussed snow removal and I will provide you a letter that we do, and will haul snow as necessary, to provide adequate parking within our parking lots.

We left the Marginal Way Parking Agreement off at Lee Urban's office.

As we discussed at the Neighbourhood meeting, we are making changes to site plans on the Falmouth Street Side of the Science Facility to improve the landscaping, entrance and appearance of the building.

Our parking management plan will also be updated. John and Larry provided us with some good input, but we need to examine the plans from Cheverus, Waynflete, Mercy, and MMC Sarah, can you get me these plans on or before Monday, 10/8? We would be very happy to pick them up at your office.

Lastly, I would like to fix at date, time, and place for us to meet before the Public Hearing on the 23rd, to turn our material over to you for your review, and distribution to the Planning Board.

Thanks

Dave E.

**CC:** Portland.CityHall(AQJ,LDU,JBP),Portland.gwgwia("ca...



# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4200 FAX (207) 780-4860  
TTY (207) 780-5646

August 22, 2001

Ms. Sarah Hopkins  
Development Review Program Manager  
City of Portland  
389 Congress St.  
Portland, ME 04101

Dear Sarah:

As requested, I'm forwarding summaries of our meetings with neighbors who live in the vicinity of USM's Portland campus. USM representatives met with neighbors three times between February and April of this year.

For your reference, I've also included letters of invitation to each of the meetings, and USM's responses to the concerns raised at the meeting of April 5. To help you identify which pieces of information go with which meeting, I've numbered the pages as follows:

1. Invitation to the meeting of February 9.
2. Summary of the meeting of February 9.
3. Invitation to the meeting of March 16.
- 4, 5, 6. Summary of the meeting of March 16.
7. Invitation to meeting of April 5.
- 8, 9. Summary of meeting of April 5.
- 10, 11, 12. USM response to concerns raised at meeting of April 5.

To continue our dialog with neighbors, we will offer neighborhood representation on all future building committees; hold informational meetings each fall; and host meetings related to specific building projects to identify any concerns.

Please feel free to give me a call (780-4200) or an e-mail ([caswell@usm.maine.edu](mailto:caswell@usm.maine.edu)) if you have any questions regarding this information.

Sincerely,

A handwritten signature in black ink that reads "Bob Caswell".

Bob Caswell  
USM Media & Community Relations

RSC/jao

Enclosures

1



# UNIVERSITY OF SOUTHERN MAINE

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January 25, 2001

Dear USM Neighbor:

We'd like to invite you to a neighborhood meeting to find out more about building plans on the Portland campus of the University of Southern Maine.

The meeting will be held at 7 p.m., Friday, February 9, in the cafeteria of the Woodbury Campus Center, Bedford St., Portland. There should be adequate parking in the Bedford St. lots, located a short distance from the intersection of Bedford and Forest Ave.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP to 780-4200.

USM officials will be outlining the plans for the Biosciences Research Institute. This facility, which will be financed with state research and development funds, is planned as an addition to the Science Building on Falmouth St.

We also want to share with you our master plan for the development of the Portland campus. For the most part, the plan represents a 20 to 30-year vision as to how our Portland campus might develop, depending on state funds, private gifts, enrollment trends and other factors. There are only three projects under consideration during the next five years: the Biosciences Research Institute; renovation of the former Steego Building on Bedford St. to house our Muskie School; and construction of a Community Education Center/parking garage on the current Bedford St. parking lot.

USM's mission is to enhance the region's quality of life. Our success in doing that depends on thoughtful partnerships with the community. We value your input and hope to see you on the 9th.

Sincerely,

Bob Caswell  
Executive Director, Media & Community Relations

Dave Early  
Executive Director, Facilities Management

RSC/jao

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## REVIEW OF USM NEIGHBORHOOD MEETING

The following is an outline of the discussions from the neighborhood meeting, held at 7 p.m., Friday, February 9, in the cafeteria at the University of Southern Maine's Woodbury Campus Center, Portland.

Thanks to the cooperation of the city's Planning Department, we were able to mail 1,219 letters of invitation to residents in an area roughly bounded by Back Cove, the Parkside neighborhood, St. John St. and through the Oakdale section to Douglass St.

Thirty-five people attended, and shared with us concerns ranging from parking to the type of research to be conducted in the proposed Biosciences Institute.

I've grouped those concerns under the following headings, and under each heading have summarized issues that were addressed.

### **PARKING/TRAFFIC:**

Much of the 3+ hour meeting was devoted to this issue. In the short term, many residents felt that our plan to compensate for loss of parking during construction of the Biosciences Institute was flawed in that most people would not use the Marginal Way lot. We responded that an aggressive campaign to inform people of the Marginal Way lot, coupled with a beefed up shuttle bus service, would serve us and the neighbors well during the construction period.

It was further explained that the University is implementing a revised class schedule for the fall, 2001. The schedule is designed to help distribute uniformly space demands throughout the week as opposed to the current schedule that causes peak demands on Tuesdays and Thursdays.

Several questioned why we are planning to construct a research building before a parking garage. We explained that dedicated funding that must be used or be forfeited is in place for the research facility. We also noted that the facility will be dedicated exclusively to research and will not include classrooms and other high-traffic areas. Guests questioned the number of researchers that would be housed in the building.

Neighbors also voiced frustration at students' cars that are taking up on-street parking in and around their neighborhoods. A USM/city collaborative effort to address the problem ought to be initiated, they said. As part of this discussion, people asked why the long-range campus master plan did not contain any provisions for developing on-campus student housing. We explained that our plans call for continuing to emphasize Gorham as a traditional campus environment. A new 224-bed residence hall will open in Gorham this summer. Portland Hall on Congress St. seems to satisfy student demand for Portland-based housing. We have had informal discussions about the possibility of a cooperative agreement with private developers to provide housing but there are no plans to do so.

Guests also expressed concerns over the lack of studies detailing how the closing of Bedford St., for example, would affect traffic flows in the neighborhoods. We explained that the Bedford St. proposal is one of several long-range visions included as part of the campus master plan. As such, it is so far in the future that it does not yet warrant detailed studies.

//more on back

3

USM

# UNIVERSITY OF Southern Maine

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FAX (207) 780-4860

March 2, 2001

Dear USM Neighbor:

We'd like to invite you to a neighborhood meeting at 7 p.m., Friday, March 16, in the cafeteria of the Woodbury Campus Center, Bedford St., Portland.

Parking Manager John Peverada and Traffic Engineer Larry Ash of the City of Portland will join us so that we can respond to your questions and concerns about traffic and parking in the campus neighborhoods. A special thanks goes to City Councilor Tom Kane for his help in making these arrangements.

We also want you to know that we heard the concerns expressed at the meeting of February 9, especially those related to a lack of neighborhood input into the development of campus plans.

As a start toward building a better relationship, we want to offer neighborhood representation on university building committees, and host meetings related to specific projects so that we can identify concerns and gain your input. We can discuss these and other ideas in more detail on the 16th.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP at 780-4200.

We hope to see you on the 16th.

Sincerely,



Bob Caswell  
Executive Director, Media & Community Relations



Dave Early,  
Executive Director, Facilities Management

RSC/jao



SUMMARY OF FLIP CHARTS AT  
USM/NEIGHBORHOOD/CITY MEETING ON  
MARCH 16, 2001

Consensus of meeting was that the University, neighborhood and City need to join together as **partners** to develop a common vision.

Phase I of the Biotech Facility (2 stories) is about to begin the city planning board process. The University must spend \$2, 400,000 of the construction monies authorized for this building by November 2001 or lose it. The group agreed that this project will be the first test of the commitment by all parties to the partnership.

The group identified the following issues that need to be addressed regarding the Biotech Facility (Phase 1)

- \*Parking
  - During Construction
  - will it make a bad situation even worse?
- \*Traffic
  - more people, more cars
  - (USM said that there will be 12 new employees and no new classrooms)
- \*Construction Management
  - Time, dust, blasting
- \*Safety of Activities
  - What materials used in labs, infectious viruses, etc.?
- \*Process
  - Adversaries or Partners

5

## SUMMARY OF 3/16 MEETING

The group also addressed the following parking/traffic issues and potential solutions:

### ISSUES

#### "Arrogant" parkers who:

- block driveways
- park too near driveways
- park too close to corners
- park in front of fire hydrants
- dump trash in the neighborhood

\*USM mail and security vehicles don't use USM building driveways

\*Need more enforcement/ticketing

\*Tickets are just a slap on the wrist

\*Falmouth Street too narrow because of snow

\*Takes an hour to tow a car that's blocking a driveway

\*USM advertises/encourages neighborhood parking in Lifeline brochure

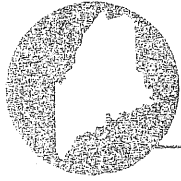
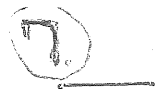
\*Parking on Brighton Avenue causes safety problems (too narrow in winter and sight problems coming out of William and Pitt Streets)

\*No parking control coverage after 5:00 p.m.

## POTENTIAL SOLUTIONS

USM and City need to work together on parking/traffic issues

- \*Enforce existing laws (1& 2 hour limits)
- \*Designate parking control officer specifically for USM area (pay for itself or USM pay?)
- \*Discourage students parking in Portland and taking shuttle bus to Gorham
- \*Resident parking only streets
- \*Resident only on one side of street
- \*Ban parking on Brighton Avenue from Falmouth to William
- \*Reduce parking from two hours to one hour
- \*Improve people moving from satellite parking areas
- \*Chamberlain Street needs time limit to on street parking
- \*More "No Parking Between Signs" (near driveway) further apart
- \*Meter parking on Falmouth Street (USM side)
- \*Stiffer fine for ticket on blocked driveway
- \*Join forces to get bond for parking structure
- \*USM staff use USM driveway
- \*USM offer incentive for staff to park at satellite lots
- \*During construction require contractor to park at satellite lot – and enforce!
- \*Improve Forest Avenue I-295 exit/entrance ramps



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March 27, 2001

Dear USM Neighbor:

Our next neighborhood meeting will be held at **6:30 p.m., Thursday, April 5**, in Room 1 (basement level) of Payson Smith Hall, Falmouth Street, Portland.

As we agreed, the next meeting will be a facilitated discussion on USM's plans for a Bioscience Research Institute. We have retained the services of Susanna Liller of Barton & Gingold to lead the discussion. USM President Rich Pattenaude also will be joining us.

Notes from our meeting of March 16th are attached, courtesy of City Councilor Tom Kane.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP at 780-4200.

We hope to see you on the 5th.

Sincerely,

Bob Caswell  
Executive Director, Media & Community Relations

RSC/jao

(8)

USM Neighborhood Meeting #3: April 5, 2001

**Meeting Purpose:** To provide an opportunity for the neighbors of USM to give feedback and ask questions about the Biosciences Research Institute and for USM to understand their concerns and answer their questions.

**The following ideas / issues were raised at the meeting:**

**HOW TO MAKE THE BUILDINGS MORE ATTRACTIVE?**

- Shift building as far away from Falmouth Street to minimize shadow effect — look at shadow study
- Have pictures of what homes across the street will look like — 3D models

**PHASE II ISSUE — ADDITIONAL STORIES**

- 1988 — Building always been planned
- As a concept, we like the project — it's the location — find way to work together
- Don't want to take over more parking in that location — larger footprint — park within two blocks
- Issue is with phase II — alternatives to phase II?
- Options to flow out? Make it larger floor plate and then take up parking/green space
- Less focused on height — not sure getting funding anyway
- Need to consider functionality and cost
- Don't know what trade offs are to remove another floor
- Willing to entertain question
- Until see shadow lines — don't know how much impact
- Issue — lose view of city

**PARKING (MAJOR ISSUE)**

- Find a way to identify contractor vehicles to make sure they are parking in Marginal Way lot and nowhere else!
- Teeth in their contract (contractor's contract) to make sure significant penalty if they don't follow through
- Love seeing growth at the University — cultural/economic focus it brings to our area is very positive
- Could USM contract with city for 1 full time parking officer until building is built (at least)? Though we know, enforcement is not only answer. USM hoping by construction begins — we add 161 spaces (Green Street — incorporating existing lot) (27 spaces near Chamberlain)
- Our staff/students no right to park incorrectly — stiffer fines
- During construction — requirement to park in Marginal Way lot — contract — put workers there
- Bus employees from remote shuttle lot? Contract problems
- Come up with incentives to get students to park in shuttle lot?
  - \$5 vs. \$25 to park in Marginal Way is current incentive
  - Union issue to enforce faculty
  - Maybe make it free?
  - Certificates?

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## USM Neighborhood Meeting #3: April 5, 2001

- Why can't campus police ask students to move car? Federal law USM can't. We can't release info — Buckley Amendment
- Have you looked at scheduling of courses? Yes, next year, change in schedule to reduce peaks
- Coffee in Marginal Way lot/Dunkin' Donuts (media campaign) — more incentive for students to park there
- Get people to walk across Forest Ave
- What have other schools done — have you looked into this? What they've done with parking.
  - \$250/yr average urban parking — we have \$25
- Utilize parking garage within city — get cut rates then bus students
- Consistent, focused enforcement (now spotty at best) \$10 not deterrent for ticket — boots?

### CONSTRUCTION MANAGEMENT

- Hot line
- Communicate rules in advance
- Will do renovations in building this summer — construction manager can meet with anyone

### WHAT WILL BE GOING ON IN THE LAB?

- Hiring new people — don't completely know at this point
- Safety will be a major focus
- Probably people working on infectious diseases
- Variety
- Researchers will do some teaching — not about enrollment growth
- Nothing that isn't already going on now at USM

### NEXT STEPS

- Another meeting?
  - Give USM time to review input from tonight and develop response
  - Yes — USM will let people know when the next meeting will be
- Master plan involvement?
  - Comprehensive plan — always in evolution
  - Over next couple years — should involve community
- Add this attendance list to planning board meetings announcements so we can attend planning board meetings — change venue?
- List of local legislators — let them know how you feel about the funding — USM will provide list

(text of e-mail sent to neighbors, response to the meeting of April 5.)

Dear Neighbors:

As a follow up to our last meeting, we're responding to your concerns. USM's commencement and related on-campus events, coupled with discussions with City Hall to clarify some zoning issues, have contributed to the delay in responding to you.

Many thanks for your patience. And please, don't hesitate to give us a ring (780-4200) or e-mail (caswell@usm.maine.edu) if you want some university updates.

Below, we've addressed one-by-one the Bioscience building project and related concerns.

**BUILDING LOCATION:**

This is a fundamental issue. Some of you have asked us to reconsider the location of the Bioscience Research Institute. To cut right to the chase, we still plan to build the institute as an addition to the current Science Building on Falmouth St. For the sake of operating efficiency and cost effectiveness, a building dedicated to scientific research has to be attached to our current science facility. In fact, there were several sites considered. The location was selected based on availability of needed services in the current Science Building; cost; and the fact that constructing the institute as an attachment to the Science Building will allow us to improve the appearance of the cement "block house" section of the Science Building.

We started working with the city on this project in August of 2000, but unfortunately did not start working with you during the project's early stages. We regret that. All we can do is make a genuine commitment to giving you input into the early planning of all future projects.

**BUILDING HEIGHT:**

The funding picture for the Bioscience Research Institute has become clearer since our last meeting. We now are certain that available funding only will allow us to construct a two-story building. (Incidentally, you may hear or read references to a three-story building. Those three-story references include a basement level, plus two levels above ground.)

Granted, we hope in the future to add up to three additional stories, for a total of five above ground. But in recent discussions with the city, we have learned that any additional stories will require an entirely new application and permitting process.

As promised at our last meeting, we will share with you architectural studies of the building's shadow effects. Frankly, the timing of that isn't as critical since, as noted above, there's no money for more than two stories, plus any additional stories in the future will necessitate a completely new application and permitting process.

(text of e-mail sent to neighbors, response to the meeting of April 5.)

#### CONSTRUCTION PHASE:

In all likelihood, we will not be breaking ground until late fall, at the earliest. We will make sure that the contract requires construction workers to park at Marginal Way. We also will ask the on-site construction managers to attend a neighborhood meeting so that they are aware, firsthand, of your concerns. Finally, Dave Barbour, USM director of facilities management, will set up a special phone line in his office to field any construction-related questions and concerns.

PLEASE NOTE: Beginning in mid-June, you will notice some construction activity around the existing Science Building on Falmouth St. This work is NOT associated with the proposed Bioscience addition. The work is previously planned renovations to the existing building that include: installation of sprinkler/fire alarm systems; renovations of bathrooms; creation of a high-tech classroom in the basement; connection of sewer/water lines to the city line; and upgrades to the road entrance off Falmouth Street. The contractor will be using the loading dock as his staging area and for deliveries. The construction manager for Pizzagalli Construction also has been instructed that all personal vehicles of employees are to be parked in the Marginal Way lot.

#### PARKING:

It's clear from your feedback that we must address the parking issue, through stricter enforcement and/or creation of additional parking.

Several of you suggested that we enter into an agreement with the city to fund an additional parking officer to patrol the streets surrounding the Portland campus. After getting some feedback from Portland Parking Manager John Peverada, we've decided that it would be more effective to create new spaces on campus. More officers on neighborhood streets, in our opinion, will only serve to spread the problem further into the neighborhoods.

Consequently, we are working on creating well over 100 new parking spaces on campus. Those spots will be realized through expansion of the current Alumni House lot and the lot at the Deering Ave. end of Payson Smith Hall; conversion of the university-owned Grand Street into new spaces; and construction of additional spaces in the driveways behind university-owned houses along Bedford St. We will have to seek city approval for these spaces, but we plan to have them in place by the time construction starts on the Bioscience building. According to a city formula, the Bioscience project requires 33 new parking spaces, plus replacing the 38 spaces that will be lost in the lot between the Science Building and the Sullivan Gym due to construction.

You also asked us to consider ways to promote parking at Marginal Way, the 300-space lot that is underutilized by USM faculty, staff and students. Beginning this fall, we will offer free parking to any USM student or employee who wishes to use Marginal Way. We'll be promoting the offer on campus, and also reminding people of the free shuttle



(text of e-mail sent to neighbors, response to the meeting of April 5.)

service between Marginal Way and campus locations. We'll also continue to tweak the class schedule to reduce peak demands.

Our long-range plans call for construction of a community education center/parking garage on the current Bedford St. lot that will net us 600 spaces. That will be adequate to meet projected parking demands associated with the proposed Muskie Building (the former Steego Building), the Biosciences project and the community education building. Those plans are contingent upon legislative approval of a university bond issue and voter approval of the bond in the November elections. If you'd like to advocate for passage of the bond issue, contact your local legislator and ask him or her to support L.D. 1378, a bill to authorize a general fund bond issue of \$65.2 million for the University of Maine System and Maine Maritime Academy.

Hopefully, this addresses your major concerns. There undoubtedly are additional concerns that will need to be addressed before we receive final approval. We'll work those out in collaboration with you, either during the Planning Board sessions, or in campus meetings with you.

As we've said at previous meetings, we are committed to making sure that we gain your input on all future projects.

We will:

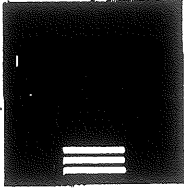
1. Offer neighborhood representation on all our building committees that are formed to develop campus-based projects.
2. Hold an informational meeting each fall to update neighbors on our plans and priorities for the coming academic year.
3. Host neighborhood meetings related to specific building projects to identify any concerns and gain neighbors' input.
4. Add neighbors to our mailing lists for calendars of events/internal newsletter.

NEXT STEP:

We probably won't have any new issues to discuss until late summer or early fall, once the Planning Board schedule is finalized and we have a clearer picture of the proposed construction schedule. Nevertheless, if you'd like to meet with us this summer, we'd be happy to do so. Please call 780-4200 or drop an e-mail to [caswell@usm.maine.edu](mailto:caswell@usm.maine.edu)

We will keep you posted on any new information related to the above issues.

Sincerely,  
Bob Caswell  
USM Media & Community Relations



DELUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

778 MAIN STREET  
SUITE 8  
SOUTH PORTLAND, MAINE 04106  
TEL. 207 775 1121  
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- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

September 7, 2001

Ms. Sarah Hopkins  
Planning Division  
Portland City Hall  
389 Congress Street  
Portland, Maine 04101

**RE: USM - Bio-Science Building Project 114A-A-001  
Parking Requirements**

Dear Sarah:

We have reevaluated the parking requirements for the proposed Bio-Science building in response to Marge Schmuckal's letter of September 4, 2001. We had included on-street parking spaces in our parking supply as has been done historically for the roads in the immediate vicinity of the campus. However, Marge has indicated that the specific ordinance requirement is for off-street spaces and we cannot include the 179 on-street spaces in our supply.

Marge also indicated that the ordinance allows the Planning Board to consider joint use of parking as described in section 14-343. We have previously conducted parking studies and surveys to assess joint use of parking on the campus as summarized in the attached document. The joint use parking analysis indicates that once the Bio-Science building is constructed for phase 1 (floors 1 & 2 only), the campus will have a surplus of three off-street parking spaces. The 179 on-street parking spaces will serve as an additional buffer to the 3-space surplus.

Please review the attached "Joint Use Parking Discussion" and contact me with any questions you may have.

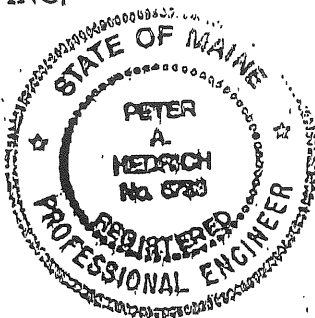
Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.

Peter A. Hedrich, P.E., PTOE  
Senior Engineer

PAH/sq/JN2074/Hopkins9-7

Attachment



c: David Early

Sam Andrews

Joe Laverriere

University of Southern Maine  
Joint Use Parking Discussion  
September 7, 2001

The USM Portland Campus includes a variety of uses to serve the needs of the student population. We would expect that these uses such as classroom space, library space, lounge areas, food facilities and merchandising provide students with the opportunity to participate in several activities on each visit to the campus. Therefore, a student will occupy a parking space to attend classes and then utilize one or more of the complementary facilities. Given these occurrences, the overall parking demand for the campus would not be a summation of the individual demand for each facility, but some lesser amount depending upon the frequency of these joint use visits.

To determine the joint use parking demand, DeLuca-Hoffman Associates, Inc. performed parking demand counts on the campus and surrounding street system. The results of these counts that were performed in the fall of 2000 are summarized in the DeLuca-Hoffman Associates, Inc. report "Final USM Portland Campus Parking and Traffic Study for Campus Master Plan" dated December 2000 and further reported in the DeLuca-Hoffman Associates, Inc. letter dated June 20, 2001. This letter indicates a total campus parking demand on the busiest day of the week to be 1,517 spaces. For comparison purposes, the parking demand for individual uses was determined to be 1,625 spaces based on City Ordinance. The attached summary shows that the most likely candidates for shared uses such as the library, lounge, food facility, and merchandising spaces indicate a total parking demand of 218 spaces, which is roughly double the difference (1625-1517=108) in actual versus ordinance parking demand. Therefore, approximately 50% of the users of these facilities may be on campus for other uses. To confirm this shared use premise, DeLuca-Hoffman Associates, Inc. performed a survey of random students entering/leaving the library and Luther Bonney Hall on May 3, 2001 from approximately 9 AM to 5 PM. This survey indicated that approximately 66% of students utilizing those buildings were going to visit another use on campus on that day. This reinforces the concept that shared use of parking is occurring on the campus.

Given the survey results, we are comfortable that the field count of 1517 occupied spaces fairly represents the actual parking demand for the existing facilities and class schedules for the fall 2000 schedule. The University has implemented schedule changes and parking incentives to balance parking demands that may further reduce the actual parking demands. To determine the parking requirement after construction of the Bio Science Building, we have determined the assignable space and associated ordinance requirements for this building without joint use adjustments. The construction will result in the loss of 44 spaces on the Bio Science lot. The joint use versus ordinance requirements are summarized in the following table:

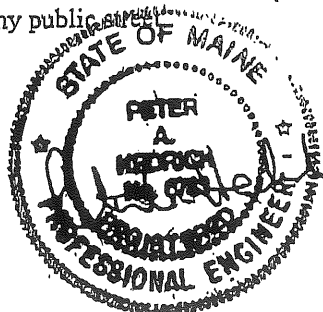
USM Portland Campus, Parking Summary					
Parking Requirements Based On:	Parking Req. to Meet Existing Demand	Parking Req. for Proposed Bio Science**	Total Required Parking	Proposed Supply ***	Req. New Parking Spaces
Joint Use	1,517* (Includes 179 On Street)	34	1,551	1,554	(3)
City Ordinance	1,625	34	1,659	1,554	105

\* This is the demand for the busiest hour of the day on the busiest day of the week (1338 in USM lots plus 179 on street).

\*\* Ordinance: 12,940 sf office @ 1/400 sf=33 spaces plus 1,000 sf storage @ 1/1,000=1 space

\*\*\* Includes on campus (1,200), outbuildings (38), Grand Street (12), Baxter Blvd. (48), Marginal Way (300) and loss of 44 spaces for construction of the Bio Science Building

The above table indicates a surplus of three spaces based on the shared use assumption for parking demand. This does not include any consideration of use of on street parking on any public street.



USM.

Marginal Way.

300 spaces never used.

If students don't use MW, they park in h'hood.  
Joint funding for pkg officer.

rumors on Exeter... pkg behind Law school to be expanded.  
increase shuffle.

[Bldg. - Leslie.

No plans to tear down houses on Exeter.

Need more aggressive parking mgmt plan.

ex. Wayneville, MENE, other examples  
Cheverus.

Ed Suslovick

1. sunk pkg lot provides barriers.

2. pkg plan is not enough.

- need pkg officer, need help w/

- master plan:

Michael Gibbs

Snow takes up pkg spaces. — check on mainting  
list at USM.

ped access

winter snow-effect.

pkg plan.

→ See ME Med. / Cheverus.

Cond hand Mfg.  
Should haul snow.  
23rd.

Get Stebbins.

- Dragon.
- History of site.

Language ~~of~~ in contract - peer review

Need to meet w/ PW. on landfill.

Rock crusher? When? Which standards? Dust, noise

Possible to be present for a blast

CHANGES TO CONTRACT

⇒ Roadway mps will meet tech. design standards

⇒ Do we need a policy statement

⇒ Renew new owner. - Planning Bd approves  
successor-ability to meet.

⇒ Reclamation Standards -

How will it look

⇒ Air blast? How do we restrict, regulate it?

Air blast is included in DEP standards.

Mr DeWitt. Drill - noise

Dust

Brad K. T76 Ocean.

Consistent w/ uses.

9/25/01

Mercy Wkshp.

Orlando - no parking concerns.

- limit one helipad.

Cyrus - Where is front yard?

Need more than 15ft buffer/front yard setback

Steve Bushey. 40ft ROW

Look at other setbacks.

⇒ 40ft?

Ken - use centerline of road.

Development Standards - Supplemental Standards  
Difficulties w/ out-parcels - No frontage?  
private roadway?

Deb ambiguity of guidelines /

"such as"... high degree of ped. orientation.

Eric

Zoning Division  
Marge Schmuckal  
E. Gray, Jr.  
Zoning Administrator  
Director

Department of Urban Development  
Joseph



## CITY OF PORTLAND

September 4, 2001

Randall E. Dunton, P.E  
C/o Deluca-Hoffman Associates, Inc.  
778 Main Street  
Suite 8  
South Portland, ME 04106

RE: USM – Bio-Science Bldg. Project - 96 Falmouth St. – 114A-A-001 – R-5 zone

Dear Randy,

My initial reading of your parking study did not pick-up the fact that your parking summary includes 179 parking spaces “on street”. The City’s parking requirements are for off-street parking requirements. Those are public parking spaces and can not be claimed by the University for their purposes only. If those 179 on-street, public parking spaces are removed from your equation, the Bio-Science building project would be lacking 27 required off-street parking spaces.

It seems to me that the University has several options to pursue concerning this matter. First, your study stresses that there are many incidents where “shared” parking comes into play. Section 14-343 of the Zoning Ordinance allows the Planning Board to approve “joint use” of parking spaces. When you go before the Planning Board for your required Site Plan Review, that Board may also approve the joint use of parking in order to meet your required off-street parking requirements.

Alternatively, you would also have the right to go before the Zoning Board of Appeals to request a variance in the required number of parking spaces. Please note that variance appeals are very difficult to have granted by the Zoning Board. You would have thirty (30) days from the date of this letter in which to appeal this decision. If you choose to appeal before the Zoning Board of Appeals, please contact me as soon as possible in order to obtain the necessary paperwork in order to apply for an appeal.

Please call me if you have any questions.

Very truly yours,

Marge Schmuckal  
Zoning Administrator

Cc: Sarah Hopkins, Planning  
Sam Andrews, University of Southern Maine  
Dave Early, University of Southern Maine  
File



Attachment 6

**From:** Marge Schmuckal  
**To:** "dearly@usm.maine.edu"@Portland.gwgwia; Sarah Hop...  
**Date:** Wed, Sep 5, 2001 5:00 PM  
**Subject:** Re: Planning Board Workshop

Sarah,

I have spoken with Peter Hedrick concerning this matter. The zoning ordinance requires "off-street" parking for uses. On street parking is considered public parking and can not be assigned to any specific company or use. I am not aware that any on street parking spaces had been allowed to count towards required off-street spaces in the past. That surprises me. I will also point out that I used the wrong figures to determine the number of spaces that would be lacking. It now looks like they are lacking 117 required spaces.

Marge Schmuckal, Zoning Administrator

>>> "Dave Early" <dearly@usm.maine.edu> 09/05 4:07 PM >>>

Sarah,

As you may know, Randy Dunton is not longer with DeLuca Hoffman. I do remember Marge asking about the on-street parking. Can you please explain to me why it can no longer be counted?

Both our engineering consultants and the architects are scheduled to be available for the workshop on the 11th.

Were pleased to know we are 2nd.

Thanks, and any help you can give me on the explanation of the on street parking would be appreciated.

Dave E.

Date sent: Wed, 05 Sep 2001 13:24:19 -0400  
From: "Sarah Hopkins " <SH@ci.portland.me.us>  
To: [dearly@usm.maine.edu](mailto:dearly@usm.maine.edu)  
Copies to: [MES@ci.portland.me.us](mailto:MES@ci.portland.me.us)  
Subject: Planning Board Workshop

Dave,

I just wanted to confirm that we are on for the September 11 planning board workshop. There has been some discussion between Marge and Randy regarding the on-street spaces being counted for parking totals. Since they cannot be counted, the Planning Board will have to approve a shared parking scheme. We will need something in writing describing the shared aspect of the parking (it makes a lot of sense) for the Board to approve.

I am going to give Joe at DeLuca a call to get some reduced plans for the Planning Board's memo.

USM is 2nd on the workshop agenda. The meeting begins at 3:30 in City Council Chambers.

-Sarah

**CC:** "sandrews@usm.maine.edu"@Portland.gwgwia

Zoning Division  
Marge Schmuckal  
E. Gray, Jr.  
Zoning Administrator  
Director

Department of Urban Development  
Joseph



## CITY OF PORTLAND

September 4, 2001

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778 Main Street  
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USM is 2nd on the workshop agenda. The meeting begins at 3:30 in City Council Chambers.

-Sarah

**CC:** "sandrews@usm.maine.edu"@Portland.gwgwia



# UNIVERSITY OF SOUTHERN MAINE

Office of Finance and Administration

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4484  
FAX (207) 780-4549  
TTY (207) 780-5646

DATE: January 18, 2001

TO: Mr. David Early, Executive Director, Facilities Management

FROM: Samuel G. Andrews, Office of Finance and Administration

RE: Biosciences Research Institute Addition

As I understand it, the City of Portland Planning Board and Planning Staff requested some additional information relative to future enrollments at the Portland campus as well as information on USM's parking mitigation efforts. In regard to the Biosciences Research Institute Addition to the Portland Science Building, it should be noted that this is a research facility only and will not have any impact at all on student enrollment.

The Maine Legislature has made two targeted additions to the University of Maine System base budget designed to substantially increase research and development (R&D) activity at the University of Maine and the University of Southern Maine, in particular to strengthen the capacity of both Institutions to compete for Federal grants and contracts. The annual base budget operating increase is \$10 million plus \$2.5 million to cover debt service payments on a \$25 million internal bond issue for a period of 15 years. The general goal is that together the Universities will generate an additional \$40-\$50 million annually in external research funding from the investment base of \$10 million. For USM the additional per year operation investment will be \$2 million directed at research faculty and staff, equipment and materials in the biosciences, information sciences, and advanced materials, primarily electronics. In addition, USM has a total of \$7.7 to build and renovate research laboratories (\$2.7 M from a bond referendum approved by voters in 1998 and \$5M from the campus share of the internal bonding capacity).

Exhibit I presents student headcount enrollment for the Portland campus for the fall, 1998 through the fall, 2000 semester with a projection through the fall, 2020 semester. The table shows Weekend College enrollment (classes offered at 4:00 p.m. and after on Friday through Saturday and Sunday) and Regular Enrollment (classes offered from 8 a.m. on Monday through 4:00 p.m. on Friday). About 20% of the students attending the Portland campus are present on Friday, Saturday and Sunday and not during the week days. The introduction of Weekend College was to provide access to adult learners and to relieve pressure on classrooms, laboratories, and parking resources during the week. The enrollment projection takes into account the projected population trends in southern Maine over the next twenty years; projected high school senior population in Maine as a whole, and the high school senior population in southern Maine, specifically; the projected rate of college attendance among high school seniors; and the post-secondary educational participation rate among adults. The projected rates of increase in student headcount enrollment are noted at the bottom of Exhibit I.

The parking and traffic study carried-out in connection with the permitting of the Science Building Addition was carried out on a Tuesday. Exhibit II shows the number of students registered for Portland campus classes at one hour intervals throughout the week. For example, at 8:00 a.m. on Monday morning there were 248 students registered to take classes at the Portland campus while at 2:00 p.m. on Monday, there were 1,116 students registered to be in class at that time. The

**UNIVERSITY OF SOUTHERN MAINE  
ACTUAL & PROJECTED STUDENT HEADCOUNT  
FALL, 1998 to FALL, 2020**

<u>Actual Student Headcount</u>	<u>Weekend College*</u>	<u>Regular Enrollment**</u>	<u>TOTAL</u>
Fall, 1998	940	4,786	5,726
Fall, 1999	1,015	4,500	5,515
Fall, 2000	1,077	4,386	5,463
 <u>Projected Student Headcount</u>			
Fall, 2001	1,109	4,474	5,583
Fall, 2002	1,142	4,563	5,705
Fall, 2003	1,176	4,654	5,830
Fall, 2004	1,211	4,747	5,958
Fall, 2005	1,247	4,842	6,089
Fall, 2006	1,259	4,890	6,149
Fall, 2007	1,272	4,939	6,211
Fall, 2008	1,284	4,989	6,273
Fall, 2009	1,297	5,039	6,336
Fall, 2010	1,310	5,089	6,399
Fall, 2011	1,317	5,113	6,430
Fall, 2012	1,324	5,137	6,461
Fall, 2013	1,331	5,161	6,492
Fall, 2014	1,338	5,185	6,523
Fall, 2015	1,345	5,209	6,554
Fall, 2016	1,352	5,233	6,585
Fall, 2017	1,359	5,258	6,617
Fall, 2018	1,366	5,283	6,649
Fall, 2019	1,373	5,308	6,681
Fall, 2020	1,380	5,333	6,713

Enrollment projection for Fall, 2001 to Fall, 2005 includes a 2% annual increase for the School of Law, a 3% annual increase for Weekend College and a 2% annual increase for Regular Enrollment. Enrollment projection for Fall, 2006 to Fall, 2010 includes a 1% annual increase for all categories and the projection for Fall, 2011 to Fall, 2020 includes a 1/2% annual increase for Weekend College and Regular Enrollment and no increase for School of Law

\* Includes classes offered at 4:00 p.m. and after on Friday and those offered on Saturday and Sunday.

\*\* Includes classes offered from 8 a.m. on Monday through 4:00 p.m. on Friday.

**UNIVERSITY OF SOUTHERN MAINE  
STUDENT ENROLLMENT BY HOUR  
PORTLAND CAMPUS - FALL, 2000**

	<u>Monday</u>	<u>Tuesday</u>	<u>Wednesday</u>	<u>Thursday</u>	<u>Friday</u>	<u>Saturday</u>
8 a.m.	248	11	218	2	41	20
9 a.m.	548	842	534	768	218	436
10 a.m.	595	1,239	568	1,180	287	441
11 a.m.	498	1,270	462	1,194	270	448
Noon	905	1,006	874	914	203	108
1 p.m.	941	1,090	785	976	273	106
2 p.m.	1,116	1,153	839	1,064	273	111
3 p.m.	888	887	689	840	121	73
4 p.m.	1,235	1,182	1,127	1,092	352	36
5 p.m.	1,091	1,083	1,040	1,019	344	0
6 p.m.	1,056	988	1,051	909	345	0
7 p.m.	774	767	798	648	40	0
8 p.m.	736	666	689	527	35	0
9 p.m.	<u>710</u>	<u>617</u>	<u>666</u>	<u>503</u>	<u>16</u>	<u>0</u>
<b>TOTAL</b>	<b><u>11,341</u></b>	<b><u>12,801</u></b>	<b><u>10,340</u></b>	<b><u>11,636</u></b>	<b><u>2,818</u></b>	<b><u>1,779</u></b>

Dave Early cont'd  
page 2  
January 18, 2001

enrollment by hour data clearly indicates that Tuesday is the busiest day of the week and Tuesday attendance exceeds Monday attendance by 12.9%. Tuesday enrollment is 23.8% greater than that on Wednesday; Tuesday has 10.0% more students than on Thursday; and Tuesday has enrollment that is 354.3% greater than on Friday.

The University of Southern Maine provides three shuttle bus services. One such service runs between the Portland and Gorham campuses, with one bus leaving each campus at 30 to 45 minute intervals from about 7:00 a.m. to 10:00 p.m. During the 1999/00 academic year, this shuttle service provided 92,936 riders with transportation between the two campuses. Exhibit III, Page 1 shows time schedule for this shuttle service during the spring, 2001 semester and Exhibit III, Page 2 indicates the service times on Saturday for the Gorham and Portland shuttle, and the Monday through Friday service times for the separate and distinct shuttle service provided between Portland Hall and the Portland campus. During the 1999/00 academic year, there were 23,234 riders who took the Portland Hall/Portland campus shuttle service.

Exhibit III, Page 1 also indicates that a shuttle bus runs continually from the Marginal Way parking lot to the Portland campus, starting at 7:15 a.m. with the last shuttle returning to the Marginal Way lot at 10:00 p.m. For University staff or students wishing to use a University parking lot, there is a parking decal fee of \$15 per semester or \$25 per year. Those who purchase a Marginal Way decal (322 were sold in 1999/00), the cost is \$5 per year. The Marginal Way shuttle service operates Monday through Thursday when classes are in session. When the Marginal Way shuttle service is not being provided, those with Marginal Way parking decals are able to park in campus lots. During the 1999/00 academic year, the Marginal Way shuttle service to the Portland campus had 7,397 riders with 60% of the ridership occurring during the spring semester.

During the 1999/00 fiscal year, the University took a lease for office space at 15 Baxter Boulevard, just across from the University Library and a short walk from campus. This was for a three year period with renewal options and provided office space for several Muskie School of Public Service grant/contract activities. This property included 52 additional parking spaces that were available for use by University of Southern Maine staff and students with University parking decals.

Construction of the Biosciences Research Institute Addition to the Portland Science Building will cause much of the current parking lot between the Science Building and the Sullivan Gym (approximately 104 parking spaces) to be used for construction and staging. Currently, this is a parking lot designated for faculty/staff parking. The lot in front of the Woodbury Campus Center which is currently designated for students only will be opened to faculty and staff during the construction period. Also, during this construction period, Marginal Way and Baxter Boulevard will be available as alternative parking sites for the academic year. In order to have people use Marginal Way or Baxter Boulevard as alternatives, there will be better designation and publicity about where the



SHUTTLE BUS SCHEDULE  
EFFECTIVE JANUARY 16, 2001  
MONDAY THROUGH FRIDAY

There will be one bus per run in each direction. If all seats are filled, standing will be allowed.

