

City of Portland  
Development Review Application  
Planning Division Transmittal Form

**Application Number:** 2015-080                      **Application Date:** 05/14/2015  
**CBL:** 113 A025001                                      **Application Type:** Level III Site Plan Under 50,000 sq f  
**Applicant:** Bangor Savings Bank                      /Wendy Durrah  
**Project Name:** Bangor Savings  
**Address:** 20 MARGINAL WAY  
**Project Description:** Three Story, 2,100 Square Footprint bank/office with a double drive thru at 20 Marginal Way in Portland.  
**Zoning:** B7

<b>Other Required Reviews:</b>		
<input type="checkbox"/> Traffic Movement	<input type="checkbox"/> 14-403 Streets	<input type="checkbox"/> Housing Replacement
<input type="checkbox"/> Storm Water	# Units _____	<input type="checkbox"/> Historic Preservation
<input type="checkbox"/> Subdivision	<input type="checkbox"/> Flood Plain	<input checked="" type="checkbox"/> Other: <b>Conditional Use Review</b>
# Lots _____	<input type="checkbox"/> Shoreland	
<input type="checkbox"/> Site Location	<input type="checkbox"/> Design Review	
# Unit _____		

**Distribution List:**

<b>Planner</b>	Jean Fraser	<b>Parking</b>	John Peverada
<b>Zoning</b>	Ann Machado	<b>Design Review</b>	Caitlin Cameron
<b>Traffic Engineer</b>	Tom Errico	<b>Corporation Counsel</b>	Jennifer Thompson
<b>Civil Engineer</b>	David Sensus	<b>Sanitary Sewer</b>	John Emerson
<b>Fire Department</b>	Keith Gautreau	<b>Inspections</b>	Tammy Munson
<b>City Arborist</b>	Jeff Tarling	<b>Historic Preservation</b>	Deb Andrews
<b>Engineering</b>	David Margolis-Pineo	<b>DRC Coordinator</b>	Phil DiPierro
		<b>Outside Agency</b>	

**Comments needed by 6/5/2015**



Jeff Levine, AICP  
Director, Planning & Urban Development Department

January 22, 2016

David Latulippe  
C J Developers, Inc  
35 Primrose Lane  
Freeport, ME 04032

Project Name: **AMENDMENT re entrance design;  
Bangor Savings Bank Bank/Office building with two lane drive-through**  
Project ID: #2015-080  
Address: 20 Marginal Way CBL: 113 A 025001  
Applicant: Bangor Savings Bank  
Planner: Jean Fraser

Dear David;

I refer to your letter addressed to Jeff Levine dated October 22, 2015 which explained the need for the previously approved site and grading plans to be revised to allow for raising the finished floor level by one foot. We appreciate that this was a prudent decision based on the recent rain events.

Unfortunately this necessitated the introduction of two sets of ramps and steps between the sidewalk and the Bank entrance doors where previously these had been at grade. The implications for the sidewalk, bicycle parking, Marginal Way Improvement Plan and for ADA accessibility resulted in a somewhat complicated review process, and I appreciate your patience as we considered these issues.

The final amended plans were approved on January 6, 2016 and this letter confirms that approval. Doug Reynolds of Gorrill Palmer has provided us with paper copies which have been circulated to the site inspectors.

Sincerely,

Jean Fraser, Planner

**Electronic Distribution:**

cc: Jeff Levine, AICP, Director of Planning and Urban Development  
Stuart G. O'Brien, City Planning Director  
Barbara Barhydt, Development Review Services Manager  
Rick Knowland, Senior Planner  
Caitlin Cameron, Urban Designer  
Philip DiPierro, Development Review Coordinator, Planning  
Bruce Hyman, Transportation Program Manager  
Ann Machado, Zoning Administrator, Inspections Division  
Tammy Munson, Inspections Division Director  
Jonathan Rioux, Inspections Division Deputy Director  
Jeanie Bourke, Plan Reviewer/CEO, Inspections Division  
Brad Saucier, Administration, Inspections Division  
Katherine Earley, Engineering Services Manager, Public Services  
Bill Clark, Project Engineer, Public Services  
David Margolis-Pineo, Deputy City Engineer, Public Services  
Greg Vining, Associate Engineer, Public Services  
Rhonda Zazzara, Field Inspection Coordinator, Public Services  
Jeff Tarling, City Arborist, Public Services  
Jeremiah Bartlett, Public Services  
Keith Gautreau, Fire Department  
Thomas Errico, P.E., TY Lin Associates

# CITY OF PORTLAND, MAINE

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## PLANNING BOARD

Stuart O'Brien, Chair  
Elizabeth Boepple, Vice Chair  
Sean Dundon  
Dave Eaton  
Bill Hall  
Carol Morrissette  
Jack Soley

July 22, 2015

Wendy Durrah  
Bangor Savings Bank  
99 Franklin Street  
Bangor, ME 04401

David Latulippe  
C J Developers, Inc  
35 Primrose Lane  
Freeport, ME 04032

Gorrill-Palmer  
PO Box 1237  
Gray, ME 04039

Project Name: **Bangor Savings Bank Bank/Office building with two lane drive-through**  
Project ID: #2015-080  
Address: 20 Marginal Way CBL: 113 A 025001  
Applicant: Wendy Durrah, Bangor Savings Bank  
Planner: Jean Fraser

Dear Ms Durrah:

On July 14, 2015, the Planning Board considered your application for a three story bank and office development with a two lane drive through at 20 Marginal Way. The Planning Board reviewed the proposal for conformance with the standards of the Conditional Use Review and Site Plan Ordinances of the Land Use Code. The Planning Board voted 7-0 to approve the application with the following waivers and conditions as presented below.

### WAIVERS

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations, contained in the Planning Board Report for the public hearing on July 14, 2015 for application #2015-080 (20 Marginal Way) relevant to Portland's Technical and Design Standards, the B7 Design Guidelines and Standards and other regulations, and the testimony presented at the Planning Board hearing:

1. Driveway Separation

The Planning Board voted 7-0 to waive Technical Design Standard Section 1.7.1.7 *Location and spacing of driveways* to allow the proposed driveway to be located as shown on Site Plan Plan 4.

2. Bicycle parking dimensional standards

The Planning Board voted 7-0 to waive Technical Design Standard Section 1.15.2 which specifies a minimum of 24 inches between the bike rack and the building wall, to allow a separation of 12 inches, subject to the required bicycle parking being met on the non-building side of the rack.

3. Building Signage:

The Planning Board voted 7-0 to waive *B7 Mixed Use Urban District Zone Design Principles and Standard E-16: Signage* to allow a total of two signs at the pedestrian level and three signs at upper levels, and to allow illuminated individual plastic letters, to address the unique circumstances of the site and the user.

### CONDITIONAL USE

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report for the public hearing on July 14, 2015 for application #2015-080 (Conditional Use and Site Plan) (20 Marginal Way), relevant to Portland's Conditional Use Standards and other regulations, and the testimony presented at the Planning Board hearing:

The Planning Board voted 7-0 that the proposed conditional use for a bank drive-through associated with the new Bangor Savings Bank at 20 Marginal Way in the B-7 zone does meet the standards of §14-474 and the standards of §14-296(a)1(c) for the B-7 zone.

## SITE PLAN REVIEW

On the basis of the application, plans, reports and other information submitted by the applicant; findings and recommendations contained in Planning Board Report for the public hearing on July 14, 2015 for application #2015-080 (Conditional Use and Site Plan) (20 Marginal Way), relevant to the site plan regulations; and the testimony presented at the Planning Board hearing; the Planning Board voted 7-0 that the plan is in conformance with the site plan standards of the land use code, subject to the following conditions of approval:

- i. That this approval is subject to the Zoning Text Amendments relating the bank drive-throughs in the B7 zone, as recently approved by the City Council, taking effect without delays such as an appeal; and
- ii. That the Planning Board has established that the maximum level of parking is 24 parking spaces; and
- iii. That the applicant shall reconsider the window design, including transparency, color, and size, in consultation with the City's Urban Designer, to meet the intent of the B-7 Design Principles and Standards. The final design to be reviewed and approved by the Planning Authority prior to the issuance of a building permit; and
- iv. That the applicant shall submit a revised Landscape Plan, for review and approval by the City Arborist and the Planning Authority prior to the issuance of a building permit, that adds one tree to be planted in the rear right side traffic island and adds vegetation to the proposed rear curbing area to address the City Arborist comments dated 7.2.2015; and
- v. That the developer/contractor/subcontractor must comply with conditions of the construction stormwater management plan and sediment and erosion control plan submitted May 2015 based on City standards and state guidelines. The owner/operator of the approved stormwater management system and all assigns shall comply with the conditions of Chapter 32 Stormwater including Article III, Post Construction Stormwater Management, which specifies the annual inspections and reporting requirements; and
- vi. That the applicant shall submit a copy of the ability to serve (water supply and wastewater) letters prior to the issuance of a building permit; and
- vii. That the applicant shall address the Department of Public Services comments dated 7.2.2015 to revise the notes on the Site Plan prior to the issuance of a building permit; and
- viii. That the applicant shall submit, for review and approval by the Planning Authority prior to the issuance of a building permit, a revised photometric plan for the rear parking lot area that includes the lighting fixture specification, reduces trespass along the eastern boundary to meet Technical Standards, and increases the lighting levels in the vicinity of the drive-through; and
- ix. That separate permits are also required for all new signs, including those on the building, and for HVAC systems, which require submission of dBA levels to confirm compliance with the maximum sound limits of the ordinance.

The approval is based on the submitted plans and the findings related to conditional use and site plan review standards as contained in Planning Report for application #2015-080 which is attached.

## STANDARD CONDITIONS OF APPROVAL

Please note the following standard conditions of approval and requirements for all approved site plans:

1. **Develop Site According to Plan** The site shall be developed and maintained as depicted on the site plan and in the written submission of the applicant. Modification of any approved site plan or alteration of a parcel which was the subject of site plan approval after May 20, 1974, shall require the prior approval of a revised site plan by the Planning Board or Planning Authority pursuant to the terms of Chapter 14, Land Use, of the Portland City Code.
2. **Separate Building Permits Are Required** This approval does not constitute approval of building plans, which must be reviewed and approved by the City of Portland's Inspection Division.

3. **Site Plan Expiration** The site plan approval will be deemed to have expired unless work has commenced within one (1) year of the approval or within a time period up to three (3) years from the approval date as agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the one (1) year expiration date.
4. **Performance Guarantee and Inspection Fees** A performance guarantee covering the site improvements, inspection fee payment of 2.0% of the guarantee amount and seven (7) final sets of plans must be submitted to and approved by the Planning Division and Public Services Department prior to the release of a building permit, street opening permit or certificate of occupancy for site plans. If you need to make any modifications to the approved plans, you must submit a revised site plan application for staff review and approval.
5. **Defect Guarantee** A defect guarantee, consisting of 10% of the performance guarantee, must be posted before the performance guarantee will be released.
6. **Preconstruction Meeting** Prior to the release of a building permit or site construction, a pre-construction meeting shall be held at the project site. This meeting will be held with the contractor, Development Review Coordinator, Public Service's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the Development Review Coordinator will confirm that the contractor is working from the approved site plan. The site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the pre-construction meeting. (If applicable)
7. **Department of Public Services Permits** If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)
8. **As-Built Final Plans** Final sets of as-built plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (\*.dwg), release AutoCAD 2005 or greater.

The Development Review Coordinator must be notified five (5) working days prior to the date required for final site inspection. The Development Review Coordinator can be reached at the Planning Division at 874-8632. All site plan requirements must be completed and approved by the Development Review Coordinator prior to issuance of a Certificate of Occupancy. Please schedule any property closing with these requirements in mind.

If there are any questions, please contact Jean Fraser at (207) 874-8728 and via [jf@portlandmaine.gov](mailto:jf@portlandmaine.gov).

Sincerely,

Stuart O'Brien, Chair  
Portland Planning Board

**Attachments:**

1. City Arborist Review comments dated 7.2.2015
2. DPS comments dated 7.2.2015
3. B7 Design Review comments 7.8.2015
4. Planning Board Report
5. City Code Chapter 32
6. Sample Stormwater Maintenance Agreement
7. Performance Guarantee Packet

(continued)

**Electronic Distribution:**

Jeff Levine, AICP, Director of Planning and Urban Development  
Barbara Barhydt, Development Review Services Manager  
Jean Fraser, Planner  
Philip DiPierro, Development Review Coordinator, Planning  
Caitlin Cameron, Urban Designer, Planning  
Ann Machado, Zoning Administrator, Inspections Division  
Tammy Munson, Inspections Division Director  
Jonathan Rioux, Inspections Division Deputy Director  
Jeanie Bourke, Plan Reviewer/CEO, Inspections Division  
Lannie Dobson, Administration, Inspections Division  
Brad Saucier, Administration, Inspections Division  
Michael Bobinsky, Public Services Director  
Katherine Earley, Engineering Services Manager, Public Services  
Bill Clark, Project Engineer, Public Services  
David Margolis-Pineo, Deputy City Engineer, Public Services  
Doug Roncarati, Stormwater Coordinator, Public Services

Greg Vining, Associate Engineer, Public Services  
Michelle Sweeney, Associate Engineer  
John Low, Associate Engineer, Public Services  
Rhonda Zazzara, Field Inspection Coordinator, Public Services  
Mike Farmer, Project Engineer, Public Services  
Jane Ward, Administration, Public Services  
Jeff Tarling, City Arborist, Public Services  
Jeremiah Bartlett, Public Services  
Keith Gautreau, Fire Department  
Jennifer Thompson, Corporation Counsel  
Thomas Errico, P.E., TY Lin Associates  
David Senus, P.E., Woodard and Curran  
Rick Blackburn, Assessor's Department  
Approval Letter File

**From:** Jeff Tarling  
**To:** Errico Thomas; Jean Fraser  
**CC:** Barbara Barhydt; Caitlin Cameron; David Margolis-Pineo  
**Date:** 7/2/2015 4:09 PM  
**Subject:** Re: Fwd: 2970 Bangor Savings - Marginal Way  
**Attachments:** Beachgrass.JPG; BGisland.JPG

Jean -

The proposed 2970 Bangor Savings Bank landscape plan for Marginal Way is acceptable with the condition that ONE tree is planted in the rear right side traffic island and vegetation is added to the proposed rear curbing area.

The attached photos demonstrate that ornamental grasses can grow in restricted spaces, this is an MDOT project called Hagis Parkway / Route One in Scarborough where they successfully use beach grass in a high salt area with good results. The overall Bangor Savings Bank project has much less than average landscape percentage due to lot orientation and circulation patterns, however a simple planting of low maintenance beach grass would work, thus is a recommended condition of approval.

Thanks,

Jeff Tarling  
City Arborist

**From:** David Margolis-Pineo  
**To:** Jean Fraser  
**CC:** Barbara Barhydt; Tom Errico  
**Date:** 7/2/2015 11:48 AM  
**Subject:** 20 Marginal Way

Jean,

July 2, 2015

**To:** Jean Fraser  
Barbara Barhydt  
**From:** David Margolis-Pineo  
**Re:** 20 Marginal Way (2015-080) – Bangor Savings Drive Thru

The Department of Public Services has the following final comments on the proposed project mentioned above.

1. Revise note on Sheet C101 which currently states "Plug or remove existing san sewer" to read, "Remove or grout fill sewer to be abandoned within the road right of way."

We have no further comments.



## Planning and Urban Development Department Planning Division



**Subject:** B7 Design Review – 20 Marginal Way

**Written by:** Caitlin Cameron, Urban Designer

**Date of Review:** Wednesday, July 8, 2015

On Wednesday, July 8, a design review according to the *City of Portland Design Manual* Standards was performed for the new construction proposal for redevelopment at 20 Marginal Way. The revisions were reviewed by Caitlin Cameron, Urban Designer, and Jean Fraser, Planner, Planning Division of the Department of Planning & Urban Development. The project was reviewed against the *B-7 Mixed Use Urban District Zone Design Principles & Standards* (Appendix 4 of the Design Manual).

### **Design Review Criteria:**

The project must meet all *B-7 Design Principles & Standards*.

### **Findings of the Design Review:**

Overall the proposal meets the B-7 Design Guidelines with the exception of the signage guidelines for which the applicant is seeking waivers. The building is of a scale and orientation conducive to an urban, multi-modal, and on-peninsula site. The ground level provides an active frontage with storefront and street-facing entries. The project also provides a sustainable design employing solar panels.

The proposed design currently **does not pass** all of the criteria of the *B7 Design Guidelines*. Refer to comments below regarding **waiver requests** and design elements which do not meet the standards - Staff recommends **condition of approval** wherein the window design issues including transparency, color, and size, be resolved with City staff including the Urban Designer to meet the intent of the B-7 Design Principles and Standards.

### **Principle A: Urban Design**

**A-5: Pedestrian Environment** – *Development on public streets or public spaces shall . . . enhance the pedestrian environment through the use of elements at the first floor such as fully functioning entries oriented to the street; active windows and storefronts; awnings and weather protection; appropriately scaled streetlights; trees and landscaping; . . .*

- The project places an **active door and storefront** facing Marginal Way and **contributes to the street wall. The sidewalk is placed adjacent to the building** as recommended by staff. The applicant recessed the entries in order to accommodate the outward door swing into the public right-of-way. **A canopy is used to emphasize the entry points** of the building.

**A-7: Building Orientation** – *The primary facades and entrances of buildings shall be oriented to streets, major pedestrian routes, or open spaces in order enhance the pedestrian-oriented environment. The primary facades and entrances of buildings shall not be oriented toward parking lots.*

- The project is **oriented towards the street creating a street wall** and includes **street-facing entries** at the sidewalk.

**Principle C: Parking, Loading and Service Areas**

**C-6: Surface Lots** – *Areas devoted to surface parking shall be screened from public rights of way and significant views through the use of design elements such as plantings, fencing, grade changes, and/or walls. A landscaped border shall be created around all surface parking lots. Any parking lot containing ten (10) or more parking spaces shall include one (1) or more landscaped islands within the interior of the lot. There shall be at least one (1) island for every twenty (20) spaces.*

- Staff suggests landscaping choices that are tall enough to **provide adequate screening from neighboring properties and views of parking from the street**. Staff does not support a waiver request from the landscaping requirement from an urban design standpoint.

**Principle E: Architectural Design**

**E-12: Materials** – *Facades visible from public rights of way shall use natural and authentic building materials that are expected to last at least 50 years. Predominant materials shall be brick, stone, precast concrete and other masonry products, wood, glass, and high quality metals such as steel, titanium and copper. . . . Materials such as thin gauge metal panels, exterior insulation and finish systems (EIFS), panelized “thin brick,” vinyl siding, or stucco on Styrofoam or a similar backing shall not be used on facades visible from the public rights of way. Fiber-cement clapboard and shingles may be used. Fiber cement panels shall only be used on portions of the building not visible from public rights of way. Public spaces shall be constructed of permanent, durable materials such as concrete, brick or stone.*

- **The building material palette** is similar to the recent commercial buildings on Marginal Way and **meets the design standard** – brick, composite panels, and storefront.

**E-7: Windows** – *The first floor transparency (minimum visible transmittance (VT) of .7 or greater) along public streets and the trail shall be equal to at least 50% of the wall area between the height of 2 and 9 feet. The first floor windows and storefronts shall be transparent with active uses visible behind them. Opaque glass shall not be allowed at the first floor level [See Standard E-13 Transparency]. Upper floors of all new buildings shall have at least 15% to 40% transparency of wall surface requirements. If it can be demonstrated that the building program precludes windows along first floor street frontages, then other surface details shall be used in accordance with Standard E-4 Articulation.*

- **Proposed glass has a VLT of .56 which is lower than the allowed .7. A sample is required** before staff can assess whether the transparency is adequate.
- **Spandrel glass (opaque) is proposed** for the upper portion of the ground floor storefront which is **not allowed by the Standard**. See comments below.

**E-8: Storefronts** - *Storefront glass shall be transparent in accordance with Standard E-13: Transparency, and shall not be blocked with opaque glass, or other means.*

**Jean Fraser - 20 Marginal Way - condition of approval iii**

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**From:** Caitlin Cameron  
**To:** Fraser, Jean  
**Date:** 7/27/2015 2:00 PM  
**Subject:** 20 Marginal Way - condition of approval iii  
**CC:** David Latulippe

*does this in permit for bldg.*

I am confirming that for 20 Marginal Way, Condition of Approval iii. regarding the glazing has been met with the following agreement:

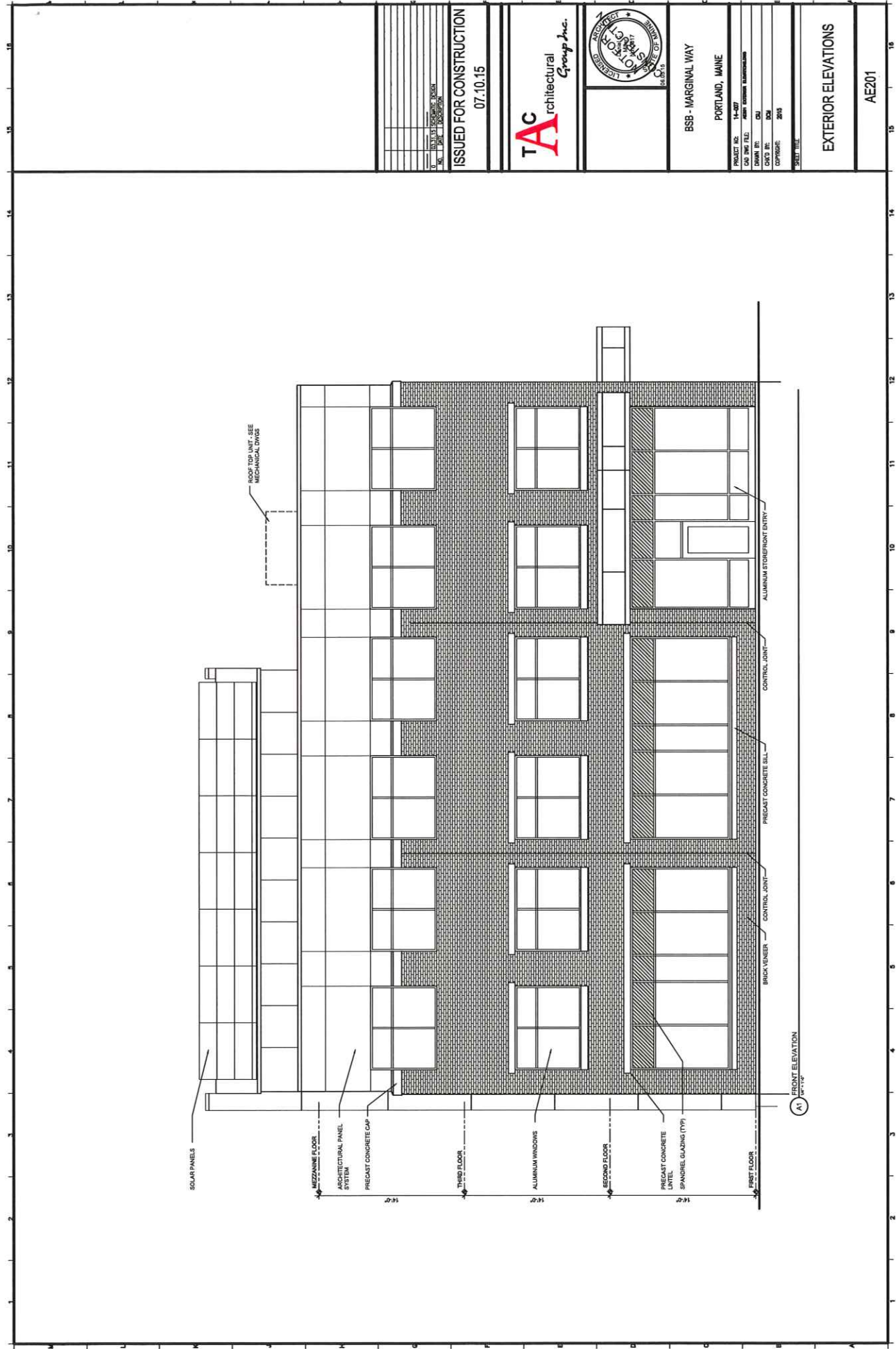
- Building elevations remain the same
- Glazing for entire building shall be Solexia + Solarban 60 with a VT of .61 and which has a slight green tint. The lower Visible Transmittance and slight green tint are allowed as a compromise for better energy performance in the building and with the understanding that interior shades will not be used.
- Spandrel glass shall be used on the transom section of storefront on the first floor only (as shown in the elevations) to mask the floor plate behind.

Let me or the applicant know if anything else is needed to show the condition of approval is met.

Caitlin Cameron, LEED AP, Associate AIA  
Urban Designer | Planning & Urban Development Department  
City of Portland, Maine  
389 Congress Street, 4th Floor Portland, ME 04101  
(207) 874-8901 | [ccameron@portlandmaine.gov](mailto:ccameron@portlandmaine.gov)

*2015-080*

*CR resp. to comments  
plans 7.16.15*



SOLAR PANELS

ROOF TOP UNIT - SEE MECHANICAL DWG

MIZZANINE FLOOR

ARCHITECTURAL PANEL SYSTEM

PRECAST CONCRETE CAP

THIRD FLOOR

ALUMINUM WINDOWS

SECOND FLOOR

PRECAST CONCRETE LANTERNE SPANDREL GLAZING (TYP)

FIRST FLOOR

BRICK VENEER

CONTROL JOINT

CONTROL JOINT

PRECAST CONCRETE SILL

CONTROL JOINT

ALUMINUM STOREFRONT ENTRY

(A) FRONT ELEVATION  
1/8" = 1'-0"

NO.	DATE	DESCRIPTION
1	07.10.15	ISSUED FOR CONSTRUCTION

ISSUED FOR CONSTRUCTION  
07.10.15

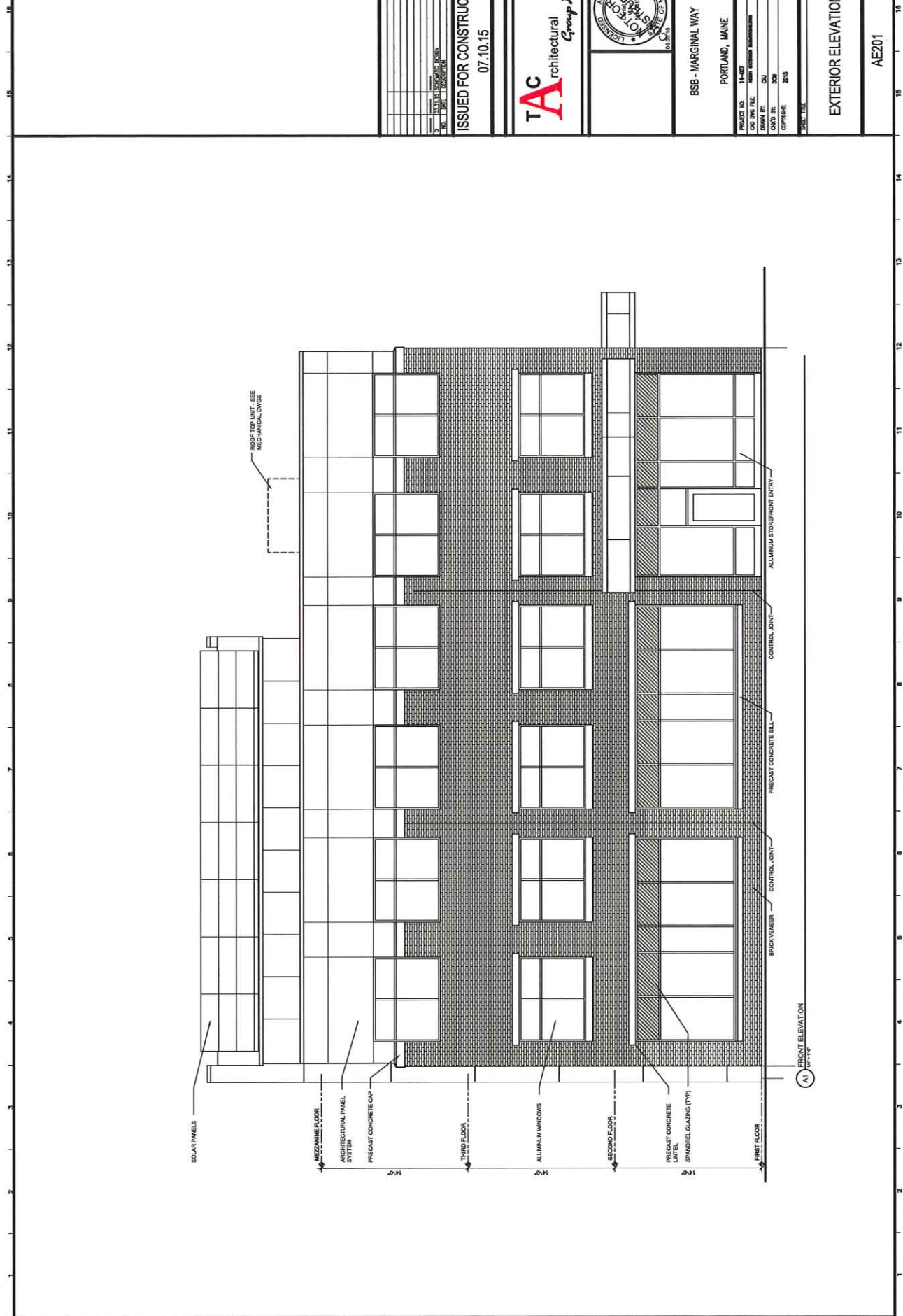
**TAC** architectural  
Group Inc.