LEAVES PORTLAND

7:25 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4:10  
 4:50  
 5:35  
 6:15  
 7:00  
 8:30  
 8:45(via Mall Mon-Fri)  
 10:00 (Mon-Thurs)

LEAVES GORHAM

7:15 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4:10  
 4:50  
 5:35  
 6:15  
 7:00  
 8:15 (via Mall Mon-Fri)  
 9:35 (Mon-Thurs)  
 10:20 (Mon-Thurs)

A shuttle bus will run continually from the Marginal Way lot, in Portland, with stops at the Campus Center, Payson Smith Hall and other on-campus locations as needed, and return to the Marginal Way lot at 7:15a.m., Monday through Thursday. **At 8pm, the shuttle that serves Portland Hall will also serve as the Marginal Way shuttle.** The last bus will return to the Marginal Way lot at 10:00pm, Monday through Thursday. **There will be no Marginal Way bus on Fridays and weekends.** **NOTE: VIP driver shift change occurs at approximately 3:30pm.**

**BUS SCHEDULES ARE SUBJECT TO CHANGE EACH SEMESTER AND DURING FINAL EXAMS.**

PORTLAND HALL SHUTTLE BUS SCHEDULE  
PORTLAND HALL RESIDENTS  
EFFECTIVE JANUARY 16, 2001

LEAVES PORTLAND HALL

7:00a.m.  
 7:45  
 8:15  
 8:45  
 9:15  
 9:45  
 10:15  
 10:45  
 11:45  
 12:15p.m.  
 12:45  
 1:15  
 1:45 (last run on Fri)  
 2:15  
 3:45  
 4:15  
 4:45  
 5:15  
 5:45  
 6:15  
 6:45  
 7:15  
 7:45  
 8:15 \*also serves Marginal Way  
 8:45 \*  
 9:15 \*  
 9:45 \*

LEAVES CAMPUS CENTER

7:30a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 12:00noon  
 12:30p.m.  
 1:00  
 1:30 (last run on Fri)  
 2:00  
 2:30  
 4:00  
 4:30  
 5:00  
 5:30  
 6:00  
 6:30  
 7:00  
 7:30  
 8:00 \*also serves Marginal Way  
 8:30 \*  
 9:00 \*  
 9:30 \*  
 10:05 \*

No Runs on Saturday and Sunday. No Runs to 68 High St.

NOTE: If the shuttle is full, there will not be another shuttle until the next scheduled departure.

SATURDAY MINIBUS SCHEDULE  
EFFECTIVE JANUARY 16, 2001

LEAVES GORHAM

8:30am (no Mall Stop)  
 9:30  
 11:30  
 2:00pm  
 4:00  
 6:00

LEAVES PORTLAND

9:00am (no Mall Stop)  
 10:30  
 12:30pm  
 3:00  
 5:00  
 7:00

NOTE: All Saturday Trips, except 8:30am & 9:00am, Stop at the Maine Mall.

Bus service is provided during the fall and spring semesters when classes are in session. All bus schedules are reviewed by Student Life and the Registrar's Office. Copies are available at the USM Police Department. Please call USM Parking and Transportation at 780-5254, with any questions or concerns regarding transportation services.

Dave Early, cont'd  
page 3  
January 18, 2001

Marginal Way shuttle bus stops are located on the campus and about the location of a shuttle bus stop being instituted at Baxter Boulevard. Also, there will be a more clearly delineated time schedule for the Marginal Way/Baxter Boulevard shuttle and better maintenance of the time schedule to instill confidence in the service on the part of users. Once the construction phase of the Biosciences Addition is completed, the lot between the Science Building and the Sullivan Gymnasium will be returned to use, although there will be a loss of 44 parking spaces due to the Addition.

Over the past two years, the University Community has discussed changes in the scheduling of classes in order to spread the scheduling of classes more evenly over the Monday through Friday period. Through this revamping of the course scheduling, it is hoped that time blocks can be more effectively utilized and will even-out some of the peaks and valleys in student attendance. Exhibit IV, Pages 1 & 2, are the announcement from Provost Joseph S. Wood to USM Faculty and Staff concerning the new class schedule to be implemented effective with the fall, 2001 semester. As indicated in the memo, "implementation should open-up classrooms and also help relieve some parking stress on Tuesdays and Thursdays", the two days that have the highest parking demand.

The use of the three University of Southern Maine shuttle bus services, the invigoration of the Marginal Way parking alternative, the addition of parking spaces at 15 Baxter Boulevard and the implementation of the new class schedule should provide sufficient results to cope with any new parking demand resulting from the addition of the Biosciences Research Institute to the Portland campus. As a purely research oriented facility, the new Addition will not generate any new students for the campus; only research personnel who will be working in the new faculty.


In addition to the items outlined, the University of Southern Maine is and has been an active partner with the Metro to create awareness and use of the Metro as an alternative to bringing a vehicle to the Portland campus. Exhibit V is material shared with the USM Parking and Transportation Review Committee concerning USM's effort with the Metro since March of 2000. From March through August, 2000, the free Metro ride program involved 11,346 riders, and the "50-Cent Ride" program generated 4,517 Metro riders during the September through November period. Through the \$11,000 in financial support being provided to the Metro by USM for the "50-Cent Ride Anywhere, Anytime" program, hopefully, some current drivers to campus will switch and use public transportation.

SGA/c

attachments

December 14, 2000

Office of the Provost  
96 Falmouth Street  
P.O. Box 9300  
Portland, ME 04104-9300  
(207) 780-4485  
TTY (207) 780-5646  
FAX (207) 780-4549

TO: USM Faculty and Staff  
FROM: Joseph S. Wood, Provost   
RE: New Class Schedule for Fall 2001

With full administrative support and endorsement of the Faculty Senate, we will implement a new class schedule effective in the fall semester of 2001. Perhaps the least problematic of various options we have considered over the last few years, the new schedule builds on 90-minute morning time blocks on Monday-Wednesday-Friday, as well as on Tuesday-Thursday. See the attached complete schedule prepared by the Registrar.

Within the new 90-minute morning time blocks for Monday-Wednesday-Friday, scheduling priority will be given to 50-minute classes (3 credit hours) or 75-minute classes (4 credit hours) running over all three days of the week. Friday mornings also provide opportunity for innovative pedagogies that do not correspond to conventional scheduling.

We will adhere strictly to time blocks due to the availability of classroom spaces. The schedule does not affect offering of studio and laboratory classes, except insofar as they may necessarily have to adapt to new starting times.

The new schedule will incorporate afternoon changes established for spring semester 2001. To allow for a 45-minute bus trip between campuses all day long, Portland classes will begin at 8:45 a.m. all days. Implementation should open up classrooms and also help relieve some parking stress on Tuesdays and Thursdays.

As always, Deans are responsible to work with faculty to develop schedules that most effectively allow students to meet degree requirements. Registrar Steve Rand will work closely with Deans to reduce or eliminate scheduling conflicts.

I recognize that we will encounter unforeseen difficulties in implementing this schedule change, and I ask your support and patience as we proceed.

JSW/es

Beginning with the Fall 2001 semester the normal scheduling periods will be changed.

All periods on Monday thru Thursday before 4:00 p.m., will be 75 minutes with a 15 minute break between periods. This schedule will be the same for each day.

Periods after 4:00 p.m. will accommodate either 75 minute periods or 150 minute (2.5 hours) periods.

Friday will normally consist of two 2.5 hour periods.

PORTLAND		GORHAM	
MTWR	F	MTWR	F
8:45 - 10:00	8:45 - 11:15	8:00 - 9:15	8:00 - 10:30
10:15 - 11:30	11:45 - 2:15	9:30 - 10:45	11:00 - 1:30
11:45 - 1:00		11:00 - 12:15	
1:15 - 2:30		12:30 - 1:45	
2:45 - 4:00		2:00 - 3:15	
4:10 - 5:25		4:10 - 5:25	
5:35 - 6:50		5:35 - 6:50	
(4:10 - 6:40)		(4:10 - 6:40)	
7:00 - 9:30		7:00 - 9:30	

Notes

- 1) A 3 credit class offered in a 75 minute period will be offered either M/W or T/R.
- 2) A 3 credit class offered 3 times a week will be offered M W F during 50 minutes of a single period time block.
- 3) Classes that are 4 credit hours can be offered in a single period time block on M W F, with one meeting per week being 50 minutes.



## UNIVERSITY OF SOUTHERN MAINE

Office of Finance and Administration

96 Falmouth Street  
 P.O. Box 9300  
 Portland, Maine 04104-9300  
 (207) 780-4484  
 FAX (207) 780-4549  
 TTY (207) 780-5646

DATE: September 21, 2000

TO: University of Southern Maine Parking/Transportation Committee

FROM: Mr. Samuel G. Andrews, Office of Finance and Administration / C

RE: USM/METRO Cooperative Efforts

At the September 19 meeting of the USM Parking/Transportation Review Committee, a Committee member noted the advertisements that have been appearing in the FREE PRESS about the "50¢ Fares, Any where, Anytime" and the METRO "FAST Service on Forest Avenue". (I have enclosed a copy of advertisements from the 9/5/00 and 9/18/00 issues.) Beginning last spring, USM began cooperating with METRO to create awareness and travel usage of the METRO via a free ride program. As indicated in the attached memo from Philip Chin, Director of Marketing for METRO, by displaying a USM photo ID card, any member of the USM Community (faculty, staff or student) was eligible to ride free on the METRO. Mr. Chin's memo to me outlined the ridership that resulted from the pilot program. We also cooperated with the Greater Portland Council of Governments (COG) and METRO in having an electronic touchscreen-Smart Kiosk installed in the Woodbury Campus Center in Portland with the kiosk providing bus, area transportation and tourism information.

In late summer the METRO began the "FAST Service on Forest Avenue" which provides METRO service every fifteen minutes along Forest Avenue from Pride's Corner in Westbrook to Downtown Portland. This "FAST Service" significantly improved the ability of folks living near or along bus route #2 (as well as those who might use the Park and Ride lot at Prides' Corner) to get to the Portland campus. The University was involved with the group that helped put together the funding proposal for this service. In addition, for the 2000/01 academic year, we are cooperating with METRO on the "50¢ Fares for USM, Anytime, Anywhere" program. In this program, anyone with a USM photo ID or USM ID card can ride the METRO for 50¢, or half the regular fare.

To promote these programs, the University has provided up to \$5,000 to METRO to be used to purchase display advertising in the FREE PRESS during the academic year. USM has also provided \$6,000 toward the cost of printed materials including informational brochures, posters, and other educational outreach materials about the reduced fare and Forest Avenue programs and about the benefits of using public transportation rather than ones own vehicle. Throughout the academic year, METRO will provide us with usage data. Hopefully the efforts from METRO will help make the USM community aware of METRO services that can facilitate its commute to Portland campus. As the academic year progresses, I will keep you informed as to the on-going results.

SGA/c

pc: Ms. Judith Ryan; Mr. Craig Hutchinson;

Ms. Helen Gorgas-Goulding



August 3, 2000

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO Ridership Program

The following is an update on the METRO-USM pilot program:

Background - USM/Free METRO Fare Pilot Program

Since March 2000, we have been working with Craig Hutchinson, Helen Gorgas, and Judy Ryan on creating awareness and trial usage of public transportation via a free METRO ride program. By displaying a USM photo ID card, any member of the USM community rides for free. The METRO has spent over \$1,000 in display advertising in the USM Free Press in promoting the free service. We have received very positive support from Craig and Helen via USM on-campus promotions. In June, our FAST (Forest Avenue Smart Travel) or 15-minute frequency service was introduced. METRO FAST has been a strong complement to the free USM fare program. The accessibility of the METRO bus on Forest Avenue near the campus and higher service frequency have proven to be highly attractive to USM riders.

The pilot program's ridership to date is:

USM METRO trips

March	April	May	June	July	August	Cumulative
1,234	2,024	2,227	1,893	1909		9,287

# FAST Service on Forest Avenue

## 50¢ Fare

*Anytime. Anywhere.*

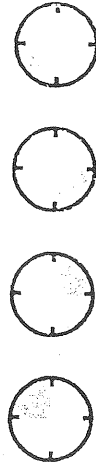
If you attend USM at the Portland Campus or at 68 High Street, we'll make your weekday commute between Forest Avenue and downtown Portland a little easier.

METRO #2 bus will provide service **every 15 minutes** along Forest Avenue, Monday thru Friday, for your school and work commute, shopping trips, short errands, medical appointments, and other intown and crosstown trips.

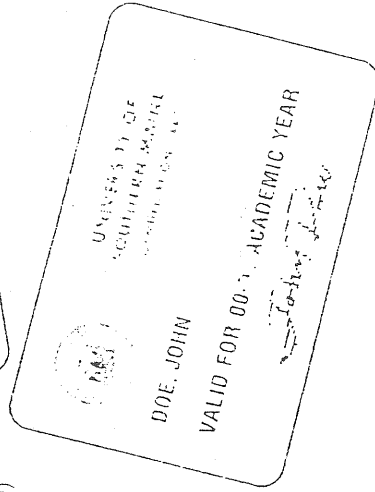
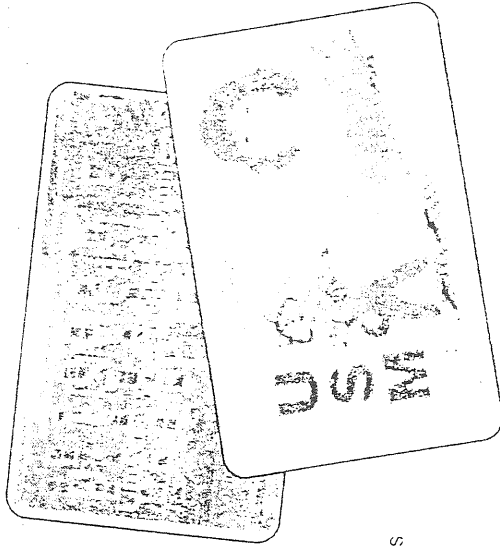
**With a USM Photo ID card and/or a current USM Registration card, the fare is only 50¢!**

Ride in brand new buses. Use the bicycle racks on each bus to combine bike and bus commuting. Visit our exciting, electronic touchscreen SMART Kiosks at the METRO PULSE (Elm Street Garage) and at the Woodbury Campus Center (Bedford Street) for bus, area transportation, and tourism information.

## METRO Bus #2



*Every 15 minutes daily along Forest Avenue!*

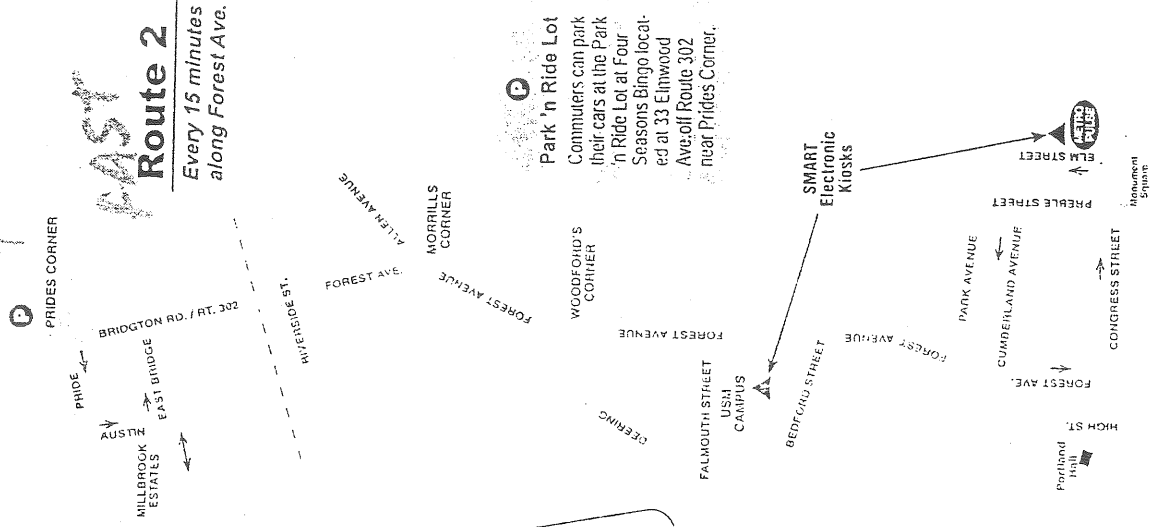


# METRO

*Your City Pass*

**FAST**  
**Route 2**

*Every 15 minutes along Forest Ave.*



**C** **Park 'n Ride Lot**  
 Commuters can park their cars at the Park 'n Ride Lot at Four Seasons Bingo located at 33 Elwood Ave. off Route 302 near Prides Corner.

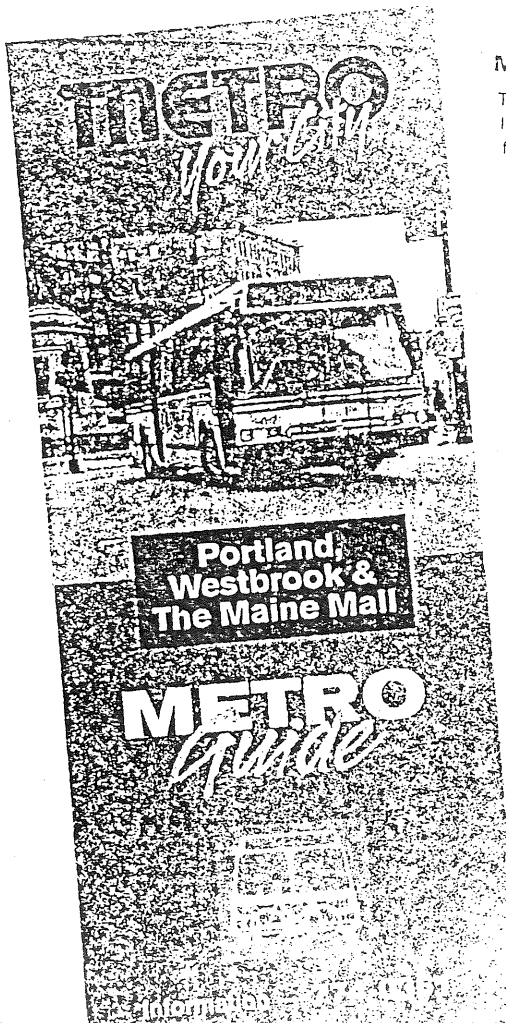


## 50¢ Fares for USM.

Anytime. Anywhere.



- Show a **USM Photo ID** or a current **USM ID registration card** to a METRO driver and ride for 50¢ (regular fare is \$1). No other USM ID will be accepted.
- Get a free USM Photo ID card at Payson Smith Hall in Room 4.
- Ride any route – anytime, anywhere.
- Use the METRO for your commute to school, work, intown Portland, shopping, medical appointments and for other daily trips.
- Use the METRO Smart KIOSK at the Woodbury Campus Center and at the METRO Pulse for bus time schedule and information.



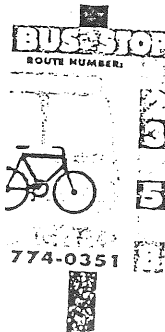
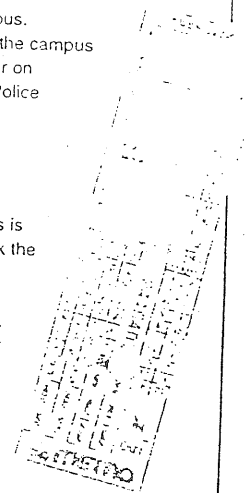
### METRO to USM – #2 or #4 Bus

The #2 bus (Forest Avenue) stops by Back Bay Bicycle on Forest Avenue. It is a short walk up Bedford Street to the campus. The #2 bus stops in front of Pier One for the return intown trip.

The #4 bus (Westbrook/Exit 8) also takes you to the campus. Traveling to the campus, the bus stops on Bedford Street on the campus side. Leaving the campus, the #4 bus stops at the bus shelter on Bedford Street opposite the Campus Center near the USM Police Department building.

### How do I change or transfer to another METRO bus?

You may need to change buses to reach your location. This is called a *transfer*. If you need to change buses, please ask the bus driver for a *transfer* when you first get on the bus and pay for your ride. When you change your bus, please give the transfer to the second bus driver. The *transfer* is *free*. You do not need to pay when you change buses. On your ride home, you will need to pay another fare. (See picture of transfer at the right.) You can change buses at various METRO bus stops along Congress Street which runs through downtown Portland. You can also change buses at the METRO PULSE located at the Elm Street Garage.



### Where do I wait for the bus?

Please wait for a METRO bus where you see a *bus stop sign* (see left). There are also glass structures that are METRO bus stops called *bus shelters*.

### Are there special bus fare plans?

- \$30 Monthly Pass: unlimited rides for month purchased
- \$9 TenRide Ticket: 10 rides with no time limit

For information — 774-0351



December 8, 2000

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program

For your posting the following is ridership for the program:

March	1,234
April	2,024
May	2,227
June	1,893
July	1,909
August	2,059
Cumulative Free Rides:	11,346
September	1,313
October	1,616
November	1,588
Cumulative 50-Cent Rides:	4,517

The baseline of ridership provides optimism that we can continue to build interest in public transit within the USM community. In the months ahead, we will focus on USM faculty and staff, and on the Gorham campus. Thank you for your continued support and I will provide future updates.

**From:** Larry Ash  
**To:** Alex Jaegerman , Sarah Hopkins  
**Date:** Fri, Feb 9, 2001 7:27 AM  
**Subject:** USM expansion

Sarah:

As regards the traffic report prepared by DeLuca-Hoffman Associates, I have the following comments:

1. The report states that the Forest Ave/Bedford St intersection operates at an acceptable Level of Service(LOS). I disagree. Lane approaches for the eastbound left-turn, northbound left-turn and the westbound left-turn operate at LOS E, E and F respectfully and therefore the intersection LOS as a whole cannot be acceptable. The latest Highway Capacity Manual (HCM) defines delay and LOS in terms of "control delay" rather than "stopped delay". Thus, the latest HCM defines the "D" range LOS to be 35 to 55 seconds instead of the previous 25 to 40 seconds. It is my opinion that any lane approach with a **stopped delay** in excess of 40 seconds operates at an **unacceptable** LOS and therefore the **intersection** requires mitigation. AM LOS analysis also needs to be provided.
2. The intersections of Forest Ave/Dartmouth and Brighton/Falmouth/Deering need to be included and AM analysis also provided.
3. Assuming that Bedford Street would be closed some assumptions and a comprehensive analysis needs to be made **and** justified on vehicular dispersions or assignment at each of the above referenced intersections and a capacity/LOS analysis reiterated. Unacceptable Levels of Service need to be addressed and mitigation proposals/alternatives reviewed.
4. The proposed exit/entrance for the parking garage on Chamberlain Street is unacceptable. Bill Bray, Public Works Director had made this clear very early in the discussions of the expansion.
5. If the Marginal Way parking lot is lost due to Amtrak coming to Portland, how will the parking be affected and what adjustments would USM have to make?
6. How will parking be handled during the construction phases, e.g. the Bioscience Bldg and the parking ramp?
7. If the Bioscience Bldg is built before the parking ramp where will these users park?
8. I think more information needs to be provided for the Community Center and auditorium, when will these be used at their peaks, by how many, and where will users be parking, etc.???

**CC:** Penny Littell , William Bray



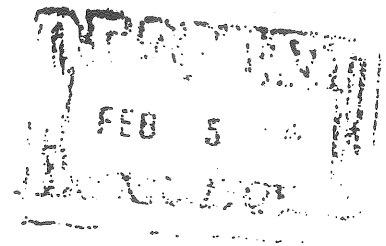
# METRO

February 1, 2001

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program



For your posting the following is ridership for the program:

Year 2000

March 1,234

April 2,024

May 2,227

June 1,893

July 1,909

August 2,059

**Cumulative Free Rides: 11,346**

September 1,313

October 1,616

November 1,588

December 1,504

January '01 1,734

**Cumulative 50-Cent Rides: 7,755**

**Total All Rides (March-January): 19,101**

In the upcoming weeks, we will be actively working with Rodney Mondor to introduce students on the Gorham campus to the METRO, as well as, Chris O'Connor at Portland Hall with Portland campus students..

GREATER PORTLAND TRANSIT DISTRICT

114 Valley Street • PO Box 1097 • Portland, Maine 04104 • (207)774-0351 • FAX (207)774-6241



# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4200 FAX (207) 780-4860  
TTY (207) 780-5646

January 25, 2001

Dear USM Neighbor:

We'd like to invite you to a neighborhood meeting to find out more about building plans on the Portland campus of the University of Southern Maine.

The meeting will be held at 7 p.m., Friday, February 9, in the cafeteria of the Woodbury Campus Center, Bedford St., Portland. There should be adequate parking in the Bedford St. lots, located a short distance from the intersection of Bedford and Forest Ave.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP to 780-4200.

USM officials will be outlining the plans for the Biosciences Research Institute. This facility, which will be financed with state research and development funds, is planned as an addition to the Science Building on Falmouth St.

We also want to share with you our master plan for the development of the Portland campus. For the most part, the plan represents a 20 to 30-year vision as to how our Portland campus might develop, depending on state funds, private gifts, enrollment trends and other factors. There are only three projects under consideration during the next five years: the Biosciences Research Institute; renovation of the former Steego Building on Bedford St. to house our Muskie School; and construction of a Community Education Center/parking garage on the current Bedford St. parking lot.

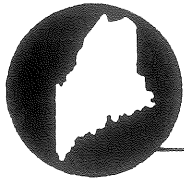
USM's mission is to enhance the region's quality of life. Our success in doing that depends on thoughtful partnerships with the community. We value your input and hope to see you on the 9th.

Sincerely,

Bob Caswell  
Executive Director, Media & Community Relations

Dave Early  
Executive Director, Facilities Management

RSC/jao



# UNIVERSITY OF SOUTHERN MAINE

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96 Falmouth Street  
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## REVIEW OF USM NEIGHBORHOOD MEETING

The following is an outline of the discussions from the neighborhood meeting, held at 7 p.m., Friday, February 9, in the cafeteria at the University of Southern Maine's Woodbury Campus Center, Portland.

Thanks to the cooperation of the city's Planning Department, we were able to mail 1,219 letters of invitation to residents in an area roughly bounded by Back Cove, the Parkside neighborhood, St. John St. and through the Oakdale section to Douglass St.

Thirty-five people attended, and shared with us concerns ranging from parking to the type of research to be conducted in the proposed Biosciences Institute.

I've grouped those concerns under the following headings, and under each heading have summarized issues that were addressed.

### **PARKING/TRAFFIC:**

Much of the 3+ hour meeting was devoted to this issue. In the short term, many residents felt that our plan to compensate for loss of parking during construction of the Biosciences Institute was flawed in that most people would not use the Marginal Way lot. We responded that an aggressive campaign to inform people of the Marginal Way lot, coupled with a beefed up shuttle bus service, would serve us and the neighbors well during the construction period.

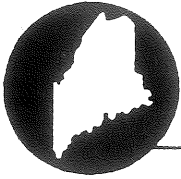
It was further explained that the University is implementing a revised class schedule for the fall, 2001. The schedule is designed to help distribute uniformly space demands throughout the week as opposed to the current schedule that causes peak demands on Tuesdays and Thursdays.

Several questioned why we are planning to construct a research building before a parking garage. We explained that dedicated funding that must be used or be forfeited is in place for the research facility. We also noted that the facility will be dedicated exclusively to research and will not include classrooms and other high-traffic areas. Guests questioned the number of researchers that would be housed in the building.

Neighbors also voiced frustration at students' cars that are taking up on-street parking in and around their neighborhoods. A USM/city collaborative effort to address the problem ought to be initiated, they said. As part of this discussion, people asked why the long-range campus master plan did not contain any provisions for developing on-campus student housing. We explained that our plans call for continuing to emphasize Gorham as a traditional campus environment. A new 224-bed residence hall will open in Gorham this summer. Portland Hall on Congress St. seems to satisfy student demand for Portland-based housing. We have had informal discussions about the possibility of a cooperative agreement with private developers to provide housing but there are no plans to do so.

Guests also expressed concerns over the lack of studies detailing how the closing of Bedford St., for example, would affect traffic flows in the neighborhoods. We explained that the Bedford St. proposal is one of several long-range visions included as part of the campus master plan. As such, it is so far in the future that it does not yet warrant detailed studies.

//more on back



# UNIVERSITY OF SOUTHERN MAINE

Office of Finance and Administration

DATE: January 18, 2001

TO: Mr. David Early, Executive Director, Facilities Management

FROM: Samuel G. Andrews, Office of Finance and Administration

RE: Biosciences Research Institute Addition

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4484  
FAX (207) 780-4549  
TTY (207) 780-5646

As I understand it, the City of Portland Planning Board and Planning Staff requested some additional information relative to future enrollments at the Portland campus as well as information on USM's parking mitigation efforts. In regard to the Biosciences Research Institute Addition to the Portland Science Building, it should be noted that this is a research facility only and will not have any impact at all on student enrollment.

The Maine Legislature has made two targeted additions to the University of Maine System base budget designed to substantially increase research and development (R&D) activity at the University of Maine and the University of Southern Maine, in particular to strengthen the capacity of both Institutions to compete for Federal grants and contracts. The annual base budget operating increase is \$10 million plus \$2.5 million to cover debt service payments on a \$25 million internal bond issue for a period of 15 years. The general goal is that together the Universities will generate an additional \$40-\$50 million annually in external research funding from the investment base of \$10 million. For USM the additional per year operation investment will be \$2 million directed at research faculty and staff, equipment and materials in the biosciences, information sciences, and advanced materials, primarily electronics. In addition, USM has a total of \$7.7 to build and renovate research laboratories (\$2.7 M from a bond referendum approved by voters in 1998 and \$5M from the campus share of the internal bonding capacity).

Exhibit I presents student headcount enrollment for the Portland campus for the fall, 1998 through the fall, 2000 semester with a projection through the fall, 2020 semester. The table shows Weekend College enrollment (classes offered at 4:00 p.m. and after on Friday through Saturday and Sunday) and Regular Enrollment (classes offered from 8 a.m. on Monday through 4:00 p.m. on Friday). About 20% of the students attending the Portland campus are present on Friday, Saturday and Sunday and not during the week days. The introduction of Weekend College was to provide access to adult learners and to relieve pressure on classrooms, laboratories, and parking resources during the week. The enrollment projection takes into account the projected population trends in southern Maine over the next twenty years; projected high school senior population in Maine as a whole, and the high school senior population in southern Maine, specifically; the projected rate of college attendance among high school seniors; and the post-secondary educational participation rate among adults. The projected rates of increase in student headcount enrollment are noted at the bottom of Exhibit I.

The parking and traffic study carried-out in connection with the permitting of the Science Building Addition was carried out on a Tuesday. Exhibit II shows the number of students registered for Portland campus classes at one hour intervals throughout the week. For example, at 8:00 a.m. on Monday morning there were 248 students registered to take classes at the Portland campus while at 2:00 p.m. on Monday, there were 1,116 students registered to be in class at that time. The

**UNIVERSITY OF SOUTHERN MAINE  
ACTUAL & PROJECTED STUDENT HEADCOUNT  
FALL, 1998 to FALL, 2020**

<u>Actual Student Headcount</u>	<u>Weekend College*</u>	<u>Regular Enrollment**</u>	<u>TOTAL</u>
Fall, 1998	940	4,786	5,726
Fall, 1999	1,015	4,500	5,515
Fall, 2000	1,077	4,386	5,463
 <u>Projected Student Headcount</u>			
Fall, 2001	1,109	4,474	5,583
Fall, 2002	1,142	4,563	5,705
Fall, 2003	1,176	4,654	5,830
Fall, 2004	1,211	4,747	5,958
Fall, 2005	1,247	4,842	6,089
Fall, 2006	1,259	4,890	6,149
Fall, 2007	1,272	4,939	6,211
Fall, 2008	1,284	4,989	6,273
Fall, 2009	1,297	5,039	6,336
Fall, 2010	1,310	5,089	6,399
Fall, 2011	1,317	5,113	6,430
Fall, 2012	1,324	5,137	6,461
Fall, 2013	1,331	5,161	6,492
Fall, 2014	1,338	5,185	6,523
Fall, 2015	1,345	5,209	6,554
Fall, 2016	1,352	5,233	6,585
Fall, 2017	1,359	5,258	6,617
Fall, 2018	1,366	5,283	6,649
Fall, 2019	1,373	5,308	6,681
Fall, 2020	1,380	5,333	6,713

Enrollment projection for Fall, 2001 to Fall, 2005 includes a 2% annual increase for the School of Law, a 3% annual increase for Weekend College and a 2% annual increase for Regular Enrollment. Enrollment projection for Fall, 2006 to Fall, 2010 includes a 1% annual increase for all categories and the projection for Fall, 2011 to Fall, 2020 includes a 1/2% annual increase for Weekend College and Regular Enrollment and no increase for School of Law

\* Includes classes offered at 4:00 p.m. and after on Friday and those offered on Saturday and Sunday.

\*\* Includes classes offered from 8 a.m. on Monday through 4:00 p.m. on Friday.



**UNIVERSITY OF SOUTHERN MAINE  
STUDENT ENROLLMENT BY HOUR  
PORTLAND CAMPUS - FALL, 2000**

	<u>Monday</u>	<u>Tuesday</u>	<u>Wednesday</u>	<u>Thursday</u>	<u>Friday</u>	<u>Saturday</u>
8 a.m.	248	11	218	2	41	20
9 a.m.	548	842	534	768	218	436
10 a.m.	595	1,239	568	1,180	287	441
11 a.m.	498	1,270	462	1,194	270	448
Noon	905	1,006	874	914	203	108
1 p.m.	941	1,090	785	976	273	106
2 p.m.	1,116	1,153	839	1,064	273	111
3 p.m.	888	887	689	840	121	73
4 p.m.	1,235	1,182	1,127	1,092	352	36
5 p.m.	1,091	1,083	1,040	1,019	344	0
6 p.m.	1,056	988	1,051	909	345	0
7 p.m.	774	767	798	648	40	0
8 p.m.	736	666	689	527	35	0
9 p.m.	<u>710</u>	<u>617</u>	<u>666</u>	<u>503</u>	<u>16</u>	<u>0</u>
<b>TOTAL</b>	<b><u>11,341</u></b>	<b><u>12,801</u></b>	<b><u>10,340</u></b>	<b><u>11,636</u></b>	<b><u>2,818</u></b>	<b><u>1,779</u></b>

Dave Early cont'd  
page 2  
January 18, 2001

enrollment by hour data clearly indicates that Tuesday is the busiest day of the week and Tuesday attendance exceeds Monday attendance by 12.9%. Tuesday enrollment is 23.8% greater than that on Wednesday; Tuesday has 10.0% more students than on Thursday; and Tuesday has enrollment that is 354.3% greater than on Friday.

The University of Southern Maine provides three shuttle bus services. One such service runs between the Portland and Gorham campuses, with one bus leaving each campus at 30 to 45 minute intervals from about 7:00 a.m. to 10:00 p.m. During the 1999/00 academic year, this shuttle service provided 92,936 riders with transportation between the two campuses. Exhibit III, Page 1 shows time schedule for this shuttle service during the spring, 2001 semester and Exhibit III, Page 2 indicates the service times on Saturday for the Gorham and Portland shuttle, and the Monday through Friday service times for the separate and distinct shuttle service provided between Portland Hall and the Portland campus. During the 1999/00 academic year, there were 23,234 riders who took the Portland Hall/Portland campus shuttle service.

Exhibit III, Page 1 also indicates that a shuttle bus runs continually from the Marginal Way parking lot to the Portland campus, starting at 7:15 a.m. with the last shuttle returning to the Marginal Way lot at 10:00 p.m. For University staff or students wishing to use a University parking lot, there is a parking decal fee of \$15 per semester or \$25 per year. Those who purchase a Marginal Way decal (322 were sold in 1999/00), the cost is \$5 per year. The Marginal Way shuttle service operates Monday through Thursday when classes are in session. When the Marginal Way shuttle service is not being provided, those with Marginal Way parking decals are able to park in campus lots. During the 1999/00 academic year, the Marginal Way shuttle service to the Portland campus had 7,397 riders with 60% of the ridership occurring during the spring semester.

During the 1999/00 fiscal year, the University took a lease for office space at 15 Baxter Boulevard, just across from the University Library and a short walk from campus. This was for a three year period with renewal options and provided office space for several Muskie School of Public Service grant/contract activities. This property included 52 additional parking spaces that were available for use by University of Southern Maine staff and students with University parking decals.

Construction of the Biosciences Research Institute Addition to the Portland Science Building will cause much of the current parking lot between the Science Building and the Sullivan Gym (approximately 104 parking spaces) to be used for construction and staging. Currently, this is a parking lot designated for faculty/staff parking. The lot in front of the Woodbury Campus Center which is currently designated for students only will be opened to faculty and staff during the construction period. Also, during this construction period, Marginal Way and Baxter Boulevard will be available as alternative parking sites for the academic year. In order to have people use Marginal Way or Baxter Boulevard as alternatives, there will be better designation and publicity about where the

SHUTTLE BUS SCHEDULE  
EFFECTIVE JANUARY 16, 2001  
MONDAY THROUGH FRIDAY

There will be one bus per run in each direction. If all seats are filled, standing will be allowed.

LEAVES PORTLAND

7:25 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4:10  
 4:50  
 5:35  
 6:15  
 7:00  
 8:30  
 8:45(via Mall Mon-Fri)  
 10:00 (Mon-Thurs)

LEAVES GORHAM

7:15 a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 11:45  
 12:30 p.m.  
 1:15  
 2:00  
 2:45  
 3:25  
 4:10  
 4:50  
 5:35  
 6:15  
 7:00  
 8:15 (via Mall Mon-Fri)  
 9:35 (Mon-Thurs)  
 10:20 (Mon-Thurs)

A shuttle bus will run continually from the Marginal Way lot, in Portland, with stops at the Campus Center, Payson Smith Hall and other on-campus locations as needed, and return to the Marginal Way lot at 7:15a.m., Monday through Thursday. At 8pm, the shuttle that serves Portland Hall will also serve as the Marginal Way shuttle. The last bus will return to the Marginal Way lot at 10:00pm, Monday through Thursday. There will be no Marginal Way bus on Fridays and weekends. NOTE: VIP driver shift change occurs at approximately 3:30pm.

**BUS SCHEDULES ARE SUBJECT TO CHANGE EACH SEMESTER AND DURING FINAL EXAMS.**

PORTLAND HALL SHUTTLE BUS SCHEDULE  
PORTLAND HALL RESIDENTS  
EFFECTIVE JANUARY 16, 2001

LEAVES PORTLAND HALL

7:00a.m.  
 7:45  
 8:15  
 8:45  
 9:15  
 9:45  
 10:15  
 10:45  
 11:45  
 12:15p.m.  
 12:45  
 1:15  
 1:45 (last run on Fri)  
 2:15  
 3:45  
 4:15  
 4:45  
 5:15  
 5:45  
 6:15  
 6:45  
 7:15  
 7:45  
 8:15 \*also serves Marginal Way  
 8:45 \*  
 9:15 \*  
 9:45 \*

LEAVES CAMPUS CENTER

7:30a.m.  
 8:00  
 8:30  
 9:00  
 9:30  
 10:00  
 10:30  
 11:00  
 12:00noon  
 12:30p.m.  
 1:00  
 1:30 (last run on Fri)  
 2:00  
 2:30  
 4:00  
 4:30  
 5:00  
 5:30  
 6:00  
 6:30  
 7:00  
 7:30  
 8:00 \*also serves Marginal Way  
 8:30 \*  
 9:00 \*  
 9:30 \*  
 10:05 \*

No Runs on Saturday and Sunday. No Runs to 68 High St.

NOTE: If the shuttle is full, there will not be another shuttle until the next scheduled departure.