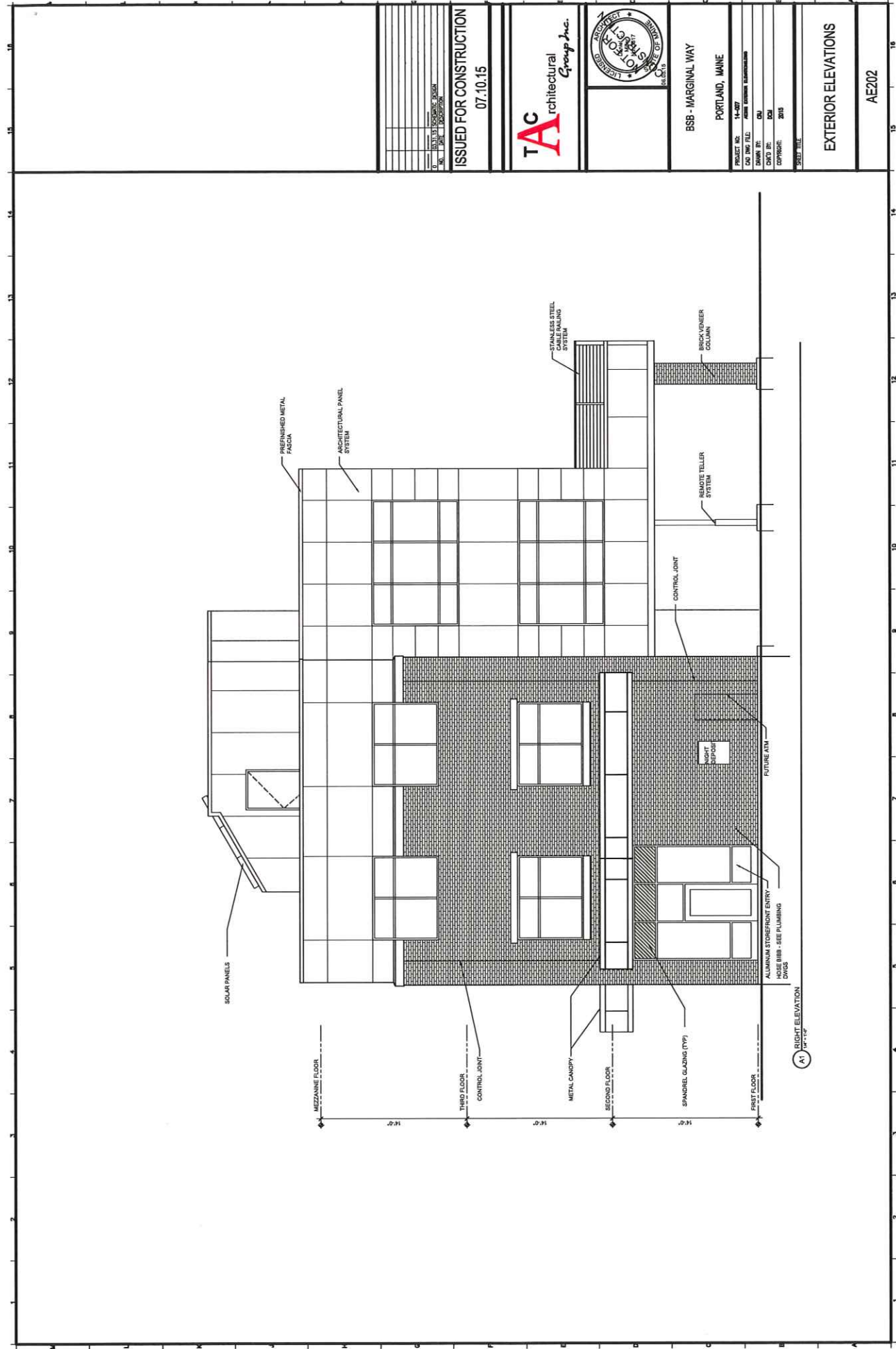


BSS - MARGINAL WAY  
PORTLAND, MAINE

PROJECT NO: 14-007  
CAD FILE FILE: MAIN EXTERIOR ELEVATION  
DRAWN BY: DJJ  
CHECK BY: DJJ  
DATE: 07.10.15

EXTERIOR ELEVATIONS  
AE201





ISSUED FOR CONSTRUCTION  
07.10.15

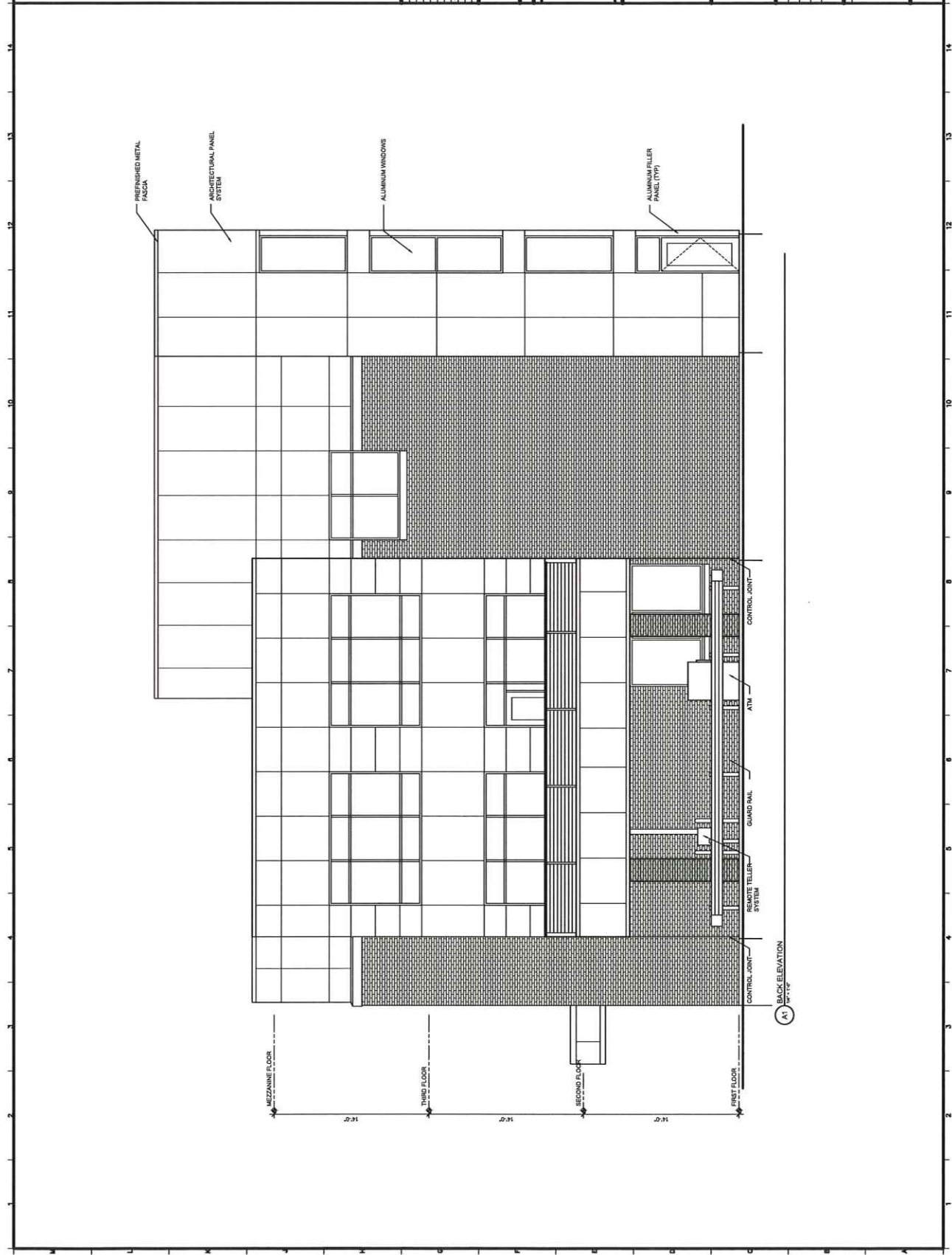


BSS - MARGINAL WAY  
PORTLAND, MAINE



PROJECT NO. 14-007  
CAD FILE: 1408 EXTERIOR ELEVATIONS  
DRAWN BY: CJM  
CHECKED BY: DCJ  
DATE: 2013  
SHEET TITLE

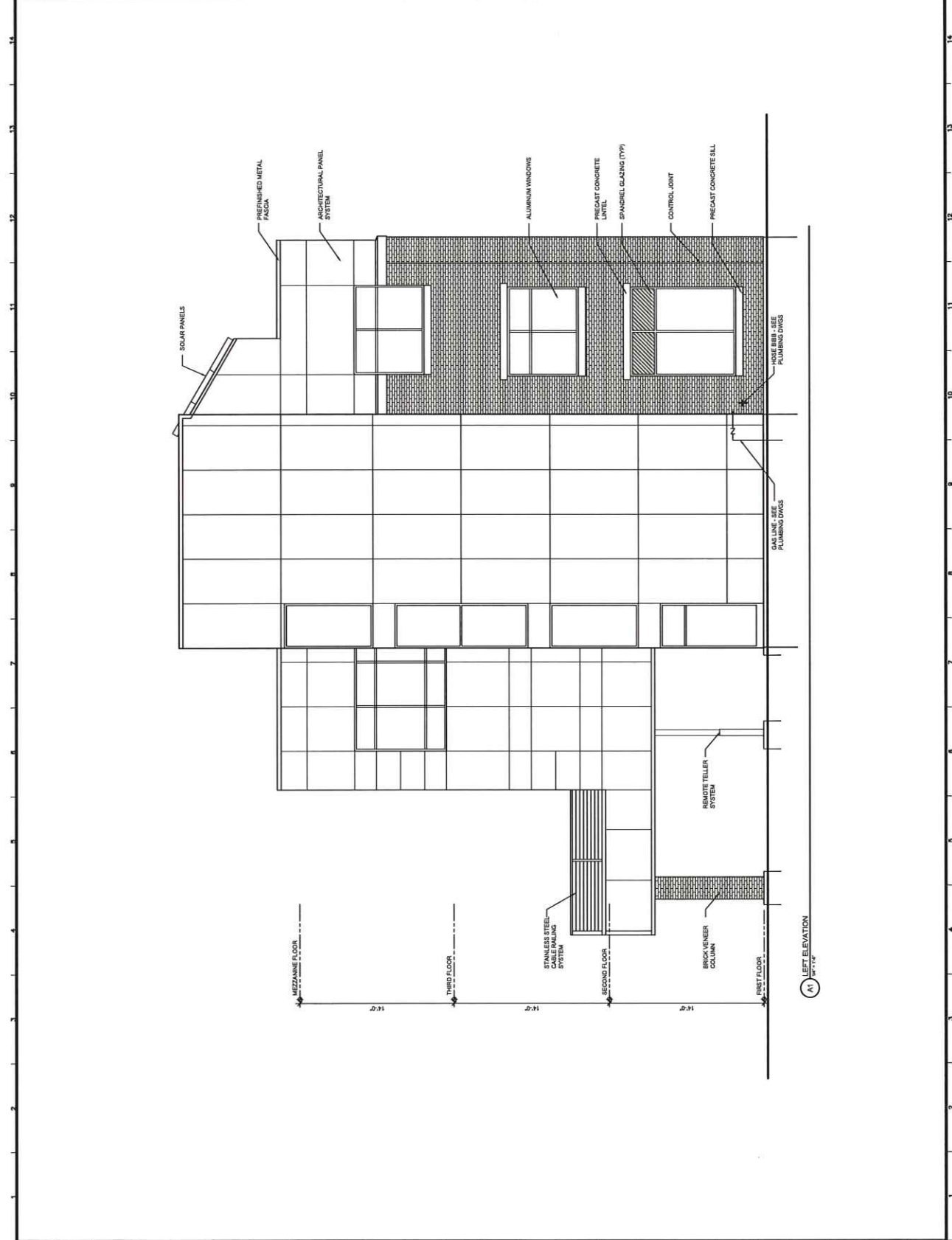
EXTERIOR ELEVATIONS  
AE202

PROJECT NO. 14-007 DO NOT FILE - ARCHITECTURAL SUBMITTALS DRAWN BY: CM CHECK BY: DM CONTRACT: 2013 SHEET TITLE:	
BSB - MARGINAL WAY PORTLAND, MAINE	
ISSUED FOR CONSTRUCTION 07.10.15	
<b>TAC</b> architectural Group Inc.	
EXTERIOR ELEVATIONS AE203	



A1 BACK ELEVATION  
07/14/15

ISSUED FOR CONSTRUCTION 07.10.15				BSP - MARGINAL WAY PORTLAND, MAINE	
PROJECT NO: 14-007 DATE: 07.10.15 DRAWN BY: CJM CHECKED BY: JCM COPYRIGHT: 2015 SHEET TITLE:				EXTERIOR ELEVATIONS AE204	



(A) LEFT ELEVATION

# PLANNING BOARD REPORT PORTLAND, MAINE



Bank/Office Building with Bank Drive-through, 20 Marginal Way  
Level III Conditional Use and (Level II) Site Plan  
Bangor Savings Bank, Applicant  
Project #: 2015-080  
CBL 113 A025001

Submitted to: Portland Planning Board Public Hearing Date: July 14 <sup>th</sup> , 2015	Prepared by: Jean Fraser, Planner Date: July 10 <sup>th</sup> , 2015
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## I. INTRODUCTION

Bangor Savings Bank has applied for conditional use and site plan approval for a three story (7,284 sq ft floorspace) bank and office building, including a drive-through for the bank at the rear of the building. The lot at 20 Marginal Way is 17,862 sq ft and located within the B-7 zone. The proposal includes 24 parking spaces and landscaping.

The Board saw this proposal at a conceptual stage when reviewing the Bank's request for a text amendment to the B7 drive-through conditions. In June the City Council approved the text amendments which support the proposal by allowing this smaller scale of floorspace in the development.

Staff have brought the associated site plan and conditional use application direct to a Hearing as the proposal is similar in all key respects to the proposal presented during the meetings on the text amendments and the applicant met with staff to resolve design details during that review.

This use requires a conditional use permit to be approved by the Planning Board and to meet the requirements of the B-7 Conditional Use standards (§14-296(a)1(c)) as well as the B7 Design Standards.



## II. REQUIRED REVIEWS

<i>Applicant's Proposal</i>	<i>Applicable Standards</i>
New structure of 7,284 sq ft	Level II Site Plan
Bank Drive-through in B7 zone	Level III Conditional Use in the B7 zone (§14-296(a)1(c))

## III. WAIVER REQUESTS

*Note: the applicant originally requested a waiver from Parking Lot Landscaping but now intends to meet the Ordinance requirements. (See section IX C ii)*

<i>Applicant's Proposal</i>	<i>Applicable Standards</i>
<u>Driveway Separation:</u> The proposed drive access does not meet separation standards in relation to the PO access.	<i>Technical Manual Section 1.7.1.7 Location and spacing of driveways</i> specifies minimum spacing of driveways. Staff support this waiver.
<u>Bicycle Parking Dimensional Standards:</u> the proposed bike racks are closer to the building than specified by the standards due to limited site area, but the required spaces will be accessible from the non-building side.	<i>Technical Manual Section 1.15.2 Placement of off street bicycle parking racks</i> requires a 24" separation form a building to allow access. The proposal provides 12" but does not count the parking spaces on that side towards the total required and the waiver is supported by staff.
<u>Building Signage:</u> the Master Signage Plan (Plan P12) proposes more signs than allowed by the standards and incorporates materials that are not mentioned in the standards.	<i>B7 Mixed Use Urban District Zone Design Principles and Standard E-16: Signage</i> specifies the number of building signs allowed and associated technical specifications. Staff support the waiver for materials but not for the number of proposed signs.



<p><u>Glass Transparency:</u> the applicant is requesting a waiver from the B7 Design Standards because of the need to take account of the solar gain on the south facing large windows</p>	<p><i>B7 Mixed Use Urban District Zone Design Principles and Standard E-13: Transparency</i> specifies that windows that have daylighting application on all levels of the façade shall use glass with a visible transmittance (VT) value of .7 or greater, which looks clear. . . . Opaque, heavily tinted or reflective glass shall not be used at the pedestrian level unless it can be demonstrated that the building program precludes the use of transparent glass. Staff needs more information and a sample before a recommendation can be made. A condition of approval regarding the window design is suggested. See Design Review comments (<u>Att. 6</u>).</p>
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**IV. PROJECT DATA**

<i>SUBJECT</i>	<i>DATA</i>
<b>Existing Zoning</b>	B- 7
<b>Existing Use</b>	Car Wash
<b>Proposed Use</b>	Bank , offices and bank drive-through (new building)
<b>Parcel Size</b>	17,862 sq ft
<b>Impervious Surface Area</b>	
--Existing	15,579 sq ft
--Proposed	15,976 sq ft
--Net Change	+397sq ft
<b>Total Disturbed Area</b>	Approx 85,000 sq ft
<b>Building Footprint</b>	
--Existing	2,706 sq ft
--Proposed	2,100 sq ft
--Net Change	(606 sq ft)
<b>Building Floor Area</b>	
--Existing	2,706 sq ft
--Proposed	7,284 sq ft
--Net Change	+4,578 sq ft
<b>Parking Spaces</b>	
-Existing	9
-Proposed	24
# handicapped	2
<b>Bicycle parking Spaces</b>	
-Existing	0
-Proposed	2
<b>Estimated cost of the project</b>	\$1,800,000

**V. BACKGROUND AND EXISTING CONDITIONS**

**Zoning**

The applicant has requested and received a text amendment to the B7 Conditional Use requirements that apply to this proposal to allow bank drive-throughs in buildings with a smaller floor area than 20,000 sq ft. The Planning Board reviewed the text amendments in early 2015. During that review the applicant submitted detailed site plan proposals and associated renderings and details, although at the time staff clearly indicated that a detailed site plan review had not been undertaken (for example, in respect of architectural design, lighting, landscaping etc). The amended text added requirements for “...storefront windows, entries, and interior public space oriented to and visible from the street, with front entry access facing the street and directly accessible from the public sidewalk” which need to be addressed in the site plan.

**Existing conditions**

The existing site is a busy car wash housed in a single-story building (approx. 2700 sq ft). The owner obtained traffic generation information in mid-2014 (see Attachment F) that indicated that this use generates up to 118 trips/hour during the peak hour.



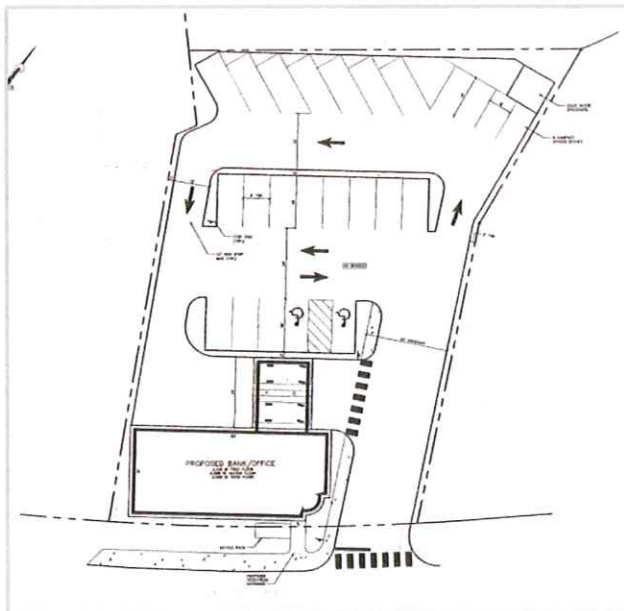
To the east the site abuts the Post Office site (an expanse of paved area/parking) which is separated by a chain link fence. To the west is an area of mature conifers that is part of a site owed by Central Maine Power, with the north boundary along MDOT and that buffers the Rt I-295 interchange.

### VI. PROPOSED DEVELOPMENT

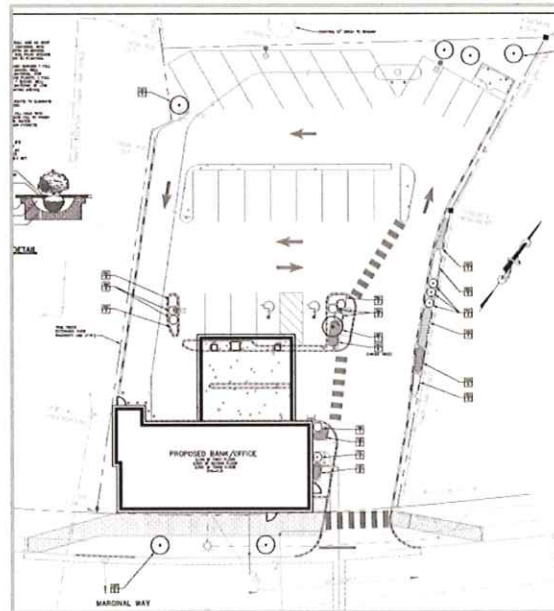
The proposals are described in Attachments A, B, C and D and illustrated in the attached Plan Set. The existing car wash is proposed to be demolished and replaced with:

- A new three story mainly brick bank/office building using high quality materials;
- Strong and attractive interface with Marginal Way, including elements identified by and integrated with the Marginal Way Improvement Plan;
- Double drive-through located behind the building with any stacking away from Marginal Way;
- Inclusion of "green" initiatives such as the prominent solar panels and Electric Vehicle Charging Stations.

Since the development of the Concept Plan for the zoning amendments, the applicant has worked with staff to improve the elevation design, door entrances facing Marginal Way, pedestrian accommodations, landscaping and treatment along the frontage, as shown in a comparison of the plans below:



**ZONING CONCEPT**



**CURRENT FINAL PROPOSAL  
(Landscape Plan in Plan P10)**

The final elevations are shown in architectural format and renderings (with and without signage) in Plans P12 and P13, with details in Plans P14 and P15. The extracts below give an indication of the design and this is discussed in greater detail in section IX C iv below Design Review:



From Marginal Way



From I-295 and interchange

## VII. PUBLIC COMMENT

A notice of this Hearing was sent to 49 property owners within 500 feet and interested citizens, and appeared in the July 3<sup>rd</sup> / 6<sup>th</sup>, 2015 editions of the *Portland Press-Herald*. A Neighborhood Meeting is not required for this project and the Planning Division has not received any public comments since the two letters of support that were received during the text amendment review.

## VIII. RIGHT, TITLE AND INTEREST AND FINANCIAL/TECHNICAL CAPACITY

The owner of the property has entered into a term sheet for the lease of the site to Bangor Savings Bank; this is included in Attachment E.

## IX. STAFF REVIEW

### A. ZONING ASSESSMENT

The proposal is a conditional use in the B7 zone and meets all of the B7 dimensional requirements and the parking requirements of Division 20.

The B7 zone includes a particular requirement (14-332.2.c) for the submission of a parking analysis and for the Planning Board to establish the parking requirement for the project. The applicant has submitted a parking analysis (Attachment J) which supports the proposed parking at 24 spaces. Staff support the proposed parking and a suggested condition of approval allows the Board to document that this is the established requirement.

### B. CONDITIONAL USE REVIEW

The applicant has submitted an analysis of how the proposal meets the B7 Conditional Use Standards (Attachment C) and staff confirm that the project meets the requirements of the amended standards as follows:

#### Sec.14-296. Conditional uses.

(a) *The following uses shall be permitted as conditional uses in the B-7 zone as provided in section 14-474 (conditional uses), provided that, notwithstanding section 14-474 (a) or any other provision of this code, the planning board shall be substituted for the board of appeals as the reviewing authority:*

1. *Commercial use:*

- a. *Meeting, convention and exhibition halls limited to a total of twenty-five thousand (25,000) gross square feet of interior floor area.*
- b. *Wholesaling, providing that the wholesale operation is associated with an onsite retail establishment and that the wholesaling component of the facility occupies a building gross floor area of less than fifteen thousand (15,000) square feet.*
- c. *Drive-up banking provided that:*
  - i. *The drive-up is accessory to a banking service occupying a minimum floor area of four thousand (4,000) square feet; and*

- ii. *The drive-up is attached to or included within a building with a minimum floor area of twenty thousand (20,000) square feet, except that for lots of less than 20,000 square feet and in existence as of March 9, 2005, a drive-up may be included in a building of less than 20,000 sq ft.*
- iii. *The drive-up facility is attached or included within a building and/or addition meeting the minimum height of four stores in the Bayside Height District A and three stories in the Bayside Height Districts B and C. For the purposes of this conditional use, the minimum height exceptions contained in Section 14-298 (h) 5, 7, and 8 shall not apply.*

Staff comment: The proposal meets the standards as it is in Bayside Height District B which requires a 3- story building at this site.

- iv. *The first floor of the building shall include banking or other retail storefront uses with storefront windows, entries, and interior public space oriented to and visible from the street, with front entry access facing the street and directly accessible from the public sidewalk; and*

Staff comment: The applicant has revised the proposals a number of times to meet this requirement. The lowest floor (see Plan P15) includes a coffee shop and lobby along the large front windows that face directly onto the sidewalk, and the entrance doors have been located to face the street and be directly accessible from the sidewalk.

- iv. *All drive-up features, such as automated teller machines and service windows, shall not extend nearer than twenty-five (25) feet to the street right-of-way line; and*

Staff comment: All of these features are at least 30 feet from the street ROW line, and in addition are located behind the building and so generally not viewed from the public domain.

- v. *The site must have adequate stacking capacity for vehicles waiting to use these service features without impeding vehicular or pedestrian circulation or creating hazards to vehicular or pedestrian circulation on adjoining streets; and*
- vi. *Drive-up vehicle circulation shall not be located between the building and any adjacent public streets; and*

Staff comment: The proposed vehicle circulation takes the drive-through customers around the rearmost drive and then down the west side to enter the drive-through from the west. This provides stacking area along the west boundary of the site and ensures that it is not near Marginal Way nor across any pedestrian routes.

- vi. *The drive-up shall be limited to two vehicle drive-up lanes; and*

Staff comment: The proposed drive-through comprises two vehicle lanes.

- vii. *The location of any drive-up shall be limited to the geographic area between Somerset/Kennebec Streets/ I-295/Franklin Street/Forest Avenue.*

Staff comment: The proposal is located within the area identified for bank drive-throughs, as shown below.



The following standards 14-474 apply to all conditional uses:

2. *Standards.* The Board shall, after review of required materials, authorize issuance of a conditional use permit, upon a showing that the proposed use, at the size and intensity contemplated at the proposed location, will not have substantially greater negative impacts than would normally occur from surrounding uses or other allowable uses in the same zoning district. The Board shall find that this standard is satisfied if it finds that:

a. The volume and type of vehicle traffic to be generated, hours of operation, expanse of pavement, and the number of parking spaces required are not substantially greater than would normally occur at surrounding uses or other allowable uses in the same zone; and

Staff comment: The traffic generation for the proposed use is a reduction from the previous use (Attachment 2) and the parking layout and circulation area is not substantially greater that would normally occur at surrounding uses.

b. The proposed use will not create unsanitary or harmful conditions by reason of noise, glare, dust, sewage disposal, emissions to the air, odor, lighting, or litter; and

Staff comment: The proposal would not create harmful conditions.

c. The design and operation of the proposed use, including but not limited to landscaping, screening, signs, loading, deliveries, trash or waste generation, arrangement of structures, and materials storage will not have a substantially greater effect/impact on surrounding properties than those associated with surrounding uses or other allowable uses in the zone.

Staff comment: The design and operation of the proposed use is anticipated to have similar effects/impacts as for surrounding uses.

### C. DEVELOPMENT REVIEW (SITE PLAN STANDARDS in Section 14-526)

The applicant has provided a comprehensive application and additional information and revisions in response to staff comments. The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and applicable regulations, and meets the standards except in a few cases where suggested conditions are recommended to fully address site plan standards.