SATURDAY MINIBUS SCHEDULE  
EFFECTIVE JANUARY 16, 2001

LEAVES GORHAM

8:30am (no Mall Stop)  
 9:30  
 11:30  
 2:00pm  
 4:00  
 6:00

LEAVES PORTLAND

9:00am (no Mall Stop)  
 10:30  
 12:30pm  
 3:00  
 5:00  
 7:00

NOTE: All Saturday Trips, except 8:30am & 9:00am, Stop at the Maine Mall.

Bus service is provided during the fall and spring semesters when classes are in session. All bus schedules are reviewed by Student Life and the Registrar's Office. Copies are available at the USM Police Department. Please call USM Parking and Transportation at 780-5254, with any questions or concerns regarding transportation services.

Dave Early, cont'd

page 3

January 18, 2001

Marginal Way shuttle bus stops are located on the campus and about the location of a shuttle bus stop being instituted at Baxter Boulevard. Also, there will be a more clearly delineated time schedule for the Marginal Way/Baxter Boulevard shuttle and better maintenance of the time schedule to instill confidence in the service on the part of users. Once the construction phase of the Biosciences Addition is completed, the lot between the Science Building and the Sullivan Gymnasium will be returned to use, although there will be a loss of 44 parking spaces due to the Addition.

Over the past two years, the University Community has discussed changes in the scheduling of classes in order to spread the scheduling of classes more evenly over the Monday through Friday period. Through this revamping of the course scheduling, it is hoped that time blocks can be more effectively utilized and will even-out some of the peaks and valleys in student attendance. Exhibit IV, Pages 1 & 2, are the announcement from Provost Joseph S. Wood to USM Faculty and Staff concerning the new class schedule to be implemented effective with the fall, 2001 semester. As indicated in the memo, "implementation should open-up classrooms and also help relieve some parking stress on Tuesdays and Thursdays", the two days that have the highest parking demand.

The use of the three University of Southern Maine shuttle bus services, the invigoration of the Marginal Way parking alternative, the addition of parking spaces at 15 Baxter Boulevard and the implementation of the new class schedule should provide sufficient results to cope with any new parking demand resulting from the addition of the Biosciences Research Institute to the Portland campus. As a purely research oriented facility, the new Addition will not generate any new students for the campus; only research personnel who will be working in the new faculty.

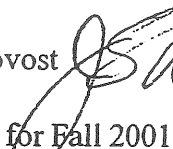
In addition to the items outlined, the University of Southern Maine is and has been an active partner with the Metro to create awareness and use of the Metro as an alternative to bringing a vehicle to the Portland campus. Exhibit V is material shared with the USM Parking and Transportation Review Committee concerning USM's effort with the Metro since March of 2000. From March through August, 2000, the free Metro ride program involved 11,346 riders, and the "50-Cent Ride" program generated 4,517 Metro riders during the September through November period. Through the \$11,000 in financial support being provided to the Metro by USM for the "50-Cent Ride Anywhere, Anytime" program, hopefully, some current drivers to campus will switch and use public transportation.

SGA/c

attachments

December 14, 2000

Office of the Provost  
96 Falmouth Street  
P.O. Box 9300  
Portland, ME 04104-9300  
(207) 780-4485  
TTY (207) 780-5646  
FAX (207) 780-4549

TO: USM Faculty and Staff  
FROM: Joseph S. Wood, Provost   
RE: New Class Schedule for Fall 2001

With full administrative support and endorsement of the Faculty Senate, we will implement a new class schedule effective in the fall semester of 2001. Perhaps the least problematic of various options we have considered over the last few years, the new schedule builds on 90-minute morning time blocks on Monday-Wednesday-Friday, as well as on Tuesday-Thursday. See the attached complete schedule prepared by the Registrar.

Within the new 90-minute morning time blocks for Monday-Wednesday-Friday, scheduling priority will be given to 50-minute classes (3 credit hours) or 75-minute classes (4 credit hours) running over all three days of the week. Friday mornings also provide opportunity for innovative pedagogies that do not correspond to conventional scheduling.

We will adhere strictly to time blocks due to the availability of classroom spaces. The schedule does not affect offering of studio and laboratory classes, except insofar as they may necessarily have to adapt to new starting times.

The new schedule will incorporate afternoon changes established for spring semester 2001. To allow for a 45-minute bus trip between campuses all day long, Portland classes will begin at 8:45 a.m. all days. Implementation should open up classrooms and also help relieve some parking stress on Tuesdays and Thursdays.

As always, Deans are responsible to work with faculty to develop schedules that most effectively allow students to meet degree requirements. Registrar Steve Rand will work closely with Deans to reduce or eliminate scheduling conflicts.

I recognize that we will encounter unforeseen difficulties in implementing this schedule change, and I ask your support and patience as we proceed.

JSW/es

Beginning with the Fall 2001 semester the normal scheduling periods will be changed.

All periods on Monday thru Thursday before 4:00 p.m., will be 75 minutes with a 15 minute break between periods. This schedule will be the same for each day.

Periods after 4:00 p.m. will accommodate either 75 minute periods or 150 minute (2.5 hours) periods.

Friday will normally consist of two 2.5 hour periods.

PORTLAND		GORHAM	
MTWR	F	MTWR	F
8:45 - 10:00	8:45 - 11:15	8:00 - 9:15	8:00 - 10:30
10:15 - 11:30	11:45 - 2:15	9:30 - 10:45	11:00 - 1:30
11:45 - 1:00		11:00 - 12:15	
1:15 - 2:30		12:30 - 1:45	
2:45 - 4:00		2:00 - 3:15	
4:10 - 5:25		4:10 - 5:25	
5:35 - 6:50		5:35 - 6:50	
(4:10 - 6:40)		(4:10 - 6:40)	
7:00 - 9:30		7:00 - 9:30	

Notes

- 1) A 3 credit class offered in a 75 minute period will be offered either M/W or T/R.
- 2) A 3 credit class offered 3 times a week will be offered M W F during 50 minutes of a single period time block.
- 3) Classes that are 4 credit hours can be offered in a single period time block on M W F, with one meeting per week being 50 minutes.



## UNIVERSITY OF SOUTHERN MAINE

Office of Finance and Administration

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4484  
FAX (207) 780-4549  
TTY (207) 780-5646

DATE: September 21, 2000

TO: University of Southern Maine Parking/Transportation Committee

FROM: Mr. Samuel G. Andrews, Office of Finance and Administration / C

RE: USM/METRO Cooperative Efforts

At the September 19 meeting of the USM Parking/Transportation Review Committee, a Committee member noted the advertisements that have been appearing in the FREE PRESS about the "50¢ Fares, Any where, Anytime" and the METRO "FAST Service on Forest Avenue". (I have enclosed a copy of advertisements from the 9/5/00 and 9/18/00 issues.) Beginning last spring, USM began cooperating with METRO to create awareness and travel usage of the METRO via a free ride program. As indicated in the attached memo from Philip Chin, Director of Marketing for METRO, by displaying a USM photo ID card, any member of the USM Community (faculty, staff or student) was eligible to ride free on the METRO. Mr. Chin's memo to me outlined the ridership that resulted from the pilot program. We also cooperated with the Greater Portland Council of Governments (COG) and METRO in having an electronic touchscreen-Smart Kiosk installed in the Woodbury Campus Center in Portland with the kiosk providing bus, area transportation and tourism information.

In late summer the METRO began the "FAST Service on Forest Avenue" which provides METRO service every fifteen minutes along Forest Avenue from Pride's Corner in Westbrook to Downtown Portland. This "FAST Service" significantly improved the ability of folks living near or along bus route #2 (as well as those who might use the Park and Ride lot at Prides' Corner) to get to the Portland campus. The University was involved with the group that helped put together the funding proposal for this service. In addition, for the 2000/01 academic year, we are cooperating with METRO on the "50¢ Fares for USM, Anytime, Anywhere" program. In this program, anyone with a USM photo ID or USM ID card can ride the METRO for 50¢, or half the regular fare.

To promote these programs, the University has provided up to \$5,000 to METRO to be used to purchase display advertising in the FREE PRESS during the academic year. USM has also provided \$6,000 toward the cost of printed materials including informational brochures, posters, and other educational outreach materials about the reduced fare and Forest Avenue programs and about the benefits of using public transportation rather than ones own vehicle. Throughout the academic year, METRO will provide us with usage data. Hopefully the efforts from METRO will help make the USM community aware of METRO services that can facilitate its commute to Portland campus. As the academic year progresses, I will keep you informed as to the on-going results.

SGA/c

pc: Ms. Judith Ryan; Mr. Craig Hutchinson;  
Ms. Helen Gorgas-Goulding





August 3, 2000

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO Ridership Program

The following is an update on the METRO-USM pilot program:

Background - USM/Free METRO Fare Pilot Program

Since March 2000, we have been working with Craig Hutchinson, Helen Gorgas, and Judy Ryan on creating awareness and trial usage of public transportation via a free METRO ride program. By displaying a USM photo ID card, any member of the USM community rides for free. The METRO has spent over \$1,000 in display advertising in the USM Free Press in promoting the free service. We have received very positive support from Craig and Helen via USM on-campus promotions. In June, our FAST (Forest Avenue Smart Travel) or 15-minute frequency service was introduced. METRO FAST has been a strong complement to the free USM fare program. The accessibility of the METRO bus on Forest Avenue near the campus and higher service frequency have proven to be highly attractive to USM riders.

The pilot program's ridership to date is:

USM METRO trips

March	April	May	June	July	August	Cumulative
1,234	2,024	2,227	1,893	1909		9,287

# METRO *Your City Buses*

## FAST Service on Forest Avenue

**50¢ Fare**

*Anytime. Anywhere.*

If you attend USM at the Portland Campus or at 68 High Street, we'll make your weekday commute between Forest Avenue and downtown Portland a little easier.

METRO #2 bus will provide service **every 15 minutes** along Forest Avenue, Monday thru Friday, for your school and work commute, shopping trips, short errands, medical appointments, and other intown and crosstown trips.

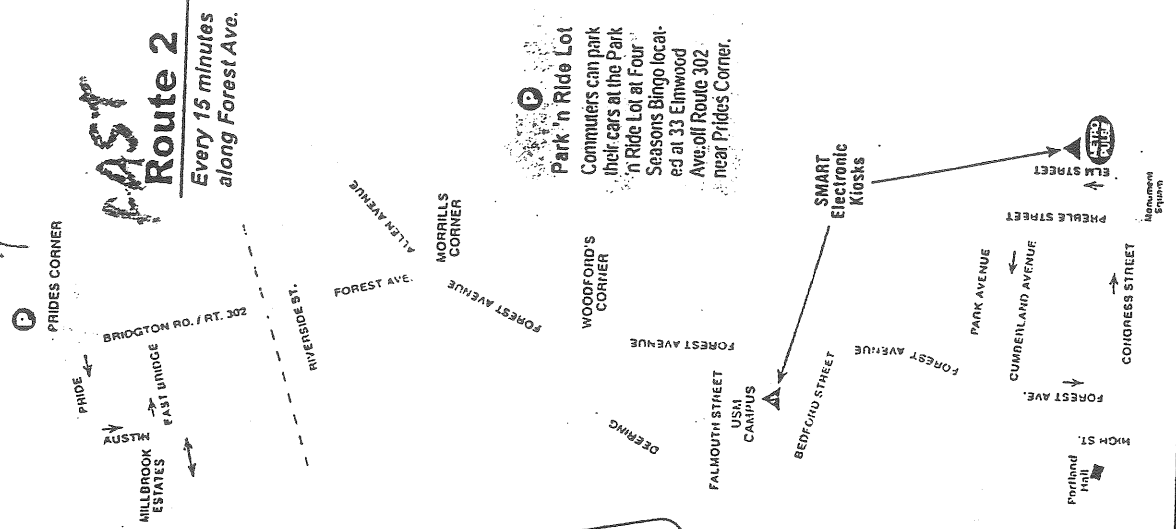
**With a USM Photo ID card and/or a current USM Registration card, the fare is only 50¢!**

Ride in brand new buses. Use the bicycle racks on each bus to combine bike and bus commuting. Visit our exciting, electronic touchscreen SMART Kiosks at the METRO PULSE (Elm Street Garage) and at the Woodbury Campus Center (Bedford Street) for bus, area transportation, and tourism information.

## METRO Bus #2

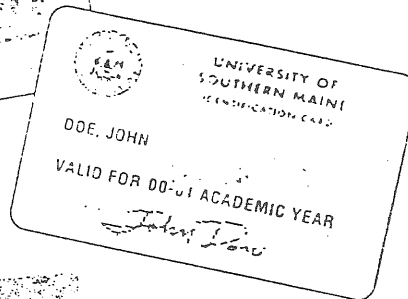
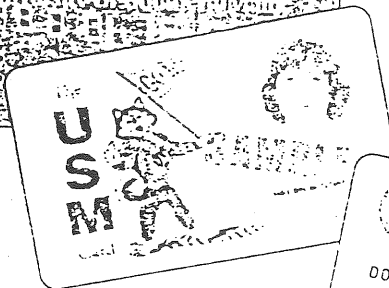
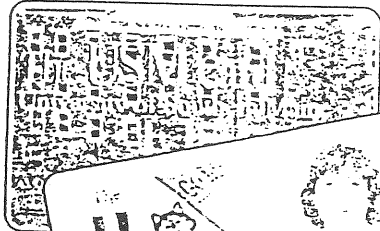


*Every 15 minutes daily along Forest Avenue!*

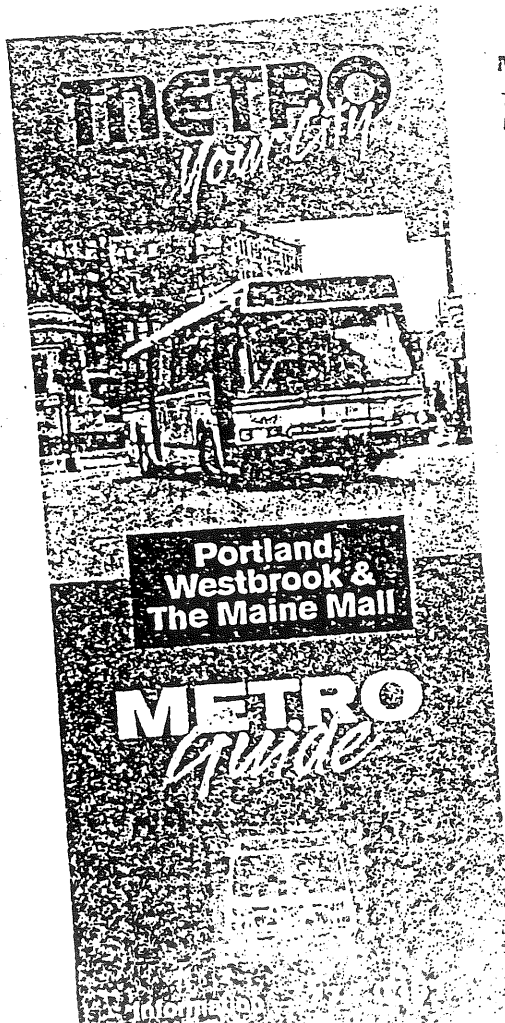


## 50¢ Fares for USM.

Anytime. Anywhere.



- Show a **USM Photo ID** or a current **USM ID registration card** to a METRO driver and ride for 50¢ (regular fare is \$1). No other USM ID will be accepted.
- Get a free USM Photo ID card at Payson Smith Hall in Room 4.
- Ride any route – anytime, anywhere.
- Use the METRO for your commute to school, work, intown Portland, shopping, medical appointments and for other daily trips.
- Use the METRO Smart KIOSK at the Woodbury Campus Center and at the METRO Pulse for bus time schedule and information.



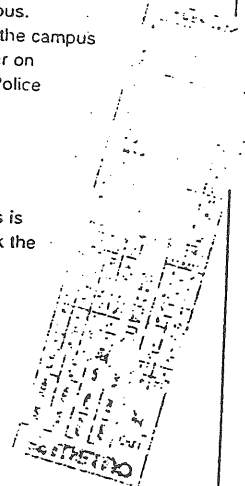
### METRO to USM – #2 or #4 Bus

The #2 bus (Forest Avenue) stops by Back Bay Bicycle on Forest Avenue. It is a short walk up Bedford Street to the campus. The #2 bus stops in front of Pier One for the return intown trip.

The #4 bus (Westbrook/Exit 8) also takes you to the campus. Traveling to the campus, the bus stops on Bedford Street on the campus side. Leaving the campus, the #4 bus stops at the bus shelter on Bedford Street opposite the Campus Center near the USM Police Department building.

### How do I change or transfer to another METRO bus?

You may need to change buses to reach your location. This is called a *transfer*. If you need to change buses, please ask the bus driver for a *transfer* when you first get on the bus and pay for your ride. When you change your bus, please give the transfer to the second bus driver. The *transfer is free*. You do not need to pay when you change buses. On your ride home, you will need to pay another fare. (See *picture of transfer at the right.*) You can change buses at various METRO bus stops along Congress Street which runs through downtown Portland. You can also change buses at the METRO PULSE located at the Elm Street Garage.

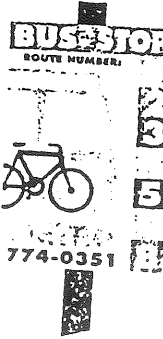


### Where do I wait for the bus?

Please wait for a METRO bus where you see a *bus stop sign* (see left). There are also glass structures that are METRO bus stops called *bus shelters*.

### Are there special bus fare plans?

- \$30 Monthly Pass: unlimited rides for month purchased
- \$9 TenRide Ticket: 10 rides with no time limit



For information — 774-0351

## AMENDMENT OF LEASE AGREEMENT BETWEEN THE CITY OF PORTLAND AND THE UNIVERSITY OF MAINE SYSTEM FOR MARGINAL WAY PARKING AREA

Whereas, the City of Portland, a body politic and corporate having a place of business in the county of Cumberland and State of Maine (hereinafter "CITY") and the University of Maine System, a body politic and corporate, having a place of business in said Portland (hereinafter "SYSTEM"), entered into a January 16, 1992 Lease Agreement (exhibit A) whereby the CITY leased to the UNIVERSITY a certain tract or parcel of land, adjacent to Marginal Way is said City's, and known as the "Marginal Way Lot" for use as a parking lot with the SYSTEM having the exclusive right to use and occupy Section A (approximately 290 parking spaces) of the Premises, as shown on the Use Plan attached hereto as Exhibit E, for parking by SYSTEM students, faculty and staff. The CITY shall have the right to use and occupy Section B (approximately 70 parking spaces) as shown on Exhibit B. SYSTEM'S use of the Premises shall be limited to days when the SYSTEM is in session at the Portland or Gorham campus of SYSTEM as determined by the SYSTEM calendar. At all other times, when the SYSTEM is not in session, including but not limited to weekends, school vacations and the period from May 15 to September 1, the CITY shall have use and occupancy of the entire Premises.

Whereas, the initial term of the Lease was for a fifteen (15) year period which term commenced after the improvements described in Part I of the Lease Agreement completed and when a certificate of occupancy was issued by the appropriate official of the City of Portland for use of the Premises as a parking lot, pursuant to Portland's Code of Ordinances §14-463(4).

Notwithstanding the above provision, however, the CITY may elect to terminate this Lease at the end of ten (10) years, provided that it has given written notice of its intention to do so at least twelve (12) months prior to the expiration of the tenth year of the term.

If CITY should elect to terminate the Agreement, it will pay SYSTEM \$166,666 within sixty (60) days from the date it acquires the exclusive use and occupancy of the Premises. Payment of the \$166,666 shall be in full settlement of any claims for damages for any which SYSTEM might have as a result of such termination.

Whereas, the CITY has notified the UNIVERSITY of its intent to terminate the Agreement as of January, 2002 and has given written notice of such intent (Exhibit C), the CITY and the UNIVERSITY wish to amend the original Lease Agreement in the following manner:

- A. The CITY will continue to lease to the UNIVERSITY the use of the Marginal Way Lot under the same terms and conditions as outlined in the 1992 Lease Agreement and as amended by this Agreement;
- B. The period of use shall be for the remaining five (5) years of the January 16, 1992 Lease Agreement except that either the UNIVERSITY or the CITY may terminate this Amendment with twelve (12) months written notice of such termination;
- C. Should termination by either the CITY or the UNIVERSITY occur during the five (5) year period remaining on the 1992 Lease Agreement, the CITY shall pay the UNIVERSITY an amount equal to \$16,667 for each fall and spring semester of the remaining period of annual use that did not take place.

EXHIBIT A ✓

## LEASE AGREEMENT

THIS AGREEMENT made this 16 day of January, 1997 by and between the CITY OF PORTLAND, a body politic and corporate, having a place of business in the County of Cumberland and State of Maine (hereinafter "CITY") and the UNIVERSITY OF MAINE SYSTEM, a body politic and corporate, having a place of business in said Portland (hereinafter "SYSTEM").

## W I T N E S S E T H :

WHEREAS, the CITY is the owner of a certain tract or parcel of land, adjacent to Marginal Way in said City, and known as the "Marginal Way Lot"; and

WHEREAS, SYSTEM desires to have CITY improve the said lot, at SYSTEM's own cost and expense, for use as a parking lot; and

WHEREAS, the CITY is willing to lease the said premises to SYSTEM for use as a parking lot both by it and also SYSTEM, subject to the terms and conditions set forth below.

NOW, THEREFORE, in consideration of their mutual covenants, promises and agreements, and other good and valuable consideration, receipt of which is hereby acknowledged, the said parties agree as follows:

PART I - CONSTRUCTION OF IMPROVEMENTS1. Premises.

The Premises which are the subject of this Agreement are more fully described in the survey and the metes and bounds description which are attached hereto as Exhibits A and B, respectively.

2. Improvements.

CITY agrees to improve the Premises for use as a parking lot in accordance with the Design and Specifications.

3. Design and Specifications.

The design and specifications of the said parking lot will be that set forth in the "Plan of Marginal Way Parking Lot", dated \_\_\_\_\_, 1991, which is attached hereto as Exhibit C. The design and specifications shall be modified, as necessary, in order to conform with the lawful requirements of any federal, state, or local regulatory body having jurisdiction over the project.

4. Approval of Regulatory Boards.

The parties agree to cooperate in good faith in seeking and obtaining approvals and permits which may be necessary for the project from federal, state, or local boards having jurisdiction. The CITY shall have primary responsibility for obtaining such permits. All applications for permits and licenses shall be prosecuted diligently.

5. Bidding Project.

The parties agree that bids for the project will be administered and awarded by the CITY, whose determination, in regard to the award of the contract, shall be final.

6. Form of Construction Contract.

The construction contract shall be in the form attached hereto as Exhibit D. The contract, among its other provisions, shall require the general contractor to provide a performance bond, a labor and materials payment bond, and insurance coverage, as required by the CITY. All bonds, as well as insurance policies provided in connection with the project, shall be issued by companies authorized to do business in the State of Maine and in a form approved by the CITY.

7. Termination of Agreement.

If construction of the improvements described herein should be unable to be commenced within twelve (12) months from the date of this Agreement, then either party, within thirty (30) days from such date, may declare this Agreement terminated, and neither shall have a claim for damages of any kind against the other as a result of such termination.

8. Construction Costs in Excess of \$500,000.

If, after receipt of bids pursuant to Paragraph 5 above, the cost of construction of the improvements described herein

should exceed \$500,000 (exclusive of the value of services provided by the CITY's regular employees), and, after having been requested to do so, the parties should be unable to agree either on:

- (1) a mutually acceptable way of sharing such increased costs, or
- (2) a mutually acceptable way of modifying the plans to reduce the costs of constructions, then either party may terminate this Agreement by giving notice of its intention to do so to the other.

## PART II - LEASE

### 1. Premises.

CITY hereby leases and lets to SYSTEM, and SYSTEM hereby takes and hires from CITY, subject to the terms, conditions, covenants and provisions hereof, the Premises which are the subject of this Agreement as described in Part I, including all improvements thereon, appurtenances thereto and all necessary easements for access from and egress to public ways and any necessary utility easements.

### 2. Term.

The initial term of this Lease shall be for a fifteen (15) year period which term shall commence after the improvements described in Part I have been completed and when a certificate of occupancy has been issued by the appropriate official of the City of Portland for use of the Premises as a parking lot, pursuant to Portland's Code of Ordinances §14-463(4).

Notwithstanding the above provision, however, the CITY may elect to terminate this Lease at the end of ten (10) years, provided that it has given written notice of its intention to do so at least twelve (12) months prior to the expiration of the tenth year of the term.

If CITY should elect to terminate the Agreement, it will pay SYSTEM \$166,666 within sixty (60) days from the date it acquires the exclusive use and occupancy of the Premises. Payment of the \$166,666 shall be in full settlement of any claims for damages for any which SYSTEM might have as a result of such termination.

3. Rent.

SYSTEM shall pay to CITY rent for the full term of this Lease in the amount of FIVE HUNDRED THOUSAND DOLLARS (\$500,000.00) which rent shall be in full and complete satisfaction of all of SYSTEM's obligations for the cost of development and construction of the Premises and rent for said initial term of fifteen (15) years' use and occupancy of the Premises by SYSTEM.

The rental payments shall be made in three (3) installments. The first installment shall be paid thirty (30) days after the commencement of construction activity on the Premises and shall be in an amount representing the costs of the work completed to date, less a 10% retainage.

The second payment shall be due and payable on the forty-fifth (45th) day from the start of construction and shall be in an amount representing the costs of the work completed to date, less 10% retainage.

The final payment, including retainage, shall be due and payable fifteen (15) days after completion of the project and its acceptance by CITY.

4. Use and Occupancy.

SYSTEM shall have the exclusive right to use and occupy Section A (approximately 290 parking spaces) of the Premises, as shown on the Use Plan attached hereto as Exhibit E, for parking by SYSTEM students, faculty, and staff. The CITY shall have the right to use and occupy Section B (approximately 70 parking spaces) as shown on Exhibit E. SYSTEM's use of the Premises shall be limited to days when the SYSTEM is in session at the Portland or Gorham campus of SYSTEM as determined by the SYSTEM calendar. At all other times, when the SYSTEM is not in session, including but not limited to weekends, school vacations and the period from May 15 to September 1, the CITY shall have use and occupancy of the entire Premises.

5. Covenant of Quiet Enjoyment.

Subject to the provisions of Paragraph 4, upon payment by SYSTEM of the rent herein provided and upon the observance and performance of all the covenants, terms and conditions on SYSTEM's part to be observed and performed, SYSTEM shall peaceably and quietly hold and enjoy the Premises for the term hereof without hindrance or interruption by CITY or any person or persons claiming by, through or under CITY.



6. Compliance with Laws.

SYSTEM agrees that during the term of this Lease it shall comply with all present and future laws, ordinances, orders, rules and regulations of the federal, state and city governments with respect to the Premises and their use and occupancy; provided, however, that any future City ordinances, orders, rules or regulations shall not impair or interfere with SYSTEM's use or occupancy of the Premises.

7. Repair and Maintenance.

SYSTEM shall, at its own expense, keep the Premises free of snow and ice, and in a good state of repair to the reasonable satisfaction of the City Manager or his designee. In addition, SYSTEM shall sweep the Premises at least three (3) times every year; maintain the landscaped areas, including but not limited to the lawn and other vegetation; and at least once every five (5) years stripe and seal-coat the paved area of the Premises in accordance with the City specifications for such work which are in effect at that time.

8. Insurance.

A. Insurance Provided by CITY

During the period when the CITY and SYSTEM shall share the use and occupancy of the Premises, as provided herein, CITY will maintain the following insurance coverage:

<u>DESCRIPTION</u>	<u>COVERAGE</u>	<u>EACH OCCURRENCE</u>
General liability, including special broad form property damage coverage	B.I./Death P.D.	\$500,000
Contractual liability	B.I./Death P.D.	\$500,000
Employer's liability	B.I./Death P.D.	\$500,000
Garage liability (per vehicle limit \$30,000)	B.I./Death P.D.	\$500,000

The SYSTEM shall be named as an additional insured in all such policies.

The CITY will provide SYSTEM with a certificate of insurance which shall provide that thirty (30) days written notice of non-renewal, material modification, or cancellation must be given prior to the effective date of such non-renewal, material modification, or cancellation.

B. Insurance Provided by SYSTEM

During the period when the CITY and SYSTEM shall share the use and occupancy of the Premises, as provided herein, SYSTEM will maintain the following insurance coverage:

<u>DESCRIPTION</u>	<u>COVERAGE</u>	<u>EACH OCCURRENCE</u>
General liability, including special broad form property damage coverage	B.I./Death P.D.	\$500,000
Contractual liability	B.I./Death P.D.	\$500,000

The CITY shall be named as an additional insured in all such policies.

The SYSTEM will provide CITY with a certificate of insurance which shall provide that thirty (30) days written notice of non-renewal, material modification, or cancellation must be given prior to the effective date of such non-renewal, material modification, or cancellation.

C. Independent Contractor's Insurance

If SYSTEM should elect to have any of its obligations under this Agreement performed by an independent contractor, then it shall require such independent contractor to provide insurance coverage in the types and amounts set forth above. Such contractor will not be required to provide garage liability coverage, however, but will be required to provide worker's compensation insurance, as required by Maine law.

Prior to such contractor undertaking any work on the Premises, SYSTEM shall provide CITY with a certificate of insurance indicating that coverage required herein is in

place. In addition, the certificate shall provide that thirty (30) days written notice of non-renewal, material modification, or cancellation must be given prior to the effective date of such non-renewal, material modification, or cancellation.

The CITY shall be named as an additional insured in all such policies.

Claims made policies will not be accepted.

9. Default.

If SYSTEM should fail to perform any covenant, obligation or agreement hereunder for a period of thirty (30) days after written notice from the CITY specifying such failure (except that if such performance cannot be completed within thirty (30) days, SYSTEM shall not be in default if it shall have commenced performance within such period and shall thereafter prosecute the same with diligence and continuity), such default shall at the option of CITY terminate this Lease.

10. CITY's Remedies.

- A. Each of the following events shall constitute a default or breach of this Lease by SYSTEM:
- (1) If SYSTEM should vacate or abandon the Premises.
- B. In the event of any default hereunder, the rights of CITY shall be as follows:
- (1) CITY shall have the right to cancel and terminate this Lease as well as all of the right, title and interest of SYSTEM hereunder by giving to SYSTEM not less than thirty (30) days' notice of the cancellation and termination. On expiration of the time fixed in the notice, this Lease and the right, title and interest shall terminate in the same manner with the same force and effect, except as to SYSTEM's liability, as if the date fixed in the notice of cancellation and termination were the end of the term.
  - (2) CITY may re-enter the Premises immediately and remove any personnel of SYSTEM. On termination CITY may recover from SYSTEM all damages proximately resulting from the breach, which sum shall be immediately due CITY from SYSTEM in addition to its reasonable attorney's fees.

11. Destruction of the Premises.

If the Premises, or any part thereof, shall be damaged by fire, the elements, or other casualty, then SYSTEM shall give notice thereof to CITY and, except as hereinafter otherwise provided, CITY shall within thirty (30) days after such notice commence to repair the Premises and thereafter prosecute the completion of such repair with due diligence, subject to delays resulting from any cause not within the control of CITY. If the damage to the Premises shall render the whole or any part thereof unsuitable for SYSTEM's use, a just proportion of the rent, according to the nature and extent of the damage to the Premises, from the date of such damage until the Premises or such part thereof shall be restored for the use and occupation of SYSTEM shall be rebated to SYSTEM by CITY.

12. Eminent Domain.

- A. If at any time during the term of this Agreement title to the whole or materially all of the Premises shall be taken by exercise of the right to condemnation or eminent domain or by agreement between CITY and those authorized to exercise such right (all such proceedings being collectively referred to herein as a "taking in condemnation"), this Agreement shall terminate and expire on the date of such taking. In the event of any difference between the parties as to whether materially all of the Premises shall have been taken, the controversy shall be resolved by arbitration as hereinafter provided.

In the event of the taking of the whole or materially all of the Premises during the term of the Agreement, the rights of CITY and SYSTEM to share in the net proceeds of any award upon any such taking, shall be as follows:

- (1) CITY shall be entitled to receive, with interest thereon, that portion of the award as shall represent compensation for the value of the demised Premises, considered as vacant and unimproved land.
  - (2) During all the term herein demised, SYSTEM shall be entitled to receive, with interest thereon, that portion of the award as shall represent compensation for the value of the improvements on the Premises.
- B. If at any time during the term, title to less than the whole or materially all of the demised Premises shall be

taken in condemnation, all of the award collected by CITY shall be held by the CITY and SYSTEM and be applied and paid over toward the cost of restoration of the Premises. Any balance remaining in said fund after payment of such cost of restoration shall be paid to SYSTEM after the restoration has been completed.

13. Assignment.

SYSTEM covenants and agrees that it will neither assign nor transfer its rights hereunder, either in whole or in part, without first obtaining the written consent of the CITY, which consent shall not be unreasonably withheld by CITY.

14. Possession and Ownership Upon Termination.

SYSTEM covenants and agrees that upon either termination or expiration of this Lease, whether by lapse of time or because of the conditions or provisions contained herein, it will peaceably and quietly quit and surrender the Premises. SYSTEM further agrees that upon either termination or expiration of its tenancy all improvements to the Premises, including but not limited to fixtures and equipment installed therein or attached thereto, shall become the sole and exclusive property of the CITY. SYSTEM shall provide CITY with any documents reasonably necessary, in the opinion of its corporation counsel, to reflect the CITY's sole and exclusive ownership of such fixtures and equipment at such time.

15. Partial Invalidity.

If any term, covenant, condition or provision of this Agreement or its application to any person or circumstances shall, at any time or to any extent, be invalid or unenforceable, the remainder of this Agreement (or the application of such term or provision to persons or circumstances, other than those as to which this Lease is held invalid or unenforceable) shall not be affected thereby; and each term, covenant, condition and provision shall be valid and enforced to the fullest extent permitted by law.

16. Notice.

Whenever under the terms of this Lease a written notice is required it shall be given by registered or certified mail. Such notice shall be deemed to have been given on the earlier of: (a) the date on which such notice is actually received, or (b) the third day following its deposit in the United States mail, postage prepaid, addressed as follows:

if intended for the CITY, to:

City Manager  
Portland City Hall  
389 Congress Street  
Portland, Maine 04101

if intended for SYSTEM, to:

University of Maine System  
c/o Associate Vice Chancellor  
Office of Facilities  
107 Maine Avenue  
Bangor, Maine 04401-1805

with a copy to: Office of University Counsel  
University of Maine System  
150 Capitol Street  
Augusta, Maine 04330

17. Arbitration.

Any disputes arising out of or in the course of the Agreement which are not settled by mutual agreement of the parties shall be resolved by litigation in the State of Maine Superior Court for Cumberland County. By the mutual agreement of the parties, however, any question in dispute under the Agreement may be settled by arbitration in accordance with the rules of the American Arbitration Association. Judgment upon any arbitration award so rendered may be entered in the court of the forum specified herein.

18. Shuttle Service.

During the periods when SYSTEM is in session and using the Premises for parking, it agrees to run a shuttle service from the Premises to the campus at intervals of not less than one (1) hour, commencing at 8:00 A.M. and ending at 10:00 P.M.

19. No Waiver.

No waiver or breach of any covenant or condition of this Agreement shall constitute a waiver or breach of any subsequent breach by either party nor justify or authorize the nonobservance on any other occasion of any covenant or condition by either party.

20. Parties.

Notwithstanding any provision of this Agreement to the contrary, if at any time or times during its term CITY as Lessor and SYSTEM as Lessee should be the same person, party or entity, their interests shall remain separate and distinct and shall not be merged into one estate so as to cancel, terminate or extinguish this Agreement by law or otherwise.

21. Governing Law.

This Agreement and the performance thereof shall be governed, interpreted, construed and regulated by the laws of the State of Maine.

22. Memorandum of Lease.

The parties will at any time, at the request of either one, promptly execute an instrument, in recordable form, which constitutes a memorandum of lease setting forth a description of the demised premises, the term of this Agreement, and any other portions thereof as either party may request or as may be required by any applicable law, ordinance or governmental rule or regulation.

23. Entire Agreement.

This Agreement contains the entire agreement of the parties with respect to the letting of the Premises.

IN WITNESS WHEREOF, the parties hereto have caused this Agreement to be duly executed the day and year first above written.

WITNESS:

*Virginia S. Kane*

CITY OF PORTLAND

By: *Robert Ganley*  
Robert Ganley  
Its City Manager

WITNESS:

*Richard Christ*

UNIVERSITY OF MAINE SYSTEM

By: *William J. Sullivan*  
William J. Sullivan  
Its Vice Chancellor for  
Administration

- EXHIBIT A: Survey
- EXHIBIT B: Metes & Bounds Description
- EXHIBIT C: Design and Specifications  
[Plan of Marginal Way Parking Lot]
- EXHIBIT D: Construction Contract
- EXHIBIT E: Use Plan

A:9100-141.1  
fpf/218

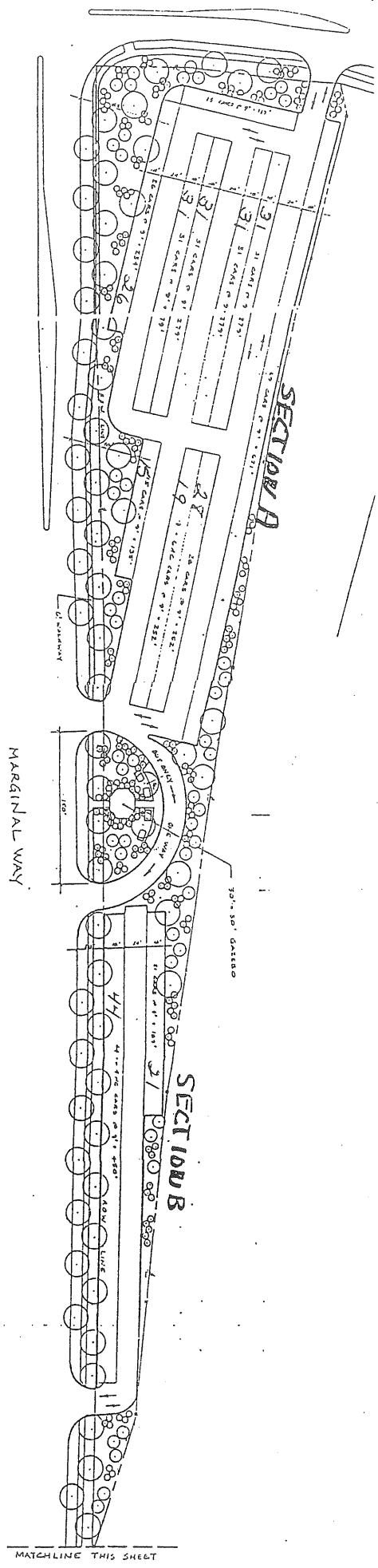




EXHIBIT C

Executive Department

Robert B. Ganley  
City Manager

## CITY OF PORTLAND

December 14, 2000

Richard Pattenaude, President  
University of Southern Maine  
96 Falmouth Street  
Portland ME 04104-9300

Re: Marginal Way Parking Lot Lease – Notice of Termination

Dear President Pattenaude:

The City of Portland and the University of Maine System entered into a parking lot lease agreement for use of the Marginal Way lot on January 16, 1992. Pursuant to part II, paragraph 2 of the lease, the City may elect to terminate the lease at the end of ten years provided that it has given written notice of its intention to do so at least twelve months prior to the expiration of the tenth year of the term.

Please consider this letter as the written notice required by the lease to notify you of the City's intention to terminate the parking lot lease, with the City obtaining possession of the Marginal Way lot in January of 2002.

The lease further provides that if the City terminates the agreement at the ten-year mark, the City will pay the University of Maine System \$166,666 within sixty days from the date that the City acquires exclusive use and occupancy of the Marginal Way lot. The City fully intends to comply with its obligations under the lease. We would like to discuss entering a new lease with you to begin in January of 2002 as we do not anticipate needing exclusive use and occupancy until May of 2003 at the earliest and we may not in fact need the property until much later. We would initially apply any new lease payments against our \$166,666 obligation so that the University would not have to use its own funds.

Please contact me if you have any questions regarding this notice.

Sincerely,

A handwritten signature in black ink that reads "Robert B. Ganley".