#### i. *Transportation Standards*

##### Impact on Surrounding Street system, Access and Circulation

The applicant has provided a Traffic Analysis (Attachment F) which has been reviewed by Tom Errico, Traffic Engineering reviewer (Attachment 2); the final comments are:

- *The applicant has revised the site circulation roadway widths (they have been narrowed slightly) to accommodate suggested landscaping enhancements and I find conditions to be acceptable. Jeff Tarling has requested the possible widening of the 2 foot raised concrete island to 3-feet with a reduction of the one-way circulatory roadway width from 19 feet to 18 feet. I am supportive of this change, if deemed necessary. I also find the pedestrian facility system to be acceptable given users and site operations.*
- *The applicant has designed the frontage of the project along Marginal Way according to our suggestions and I find it to be acceptable. I would note that this frontage area should be considered an interim streetscape plan as it may change following recommendations identified through the Bayside Transportation Master Plan process. I would note that the project has a net reduction in vehicle traffic (as compared to the former car wash) and therefore contributions to the Marginal Way Master Plan fund is not required.*
- *The proposed driveway does not meet City separation standards with the adjacent Post Office Parking Lot Driveway. I support a waiver from the City's Technical standards as it is preferred that the driveway separation from Forest Avenue be maximized.*

Parking: The Site Plan (Plan P4) shows 24 spaces on the plan, which is slightly over the zoning requirement of 19-20 spaces. As mentioned above under Zoning in IX A, the Planning Board determine the parking requirement for B7 projects and the applicant has submitted a Parking Analysis that supports the provision of 24 spaces (Attachment J). The parking provision is considered by the applicant to be of paramount importance because the new building will be intensively used for various bank office functions.

TDM: The scale of the proposal does not trigger the requirement for a TDM Plan.

## ii. *Environmental Quality Standards*

### Landscape Preservation, Buffers and Parking Lot Landscaping

The site has limited areas for landscaping and the applicant had originally requested a waiver for parking lot landscaping (with contribution to the Tree Fund instead. Staff do not support the waiver because screening of parking is also part of the B7 Design Standards, and because of the absence of landscaping along the eastern boundary (where the view towards the proposed parking is clearest from the sidewalk and street).

Staff met with the applicant and suggested that there was scope for a number of smaller spaces for planting so that cumulatively the landscape would meet the ordinance requirements which state:

(ii) *Parking Lot Landscaping:*

*Developments with five (5) or more parking spaces shall include at least two (2) trees (or one (1) tree and three (3) shrubs) per five (5) parking spaces planted in landscaped islands to screen shade and break up parking. Trees and shrubs in parking lots may be in informal groups, straight rows, or concentrated in clusters as detailed in Section 4 of the Technical Manual....*

The final Landscape Plan (Plan P10) has been prepared by a Landscape Architect and includes many improvements, although the City Arborist remains of the view that more could be done (Attachment 3):

*The proposed Bangor Savings Bank landscape plan for Marginal Way is acceptable with the condition that ONE tree is planted in the rear right side traffic island and vegetation is added to the proposed rear curbing area.*

*The attached photos demonstrate that ornamental grasses can grow in restricted spaces, this is an MDOT project called Hagsi Parkway / Route One in Scarborough where they successfully use beach grass in a high salt area with good results. The overall Bangor Savings Bank project has much less than average landscape percentage due to lot orientation and circulation patterns, however a simple planting of low maintenance beach grass would work, thus is a recommended condition of approval.*



Tom Errico, Traffic Engineering reviewer, has confirmed that the rear drive aisle could be narrowed from 19' to 18' to provide an extra foot in the curbed area for the suggested planting ([Attachment 2](#)).

A suggested condition of approval is included to reflect Mr. Tarling's recommendation.

#### Street Trees

It should be noted that the street trees, brick sidewalk, and design of the frontage area is welcome as it includes some of the Marginal Way Improvement Plan elements and is integrated with the future plans for that stretch of Marginal Way.

#### Water quality; Stormwater Management; Erosion Control:

The project has been reviewed by Dave Senus, consultant Peer Engineering Reviewer who provided detailed comments in June, 2015. The applicant has fully addressed those comments with the exception of a Construction Management Plan, which staff consider is not a concern at this location ([Attachment 1](#)).

### *iii. Public Infrastructure and Community Safety Standards (I)*

#### Consistency with Master Plans

The project has incorporated the staff recommendations regarding the frontage so that it contributes to the Marginal Way Improvement Plan. The proposal includes a new street light to match the streetlights in this part of Bayside.

#### Public Safety and Fire Prevention

The proposals are generally acceptable in relation to the CPTED standards which encourage natural surveillance, access control, and territorial reinforcement. The windows and the rear deck of the building provide some surveillance, but the lighting levels in and around the drive-through are low and staff recommend further consideration to maximize security there.

The Fire Department has confirmed that the proposals meet conditions in respect of access, hydrants, and construction ([Attachment 5](#)).

#### Public Utilities

The applicant sent letters to the Portland Water District and Portland Department of Public Services in May in respect of the water supply and sewerage capacity. Confirmation of capacity has not yet been received and a suggested condition of approval has been included requiring these be received before the issuance of a building permit.

### *iv. Site Design Standards*

Historic Resources: Not applicable.

Exterior Lighting: The applicant has submitted a photometric plan at [Plan P11](#). The plan omits some of the information required (such as the specs for the lights to confirm they are full cut-off) and shows levels considerably above the Technical Standards for trespass along the eastern boundary. While this boundary is not "sensitive" at this time, the high lighting levels could impact a future development or use.

As mentioned above under "Public Safety", the area of the drive-through is not overlooked from the sidewalk and increased lighting levels should be incorporated here to improve security. Staff recommend a condition that requires a revised lighting plan with further details and revised levels to meet these concerns.

B7 Design Standards: the applicant has been working with the Planning Division's Urban Designer Caitlin Cameron to address the extensive B7 Design Guidelines and Standards. Ms. Cameron's final comments are included as [Attachment 6](#) and confirm that most of the associate requirements have been met. The outstanding issues are:

- Building Signage (see submission in [Attachment H](#) and [Plan P12](#))
- Transparency and tinting of the glass, particularly along the south facing frontage next to the sidewalk (see submitted info in [Attachment I](#))
- Landscaping to screen parking (see [Plan P9](#) Landscape Plan and discussion above under Site Plan)

Staff recommends a condition of approval that requires the applicant to work with staff to resolve the window design including transparency, color, and size to meet the intent of the B-7 Design Principles and Standards.

**X. STAFF RECOMMENDATION**

Subject to the proposed motions and conditions of approval listed below, Planning Division staff recommends that the Planning Board approve the proposed bank and office building with bank drive-through at 20 Marginal Way. The proposals would transform a single story car wash on a constrained site into a high quality development that is more appropriate to Bayside. The applicant has worked to meet or exceed the relevant standards. In addition, this project will contribute to the Marginal Way Improvement Plan and features "green" elements such as the solar panels.

**XI. PROPOSED MOTIONS**

*Beth reads*

**A. WAIVERS**

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations, contained in the Planning Board Report for the public hearing on July 14, 2015 for application #2015-080 (20 Marginal Way) relevant to Portland's Technical and Design Standards, the B7 Design Guidelines and Standards and other regulations, and the testimony presented at the Planning Board hearing:

- 1. Driveway Separation  
The Planning Board (waives/does not waive) Technical Design Standard Section 1.7.1.7 *Location and spacing of driveways* to allow the proposed driveway to be located as shown on Site Plan Plan 4.
- 2. Bicycle parking dimensional standards  
The Planning Board (waives/does not waive) Technical Design Standard Section 1.15.2 which specifies a minimum of 24 inches between the bike rack and the building wall, to allow a separation of 12 inches, subject to the required bicycle parking being met on the non-building side of the rack.
- 3. Building Signage:  
The Planning Board (waives/does not waive) *B7 Mixed Use Urban District Zone Design Principles and Standard E-16: Signage* to allow a total of 2 signs at the pedestrian level and 3 signs at upper levels, and to allow    materials, to address the unique circumstances of the site and the user.  
{Planning Board to determine}
- 4. Glass Transparency:  
The Planning Board (waives/does not waive) *B7 Mixed Use Urban District Zone Design Principles and Standard E-13: Transparency* to allow a waiver from the specified VT value, but that the scale of the waiver be determined by the City's Urban Designer based on further information and discussions regarding ways to meet the objectives of the standard.

*2nd BH 7-0*  
*2nd BH 7-0*  
*2nd BH 7-0*  
*W/D needed*  
*?*

**B. CONDITIONAL USE**

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report for the public hearing on July 14, 2015 for application #2015-080 (Conditional Use and Site Plan) (20 Marginal Way), relevant to Portland's Conditional Use Standards and other regulations, and the testimony presented at the Planning Board hearing:

( The Planning Board finds that the proposed conditional use for a bank drive-through associated with the new Bangor Savings Bank at 20 Marginal Way in the B-7 zone does / does not meet the standards of § 14-474 and the standards of §14-296(a)1(c) for the B-7 zone.

**C. DEVELOPMENT REVIEW**

On the basis of the application, plans, reports and other information submitted by the applicant; findings and recommendations contained in Planning Board Report for the public hearing on July 14, 2015 for application #2015-080 (Conditional Use and Site Plan) (20 Marginal Way), relevant to the site plan regulations; and the testimony presented at the Planning Board hearing; the Planning Board finds that the plan is/is not in conformance with the site plan standards of the land use code, subject to the following conditions of approval:

*2nd BH 7-0*



*amended  
by  
Tuck.  
BH ended  
Passed  
7-0*

- i. That this approval is subject to the Zoning Text Amendments relating the bank drive-throughs in the B7 zone, as recently approved by the City Council, taking effect without delays such as an appeal; and
- ii. That the Planning Board has established that the <sup>maximum</sup> ~~required~~ level of parking is 24 parking spaces; and *(TO)*
- iii. That the applicant shall reconsider the window design, including transparency, color, and size, in consultation with the City's Urban Designer, to meet the intent of the B-7 Design Principles and Standards. The final design to be reviewed and approved by the Planning Authority prior to the issuance of a building permit; and
- iv. That the applicant shall submit a revised Landscape Plan, for review and approval by the City Arborist and the Planning Authority prior to the issuance of a building permit, that adds one tree to be planted in the rear right side traffic island and adds vegetation to the proposed rear curbing area to address the City Arborist comments dated 7.2.2015; and
- v. That the developer/contractor/subcontractor must comply with conditions of the construction stormwater management plan and sediment and erosion control plan submitted May 2015 based on City standards and state guidelines. The owner/operator of the approved stormwater management system and all assigns shall comply with the conditions of Chapter 32 Stormwater including Article III, Post Construction Stormwater Management, which specifies the annual inspections and reporting requirements; and
- vi. That the applicant shall submit a copy of the ability to serve (water supply and wastewater) letters prior to the issuance of a building permit; and
- vii. That the applicant shall address the Department of Public Services comments dated 7.2.2015 to revise the notes on the Site Plan prior to the issuance of a building permit; and
- viii. That the applicant shall submit, for review and approval by the Planning Authority prior to the issuance of a building permit, a revised photometric plan for the rear parking lot area that includes the lighting fixture specification, reduces trespass along the eastern boundary to meet Technical Standards, and increases the lighting levels in the vicinity of the drive-through; and
- ix. That separate permits are also required for all new signs, including those on the building, and for HVAC systems, which require submission of dBA levels to confirm compliance with the maximum sound limits of the ordinance.

**ATTACHMENTS:**

<p><b><u>Report Attachments</u></b></p> <ol style="list-style-type: none"> <li>1. Peer Engineer Review comments 6.10.2015 and 7.6.2015</li> <li>2. Traffic Engineering Review comments 7.1.2015</li> <li>3. City Arborist comments 7.2.2015</li> <li>4. DPS (David Margolis-Pineo) comments 7.2.2015</li> <li>5. Fire Department comments 7.8.2015</li> <li>6. Urban Design comments 7.8.2015</li> </ol> <p><b><u>Public Comments</u></b> (none)</p> <p><b><u>Applicant's Submittal</u></b></p> <ol style="list-style-type: none"> <li>A. Cover letter</li> <li>B. Site Plan Application</li> <li>C. Conditional Use Application</li> <li>D. Project Description</li> <li>E. Right, title &amp; Interest (lease)</li> <li>F. Traffic Analysis</li> <li>G. Erosion Control and Stormwater Plans</li> </ol>	<ol style="list-style-type: none"> <li>H. Master Sign Narrative and Plans</li> <li>I. Glass Transparency Information</li> <li>J. Parking Demand Analysis</li> </ol> <p><b><u>Plans</u></b></p> <ol style="list-style-type: none"> <li>P1. Survey</li> <li>P2. Cover sheet</li> <li>P3. Demolition Plan</li> <li>P4. Site and Utility Plan</li> <li>P5. Grading plan</li> <li>P6. Details</li> <li>P7. Details</li> <li>P8. Details</li> <li>P9. Landscape Plan</li> <li>P10. Photometric Plan</li> <li>P11. Proposed Signage</li> <li>P12. Elevations – renderings</li> <li>P13. Elevations - architectural</li> <li>P14. Ground floor plan showing coffee shop</li> </ol>
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## MEMORANDUM



**TO:** Jean Fraser, Planner  
**FROM:** David Senus, PE  
**DATE:** June 10, 2015  
**RE:** Bangor Savings Bank, Preliminary Level III Site Plan Application

---

Woodard & Curran has reviewed the Preliminary Level III Site Plan Application for the proposed Bangor Savings Bank located at 20 Marginal Way in Portland, Maine. The project involves the construction of a three story, 2,100 SF bank/office building with a double drive thru.

### Documents Reviewed by Woodard & Curran

- Preliminary Level III Site Plan Application and attachments, dated May 13, 2015, prepared by Gorrill Palmer, on behalf of Bangor Savings Bank.
- Engineering Plans, Sheets C002, C101, C102, & C401, dated May 13, 2015, prepared by Gorrill Palmer, on behalf of Bangor Savings Bank.
- Boundary & Topographic Survey, dated March 24, 2015, prepared by Owen Haskell, Inc., on behalf of Gorrill Palmer.

### Comments

- 1) The Cover Sheet to the Engineering Plan set indicates that Sheets C402 & C403 contain additional details; however, we have not received these sheets at this time. Woodard & Curran will conduct a review of the proposed details upon receipt of the additional sheets.
- 2) Note 5 on Sheet C002 states that the General Contractor shall submit a Construction Management Plan to the City of Portland prior to demolition; the Applicant should note that a Construction Management Plan is required to be submitted as part of the Final Site Plan Application, per the General Written Submissions Checklist. If requested by the Applicant, Planning Staff and the Planning Board may allow for the construction management plan to be submitted for review and approval prior to the issuance of a building permit (as a condition of the Site Plan approval).
- 3) In accordance with Section 5 of the City of Portland Technical Manual, a Level III development project is required to submit a stormwater management plan pursuant to the regulations of MaineDEP Chapter 500 Stormwater Management Rules, including conformance with the Basic, General, and Flooding Standards. We offer the following comments:
  - a) Basic Standard: A plan has been provided to address erosion and sediment control requirements, inspection and maintenance requirements, and good housekeeping practices in general accordance with Appendix A, B, & C of MaineDEP Chapter 500; however, it does not appear that details of the proposed erosion and sediment control measures have been provided at this time.
  - b) General Standards: The project will result in an increase in impervious area of approximately 207 square feet. As such, the project is not required to include any specific stormwater management features for stormwater quality control. We encourage the Applicant to review the City's Stormwater Service Charge Credit Manual (available online) to evaluate whether they may want to incorporate stormwater quality treatment measures that qualify for a future Stormwater Service Charge credit.
  - c) Flooding Standard: The project will result in an increase in impervious area of approximately 207 square feet. As such, the project is not required to include any specific stormwater management features to control the rate or quantity of stormwater runoff from the site.



- 4) The Applicant has noted that letters have been sent to the Portland Water District and Portland Public Services requesting confirmation of their ability to serve the project and that responses will be forwarded to the City upon receipt.
- 5) The edge of pavement line at the rear parking area has many angles to define the edge of the angled parking spaces and the adjacent, abutting features. We recommend a more linear edge of pavement line for ease of construction and for winter plowing and site maintenance operations.
- 6) The Applicant has noted that a lighting plan and catalog cuts of the proposed lighting will be provided in the final submission.

## MEMORANDUM



**TO:** Jean Fraser, Planner  
**FROM:** David Senus, PE  
**DATE:** July 6, 2015  
**RE:** Bangor Savings Bank, Level III Site Plan Application

---

Woodard & Curran has reviewed the response to comments for the Final Level III Site Plan Application for the proposed Bangor Savings Bank located at 20 Marginal Way in Portland, Maine. The project involves the construction of a three story, 2,100 SF bank/office building with a double drive thru.

### Documents Reviewed by Woodard & Curran

- Response to Comments Letter dated June 29, 2015, prepared by Gorrill Palmer, on behalf of Bangor Savings Bank.
- Engineering Plans, Sheets C001, C002, C101, C102, C401, C402, C403 Rev dated June 29, 2015, prepared by Gorrill Palmer, on behalf of Bangor Savings Bank.

### Comments

- 1) The Applicant had previously noted that letters had been sent to the Portland Water District and Portland Public Services requesting confirmation of their ability to serve the project, and that responses will be forwarded to the City upon receipt. We have not reviewed responses confirming ability to serve the proposed development.
- 2) All previous review comments have been adequately addressed.

**From:** Tom Errico <thomas.errico@tylin.com>  
**To:** Jean Fraser <JF@portlandmaine.gov>  
**CC:** David Margolis-Pineo <DMP@portlandmaine.gov>, Katherine Earley <KAS@port...>  
**Date:** 7/1/2015 2:45 PM  
**Subject:** 20 Marginal Way - Final Traffic Comments

Hi Jean - I have reviewed the final site plan and find it to be acceptable with the following comments.

\* The applicant has revised the site circulation roadway widths (they have been narrowed slightly) to accommodate suggested landscaping enhancements and I find conditions to be acceptable. Jeff Tarling has requested the possible widening of the 2 foot raised concrete island to 3-feet with a reduction of the one-way circulatory roadway width from 19 feet to 18 feet. I am supportive of this change, if deemed necessary. I also find the pedestrian facility system to be acceptable given users and site operations.

\* The applicant has designed the frontage of the project along Marginal Way according to our suggestions and I find it to be acceptable. I would note that this frontage area should be considered an interim streetscape plan as it may change following recommendations identified through the Bayside Transportation Master Plan process. I would note that the project has a net reduction in vehicle traffic (as compared to the former car wash) and therefore contributions to the Marginal Way Master Plan fund is not required.

\* The proposed driveway does not meet City separation standards with the adjacent Post Office Parking Lot Driveway. I support a waiver from the City's Technical standards as it is preferred that the driveway separation from Forest Avenue be maximized.

If you have any questions, please contact me.

Best regards,

Thomas A. Errico, PE  
Senior Associate  
Traffic Engineering Director  
[T.Y. Lin International]T.Y. Lin International  
12 Northbrook Drive  
Falmouth, ME 04105  
207.781.4721 (main)  
207.347.4354 (direct)  
207.400.0719 (mobile)  
207.781.4753 (fax)  
thomas.errico@tylin.com<mailto:thomas.errico@tylin.com>  
Visit us online at [www.tylin.com](http://www.tylin.com)<http://www.tylin.com>  
Twitter | Facebook | LinkedIn | YouTube

"One Vision, One Company"

Please consider the environment before printing.

**From:** Jeff Tarling  
**To:** Errico Thomas; Jean Fraser  
**CC:** Barbara Barhydt; Caitlin Cameron; David Margolis-Pineo  
**Date:** 7/2/2015 4:09 PM  
**Subject:** Re: Fwd: 2970 Bangor Savings - Marginal Way  
**Attachments:** Beachgrass.JPG; BGisland.JPG

Jean -

The proposed 2970 Bangor Savings Bank landscape plan for Marginal Way is acceptable with the condition that ONE tree is planted in the rear right side traffic island and vegetation is added to the proposed rear curbing area.

The attached photos demonstrate that ornamental grasses can grow in restricted spaces, this is an MDOT project called Hags Parkway / Route One in Scarborough where they successfully use beach grass in a high salt area with good results. The overall Bangor Savings Bank project has much less than average landscape percentage due to lot orientation and circulation patterns, however a simple planting of low maintenance beach grass would work, thus is a recommended condition of approval.

Thanks,

Jeff Tarling  
City Arborist

**Jean Fraser - RE: PB Report on 20 Marginal Way**

Att. 2 b

**From:** Tom Errico <thomas.errico@tylin.com>  
**To:** Jean Fraser <JF@portlandmaine.gov>  
**Date:** 7/14/2015 11:45 AM  
**Subject:** RE: PB Report on 20 Marginal Way  
**CC:** David Margolis-Pineo <DMP@portlandmaine.gov>, Katherine Earley <KAS@port...>

Traffic Comments  
on Parking Analysis

Jean – I have reviewed the parking analysis conducted by Gorrill-Palmer and concur with their conclusion that the project will provide an acceptable supply of parking spaces given project details. I would note that the GP parking analysis used ITE data for a suburban site location. Given the urban location, I would expect a lower than noted parking generation estimate.

If you have any questions or comments, please contact me.

Best regards,

Thomas A. Errico, PE  
Senior Associate  
Traffic Engineering Director  
**TYLIN** INTERNATIONAL  
12 Northbrook Drive  
Falmouth, ME 04105  
207.781.4721 (main)  
207.347.4354 (direct)  
207.400.0719 (mobile)  
207.781.4753 (fax)  
[thomas.errico@tylin.com](mailto:thomas.errico@tylin.com)  
Visit us online at [www.tylin.com](http://www.tylin.com)  
Twitter | Facebook | LinkedIn | YouTube

"One Vision, One Company"

Please consider the environment before printing.

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**From:** Jean Fraser [mailto:JF@portlandmaine.gov]  
**Sent:** Friday, July 10, 2015 12:32 PM  
**To:** David  
**Cc:** jason.donovan@bangor.com; APalmer@gorrillpalmer.com; Doug Reynolds; Randy Dunton; Caitlin Cameron; Tom Errico  
**Subject:** PB Report on 20 Marginal Way

## Jean Fraser - Re: Fwd: 2970 Bangor Savings - Marginal Way

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**From:** Jeff Tarling  
**To:** Jean Fraser  
**Date:** 7/13/2015 8:31 AM  
**Subject:** Re: Fwd: 2970 Bangor Savings - Marginal Way

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Jean -

The plants will be fine and should not expect them to be full of cigarette butts considering the intended use.

They are over-worrying about this !

Jeff

>>> Jean Fraser 7/6/2015 10:27 AM >>>  
Jeff

Could you give me a quick call on this- David (applicant) spoke to me late on thursday last week to explain that Bangor Bank don't want the planting in the curb area because they have it somewhere else and it ends up being a cigarette butt holder and looking awful.

I am wondering about adding a condition that allows them to remove these plants at the end of the defect period (in consultation with you maybe) if they do not survive the winter conditions there.

What do you think?

thanks  
Jean  
874 8728

>>> Jeff Tarling 7/2/2015 4:09 PM >>>  
Jean -

The proposed 2970 Bangor Savings Bank landscape plan for Marginal Way is acceptable with the condition that ONE tree is planted in the rear right side traffic island and vegetation is added to the proposed rear curbing area.

The attached photos demonstrate that ornamental grasses can grow in restricted spaces, this is an MDOT project called Hags Parkway / Route One in Scarborough where they successfully use beach grass in a high salt area with good results. The overall Bangor Savings Bank project has much less than average landscape percentage due to lot orientation and circulation patterns, however a simple planting of low maintenance beach grass would work, thus is a recommended condition of approval.

Thanks,

Jeff Tarling  
City Arborist



**From:** David Margolis-Pineo  
**To:** Jean Fraser  
**CC:** Barbara Barhydt; Tom Errico  
**Date:** 7/2/2015 11:48 AM  
**Subject:** 20 Marginal Way

Jean,

July 2, 2015

**To:** Jean Fraser  
Barbara Barhydt  
**From:** David Margolis-Pineo  
**Re:** 20 Marginal Way (2015-080) – Bangor Savings Drive Thru

The Department of Public Services has the following final comments on the proposed project mentioned above.

1. Revise note on Sheet C101 which currently states "Plug or remove existing san sewer" to read, "Remove or grout fill sewer to be abandoned within the road right of way."

We have no further comments.

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**MEMORANDUM**

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**To:** FILE  
**From:** Jean Fraser  
**Subject:** Application ID: 2015-080  
**Date:** 7/8/2015

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**Comments Submitted by: Keith Gautreau/Fire on 6/2/2015**

## Premises Identification

The main entrance of the building must be the address for the property. This should be consistent with 911, tax assessor, Inspections Division and future mailing address.

Street addresses shall be marked on the structure and shall be as approved by the City E-911 Addressing Officer.

If the building entry faces a different street, both the street name and number should be large enough to read from the street.

Address numbers must be a minimum of 4 inches high.

The number should be in Arabic numerals rather than spelled out (for example, "130" instead of "One Hundred and Thirty").

Color: Addresses should be in a color that contrasts with the background.

Whenever possible, should be illuminated.

**Comments Submitted by: Keith Gautreau/Fire on 6/2/2015**

## Hydrants

2009 NFPA 1 18.3 Water Supplies and Fire Hydrants

-Fire Department Connections shall not be located where large diameter hose may block egress.

~~F~~ire Vehicle Access

1. Largest Fire Department Vehicle must be able to navigate through the parking lot to access building.

2. Fire Department Access shall have an unobstructed vertical clearance of not less than 13 ft 6 in.

**Comments Submitted by: Keith Gautreau/Fire on 6/2/2015**

## Construction Management Plan

Streets must maintain a 20' width for Fire Department access at all times.

Fire Hydrants shall not be blocked or enclosed by fencing. A 3' foot clearance must be kept at all times around the fire hydrant.

If gates are locked, a Portland Fire Department Knox padlock must be purchased by the applicant to allow access for the Fire Department.

The Construction Company' emergency contact information shall be posted on the property in case of an after hours emergency.

All construction shall comply with 2009 NFPA 1 Chapter 16 Safeguards During Building Construction, Alteration, and Demolition Operations.

Any cutting and welding done will require a Hot Work Permit from Fire Department.

**Comments Submitted by: Keith Gautreau/Fire on 7/8/2015**

The final plans have met the conditions in regards to access, hydrants, construction.

## Planning and Urban Development Department Planning Division

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**Subject:** B7 Design Review – 20 Marginal Way

**Written by:** Caitlin Cameron, Urban Designer

**Date of Review:** Wednesday, July 8, 2015

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On Wednesday, July 8, a design review according to the *City of Portland Design Manual Standards* was performed for the new construction proposal for redevelopment at 20 Marginal Way. The revisions were reviewed by Caitlin Cameron, Urban Designer, and Jean Fraser, Planner, Planning Division of the Department of Planning & Urban Development. The project was reviewed against the *B-7 Mixed Use Urban District Zone Design Principles & Standards* (Appendix 4 of the Design Manual).

### **Design Review Criteria:**

The project must meet all *B-7 Design Principles & Standards*.

### **Findings of the Design Review:**

Overall the proposal meets the B-7 Design Guidelines with the exception of the signage guidelines for which the applicant is seeking waivers. The building is of a scale and orientation conducive to an urban, multi-modal, and on-peninsula site. The ground level provides an active frontage with storefront and street-facing entries. The project also provides a sustainable design employing solar panels.

The proposed design currently **does not pass** all of the criteria of the *B7 Design Guidelines*. Refer to comments below regarding **waiver requests** and design elements which do not meet the standards - Staff recommends **condition of approval** wherein the window design issues including transparency, color, and size, be resolved with City staff including the Urban Designer to meet the intent of the B-7 Design Principles and Standards.

### **Principle A: Urban Design**

**A-5: Pedestrian Environment** – *Development on public streets or public spaces shall . . . enhance the pedestrian environment through the use of elements at the first floor such as fully functioning entries oriented to the street; active windows and storefronts; awnings and weather protection; appropriately scaled streetlights; trees and landscaping; . . .*

- The project places an **active door and storefront** facing Marginal Way and **contributes to the street wall**. The sidewalk is placed adjacent to the building as recommended by staff. The applicant recessed the entries in order to accommodate the outward door swing into the public right-of-way. A **canopy is used to emphasize the entry points** of the building.

**A-7: Building Orientation** – *The primary facades and entrances of buildings shall be oriented to streets, major pedestrian routes, or open spaces in order enhance the pedestrian-oriented environment. The primary facades and entrances of buildings shall not be oriented toward parking lots.*

- The project is **oriented towards the street creating a street wall** and includes **street-facing entries** at the sidewalk.

#### **Principle C: Parking, Loading and Service Areas**

**C-6: Surface Lots** – *Areas devoted to surface parking shall be screened from public rights of way and significant views through the use of design elements such as plantings, fencing, grade changes, and/or walls. A landscaped border shall be created around all surface parking lots. Any parking lot containing ten (10) or more parking spaces shall include one (1) or more landscaped islands within the interior of the lot. There shall be at least one (1) island for every twenty (20) spaces.*

- Staff suggests landscaping choices that are tall enough to **provide adequate screening from neighboring properties and views of parking from the street**. Staff does not support a waiver request from the landscaping requirement from an urban design standpoint.

#### **Principle E: Architectural Design**

**E-12: Materials** – *Facades visible from public rights of way shall use natural and authentic building materials that are expected to last at least 50 years. Predominant materials shall be brick, stone, precast concrete and other masonry products, wood, glass, and high quality metals such as steel, titanium and copper. . . . Materials such as thin gauge metal panels, exterior insulation and finish systems (EIFS), panelized “thin brick,” vinyl siding, or stucco on Styrofoam or a similar backing shall not be used on facades visible from the public rights of way. Fiber-cement clapboard and shingles may be used. Fiber cement panels shall only be used on portions of the building not visible from public rights of way. Public spaces shall be constructed of permanent, durable materials such as concrete, brick or stone.*

- **The building material palette** is similar to the recent commercial buildings on Marginal Way and **meets the design standard** – brick, composite panels, and storefront.

**E-7: Windows** – *The first floor transparency (minimum visible transmittance (VT) of .7 or greater) along public streets and the trail shall be equal to at least 50% of the wall area between the height of 2 and 9 feet. The first floor windows and storefronts shall be transparent with active uses visible behind them. Opaque glass shall not be allowed at the first floor level [See Standard E-13 Transparency]. Upper floors of all new buildings shall have at least 15% to 40% transparency of wall surface requirements. If it can be demonstrated that the building program precludes windows along first floor street frontages, then other surface details shall be used in accordance with Standard E-4 Articulation.*

- **Proposed glass has a VLT of .56 which is lower than the allowed .7. A sample is required** before staff can assess whether the transparency is adequate.
- **Spandrel glass (opaque) is proposed** for the upper portion of the ground floor storefront which is **not allowed by the Standard**. See comments below.

**E-8: Storefronts** - *Storefront glass shall be transparent in accordance with Standard E-13: Transparency, and shall not be blocked with opaque glass, or other means.*

- **Proposed transparency does not meet the Standard.**
- **Spandrel glass (opaque) is proposed** for the upper portion of the ground floor storefront **which is not allowed by the Standard. Staff does not support the use of spandrel glass in this project. Staff recommends that the ground floor windows be reduced in height** (and the canopy subsequently lowered) and the area where spandrel glass is proposed be eliminated. Reducing the amount of glazing will also reduce the solar gain in the façade.

**E-13: Transparency** – *Windows that have daylighting application on all levels of the façade shall use glass with a visible transmittance (VT) value of .7 or greater, which looks clear. . . . Opaque, heavily tinted or reflective glass shall not be used at the pedestrian level unless it can be demonstrated that the building program precludes the use of transparent glass.*

- **Proposed glass has a VLT of .56 which is lower than the allowed .7. A sample is required** before staff can assess whether the transparency is adequate.
- **Proposed glass has a green tint** which is not allowed by the Standard. **Glazing should look clear. A sample is required** before staff can assess whether the color is adequate.
- **Spandrel glass (opaque) is proposed** for the upper portion of the ground floor storefront **which is not allowed by the Standard. See comments above.**

**E-16: Signage** – *A master signage plan shall be required for all new construction as part of the site plan review process. Signage on new buildings shall be related to, and an integral part of, the design of the building. The master sign plan shall allow adaptability for changing tenants and uses over time. See Appendix 4 for additional signage standards.*

- **Applicant is proposing a signage master plan that does not meet the Design Standards and will require waivers for the following:**
  - **Illuminated plastic sign letters** - Given the precedent of waivers granted in B-7 for plastic illuminated letters, **staff supports the waiver request** for allowing this sign design.
  - **Number of upper level signs** - The guidelines limit the number of upper story signs to two (2) and the applicant is requesting three (3). **Staff defers to the Planning Board** on the appropriateness of additional signs. If the Planning Board does not support the waiver request, staff recommends the two upper level signs on the building's short faces which will be visible to vehicular traffic.
  - **Number of pedestrian level signs** – The guidelines limit the number of pedestrian level signs to one (1) and the applicant is requesting two (2). **Staff defers to the Planning Board** on the appropriateness of additional signs. If the Planning Board does not support the waiver request, staff recommends the pedestrian level sign facing Marginal Way.



June 29, 2015

Ms. Jean Fraser  
Planner  
City of Portland Planning Division  
389 Congress Street  
Portland, Maine 04101

**Subject: Bangor Savings Bank**  
**Applicant: Bangor Savings Bank**  
**Letter of Correspondence #1**

Dear Jean,

As you are aware, Bangor Savings Bank is under Level III Site Plans Review for its proposed facility at 20 Marginal Way in Portland. As you are also aware, the project has gone to the City Council and received approval of a text amendment, allowing a bank drive thru in a building less than 20,000 sf as a conditional use. Throughout the process, there have been numerous conversations between the project team and City Staff. At this time, we are providing the City with revised plans based upon the comments received from City Staff. This letter is provided to summarize the changes to the plans and respond to comments received from staff in an email from you dated June 8, 2015. For ease of review, the comments are provided followed by our response.

**Comment:** *Re Landscaping: We would like to see landscaping (mostly trees) along the east side of the drive (along the existing chain link fence), along the rear boundary and in the parking lot islands. In order to free up more land for these planted areas we have the following suggestions that are OK with the Traffic Engineering reviewer:*

- *Narrow the drive for the whole of its 2-way length to 22 feet in width (also OK from the Fire Dept viewpoint);*
- *Reduce the paved area in "middle" area near the handicapped spaces and 2 way access to central parking;*
- *Have compact parking spaces (for employees, so low turnover) along the entire rear "angled" row of parking (a waiver for the larger % of compact spaces is supported);*

**Response:** These items were further discussed with City Staff at a meeting on June 17, 2015. It was agreed that a portion of the access drive would be reduced to allow the planting of smaller items, such as daylilies, along the east side of the drive. Ornamental Pear trees were placed along the rear of the site where space was available. The parking spaces associated with the barrier free spaces were also shifted slightly to allow a larger end island for planting of additional day lilies.

**Comment:** *Re Pedestrian Safety:*

- *The crosswalk in front of the exiting ATM vehicles is OK except that the tactile strips are not necessary - and the side nearest the handicapped parking needs to be integrated into an extended pedestrian route that serves more of the parking lot and better serves the handicap spaces-enlargement/addition of islands (to include trees and ped routes) and additional striping is needed;*
- *Cars entering the central parking aisle near the handicap parking need some "warning" of possible peds crossing before they turn into that area - so the design should ensure that the ped crossing (to be added in some way) is visible to entering drivers, or otherwise slow up drivers at that point.*



Ms. Jean Fraser  
June 29, 2015  
Page 2 of 2

**Response:** The tactile strips within the sidewalks were removed from the plan. Additional crosswalk striping was added from the barrier free island to the rear parking area, to extend the pedestrian route. This island was also enlarged for additional plantings. As mentioned, an additional painted crosswalk was added to provide additional warning of possible pedestrians.

*Comment: Marginal Way Improvements: I am waiting for reviewers to confirm re the area along the front of the building-the location of the curb, the material for the sidewalk (technically should be brick), the number of trees (Jeff Tarling recommends another one) and the need for a streetlight are all part of the mix, but the plan for the Marginal Way Improvements at this location has not been finalized so the internal discussion is "what should be implemented as part of this project?" (or maybe a contribution instead) given the anticipated improvements to be done later on by the City. Leaving improvements to a later date does not fit well with the zoning requirements for pedestrian orientation, so we are trying to find a common-sense balance. Hence I am not able to send final review comments at this time but wanted you to be aware of the current thinking.*

**Response:** As discussed at the June 17<sup>th</sup> meeting, the revised plans include all of the requested improvements requested by Staff along the Marginal Way right of way. These improvements include the following:

- Incorporation of an 8' brick sidewalk
- The addition of an ornamental street light with conduit for future connection
- The specification of the street trees as Karpick Red Maple, as requested by Mr. Tarling.
- Reduction of new granite curb along the frontage.

Other improvements, in addition to those listed above, have been made to the plans, subsequent to the original submission include the following:

- The addition of sloped concrete curb along the edge of east and west side of the parking lot.
- The addition of a propane tank to serve the project.
- Modifications to the site signage package.
- As well as minor revisions to coordinate with building plans.
- A revised lighting plan is also included for your review

Updated building elevations will be forwarded under separate cover.

We appreciate the Planning Authority's review of the project to date and look forward to meeting with the Planning Board to present the proposal and address any questions. If you require any additional information, please don't hesitate to contact our office.

Sincerely,  
Gorrill Palmer

A handwritten signature in blue ink, appearing to read 'Douglas Reynolds', is written over the typed name.

Douglas Reynolds, PE  
Project Manager

Enclosures: Site Plans

cc: David Latulippe, Jason Donovan



Att. B

PO Box 1237, 15 Shaker Road  
Gray, Maine 04039  
207.657.6910

May 13, 2015

Barbara Barhydt  
Development Review Manager  
City of Portland Planning Division  
389 Congress Street  
Portland, Maine 04101

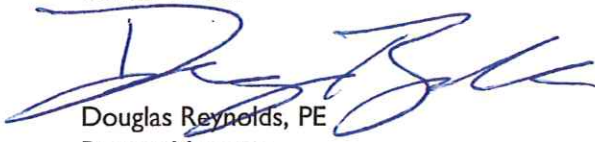
**Subject: Bangor Savings Bank**  
**Applicant: Bangor Savings Bank**  
**Level III Site Plan Application**

Dear Barbara,

On behalf of Bangor Savings Bank we are pleased to submit the enclosed Level III Site Plan Application for the proposed construction of a three story, 2,100 sf footprint bank/office building with a double drive thru at 20 Marginal Way in Portland. On April 14, 2015 the Planning Board voted to send a text change application from the Applicant to the City Council for review. The change would allow a bank drive thru in a building less than 20,000 sf as a conditional use. Since the conditional use requires planning board approval, a Level III application has been submitted.

Enclosed you will find a Preliminary Level III Site Plan Application package and set of plans illustrating the proposal. We appreciate the Planning Authority's consideration of our proposal and look forward to meeting with City staff, as necessary, and with the Planning Board to present the proposal and address any questions. If you require any additional information, please don't hesitate to contact our office.

Sincerely,  
Gorrill Palmer



Douglas Reynolds, PE  
Project Manager

Enclosures: Level III Site Plan Application  
Site Plans

cc: David Latulippe, Jason Donovan

DER/jwa/U:\2970 - Marginal Way Portland\IP Applications\Local\Site Plan application\Cover letter 4-7-15.doc



# APPLICATION



Jeff Levine, AICP, Director  
Planning & Urban Development Department

**Electronic Signature and Fee Payment Confirmation**

Notice: Your electronic signature is considered a legal signature per state law.

By digitally signing the attached document(s), you are signifying your understanding this is a legal document and your electronic signature is considered a **legal signature** per Maine state law. You are also signifying your intent on paying your fees by the opportunities below.

I, the undersigned, intend and acknowledge that no Site Plan or Historic Preservation Applications can be reviewed until payment of appropriate application fees are **paid in full** to the Inspections Office, City of Portland Maine by method noted below:

- Within 24-48 hours, once my complete application and corresponding paperwork has been electronically delivered, I intend to **call the Inspections Office** at 207-874-8703 and speak to an administrative representative and provide a credit/debit card over the phone.
- Within 24-48 hours, once my application and corresponding paperwork has been electronically delivered, I intend to **call the Inspections Office** at 207-874-8703 and speak to an administrative representative and provide a credit/debit card over the phone.
- I intend to deliver a payment method through the U.S. Postal Service mail once my application paperwork has been electronically delivered.

Applicant Signature:  Agent For Applicant

Date: 5-13-15

I have provided digital copies and sent them on: 

Date: 5-13-15

NOTE: All electronic paperwork must be delivered to [buildinginspections@portlandmaine.gov](mailto:buildinginspections@portlandmaine.gov) or by physical means i.e. a thumb drive or CD to the Inspections Office, City Hall, 3<sup>rd</sup> Floor, Room 315.



## Level III – Preliminary and Final Site Plans Development Review Application Portland, Maine

Planning and Urban Development Department  
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Level III: Preliminary or Final Site Plan. Please note that Portland has delegated review from the State of Maine for reviews under the Site Location of Development Act, Chapter 500 Stormwater Permits, and Traffic Movement Permits.

### Level III: Site Plan Development includes:

- New structures with a total floor area of 10,000 sq. ft. or more except in Industrial Zones.
- New structures with a total floor area of 20,000 sq. ft. or more in Industrial Zones.
- New temporary or permanent parking area(s) or paving of existing unpaved parking areas for more than 75 vehicles.
- Building addition(s) with a total floor area of 10,000 sq. ft. or more (cumulatively within a 3 year period) except in Industrial Zones.
- Building addition(s) with a total floor area of 20,000 sq. ft. or more in Industrial Zones.
- A change in the use of a total floor area of 20,000 sq. ft. or more in any existing building (cumulatively within a 3 year period).
- Multiple family development (3 or more dwelling units) or the addition of any additional dwelling unit if subject to subdivision review.
- Any new major or minor auto business in the B-2 or B-5 Zone, or the construction of any new major or minor auto business greater than 10,000 sq. ft. of building area in any other permitted zone.
- Correctional prerelease facilities.
- Park improvements: New structures greater than 10,000 sq. ft. and/or facilities encompassing 20,000 sq. ft. or more (excludes rehabilitation or replacement of existing facilities); new nighttime outdoor lighting of sports, athletic or recreation facilities not previously illuminated.
- Land disturbance of 3 acres or more (includes stripping, grading, grubbing, filling or excavation).

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14) which is available on our website:

Land Use Code: <http://me-portland.civicplus.com/DocumentCenter/Home/View/1080>

Design Manual: <http://me-portland.civicplus.com/DocumentCenter/View/2355>

Technical Manual: <http://me-portland.civicplus.com/DocumentCenter/View/2356>

**Planning Division**  
Fourth Floor, City Hall  
389 Congress Street  
(207) 874-8719

**Office Hours**  
Monday thru Friday  
8:00 a.m. – 4:30 p.m.

PROJECT NAME: Bangor Savings Bank

PROPOSED DEVELOPMENT ADDRESS:

20 Marginal Way, Portland, Maine

PROJECT DESCRIPTION:

Proposed approx. 2,100 sf footprint, 3 story bank/office  
building with double bank drive thru.

CHART/BLOCK/LOT: 113-A-25

PRELIMINARY PLAN

5-13-15 (date)

FINAL PLAN

\_\_\_\_\_ (date)

CONTACT INFORMATION:

<b>Applicant – must be owner, Lessee or Buyer</b> Name: Wendy Durrah Business Name, if applicable: Bangor Savings Bank Address: 99 Franklin Street City/State: Bangor, ME Zip Code: 04401	<b>Applicant Contact Information</b> Work # 207-541-2715 Home# Cell # Fax# e-mail: wendy.durrah@bangor.com
<b>Owner – (if different from Applicant)</b> Name: Northern Pride Auto Wash Address: P.O. Box 2147 City/State: So. Portland, Me Zip Code: 04116	<b>Owner Contact Information</b> Work # 207-776-5565 Home# Cell # Fax# e-mail: wdesena@maine.rr.com
<b>Agent/ Representative</b> Name: Gorrill Palmer Address: P.O. Box 1237 Gray, ME 04039 City/State: Zip Code:	<b>Agent/Representative Contact information</b> Work # 207-657-6910 Cell # 207-329-5584 e-mail: dreynolds@gorrillpalmer.com
<b>Billing Information</b> Name: Jason Donovan, Bangor Savings Bank Address: 99 Franklin Street City/State: Bangor, Me Zip Code: 04401	<b>Billing Information</b> Work # Cell # Fax# e-mail: jason.donovan@bangor.com

<b>Engineer</b> Name: Gorrill Palmer Address: P.O. Box 1237 Gray, ME 04039 City/State : Zip Code:	<b>Engineer Contact Information</b> Work # 207-657-6910 Cell # 207-329-5584 Fax# e-mail: dreynolds@gorrillpalmer.com
<b>Surveyor</b> Name: Owen Haskell, Inc. Address: 390 U.S. Route 1 City/State: Falmouth, ME Zip Code: 04105	<b>Surveyor Contact Information</b> Work # 207-774-0424 Cell # Fax# e-mail:
<b>Architect</b> Name: Address: City/State : Zip Code:	<b>Architect Contact Information</b> Work # Cell # Fax# e-mail:
<b>Attorney</b> Name: Address: City/State : Zip Code:	<b>Attorney Contact Information</b> Work # Cell # Fax# e-mail:

**APPLICATION FEES:**

Check all reviews that apply. (Payment may be made by Credit Card, Cash or Check payable to the City of Portland.)

<b>Level III Development (check applicable reviews)</b> <input checked="" type="checkbox"/> Less than 50,000 sq. ft. (\$500.00) <input type="checkbox"/> 50,000 - 100,000 sq. ft. (\$1,000) <input type="checkbox"/> 100,000 - 200,000 sq. ft. (\$2,000) <input type="checkbox"/> 200,000 - 300,000 sq. ft. (\$3,000) <input type="checkbox"/> over \$300,00 sq. ft. (\$5,000) <input type="checkbox"/> Parking lots over 11 spaces (\$1,000) <input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)  <b>Plan Amendments (check applicable reviews)</b> <input type="checkbox"/> Planning Staff Review (\$250) <input type="checkbox"/> Planning Board Review (\$500)  The City invoices separately for the following: <ul style="list-style-type: none"> <li>• Notices (\$.75 each)</li> <li>• Legal Ad (% of total Ad)</li> <li>• Planning Review (\$40.00 hour)</li> <li>• Legal Review (\$75.00 hour)</li> </ul> Third party review fees are assessed separately. Any outside reviews or analysis requested from the Applicant as part of the development review, are the responsibility of the Applicant and are separate from any application or invoice fees.	<b>Other Reviews (check applicable reviews)</b> <input type="checkbox"/> Traffic Movement (\$1,000) <input type="checkbox"/> Stormwater Quality (\$250) <input type="checkbox"/> Subdivisions (\$500 + \$25/lot) # of Lots ___ x \$25/lot = _____ <input type="checkbox"/> Site Location (\$3,000, except for residential projects which shall be \$200/lot) # of Lots ___ x \$200/lot = _____ <input type="checkbox"/> Other _____ <input type="checkbox"/> Change of Use <input type="checkbox"/> Flood Plain <input type="checkbox"/> Shoreland <input type="checkbox"/> Design Review <input type="checkbox"/> Housing Replacement <input type="checkbox"/> Historic Preservation
--	--

**APPLICATION SUBMISSION:**

1. All site plans and written application materials must be submitted electronically on a CD or thumb drive with each plan submitted as separate files, with individual file which can be found on the **Electronic Plan and Document Submittal** page of the City's website at <http://me-portland.civicplus.com/764/Electronic-Plan-and-Document-Submittal>
2. In addition, one (1) paper set of the plans (full size), one (1) paper set of plans (11 x 17), paper copy of written materials, and the application fee must be submitted to the Building Inspections Office to start the review process.

The application must be complete, including but not limited to the contact information, project data, application checklists, wastewater capacity, plan for fire department review, and applicant signature. The submissions shall include one (1) paper packet with folded plans containing the following materials:

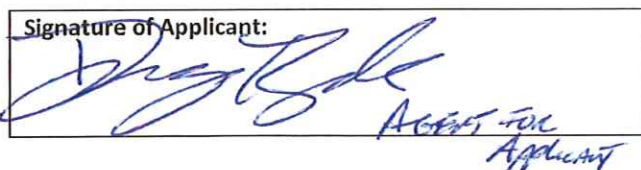
1. One (1) full size site plans that must be folded.
2. One (1) copy of all written materials or as follows, unless otherwise noted:
  - a. Application form that is completed and signed.
  - b. Cover letter stating the nature of the project.
  - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
3. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet.
4. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
5. One (1) set of plans reduced to 11 x 17.

Please refer to the application checklist (attached) for a detailed list of submission requirements.

**APPLICANT SIGNATURE:**

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level II Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant:  <i>Agent for Applicant</i>	Date: <i>5-13-15</i>
--	-------------------------

## PROJECT DATA

The following information is required where applicable, in order to complete the application.

<b>Total Area of Site</b>	17,862	sq. ft.
<b>Proposed Total Disturbed Area of the Site</b>	17,862	sq. ft.
If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland.		
<b>Impervious Surface Area</b>		
Impervious Area (Total Existing)	15,769	sq. ft.
Impervious Area (Total Proposed)	15,976	sq. ft.
<b>Building Ground Floor Area and Total Floor Area</b>		
Building Footprint (Total Existing)	2,706	sq. ft.
Building Footprint (Total Proposed)	2,100	sq. ft.
Building Floor Area (Total Existing)	2,706	sq. ft.
Building Floor Area (Total Proposed)	7,284	sq. ft.
<b>Zoning</b>		
Existing	B-7	
Proposed, if applicable		
<b>Land Use</b>		
Existing	Commercial	
Proposed	Commercial	
<b>Residential, If applicable</b>		
# of Residential Units (Total Existing)		
# of Residential Units (Total Proposed)		
# of Lots (Total Proposed)		
# of Affordable Housing Units (Total Proposed)		
<b>Proposed Bedroom Mix</b>		
# of Efficiency Units (Total Proposed)		
# of One-Bedroom Units (Total Proposed)		
# of Two-Bedroom Units (Total Proposed)		
# of Three-Bedroom Units (Total Proposed)		
<b>Parking Spaces</b>		
# of Parking Spaces (Total Existing)	9	
# of Parking Spaces (Total Proposed)	24	
# of Handicapped Spaces (Total Proposed)	2	
<b>Bicycle Parking Spaces</b>		
# of Bicycle Spaces (Total Existing)	0	
# of Bicycle Spaces (Total Proposed)	2	
<b>Estimated Cost of Project</b>	<b>\$1,800,000</b>	

<b>PRELIMINARY PLAN (Optional) - Level III Site Plan</b>			
<b>Applicant Checklist</b>	<b>Planner Checklist</b>	<b># of Copies</b>	<b>GENERAL WRITTEN SUBMISSIONS CHECKLIST</b>
x		1	Completed Application form
x		1	Application fees
x		1	Written description of project
x		1	Evidence of right, title and interest
N/A		1	Evidence of state and/or federal approvals, if applicable
x		1	Written assessment of proposed project's compliance with applicable zoning requirements
x		1	Summary of existing and/or proposed easement, covenants, public or private rights-of-way, or other burdens on the site
x		1	Written requests for waivers from site plan or technical standards, if applicable.
x		1	Evidence of financial and technical capacity
x		1	Traffic Analysis (may be preliminary, in nature, during the preliminary plan phase)
<b>Applicant Checklist</b>	<b>Planner Checklist</b>	<b># of Copies</b>	<b>SITE PLAN SUBMISSIONS CHECKLIST</b>
x		1	Boundary Survey meeting the requirements of Section 13 of the City of Portland's Technical Manual
x		1	<b>Preliminary Site Plan including the following: (information provided may be preliminary in nature during preliminary plan phase)</b>
x			Proposed grading and contours;
x			Existing structures with distances from property line;
x			Proposed site layout and dimensions for all proposed structures (including piers, docks or wharves in Shoreland Zone), paved areas, and pedestrian and vehicle access ways;
x			Preliminary design of proposed stormwater management system in accordance with Section 5 of the Technical Manual (note that Portland has a separate applicability section);
x			Preliminary infrastructure improvements;
x			Preliminary Landscape Plan in accordance with Section 4 of the Technical Manual;
N/A			Location of significant natural features (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features) located on the site as defined in Section 14-526 (b) (1);
N/A			Proposed buffers and preservation measures for significant natural features, as defined in Section 14-526 (b) (1);
x			Location , dimensions and ownership of easements, public or private rights of way, both existing and proposed;
x			Exterior building elevations.



<b>FINAL PLAN - Level III Site Plan</b>			
<b>Applicant Checklist</b>	<b>Planner Checklist</b>	<b># of Copies</b>	<b>GENERAL WRITTEN SUBMISSIONS CHECKLIST (* If applicant chooses to submit a Preliminary Plan, then the * items were submitted for that phase and only updates are required)</b>
		1	* Completed Application form
		1	* Application fees
		1	* Written description of project
		1	* Evidence of right, title and interest
		1	* Evidence of state and/or federal permits
		1	* Written assessment of proposed project's specific compliance with applicable Zoning requirements
		1	* Summary of existing and/or proposed easements, covenants, public or private rights-of-way, or other burdens on the site
		1	* Evidence of financial and technical capacity
		1	Construction Management Plan
		1	A traffic study and other applicable transportation plans in accordance with Section 1 of the technical Manual, where applicable.
		1	Written summary of significant natural features located on the site (Section 14-526 (b) (a))
		1	Stormwater management plan and stormwater calculations
		1	Written summary of project's consistency with related city master plans
		1	Evidence of utility capacity to serve
		1	Written summary of solid waste generation and proposed management of solid waste
		1	A code summary referencing NFPA 1 and all Fire Department technical standards
		1	Where applicable, an assessment of the development's consistency with any applicable design standards contained in Section 14-526 and in City of Portland Design Manual
		1	Manufacturer's verification that all proposed HVAC and manufacturing equipment meets applicable state and federal emissions requirements.

Applicant Checklist	Planner Checklist	# of Copies	<b>SITE PLAN SUBMISSIONS CHECKLIST</b> (* If applicant chooses to submit a Preliminary Plan, then the * items were submitted for that phase and only updates are required)
		1	* Boundary Survey meeting the requirements of Section 13 of the City of Portland's Technical Manual
		1	<b>Final Site Plans including the following:</b>
			Existing and proposed structures, as applicable, and distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone);
			Existing and proposed structures on parcels abutting site;
			All streets and intersections adjacent to the site and any proposed geometric modifications to those streets or intersections;
			Location, dimensions and materials of all existing and proposed driveways, vehicle and pedestrian access ways, and bicycle access ways, with corresponding curb lines;
			Engineered construction specifications and cross-sectional drawings for all proposed driveways, paved areas, sidewalks;
			Location and dimensions of all proposed loading areas including turning templates for applicable design delivery vehicles;
			Existing and proposed public transit infrastructure with applicable dimensions and engineering specifications;
			Location of existing and proposed vehicle and bicycle parking spaces with applicable dimensional and engineering information;
			Location of all snow storage areas and/or a snow removal plan;
			A traffic control plan as detailed in Section 1 of the Technical Manual;
			Proposed buffers and preservation measures for significant natural features, where applicable, as defined in Section 14-526(b)(1);
			Location and proposed alteration to any watercourse;
			A delineation of wetlands boundaries prepared by a qualified professional as detailed in Section 8 of the Technical Manual;
			Proposed buffers and preservation measures for wetlands;
			Existing soil conditions and location of test pits and test borings;
			Existing vegetation to be preserved, proposed site landscaping, screening and proposed street trees, as applicable;
			A stormwater management and drainage plan, in accordance with Section 5 of the Technical Manual;
			Grading plan;
			Ground water protection measures;
			Existing and proposed sewer mains and connections;

- Continued on next page -

		Location of all existing and proposed fire hydrants and a life safety plan in accordance with Section 3 of the Technical Manual;
		Location, sizing, and directional flows of all existing and proposed utilities within the project site and on all abutting streets;
		Location and dimensions of off-premises public or publicly accessible infrastructure immediately adjacent to the site;
		Location and size of all on site solid waste receptacles, including on site storage containers for recyclable materials for any commercial or industrial property;
		Plans showing the location, ground floor area, floor plans and grade elevations for all buildings;
		A shadow analysis as described in Section 11 of the Technical Manual, if applicable;
		A note on the plan identifying the Historic Preservation designation and a copy of the Application for Certificate of Appropriateness, if applicable, as specified in Section Article IX, the Historic Preservation Ordinance;
		Location and dimensions of all existing and proposed HVAC and mechanical equipment and all proposed screening, where applicable;
		An exterior lighting plan in accordance with Section 12 of the Technical Manual;
		A signage plan showing the location, dimensions, height and setback of all existing and proposed signs;
		Location, dimensions and ownership of easements, public or private rights of way, both existing and proposed.



PORTLAND FIRE DEPARTMENT  
SITE REVIEW  
FIRE DEPARTMENT CHECKLIST



A separate drawing[s] shall be provided as part of the site plan application for the Portland Fire Department's review.

1. Name, address, telephone number of applicant
- 2.
3. Name address, telephone number of architect
4. Proposed uses of any structures [NFPA and IBC classification]
- 5.
6. Square footage of all structures [total and per story]
7. Elevation of all structures
8. Proposed fire protection of all structures
  - *As of September 16, 2010 all new construction of one and two family homes are required to be sprinkled in compliance with NFPA 13D. This is required by City Code. (NFPA 101 2009 ed.)*
9. Hydrant locations
10. Water main[s] size and location
11. Access to all structures [min. 2 sides]
12. A code summary shall be included referencing NFPA 1 and all fire department. Technical standards.

Some structures may require Fire flows using annex H of NFPA 1

# CITY OF PORTLAND WASTEWATER CAPACITY APPLICATION

Department of Public Services,  
55 Portland Street,  
Portland, Maine 04101-2991



Mr. Frank J. Brancely,  
Senior Engineering Technician,  
Phone #: (207) 874-8832,  
Fax #: (207) 874-8852,  
E-mail: fjb@portlandmaine.gov

Date: 5-5-15

**1. Please, Submit Utility, Site, and Locus Plans.**

Site Address: 20 Marginal Way Chart Block Lot Number: 113-A-25

Proposed Use: Bank/Office

Previous Use: Car Wash

Existing Sanitary Flows: \_\_\_\_\_ GPD

Existing Process Flows: \_\_\_\_\_ GPD

Description and location of City sewer that is to receive the proposed building sewer lateral.