Robert B. Ganley  
City Manager

RBG:njc

cc: Joseph E. Gray, Jr., Director, Planning & Urban Development  
Lee Urban, Director, Economic Development

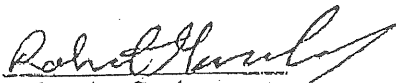
NOTICE OF TERMINATION

This notice of termination is being provided pursuant to Part II Sections 2 and 16 of a certain lease between the University of Maine System and the City of Portland. A copy of the lease is attached hereto as Exhibit A.

The City of Portland is hereby serving a twelve month notice of termination of the lease for use of a parking lot located on Marginal Way in Portland. The City will take possession of the property effective January 1, 2002.

City of Portland

Dated: 12-15-00

  
Robert B. Ganley  
City Manager

February 27, 2001

To: Sarah Hopkins, Development Review Program Manager  
City of Portland

From:  David Early, P.E. Executive Director

Subject: Proposed U.S.M. Bio-Science Institute

The following information has been enclosed as it may aid the Planing Board in their review of U.S.M.'s proposed Bio-Science Institute.

1. **Parking Inventory:**

U.S.M.'s Portland Campus presently has a parking space inventory of 1,605 parking spaces. Our peak demand per DeLuca-Hoffman Associates Parking and Traffic Study is 1,321. This leaves 284 available parking spaces. These available spaces are typically located at 15 Baxter Boulevard and the Marginal Way Parking Lot.

During the construction of the Bio-Science Institute, 98 cars will be displaced from U.S.M.'s P-9 parking lot. Ultimately the capacity for these 98 parking spaces will end up at 15 Baxter Boulevard and Marginal Way.

Our Construction Contract will specify that Construction Workers park off campus. Space will be made available for these workers only at the Marginal Way Parking Lot.

An inventory of U.S.M. Parking is attached. Our Enrollment/Parking Mitigation Plan was distributed to the Board in January. Another copy is attached for convenience. I have also included the most recent "Metro" report on the U.S.M./METRO 50-CENT FARE program. The report identifies 19,101 rides on this program since March of 2000.

## 2. **Neighborhood Meeting**

On Friday, February 9th a meeting was held at U.S.M.'s Woodbury Campus Center with Neighbors. 1,219 invitations were sent to our neighbors. We had 35 people in attendance. A copy of our letter of invitation and a record of the meeting is attached. Our next neighborhood meeting is scheduled for Friday, March 16th. This meeting will be on Parking & Traffic. The DeLuca-Hoffman Associates Report will be reviewed with our neighbors in detail. City, as well as University Officials, will be present to meet with U.S.M. neighbors.

## 3. **Building Elevations and Landscape Plan**

Leslie Glynn, AIA, Associate Principal of Symmes Maine & McKee Associates is prepared to discuss in detail the appearance and presentation of the Building from both the Falmouth Street (north) as well as Campus (south) elevations. The proposed addition to the existing Science Building results in the re-location of the present loading dock facing Falmouth Street. This loading dock is relocated to the lower, basement level, and turned to face the Sullivan Gym. It will no longer be in plain site on Falmouth Street, as the present one is. No additional entrance / exits are added to the Science Building Complex as a result of this addition, particularly for reasons of building security. The addition itself connects to existing circulation corridors, and building entrances/exits. Additional work is proposed to improve the appearance of the existing 3 story precast structure. Ms. Glynn will be prepared to provide a full explanation of the landscape plan as well.

### Attachments:

1. USM Parking Inventory, December-2000
2. USM Parking Space Availability at Peak Demand
3. January 18, 2001 Letter from Sam Andrews  
on Enrollment and Parking Mitigation
4. January 25, 2001 Letter to USM Neighbors
5. Notes - Review of USM Neighborhood Meeting
6. Colored Site Plan
7. Colored Building Elevations

**UNIVERSITY OF SOUTHERN MAINE  
PARKING INVENTORY - PORTLAND CAMPUS  
DECEMBER, 2000**

LOT	LOCATION	DESIGNATION	PARKING SPACES
P1	Luther Bonney	Faculty/Staff Handicapped	28 8
P2	Campus Center	Student Commuter Univ. Health Services Handicapped	182 1 7
P3	Bedford Street	Student Commuter Meters Handicapped	536 2 1
P4	Alumni House	Faculty/Staff	30
P5	Bedford Street	Faculty/Staff	7
P6	Law Building	Faculty/Staff Meters Handicapped	137 7 6
P7	Bookstore DFM	Faculty/Staff Handicapped	41 1
P8	Child Care	Faculty/Staff 15 Min Drop-Offs Handicapped	12 4 2
P9	Science/Gym	Faculty/Staff Motorcycles Handicapped	109 6 2
P10	Payson Smith	Meters Handicapped	6 4
P11	Science Front	Meters	4
P12	Police & Safety	Student Commuter Police Parking 15 Min Police Business Handicapped	12 1 2 1
P13	Library	Faculty/Staff Meters Handicapped	28 9 4
P14	Marginal Way	Student Commuter Handicapped	293 7
	15 Baxter Boulevard	Student Commuter Handicapped	53 2
	Grand Street	Meters	12
	Outbuildings	Faculty/Staff	38
		<b>TOTAL</b>	<b><u>1,605</u></b>

**University of Southern Maine  
Portland Campus  
Master Plan**

**Existing Overall Parking Space Availability**

<b>Location</b>	<b>Available Spaces</b>	<b>Demand*</b>	<b>Unoccupied Spaces</b>
P 1	36	37	-1
P 2	190	189	1
P 3	539	567	-28
P 4	30	32	-2
P 5	7	8	-1
P 6	150	136	14
P 7	42	45	-3
P 8	18	18	0
P 9	117	119	-2
P 10	10	9	1
P 11	4	1	3
P 12	16	13	3
P 13	41	40	1
Marginal Way	300	52	248
15 Baxter Blvd.	55	13	42
Grand Street	12	12	0
Outbuildings	38	30	8
<b>Total</b>	<b>1605</b>	<b>1321</b>	<b>284</b>

\* Demand = Peak 10:00 AM Demand on a Tuesday. Tuesday was identified by the University as the peak weekday for enrollment.



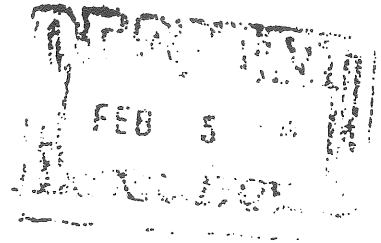
# METRO

February 1, 2001

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program



For your posting the following is ridership for the program:

Year 2000

March 1,234

April 2,024

May 2,227

June 1,893

July 1,909

August 2,059

**Cumulative Free Rides: 11,346**

September 1,313

October 1,616

November 1,588

December 1,504

January '01 1,734

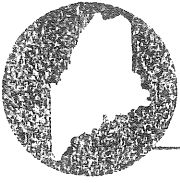
**Cumulative 50-Cent Rides: 7,755**

**Total All Rides (March-January): 19,101**

In the upcoming weeks, we will be actively working with Rodney Mondor to introduce students on the Gorham campus to the METRO, as well as, Chris O'Connor at Portland Hall with Portland campus students..

GREATER PORTLAND TRANSIT DISTRICT

114 Valley Street • PO Box 1097 • Portland, Maine 04104 • (207)774-0351 • FAX (207)774-6241



# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
(207) 780-4200 FAX (207) 780-4860  
TTY (207) 780-5646

January 25, 2001

Dear USM Neighbor:

We'd like to invite you to a neighborhood meeting to find out more about building plans on the Portland campus of the University of Southern Maine.

The meeting will be held at 7 p.m., Friday, February 9, in the cafeteria of the Woodbury Campus Center, Bedford St., Portland. There should be adequate parking in the Bedford St. lots, located a short distance from the intersection of Bedford and Forest Ave.

The meeting is open to any members of the public, so please feel free to invite your friends and neighbors. Light refreshments will be available. For planning purposes, please RSVP to 780-4200.

USM officials will be outlining the plans for the Biosciences Research Institute. This facility, which will be financed with state research and development funds, is planned as an addition to the Science Building on Falmouth St.

We also want to share with you our master plan for the development of the Portland campus. For the most part, the plan represents a 20 to 30-year vision as to how our Portland campus might develop, depending on state funds, private gifts, enrollment trends and other factors. There are only three projects under consideration during the next five years: the Biosciences Research Institute; renovation of the former Steego Building on Bedford St. to house our Muskie School; and construction of a Community Education Center/parking garage on the current Bedford St. parking lot.

USM's mission is to enhance the region's quality of life. Our success in doing that depends on thoughtful partnerships with the community. We value your input and hope to see you on the 9th.

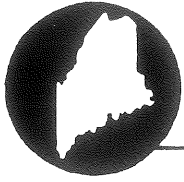
Sincerely,

Bob Caswell  
Executive Director, Media & Community Relations

Dave Early  
Executive Director, Facilities Management

RSC/jao





# UNIVERSITY OF SOUTHERN MAINE

Office of Media and Community Relations

96 Falmouth Street  
P.O. Box 9300  
Portland, Maine 04104-9300  
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## REVIEW OF USM NEIGHBORHOOD MEETING

The following is an outline of the discussions from the neighborhood meeting, held at 7 p.m., Friday, February 9, in the cafeteria at the University of Southern Maine's Woodbury Campus Center, Portland.

Thanks to the cooperation of the city's Planning Department, we were able to mail 1,219 letters of invitation to residents in an area roughly bounded by Back Cove, the Parkside neighborhood, St. John St. and through the Oakdale section to Douglass St.

Thirty-five people attended, and shared with us concerns ranging from parking to the type of research to be conducted in the proposed Biosciences Institute.

I've grouped those concerns under the following headings, and under each heading have summarized issues that were addressed.

### **PARKING/TRAFFIC:**

Much of the 3+ hour meeting was devoted to this issue. In the short term, many residents felt that our plan to compensate for loss of parking during construction of the Biosciences Institute was flawed in that most people would not use the Marginal Way lot. We responded that an aggressive campaign to inform people of the Marginal Way lot, coupled with a beefed up shuttle bus service, would serve us and the neighbors well during the construction period.

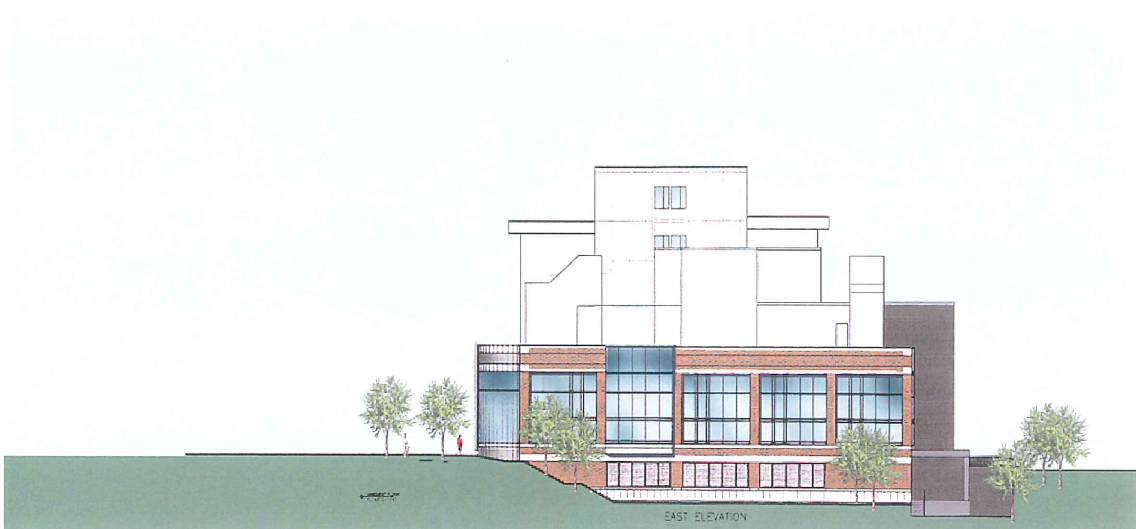
It was further explained that the University is implementing a revised class schedule for the fall, 2001. The schedule is designed to help distribute uniformly space demands throughout the week as opposed to the current schedule that causes peak demands on Tuesdays and Thursdays.

Several questioned why we are planning to construct a research building before a parking garage. We explained that dedicated funding that must be used or be forfeited is in place for the research facility. We also noted that the facility will be dedicated exclusively to research and will not include classrooms and other high-traffic areas. Guests questioned the number of researchers that would be housed in the building.

Neighbors also voiced frustration at students' cars that are taking up on-street parking in and around their neighborhoods. A USM/city collaborative effort to address the problem ought to be initiated, they said. As part of this discussion, people asked why the long-range campus master plan did not contain any provisions for developing on-campus student housing. We explained that our plans call for continuing to emphasize Gorham as a traditional campus environment. A new 224-bed residence hall will open in Gorham this summer. Portland Hall on Congress St. seems to satisfy student demand for Portland-based housing. We have had informal discussions about the possibility of a cooperative agreement with private developers to provide housing but there are no plans to do so.

Guests also expressed concerns over the lack of studies detailing how the closing of Bedford St., for example, would affect traffic flows in the neighborhoods. We explained that the Bedford St. proposal is one of several long-range visions included as part of the campus master plan. As such, it is so far in the future that it does not yet warrant detailed studies.

//more on back



# UNIVERSITY OF SOUTHERN MAINE

## PARKING MANAGEMENT PLAN

### Introduction

The University of Southern Maine is a multi-campus unit of the University of Maine System with campuses at Portland, Gorham and Lewiston and with students taking courses at the University Centers at Bath/Brunswick, Saco and Sanford; through video-conferencing, interactive television and the internet; and at other sites such as local schools and employer locations. In the fall, 1995, there were 9,721 students enrolled at the University of Southern Maine and 1,057 students enrolled at Lewiston/Auburn College, at the Centers at Bath/Brunswick, Saco, and Sanford and in distance education away from the Portland and Gorham campuses. In the fall, 2001, there were 10,966 students enrolled at the University of Southern Maine and 2,807 students enrolled at Lewiston/Auburn College, at the University Centers and in distance education. All of the student enrollment growth at USM over the period occurred at locations other than the Portland and/or Gorham campuses. Since the fall of 1995, enrollments at the Gorham campus increased while enrollments at the Portland campus decreased. With the projected decline in high school graduates beginning in the next several years, a continuation of the enrollment trends of the past six years is expected to continue.

In the 1994/95 fiscal year, the University of Southern Maine carried out \$13.7 million of externally funded grant/contract research. In the 2000/01 fiscal year, externally funded grant/contract research totaled over \$30 million. The University utilizes campus space for such efforts and has over 250 employees in leased facilities in Augusta; at 400 Congress Street and at Baxter Boulevard in Portland; and at John Roberts Road in South Portland. The Maine Legislature made two targeted additions to the University of Maine System base budget designed to substantially increase research and development (R&D) activity at the University of Maine and the University of Southern Maine, in particular to strengthen the capacity of both institutions to compete for Federal grants and contracts. The annual base budget operating increase was \$10 million plus \$2.5 million to cover debt service payments on a \$25 million internal bond issue for a period of 15 years. The general goal is for the Universities together to generate an additional \$40 - \$50 million annually in external research funding from the investment base of \$10 million. For USM the additional per year operation investment is \$2 million directed at research faculty and staff, for equipment and materials in the biosciences and information sciences, and advanced materials, primarily electronics. In addition, USM has a total of \$10.0 million to build and renovate research laboratories (\$2.7 million from a bond referendum approved by voters in 1998, \$5 million from the campus share of the internal bonding capacity, and \$2.0 million from indirect cost recovery funds). By Legislative designation, the Biosciences Addition to the Portland Science Building must be a research facility only and will have no impact on student enrollment.

## Supply and Demand

As part of the parking management process, a professional analysis of supply and demand for parking at the Portland campus was carried out by DeLuca-Hoffman Associates, Inc. This analysis involved physical counts of parkers at the University of Southern Maine, parkers on streets adjacent to the campus, and traffic patterns during the times reviewed. This analysis updated work carried out in the early 1990s in connection with the renovation of the former Johnson Supply Building into the Glickman Family Library. The existing supply of parking, together with planned changes resulting from the construction of the Bioscience Addition to the Portland Science Building, are presented in Exhibit I.

The analysis by DeLuca-Hoffman Associates indicated a relative balance of parking supply with parking demand taking into account the Biosciences Addition impact on parking. Although previous parking reviews prepared for the City of Portland allowed the inclusion of 179 on-street parking spaces (in areas such as Surrenden Street, one side of Falmouth Street, Durham Street, and Washburn Street), the current analysis did not consider the availability of any on-street parking in the area of the University.

Although the DeLuca-Hoffman study found that current supply, excluding consideration of any on-street parking, is in relative balance with demand, the challenge is to make good use of the supply to meet the needs of University parkers while minimizing the impact of the demand on the neighborhood surrounding the Portland campus. The balance of this plan identifies a number of strategies the University has and continues to pursue to better manage parking demand.

### Parking Management Strategies

The parking management strategies currently undertaken by and continuing by the University of Southern Maine are as follows:

- Course schedule review
- Weekend College/Osher Lifelong Learning Institute
- Parking resource management
- Marginal Way
- Campus shuttle systems
- Contractor parking
- METRO cooperative programs
- Street parking enforcement

### Course Schedule Review

The analysis of parking and traffic was carried out by DeLuca-Hoffman Associates in the fall, 2000 semester. Exhibit II shows the number of students registered for Portland campus classes at one hour intervals throughout the week. For example, at 8:00 a.m. on Monday morning there were 248 students registered to take classes on the Portland campus while at 2:00 p.m. on Monday, there were 1,116 students registered for classes at that time. The enrollment by hour data clearly indicates that Tuesday was the busiest day of the week and Tuesday attendance exceeded Monday attendance by 12.9%. Tuesday enrollment was 23.8% greater than that on

University of Southern Maine  
Joint Use Parking Discussion  
September 7, 2001

EXHIBIT I

The USM Portland Campus includes a variety of uses to serve the needs of the student population. We would expect that these uses such as classroom space, library space, lounge areas, food facilities and merchandising provide students with the opportunity to participate in several activities on each visit to the campus. Therefore, a student will occupy a parking space to attend classes and then utilize one or more of the complementary facilities. Given these occurrences, the overall parking demand for the campus would not be a summation of the individual demand for each facility, but some lesser amount depending upon the frequency of these joint use visits.

To determine the joint use parking demand, DeLuca-Hoffman Associates, Inc. performed parking demand counts on the campus and surrounding street system. The results of these counts that were performed in the fall of 2000 are summarized in the DeLuca-Hoffman Associates, Inc. report "Final USM Portland Campus Parking and Traffic Study for Campus Master Plan" dated December 2000 and further reported in the DeLuca-Hoffman Associates, Inc. letter dated June 20, 2001. This letter indicates a total campus parking demand on the busiest day of the week to be 1,517 spaces. For comparison purposes, the parking demand for individual uses was determined to be 1,625 spaces based on City Ordinance. The attached summary shows that the most likely candidates for shared uses such as the library, lounge, food facility, and merchandising spaces indicate a total parking demand of 218 spaces, which is roughly double the difference (1625-1517=108) in actual versus ordinance parking demand. Therefore, approximately 50% of the users of these facilities may be on campus for other uses. To confirm this shared use premise, DeLuca-Hoffman Associates, Inc. performed a survey of random students entering/leaving the library and Luther Bonney Hall on May 3, 2001 from approximately 9 AM to 5 PM. This survey indicated that approximately 66% of students utilizing those buildings were going to visit another use on campus on that day. This reinforces the concept that shared use of parking is occurring on the campus.

Given the survey results, we are comfortable that the field count of 1517 occupied spaces fairly represents the actual parking demand for the existing facilities and class schedules for the fall 2000 schedule. The University has implemented schedule changes and parking incentives to balance parking demands that may further reduce the actual parking demands. To determine the parking requirement after construction of the Bio Science Building, we have determined the assignable space and associated ordinance requirements for this building without joint use adjustments. The construction will result in the loss of 44 spaces on the Bio Science lot. The joint use versus ordinance requirements are summarized in the following table:

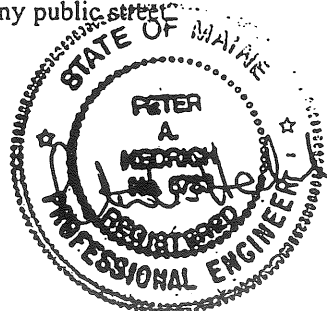
USM Portland Campus, Parking Summary					
Parking Requirements Based On:	Parking Req. to Meet Existing Demand	Parking Req. for Proposed Bio Science**	Total Required Parking	Proposed Supply ***	Req. New Parking Spaces
Joint Use	1,517* (Includes 179 On Street)	34	1,551	1,554	(3)
City Ordinance	1,625	34	1,659	1,554	105

\* This is the demand for the busiest hour of the day on the busiest day of the week (1338 in USM lots plus 179 on street).

\*\* Ordinance: 12,940 sf office @ 1/400 sf=33 spaces plus 1,000 sf storage @ 1/1,000=1 space

\*\*\* Includes on campus (1,200), outbuildings (38), Grand Street (12), Baxter Blvd. (48), Marginal Way (300) and loss of 44 spaces for construction of the Bio Science Building

The above table indicates a surplus of three spaces based on the shared use assumption for parking demand. This does not include any consideration of use of on street parking on any public street.



UNIVERSITY OF SOUTHERN MAINE  
STUDENT ENROLLMENT BY HOUR  
PORTLAND CAMPUS - FALL, 2000

	<u>Monday</u>	<u>Tuesday</u>	<u>Wednesday</u>	<u>Thursday</u>	<u>Friday</u>	<u>Saturday</u>
8 a.m.	248	11	218	2	41	20
9 a.m.	548	842	534	768	218	436
10 a.m.	595	1,239	568	1,180	287	441
11 a.m.	498	1,270	462	1,194	270	448
Noon	905	1,006	874	914	203	108
1 p.m.	941	1,090	785	976	273	106
2 p.m.	1,116	1,153	839	1,064	273	111
3 p.m.	888	887	689	840	121	73
4 p.m.	1,235	1,182	1,127	1,092	352	36
5 p.m.	1,091	1,083	1,040	1,019	344	0
6 p.m.	1,056	988	1,051	909	345	0
7 p.m.	774	767	798	648	40	0
8 p.m.	736	666	689	527	35	0
9 p.m.	<u>710</u>	<u>617</u>	<u>666</u>	<u>503</u>	<u>16</u>	<u>0</u>
<b>TOTAL</b>	<b><u>11,341</u></b>	<b><u>12,801</u></b>	<b><u>10,340</u></b>	<b><u>11,636</u></b>	<b><u>2,818</u></b>	<b><u>1,779</u></b>

Wednesday; Tuesday had 10.0% more students than on Thursday; and Tuesday had enrollment that was 354.3% greater than on Friday. The data also shows the variability of parking demand during a particular day.

As indicated in Exhibit III, with the support and endorsement of the USM Faculty Senate, a new class schedule was implemented in the fall, 2001 semester. As indicated in the memo from Provost Joseph Wood announcing this schedule change, one of the reasons for implementing this new schedule was to “open up classrooms and also help relieve some parking stress on Tuesdays and Thursdays”. Exhibit IV shows a comparison of Tuesday and Thursday class enrollment by hour for the fall, 2000 and fall, 2001 semesters. The changed schedule resulted in a decrease in Tuesday student enrollment and a decrease in several of the high parking demand periods with the same results on Thursday. While this class schedule change took two years of conversations and discussions with University faculty and students, concerns about parking demand/supply was one of the elements that was behind the initiation of such discussions. Class schedules and when classes are offered are under continual review and observation on an on-going basis to try to even out high class enrollment periods.

Weekend College/Osher Lifelong Learning Institute

Weekend College was created in response to concerns about heavy use of campus facilities and parking resources during the Monday through Thursday period as well as concerns about providing access to courses and programs for working students. Weekend College offers courses on Friday at 3:00 and after, on Saturday and on Sundays. The creation of Weekend College had the impact of shifting some course offerings from the heavily utilized Monday through Thursday time periods to lower utilization periods of Friday through Sunday and consequently lowered parking demand during the peak Monday through Thursday periods. The table below shows the number of students enrolled in Weekend College over the fall, 1995 to fall, 2001 period:

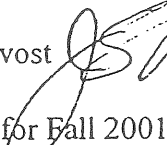
WEEKEND COLLEGE STUDENT ENROLLMENT

<u>Semester</u>	<u>Number of Students</u>
Fall, 1995	569
Fall, 1996	672
Fall, 1997	943
Fall, 1998	940
Fall, 1999	1,015
Fall, 2000	1,077
Fall, 2001	1,036

In addition, when the Osher Lifelong Learning Institute (Senior College) was begun, classes, events, and activities were scheduled on Fridays so as to bring people to campus during lower utilization periods of campus classrooms and parking resources. The Osher Lifelong Learning Institute had 496 participants during the fall, 2000 semester; 476 participants during spring, 2001 semester; and 590 participants during fall, 2001 semester. The University will continue its

December 14, 2000

Office of the Provost  
96 Falmouth Street  
P.O. Box 9300  
Portland, ME 04104-9300  
(207) 780-4485  
TTY (207) 780-5646  
FAX (207) 780-4549

TO: USM Faculty and Staff  
FROM: Joseph S. Wood, Provost   
RE: New Class Schedule for Fall 2001

With full administrative support and endorsement of the Faculty Senate, we will implement a new class schedule effective in the fall semester of 2001. Perhaps the least problematic of various options we have considered over the last few years, the new schedule builds on 90-minute morning time blocks on Monday-Wednesday-Friday, as well as on Tuesday-Thursday. See the attached complete schedule prepared by the Registrar.

Within the new 90-minute morning time blocks for Monday-Wednesday-Friday, scheduling priority will be given to 50-minute classes (3 credit hours) or 75-minute classes (4 credit hours) running over all three days of the week. Friday mornings also provide opportunity for innovative pedagogies that do not correspond to conventional scheduling.

We will adhere strictly to time blocks due to the availability of classroom spaces. The schedule does not affect offering of studio and laboratory classes, except insofar as they may necessarily have to adapt to new starting times.

The new schedule will incorporate afternoon changes established for spring semester 2001. To allow for a 45-minute bus trip between campuses all day long, Portland classes will begin at 8:45 a.m. all days. Implementation should open up classrooms and also help relieve some parking stress on Tuesdays and Thursdays.

As always, Deans are responsible to work with faculty to develop schedules that most effectively allow students to meet degree requirements. Registrar Steve Rand will work closely with Deans to reduce or eliminate scheduling conflicts.

I recognize that we will encounter unforeseen difficulties in implementing this schedule change, and I ask your support and patience as we proceed.

JSW/es



Beginning with the Fall 2001 semester the normal scheduling periods will be changed.

All periods on Monday thru Thursday before 4:00 p.m., will be 75 minutes with a 15 minute break between periods. This schedule will be the same for each day.

Periods after 4:00 p.m. will accommodate either 75 minute periods or 150 minute (2.5 hours) periods.

Friday will normally consist of two 2.5 hour periods.

PORTLAND		GORHAM	
MTWR	F	MTWR	F
8:45 - 10:00	8:45 - 11:15	8:00 - 9:15	8:00 - 10:30
10:15 - 11:30	11:45 - 2:15	9:30 - 10:45	11:00 - 1:30
11:45 - 1:00		11:00 - 12:15	
1:15 - 2:30		12:30 - 1:45	
2:45 - 4:00		2:00 - 3:15	
4:10 - 5:25		4:10 - 5:25	
5:35 - 6:50		5:35 - 6:50	
(4:10 - 6:40)		(4:10 - 6:40)	
7:00 - 9:30		7:00 - 9:30	

#### Notes

- 1) A 3 credit class offered in a 75 minute period will be offered either M/W or T/R.
- 2) A 3 credit class offered 3 times a week will be offered M W F during 50 minutes of a single period time block.
- 3) Classes that are 4 credit hours can be offered in a single period time block on M W F, with one meeting per week being 50 minutes.

**UNIVERSITY OF SOUTHERN MAINE  
ENROLLMENT BY HOUR - PORTLAND CAMPUS  
FALL 2000 and FALL 2001 SEMESTERS  
TUESDAY and THURSDAY**

	<u>Tuesday</u>				<u>Thursday</u>			
	<u>Tuesday</u> <u>2000</u>	<u>2001</u>	<u>#</u>	<u>Change</u> <u>%</u>	<u>Thursday</u> <u>2000</u>	<u>2001</u>	<u>#</u>	<u>Change</u> <u>%</u>
8 am	11	15	4	36.4%	2	15	13	650.0%
9 am	842	923	81	9.6%	768	911	143	18.6%
10 am	1,239	902	(337)	-27.2%	1,180	890	(290)	-24.6%
11 am	1,270	1,092	(178)	-14.0%	1,194	1,075	(119)	-10.0%
noon	1,006	1,077	71	7.1%	914	1,084	170	18.6%
1 pm	1,090	1,094	4	0.4%	976	1,102	126	12.9%
2 pm	1,153	1,044	(109)	-9.5%	1,064	983	(81)	-7.6%
3 pm	887	923	36	4.1%	840	904	64	7.6%
4 pm	1,182	841	(341)	-28.8%	1,092	846	(246)	-22.5%
5 pm	1,083	1,348	265	24.5%	1,019	1,161	142	13.9%
6 pm	988	1,270	282	28.5%	909	1,083	174	19.1%
7 pm	767	689	(78)	-10.2%	648	797	149	23.0%
8 pm	666	579	(87)	-13.1%	527	691	164	31.1%
9 pm	617	539	(78)	-12.6%	503	621	118	23.5%

efforts to program activities/classes to take advantage of lower utilization periods of campus classrooms and parking resources.

### Parking Resource Management

The University of Southern Maine parking and transportation resources are managed by the Parking and Transportation Division of the University of Southern Maine Police Department. Mr. Dewey Ferguson, Manager of Parking and Transportation, is the contact person at the University regarding parking and transportation issues and concerns. The University has a Parking and Transportation Review Committee comprised of faculty, professional staff, classified staff and students which serves in an advisory role to the President on parking and transportation matters. The 2001/02 membership of that Committee is listed in Exhibit V.

The University of Southern Maine has 1,605 parking spaces at the Portland campus including 46 spaces designated for handicapped parking and 40 metered spaces. Parking lots are designated for faculty/staff use (blue sign lots) and for student use (red sign lots). Parking decals are available to University parkers with the following fee schedule:

First Decal	\$35.00 each
Additional Decals*	\$20.00 each
Marginal Way Decals	\$ 0.00 each
Replacement Decals	\$ 5.00 each
Semester Decals	\$20.00 each

\*A student, faculty or staff member, who is the registered owner of more than one motor vehicle, may purchase a current 2<sup>nd</sup>, 3<sup>rd</sup>, etc. decal at a reduced rate as long as the registrant meets the following criteria; 1) at the time of the vehicle registration, the registrant must present the State vehicle registrations, originals/photocopies, for all vehicles that he/she wishes to register or has registered; 2) all of the vehicle registrations must be in the name of the registrant; and 3) the registrant must certify that there is no other student, faculty or staff member who will be parking any of these vehicles on campus.

Parkers can purchase a parking decal for the full year or a decal for a single semester. The cost of a parking decal for a year was increased from \$25.00 in the 2000/01 year to \$35.00 for the 2001/02 year. However, the Maine Labor Relations Board has ruled that the charge for a parking decal and the designation of parking lots are work conditions subject to collective bargaining. As contract negotiations have not been concluded with faculty and staff collective bargaining units, the cost of a parking decal for represented employees remains at \$25.00 for an annual decal.

Each person purchasing a parking decal is provided with a copy of University of Southern Maine "Motor Vehicle Rules"(copy included). These "... Rules" include a map which indicates parking lot locations and designations at both the Portland and Gorham campuses. Campus parking areas are patrolled by officers of the USM Police Department as well as student officers employed by the Parking and Transportation Division.

# USM PARKING AND TRANSPORTATION REVIEW COMMITTEE

## 2001-2002 COMMITTEE MEMBERS

EXHIBIT V

Jean Gutmann, Professor of Accounting, 118 Bedford Street, Portland, 780-4183, [gutmann@usm.maine.edu](mailto:gutmann@usm.maine.edu) (appointment effective 9/01. Eligible for two, four-year terms.)

Carl Hill, Assistant Director, Facilities Services, Gorham Student Life, 100 Upton, Gorham, 780-5592, [cnhill@usm.maine.edu](mailto:cnhill@usm.maine.edu) (appointment effective 9/01. Eligible for two, four-year terms.)

Philip Jagolinzer, Professor of Accounting, Emeritus, [pjagolin@usm.maine.edu](mailto:pjagolin@usm.maine.edu) (Emeritus member of Committee.) - (12 Cedar Brook Drive, Scarborough, ME 04074)

Samantha Langley-Turnbaugh, Assistant Professor, Environmental Science & Policy, 115B Bailey Hall, Gorham, 780-5361, [langley@usm.maine.edu](mailto:langley@usm.maine.edu) (appointed for 2001/02 as replacement for Chris Pennuto).

Carol McArdle, Administrative Assistant I, Ice Arena, Costello Sports Arena, Gorham, 780-5528, [mcardle@usm.maine.edu](mailto:mcardle@usm.maine.edu) (will complete 1<sup>st</sup> term 9/2003. Eligible for two, four-year terms.)

Marcy Muller, Chair, USM Student Senate; Powers House, Portland 874-6569 [mmpoohdy@hotmail.com](mailto:mmpoohdy@hotmail.com) (annual appointment - Student Senate has 2 representatives)

Heidi Noyce, Assistant Director of Transfer Affairs, Admissions House, Gorham 780-5098, [heidin@usm.maine.edu](mailto:heidin@usm.maine.edu) (completed 1<sup>st</sup> term in 9/2000. Eligible for two, four-year terms.)

George Pattershall, Electrician Coordinator, DFM, 25 Bedford Street, Portland, 780-4109/4176, [dwarren@usm.maine.edu](mailto:dwarren@usm.maine.edu) (completed 1<sup>st</sup> term in 9/2000. Eligible for two, four-year terms.)

Chris Pennuto, Assistant Professor, Environmental Science & Policy, 115D Bailey Hall, Gorham, 780-5395, [pennuto@usm.maine.edu](mailto:pennuto@usm.maine.edu) (named to first term in February, 2001. Eligible for two, four-year terms. On leave for 2001/02.

Steven Rand, Registrar, 113 Corthell Hall, Gorham, 780-5107, [srand@usm.maine.edu](mailto:srand@usm.maine.edu) (appointment effective 9/01. Eligible for two, four-year terms.)

Jane Coolidge Young, Director of Health Services, Modular Unit, Portland, 780-4083, [jyoung@usm.maine.edu](mailto:jyoung@usm.maine.edu) (started 1<sup>st</sup> term in 9/1999. Eligible for two, four-year terms.)

### Staff Members Assigned to Committee

Samuel G. Andrews, Chief Financial Officer, 724 Law Building, Portland, 780-4484, [sandrews@usm.maine.edu](mailto:sandrews@usm.maine.edu)

Lisa Beecher, Chief of USM Police, Upton Hall, Gorham, 780-5211, [beecher@portland.maine.edu](mailto:beecher@portland.maine.edu)

David J. Early, Executive Director of Facilities Management, 104 Anderson, Gorham, 780-4656, [dearly@usm.maine.edu](mailto:dearly@usm.maine.edu)

Dewey Ferguson, Parking Coordinator, USM Police & Safety, 780-5254, [misno52@usm.maine.edu](mailto:misno52@usm.maine.edu)

Judith Ryan, Executive Assistant of the President; 709 Law Bldg; 780-4712; [judyryan@usm.maine.edu](mailto:judyryan@usm.maine.edu)

**NOTE:** To send an e-mail to the entire committee, address it to [parking@usm.maine.edu](mailto:parking@usm.maine.edu)  
Micheline - a copy also.

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# Motor Vehicle Rules

## 2001-2002

USM UNIVERSITY OF  
Southern Maine



*"Husky Pride"*

USM operates three shuttle bus systems, will be discussed in subsequent sections of this plan, as well as a cooperative program with METRO. USM also has cooperated with the Greater Portland Council of Governments (COG) and METRO in having an electronic touchscreen-Smart Kiosk, which provides bus, area transportation and tourism information, installed in the Woodbury Campus Center in Portland. The University also cooperated with the COG Rideshare program. Parking information is distributed to the University Community through the weekly FREE PRESS campus newspaper, through notices and posters placed in all buildings, including residence halls. The "USM Information Exchange" a computer list-serve for University students, faculty and staff is utilized to disseminate information on the METRO program, Marginal Way parking, and other parking information. During the winter months, snow is removed from campus lots as needed. These removal operations are carried out during daylight hours so as not to disturb neighbors with the noise resulting from trucks and equipment. Last year over \$10,000 was spent in snow removal at the Portland campus.

During the 1999/00 fiscal year, the University took a lease for office space at 15 Baxter Boulevard, just across from the University Library and a short walk from campus. This was for a three year period with renewal options and provided office space for several Muskie School of Public Service grant/contract activities. This property included 55 additional parking spaces that are available for parking use by University of Southern Maine staff and students with University parking decals. The University continually assesses the current University parking areas to determine if additional spaces can be accommodated through annexing contiguous space into the lots.

Overall, the University has a pro-active parking education program for students, faculty, and staff as well as continued efforts to make parking more efficient and effective. Through the Parking and Transportation Division the parking resources are managed to provide maximum availability of parking, and the management of the University shuttle systems are integrated within these resources.

### Marginal Way

Parking is provided at the Marginal Way parking lot for vehicles displaying **any** valid USM parking permit/decal. Shuttle service is provided between Marginal Way and the Portland campus. A yellow decal is available at no charge, for employees and students during the fall and spring semesters, and authorizes parking in **only** the Marginal Way lot when shuttle bus service is provided between Marginal Way and the Portland campus. When Marginal Way bus service is suspended, a yellow decal authorizes parking in red color-coded lots and green color-coded lots, Gorham and Portland. Prior to this year, there was a \$5.00 charge for a Marginal Way parking decal.

Starting at 7:00 a.m., Monday through Thursday, a shuttle bus runs continually from the Marginal Way lot to the Portland campus with stops at the Woodbury Campus Center, Payson Smith Hall and at other on-campus locations. At 8:00 p.m., the shuttle that serves Portland Hall also serves as the Marginal Way shuttle. The last shuttle returns to the Marginal Way lot at

10:00 p.m., Monday through Thursday. For 2001/02, public service events and activities held at the Portland campus during Marginal Way shuttle hours require participants to park at Marginal Way and utilizes the shuttle service. In addition, large sandwich board-type signs are maintained in campus parking lots indicating the availability of Marginal Way parking and the shuttle service.

With these efforts, Marginal Way parking use has increased as follows during the first five weeks of the fall, 2001 semester:

<u>Week in Semester</u>	Fall, 2001 <u>Marginal Way Parking Increase</u>
1 <sup>st</sup> week	1.57%
2 <sup>nd</sup> week	37.88%
3 <sup>rd</sup> week	29.78%
4 <sup>th</sup> week	33.85%
5 <sup>th</sup> week	15.77%

The Marginal Way lot was developed in cooperation with the City of Portland. The University provided \$500,000 to develop the two parking lots and shuttle waiting station in return for use of the lot for 15 years. The City of Portland has the option to buy out the University's interest in the lot at the end of ten years. In anticipation of a train station in the area of the Marginal Way lot, the City indicated its intention to exercise this ten year option as of the spring of 2002. However, subsequent planning regarding the train found that the Marginal Way lot would not be impacted. The University and the City of Portland have reached an agreement under which University use of the Marginal Way lot will continue over the original fifteen year period.