Existing San Sewer in front of parcel in Marginal Way

Site Category	Commercial (see part 4 below)	<input checked="" type="checkbox"/>
	Industrial (complete part 5 below)	<input type="checkbox"/>
	Governmental	<input type="checkbox"/>
	Residential	<input type="checkbox"/>
	Other (specify)	<input type="checkbox"/>

*(Clearly, indicate the proposed connections, on the submitted plans)*

**2. Please, Submit Contact Information.**

City Planner's Name: Barbara Barhydt Phone: 207-874-8699

Owner/Developer Name: Bangor Savings Bank

Owner/Developer Address: 99 Franklin Street, Bangor, Maine

Phone: 207-541-2715 Fax: \_\_\_\_\_ E-mail: wendy.durrah@bangor.com

Engineering Consultant Name: Gorrill Palmer

Engineering Consultant Address: P.O. Box 1237, Gray, Me 04039

Phone: 207-657-6910 Fax: \_\_\_\_\_ E-mail: dreynolds@gorrillpalmer.com

*(Note: Consultants and Developers should allow +/- 15 days, for capacity status, prior to Planning Board Review)*

**3. Please, Submit Domestic Wastewater Design Flow Calculations.**

Estimated Domestic Wastewater Flow Generated: \_\_\_\_\_ 160 GPD

Peaking Factor/ Peak Times: \_\_\_\_\_ 288 GPD Peak

Specify the source of design guidelines: (i.e. "Handbook of Subsurface Wastewater Disposal in Maine,"  
"Plumbers and Pipe Fitters Calculation Manual," Portland Water District Records, Other (specify)  
Handbook of Subsurface Wastewater Disposal in Maine

*(Note: Please submit calculations showing the derivation of your design flows, either on the following page, in the space provided, or attached, as a separate sheet)*

**4. Please, Submit External Grease Interceptor Calculations.**

Total Drainage Fixture Unit (DFU) Values: N/A  
Size of External Grease Interceptor: \_\_\_\_\_  
Retention Time: \_\_\_\_\_  
Peaking Factor/ Peak Times: \_\_\_\_\_

*(Note: In determining your restaurant process water flows, and the size of your external grease interceptor, please use The Uniform Plumbing Code. Note: In determining the retention time, sixty (60) minutes is the minimum retention time. Note: Please submit detailed calculations showing the derivation of your restaurant process water design flows, and please submit detailed calculations showing the derivation of the size of your external grease interceptor, either in the space provided below, or attached, as a separate sheet)*

**5. Please, Submit Industrial Process Wastewater Flow Calculations**

Estimated Industrial Process Wastewater Flows Generated: N/A GPD  
Do you currently hold Federal or State discharge permits? Yes \_\_\_\_\_ No \_\_\_\_\_  
Is the process wastewater termed categorical under CFR 40? Yes \_\_\_\_\_ No \_\_\_\_\_  
OSHA Standard Industrial Code (SIC): <http://www.osha.gov/oshstats/sicser.html>  
Peaking Factor/Peak Process Times: \_\_\_\_\_

*(Note: On the submitted plans, please show where the building's domestic sanitary sewer laterals, as well as the building's industrial-commercial process wastewater sewer laterals exits the facility. Also, show where these building sewer laterals enter the city's sewer. Finally, show the location of the wet wells, control manholes, or other access points; and, the locations of filters, strainers, or grease traps)*

*(Note: Please submit detailed calculations showing the derivation of your design flows, either in the space provided below, or attached, as a separate sheet)*

**Notes, Comments or Calculation**

From State of Maine Subsurface Wastewater Disposal Rules:  
Table 4C  
Employees at place of employment with no showers = 12 GPD/Employee  
Assume 24 employees in a 24 hour period  
Design Flow = 24x12 GPD = 288 GPD

**Bangor**  
Savings Bank

You matter more.

April 20, 2015

Re: Bangor Savings Bank  
20 Marginal Way  
Portland, Maine

To Whom It May Concern:

Jason Donovan authorizes Gorrill Palmer to execute land development permit applications on behalf of Bangor Savings Bank for the referenced project.

If you have any questions or if I can be of any further assistance, please contact me at 207-949-4027.

Very truly yours,



Jason Donovan  
VP, Facilities Manager  
Bangor Savings Bank

**ATTACHMENT 2**  
**FINANCIAL CAPACITY**



**Bangor**  
Savings Bank

You matter more.

May 5, 2015

Portland Planning Board

Re: Proposed Redevelopment of 20 Marginal Way, Portland, Maine

To Whom It May Concern:

Bangor Savings Bank has previously completed bank branch and office developments within Portland and throughout the State of Maine and has the technical expertise and financial capacity to complete the proposed redevelopment of 20 Marginal Way, Portland, Maine. Bangor Savings Bank has ample, liquid funding resources available to self-fund this project.

Sincerely,



Bruce G. Nickerson, CPA  
Executive Vice President,  
Chief Financial Officer and  
Treasurer

**ATTACHMENT 5**

**UTILITIES**

**CITY OF PORTLAND WASTEWATER CAPACITY APPLICATION**

Department of Public Services,  
55 Portland Street,  
Portland, Maine 04101-2991



Mr. Frank J. Brancely,  
Senior Engineering Technician,  
Phone #: (207) 874-8832,  
Fax #: (207) 874-8852,  
E-mail: fjb@portlandmaine.gov

Date: 5-5-15

**1. Please, Submit Utility, Site, and Locus Plans.**

Site Address: 20 Marginal Way Chart Block Lot Number: 113-A-25

Proposed Use: Bank/Office  
 Previous Use: Car Wash  
 Existing Sanitary Flows: \_\_\_\_\_ GPD  
 Existing Process Flows: \_\_\_\_\_ GPD  
 Description and location of City sewer that is to receive the proposed building sewer lateral.  
Existing San Sewer in front of parcel in Marginal Way

Site Category	Commercial (see part 4 below)	<input checked="" type="checkbox"/>
	Industrial (complete part 5 below)	<input type="checkbox"/>
	Governmental	<input type="checkbox"/>
	Residential	<input type="checkbox"/>
	Other (specify)	<input type="checkbox"/>

*(Clearly, indicate the proposed connections, on the submitted plans)*

**2. Please, Submit Contact Information.**

City Planner's Name: Barbara Barhydt Phone: 207-874-8699  
 Owner/Developer Name: Bangor Savings Bank  
 Owner/Developer Address: 99 Franklin Street, Bangor, Maine  
 Phone: 207-541-2715 Fax: \_\_\_\_\_ E-mail: wendy.durrah@bangor.com  
 Engineering Consultant Name: Gorrill Palmer  
 Engineering Consultant Address: P.O. Box 1237, Gray, Me 04039  
 Phone: 207-657-6910 Fax: \_\_\_\_\_ E-mail: dreynolds@gorrillpalmer.com

*(Note: Consultants and Developers should allow +/- 15 days, for capacity status, prior to Planning Board Review)*

**3. Please, Submit Domestic Wastewater Design Flow Calculations.**

Estimated Domestic Wastewater Flow Generated: 160 GPD  
 Peaking Factor/ Peak Times: 288 GPD Peak  
 Specify the source of design guidelines: (i.e. "Handbook of Subsurface Wastewater Disposal in Maine,"  
"Plumbers and Pipe Fitters Calculation Manual," Portland Water District Records, Other (specify)  
Handbook of Subsurface Wastewater Disposal in Maine

*(Note: Please submit calculations showing the derivation of your design flows, either on the following page, in the space provided, or attached, as a separate sheet)*

**4. Please, Submit External Grease Interceptor Calculations.**

Total Drainage Fixture Unit (DFU) Values: N/A  
Size of External Grease Interceptor: \_\_\_\_\_  
Retention Time: \_\_\_\_\_  
Peaking Factor/ Peak Times: \_\_\_\_\_

*(Note: In determining your restaurant process water flows, and the size of your external grease interceptor, please use The Uniform Plumbing Code. Note: In determining the retention time, sixty (60) minutes is the minimum retention time. Note: Please submit detailed calculations showing the derivation of your restaurant process water design flows, and please submit detailed calculations showing the derivation of the size of your external grease interceptor, either in the space provided below, or attached, as a separate sheet)*

**5. Please, Submit Industrial Process Wastewater Flow Calculations**

Estimated Industrial Process Wastewater Flows Generated: N/A GPD  
Do you currently hold Federal or State discharge permits? Yes \_\_\_\_\_ No \_\_\_\_\_  
Is the process wastewater termed categorical under CFR 40? Yes \_\_\_\_\_ No \_\_\_\_\_  
OSHA Standard Industrial Code (SIC): <http://www.osha.gov/oshstats/sicser.html>  
Peaking Factor/Peak Process Times: \_\_\_\_\_

*(Note: On the submitted plans, please show where the building's domestic sanitary sewer laterals, as well as the building's industrial-commercial process wastewater sewer laterals exits the facility. Also, show where these building sewer laterals enter the city's sewer. Finally, show the location of the wet wells, control manholes, or other access points; and, the locations of filters, strainers, or grease traps)*

*(Note: Please submit detailed calculations showing the derivation of your design flows, either in the space provided below, or attached, as a separate sheet)*

**Notes, Comments or Calculation**

From State of Maine Subsurface Wastewater Disposal Rules:  
Table 4C  
Employees at place of employment with no showers = 12 GPD/Employee  
Assume 24 employees in a 24 hour period  
Design Flow = 24x12 GPD = 288 GPD



PO Box 1237, 15 Shaker Road  
Gray, Maine 04039  
207.657.6910

May 13, 2015

Ms. Glissen Havu  
Portland Water District  
225 Douglas Street  
Portland, Maine 04104

Re: Proposed Bangor Savings bank  
20 Marginal Way, Portland  
Letter of Ability to Serve

Dear Glissen:

Bangor Savings Bank has retained Gorrill Palmer to prepare plans and permit applications for a proposed Bank/Office building at 20 Marginal Way in Portland. The site is shown on Assessor's chart/block/lot number 113-A-25, is approximately 17,862 square feet in size and is located in the B-7 zone. Refer to Figure 1 – Location Map following this page for the project location. The project requires a site plan permit from the City of Portland. As required by the reviewing authorities, we are writing to request a letter indicating the ability of the Portland Water District to serve this project. A preliminary utility plan is enclosed for your review.

### **Project Description**

Bangor Savings Bank is proposing a three story 2,100 square foot footprint banking and office building. The building will be sprinklered for fire protection. The project is a redevelopment of the existing site. The existing building will be demolished in order to construct the proposed project.

### **Existing Service**

An existing 8" water main is located in Marginal Way. A 2" water service provides water to the existing site. The site is currently developed with the Northern Pride Car Wash and Detailing Center.

### **Anticipated Flows**

The anticipated water demand for the development was computed using the Maine Subsurface Waste Water Disposal Rules Table 4C for Employees at place of employment with no showers. Based on the publication Water Supply and Pollution Control, Third Edition, by Clark, Viessman and Hammer, Chapter 4, Section 5; the peak daily use can be considered to about 180% of the average daily use.

Using information supplied by the Applicant, the building is anticipated to have 24 employees over a 24 hour period. From Table 4C:

$$(12 \text{ GPD/employee}) \times (24 \text{ employees}) = 288 \text{ GPD}$$

The table below is a summary of the water demand that is anticipated for the development.



Ms. Glissen Havu  
May 13, 2015  
Page 2 of 2

<i>Anticipated Wastewater Generation</i>		
	Average Daily Wastewater Generation (gpd)	Peak Daily Wastewater Generation (gpd)
Proposed bank/office structure	160	288
<b>Total</b>	<b>160</b>	<b>288</b>

The proposed building will be sprinklered with a proposed 6" fire service line connected to the 8" water main in Marginal Way.

**Ability to Serve**

In support of the applications to the reviewing authorities, we are writing to request a letter indicating the ability of the Portland Water District to serve the proposed project. In addition, we are interested in receiving:

- An estimate for any work the Water District would perform within the right-of-way.
- Information as to any easements that the District may require on-site.
- Any results of hydrant tests in the vicinity of the site.
- Any other information that you believe would be useful as this project proceeds.

Please contact me if you have any questions relative to this matter.

Sincerely,

Gorrill Palmer



James Attianese

Copy:

JWA/jwa/U:\2970 - Marginal Way Portland\H Utilities\Havu\_5-5-15.doc



PO Box 1237, 15 Shaker Road  
Gray, Maine 04039  
207.657.6910

May 13, 2015

Mr. Bill Bennett  
Pine Tree Waste  
87 Pleasant Hill Road  
Scarborough, ME 04074

Subject: Bangor Savings Bank  
Portland, Maine  
Ability to Serve Letter

Dear Bill:

Gorrill Palmer has been retained by Bangor savings bank to prepare plans and permit applications for the construction of a proposed 2,100 s.f., three story bank/office building, associated parking, and infrastructure off Marginal Way in Portland, Maine. Figure 1 attached to this letter is a location map reflecting the proposed site location for your review.

As required by the reviewing authorities, we are writing to request a letter indicating the ability of Pine Tree Waste to serve this project. Using typical solid waste generation rates it is anticipated that the construction of the new development could result in the following quantities:

- Demolition – Approximately 27 c.y.
- Construction Waste – Approximately 49 c.y.
- Commercial Solid Waste, Non-Recyclable – 18 c.y. per month
- Commercial Solid Waste, Recyclable – 15 c.y. per month

Based on information provided previously by your company, construction waste and universal waste can be handled by Pine Tree Waste and would be transported to the Juniper Ridge Facility in Old Town. This facility is licensed by the MDEP to accept construction debris. It is our understanding that Pine Tree Waste would be able to provide the necessary containers for use on-site to collect the construction debris and universal waste and can also transport the waste to Old Town.

We are writing to request the ability of Pine Tree Waste to serve this project for the collection and transport of the solid and universal waste to an approved location.



Mr. Bill Bennett  
May 13, 2015  
Page 2 of 2

If you have any questions, please contact this office. An acknowledgement statement is presented below for your signature. Gorrill Palmer looks forward to your response on this matter.

Sincerely,

Gorrill Palmer

James Attianese

JWA/jwa/U:\2970 - Marginal Way Portland\H Utilities\Bennett 5-5-15 solid waste.doc

I have reviewed the contents of this letter and find that the representations made regarding Pine Tree Waste are accurate, and that Pine Tree Waste can provide services relative to transport and disposal of generated wastes from the proposed development to the facilities outlined above.

---

Bill Bennett, Pine Tree Waste

---

Date



**ATTACHMENT 6**  
**NFPA CODE SUMMARY**

CODE REVIEW – BANGOR SAVINGS BANK MARGINAL WAY

CLASSIFICATION	IBC 2009 EDITION	NFPA 101 2009 EDITION	CONCLUSION
USE GROUP	BUSINESS GROUP B	NEW BUSINESS OCCUPANCIES	BUSINESS GROUP B NEW BUSINESS OCCUPANCIES
CONSTRUCTION TYPE	TYPE IIB	TYPE II 000	TYPE IIB / TYPE II 000
HEIGHT AND AREA SEPARATIONS	BUILDING HEIGHT 75 FEET FOUR STORIES ABOVE GRADE AREA PER FLOOR 23,000 SF		BUILDING HEIGHT 75 FEET FOUR STORIES ABOVE GRADE AREA PER FLOOR 23,000 SF
REQUIREMENTS FOR AUTOMATIC SPRINKLER SYSTEMS	AUTOMATIC SPRINKLER SYSTEMS PROVIDED	AUTOMATIC SPRINKLER SYSTEMS PROVIDED	AUTOMATIC SPRINKLER SYSTEMS PROVIDED

EGRESS

OCCUPANT LOAD	BUSINESS USE 100 GROSS FIRST STORY = 22 SECOND STORY = 28 THIRD STORY = 28 MECHANICAL USE 300 GROSS PENTHOUSE = 3 TOTAL BUILDING = 81	BUSINESS USE 100 GROSS FIRST STORY = 22 SECOND STORY = 28 THIRD STORY = 28 MECHANICAL USE 300 GROSS PENTHOUSE = 3 TOTAL BUILDING = 81	BUSINESS USE 100 GROSS FIRST STORY = 22 SECOND STORY = 28 THIRD STORY = 28 MECHANICAL USE 300 GROSS PENTHOUSE = 3 TOTAL BUILDING = 81
MEANS OF EGRESS	MINIMUM NUMBER OF EXITS PER STORY TWO PENTHOUSE CONSIDERED APART OF THE STORY BELOW.	MINIMUM NUMBER OF EXITS PER STORY TWO, EXCEPT FOR MECHANICAL EQUIPMENT ROOM STORY A SINGLE MEANS OF EXIT.	MINIMUM NUMBER OF EXITS PER STORY TWO, EXCEPT FOR MECHANICAL EQUIPMENT ROOM STORY A SINGLE MEANS OF EXIT.
LOCATIONS OF MEANS OF EGRESS	LOCATED AT A DISTANCE FROM ONE ANOTHER NOT LESS THAN ONE-THIRD THE LENGTH OF THE MAXIMUM OVERALL DIAGONAL DIMENSION OF THE BUILDING.	LOCATED AT A DISTANCE FROM ONE ANOTHER NOT LESS THAN ONE-THIRD THE LENGTH OF THE MAXIMUM OVERALL DIAGONAL DIMENSION OF THE BUILDING.	LOCATED AT A DISTANCE FROM ONE ANOTHER NOT LESS THAN ONE-THIRD THE LENGTH OF THE MAXIMUM OVERALL DIAGONAL DIMENSION OF THE BUILDING.

TRAVEL DISTANCE TO EXITS	MAXIMUM 300 FEET	MAXIMUM 300 FEET	MAXIMUM 300 FEET
COMMON PATH OF TRAVEL	MAXIMUM 100 FEET	MAXIMUM 100 FEET, EXCEPT FOR MECHANICAL EQUIPMENT ROOM STORY IS 50 FEET.	MAXIMUM 100 FEET, EXCEPT FOR MECHANICAL EQUIPMENT ROOM STORY IS 50 FEET.
DEAD END CORRIDORS	MAXIMUM 50 FEET	MAXIMUM 50 FEET	MAXIMUM 50 FEET

FIRE RESISTANCE RATINGS BASED ON TYPE VB/V 000 CONSTRUCTION

	IBC 2009 EDITION	NFPA 101 2009 EDITION	CONCLUSION
PRIMARY STRUCTURAL FRAME	0	0	0
BEARING WALL EXTERIOR	0	0	0
BEARING WALL INTERIOR	0	0	0
NONBEARING WALLS AND PARTITIONS EXTERIOR	0	0	0
NONBEARING WALLS AND PARTITIONS INTERIOR	0	0	0
FLOOR CONSTRUCTION AND SECONDARY MEMBERS	0	0	0
ROOF CONSTRUCTION AND SECONDARY MEMBERS	0	0	0
CORRIDOR FIRE-RESISTANCE RATING	0	0	0
SHAFTS CONNECTING LESS THEN FOUR STORIES	1	1	1
SHAFTS CONNECTING FOUR OR MORE STORIES	2	2	2
EXIT ENCLOSURES CONNECTING LESS THEN FOUR STORIES	1	1	1
EXIT ENCLOSURES CONNECTING FOUR OR MORE STORIES	2	2	2



PO Box 1237, 15 Shaker Road  
Gray, Maine 04039  
207.657.6910

May 13, 2015

Barbara Barhydt  
Development Review Manager  
City of Portland Planning Division  
389 Congress Street  
Portland, Maine 04101

**Subject: Bangor Savings Bank**  
**Applicant: Bangor Savings Bank**  
**Conditional Use Application**

Dear Barbara,

On behalf of Bangor Savings Bank we are pleased to submit the enclosed Conditional Use Application for the proposed construction of a three story, 2,100 sf footprint bank/office building with a double drive thru at 20 Marginal Way in Portland. On April 14, 2015 the Planning Board voted to send a text change application from the Applicant to the City Council for review. The change would allow a bank drive thru in a building less than 20,000 sf as a conditional use. This application is for the proposed double drive thru at 20 Marginal Way which would be considered a conditional use subsequent to the text change.

Enclosed you will find a Conditional Use Application package and set of plans illustrating the proposal. We appreciate the Planning Authority's consideration of our proposal and look forward to meeting with City staff, as necessary, and with the Planning Board to present the proposal and address any questions. If you require any additional information, please don't hesitate to contact our office.

Sincerely,

Gorrill Palmer

A handwritten signature in blue ink, appearing to read 'Douglas Reynolds', is written over the printed name.

Douglas Reynolds, PE  
Project Manager

Enclosures: Conditional Use Application  
Site Plans

cc: David Latulippe, Jason Donovan

DER/jwa/U:\2970 - Marginal Way Portland\IP Applications\Local\Site Plan application\conditional use\Cover letter 5-12-15.doc



## Conditional Use for Planning Board Review Development Review Application Portland, Maine

Planning and Urban Development Department  
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Conditional Use where the Planning Board is listed as the reviewing authority rather than the Zoning Board of Appeals. Please note that Portland has delegated review from the State of Maine for reviews under the Site Location of Development Act, Chapter 500 Stormwater Permits, and Traffic Movement Permits.

### A. Conditional Use: Standards and Criteria – Section 14-474 and Zone Related Criteria

Conditional uses are listed in the Zoning Ordinance for each zone and these proposed uses must address the criteria contained in Section 14-474 in the written application. In addition to the criteria in Section 14-474, each application must address any applicable conditional use standards contained in the applicable zone and the proposed specific use. The Zoning Ordinance specifies when the Planning Board is designated as the reviewing authority rather than the Zoning Board of Appeals. This application is for Planning Board reviews only.

Upon showing that a proposed use is a conditional use under this article, a conditional use permit shall be granted unless the Board determines that:

- a. The volume and type of vehicle traffic to be generated, hours of operation, expanse of pavement, and the number of parking spaces required are not substantially greater than would normally occur at surrounding uses or other allowable uses in the same zone; and
- b. The proposed use will not create unsanitary or harmful conditions by reason of noise, glare, dust, sewage disposal, emissions to the air, odor, lighting, or litter; and
- c. The design and operation of the proposed use, including but not limited to landscaping, screening, signs, loading, deliveries, trash or waste generation, arrangement of structures, and materials storage will not have a substantially greater effect/impact on surrounding properties than those associated with surrounding uses or other allowable uses in the zone.

### B. Site Plan Application: Please submit the Conditional Use Application in addition to the applicable Site Plan Application.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14) which is available on our website:

Land Use Code: <http://me-portland.civicplus.com/DocumentCenter/Home/View/1080>

Design Manual: <http://me-portland.civicplus.com/DocumentCenter/View/2355>

Technical Manual: <http://me-portland.civicplus.com/DocumentCenter/View/2356>

**Planning Division**  
Fourth Floor, City Hall  
389 Congress Street  
(207) 874-8719

**Office Hours**  
Monday thru Friday  
8:00 a.m. – 4:30 p.m.

PROJECT NAME: Bangor Savings Bank

**PROPOSED DEVELOPMENT ADDRESS:**

20 Marginal Way

ASSESSOR'S CHART/BLOCK/LOT (s): 113-A-25

Applicable Zone: B-7

**CONTACT INFORMATION:**

<b>Applicant – must be owner, Lessee or Buyer</b> Name: Wendy Durrah Business Name, if applicable: Bangor Savings Bank Address: 99 Franklin Street City/State : Bangor, Me      Zip Code: 04401	<b>Applicant Contact Information</b> Work # 207-541-2715 Home# Cell #                              Fax# e-mail: wendy.durrah@bangor.com
<b>Owner – (if different from Applicant)</b> Name: Northern Pride Auto Wash Address: P.O. Box 2147 City/State : So. Portland, Me      Zip Code: 04116	<b>Owner Contact Information</b> Work # 207-776-5565 Home# Cell #                              Fax# e-mail: wdesena@maine.rr.com
<b>Billing Information</b> Name: Jason Donovan, Bangor Savings Bank Address: 99 Franklin Street City/State : Bangor, Me      Zip Code: 04401	<b>Billing Information</b> Work # Cell #                              Fax# e-mail: jason.donovan@bangor.com

**RIGHT, TITLE OR INTEREST:**

Lease, see narrative

(Please identify the status provide documentary evidence, attached to this application, of the applicant's right, title, or interest in the subject property (ex: deed, option or contract to purchase or lease the property.)

**VICINITY MAP:** (Please attach a map showing the subject parcel and abutting parcels, labeled as to ownership and/or current use.)

**EXISTING USE:** Describe the existing use of the subject property.

Car Wash

**PROPOSED USE:** Describe the proposed use of the subject property

Bank with double drive thru and office space

**TYPE OF CONDITIONAL USE PROPOSED (Refer To Specific Provisions Of Land Use Code Authorizing The Proposed Conditional Use).**

Banking with drive thru, Chapter 14, Division 17, Section 14-296

See narrative for discussion of zoning text change to allow drive through for building less than 20,000 sf

**SITE PLAN:** Submit a separate site plan application for the proposal that provides a site plan for the property, showing existing and proposed improvements, which meets the submission requirements of the applicable level of site plan review.

**CONDITIONAL USE STANDARDS AND CONDITIONS – 14-474**

Address the following criteria in your written application and any applicable conditional use standards contained in the zoning code for the specific use. Upon showing that a proposed use is a conditional use under this article, a conditional use permit shall be granted unless the Board determines that:

- a. The volume and type of vehicle traffic to be generated, hours of operation, expanse of pavement, and the number of parking spaces required are not substantially greater than would normally occur at surrounding uses or other allowable uses in the same zone; and
- b. The proposed use will not create unsanitary or harmful conditions by reason of noise, glare, dust, sewage disposal, emissions to the air, odor, lighting, or litter; and
- c. The design and operation of the proposed use, including but not limited to landscaping, screening, signs, loading, deliveries, trash or waste generation, arrangement of structures, and materials storage will not have a substantially greater effect/impact on surrounding properties than those associated with surrounding uses or other allowable uses in the zone.

**CONDITIONAL USE AUTHORIZED BY: SECTION 14- 296**

Address any specific conditional use standards for the specific use contained in the zoning code in the written submission.

**APPLICATION FEES:**

**(Payment may be made by Credit Card, Cash or Check payable to the City of Portland.)**

<p><input checked="" type="checkbox"/> Conditional Use Review (\$100.00)</p> <p>(Please submit a separate application for the applicable site plan review. Fees and charges are listed within the application)</p>	<p>The City invoices separately for the following:</p> <ul style="list-style-type: none"> <li>• Notices (\$.75 each)</li> <li>• Legal Ad (% of total Ad)</li> <li>• Planning Review (\$40.00 hour)</li> <li>• Legal Review (\$75.00 hour)</li> </ul> <p>Third party review fees are assessed separately. Any outside reviews or analysis requested from the Applicant as part of the development review, are the responsibility of the Applicant and are separate from any application or invoice fees.</p>
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**APPLICATION SUBMISSION:**

1. All site plans and written application materials must be submitted electronically on a CD or thumb drive with each plan submitted as separate files, with individual file which can be found on the Electronic Plan and Document Submittal page of the City's website at <http://me-portland.civicplus.com/764/Electronic-Plan-and-Documents-Submittal>
2. In addition, one (1) paper set of the plans (full size), one (1) paper set of plans (11 x 17), paper copy of written materials, and the application fee must be submitted to the Building Inspections Office to start the review process.
3. The application must be complete, including but not limited to the contact information, project data, application checklists, wastewater capacity, plan for fire department review, and applicant signature. The submissions shall include one (1) paper packet with folded plans containing the following materials:
  1. One (1) full size site plans that must be folded.
  2. One (1) copy of all written materials or as follows, unless otherwise noted:

Applicant Checklist	Planner Checklist	# of Copies	
x		1	Application form that is completed and signed.
x		1	Cover letter stating the nature of the project.
x		1	Evidence of right, title and interest.
x		1	Written Submittals that address the conditional use standards of Sec. 14-474.
x		1	Written submittals that address any applicable standards of review contained in the zoning code for the specific use.
x		1	A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet and containing the information required for the applicable level of site plan review.
x		1	An accompanying site plan application, as applicable.

**APPLICANT SIGNATURE:**

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Conditional Use Review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant:  AGENT FOR APPLICANT	Date: 5-13-15
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**Bangor**  
Savings Bank

You matter more.

April 20, 2015

Re: Bangor Savings Bank  
20 Marginal Way  
Portland, Maine

To Whom It May Concern:

Jason Donovan authorizes Gorrill Palmer to execute land development permit applications on behalf of Bangor Savings Bank for the referenced project.

If you have any questions or if I can be of any further assistance, please contact me at 207-949-4027.

Very truly yours,



Jason Donovan  
VP, Facilities Manager  
Bangor Savings Bank

## Conditional Use Narrative

The following narrative presents the information required for the Preliminary Level III Site plan application.

### Applicable Standards:

Section 14-296.I.C.ii of the Land Use Code states that a drive up attached to a bank requires a minimum floor area of 20,000 sf. in the B7 zone and is considered a conditional use. A text change application has been submitted to the City and is under review of the City Council for a change to the Land Use Ordinance allowing a bank with less than 20,000 sf to have drive up windows as a conditional use in the B7 zone.

The applicable standards of review in the B7 zone and the project's conformance are as follows.

14-296.I.C.i - *The drive-up is accessory to a banking service occupying a minimum floor area of 4,000 sf.*

The total proposed floor area is 7,708 sf.

14-296.I.C.ii - *The drive-up is attached to or included within a building with a minimum floor area of twenty thousand (20,000) square feet.*

A proposed text change has been filed which will allow a bank use with less than 20,000 sf to be approved as a conditional use.

14-296.I.C.iii - *All drive-up features, such as automated teller machines and service windows, shall not extend nearer than twenty-five (25) feet to the street right-of-way line*

The proposed drive-up is located a minimum of 31 feet from the street right-of-way line.

14-296.I.C.iv - *The site must have adequate stacking capacity for vehicles waiting to use these service features without impeding vehicular or pedestrian circulation or creating hazards to vehicular or pedestrian circulation on adjoining streets.*

The site has stacking capacity within the parking lot to adequately handle the anticipated usage without impeding vehicular or pedestrian circulation on adjoining streets.

14-296.I.C.v - *Drive-up vehicle circulation shall not be located between the building and any adjacent public streets.*

The drive-up circulation is behind the building and therefore not adjacent to public streets.

14-296.I.C.vi - *The drive-up shall be limited to two vehicle drive-up lanes.*

Two vehicle lanes are proposed.

14-296.I.C.vii - *The location of any drive-up shall be limited to the geographic area between Somerset Street/I-295/Franklin Arterial/Forest Avenue.*

The proposed project is within the required geographic area.

### Conditional Use Standards Section 14-474:

The following addresses the conditional use standards contained in Section 14-474 of the Land Use Code.

14-474.C.2.a – *The volume and type of vehicle traffic to be generated, hours of operation, expanse of pavement, and the number of parking spaces required are not substantially greater than would normally occur at surrounding uses or other allowable uses in the same zone.*

The traffic generated will be consistent with other banks in the area. Section 7 of the site plan application contains a traffic report for the project. The site contains the required parking spaces and has utilized compact spaces in order to reduce the expanse of pavement.

14-474.C.2.b - *The proposed use will not create unsanitary or harmful conditions by reason of noise, glare, dust, sewage disposal, emissions to the air, odor, lighting, or litter.*

The proposed bank is not anticipated to create noise, glare, dust, emissions to the air, or odor greater than other banks in the area. A letter has been sent to the Portland Public Services as part of the site plan application requesting their ability to serve the project for sewage disposal. A lighting plan will be submitted in the final site plan application depicting full cut off lighting to avoid impacting abutting properties. A letter has been sent to Pine Tree Waste requesting their ability to serve the project for waste disposal. The dumpster will be stored within a fenced enclosure which will reduce the possibility of litter.

14-474.C.2.c - The design and operation of the proposed use, including but not limited to landscaping, screening, signs, loading, deliveries, trash or waste generation, arrangement of structures, and materials storage will not have a substantially greater effect/impact on surrounding properties than those associated with surrounding uses or other allowable uses in the zone.

The parking and dumpster enclosure are at the rear of the building and will not have an adverse impact on surrounding properties. Signage will conform to City sign regulations and will be submitted for review as part of the final site plan application. No material storage is anticipated.

CJ Developers, Inc.  
35 Primrose Lane  
Freeport, Maine 04032

September 30, 2014

Northern Pride Auto Wash  
c/o Joe Malone  
Malone Commercial Brokers  
5 Moulton Street  
Portland, ME 04101

RE: Letter of Intent re Lease of Property Located at  
20 Marginal Way, Portland, ME

Dear Joe:

This Letter of Intent sets forth the terms and conditions under which CJ Developers, Inc. on behalf of Bangor Savings Bank and/or assigns ("Tenant") is willing to enter into a lease with Northern Pride Car Wash ("Landlord") for the 0.41 ± acre parcel with improvements thereon located at 20 Marginal Way (the "Leased Premises").

Tenant: Bangor Savings Banks

Landlord: Northern Pride Car Wash

Leased Premises: 0.41 +/- acre parcel with improvements thereon located at 20 Marginal Way Portland, Maine. The Leased Premises includes the carwash building located thereon (the "Building") and any improvements.

Initial Term: Twenty (20) years from Rent Commencement Date (as hereinafter defined).

Renewal Terms: Three (3), ten (10) year renewal terms.

NNN Rent: 

Delivery Condition: Tenant will accept the Leased Premises in AS-IS condition without any work by Landlord. Landlord shall remove all chemicals associated with the car wash operation and deliver the Lease Premises free site being clean environmentally under State of Maine General.

**Rent**  
**Commencement**  
**Date:**

Five months after all permits and approvals are received for the construction of a branch bank with drive through..

**Real Estate Taxes**  
**and Utilities:**

Tenant shall be responsible for all real estate taxes and utilities with respect to the Leased Premises.

**Repair and**  
**Maintenance**  
**Obligations:**

Tenant shall be responsible for repairing and maintaining all interior, exterior and structural portions of the Building, including without limitation all electric, plumbing, and mechanical systems serving the Leased Premises.

**Insurance:**

Tenant shall be responsible for maintaining property and casualty insurance on the Building. Tenant shall maintain commercially reasonable liability insurance on Leased Premises indemnifying Landlord from all harm.

**Security Deposit:**

No security deposit.

**Brokerage**  
**Commission:**

Landlord shall be responsible for all brokerage commissions relate to the Lease.

**Contingencies:**

Tenant's obligations under the Lease will be contingent upon the following:

- (i) Tenant being satisfied with its due diligence investigation of the Leased Premises, such due diligence investigation to be completed within 75 days from signing of this Letter of Intent.
- (ii) Tenant obtaining approval to enter into a Lease Agreement from the Bangor Savings Bank Board of Directors within 75 days from signing of this Letter of Intent.
- (iii) Tenant obtaining all necessary state and local permits and approvals for the construction of a new building including a drive thru no later than [REDACTED] from execution of the Lease Agreement. Tenant to diligently pursue such permits and approvals in good faith;

**Lease Agreement:**

Parties shall negotiate in good faith to execute a Lease Agreement within

██████ of the full execution of this Letter of Intent.

**Offer Expiration**

This Letter of Intent will expire on October 8, 2014 at 5:00 p.m. if not executed by Landlord prior to then.

**Exclusivity:**

Upon acceptance of this Non-Binding Letter of Intent, Landlord agrees to take the Leased Premises off the market and not enter into any discussions with other third parties regarding the acquisition or leasing of its property. Notwithstanding anything contained herein to the contrary, this provisions shall be binding on the Landlord.

**Binding Effect:**

This Letter is intended to be confirmation of interest between the parties in pursuing negotiations for a definitive agreement based on the terms hereof and, shall not constitute a binding agreement between the parties hereto. No agreement shall be binding unless and until each party has reviewed and approved (in its sole discretion) a definitive written agreement incorporating all the terms, conditions, and obligations of the parties, and has duly executed and delivered such agreement.

CJ Developers, Inc.

By: David Catalipso

SEEN & AGREED

Northern Pride Auto Wash, Landlord

By: [Signature]

Its: \_\_\_\_\_

Print

Name: William De Sena

# NARRATIVE

## Project Narrative

The following narrative presents the information required for the Preliminary Level III Site plan application.

### Project Description:

The site is located adjacent to the northeast corner of the intersection of Marginal Way and Forest Avenue in Portland. The existing site is currently occupied by Northern Pride Car Wash and Detailing Center. The Car Wash is located approximately 250' from the above referenced intersection. There are currently two curb cuts for the car wash site. The existing easterly driveway for the car wash is located at the beginning of a two way left turn lane within Marginal Way.

Bangor Savings Bank intends to demolish the existing building and construct a 2,100 sf footprint branch with 2 drive-thru lanes. The structure would also include two additional stories of approximately 2,800 sf each for use as office space. One curb cut is proposed adjacent to the easterly property line. Based on the City of Portland zoning map, the property is zoned B-7, Mixed Development District Zone. On April 14, 2015 the Planning Board voted to send a text change application from the Applicant to the City Council for review. The change would allow a bank drive thru in a building less than 20,000 as a conditional use. A conditional use permit application has been filed with this preliminary site plan application in anticipation of the text change approval.

### Right, Title and Interest:

Bangor Savings Bank intends to lease the parcel from the current owner. Attachment I contains a notice of intent to lease for the subject parcel.

### State and Federal Permits:

No state or federal permits are required for this project. The project disturbs less than one acre and results in an impervious area of less than one acre.

### Zoning Assessment:

The project is located in the B-7 zone. A bank and office space is a permitted use within the zone. The project proposes a double drive through for the bank. The zoning ordinance lists a bank drive through as a conditional use for banks greater than 20,000 sf of floor area. Since the proposed project has a floor area less than 20,000 sf, an application for a text change to the zoning was submitted to the City. On April 14, 2015 the Planning Board voted to send a text change application from the Applicant to the City Council for review. The change would allow a bank drive thru in a building less than 20,000 sf as a conditional use. A conditional use permit application has been filed with this preliminary site plan application in anticipation of the text change approval.

### Easements or Other Burdens:

There are no existing or proposed easements on-site.

### Proposed Waivers:

The following waivers are requested:

- The applicant requests a waiver from the dimensional requirements for bicycle parking. The City technical standards show a minimum distance for a bike rack from a building to be 24". Due to limited site area, the applicant is proposing to place the bike racks 12" from the building. The 12" dimension will allow for bicycle parking on one side of the bike racks rather than the two sides available with a 24" spacing. The required number of bicycle spaces for this project is 3. The applicant meets the requirement of three spaces by providing three racks with a single bike per rack.
- Due to limited site area the applicant requests a waiver pursuant to 14-526.b.2.b.ii.e *Where site constraints prevent implementation of all or a portion of required parking lot landscaping, as*



*determined by the Reviewing Authority, the requirements may be all or partially waived and the applicant shall contribute an amount proportionate to the cost of required parking lot trees to the City of Portland Tree Fund.*

**Evidence of Financial and Technical Capacity:**

Bangor Savings Bank has constructed numerous banks throughout Maine and has hired Professional Engineers and Architects to prepare construction plans for this project, therefore the Applicant possesses sufficient technical capacity. Attachment 2 contains evidence of the financial capacity for this project.

**Construction Management Plan:**

A construction management plan will be prepared by the General Contractor prior to demolition work at the site as noted on the demolition plan and site plan contained within the plan set. The management plan will address the anticipated start and end date of the project, discuss the construction sequence, and provide a pedestrian circulation plan. The management plan will be submitted to the City Planning Division prior to demolition work.

**Traffic:**

Attachment 3 contains the Traffic Report for this project.

**Significant Natural Features:**

The project site is currently developed with a car wash and associated parking and drive aisles. The parcel size is 17,862 sf with an existing non-vegetated area of 15,769 sf. The remainder of the site is landscaped or grass. No significant natural features are present on-site. There are no wetlands on-site.

**Stormwater:**

Section 14-526.b.3.b of the Land Use Ordinance states that all development other than Level I residential shall comply with Section 5 of the Technical Manual including Basic, General, and Flooding standards as applicable to prevent and control the release of pollutants to waterbodies, watercourses, wetlands and groundwater, and reduce adverse impacts associated with increases or changes in flow, soil erosion and sedimentation.

Section 5.II.c of the Technical Manual states that Level II and III site plans shall be required to submit a stormwater management plan pursuant to the regulations of Maine DEP Chapter 500 Stormwater Management Rules, including Basic, General and Flooding standards.

The Basic Standard is met by the Erosion and Sedimentation Control Report submitted with this application.

**General Standards:** The project site is not tributary to an Urban Impaired Stream. Section 5.III.4.B.1 States when general Standards must be met for areas not tributary to Urban Impaired Streams: A project disturbing one acre or more and resulting in any of the following must meet the general standards:

5.III.4.B.1.b - Other stream, coastal and freshwater wetland watersheds. One acre or more of impervious area, or 5 acres or more of developed area, in any other stream, coastal, or wetland watershed.

Section 5.III.4.B.3.e contains requirements for redevelopment; Stormwater Management Law project including

redevelopment. For a project requiring a Stormwater Management Law permit that includes redevelopment of impervious area that was in existence as of November 16, 2005 (the effective date of Chapter 500 revisions), the redevelopment of that impervious area is not required to meet General standards provided the department determines that the new use of the existing impervious area is not likely to increase stormwater impacts resulting from the proposed project's stormwater runoff beyond the level of impact already caused by the runoff from the existing impervious area. The requirements of Appendix D must still be met, if applicable.

The proposed project will disturb a maximum of 17,862 sf (0.41 acres) and is a redevelopment of an existing developed site. The proposed bank and associated parking will not increase the stormwater impacts beyond the level of the existing car wash. As discussed with Dave Margolis-Pineo of the City of Portland, the Applicant proposes to maintain the current runoff characteristics by having a portion of the site draining forward to the Marginal Way stormdrain system and the remainder of the site draining to the rear where it will sheet flow through vegetation before crossing the rear property line.

Since the proposed impervious and developed area are less than the Chapter 500 thresholds and the project is a redevelopment which will not increase stormwater impacts, the General Standards have been met.

Since the disturbed area is less than one acre, neither a Construction General Permit nor MDEP Stormwater Permit are required.

**Flooding Standard:** Section 5.III.4.E states: If required, the flooding standard applies in addition to the basic standards, general standards, phosphorus standards and urban impaired stream standards.

When the flooding standard must be met. If a project results in three acres or more of impervious area or 20 acres or more of developed area, requires review pursuant to the Site Law, or is a Site Law modification of any size, the flooding standard must be met.

The project proposes 0.37 acres of impervious area and 0.41 acres of developed area. The proposed project will result in an insignificant increase in impervious area on-site (0.005 acres). The current impervious area is approximately 15,769 square feet. The proposed project will result in approximately 15,976 sf (0.37 acres) of impervious area. The project results in an insignificant increase in impervious area, therefore there is no increase in the runoff from the site as a result of development.

Since the proposed impervious and developed area are less than the Chapter 500 thresholds and there is no increase in runoff, the Flooding Standard is met.

**Conclusion:** The proposed project results in a redevelopment of an existing car wash site and will not increase the stormwater impacts from the site. The disturbed area of 0.41 acres and the proposed impervious area of 0.37 acres are well below the Chapter 500 standards included in Chapter 5 of the Portland Technical Standards. As discussed with Dave Margolis-Pineo the runoff from the site will enter the Marginal Way stormdrain or sheet flow to the rear of the site. The proposed project will not have an adverse impact due to the stormwater runoff from the site.

#### **Master Plan:**

The proposal aligns with the relevant goals and objectives of the City's Master Plan for Marginal Way. The building is located adjacent to the Right-of-Way with parking located to the rear of the site. As shown on the Master Plan for the area, the sidewalk is proposed adjacent to the building with an esplanade adjacent to Marginal Way. One access drive to the site is proposed as shown on the Master Plan, and the building is three stories tall.

#### Utility Capacity:

Attachment 5 contains letters sent to the Portland Water District and Portland Public Services requesting confirmation of their ability to serve the project. Responses will be forwarded to the City upon receipt.

#### Solid Waste Management:

An enclosed dumpster pad is shown on the proposed plans. The project is anticipated to produce the following quantities of solid waste.

- Demolition – Approximately 27 c.y.
- Construction Waste – Approximately 49 c.y.
- Commercial Solid Waste, Non-Recyclable – 18 c.y. per month

Commercial Solid Waste, Recyclable – 15 c.y. per month

Attachment 5 contains a letter sent to Pine Tree Waste requesting their ability to serve the project. The response from Pine Tree Waste will be forwarded to the City upon receipt.

#### NFPA:

A code summary is included in Attachment 6. An existing fire hydrant is located approximately 180 feet from the Marginal Way Right-Of-Way.

#### Design Standards:

The development is in conformance with the design standards of Section 14-526 of the City Land Use Code as noted below. Waiver requests from the standards are also noted below.

14-526 a Transportation Standards – See Traffic Report contained in Attachment 3. The required offstreet parking is shown on the plans. Three bicycle parking spaces are shown on the plan. A waiver from the dimensional requirements of the bicycle parking spaces is requested. Due to limited site area snow will not be stored on-site and will be removed by the snow removal contractor as noted on the site plan.

14-526 b Environmental Quality Standards – The site is currently developed and there are no significant natural features on-site. The dumpster is screened from view by the proposed enclosure, and mechanical systems will be roof mounted and screened from view. Due to limited site area, a waiver from the parking lot landscaping requirements is requested as noted in the waiver request noted previously. Stormwater management is discussed above.

14-526 c Public Infrastructure and Community Safety Standards – The orientation of the building, parking, and access drive is in conformance with the City Master Plan. The site is within 180 feet of an existing fire hydrant. The proposed electric service is underground and the applicant has contacted the Portland Water District and the Department of Public services to obtain ability to serve letters for water service and wastewater service. The proposed utility connections are shown on the Site and Utility plan of the plan set.

14-526 d Site Design Standards – The proposed building location and height is in conformance with the Marginal Way master plan. Attachment 7 contains the proposed building elevations. All site lighting will be full cut off and will not impact adjoining properties. A lighting plan and catalog cuts of the proposed lighting will be provided in the final submission. HVAC units shall comply with applicable state and federal emission requirements. Signage will comply with City of Portland regulations. The signage information will be submitted at a later date.

**HVAC Verification:**

The actual HVAC units are to be determined. All units used on the project will comply with all applicable state and federal emissions standards.

**Boundary Survey**

A boundary survey is included within the plan set.

**Plan Set**

A plan set containing the required information is included with this application.

JM's 5/27/15 & 5/28/15 & 5/29/15 changes to SMOOTH 05.22.15

TERM SHEET

Between

Bangor Savings Bank, as Tenant

And

Northern Pride Auto Wash, as Landlord

SUMMARY INFORMATION

Premises Address: 20 Marginal Way  
Portland, Maine

Land: The parcel of land owned by Landlord upon which Tenant shall construct improvements to be located thereon being more particularly portrayed on Exhibit A and bounded and described in Exhibit A-1 attached hereto and made a part hereof by reference (the "Leased Premises").

Building: The building to be built by Tenant will consist of (i) 7,500 square feet of building on the Land (the "Building"), (ii) the drive-thru canopy structure (the "Drive-thru Canopy") attached to the Building, and (iii) the two (2) drive lanes servicing the Drive-thru Canopy; the space, the Drive-thru Canopy and the drive lanes are outlined on the conceptual site plan attached as Exhibit B (the "Site Plan") (the "Improvements").

Commencement Date: 10/3 2015.

Expiration Date:            years next following the Occupancy and Rent Commencement Date.

Extensions:            Ten (10) year extensions, provided Tenant provides written notice to Landlord not less than Two Hundred Seventy (270) days prior to the expiration of the initial term or applicable extension term. If Tenant fails to exercise any such extension, Landlord will give a Thirty (30) day written notice to Tenant during which Tenant may then exercise such applicable extension, all to afford an inadvertent failure to notify Landlord of the exercise of the extension.

Right of First Refusal: During the term of the Lease, including any extensions provided Tenant is not then in default beyond any applicable grace or cure period, Tenant shall have a right of first refusal to purchase the Leased Premises as follows:

Landlord shall have the right to list and show the Leased Premises for sale at any time during the Lease (or any extension). If Landlord wishes to sell the Leased Premises Landlord shall give to Tenant a notice in writing specifying the terms and conditions upon which another party desires to purchase the Leased Premises and Landlord shall provide Tenant with a certified copy of any executed purchase and sale contract evidencing said terms and conditions. In this notice Landlord shall offer to sell the said Leased Premises to Tenant upon said terms and conditions. If, within ten (10) days after receipt of such notice, Tenant desires to purchase the Leased Premises on the same terms and conditions it shall provide timely written notice to Landlord of such intention and the parties shall then proceed to close the transaction. If Tenant rejects the offer, or if Tenant does not respond to said notice within the 10 days period, then Landlord shall be free to sell to any person upon terms and conditions no more favorable than those specified in said notice. If a sale is to be made on terms and conditions other than so specified then the right to purchase shall again be offered to Tenant as set forth above. However, this right of first refusal shall expire upon the sale of the Leased Premises to a third party in a transaction complying with this provision.

Tenant Deposits: There shall be no security deposits.

Conditions of Lease: This Lease is conditioned upon both of the following:

- a. Final Approval by Tenant's Board by July 15, 2015; and,
- b. Obtaining all necessary approvals and permits.

Landlord Contact: Address: Northern Pride Auto Wash  
c/o Mr. William Desena

1849 Sunset Ridge Drive  
The Villages, FL 32162

Tenant Contact: Bangor Savings Bank  
Real Estate Management Department  
99 Franklin Street  
P.O. Box 930  
Bangor, ME 04402-0930  
Attention: Robert Montgomery-Rice, Senior Vice President

## GROUND LEASE

### 1. BASIC PROVISIONS ("Basic Provisions").

1.1 **Parties.** This Lease ("Lease") dated, for reference purposes only, June 3, 2015, is made by and between **Bangor Savings Bank**, a Maine financial institution ("Tenant") and **Northern Pride Auto Wash**, a Maine corporation (or assigns) ("Landlord"), (collectively the "Parties," or individually a "Party").

1.2 **Conditions of Lease.** Notwithstanding any provision herein, this Lease shall become an obligation of the Tenant only if its Board grants final approval of this Lease by July 15, 2015 and when all necessary permits and approvals have been obtained.

1.3 **Land.** The parcel of land owned by Landlord upon which Tenant shall construct the Building and Improvements, being more particularly bounded and described in Exhibit A attached hereto and made a part hereof by reference (the "Leased Premises").

1.4 **Building.** Consisting of (i) a building with a footprint of approximately 2,108 square feet (the "Building") to be constructed by Tenant on the Leased Premises, (ii) the drive-thru canopy structure (the "Drive-thru Canopy") attached to the Building, and (iii) the two (2) drive lanes servicing the Drive-thru Canopy; the space, the Drive-thru Canopy and the drive lanes are outlined on the conceptual site plan attached as Exhibit B as same may be modified based on the final site plan approval by the City of Portland and to be attached and initialed by Landlord and Tenant, if modified (the "Site Plan") (the "Improvements").

1.5 **Demolition.** During the "Construction Term" defined at 1.6 (a), Tenant will demolish the current building on the Leased Premises at its sole cost and expense as part of the construction project, and shall be obligated to construct the building described in section 1.4 above

1.6 **Terms.** There shall be two Terms to this Lease as follows:

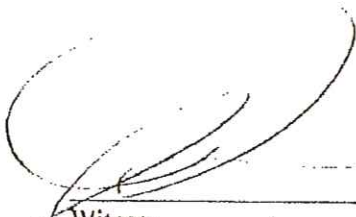
(a) **Construction Term.** The Construction Term shall commence on the first date following the date on which the building permit is issued and continue until the same date of the month four (4) calendar months following.

(b) **Occupancy and Rent Commencement Date.** Occupancy may commence and Rent shall commence on the earlier of December 1, 2015 or the date the Occupancy Permit issues. Rent shall commence whether or not an Occupancy Permit has been issued if the earlier event is December 1, 2015. If occupancy commences on other than the first day of the month then rent shall be prorated for the number of days in that month. The Terms of this Lease shall commence on the Occupancy and Rent Commencement Date and shall terminate on the last day of the Two Hundred Forty (240) full months (20 years) following the Occupancy and Rent Commencement Date. Provided Tenant is not in default, Tenant shall have the option to extend this Lease for Five (5) successive Ten (10) year terms, each pursuant to the provisions of Section 3.2.


Tenant agrees to execute such documents as Landlord may reasonably request to evidence such waiver. An affidavit filed by Landlord in the Cumberland County Registry of Deeds certifying that Tenant's Right of First Offer has been waived or has been deemed to be waived as provided herein may be relied upon by any third party as to the accuracy and truthfulness of the statement set forth herein and such third party may rely upon such affidavit.

**39. BINDING EFFECT.**

This lease shall be binding upon and inure to the benefit of the parties hereto and their respective heirs, legal representatives, successors and assigns.

  
\_\_\_\_\_  
Witness


**"Landlord"**

  
\_\_\_\_\_  
*William De Sena*  
Its Authorized Member

**"Tenant"**

Bangor Savings Bank

  
\_\_\_\_\_  
Witness

By:   
\_\_\_\_\_  
Its President and CEO



# ATTACHMENT 3

## TRAFFIC

Traffic Impact Study  
Proposed Bangor Savings Bank  
Marginal Way  
Portland, Maine  
May 2015

*Index*

<i>Section</i>	<i>Description</i>	<i>Page</i>
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VII.	2016 Post Development Traffic	4
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*Appendix A*

Site Location Diagram  
Turning Movement Diagrams

*Appendix B*

Capacity Analyses Results

*Appendix C*

Trip Generation calculations  
Sebago Tech Memo  
MaineDOT Crash Data  
Site Plan

## Executive Summary

The following Executive Summary is prepared for the reader's convenience, but is not intended to be a substitute for reading the full report.

Gorrill-Palmer (GP) was retained by Bangor Savings Bank to examine the traffic impacts associated with a proposed three story 7,708 square foot Bangor Savings Bank building with two drive-through lanes to be located on the northwesterly side of Marginal Way in Portland. The site is currently occupied by the Northern Pride Auto Wash, which will be closed and demolished as part of this development. The site is currently served by two driveways. The westerly driveway will be removed and the easterly driveway will remain to serve the proposed bank. A site location map is included as Figure I in Appendix A.

Based on the results of the study, our office finds the following:

1. The proposed development is forecast to generate 61 and 84 trip ends for the weekday AM and PM peak hours of the generator, respectively. However, the existing Northern Pride Auto Wash generates 106 and 118 trip ends for the weekday AM and PM peak hours, respectively, for which credit can be taken. Therefore, this project will result in a decrease of 45 and 34 trip ends in the weekday AM and PM peak hours for the cart wash, respectively. Since there is not a net traffic increase of 100 peak hour trip ends, a MaineDOT traffic movement permit will not be required.
2. The level of service analyses show that the proposed project will have a minimal impact on traffic flow in the vicinity of the site.
3. GP reviewed the MaineDOT crash data for the years 2012 – 2014. Based on the published history, the roadway segment where the site drive is located is not classified as a high crash location.
4. The proposed project will result in the closure of one of the two driveways currently serving the site. The available sight distances at the remaining easterly driveway exceed local and MaineDOT sight distance requirements. GP recommends that all plantings, which will be located within the right of way, not exceed three feet in height and be maintained at or below that height. Signage should not interfere with sight lines. In addition, we recommend that during construction, when heavy equipment is entering and exiting the site, that appropriate measures, such as signage and flag persons, be utilized in accordance with the Manual on Uniform Traffic Control Devices.
5. The proposed Bangor Savings site has been designed to facilitate pedestrian circulation and safety.

Based on these findings, it is the opinion of GP that the existing street system can accommodate the traffic generated by the redevelopment of this site.

## I. *Proposed Site*

The site is currently occupied by the Northern Pride Auto Wash, which will be closed and demolished as part of this development. The site is currently served by two driveways. The westerly driveway will be removed and the easterly driveway will remain to serve the proposed bank. A site location map is included as Figure I in Appendix A.

Proposed for the site is a three story 7,708 square foot Bangor Savings Bank building with two drive-through lanes. The lower level of the building will be a retail branch with two drive thru lanes and the upper two floors will be offices for bank management.

## II. *Background Traffic Conditions*

GP based the study on the following information:

- A proposed site plan prepared by GP dated May 2015.
- Crash information for 2012-2014 provided by the Maine Department of Transportation (MaineDOT).
- Post development traffic volumes for the Federated property project in Bayside furnished by FST for the following intersections
  - ❖ State, Kennebec and Marginal
  - ❖ Marginal and Preble
- Memorandum from Bradley Lyon of Sebago Technics entitled "Trip Generation for 20 Marginal Way, Portland, Maine"

### **Predevelopment Traffic Volumes**

#### *Federated Properties*

The Bayside area development proposed by Federated Properties was recently approved by the City of Portland. FST furnished their projected post development traffic volumes to GP which were utilized as the predevelopment volumes for the proposed Bangor Saving Bank Project.

## III. *Trip Generation*

Proposed for the site is a three story 7,708 sf Bangor Savings Bank building. The ground floor of the building will be a 2,108 sf retail bank with two drive-through lanes. The upper two floors will be bank offices consisting of 2,800 sf each. The existing Northern Pride car wash currently on the site will be demolished.

In order to determine the need for a MaineDOT Traffic Movement Permit, GP has estimated the trip ends generated by the current use of the site as well as the proposed bank and office building using the Institute of Transportation Engineers (ITE) publication Trip Generation. A trip end is defined as an in or out, thus a round trip is equal to two trip ends. These calculations presented below show there is

a net decrease in trip ends during the AM and PM peak hours of the proposed bank building compared to the car wash. Since there is not a net traffic increase of 100 peak hour trip ends, a MaineDOT traffic movement permit will not be required.