### Campus Shuttle Systems

The University of Southern Maine provides three shuttle bus services. One such service runs between the Portland and Gorham campuses, with one bus leaving each campus at 45 minute intervals from about 7:15 a.m. to 10:00 p.m. During the 2000/01 academic year, this shuttle service provided 78,336 riders with transportation between the two campuses. Exhibit VI, page 1 shows time schedule for this shuttle service during the fall, 2001 semester and Exhibit VI, page 2 indicates the service times on Saturday for the Gorham and Portland shuttle, and the Monday through Friday service times for the separate and distinct shuttle service provided between Portland Hall and the Portland campus. During the 2000/01 academic year, there were 24,799 riders who took the Portland Hall/Portland campus shuttle service. The third shuttle system runs between Marginal Way parking lot and the Portland campus.

Between the Portland-Gorham and Portland Hall-Portland campus shuttle systems, over 100,000 passengers were transported during the 2000/01 academic year. Clearly, the provision of these shuttle systems reduces the 'single vehicle' impact on the Portland campus. The new class schedule implemented in the fall of 2001 allows 45 minutes travel time between the two

SHUTTLE BUS SCHEDULE  
EFFECTIVE SEPTEMBER 4, 2001  
MONDAY THROUGH FRIDAY

There will be one bus per run in each direction. If all seats are filled, standing will be allowed.

LEAVES PORTLAND

7:15 a.m.  
8:00  
8:45  
9:30  
10:15  
11:00  
11:45  
12:30 p.m.  
1:15  
2:00  
2:45  
3:25  
4:10  
4:50  
5:35  
6:15  
7:00  
8:45(via Mall Mon-Fri)  
10:00 (Mon-Thurs)

LEAVES GORHAM

7:15 a.m.  
8:00  
8:45  
9:30  
10:15  
11:00  
11:45  
12:30 p.m.  
1:15  
2:00  
2:45  
3:25  
4:10  
4:50  
5:35  
6:15  
7:00  
8:15 (via Mall Mon-Fri)  
9:35 (Mon-Thurs)  
10:20 (Mon-Thurs)

**BUS SCHEDULES ARE SUBJECT TO CHANGE EACH SEMESTER AND DURING FINAL EXAMS.**



PORTLAND HALL SHUTTLE BUS SCHEDULE  
PORTLAND HALL RESIDENTS  
EFFECTIVE SEPTEMBER 4, 2001

LEAVES PORTLAND HALL

LEAVES CAMPUS CENTER

7:00a.m.	7:30a.m.
7:45	8:00
8:15	8:30
8:45	9:00
9:15	9:30
9:45	10:00
10:15	10:30
10:45	11:00
11:45	12:00noon
12:15p.m.	12:30p.m.
12:45	1:00
1:15	1:30 (last run on Fri)
1:45 (last run on Fri)	2:00
2:15	2:30
3:45	4:00
4:15	4:30
4:45	5:00
5:15	5:30
5:45	6:00
6:15	6:30
6:45	7:00
7:15	7:30
7:45	8:00 *also serves Marginal Way
8:15 *also serves Marginal Way	8:30 *
8:45 *	9:00 *
9:15 *	9:30 *
9:45 *	10:05 *

No Runs on Saturday and Sunday. No Runs to 68 High St.

**NOTE:** If the shuttle is full, there will not be another shuttle until the next scheduled departure.

SATURDAY MINIBUS SCHEDULE  
EFFECTIVE SEPTEMBER 4, 2001

LEAVES GORHAM

LEAVES PORTLAND

8:30am (no Mall Stop)	9:00am (no Mall Stop)
9:30	10:30
11:30	12:30pm
2:00pm	3:00
4:00	5:00
6:00	7:00

**NOTE: All Saturday Trips, except 8:30am & 9:00am, Stop at the Maine Mall.**  
 Bus service is provided during the fall and spring semesters when classes are in session. All bus schedules are reviewed by Student Life and the Registrar's Office. Copies are available at the USM Police Department. Please call USM Parking and Transportation at 780-5254, with any questions or concerns regarding transportation services.



campuses. Previously, there was 30 minutes travel time between the campuses which was difficult to maintain because of increasing traffic congestion and occasional inclement weather. This new Portland-Gorham time schedule has resulted in increased bus ridership during the first five weeks of the fall, 2001 semester as students have greater assurance of being able to be on-time for classes

### Contractor Parking

As work on large construction projects is carried out at the Portland campus, construction agreements require that contractor and sub-contractor employees park at the Marginal Way lot and be transported to their campus worksites. This provision was included in the construction management contract for work to renovate the Portland Science Building for Research & Development efforts and is included in the contract that will be in effect for construction of the R&D Biosciences Addition to the Portland Science Building.

Construction of the Biosciences Research Institute Addition to the Portland Science Building will cause much of the current parking lot between the Science Building and the Sullivan Gym (approximately 104 parking spaces) to be used for construction and staging. Currently, this is a parking lot designated for faculty/staff parking. The lot in front of the Woodbury Campus Center which is currently designated for students only will be opened to faculty and staff during the construction period. Also, during this construction period, Marginal Way and Baxter Boulevard will be available as alternative parking sites for the academic year. In order to have people use Marginal Way and Baxter Boulevard as alternatives, there will be better designation and publicity about where the Marginal Way shuttle bus stops are located on the campus and about the location of a shuttle bus stop being instituted at Baxter Boulevard. Also, there will be a more clearly delineated time schedule for the Marginal Way/Baxter Boulevard shuttle and better maintenance of the time schedule to instill confidence in the service on the part of users. Once the construction phase of the Biosciences Addition is completed, the lot between the Science Building and the Sullivan Gymnasium will be returned to use, although there will be a loss of 44 parking spaces due to the Addition.

### METRO Cooperative Programs

In late summer of 2000, the METRO began the "FAST Service on Forest Avenue" which provided METRO service every fifteen minutes along Forest Avenue from Pride's Corner in Westbrook to Downtown Portland. This "FAST Service" significantly improved the ability of folks living near or along bus rout #2 (as well as those who might use the Park and Ride lot at Pride's Corner) to get to the Portland campus. The University was involved with the group that helped put together the funding proposal for this service. In addition, for the 2000/01 and 2001/02 academic years, we are cooperating with METRO on the "50¢ Fares for USM, Anytime, Anywhere" program. In this program, anyone with a USM photo ID or USM ID card can ride the METRO for 50¢, or half the regular fare.

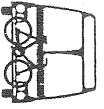
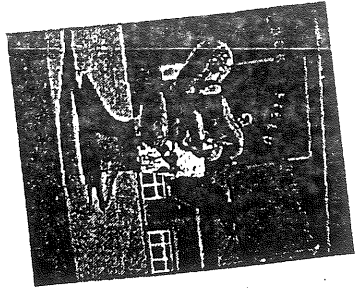
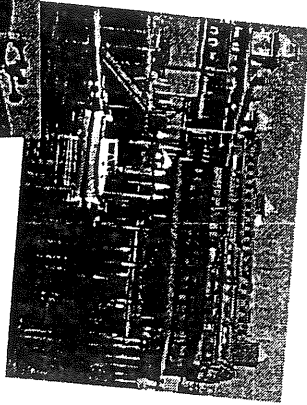
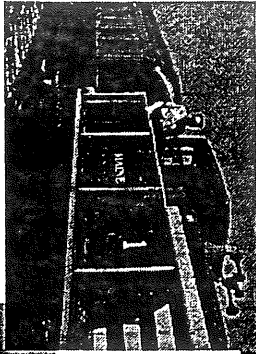
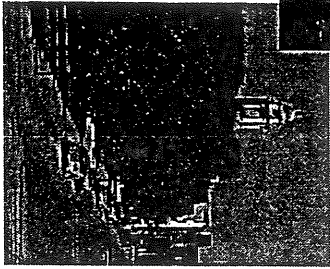
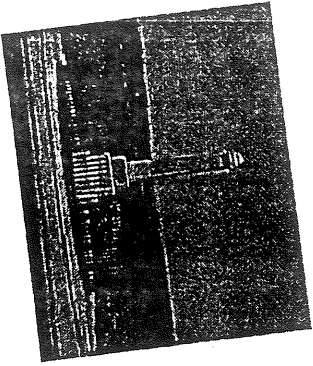
# USM Discovers Portland by METRO bus.

What are the METRO routes and places of interest along the routes?

- 1** **Route 1 / Congress Street**  
Monument Square; Old Port; Eastern Promenade; Portland Observatory; Portland City Hall; United Way; Longfellow House; Vermont Transit Lines; Arts District; The Portland Museum of Art; Children's Museum of Maine; Portland Public Market; the METRO PULSE; Farmer's Market
- 2** **Route 2 / Riverton**  
University of Southern Maine; Planned Parenthood; American Red Cross
- 3** **Route 3 / North Deering**  
Westgate Shopping Center; Evergreen Cemetery; Baxter Woods; Shaw's
- 4** **Route 4 / Westbrook - Exit 8**  
University of Southern Maine; Brighton Medical Center; Pine Tree Shopping Center; Fore River Sanctuary; Shop 'n Save; Shaw's
- 5** **Route 5 / Maine Mall**  
Maine Mall; Deering Oaks Park; Hadlock Field (Sea Dogs Baseball); Fore River Sanctuary; Portland Jetport; Concord Trailways
- 6** **Route 6 / North Deering**  
Northgate Shopping Center; Bureau of Motor Vehicles; Shaw's
- 8** **Route 8 / Pine Street**  
Maine Medical Center; Western Promenade; Casco Bay Lines; Narrow Gauge RR; Victoria Mansion; Shop 'n' Save; Back Cove Trail; Eastern Prom Trail

## METRO Bus Fares

**USM:** Special 50 cent fare when you show a USM ID card or a current USM ID registration card to a METRO driver. Ride any route - anytime, anywhere for 50 cents. Open to students, faculty, staff, and USM employees showing USM ID card.



# METRO

For information, bus schedule and routing — call METRO at 774-0351

A

1

# FAST Service on Forest Avenue

## 50¢ Fare

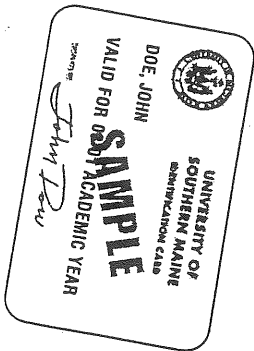
### Anytime. Anywhere.

If you attend USM at the Portland Campus or at 68 High Street, we'll make your weekday commute between Forest Avenue and downtown Portland a little easier. METRO #2 bus will provide service **every 15 minutes** along Forest Avenue, Monday thru Friday, for your school and work commute, shopping trips, short errands, medical appointments, and other intown and crosstown trips.

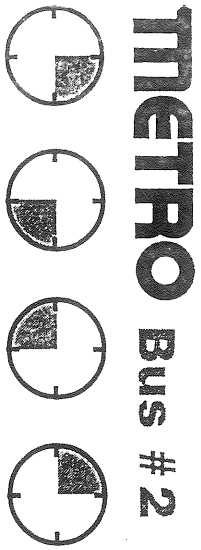
**With a USM Photo ID card and/or a current USM Registration card, the fare is only 50¢!**

Ride in brand new buses. Use the bicycle racks on each bus to combine bike and bus commuting. Visit our exciting, electronic touchscreen SMART Kiosks at the METRO PULSE (Elm Street Garage) and at the Woodbury Campus Center (Bedford Street) for bus, area transportation, and tourism information.

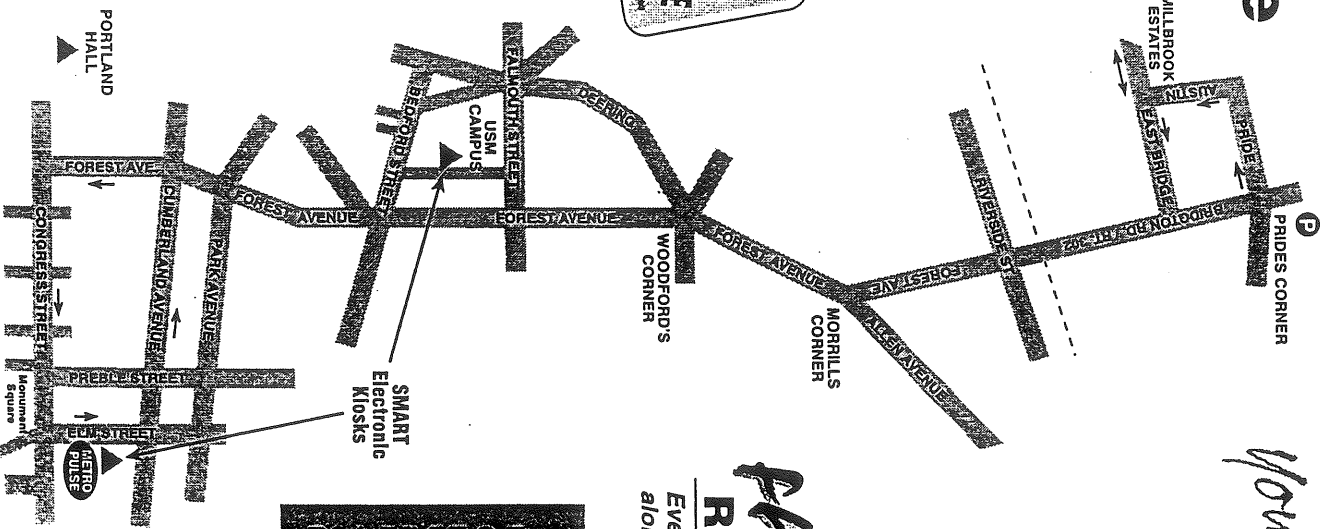
For Information, bus schedules and routing — call METRO at **774-0351**



OR



Every 15 minutes daily along Forest Avenue!



**FAST**  
Route 2  
Every 15 minutes  
along Forest Ave.

**P** Park in Ride/lot  
Commuters can park their cars at the Park in Ride/lot after four seasons. Biking located at the Elmwood Ave. off Route 202 near Fitness Center.

Note: Map not drawn to scale.

**METRO**  
*your city bus*

# USM Discovers Portland by METRO bus.

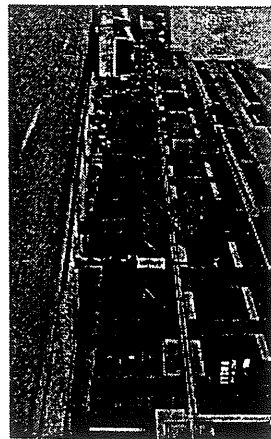
## METRO Buses to the USM Portland Campus

METRO bus #2 and #4 stop conveniently near the USM Portland campus. The #2 bus runs every 15 minutes on Forest Avenue during the week from 6:15 am to 7:15 pm. The #4 runs every 30 minutes. If you are connecting buses from downtown Portland, transfer to the #2 or #4 bus at the METRO PULSE located at the Elm Street Garage. Transfers are free.

- **Downtown Portland to Portland Campus**  
If you live in Portland and/or Westbrook, catch a bus to the METRO PULSE at the Elm Street Garage and transfer to the #2 or #4 bus. Exit the #2 bus on Forest Avenue by Back Bay Bicycles (*across from Pier One*) and walk up Bedford Street to USM. Or exit the #4 bus on Bedford Street.
- **Campus to Downtown Portland**  
Catch the #2 bus in front of Pier One and ask the driver for the PULSE stop if you need to change buses. Or catch the #4 at the Bedford Street bus shelter (*near The USM Policing Building*) and ask for the PULSE and/or your downtown stop.
- **Gorham Campus to Downtown Portland**  
Take the VIP bus (*METRO is a separate bus service*) from Gorham to the Portland Campus. Catch METRO #2 bus on Forest Avenue or METRO #4 bus at the Bedford Street bus shelter to the PULSE and/or your downtown stop.
- **Downtown Portland to Gorham Campus**  
Take bus #2 bus at the PULSE to the Portland Campus, exit at Back Bay Bicycle, and then walk to USM. Or take METRO #4 bus from the PULSE and exit on Bedford Street near the campus. The VIP bus to Gorham is parked in front of the Student Center.
- **Portland Hall to Campus**  
Take METRO bus #1 or #3 across from Portland Hall and ask the driver for a transfer and the PULSE stop. Or walk to the PULSE and wait for the #2 or #4 bus.
- **Campus to Portland Hall**  
Catch the #2 (*in front of Pier One*) or #4 bus (*bus shelter on Bedford St.*) and exit at the corner of Forest and Congress or ask the driver for the Portland Hall stop. Cross High Street and walk to Portland Hall.

## What is the METRO PULSE?

For buses or change of buses #2, #3, #4, #5, and #6, go to the METRO PULSE, located at the Elm Street Garage between Congress Street and Cumberland Avenue (*across from the Public Library*). In the Garage, you can also purchase METRO Passes and speak to a bus dispatcher at the Sales Office.



## METRO Bus 'n Buy

Ride home free when you buy at Burger King, Maine Mall, Shop 'n Save, Shaw's, The Whole Grocer, Portland Public Market, Borders, Annie's Book Stop, Just Maine Made, Harbor Fish, and Levinsky's. Call 774-0351 and ask for Bus 'n Buy details.

## METRO Bicycle Racks – Your Bike Rides Free!

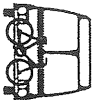
Each METRO bus is equipped with a rack that carries two bicycles.

## METRO Park 'n Ride – Your Car Parks for Free!

USM Commuters can park their car at the Park 'n Ride Lot at Four Seasons Bingo located at 33 Elmwood Ave. off Route 302 near Prides Corner. From the lot, catch the #2 bus every 15 minutes to USM and exit at Pier One. Bus trip from lot to Pier One is less than 20 minutes.

## Carpool and Save Money – call Rideshare

There are over 1,000 car commuters who want to carpool. You drive... they drive... you decide. Call 775-7433 or 1-800-280-7433 for information.



# METRO

For information, bus schedule and routing —  
call METRO at **774-0351**

# Leave your car at home... OR on campus.

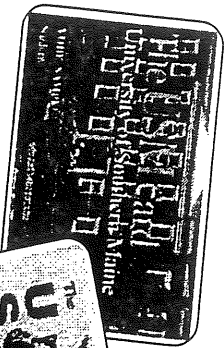
Experience the wonder and exciting attractions of Portland. The METRO makes convenient stops at over 800 locations including retail shops, restaurants, museums, and historic landmarks in downtown Portland, the Old Port, the Maine Mall, and Westbrook.

## METRO Bus Fares

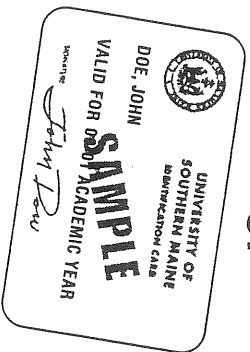
**USM:** Special 50 cent fare when you show a USM ID card or a current USM ID registration card to a METRO driver. Ride any route – anytime, anywhere for 50 cents. Open to students, faculty, staff, and USM employees showing USM ID card.



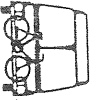
+



OR



- Regular Fare:** \$1.00 (exact change)  
— driver cannot make change
- Monthly Pass:** \$30.00 for unlimited rides for month purchased
- TenRide Ticket:** 10 rides with no time limit
- Seniors over 65:** 50 cents with Medicare Card
- Children Under 5:** two children ride free with a fare paying adult



# METRO

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**USM:** Special 50 cent fare when you show a USM ID card or a current USM ID registration card to a METRO driver. Ride any route – anytime, anywhere for 50 cents. Open to students, faculty, staff, and USM employees showing USM ID card.

## METRO Buses to the USM Portland Campus

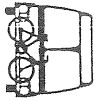
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# METRO

For information, bus schedule and routing –  
call METRO at **774-0351**

#5

To promote these programs, the University has provided up to \$5,000 to METRO to be used to purchase display advertising in the FREE PRESS during the academic year. Exhibit VII shows five of the advertisements appearing in the FREE PRESS. USM has also provided \$6,000 toward the cost of printed materials including informational brochures, posters, and other educational outreach materials about the reduced fare and Forest Avenue programs and about the benefits of using public transportation rather than one's own vehicle. Exhibit VIII shows METRO brochure distributed to USM students and staff which outlines the convenience of using the METRO service and Exhibit IX is an 8 ½ X 11 example of a large poster that has been placed in campus buildings and residence halls, encouraging use of METRO. Exhibit X are memos from Philip Chin, Director of Marketing for METRO which outline ridership in the "Free Ride" program that was in effect during 2000 and the "50 - cent ride" program that began in September of 2000. During the March, 2000 to July, 2001 period, 27,621 USM students and staff took advantage of the cooperative USM/METRO program. As noted in the 11/1/01 memo, METRO usage through the cooperative program is up 22% for September and October. These efforts with METRO help to make the USM community aware of the METRO services that facilitate an easy commute to the Portland campus.

### Street Parking Enforcement

The University of Southern Maine fully supports efforts by the City of Portland to enforce parking rules and regulations on the streets surrounding the Portland campus. It encourages the City to provide the level of enforcement needed in the area and will continue to work with the City regarding parking on City streets. As the result of meetings with campus neighbors, one hour parking was instituted on Chamberlain Avenue. A residential parking sticker program was instituted for that Street as well as in the other neighborhood areas near the Portland campus. In addition, the University supports the installation of two hour meters on the University side of Falmouth Street to encourage turnover of parking spaces in that area. In the past, USM has worked with the City of Portland and neighbors regarding changes in on-street parking in the vicinity of the University and will continue to do so.

### Conclusion

The University of Southern Maine is committed to effectively managing parking at the Portland campus. While the Biosciences Addition to the Portland Science Building is focused on the Research & Development initiatives mandated by the Maine Legislature and will result in no new student enrollments or teaching space, the effective management of USM's parking resources will result in less impact on campus neighbors. This plan outlines the following measures:

- Continue to evaluate the class schedule as was done for the fall, 2001 semester which resulted in a lessening of peak parking demand, particularly on Tuesdays and Thursdays.
- Constantly review the course offerings and their times to attempt to spread the offerings throughout the day and evening periods.







August 30, 2001

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program

For your posting the following is ridership for the program:

Year 2000			
March	1,234		
April	2,024		
May	2,227		
June	1,893		
July	1,909		
August	2,059		
<b>Cumulative Free Rides:</b>		<b>11,346</b>	
September	1,313	February '01	1,459
October	1,616	March '01	1,567
November	1,588	April '01	1,587
December	1,504	May '01	1,525
January '01	1,734	June '01	1,253
<b>Cumulative 50-Cent Rides:</b>		<b>16,275</b>	
<b>Total All Rides (March '00-July '01): 27,621</b>			



November 1, 2001

To: Sam Andrews, USM

Fr: Philip Chin, Director of Marketing

Re: USM/METRO 50-CENT FARE Program

With two months into the new academic year, the USM 50-cent METRO fare continues to show steady inroads.

As summarized below, comparative September ridership increased by 398 riders or 30% while October showed an increase of 262 riders or 16%. The combined two months show an increase of 660 riders or a 22% gain vs last year. We thank you and the University on your ongoing, continued support of the program.

	Year 2000	Year 2001	rider increase change	% change
January		1,734		
February		1,459		
March		1,567		
April		1,587		
May		1,525		
June		1,253		
July		1,129		
August		1,272		
<b>September</b>	<b>1,313</b>	<b>1,711</b>	<b>+398</b>	<b>+30%</b>
<b>October</b>	<b>1,616</b>	<b>1,878</b>	<b>+262</b>	<b>+16%</b>
<b>Year to Date (Sept-Oct)</b>			<b>+600</b>	<b>+22%</b>
November	1,588			
December	1,504			

- Continue efforts to utilize low demand parking times for a new initiatives such as was done with Weekend College (over 1,000 students enrolled) and the Osher Lifelong Learning Institute (Senior College) which has approximately 500 enrollees in Friday offerings.
- Maximize campus parking resources through:
  1. Continual assessment of parking supply and usage among staff, student, handicapped and metered spaces.
  2. Increase parking decal fees (a \$10 increase for the fall, 2001 semester) to provide additional funds for parking improvements and enforcement.
  3. Providing user education through “Motor Vehicle Rules” distribution to decal purchasers; campus map distribution indicating locations of campus parking; campus newspaper, building poster and notice postings; and use of the University e-mail list serve.
- Continue cooperative efforts with METRO including funding of efforts to acquaint staff/students with METRO services/conveniences; continuation of the “50-Cent Ride Anywhere” or a similar program; and the Forest Avenue program.
- Continue cooperative efforts with the Greater Portland Council of Governments including participation in its Ride-Share program and utilization of its transportation consulting services.
- Provide an appropriate patrol level in USM parking areas to assure safety of parkers and enforcement of USM parking regulations.
- Continue the no fee for marginal Way lot users, which has resulted in a 25% increase in usage, together with increased educational publicity regarding the availability of Marginal Way, and continue to have campus activities/events have their participants utilize Marginal Way.
- Assess and refine campus shuttle systems between Portland and Gorham campuses and between Portland Hall and the Portland campus to continue to provide students with campus access without use of their own vehicles.
- Require contractors on building projects, such as the Biosciences Addition, to have workers park at off-site locations.
- Work with the City of Portland and neighbors on steps to ease neighbor concerns about parking around the campus, such as the recently implemented changes on Chamberlain Avenue and the proposed meter installation on the University side of Falmouth Street.

DeLuca - Hoffman Associates, Inc  
 Saco Office  
 Phone: 207-286-8417  
 Fax: 207-286-3220

RECEIVED  
 SEP 8 2001  
 SYMMES MAIN & MCKEE ASSOCIATES, INC.

MF ORIGINAL  
 Proj. No. 00013.00 File \_\_\_\_\_  
 Proj. USM - PORTLAND  
 Circ. (1)  
 Copies JB (MF)

Fax

To: JOE BURKE From: JOE L.  
 Fax: 1-617-354-5758 Pages Including Cover: 2  
 Phone : \_\_\_\_\_ Date: 7-18-01  
 Re: \_\_\_\_\_ JN#: \_\_\_\_\_

For Review  Please Comment  Please Reply

● Comments:

JOE -  
 WE SHOULD PLAN TO RESUBMIT RESPONSE  
 TO SARAH BY END OF WEEK.  
 CALL ME.  
 THANKS  
 JL



22 Free Street, Portland, Maine 04101-3900 . Tel: 207.775.5211 . Fax: 207.775.6434 . E-mail: dhmaine@agass.net

September 12, 2001

Ms. Sarah Greene Hopkins, Senior Planner  
Planning & Urban Development  
389 Congress Street  
Portland, Maine 04101

RE: University of Southern Maine Bioscience Institute Development

Dear Sarah:

We have reviewed the revised University of Maine System's application for the Proposed Bioscience Institute Development. It appears most of the initial comments were addressed. There are a few additional items that are summarized below.

- The applicant has added a storm water quality unit to the plans, however, under current MDEP criteria, there is only one approved water quality unit (Vortechics). In addition, there is a discrepancy between 63% removal efficiency noted in the response letter and the 50% removal efficiency noted in Attachment B.
- The plans should be stamped by a Maine Registered professional Engineer.

Please contact me if we can be of any further assistance.

Respectfully submitted,

DUFRESNE-HENRY, INC.

Jeffrey D. Preble, P.E.  
Senior Project Manager

C:\Projects\8160054\USM Bio Sciences Bldg\Comment Letter 9-12-01.wpd



Symmes Maini & McKee Associates

LETTER OF TRANSMITTAL

CAMBRIDGE, MA • MINNEAPOLIS, MN • ST. PAUL, MN

Project #: 00013.00

Date: 11/01/01

Project: USM – Portland/ Bioscience Research Facility  
 To: Planning and Urban Development  
 389 Congress Street  
 Portland, ME 04104  
 Attention: Sarah Hopkins, Senior Planner  
 Regarding: Site Plan Review

THESE ARE TRANSMITTED AS CHECKED BELOW:

- Attached       Under separate cover via      **FEDEX-OVERNIGHT**
- Shop Drawings       Plans       Specifications       Samples
- Copy of letter       Change Order       Diskettes       Other: \_\_\_\_\_

COPIES	DATE	NO.	DESCRIPTION
15	10/31/01		Site Plan Documents
1	-		Letter from Dufresne-Henry, Inc.
1			Response letter from SMMA
15			Elevations (11"x17")


THESE ARE TRANSMITTED AS CHECKED BELOW:

REMARKS: **PLEASE ACCEPT THE ATTACHED PLANTING PLAN FOR YOUR REVIEW. PLEASE CALL CONTACT ME WITH YOUR COMMENTS AT 617-520-9451. THANK YOU.**

- For approval       Approved as submitted       Resubmit      \_\_\_\_\_ copies for approval
- For your use       Approved as noted       Submit      \_\_\_\_\_ copies for distribution
- As requested       Returned for corrections       Return      \_\_\_\_\_ corrected prints
- For record       Other: \_\_\_\_\_
- FOR BIDS DUE ON: \_\_\_\_\_       PRINTS RETURNED AFTER LOAN TO US

COPIES TO: JAS, LG, JB, MJZ, (MF),  
**Transmittal only**

SIGNED:

  
 Joseph P. Burke, PE

If enclosures are not as noted, kindly notify us at once.

J:\BP\PROJECTS\2000\00013\design\transmittals\c-lot-hopkins2\doc



DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

778 MAIN STREET  
SUITE 8  
SOUTH PORTLAND, MAINE 04106  
TEL. 207 775 1121  
FAX 207 879 0896

- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- TRAFFIC STUDIES AND MANAGEMENT
- PERMITTING
- AIRPORT ENGINEERING
- SITE PLANNING
- CONSTRUCTION ADMINISTRATION

September 7, 2001

Ms. Sarah Hopkins  
Planning Division  
Portland City Hall  
389 Congress Street  
Portland, Maine 04101

**RE: USM – Bio-Science Building Project 114A-A-001  
Parking Requirements**

Dear Sarah:

We have reevaluated the parking requirements for the proposed Bio-Science building in response to Marge Schmuckal's letter of September 4, 2001. We had included on-street parking spaces in our parking supply as has been done historically for the roads in the immediate vicinity of the campus. However, Marge has indicated that the specific ordinance requirement is for off-street spaces and we cannot include the 179 on-street spaces in our supply.

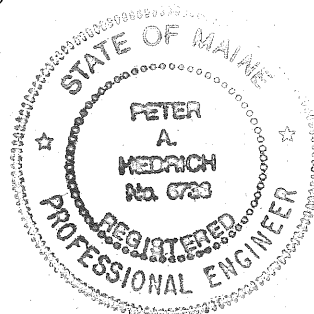
Marge also indicated that the ordinance allows the Planning Board to consider joint use of parking as described in section 14-343. We have previously conducted parking studies and surveys to assess joint use of parking on the campus as summarized in the attached document. The joint use parking analysis indicates that once the Bio-Science building is constructed for phase 1 (floors 1 & 2 only), the campus will have a surplus of three off-street parking spaces. The 179 on-street parking spaces will serve as an additional buffer to the 3-space surplus.

Please review the attached "Joint Use Parking Discussion" and contact me with any questions you may have.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.

Peter A. Hedrich, P.E., PTOE  
Senior Engineer



PAH/sq/JN2074/Hopkins9-7

Attachment

c: David Early                      Sam Andrews                      Joe Laverriere



**University of Southern Maine**  
**Joint Use Parking Discussion**  
**September 7, 2001**

The USM Portland Campus includes a variety of uses to serve the needs of the student population. We would expect that these uses such as classroom space, library space, lounge areas, food facilities and merchandising provide students with the opportunity to participate in several activities on each visit to the campus. Therefore, a student will occupy a parking space to attend classes and then utilize one or more of the complementary facilities. Given these occurrences, the overall parking demand for the campus would not be a summation of the individual demand for each facility, but some lesser amount depending upon the frequency of these joint use visits.

To determine the joint use parking demand, DeLuca-Hoffman Associates, Inc. performed parking demand counts on the campus and surrounding street system. The results of these counts that were performed in the fall of 2000 are summarized in the DeLuca-Hoffman Associates, Inc. report "Final USM Portland Campus Parking and Traffic Study for Campus Master Plan" dated December 2000 and further reported in the DeLuca-Hoffman Associates, Inc. letter dated June 20, 2001. This letter indicates a total campus parking demand on the busiest day of the week to be 1,517 spaces. For comparison purposes, the parking demand for individual uses was determined to be 1,625 spaces based on City Ordinance. The attached summary shows that the most likely candidates for shared uses such as the library, lounge, food facility, and merchandising spaces indicate a total parking demand of 218 spaces, which is roughly double the difference (1625-1517=108) in actual versus ordinance parking demand. Therefore, approximately 50% of the users of these facilities may be on campus for other uses. To confirm this shared use premise, DeLuca-Hoffman Associates, Inc. performed a survey of random students entering/leaving the library and Luther Bonney Hall on May 3, 2001 from approximately 9 AM to 5 PM. This survey indicated that approximately 66% of students utilizing those buildings were going to visit another use on campus on that day. This reinforces the concept that shared use of parking is occurring on the campus.

Given the survey results, we are comfortable that the field count of 1517 occupied spaces fairly represents the actual parking demand for the existing facilities and class schedules for the fall 2000 schedule. The University has implemented schedule changes and parking incentives to balance parking demands that may further reduce the actual parking demands. To determine the parking requirement after construction of the Bio Science Building, we have determined the assignable space and associated ordinance requirements for this building without joint use adjustments. The construction will result in the loss of 44 spaces on the Bio Science lot. The joint use versus ordinance requirements are summarized in the following table:

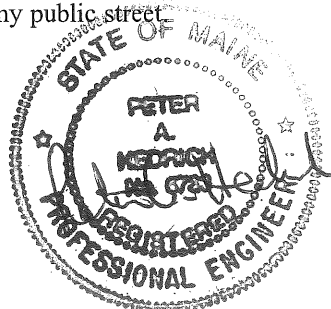
<b>USM Portland Campus, Parking Summary</b>					
<b>Parking Requirements Based On:</b>	<b>Parking Req. to Meet Existing Demand</b>	<b>Parking Req. for Proposed Bio Science**</b>	<b>Total Required Parking</b>	<b>Proposed Supply ***</b>	<b>Req. New Parking Spaces</b>
<b>Joint Use</b>	1,517* (Includes 179 On Street)	34	1,551	1,554	(3)
<b>City Ordinance</b>	1,625	34	1,659	1,554	105

\* This is the demand for the busiest hour of the day on the busiest day of the week (1338 in USM lots plus 179 on street).

\*\* Ordinance: 12,940 sf office @ 1/400 sf=33 spaces plus 1,000 sf storage @ 1/1,000=1 space

\*\*\* Includes on campus (1,200), outbuildings (38), Grand Street (12), Baxter Blvd. (48), Marginal Way (300) and loss of 44 spaces for construction of the Bio Science Building

The above table indicates a surplus of three spaces based on the shared use assumption for parking demand. This does not include any consideration of use of on street parking on any public street.



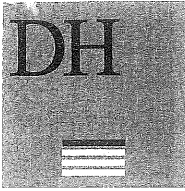
9-7-01

Portland Parking Requirement  
per Square Footage  
2001

6/6/01  
10:09 AM

Required  
Spaces

<u>Type of Facility</u>	<u>Area (s.f.) 2001</u>	<u>Parking Factor</u>	
General purpose Classroom	25,059	1.0/100 s.f.	251
Lecture room	11,945	1.0/100 s.f.	119
Seminar	3,541	1.0/100 s.f.	35
Labs	2 employees	1.0/room	2
Class Labs	26,095	1.0/100 s.f.	261
Class Labs svc	2,554	1.0/400 s.f.	6
Day Care	3,203	20 employees	20
Indivial Study Lab	2,087	1.0/100 s.f.	21
Office	100,350	1.0/400 s.f.	251
Office services	6,579	1.0/400 s.f.	16
Conference Room	1,523	1.0/400 s.f.	4
Library Reading Study	33,232	1.0/400 s.f.	83
Library Open Stack Reading Rm.	5,249	1.0/400 s.f.	13
Stack area	74,511	1.0/1000 s.f.	75
Library Process Room	11,419	1.0/400 s.f.	29
Physical Education	19,971	1.0/160 s.f.	125
ITV High tech class	2,472	1.0/400 s.f.	6
Lounge	6,341	1.0/160 s.f.	40
Amphitheater	40 (seats)	1.0/5 seats	8
Food Facilities	6,717	1.0/100 s.f.	67
Merchandising	5,364	1.0/150 s.f.	36
Recreation	15,930	1.0/400 s.f.	40
Student Health	1,480	1.0/500 s.f.	3
Computer Room	2,015	1.0/400 s.f.	5
Computer Center	7,249	1.0/400 s.f.	18
Shop	10,036	1.0/400 s.f.	25
General Storage	23,026	1.0/400 s.f.	58
Grounds Department	9,006	1.0/1000 s.f.	9
<b>Totals</b>	<b>416,954</b>	<b>(Net SF)</b>	<b>1,625</b>



DeLUCA-HOFFMAN ASSOCIATES, INC.  
CONSULTING ENGINEERS

778 MAIN STREET  
SUITE 8  
SOUTH PORTLAND, MAINE 04106  
TEL. 207 775 1121  
FAX 207 879 0896

- ▣ ROADWAY DESIGN
- ▣ ENVIRONMENTAL ENGINEERING
- ▣ TRAFFIC STUDIES AND MANAGEMENT
- ▣ PERMITTING
- ▣ AIRPORT ENGINEERING
- ▣ SITE PLANNING
- ▣ CONSTRUCTION ADMINISTRATION

April 11, 2001

Ms. Sarah Hopkins, Senior Planner  
Planning and Urban Development  
389 Congress Street  
Portland, ME 04101

**Subject: USM Portland Campus  
Bioscience Building Expansion  
Response to Engineering Review Comments**

Dear Sarah:

Our office along with Symmes, Maini & McKee Associates and the University of Southern Maine have received and reviewed the engineering review comments prepared by Anthony Lombardo (dated February 5, 2001) from the City's Engineering Department and peer review comments prepared by Jeffrey Prebble of Dufresne-Henry (dated January 5, 2001). A summary of each comment and response is provided below:

A. Anthony Lombardo Comment Memo:

Comment 1:

*Public Works is requesting the applicant install a new drain manhole structure in the location where the on site storm drain system connects in the combined sewer system, within the Falmouth Street right of way. Currently, a 10" diameter pipe outfalls the site and connects directly into the combined sewer. A manhole structure will enhance the ability of the City to maintain and repair the system in Falmouth Street.*

Response:

The site plans for the Bioscience Building Expansion have been revised to include the installation of a drain manhole structure as requested (refer to Sheet C4 attached).

Comment 2:

*Public Works is also requesting the applicant specify, as part of the "erosion and sediment control plan", that the site contractor shall clean the entire on site storm drain system, including the sumps of all catch basins. The timing of this work should coincide with the paving of the parking lot.*

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 2

Response:

The erosion and sedimentation control narrative has been revised to specify the cleaning of the onsite storm drain system within the Bioscience Building Expansion project area to be completed as requested. In addition, a special construction note has also been added to Sheet C4.

Comment 3:

*The applicant should contact Carol Merrit of Public Works, prior to beginning of construction, to determine all of the mandatory permits and fees associated with construction activity within the City right of way.*

Response:

The applicant or assigned agent will contact the City prior to construction to ensure all permits and fees are obtained for construction work within the City's right-of-way.

B. Dufresne-Henry Comment Letter

Comment 1:

*Erosion and sedimentation control measures are limited on this project and involve primarily hay bale barriers around catch basins. The parking area adjacent to the building addition will be significantly altered, with a significant amount of materials being moved into and out of the site and onto Falmouth Street. There are no measures planned for a stabilized construction entrance to the site, therefore materials could be transported onto Falmouth Street. Additional safeguards against erosion and sedimentation should be provided around the site. Seeding and mulching should be specified for disturbed areas and a schedule for stabilizing areas once they reach final grades. An implementation schedule should also be provided.*

Response:

The erosion and sedimentation control plan and narrative contained in Section 24 of the MeDEP Site Location Permit Application has been revised to include the following:

- Temporary erosion control measures including hay bales, catch basin inlet sediment sump, stabilized construction entrance, water quality treatment unit, etc.
- Permanent erosion control measures including seeding, landscape vegetation/bark mix, etc. (Refer to Sheet C5).
- Timing and sequence of construction.
- Implementation schedule of erosion control measures.

Refer to Attachment A and revised site plans.