*Trips associated with the Northern Pride car wash-* GP utilized a memorandum from Bradley Lyons of Sebago Technics to William DeSena dated August 29, 2014 to estimate the trip ends associated with the car wash. A copy of this memo is included in the Appendix to this report. The memo analyzed the trip generation associated with the car wash using sales data for weekdays in January, April and December and on a Sunday in April of 2013. The data showed a weekday average of 106 AM and 118 and PM trip ends during the peak hours for the car wash for the data compiled.

*Trip Estimate for the Proposed Bank-* For the purposes of this analysis, GP has estimated the trips generated by the proposed bank using Land Use Code 912, Drive-In Bank of the 9<sup>th</sup> Edition of the Institute of Transportation Engineers publication, Trip Generation. A summary of the resulting trip generation estimate is presented below. The trip generation calculations are included in Appendix C.

**Trip Generation – Net Increase for Proposed Bangor Savings Bank (\*Trip Ends) During the Peak hour of the Generator**

Time Period	Car Wash	Bangor Savings Bank	Net Increase
Daily	1076	432	-644
Weekday AM Peak Hr of Generator	106	61	-45
Weekday PM Peak Hr of Generator	118	84	-34
Saturday Peak Hr of Generator	116	57	-59

\*A trip end is either a trip into or out of the site. Thus a round trip equals two trip ends.

These results show that the peak hours of the proposed project will generate less than currently occurs during the peak hours of the existing car wash.

The peak hour of the adjacent street traffic on Marginal Way generally occurs from 7:30 to 8:30 AM and again from 4:30 to 5:30 PM. A comparison of the existing and proposed uses during the peak hour of the adjacent street traffic is summarized below:

**Trip Generation – Net Increase for Proposed Bangor Savings Bank (\*Trip Ends) During the Peak Hour of Adjacent Street Traffic**

Time Period	Car Wash	Bangor Savings Bank	Net Increase
Daily	1076	432	-644
Weekday AM Peak Hr of Adj Street	85	43	-42
Weekday PM Peak Hr of Adj Street	98	84	-14
Saturday Peak Hr of Adj Street	116	57	-59

These results show that during the peak hour of the adjacent street traffic the proposed project will generate less than currently occurs during the peak hour of the adjacent street traffic for the existing car wash.

IV. *Trip Distribution*

GP has estimated the trip distribution based on the information published by ITE which is summarized below.

Land Use	AM Peak	PM Peak
<b>Retail Bank</b>		
Entering	60%	50%
Exiting	40%	50%
<b>Office</b>		
Entering	90%	15%
Exiting	10%	85%

V. *Trip Composition*

GP has utilized the following trip composition based on information obtained from the ITE publication, *Trip Generation Handbook* for Land Use Codes 715 and 912, Single Tenant Office Building and Drive-In Bank respectively. The percentages were compiled for the AM and PM peak hours as follows:

Trip Composition for Proposed Bangor Savings Bank

Trip Type	AM Peak Hour				PM Peak Hour			
	Office		Bank		Office		Bank	
	Enter	Exit	Enter	Exit	Enter	Exit	Enter	Exit
Primary	18	2	3	2	2	23	7	8
Pass-by	0	0	7	5	0	0	15	15
Diverted	0	0	3	2	0	0	7	7
<b>Total</b>	<b>18</b>	<b>2</b>	<b>13</b>	<b>9</b>	<b>2</b>	<b>23</b>	<b>29</b>	<b>30</b>

VI. *Trip Assignment*

The trip assignment for the proposed site is based on existing traffic patterns. Trip assignment is shown on Figure 3 in Appendix A. Since the Northern Pride car wash was operational when the traffic counts were completed for Federated Properties we have assumed the development traffic was already in the traffic stream at the adjacent intersections.

VII. **2016 Post Development Traffic**

The anticipated year 2016 predevelopment traffic shown on Figure 2 of Appendix A has been combined with the trips forecast for the development shown on Figure 3 of Appendix A to yield the 2016 postdevelopment traffic shown on Figure 4 of Appendix A.

VIII. *Study Area*

Since the proposed project is forecast to generate less trips than the car wash during both the adjacent street as well as the overall peak hours, the study area for the project is limited to the site driveway onto Marginal Way.

IX. *Capacity Analyses*

The capacity analyses were performed using the Synchro / Simtraffic computer software, with an average of five runs. Levels of service rankings are similar to the academic ranking system where an 'A' represents little control delay and an 'F' represents significant delay. A level of service 'D' or above is desired at a signalized intersection. At an unsignalized intersection, if the level of service falls below a 'D', an evaluation should be made to determine if further mitigation is warranted.

The following table summarizes the relationship between control delay and level of service for an unsignalized intersection:

Level of Service	Control Delay per Vehicle (sec)
A	Up to 10.0
B	10.1 to 15.0
C	15.1 to 25.0
D	25.1 to 35.0
E	35.1 to 50.0
F	Greater than 50.0

Site Driveway / Marginal Way

The results of the capacity analyses for the site driveway for the AM and PM peak hours of adjacent street traffic are summarized as follows. The detailed analyses are included in Appendix B.

Approach	Peak Hour	
	AM Post	PM Post
Site Drive / Marginal Way		
Site Drive - SB	A	B
Marginal - NE	A	A
Marginal - SW	A	A

As can be seen from the results, the each of the approaches of the site driveway intersection are forecast to operate at acceptable levels of service.

X. *Crash Data*

In order to evaluate whether a location has a crash problem, MaineDOT uses two criteria to define High Crash Locations (HCL). Both criteria must be met in order to be classified as an HCL.

1. A critical rate factor of 1.00 or more for a three-year period. (A Critical Rate Factor {CRF} compares the actual accident rate to the rate for similar intersections in the State. A CRF of less than 1.00 indicates a rate less than average) and;
2. A minimum of eight crashes over the same three-year period.

The following summarizes the crash history for the roadway segment where the site driveway is located.

MaineDOT Crash Data for 2012-2014: Road Segments

Nodes	Street	From	To	# of Collisions	CRF	HCL?
60346-18999	Marginal Way	Forest Ave	Hanover St	2	0.31	No

Based on the published history provided by MaineDOT, the roadway segment where the site driveway is located is not classified as a high crash location.

XI. *Sight Line Analysis*

The Maine Department of Transportation (MaineDOT) and the City of Portland have guidelines for sight distances at roadways. The sight line standards for MaineDOT and the City of Portland are as follows:

Sight Distance Requirements

Speed (mph)	MaineDOT (ft)	City of Portland (ft)
25	200	367
30	250	440
35	305	513
40	360	587
45	425	660
50	495	773

GP has evaluated the available sight lines at the proposed site driveway on Marginal Way in accordance with MaineDOT and City of Portland standards.

The MaineDOT standards are as follows:

Roadway observation point:	10 feet off major street travelway
Height of eye at roadway:	3 ½ feet above ground
Height of approaching vehicle:	4 ¼ feet above road surface



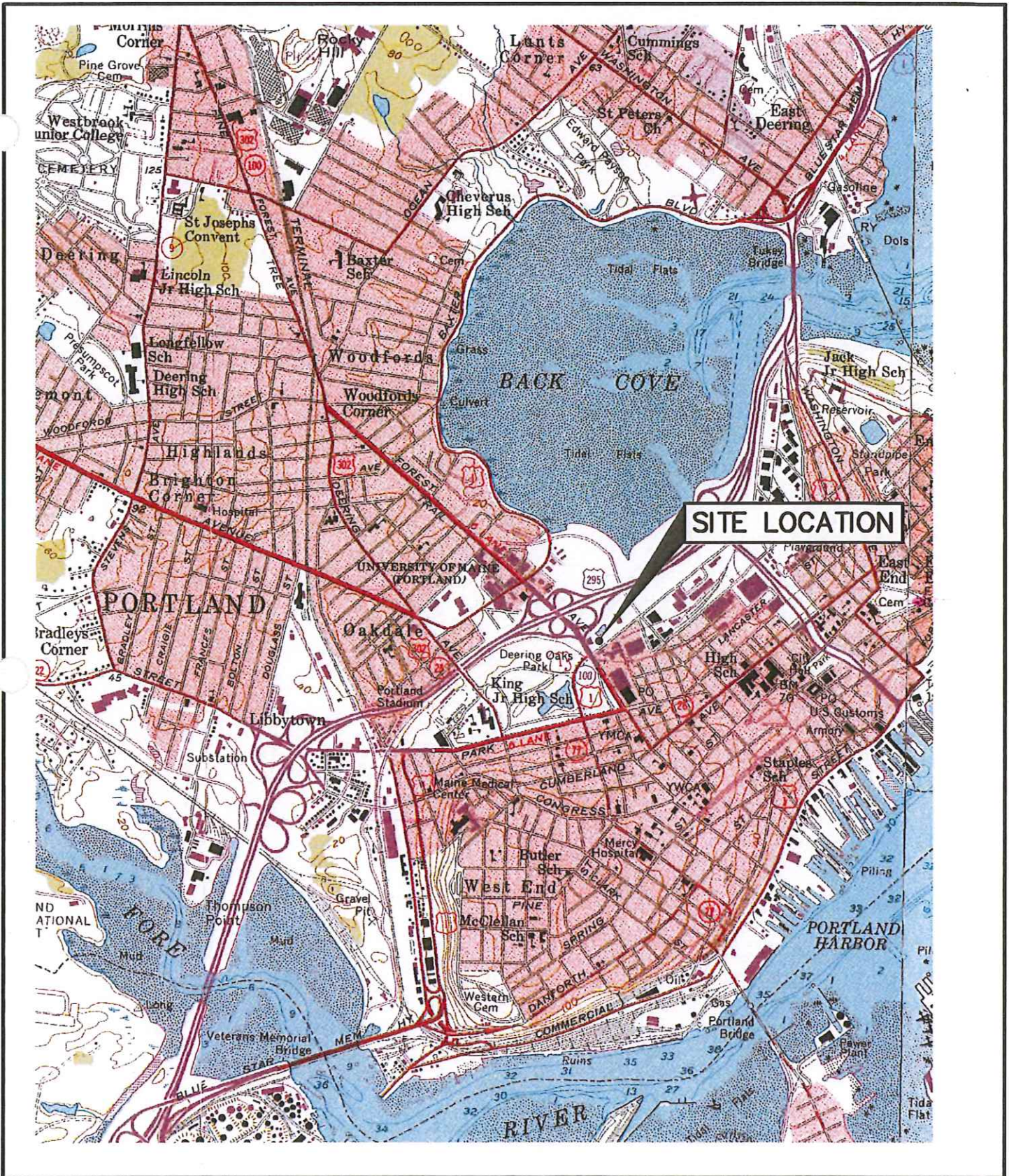
The posted speed limit on Marginal Way in the site vicinity is 35 mph. Based on a site review, the sight distance looking to the right is to Forest Avenue, and the sight distance exceeds 550 feet looking to the left. Therefore, the available sight distances are acceptable.

GP recommends that all plantings, which will be located within the right of way, not exceed three feet in height and be maintained at or below that height. Signage should not interfere with sight lines. In addition, we recommend that during construction, when heavy equipment is entering and exiting into the site, that appropriate measures, such as signage and flag persons, be utilized in accordance with the Manual on Uniform Traffic Control Devices (MUTCD).

## XII. *Pedestrian Circulation*

The proposed Bangor Savings site has been designed to facilitate pedestrian circulation and safety. The building has been brought to the front of the lot to facilitate pedestrian access to the front of the building. The building will also be fronted by a sidewalk to facilitate convenient pedestrian access to the building. Furthermore, the existing car wash has two curb cuts on Marginal Way. The proposed Bangor Saving Bank project will eliminate one of the existing curb cuts to improve pedestrian safety.

APPENDIX A



**U.S.G.S. Location Map**  
 Bangor Savings Bank - Portland, Maine  
 U.S.G.S. Portland-East, Maine-7.5 Minute Series (Topographic)

Design: JWA	Date: may 2015
Draft: CG	Job No.: 2970
Checked: AMP	Scale: None
File Name: 2970-LOCATION.dwg	

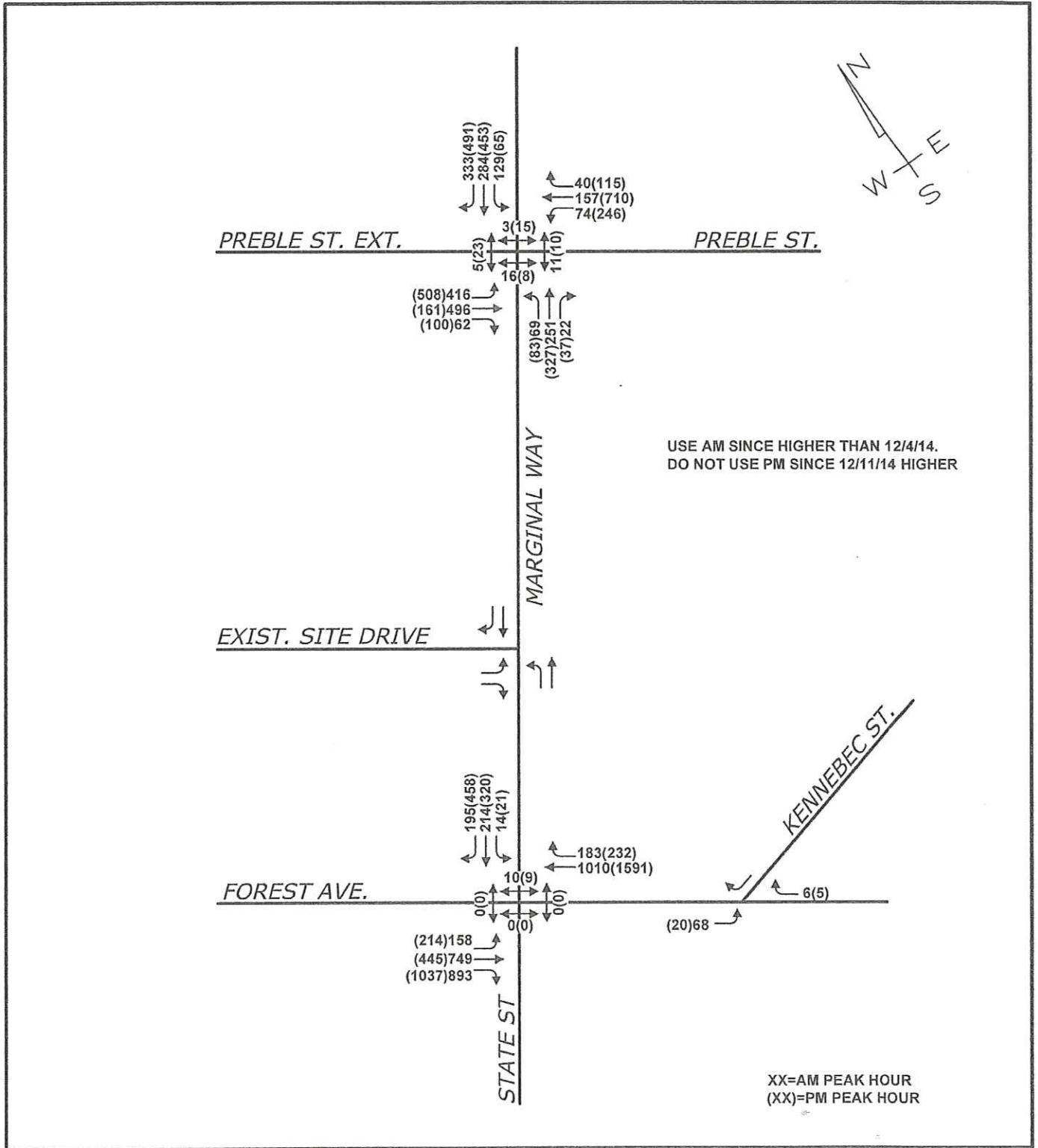


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[www.gorrillpalmer.com](http://www.gorrillpalmer.com)  
 207.657.6910

Figure

1

# 2016 Predevelopment Volumes



## PROPOSED BANGOR SAVINGS, PORTLAND, MAINE

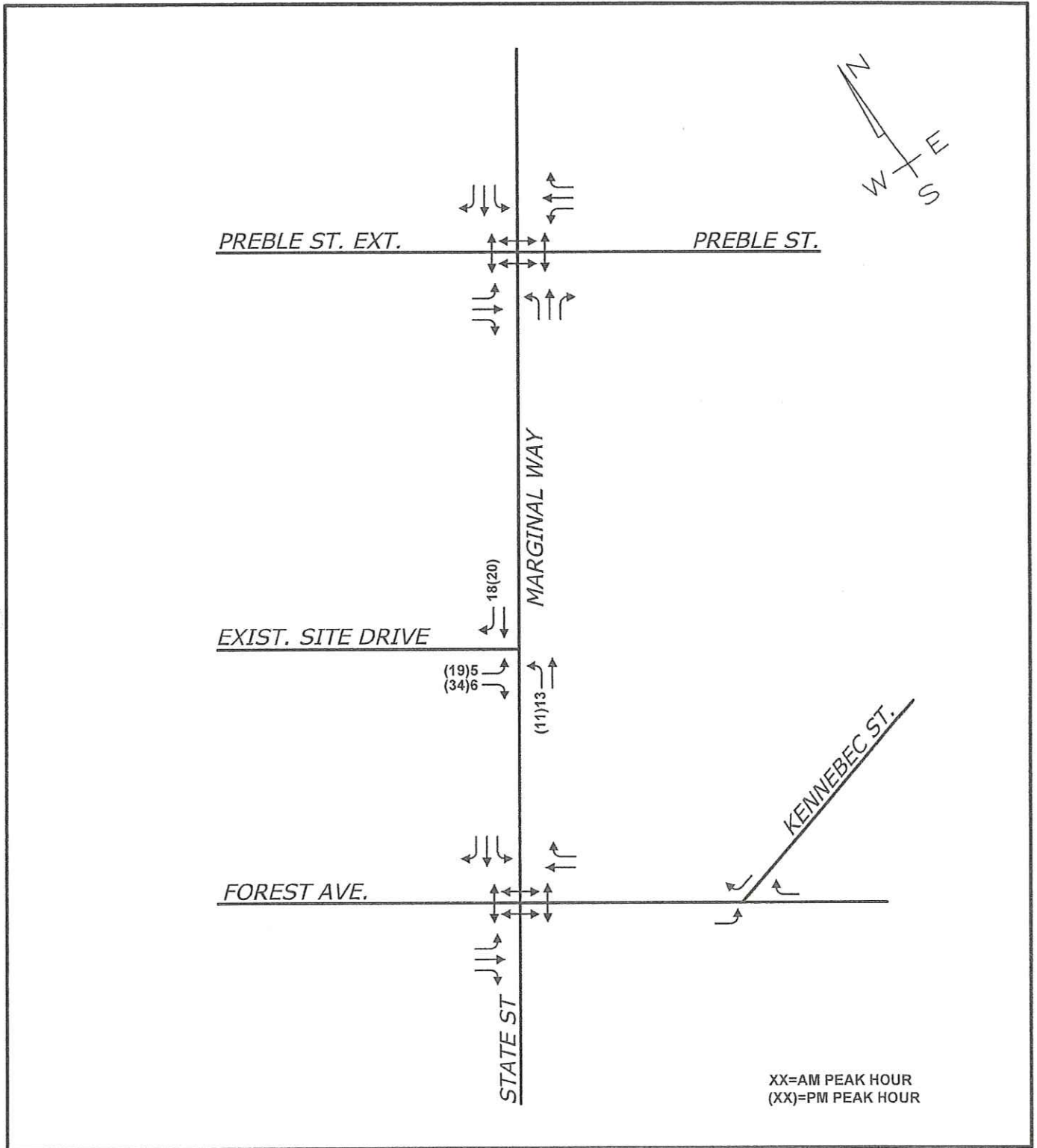
Design: TLG  
 Draft: DB  
 Checked: -

Scale: NONE  
 Date: APR 2015  
 File Name: 2970-TRAFF.dwg



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 207.657.6910

# Development Volumes



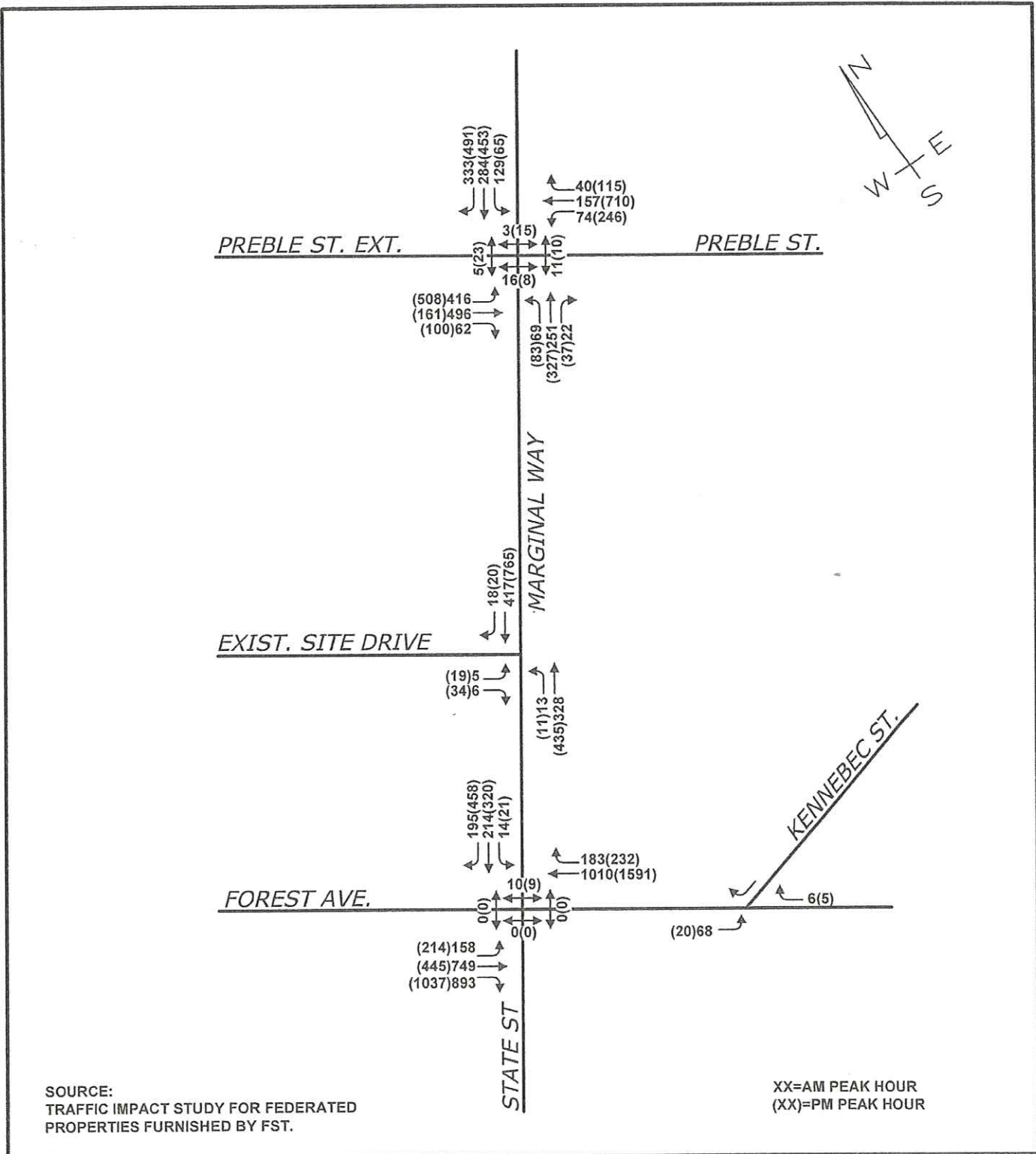
## PROPOSED BANGOR SAVINGS, PORTLAND, MAINE

Design: TLG    Scale: NONE  
 Draft: DB    Date: APR 2015  
 Checked: -    File Name: 2970-TRAFF.dwg



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 207.657.6910

# 2016 Postdevelopment Volumes



SOURCE:  
 TRAFFIC IMPACT STUDY FOR FEDERATED  
 PROPERTIES FURNISHED BY FST.

## PROPOSED BANGOR SAVINGS, PORTLAND, MAINE

Design: TLG    Scale: NONE  
 Draft: DB    Date: APR 2015  
 Checked: -    File Name: 2970-TRAFF.dwg



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APPENDIX B

## Summary of All Intervals

Run Number	1	2	3	4	5	Avg
Start Time	6:57	6:57	6:57	6:57	6:57	6:57
End Time	8:00	8:00	8:00	8:00	8:00	8:00
Total Time (min)	63	63	63	63	63	63
Time Recorded (min)	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1
Vehs Entered	811	853	785	777	739	792
Vehs Exited	809	856	785	777	738	793
Starting Vehs	6	7	7	2	5	5
Ending Vehs	8	4	7	2	6	5
Travel Distance (mi)	124	132	120	120	114	122
Travel Time (hr)	4.5	4.8	4.4	4.3	4.1	4.4
Total Delay (hr)	0.3	0.3	0.3	0.2	0.2	0.2
Total Stops	29	18	16	13	9	17
Fuel Used (gal)	3.9	4.1	3.8	3.7	3.4	3.8

## Interval #0 Information Seeding

Start Time	6:57
End Time	7:00
Total Time (min)	3
Volumes adjusted by Growth Factors.	
No data recorded this interval.	

## Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60
Volumes adjusted by Growth Factors.	

Run Number	1	2	3	4	5	Avg
Vehs Entered	811	853	785	777	739	792
Vehs Exited	809	856	785	777	738	793
Starting Vehs	6	7	7	2	5	5
Ending Vehs	8	4	7	2	6	5
Travel Distance (mi)	124	132	120	120	114	122
Travel Time (hr)	4.5	4.8	4.4	4.3	4.1	4.4
Total Delay (hr)	0.3	0.3	0.3	0.2	0.2	0.2
Total Stops	29	18	16	13	9	17
Fuel Used (gal)	3.9	4.1	3.8	3.7	3.4	3.8



3: Site Drive & Marginal Way Performance by approach

Approach	SB	NE	SW	All
Denied Del/Veh (s)	0.1	0.4	0.3	0.4
Total Del/Veh (s)	5.0	0.4	0.5	0.5

Total Network Performance

Denied Del/Veh (s)	0.4
Total Del/Veh (s)	0.7

---

**Intersection: 3: Site Drive & Marginal Way**

---

Movement	SB	NE
Directions Served	LR	L
Maximum Queue (ft)	24	31
Average Queue (ft)	7	5
95th Queue (ft)	25	25
Link Distance (ft)	243	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		50
Storage Blk Time (%)		0
Queuing Penalty (veh)		0

---

**Network Summary**

---

Network wide Queuing Penalty: 0

Summary of All Intervals

Run Number	1	2	3	4	5	Avg
Start Time	6:57	6:57	6:57	6:57	6:57	6:57
End Time	8:00	8:00	8:00	8:00	8:00	8:00
Total Time (min)	63	63	63	63	63	63
Time Recorded (min)	60	60	60	60	60	60
# of Intervals	2	2	2	2	2	2
# of Recorded Intervals	1	1	1	1	1	1
Vehs Entered	1316	1377	1264	1211	1229	1279
Vehs Exited	1321	1381	1263	1212	1230	1281
Starting Vehs	10	10	8	5	5	7
Ending Vehs	5	6	9	4	4	5
Travel Distance (mi)	202	212	194	186	189	196
Travel Time (hr)	7.7	8.2	7.4	7.0	7.1	7.5
Total Delay (hr)	0.7	0.9	0.7	0.6	0.7	0.7
Total Stops	55	62	58	55	52	57
Fuel Used (gal)	6.6	6.9	6.4	6.0	6.1	6.4

Interval #0 Information Seeding

Start Time	6:57
End Time	7:00
Total Time (min)	3
Volumes adjusted by Growth Factors.	
No data recorded this interval.	

Interval #1 Information Recording

Start Time	7:00
End Time	8:00
Total Time (min)	60
Volumes adjusted by Growth Factors.	