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 3

Comment 2:

*We agree that the proposed development will not significantly alter storm water runoff from the site. However, storm water quality issues have not been addressed as part of the application. There does not appear to be any storm water quality measures presently around this site, and there are none proposed.*

Response:

The storm drain system associated with the Bioscience Institute project has been revised to include a Stormceptor water quality treatment unit designed to achieve 63% TSS removal efficiency for the project area. It should be noted, that the Stormceptor unit will treat the 0.7 acre watershed area associated with the Bioscience Institute parking area, as well as, an additional 1.0 acre watershed tributary to the storm drain system from the Masterton Hall area upgradient of the site.

The Stormwater Management Report, as prepared by Symmes Maini & McKee Associates, contained in Section 22 of the MeDEP Site Location Permit Application has been revised to summarize the water quality measures associated with this project. A copy of the revised Stormwater Management Report is contained in Attachment B of this letter.

Comment 3:

*Several utilities need to be modified to make room for the proposed building expansion. Test pits should be called for in key utility crossings to avoid potential conflicts during construction, and verify clearances. The proposed 6-inch sewer service is shown tying directly into an existing manhole, but no invert data is given. I believe the Public Works Department prefers that services be tied into the sewer main rather than manholes. A new 12-inch CPP is shown being tied into DHM-1 and runs underneath the new concrete seat. The concrete seat detail does not provide an overall depth. Clearances in this area need to be verified. Finally, confirmation letters are needed from Public Works on the sewer capacity and Portland Water District for the water services.*

Response:

The Grading and Utilities Plan has been revised to include a note requiring the contractor to conduct testpits at all utility crossings. In addition, the new sanitary sewer service connection has been revised to tie into the sewer main rather than the manhole.

The detail cross-referencing has been revised on Sheet C3 to correspond with the detail numbering on Sheet C6.

The depth of the concrete seat wall has been defined on Sheet C6. Inverts and elevations at these crossings have been identified on Sheet C4.

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 4

Copies of the revised site plans are appended to this letter. In addition, copies of the ability to serve letter from the Portland Water District is contained in Attachment C. At this point, the ability to serve letter from the City of Portland has not yet been received and will be provided separately upon receipt.

Comment 4:

*A solid waste dumpster is called for in Section 5 of the application. The location of the dumpster is not shown on the plans.*

Response:

There are individual containers within the building for disposal of solid waste/recycling items that are collected by the USM Department of Facilities and consolidated for hauling and disposal offsite.

An exterior dumpster is not proposed for this project. The reference in Section 5 to a dumpster has been removed and an updated copy of Section 5 is contained in Attachment D.

Comment 5:

*No information has been provided on lighting fixtures proposed for the site lighting on this project.*

Response:

Details on the exterior site lighting fixtures are contained in Attachment E.

Comment 6:

*A detail has not been provided for the proposed stonewall retaining walls. The granite curbing detail calls for a 6-inch reveal, City of Portland standard is 7 inches. Details 14 and 18, Concrete Retaining Wall, are redundant.*

Response:

A detail section through the stone retaining wall along Falmouth Street has been added to Sheet C6. The granite curb detail on Sheet C6 has been revised to reflect a 7-inch reveal in accordance with the City of Portland standards. In addition, the redundant concrete retaining wall (detail 16) on Sheet C6 has been eliminated. Also, a bituminous curb detail has been added to Sheet C6.

Comment 7:

*The cover letter states that the construction sequence is provided in Sections 1 and 24 of the application. There is a very brief discussion on Construction Plan in Section 1. The construction sequencing should be further defined.*

Ms. Sarah Hopkins, Senior Planner  
April 11, 2001  
Page 5

Response:

The construction sequencing has been provided in the revised Section 24 – Erosion and Sedimentation Control Report contained in Attachment A of this letter.

Our office along with representatives from Symmes, Maini & McKee Associates and the University of Southern Maine look forward to meeting with you and the members of staff and the Planning Board to discuss the project further.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.



Joseph A. Laverriere, P.E.  
Senior Engineer

JAL/mb/JN2074/Hopkins2-28

Enclosures

- Attachment A - Revised Section 24 – Erosion and Sedimentation Control Report
  - B - Revised Section 22 – Stormwater Management Report
  - C - Ability to Serve Letters from the Portland Water District
  - D - Revised Section 5 – Solid Waste
  - E - Exterior Site Lighting Details
- Revised Site Plans

c: Dave Early, P.E. – USM  
Joe Burke, P.E. - SMMA

**ATTACHMENT A**

**REVISED SECTION 24  
EROSION AND SEDIMENTATION CONTROL REPORT  
PREPARED BY SYMMES, MAINI & MCKEE ASSOCIATES**



---

**ATTACHMENT A**  
**SECTION 24**  
**EROSION AND SEDIMENT CONTROL**

**24.0 Overview**

The project site will require minimal erosion and sediment control measures. No wetlands are associated with the project. Therefore, no siltation fencing will be used on site. Haybales will be installed around existing and new catch basins to limit discharge of sediment into the existing storm sewer system. Haybales will also be installed at the limit of construction as shown on plan sheet C2. The existing parking lot entrance shall be used as a temporary construction entrance. Measures will be taken to stabilize the entrance throughout construction. Refer to detail on plan sheet C6. The site contractor shall clean the entire onsite storm drain system, including the sumps of all catch basins. The timing of this work shall coincide with the paving of the parking lot.

New catch basins will be equipped with deep sumps to collect sediment and oil traps. The upgraded drainage system will also include a water quality unit designed to achieve 50% TSS removal efficiency for the project area. Sufficient landscaping shall reduce erosion. Seeding and mulching is specified on the Landscape Plan sheet C5. Overall, site improvements associated with this project will enhance the quality of stormwater runoff being discharged into the Falmouth Street sewer.

**24.1 Description of Proposed Earthwork**

The following earthwork is proposed for the project:

1. Excavation of the basement level of the Bioscience building expansion.
2. Removal of parking lot pavement and earthwork to lower parking lot grades.
3. Excavation for the storm drainage system and utilities.
4. Final grading of parking lot and landscape areas.

**24.2 Timing and Sequence of Erosion and Sedimentation Control Measures**

The following construction sequence is proposed for the project:

1. Install crushed stone construction stabilization entrance.
2. Install haybales in locations shown on plan sheet C2 and other areas deemed necessary.
3. Remove and dispose of parking lot pavement and curbing.
4. Begin excavation of parking lot. Grading shall approach proposed subgrade.  
Excavated Soils generated from the earthwork will be removed from the site.  
The excavated parking lot will be designated as a staging area.
5. Begin excavation for Building basement and footings.
6. Install site retaining walls.
7. Begin Installation of site drainage and utilities.
8. Begin final parking lot grading and complete installation of drainage and utilities.

- 
9. Install offsite curbing and sidewalk.
  10. Install parking lot curbing and pavement.
  11. Complete final earthwork including side slopes.
  12. Loam seed and mulch disturbed areas and complete landscaping.
  13. Existing lawn areas designated to remain shall be aerated, fertilized and overseeded. In addition to areas defined for seeding on the plans, seed grass areas, which have been disturbed by the contractor.
  14. After site is stabilized, remove erosion control devices.

The above sequencing may change to ensure optimum erosion and sedimentation control. Several items may be completed simultaneously.

Work shall be conducted such that:

- a) Exposed area will be kept to a minimum.
- b) Disturbed area shall be re-vegetated as soon as possible.
- c) Drainage erosion control devices shall be installed as early as possible.

**ATTACHMENT B**

**REVISED SECTION 22  
STORMWATER MANAGEMENT REPORT  
PREPARED BY SYMMES, MAINI & MCKEE ASSOCIATES**

---

**ATTACHMENT B**  
**SECTION 22**  
**STORMWATER MANAGEMENT**

**22.0 Overview**

The project site has an existing stormwater drainage system. The onsite system collects runoff generated from building roof drainage, parking areas, landscape areas and sanitary sewer effluent generated within buildings on campus. The drainage tributary area consists of the Bioscience building, parking lot and surrounding area, and the Masterton Hall parking lot and surrounding area. The existing combined sewer system connects to a combined sewer system in Falmouth Street. The onsite drainage system consists of storm drain pipe, storm drain inlet structures and a detention basin.

Over time, the drainage system has been changed to accommodate the continuing expansion of the campus. The proposed science building expansion will require further changes to the drainage system. An existing storm drain will be relocated around the new building expansion and additional catch basins will be installed. The additional catch basins will include deep sumps and oil traps. The upgraded system will also include a water quality unit designed to achieve 50% TSS removal efficiency for the project area.

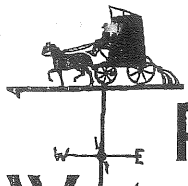
No changes will be made to the existing detention basin. The parking lot will be lowered to accommodate a new loading dock and basement. The tributary drainage area will generally remain the same. Impervious area will be reduced, which will reduce the volume of runoff generated within the development. The total impervious area within the limit of work is estimated as follows:

	Existing Conditions	Proposed Conditions
Impervious area	1.04 acres	0.91 acres

Ultimately, the proposed development is expected to reduce the total volume of runoff it discharges to the Falmouth Street combined sewer system.

ATTACHMENT C

**ABILITY TO SERVE LETTER FROM  
PORTLAND WATER DISTRICT**



# Portland Water District

225 Douglass St. • P.O. Box 3553 • Portland, ME 04104-3553

(207) 774-5961  
FAX (207) 761-8307  
www.pwd.org

December 29, 2000

Mr. John P. Burke, P.E.  
Symmes Maini & McKee Associates  
1000 Massachusetts Ave.  
Cambridge, Mass 02138

<b>MF ORIGINAL</b>
Proj. No. 00013.00 File _____
Proj. USM _____
City _____
By GPP, JB, JAS

Re: USM, Portland

Dear Mr. Burke:

The Portland Water District has an 8" water main in Falmouth Street, Portland, near the proposed site. A test on a nearby hydrant produced the following results: static pressure 77 psi; residual pressure 35 psi; with a flow of 993 gpm. With these results in mind, the District feels we have sufficient capacity available to serve this proposed project and meet all normal fire protection and domestic water service demands. **Please notify your plumber of these results so that they can design your system to best fit the available pressure.**

With certification by the developer that all required permits have been received, we look forward to serving this project.

Sincerely,

PORTLAND WATER DISTRICT

David W. Coffin, PLS  
Engineering Supervisor

RECEIVED

JAN 4 2001

SYMMES MAINI & MCKEE ASSOCIATES

**ATTACHMENT D**

**REVISED SECTION 5  
SOLID WASTE**

## SECTION 5

### SOLID WASTE

#### 5.0 Overview

The University of Maine System has existing provisions for handling solid wastes at the University of Southern Maine Portland Campus, which will be extended to the proposed Bioscience Institute.

The University is classified as a low generator of hazardous and special wastes. The firms under contract to handle hazardous and special wastes are as follows:

Firm	Type of Wastes
Stericycle	Biomedical Waste
Phillips Services	Hazardous Wastes
Phillips Services	Printing Shop
TSI	PCB Ballast

The Bioscience Institute is not anticipated to add any hazardous or special wastes to the current waste stream. The new Bioscience Institute is anticipated to generate 375 lbs. of biomedical waste on a monthly basis.

Waste Management of Maine is under contract with the University for collection and disposal of solid waste. Solid waste is taken to Regional Waste Systems where it is incinerated.

Last year the Portland Campus generated 650,725 pounds of solid waste including 34,900 pounds of construction/demolition debris. Of the total solid waste, 341,419 pounds or 52% was recycled. Regional Waste Systems incinerated 297,440 pounds. The remaining, unrecycled solid waste was hauled by Waste Management for disposal at RWS landfill in Norridgewock.

The proposed project will increase solid waste generation by a small increment, as outlined in this section.

#### 5.1 Stumps and Grubbing Debris

There are no forested areas within the proposed project site that are required to be cleared. There are select trees that will be required to be removed. The total volume of stumps and grubblings that will be generated by this project is estimated to be less than 50 cy. All stumps will be either chipped or hauled to a licensed processing or disposal facility.



## 5.2 Construction Debris

Construction debris will be placed in 30 cy. containers for transport and disposal at the Turnkey Landfill in Rochester. The debris has been estimated as follows:

Total floor area = 57,000 s.f.

Estimated construction debris = 57,000 s.f. @ 10 cy/1,500 s.f. = 380 cy

Therefore, the total estimated construction debris as a result of this projects is 380 cy.

## 5.3 Demolition Debris

Demolition debris associated with the Bioscience Institute has been estimated by the project architect/engineer as follows:

Pavement/Sidewalk Removal	900 c.y.
Loading Dock Removal	<u>30 c.y.</u>
<b>Total</b>	<b>930 c.y.</b>

The proposed project does not require any demolition work, which will require special disposal. The existing bituminous pavement within the parking lot will be reclaimed for use on the project or within another suitable site. All other demolition debris numbers anticipated to be inert materials (concrete, soil, etc).

Any demolition debris that is encountered during the building construction or site work, which requires special disposal, will be placed in containers for transport and disposal at the Turnkey Landfill in New Hampshire.

## 5.4 Biomedical Waste

The generation of biomedical waste from the Bioscience Institute is estimated to average 375 lbs./month.

## 5.5 Recycling

The University of Maine System is aggressive in their recycling. As outlined in the overview to this section, it is anticipated the recycling of waste from the new facilities will meet or exceed the 52% recycling rate experienced elsewhere in the Campus.

## 5.6 Closure

Contracts for solid waste disposal are on record with the University of Maine System and can be made available for review by the Planning Department.

The computations of waste volumes are estimates only. Contractors should compute the waste volumes separately and not rely on estimates provided in this section.

**ATTACHMENT E**

**EXTERIOR SITE LIGHTING DETAILS**

# CHARRON

P) 883-8026 INCORPORATED  
F) 883-5015 P.O. Box 4550  
Manchester, NH 03108

GEORGE CLARK  
(603) 624-4827 FAX 624-9764

*Supplier*

---

RECEIVED  
FEB 27 2001  
SYMME MAINI & MCKEE ASSOCIATES, INC.

**SPECIFICATION FEATURES**

**A...Latches**

Two spring-steel quick release latches on housing for toolless entry.

**B...Socket**

Porcelain mogul-base screw shell type lamp socket with spring-loaded center contact.

**C...Housing**

One-piece, die-cast aluminum housing features aesthetically pleasing soft-corner design.

**D...Gasketing**

Closed cell gas-filled high temperature silicone gasketing completely seals optical system from dirt, bugs or other foreign material.

**E...Lens**

Thermal shock- and impact-resistant clear tempered glass.

**F...Optics**

One-piece hydroformed anodized aluminum reflectors provide five different optical distributions and sharp cutoff control. Rotatable optics standard.

**G...Mounting**

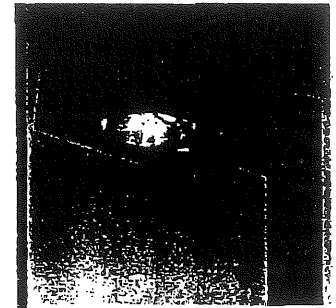
Universal mounting clamp concealed in housing fits 1 1/2" to 2 3/8" O.D. horizontal tenons without adapters. Provides a +5° vertical leveling adjustment.

**H...Ballast**

Easily removable high power factor HID Multi-Tap ballast is standard.

**J...Hinges**

Integral hinges prevent door rocking and optimize sealing capabilities.



**CS CONCOURSE III**

70 - 400 W

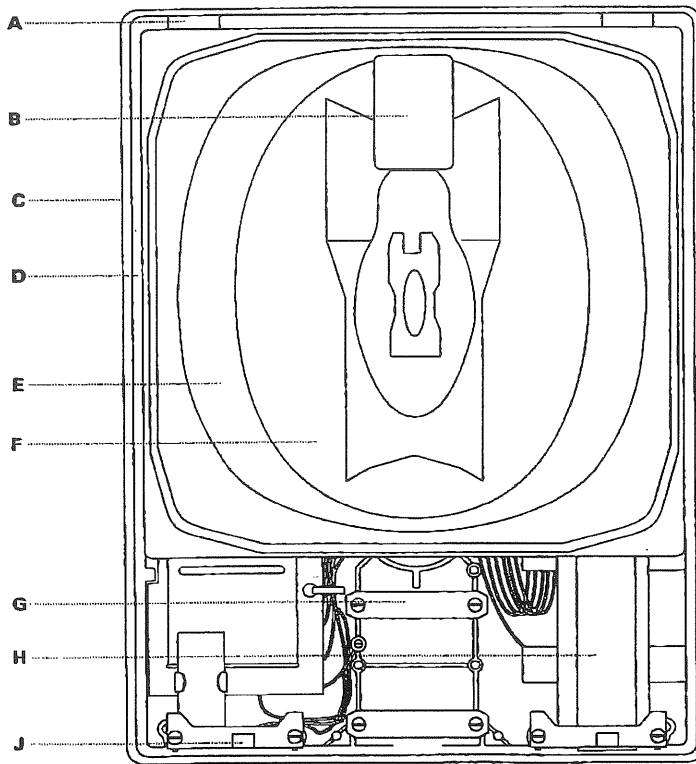
High Pressure Sodium  
Metal Halide

ARCHITECTURAL  
AREA LUMINAIRE

Edison is the most functionally versatile, universally applicable outdoor lighting luminaire. Through a variety of mounting styles, it offers a family of low profile sharp-cutoff luminaires that make optimum use of today's high output HID sources.

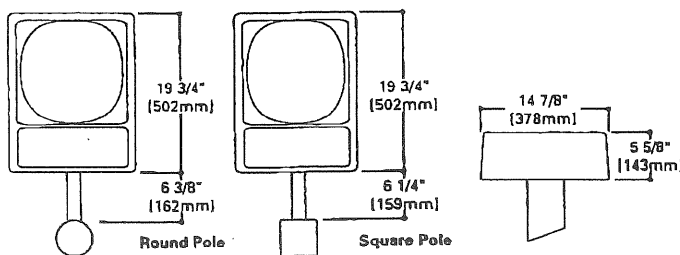
**APPLICATION**

Enhancing natural landscapes as well as cityscapes, the Concourse III brings outstanding performance and style to walkways, parking lots, roadways, loading docks, building areas, and any security lighting application. U.L. 1572 listed and CSA certified for wet locations.



RESIDENCE HALL (GORHAM Charron, Inc  
Cooper Lighting Type: R  
CS716X9-CA40-LL  
X= Optics to be specified.

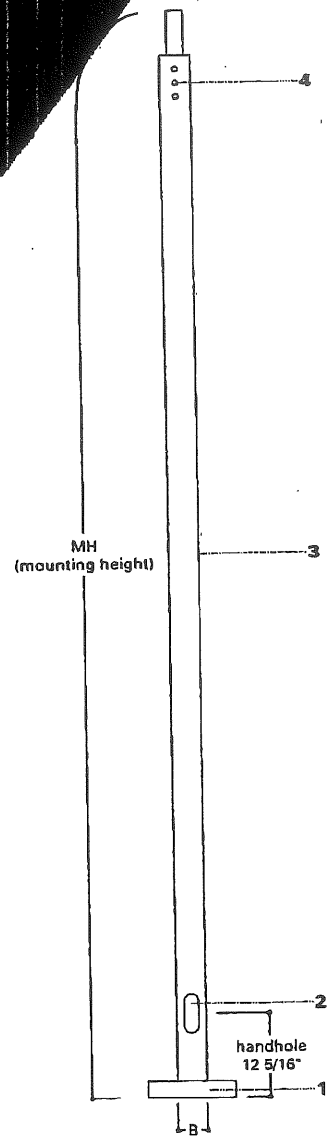
**DIMENSIONS**



CATALOG #:

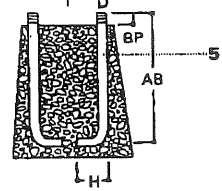
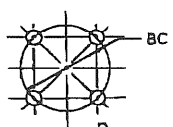
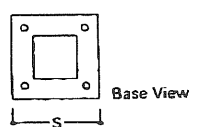
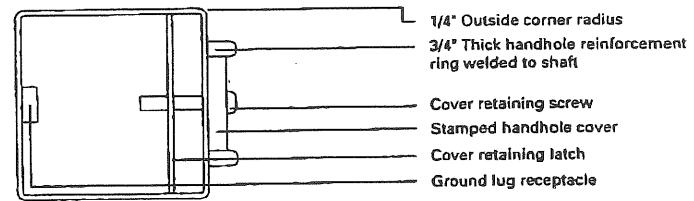
# COOPER LIGHTING

## FOR DIMENSIONAL INFORMATION



RESIDENCE HALL (GORHAM Charron, Inc  
 Cooper Lighting Type: R  
 SSS4A25SFE1

### HANDHOLE (section through standard handhole)



### SPECIFICATIONS

- 1...ASTM Grade steel base plate with ASTM A366 base cover.
- 2...Handhole assembly 3" x 5" on 5" and 6" pole; and 2" x 4" on 4" pole.
- 3...ASTM A500 grade "B" steel shaft. Shot blasted and painted with polyester powder coat.
- 4...Drilled or Tenon (specify).
- 5...Anchor bolt per ASTM A576 with (2) nuts, (2) flat washer, and (1) lock washer. Nuts, washers and threaded portion of bolt are hot dip galvanized 3" hook for 3/4" bolt. 4" hook for 1" bolt.

### FINISH COLORS

- F=Dark Bronze
- G=Galvanized
- I=Royal Blue
- V=Grey
- W=White
- X=None
- Y=Black

## SSSSQUARE STRAIGHT STEEL

10'-39'  
 Mounting Height

SQUARE STRAIGHT  
 STEEL



TRANSFORMER  
HAWTHORN, TYP.

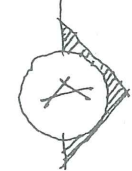
FALMOUTH STREET

SYCAMORE, TYP.  
30" HT STEEL PICKET FENCE  
30" HT GRANITE BLOCK WALL  
CONCRETE RETAINING WALL  
RED MAPLE, TYP.

EXISTING TREE, TYP.

8' HT METAL  
SCREEN FENCE

EMERGENCY  
GENERATOR



FOR SECTION SEE ATTACHED SHEET

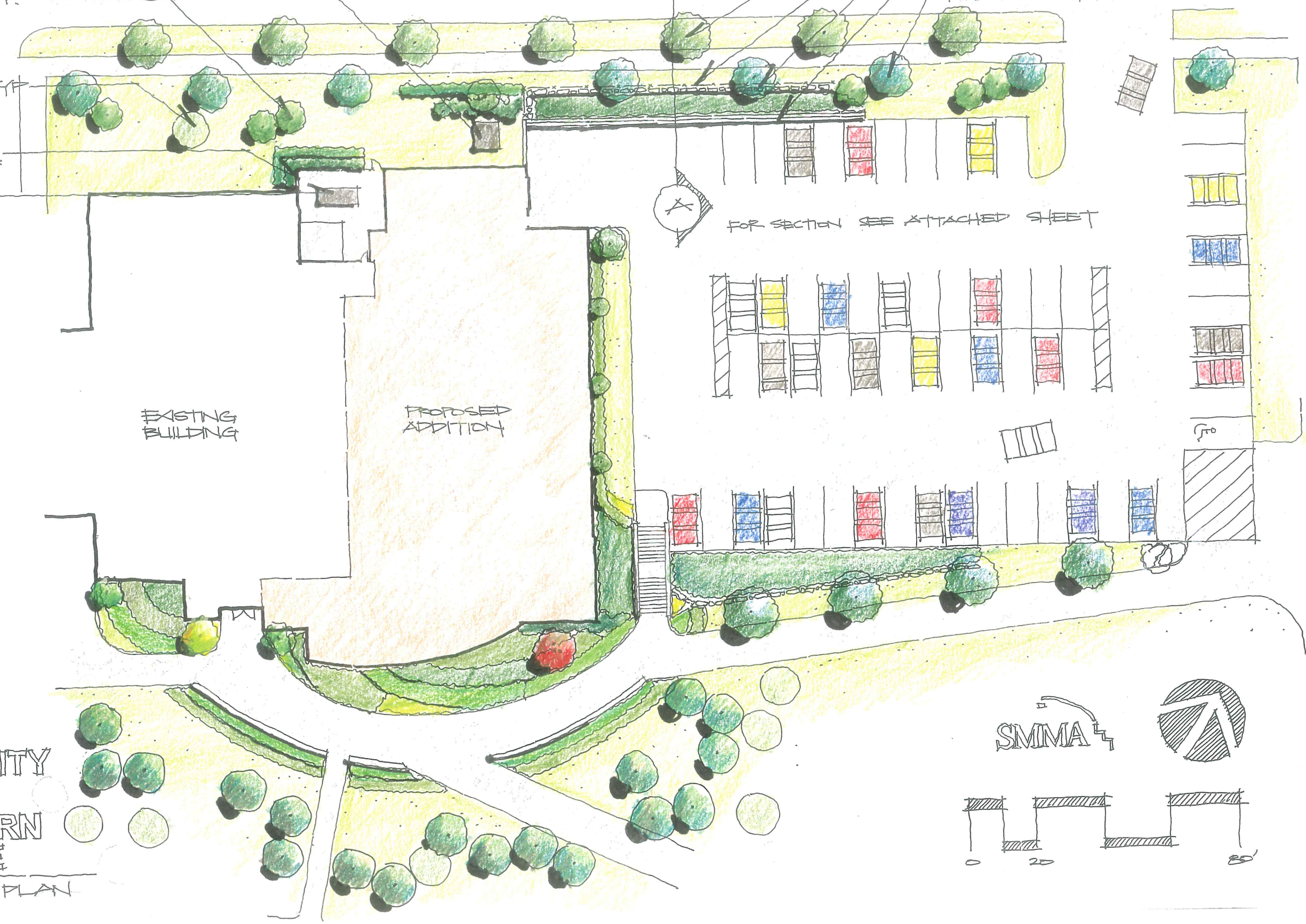
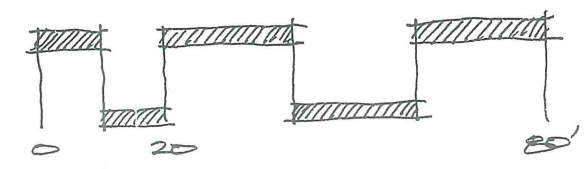
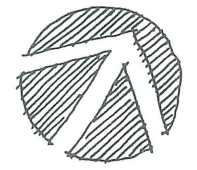
EXISTING  
BUILDING

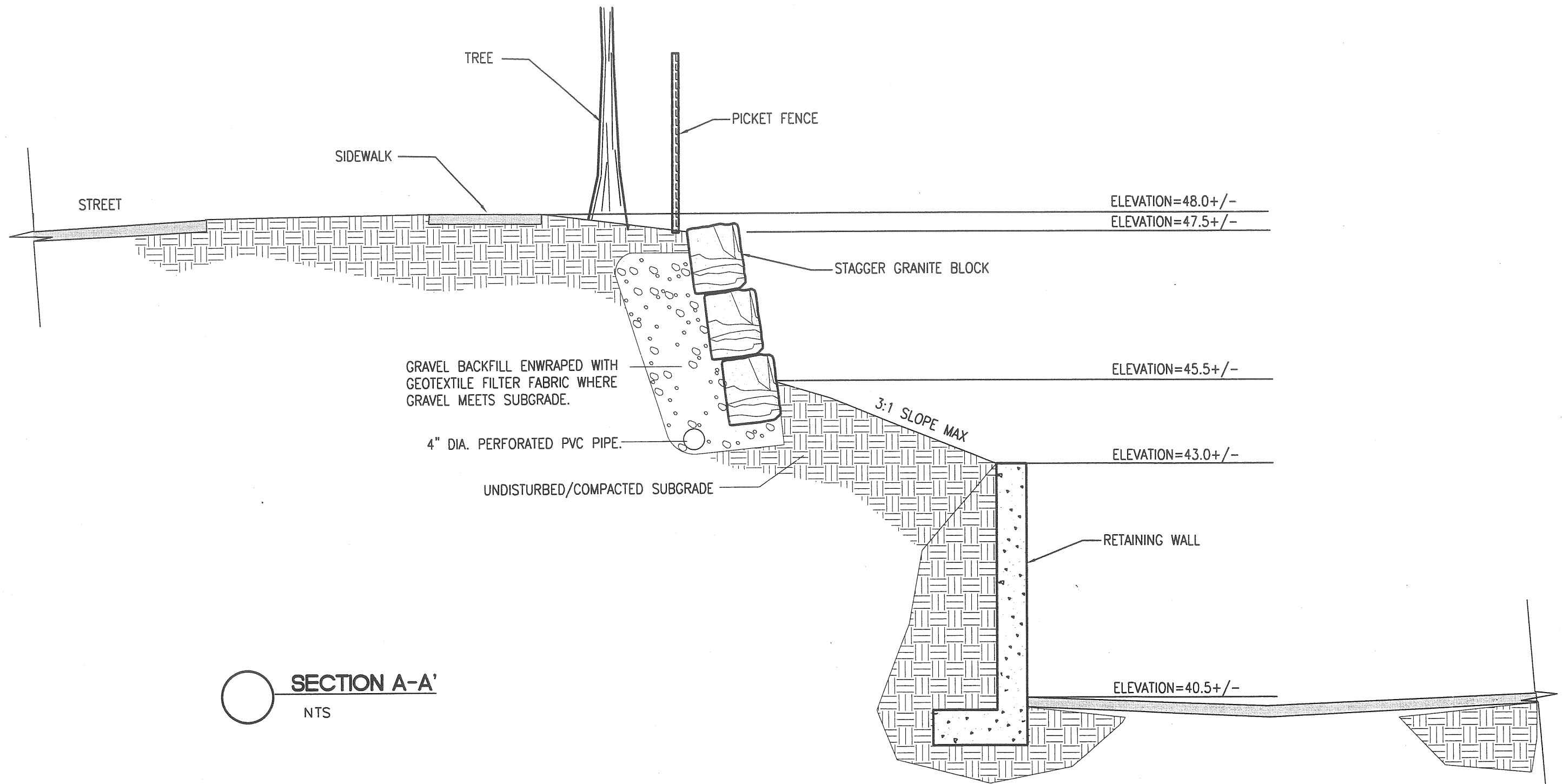
PROPOSED  
ADDITION

UNIVERSITY  
OF  
SOUTHERN  
MAINE

LANDSCAPE PLAN

SMMA





SECTION A-A'  
NTS

TRANSFORMER  
HAWTHORN, TYP.

EXISTING TREE, TYP.

8' HT METAL  
SCREEN FENCE

EMERGENCY  
GENERATOR

FALMOUTH STREET

- SYCAMORE, TYP.
- 30" HT STEEL PICKET FENCE
- 30" HT GRANITE BLOCK WALL
- CONCRETE RETAINING WALL
- RED MAPLE, TYP.

FOR SECTION SEE ATTACHED SHEET

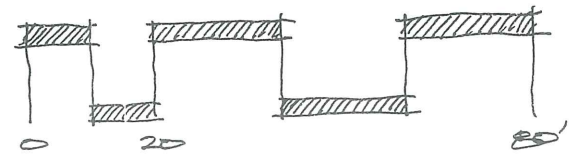
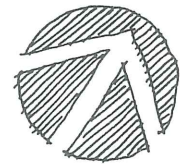
EXISTING BUILDING

PROPOSED ADDITION

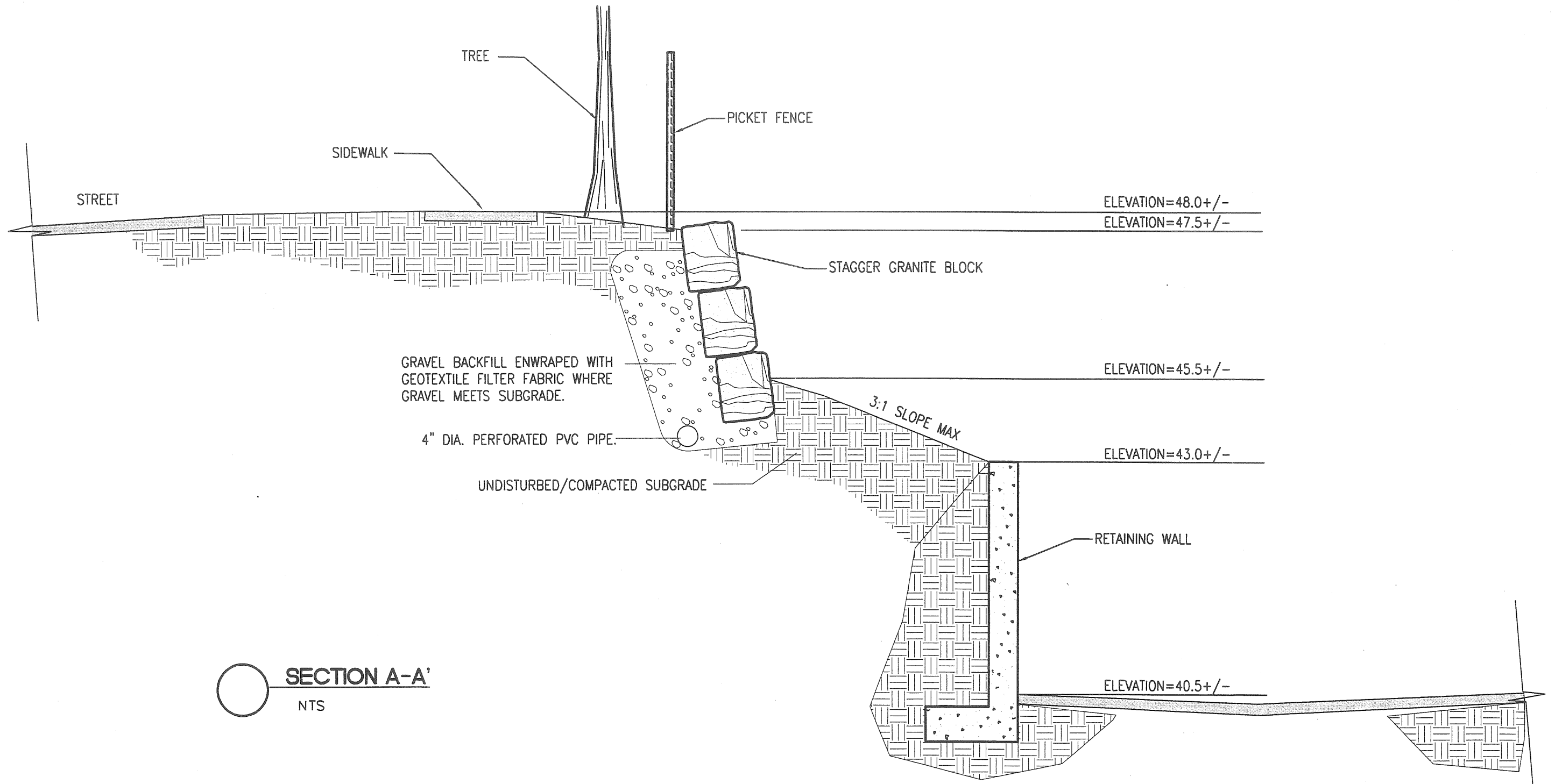
UNIVERSITY  
OF  
SOUTHERN  
MAINE

LANDSCAPE PLAN

SMMA







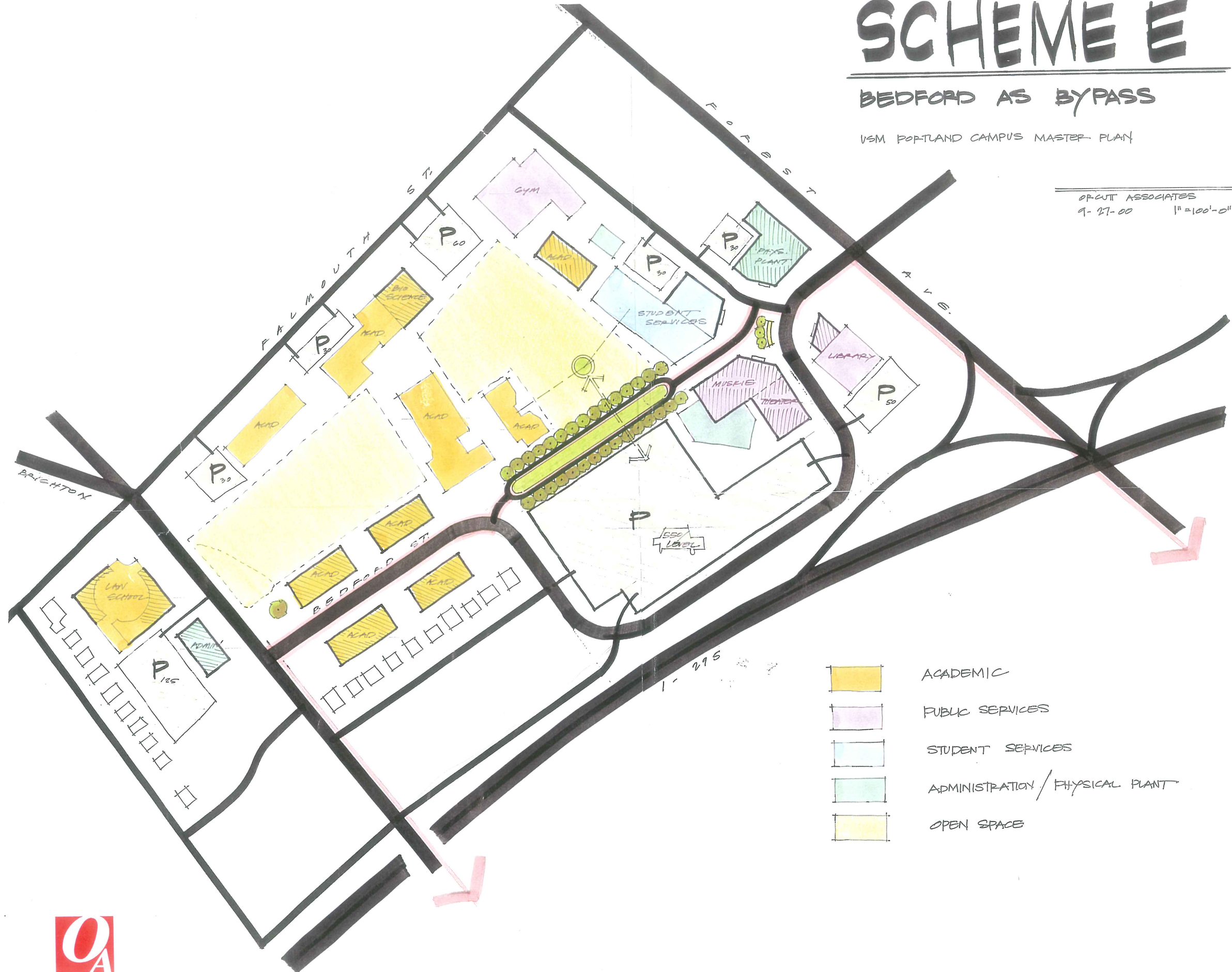
SECTION A-A'  
NTS

# SCHEME E

## BEDFORD AS BYPASS

VSM PORTLAND CAMPUS MASTER PLAN

ORCUTT ASSOCIATES  
9-27-00 1"=100'-0"



- ACADEMIC
- PUBLIC SERVICES
- STUDENT SERVICES
- ADMINISTRATION / PHYSICAL PLANT
- OPEN SPACE

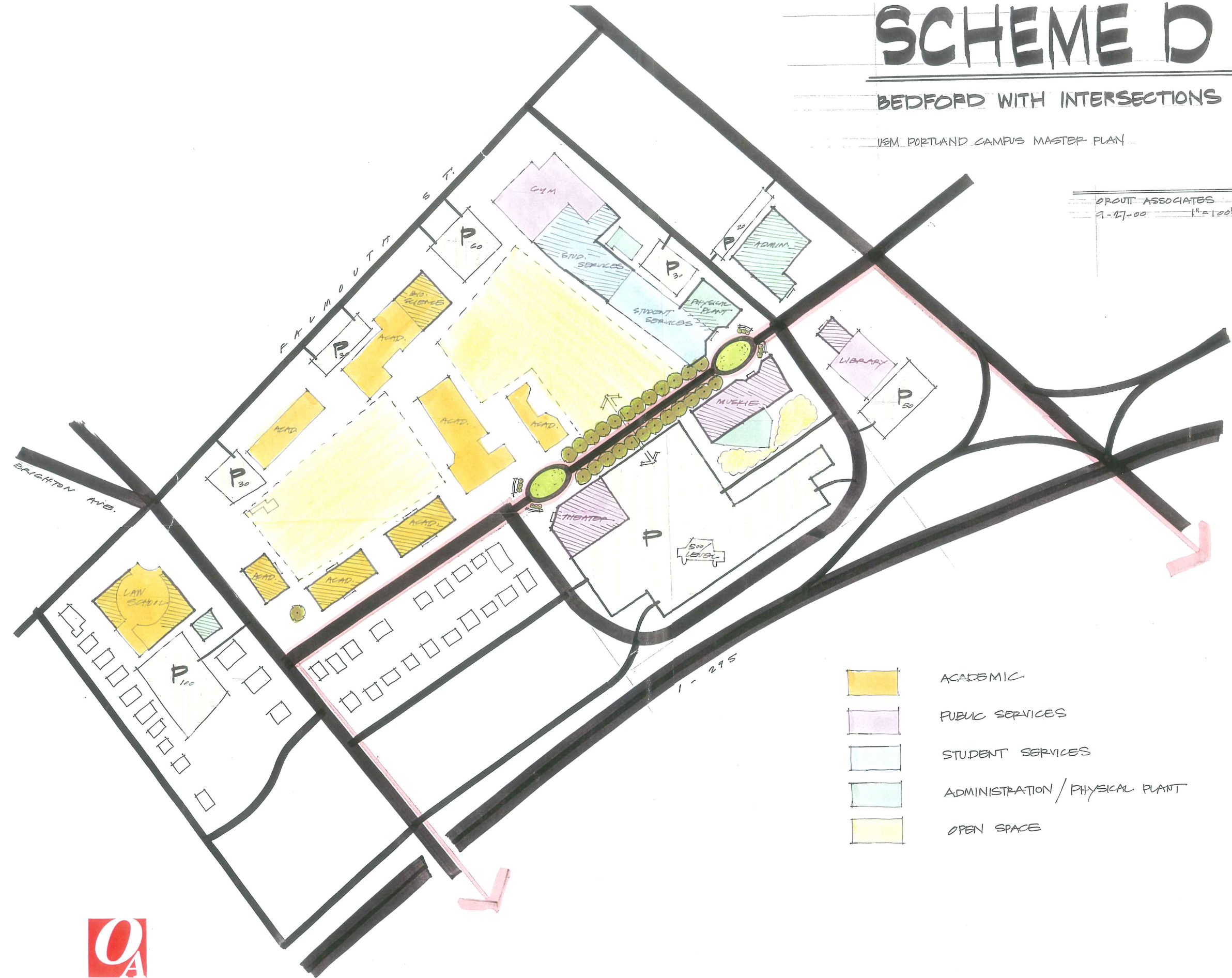







# SCHEME D

## BEDFORD WITH INTERSECTIONS

U.S.M. PORTLAND CAMPUS MASTER PLAN

OROUT ASSOCIATES  
1-27-00 1" = 100'-0"



-  ACADEMIC
-  PUBLIC SERVICES
-  STUDENT SERVICES
-  ADMINISTRATION / PHYSICAL PLANT
-  OPEN SPACE

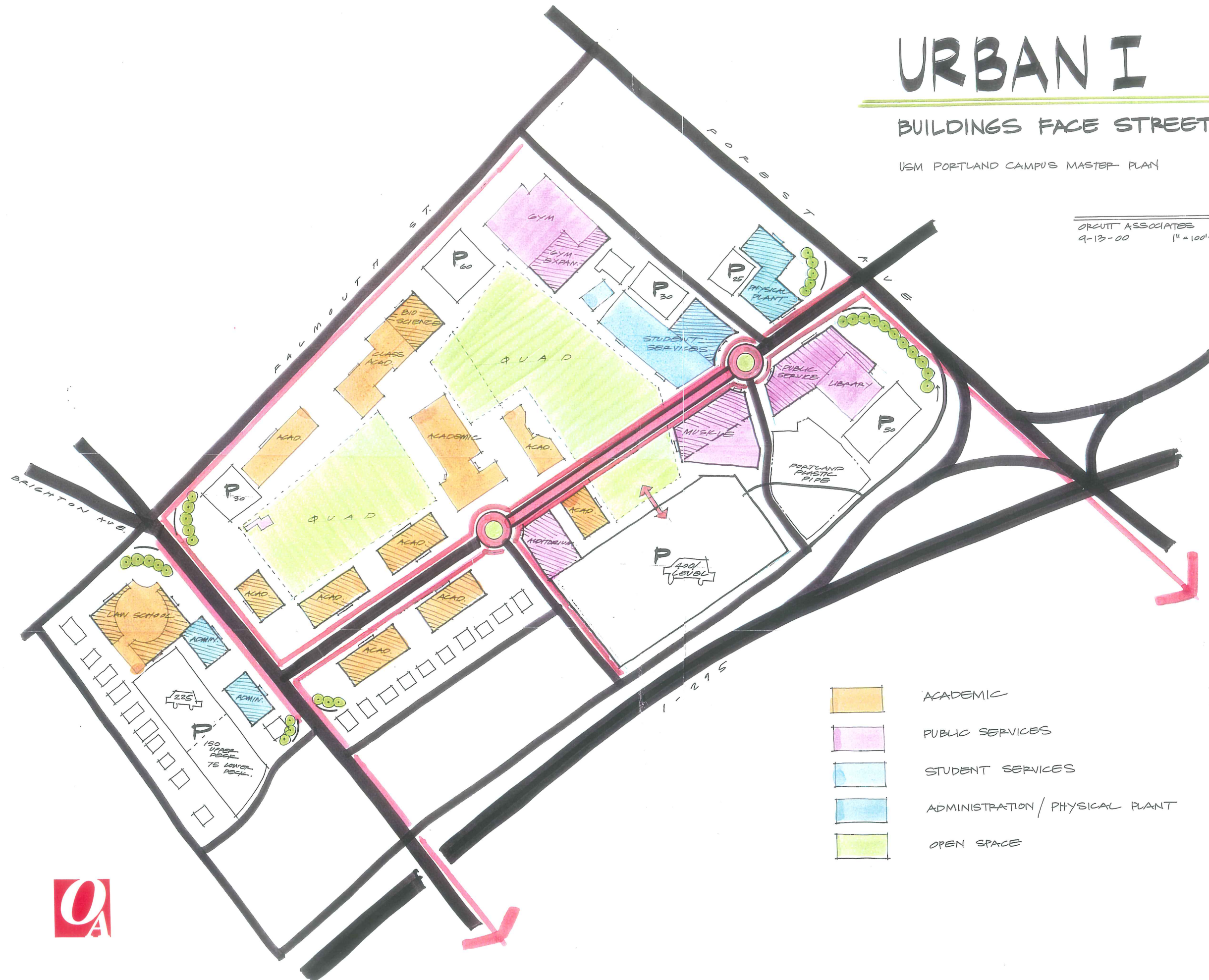


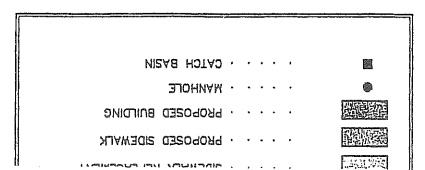
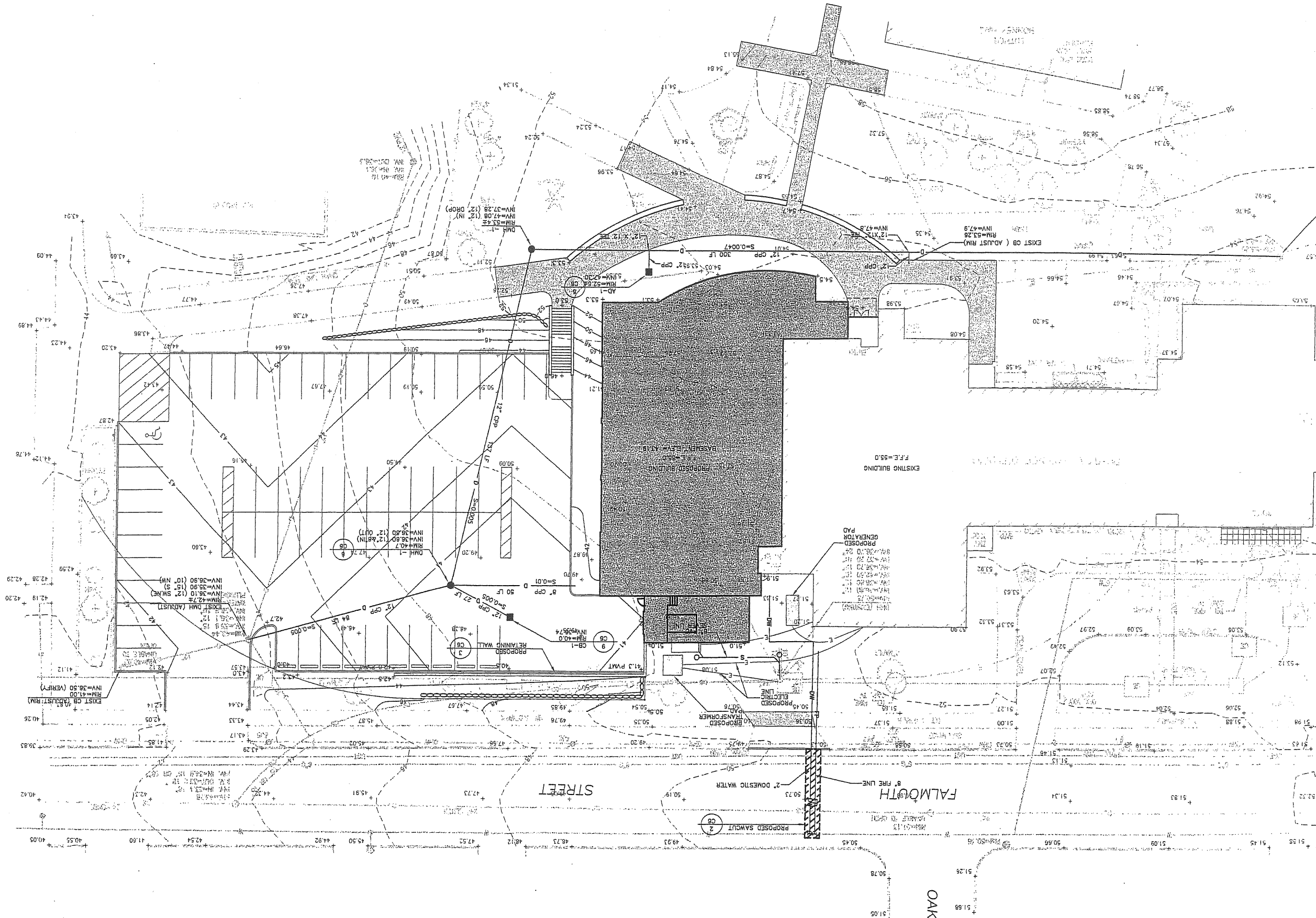
# URBANI

## BUILDINGS FACE STREET

USM PORTLAND CAMPUS MASTER PLAN

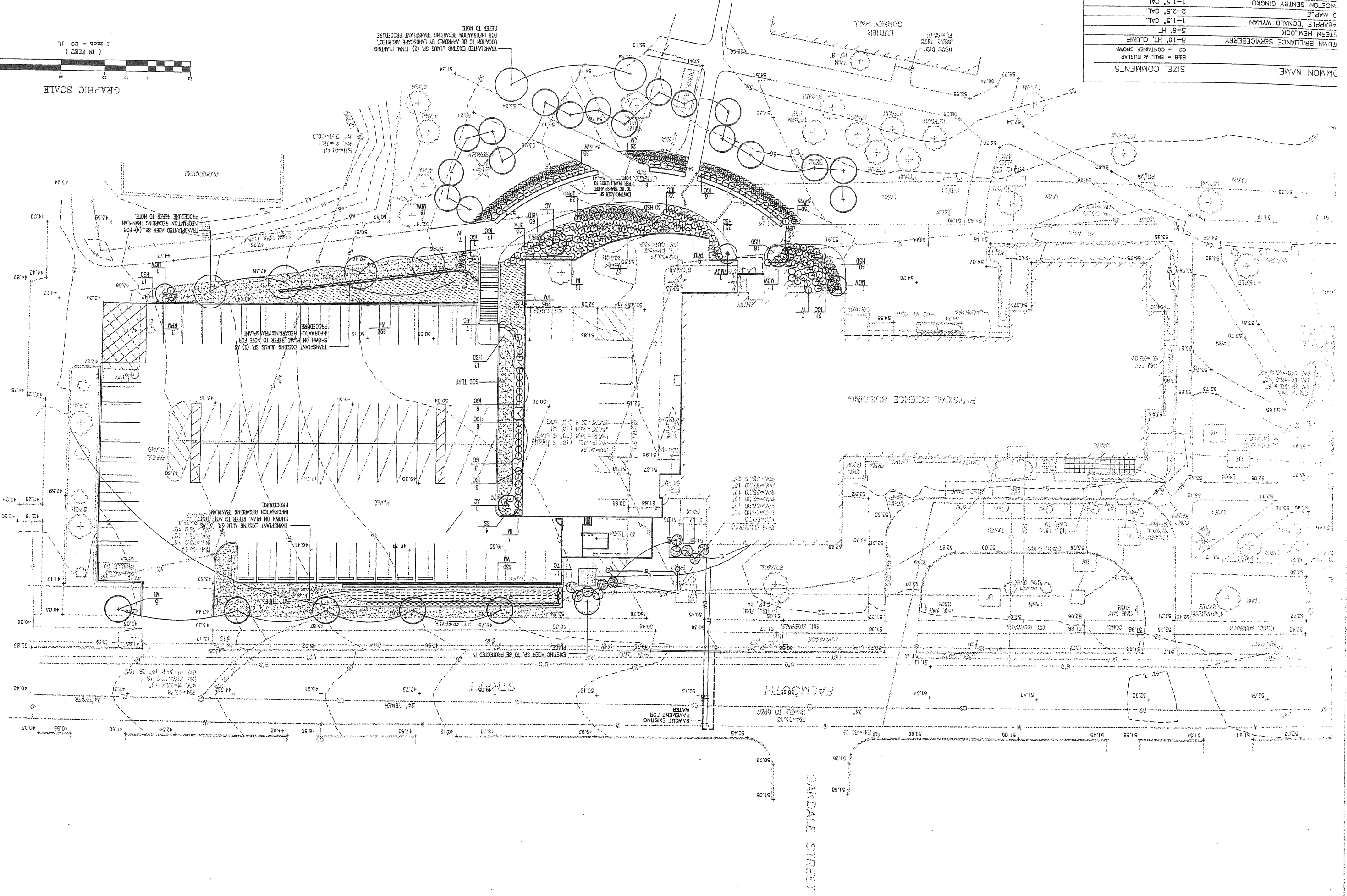
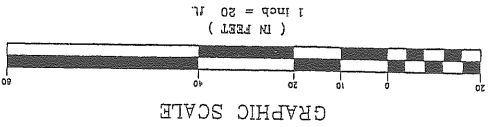
ORCUTT ASSOCIATES  
9-13-00 1" = 100'-0"





PLANS 1-5

COMMON NAME	SIZE, COMMENTS
STJMN BRILLIANCE SERVICEBERRY	8-10' HT. CLUMP
APRPLE DONALD WYMAN	5-6' HT.
1-1.5' CAL.	
2-2.5' CAL.	
MAPLE	
INCETON SENTRY GINKGO	1-1.5' CAL.



1 TYPICAL DECIDUOUS TREE PLANTING N.T.S.

2 TYPICAL EVERGREEN TREE PLANTING N.T.S.

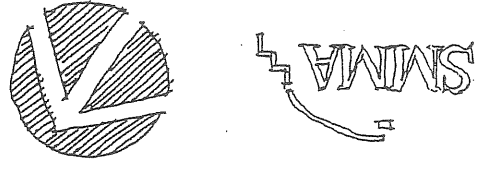
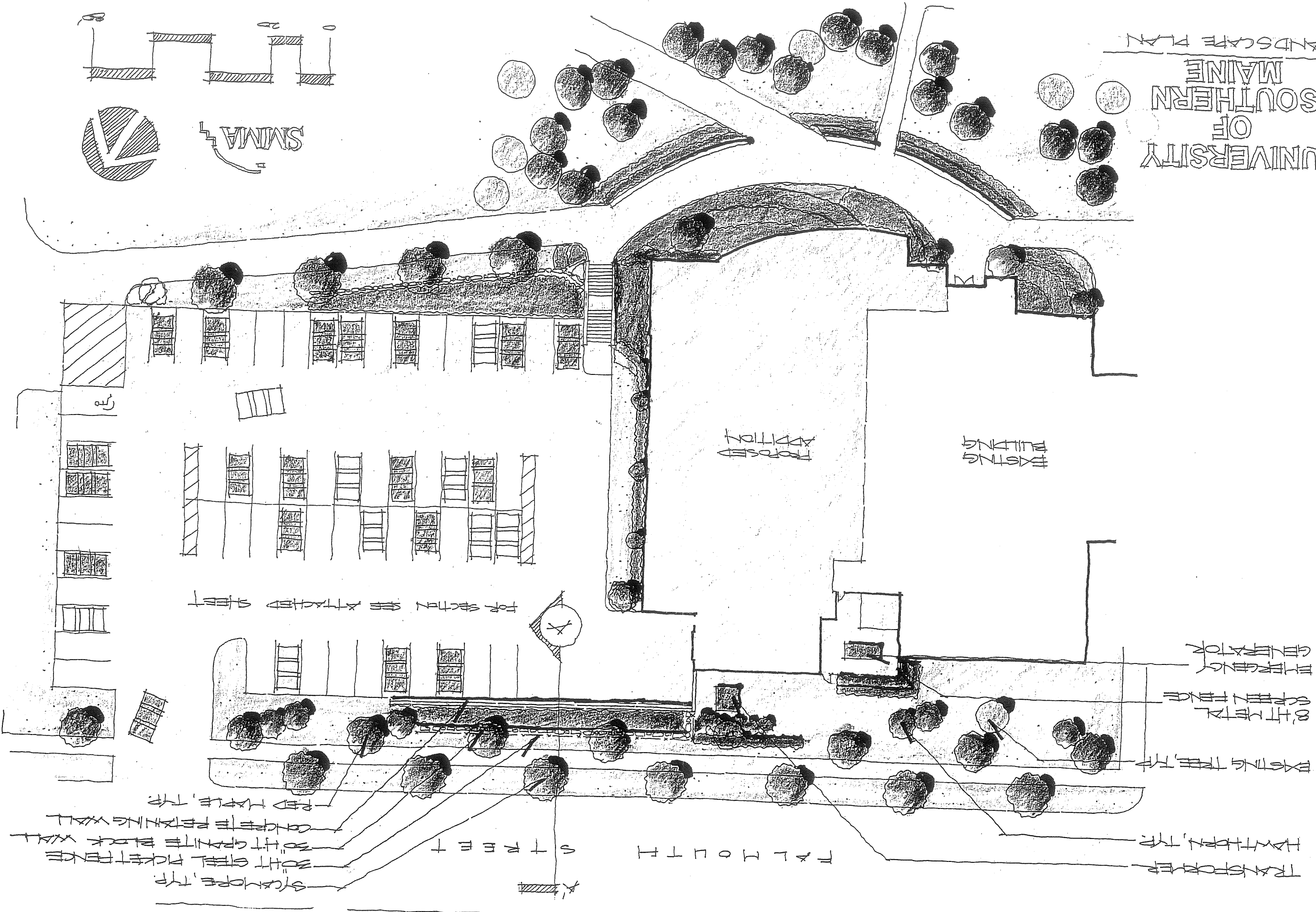
3 TYPICAL DECIDUOUS AND EVERGREEN SHRUB PLANTING N.T.S.

4 TYPICAL GROUNDCOVER PLANTING N.T.S.

pd

LANDSCAPE PLAN

UNIVERSITY OF SOUTHERN MAINE



FOR SECTION SEE ATTACHED SHEET

PROPOSED ADDITION

EXISTING BUILDING

GENERATOR

8" HT METAL SCREEN FENCE

EXISTING TREE, TR

STANFORD TR

FALMOUTH STREET

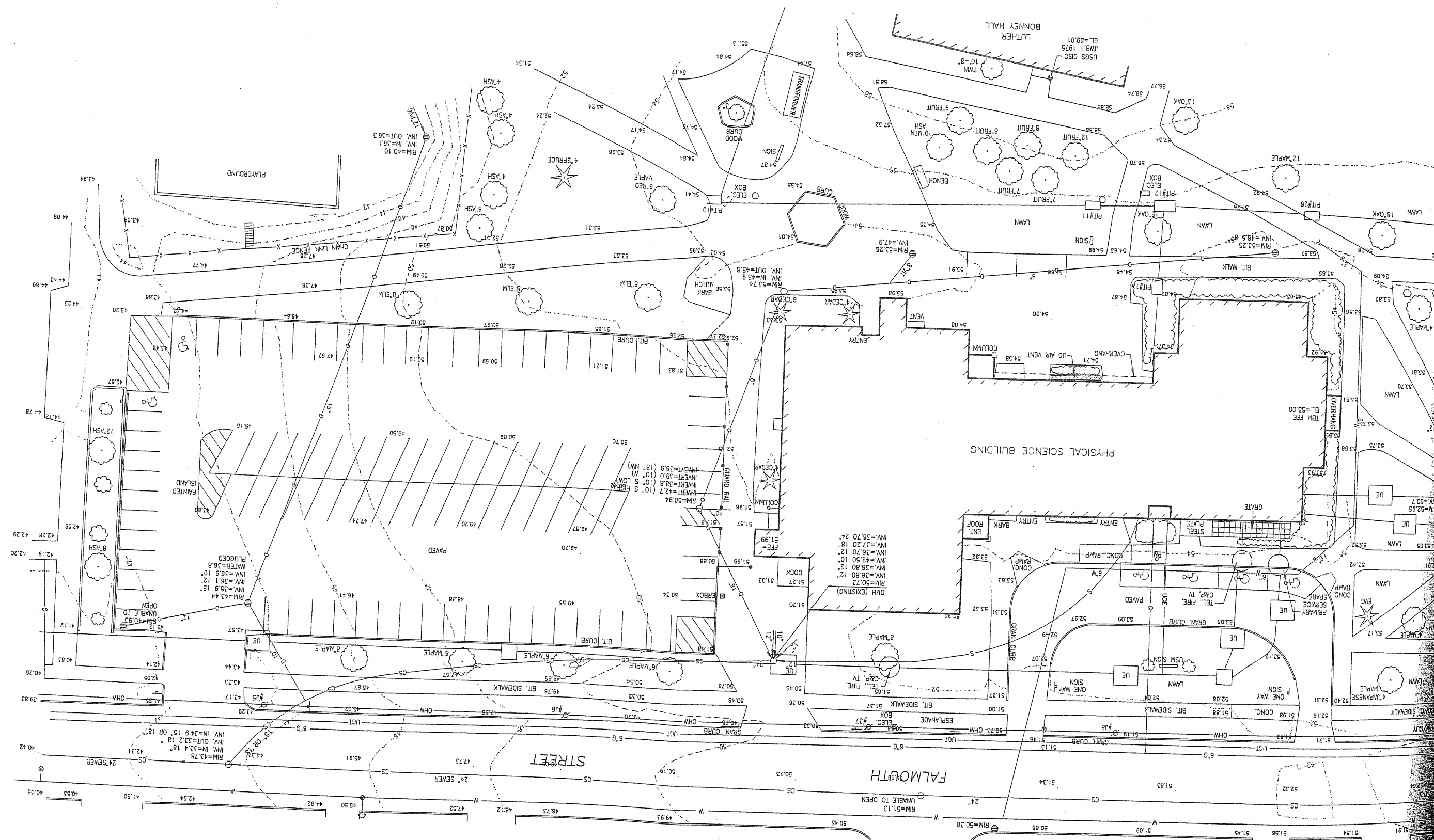
TRANSPORTER

30" HT STEEL PICKET FENCE

30" HT GRANITE BLOCK WALL

CONCRETE RETAINING WALL

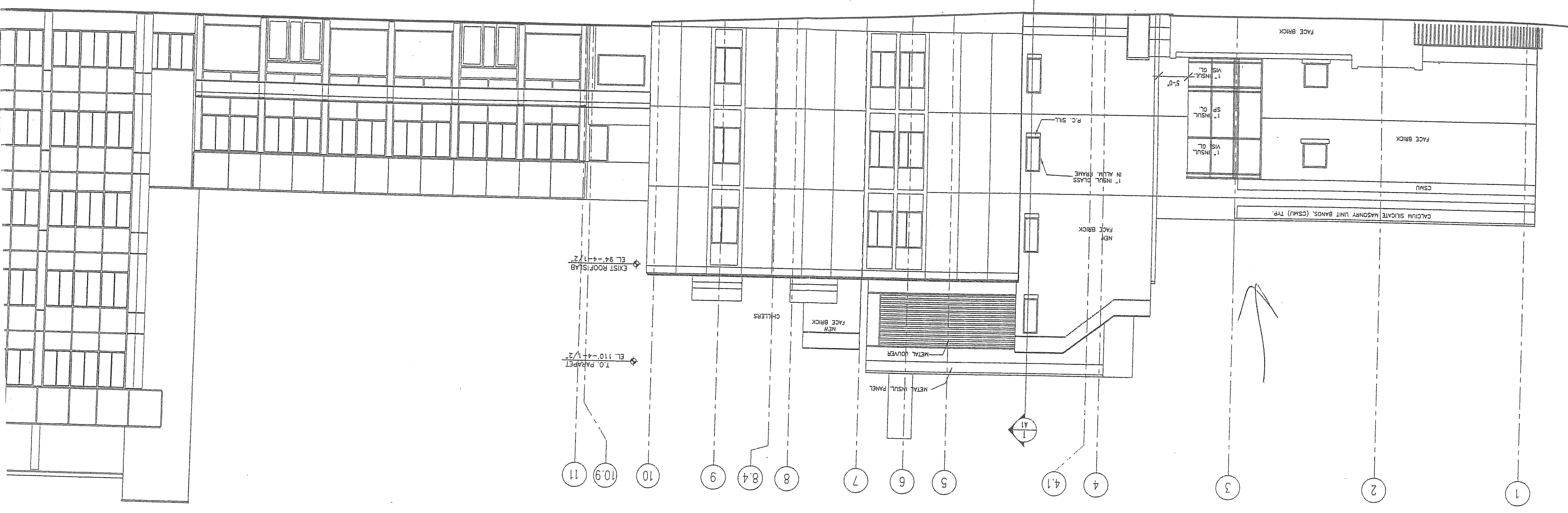
RED WALK, TR



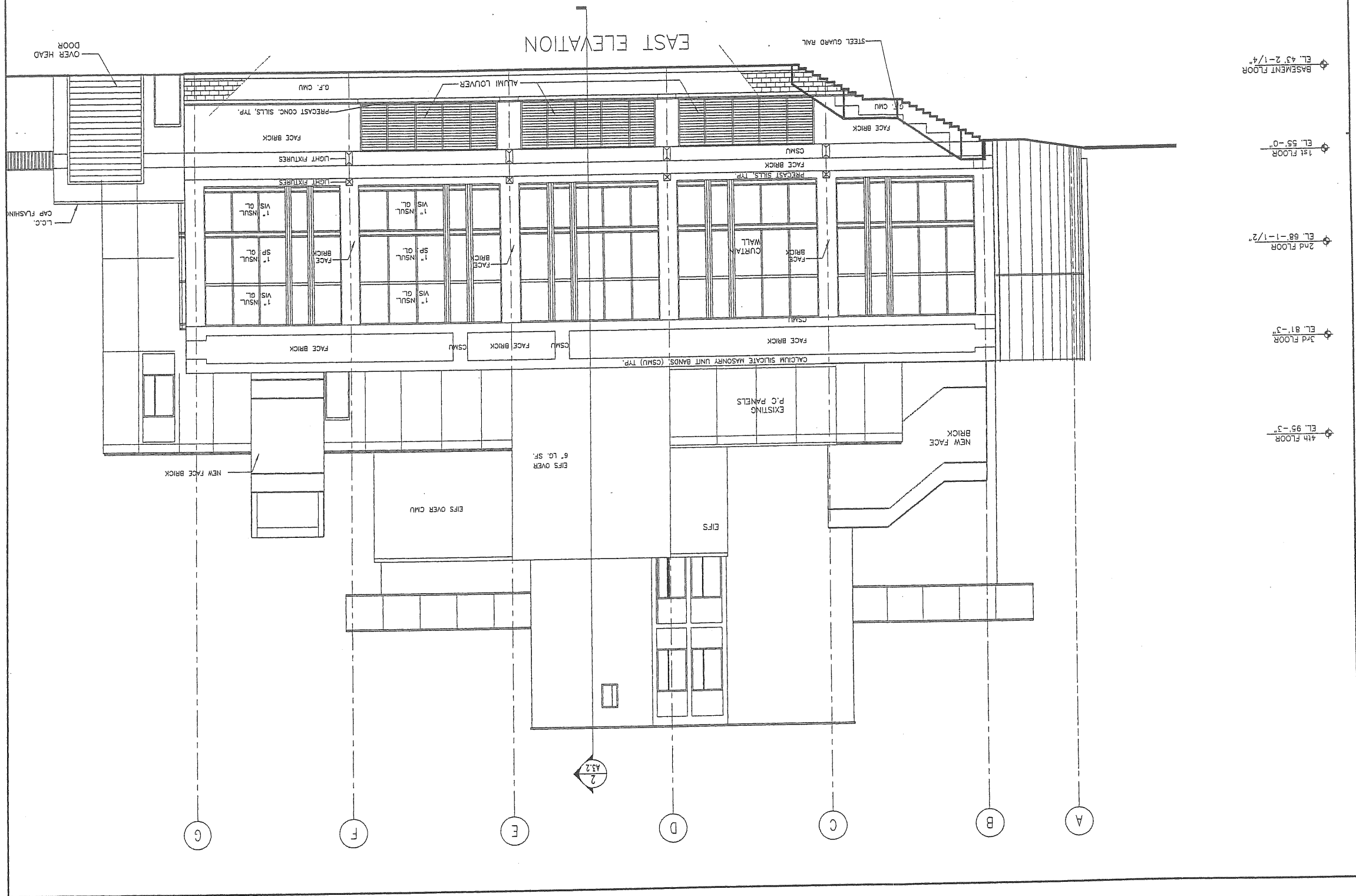
- OHW --- OVERHEAD WIRES
- UGE --- UNDERGROUND ELECTRIC
- T --- TELEPHONE
- W --- WATER LINE
- C --- GAS LINE
- S --- SANITARY SEWER
- SD --- STORM DRAIN
- CS --- COMBINED SEWER DRAIN
- 48 --- CONTOUR

P2





EAST ELEVATION



- EL. 43'-2-1/4" BASEMENT FLOOR
- EL. 55'-0" 1st FLOOR
- EL. 68'-1-1/2" 2nd FLOOR
- EL. 81'-3" 3rd FLOOR
- EL. 95'-3" 4th FLOOR

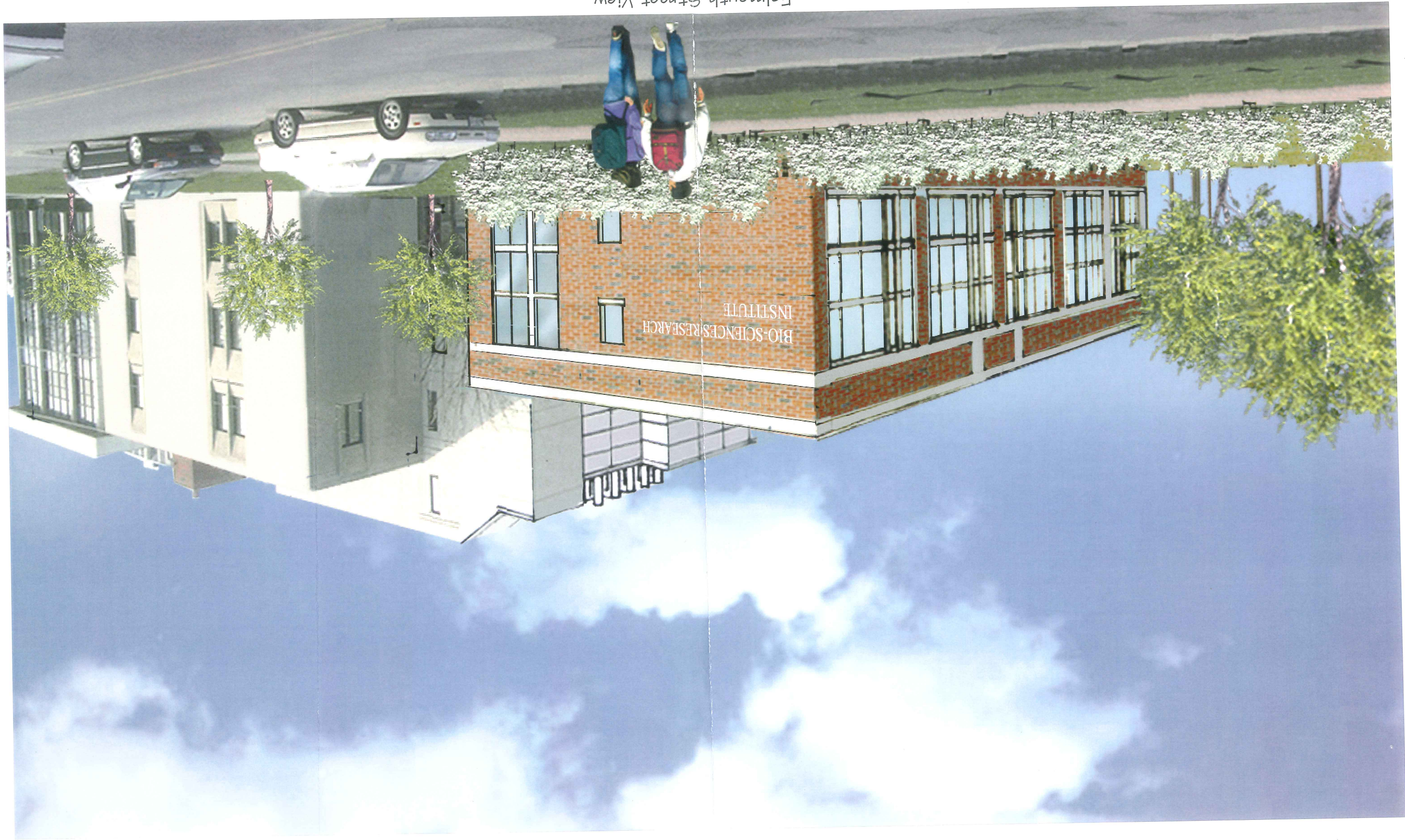
SYMBOLS  
JOB NO.  
PROJ.  
PROJ.  
CHECKED  
DRAWN  
SCALE  
EL  
EX  
SI  
NO.  
ISSUE  
A  
B  
C  
NA  
OM



View From Quad



Falmouth Street View









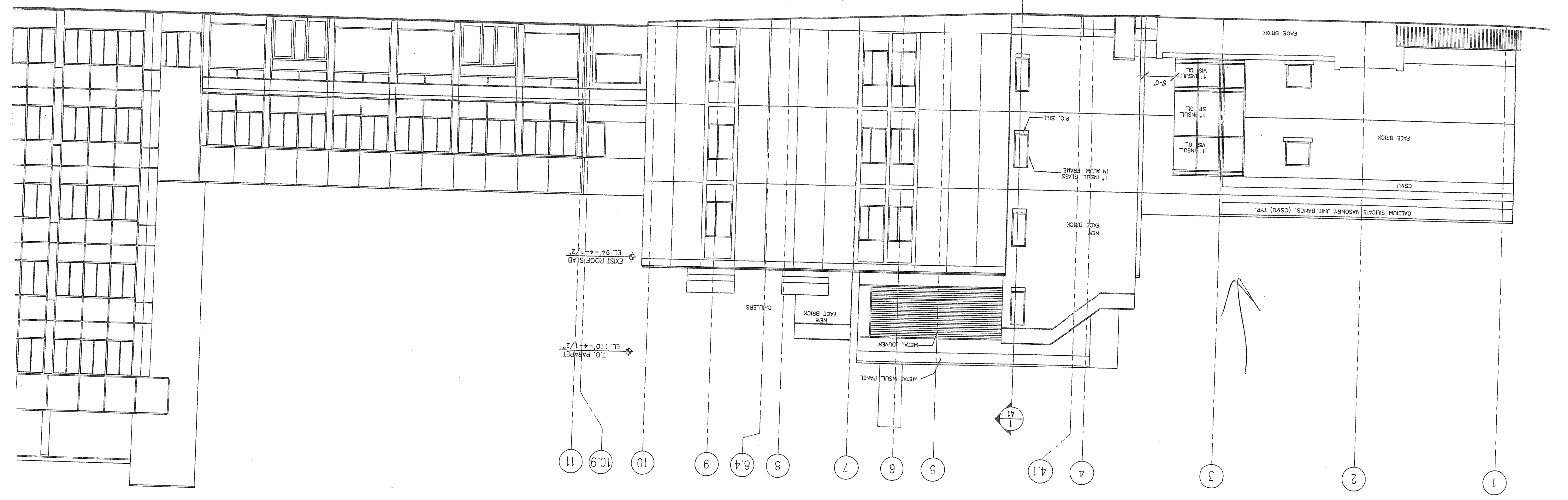








NORTH ELEVATION



SOUTH ELEVATION

A3.2  
2

A1  
1  
A3.3  
3

A3.2  
1

A3.3  
2

1

2

2.6

3

4

4.5

5

6

7

8

8.4

9

10

10.9

11

ALUMIN. ENTRANCE

1" INSUL. GLASS W/ALUMIN. STOREFRONT FRAME  
PRECAST SILL

CURTAIN WALL

ALUMIN. CANOPY

CURTAIN WALL

CSMU

CSMU

FACE BRICK

NEW

METAL PANEL

ELF.S.

ADD. ALUMIN. CANOPY

GROUND FLOOR (G.F.)

PRECAST

1" INSUL. VIS. DL.

1" INSUL. SP. DL.

1" INSUL. VIS. DL.

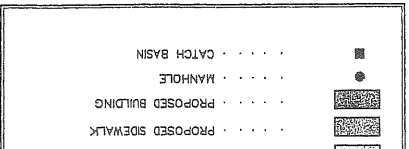
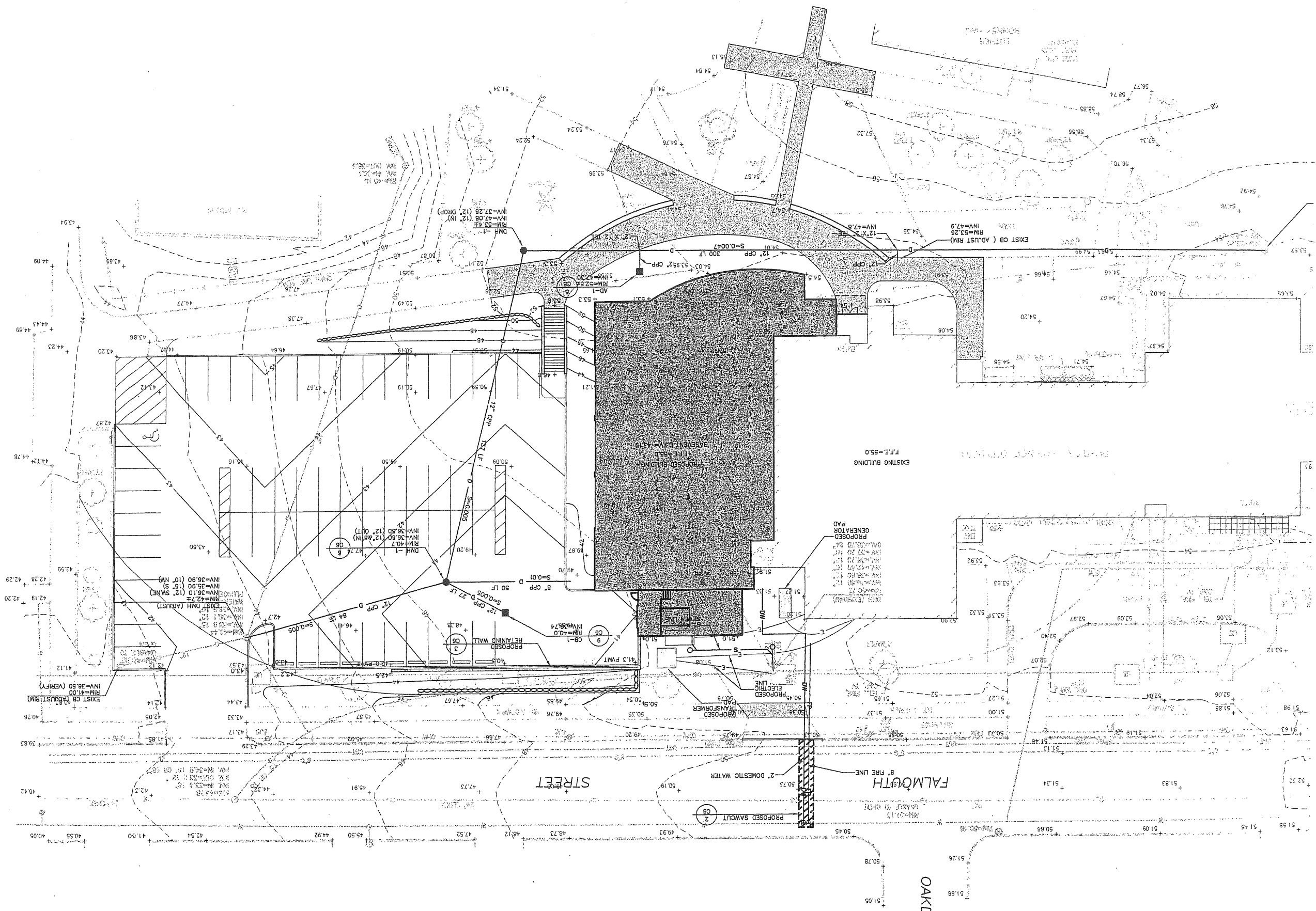
CSMU

CSMU

FACE BRICK

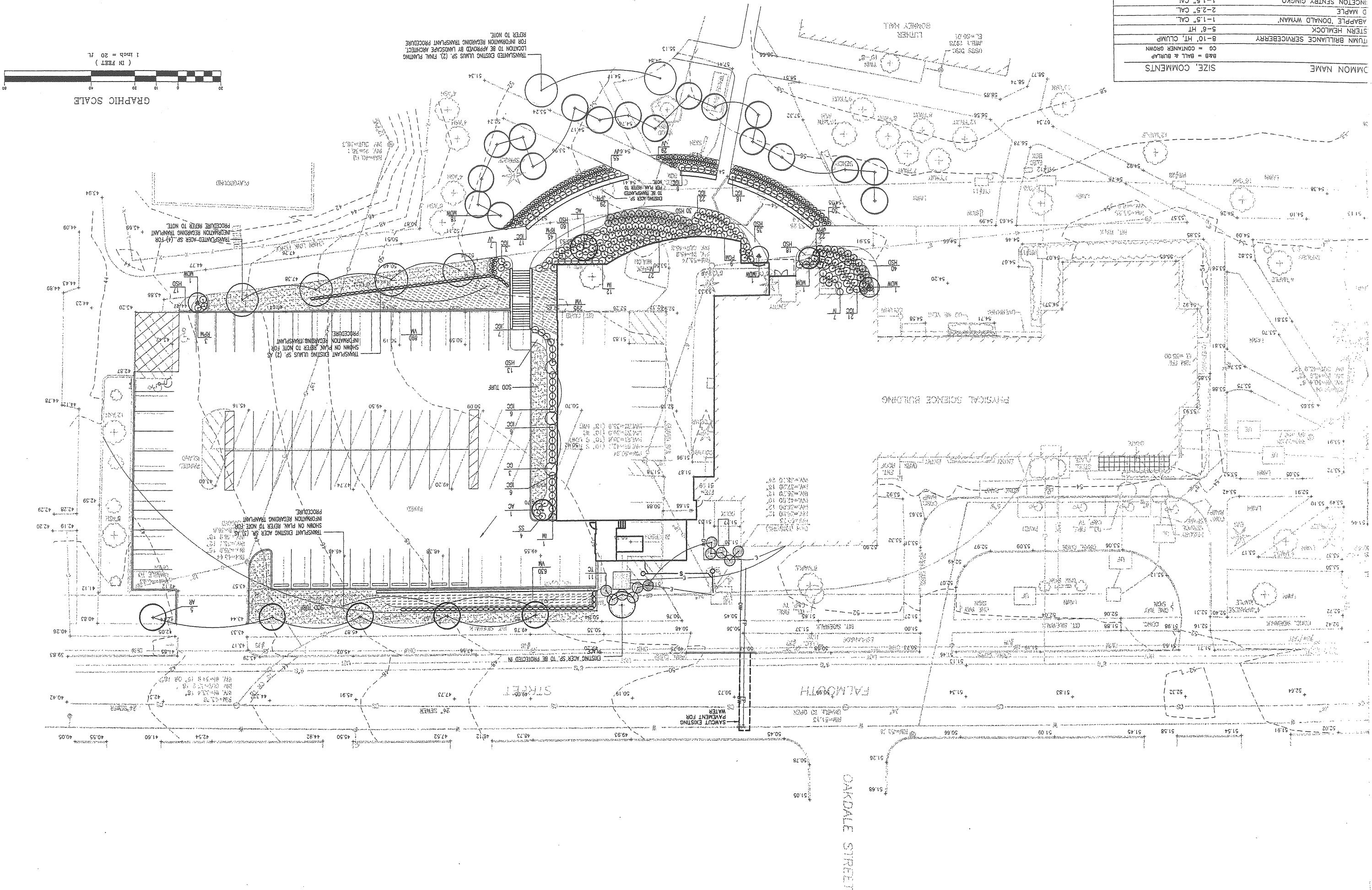
METAL PANEL

SCALE: 1/8" = 1'-0"  
DATE: 11/15/11  
DRAWN BY: [illegible]  
CHECKED BY: [illegible]  
SCALE: 1/8" = 1'-0"  
DATE: 11/15/11  
DRAWN BY: [illegible]  
CHECKED BY: [illegible]



FALMOUTH

COMMON NAME	SIZE, COMMENTS
TUMM BRILLIANCE SERVICEBERRY	8-10' HT, CLUMP
STERN HEMLOCK	5-6' HT
ABAPLE DONALD WYMAN	1-1.5" CAL
D MAPLE	2-2.5" CAL
INCITON SENTRY GINKGO	1-1.5" CAL



GRAPHIC SCALE  
1 inch = 20 feet  
( IN FEET )

TRANSPLANT EXISTING UNUS SP. (2) FINAL PLANTING LOCATION TO BE APPROVED BY LANDSCAPE ARCHITECT FOR INFORMATION REGARDING TRANSPLANT PROCEDURE REFER TO NOTE.

TRANSPLANT EXISTING UNUS SP. (4) FOR INFORMATION REGARDING TRANSPLANT PROCEDURE REFER TO NOTE.

TRANSPLANT EXISTING UNUS SP. (2) AS SHOWN ON PLAN REFER TO NOTE FOR INFORMATION REGARDING TRANSPLANT PROCEDURE.

TRANSPLANT EXISTING ACER SP. (3) AS SHOWN ON PLAN REFER TO NOTE FOR INFORMATION REGARDING TRANSPLANT PROCEDURE.

4 TYPICAL GROUNDCOVER PLANTING N.T.S.

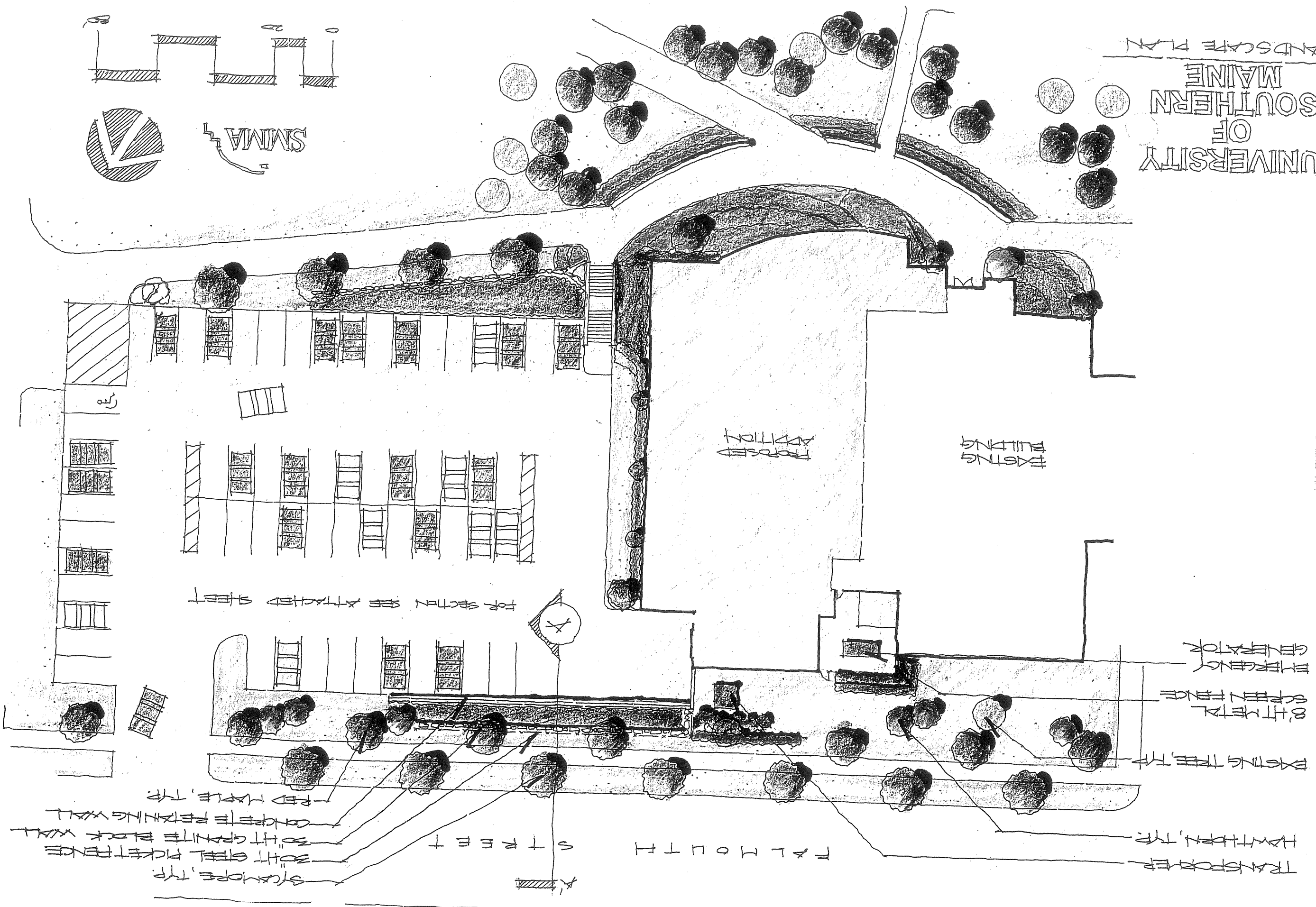
3 TYPICAL DECIDUOUS AND EVERGREEN SHRUB PLANTING N.T.S.

2 TYPICAL EVERGREEN TREE PLANTING N.T.S.

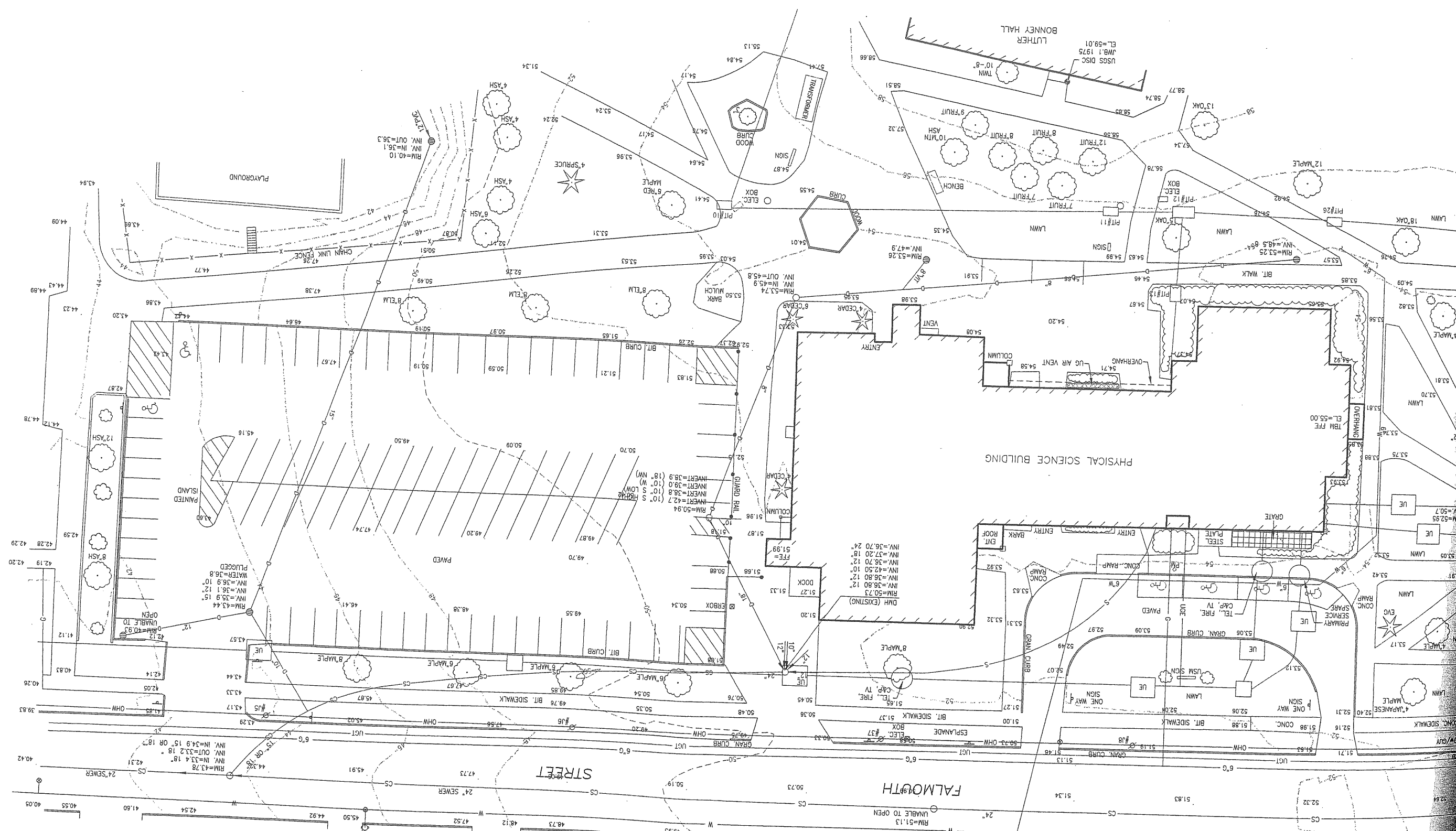
1 TYPICAL DECIDUOUS TREE PLANTING N.T.S.

pt

UNIVERSITY OF SOUTHERN MAINE  
LANDSCAPE PLAN







- OHW ----- OVERHEAD WIRES
- UGE ----- UNDERGROUND ELECTRIC
- T ----- TELEPHONE
- W ----- WATER LINE
- G ----- GAS LINE
- S ----- SANITARY SEWER
- SD ----- STORM DRAIN
- CS ----- COMBINED SEWER DRAIN
- 48 ----- CONTOUR

P2

OAKDALE ST.

FALMOUTH STREET

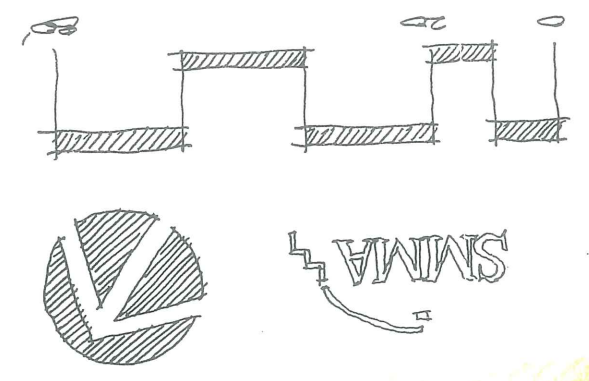
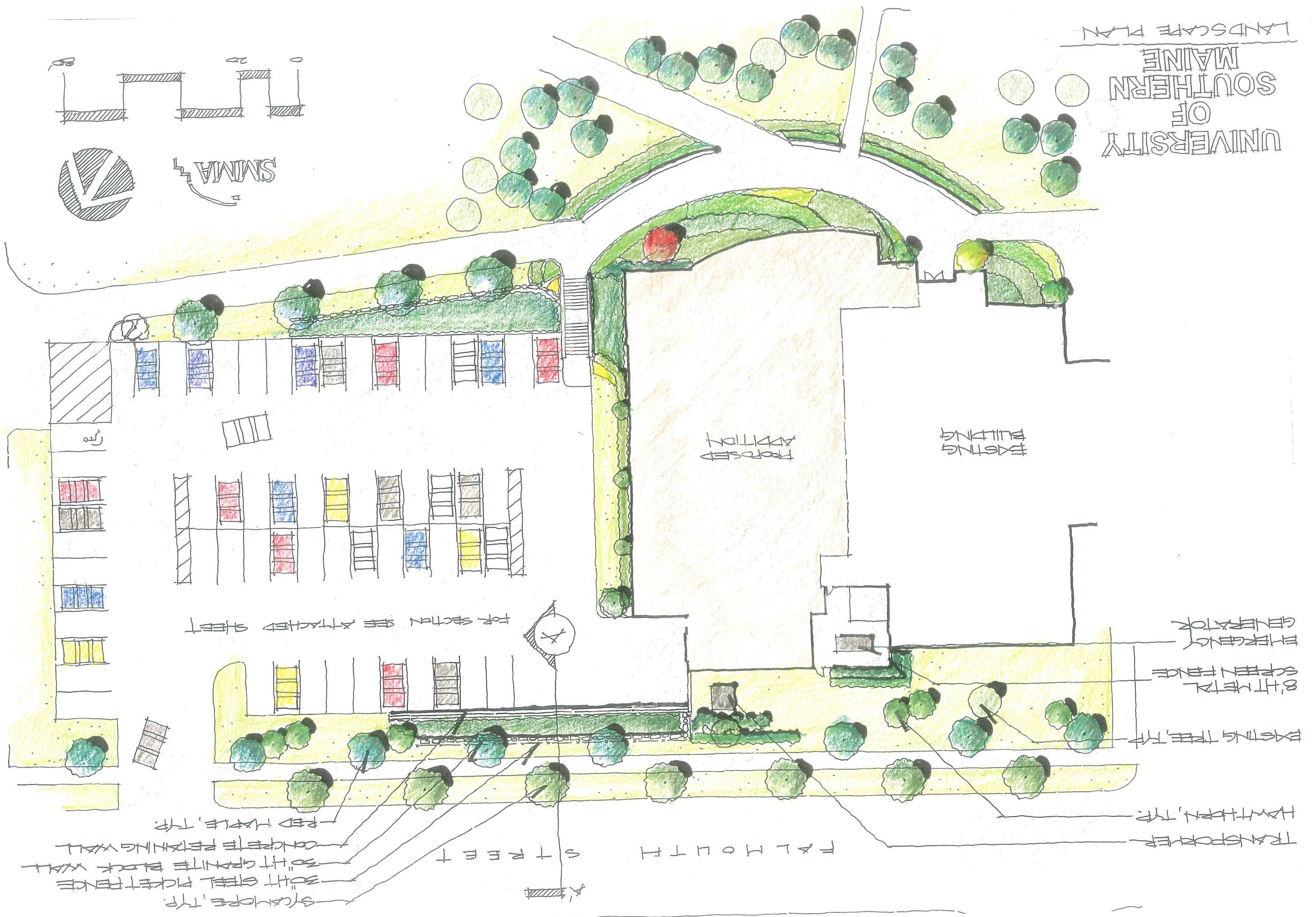
PHYSICAL SCIENCE BUILDING

LUTHER HALL  
BONNEY HALL



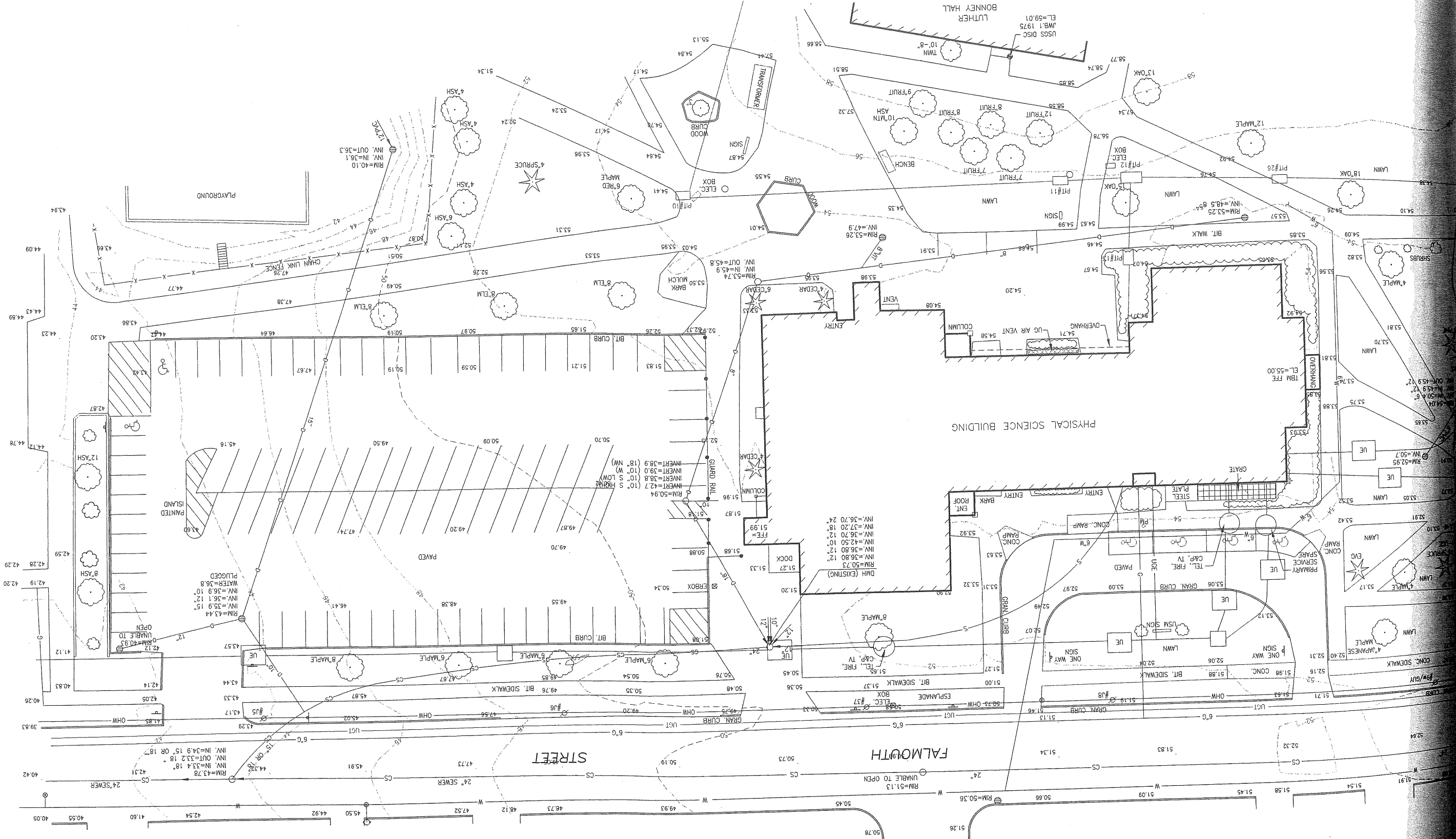
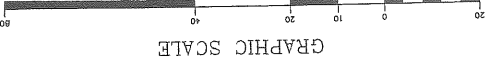


UNIVERSITY OF SOUTHERN MAINE  
LANDSCAPE PLAN



FOR SECTION SEE ATTACHED SHEET

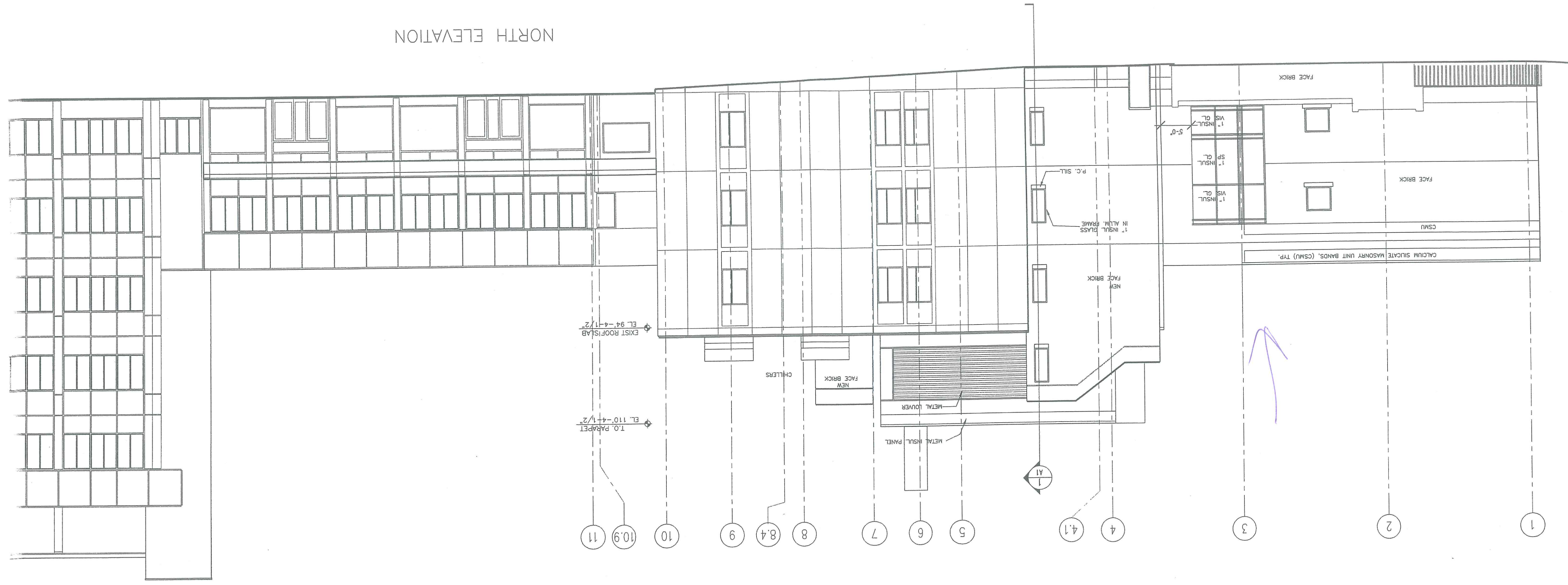
SCAMPOPE, TR  
3\"/>



- OHW --- OVERHEAD WIRES
- UCE --- UNDERGROUND ELECTRIC
- T --- TELEPHONE
- W --- WATER LINE
- G --- GAS LINE
- S --- SANITARY SEWER
- SD --- STORM DRAIN
- CS --- COMBINED SEWER DRAIN
- 48 --- CONTOUR

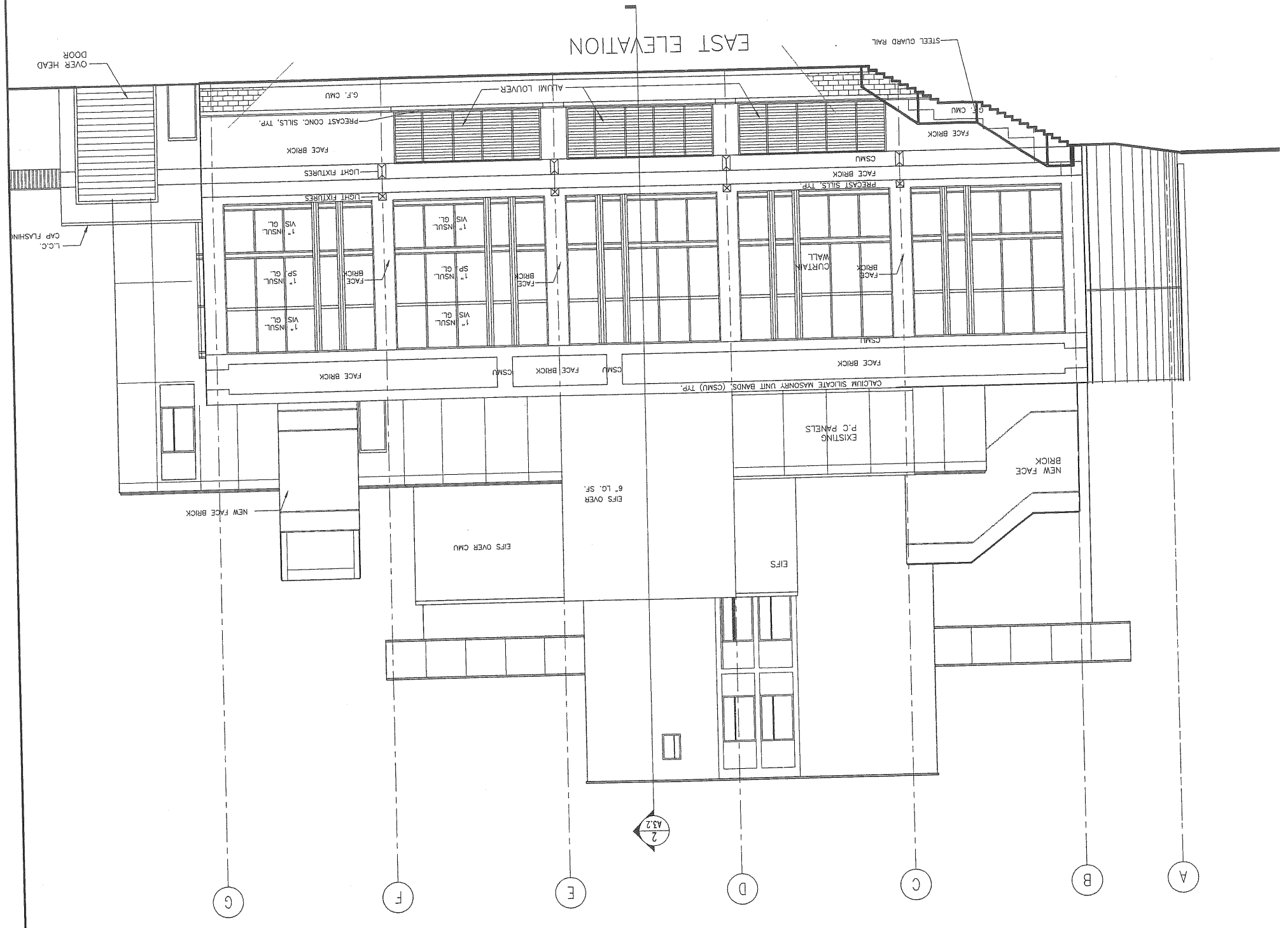
P2

NORTH ELEVATION



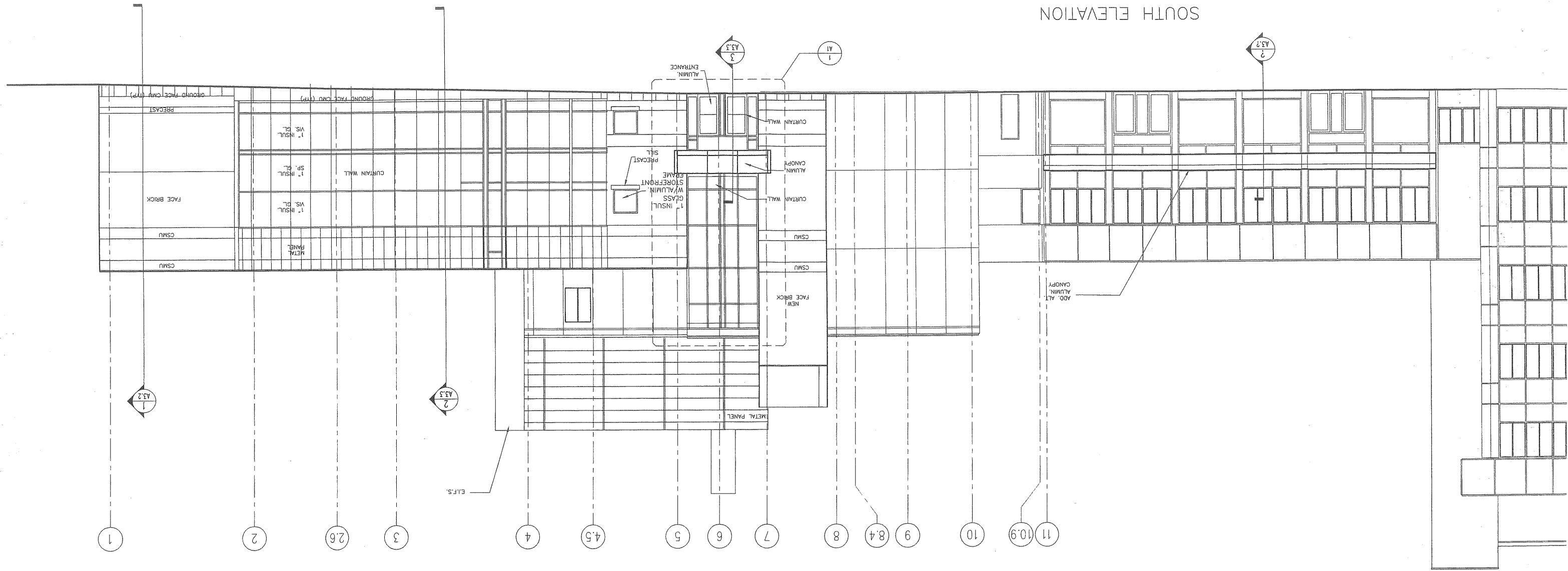
OWNER RE: \_\_\_\_\_ NAME \_\_\_\_\_  
 SCALE: \_\_\_\_\_  
 DRAWN: \_\_\_\_\_ CHECKED: \_\_\_\_\_  
 PROJECT: \_\_\_\_\_  
 JOB NO: \_\_\_\_\_  
 SYM: \_\_\_\_\_

EAST ELEVATION



- EL. 43'-2-1/4" BASEMENT FLOOR
- EL. 55'-0" 1st FLOOR
- EL. 68'-1-1/2" 2nd FLOOR
- EL. 81'-3" 3rd FLOOR
- EL. 95'-3" 4th FLOOR

SOUTH ELEVATION



DATE: 10/11/11  
PROJECT: 1000000000  
DRAWING: 1000000000  
SCALE: 1/8\"



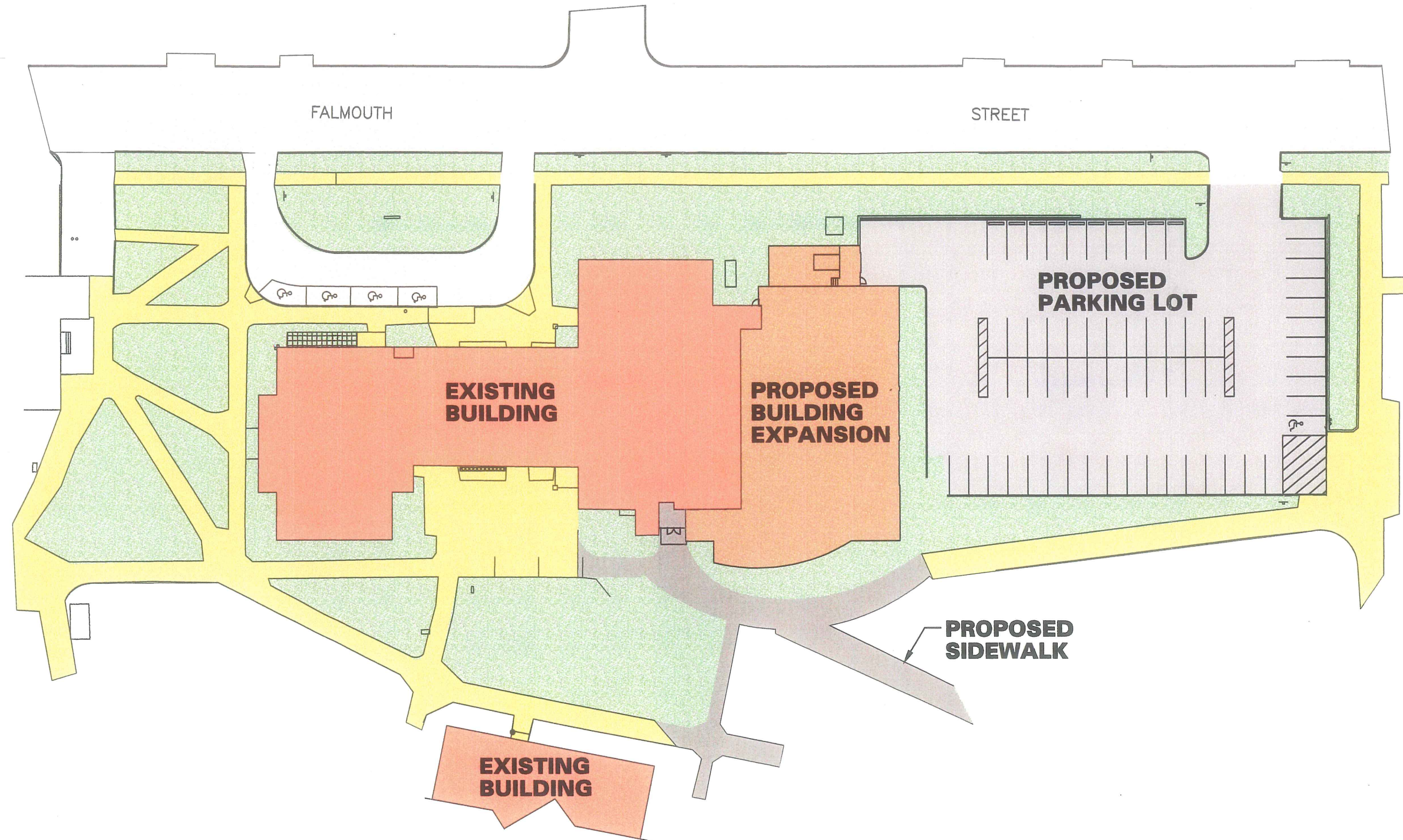


**Mall Perspective**



**Biosciences Institute**  
University of Southern Maine  
Portland, Maine





**Biosciences Institute  
University of Southern Maine  
Portland, Maine**



PRELIMINARY NOT FOR CONSTRUCTION

- X ON STREET PARKING AREAS
- X USM DESIGNATED PARKING AREAS

USM PORTLAND CAMPUS		DeLUCA-HOFFMAN ASSOCIATES, INC. 778 MAIN STREET, SUITE 8 SOUTH PORTLAND, ME 04106 (207) 775-1121 DHI@MAINE.RR.COM	
CAMPUS BOUNDARY AND LOCATION MAP			
UNIVERSITY OF SOUTHERN MAINE PORTLAND, MAINE		DRAWN: SJM   DATE: AUG. 2000 DESIGNED: RED   SCALE: 1" = 200' CHECKED: RED   JOB NO. 2074 FILE NAME: 2074-EXLIMIT.DWG SHEET 1	
REV	DATE	DESCRIPTION	P.E. LIC. #
		REVISIONS	