Run Number	1	2	3	4	5	Avg
Vehs Entered	1316	1377	1264	1211	1229	1279
Vehs Exited	1321	1381	1263	1212	1230	1281
Starting Vehs	10	10	8	5	5	7
Ending Vehs	5	6	9	4	4	5
Travel Distance (mi)	202	212	194	186	189	196
Travel Time (hr)	7.7	8.2	7.4	7.0	7.1	7.5
Total Delay (hr)	0.7	0.9	0.7	0.6	0.7	0.7
Total Stops	55	62	58	55	52	57
Fuel Used (gal)	6.6	6.9	6.4	6.0	6.1	6.4

3: Site Drive & Marginal Way Performance by approach

Approach	SB	NE	SW	All
Denied Del/Veh (s)	0.1	0.4	0.7	0.6
Total Del/Veh (s)	12.2	0.4	0.8	1.1

Total Network Performance

Denied Del/Veh (s)	0.6
Total Del/Veh (s)	1.5

---

**Intersection: 3: Site Drive & Marginal Way**

---

Movement	SB	NE
Directions Served	LR	L
Maximum Queue (ft)	66	36
Average Queue (ft)	23	6
95th Queue (ft)	50	26
Link Distance (ft)	243	
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		50
Storage Blk Time (%)		0
Queuing Penalty (veh)		0

---

**Network Summary**

---

Network wide Queuing Penalty: 0

## APPENDIX C

JN:  
 Project Description:  
 Project Location:  
 Date:

2970  
 Bangor Savings Bank  
 Marginal Way, Portland  
 5/4/2015

Gorrill-Palmer Consulting Engineers, Inc.  
 P.O. Box 1237  
 15 Shaker Road  
 Gray, Maine 04039

**Single Tenant Office Building  
 Land Use Code (LUC) 715**

Gross Floor Area (ft<sup>2</sup>): 5,600

**Average Rate**

Time Period	ITE Trip Rate	Trip Ends
Weekday	T = 11.65 (X)	65
AM Peak Hour	T = 1.80 (X)	10
PM Peak Hour	T = 1.74 (X)	10

Directional Split *		Directional Distribution	
IN	OUT	IN	OUT
50%	50%	33	32
90%	10%	9	1
15%	85%	2	8

\* Percentages rounded to nearest 5%

**Fitted Curve**

Time Period	ITE Trip Rate	Trip Ends
Weekday	$\ln(T) = 0.60 \ln(X) + 4.30$	207
AM Peak Hour	T = 1.67(X) + 21.93	31
PM Peak Hour	T = 1.52 (X) + 34.60	43

Directional Split *		Directional Distribution	
IN	OUT	IN	OUT
50%	50%	104	103
90%	10%	28	3
15%	85%	6	37

\* Percentages rounded to nearest 5%

**AVERAGE**

Time Period	Trip Ends	Directional Split *		Directional Distribution	
		IN	OUT	IN	OUT
Weekday	136	50%	50%	68	68
AM Peak Adjacent Street	21	90%	10%	18	2
PM Peak Adjacent Street	27	15%	85%	4	23

JN: 2970  
 Project Description: Bangor Savings  
 Project Location: Marginal Way Portland  
 Date: 5/4/2015

Gorrill-Palmer Consulting Engineers, Inc.  
 P.O. Box 1237  
 15 Shaker Road  
 Gray, Maine 04039

**Drive-in Bank-9th Edition  
 Land Use Code (LUC) 912**

Gross Floor Area (ft<sup>2</sup>): 2,108

Time Period	ITE Trip Rate	Trip Ends	Directional Split *		Directional Distribution		R^2
			IN	OUT	IN	OUT	
Weekday	T = 148.15 (X)	312	50%	50%	156	156	0.59
AM Peak Adjacent Street	T = 12.08 (X)	25	55%	45%	14	11	---
PM Peak Adjacent Street	T = 24.30 (X)	51	50%	50%	26	25	---
AM Peak Hour of Generator	T = 17.57 (X)	37	50%	50%	19	18	0.51
PM Peak Hour of Generator	T = 26.69 (X)	56	50%	50%	28	28	---
Saturday	T = 86.32 (X)	182	50%	50%	91	91	0.52
Saturday Peak Hour of Gen.	T = 26.31 (X)	55	50%	50%	28	27	---

\* Percentages rounded to nearest 5%

Number of Drive-in Lanes: 2

Time Period	ITE Trip Rate	Trip Ends	Directional Split *		Directional Distribution		R^2
			IN	OUT	IN	OUT	
Weekday	T = 139.25 (X)	279	50%	50%	140	139	0.52
AM Peak Adjacent Street	T = 9.29(X)	19	60%	40%	11	8	---
PM Peak Adjacent Street	T = 33.24 (X)	66	50%	50%	33	33	---
AM Peak Hour of Generator	T = 21.64 (X)	43	50%	50%	22	21	---
PM Peak Hour of Generator	T = 29.05 (X)	58	50%	50%	29	29	0.55
Saturday	Not Given	0	50%	50%	0	0	---
Saturday Peak Hour of Gen.	T = 28.78 (X)	58	50%	50%	29	29	---

\* Percentages rounded to nearest 5%

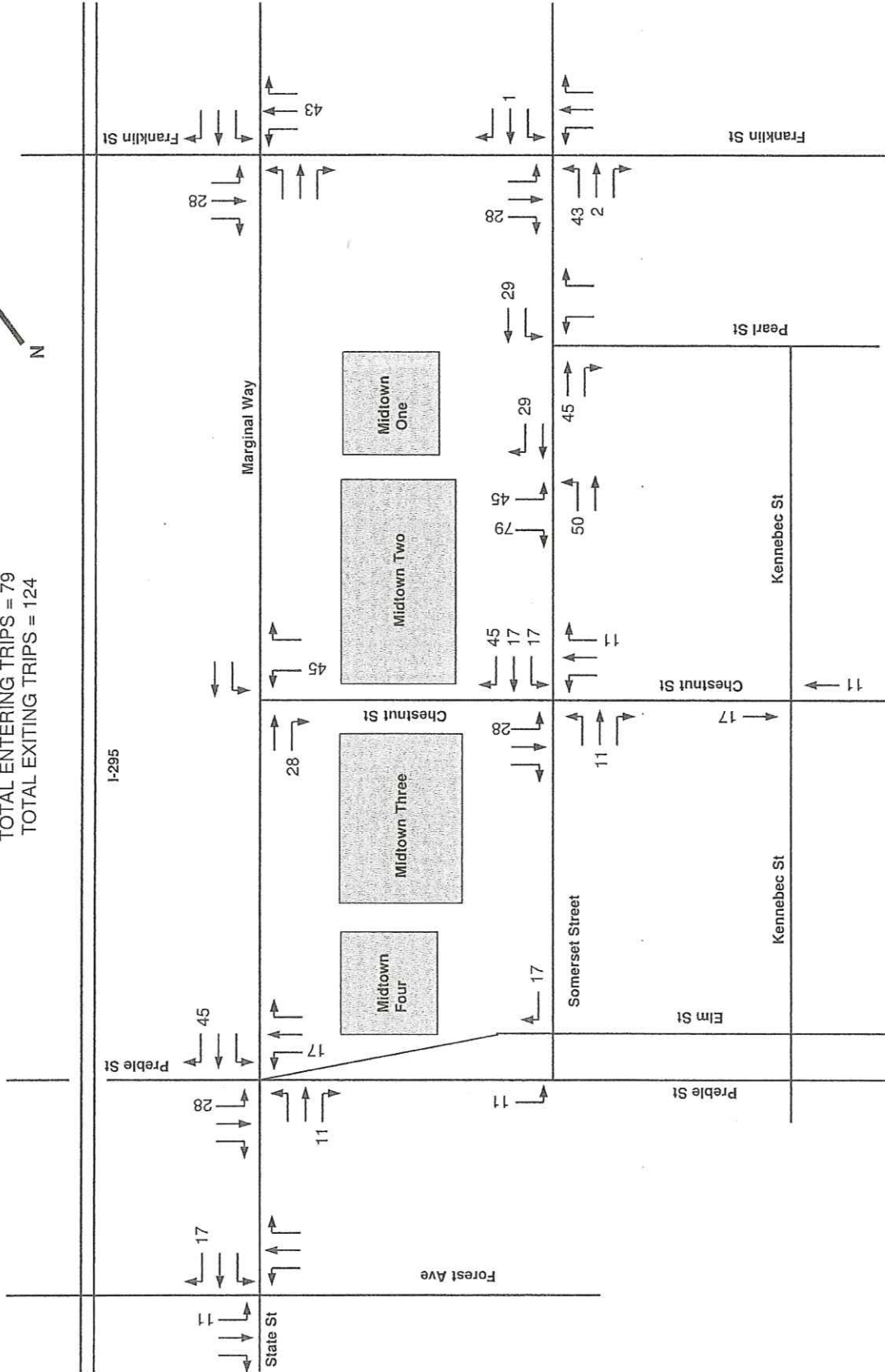
**AVERAGE**

Time Period	Trip Ends	Directional Split *		Directional Distribution	
		IN	OUT	IN	OUT
Weekday	296	50%	50%	148	148
AM Peak Adjacent Street	22	58%	42%	13	9
PM Peak Adjacent Street	59	50%	50%	29	30
AM Peak Hour of Generator	40	50%	50%	20	20
PM Peak Hour of Generator	57	50%	50%	29	28
Saturday Peak Hour of Gen.	57	50%	50%	28	29



### Site Generated Trip Ends

AM Peak Hour  
 TOTAL ENTERING TRIPS = 79  
 TOTAL EXITING TRIPS = 124



**Figure 2**  
 Not to Scale



### Site Generated Trip Ends

PM Peak Hour  
 TOTAL ENTERING TRIPS = 184  
 TOTAL EXITING TRIPS = 165

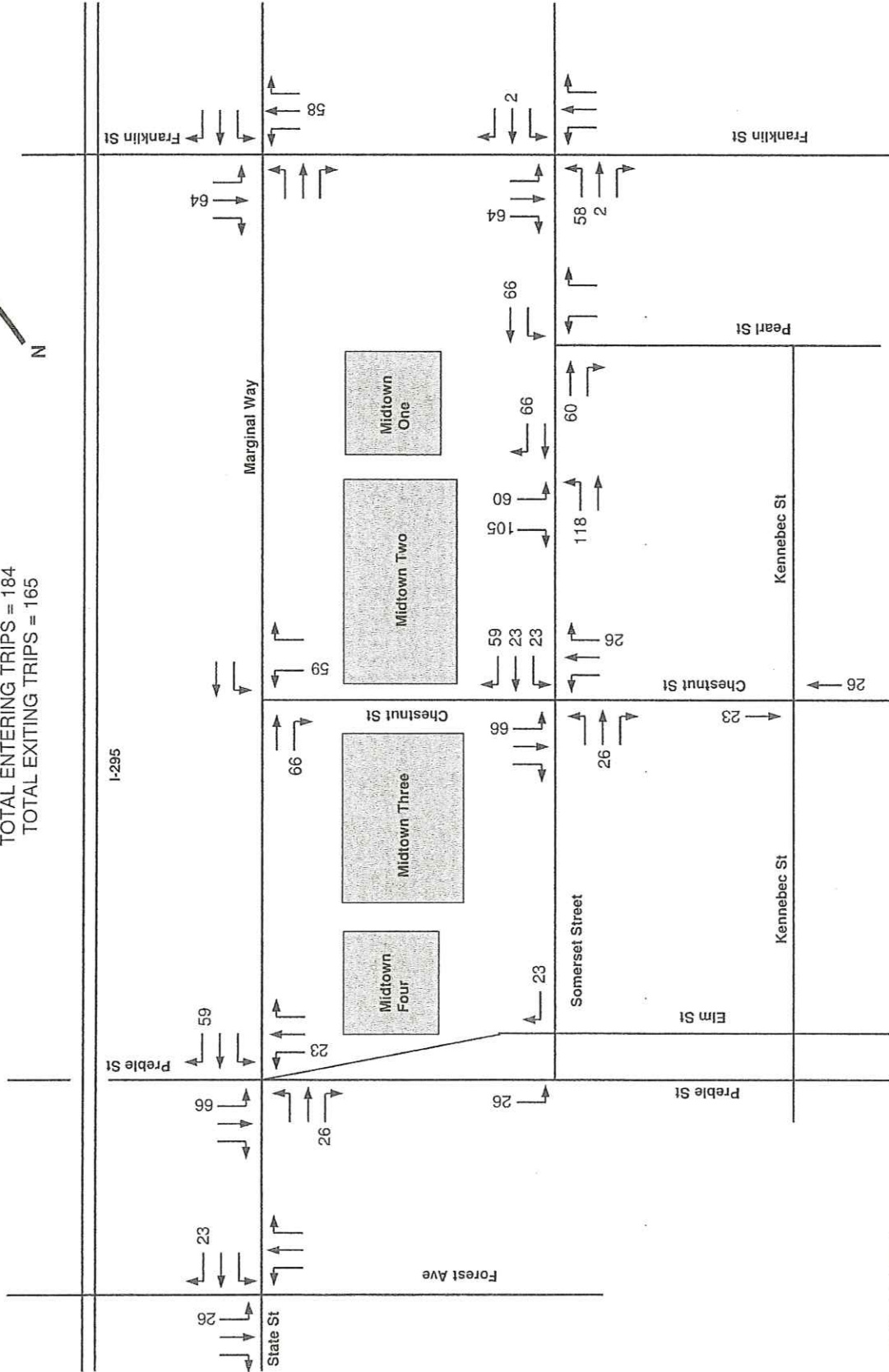
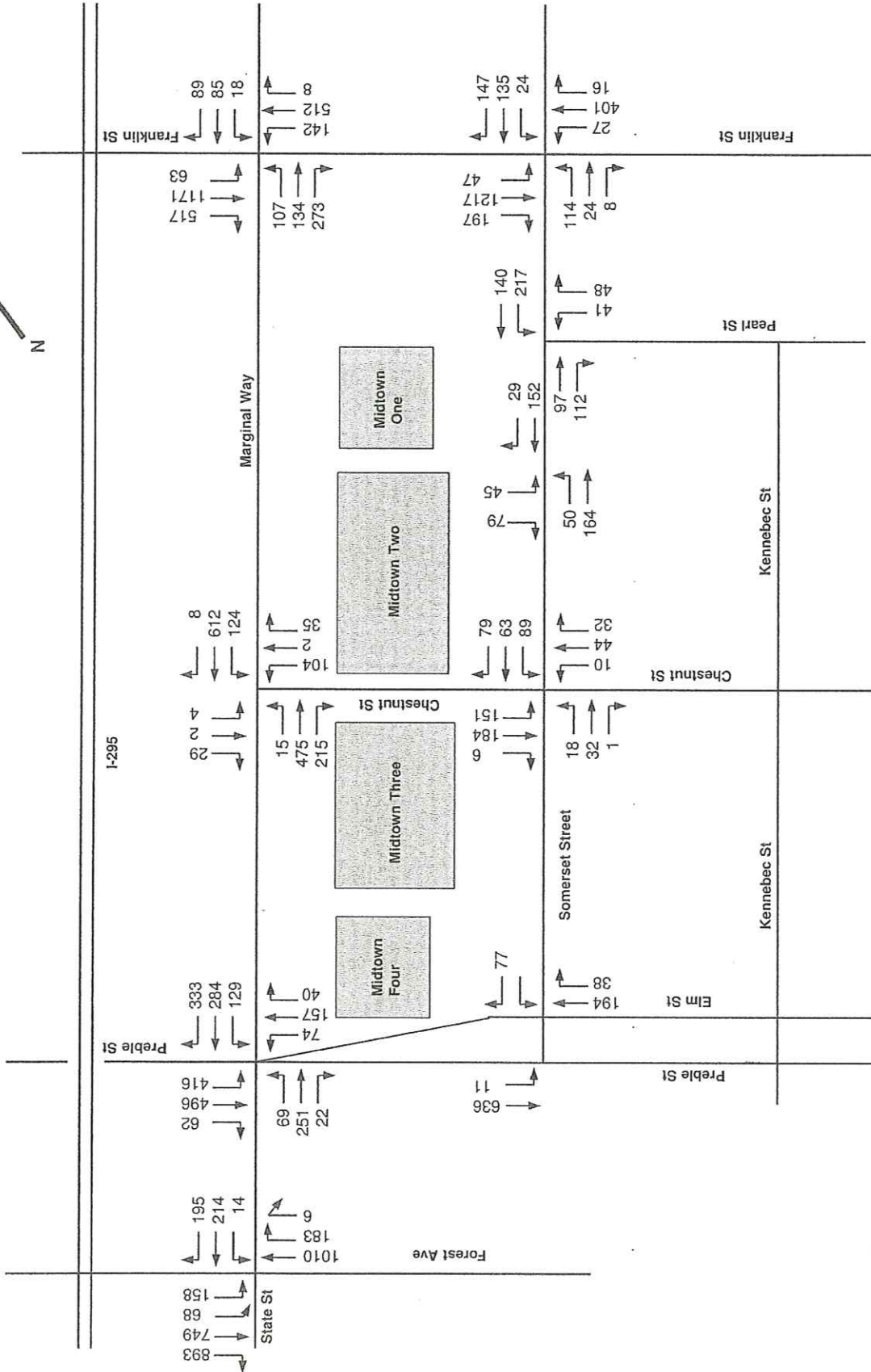


Figure 3  
 Not to Scale



# Post-Development Turning Movement Counts AM Peak Hour

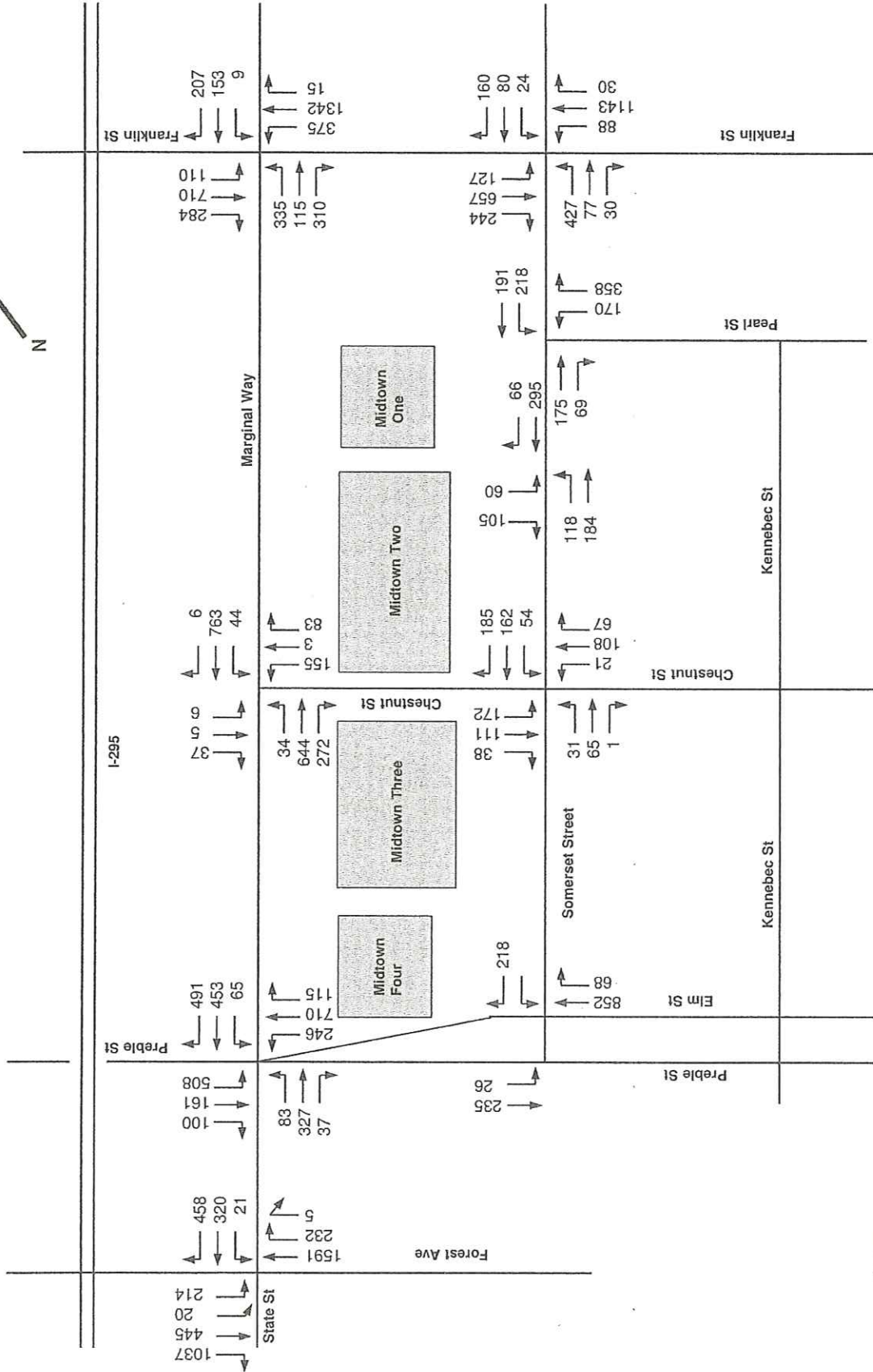


I-295



**Figure 4**  
Not to Scale

# Post-Development Turning Movement Counts PM Peak Hour



**Figure 5**  
Not to Scale



# SEBAGO TECHNICS

CIVIL ENGINEERING • SURVEYING • LANDSCAPE ARCHITECTURE

## Memorandum

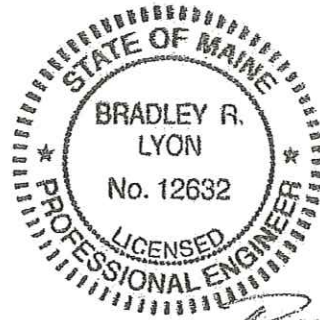
To: William DeSena

From: Bradley R. Lyon, P.E., PTOE,  
Senior Transportation Engineer

Job #: 14313

Date: August 29, 2014

Subject: Trip Generation Calculations for  
20 Marginal Way, Portland, Maine



The purpose of this memorandum is to calculate what the peak hour trip generation is on 20 Marginal Way in Portland, Maine for the existing 2,500 sf "Northern Pride" automated car wash and compare it to the following three proposed land uses:

- 2,500 sf Dunkin Donuts w/Drive Thru
- 2,500 sf Generic Coffee Shop w/Drive Thru
- 2,500 sf Drive-In Bank w/1 Drive Thru Lane

### Existing 2,500 sf "Northern Pride" Automated Car Wash

The latest edition, 8<sup>th</sup>, of the Institute of Transportation Engineers (ITE) Trip Generation Manual was referenced using Land Use Code 948, Automated Car Wash in an attempt to estimate peak hour traffic. Analysis of the land use found that only 2 observations were available, therefore existing sales data was requested. Existing hourly sales data was provided to us for weekdays in January, April and December of 2013 and a Sunday in April of 2013. The results, as well as the calculated weekday average are as follows:

**Table 1**  
**"Northern Pride Auto Wash" Sales Data**

Time Period	Wed., Dec. 11, 2013	Tues., Jan. 8, 2013	Thurs., Jan. 10, 2013	Tues., April 20, 2013	Sat., Feb. 23, 2013	Sun., April 7, 2013	Weekday Average
07:30 AM – 08:00 AM	18	22	20	20	24	19	20
08:00 AM – 09:00 AM	49	36	36	57	43	43	45
09:00 AM – 10:00 AM	71	37	46	56	54	42	<b>53</b>
10:00 AM – 11:00 AM	52	46	51	55	43	46	51
11:00 AM – 12:00 PM	48	51	57	51	42	52	52
12:00 PM – 1:00 PM	69	57	59	51	<b>58</b>	56	<b>59</b>
1:00 PM – 2:00 PM	60	51	54	46	43	56	53
2:00 PM – 3:00 PM	45	55	58	43	52	43	50
3:00 PM – 4:00 PM	61	58	64	44	47	53	57
4:00 PM – 5:00 PM	49	52	54	52	54	<b>60</b>	52
5:00 PM – 6:00 PM	34	60	45	44	47	45	46

Given this data, it was determined that the existing weekday AM Peak Hour was from 9:00 AM to 10:00 AM, generating **106 trips** (53 sales \* 2 (entering and exiting vehicles)) and the existing weekday PM Peak Hour was from 12:00 PM to 1:00 PM, generating **118 trips** (weekday average of 59 sales \* 2 (entering and exiting vehicles)). The Saturday Peak Hour occurred from 12:00 PM to 1:00 PM on February 23, 2013 with **116 trips** (58 sales \* 2 (entering and exiting vehicles)). The Sunday Peak Hour occurred from 4:00 PM to 5:00 PM on April 7<sup>th</sup>, 2013 with **120 trips** (60 sales \* 2 (entering and exiting vehicles)). A summary of this can be found in Table 2 below:

**Table 2**  
**Proposed Trip Generation based on Sales Data**  
**“Northern Pride Auto Wash”**

	<b>Total Trips</b>
Weekday AM Peak Hour of Generator	<b>106</b>
Weekday PM Peak Hour of Generator	<b>118</b>
Saturday Peak Hour of Generator	<b>116</b>
Sunday Peak Hour of Generator	<b>120</b>

**Proposed 2,500 sf Dunkin Donuts w/Drive Thru**

Dunkin Donuts stores are unique in their trip generating characteristics and as such a special study was conducted in 2005 by Gorrill - Palmer Engineers (G-P) to better define these relationships, since MaineDOT determined that standard ITE data did not seem to provide reasonable estimates. The G-P Study outlined three means of more accurately forecasting trip generation by these facilities based on their location and the traffic volumes in the vicinity of the sites. We used these methodologies to arrive at the following results, which have been averaged to determine the AM Peak Hour of the generator. It should be noted that the Annual Average Daily Traffic in front of the site was recorded by MaineDOT in 2010 to be 8,050 vehicles per day. In addition, the AM peak hour is generally considered to be 8% of the average daily traffic, which in this case would be 644 vehicles.

Trip Gen by Store Size	=	$0.0536 * (2,500 \text{ S.F.}) + 142.75$	=	276.75 trips
Trip Gen by AADT	=	$0.0081 * (8,050 \text{ AADT}) + 139.36$	=	204.57 trips
Trip Gen by AM Peak Hr	=	$0.1061 * (644 \text{ vehicles}) + 144.49$	=	212.82 trips
Average			=	<b>231 trips</b>

The above figure will place this Project in the “over 200” Traffic Movement Permit category. However, Dunkin Donuts stores have only a 15% Primary (or new) Trip production rate, (i.e. most patrons are already on the roadway network and stop in on their way by). Very few are new trips to the roadway. In this case the 231 Dunkin Donuts trips will only represent about 35 new trips or approximately 18 new trips in and 17 new trips out of the site. Given the presence of an existing shared left turn lane on Marginal Way, offsite improvements should be minimal but the extent of improvements wouldn’t be able to be determined until a “Scoping Meeting” is held with MaineDOT. The application fees to the state for permit of this level are \$2,000 plus our engineering costs to produce the application and perform a formal traffic study of the area.

**Proposed 2,500 sf Generic Coffee Shop w/Drive Thru**

Similar to the trip generation estimate for the existing "Northern Pride" automated car wash, the proposed 2,500 sf Generic Coffee Shop w/Drive Thru was estimated using the 8<sup>th</sup> Edition of the ITE Trip Generation Manual. Land Use Code 940 Bread/Donut/Bagel Shop with Drive-Through Window was used. Data was only available for the peak hour of adjacent street traffic, one hour between 7 and 9 AM and one hour between 4 and 6 PM. The results are as follows:

**Table 3**  
**Proposed Trip Generation by Square Feet**  
**Land Use Code 940, Bread/Donut/Bagel Shop with Drive-Through Window**

By Square Feet	Square Feet	Rate (Trips / 1,000 sf)	Total Trips
Weekday Peak Hour of Adjacent Street Traffic, One Hour Between 7 and 9 AM	2,500	36.92	92
Weekday Peak Hour of Adjacent Street Traffic, One Hour Between 4 and 6 PM	2,500	19.56	49

Given this information a Traffic Movement Permit would not be required from the MaineDOT due to the fact that the development would generate less than 100 peak hour trips.



**Proposed 2,500 sf Drive-In Bank w/1 Drive Thru Lane**

Similar to the trip generation estimate for the existing "Northern Pride" automated car wash and the Generic Coffee Shop, the proposed 2,500 sf Drive-In Bank w/1 Drive Thru Lane was estimated using the 8<sup>th</sup> Edition of the ITE Trip Generation Manual. Land Use Code 912 Drive-in Bank was used by using rates per 1,000 square feet of gross floor area averaged with rates by drive-in lanes. The results are as follows:

**Table 4**  
**Proposed Trip Generation by Square Feet**  
**Land Use Code 912 Drive-in Bank**

By Square Feet	Square Feet	Rate (Trips / 1,000 sf)	Total Trips
Weekday AM Peak Hour of Generator	2,500	17.31	43
Weekday PM Peak Hour of Generator	2,500	26.69	67
Saturday Peak Hour of Generator	2,500	26.53	66
Sunday Peak Hour of Generator	2,500	4.78	12

**Table 5**  
**Proposed Trip Generation by Drive-In Lane**  
**Land Use Code 912 Drive-in Bank**

By Drive-In Lane	Drive-In Lanes	Rate (Trips / Drive-In Lane)	Total Trips
Weekday AM Peak Hour of Generator	1	21.64	<b>22</b>
Weekday PM Peak Hour of Generator	1	29.05	<b>29</b>
Saturday Peak Hour of Generator	1	29.88	<b>30</b>
Sunday Peak Hour of Generator	1	N/A	<b>N/A</b>

**Table 6**  
**Proposed Trip Generation Total Average**  
**Land Use Code 912 Drive-in Bank**

By Drive-In Lane	Trips by Square Foot	Trips by Drive-In Lane	Average Trips
Weekday AM Peak Hour of Generator	43	22	<b>33</b>
Weekday PM Peak Hour of Generator	67	29	<b>48</b>
Saturday Peak Hour of Generator	66	30	<b>48</b>
Sunday Peak Hour of Generator	12	N/A	<b>12</b>

Given this information a Traffic Movement Permit would not be required from the MaineDOT due to the fact that the development would generate less than 100 peak hour trips.

**Conclusion**

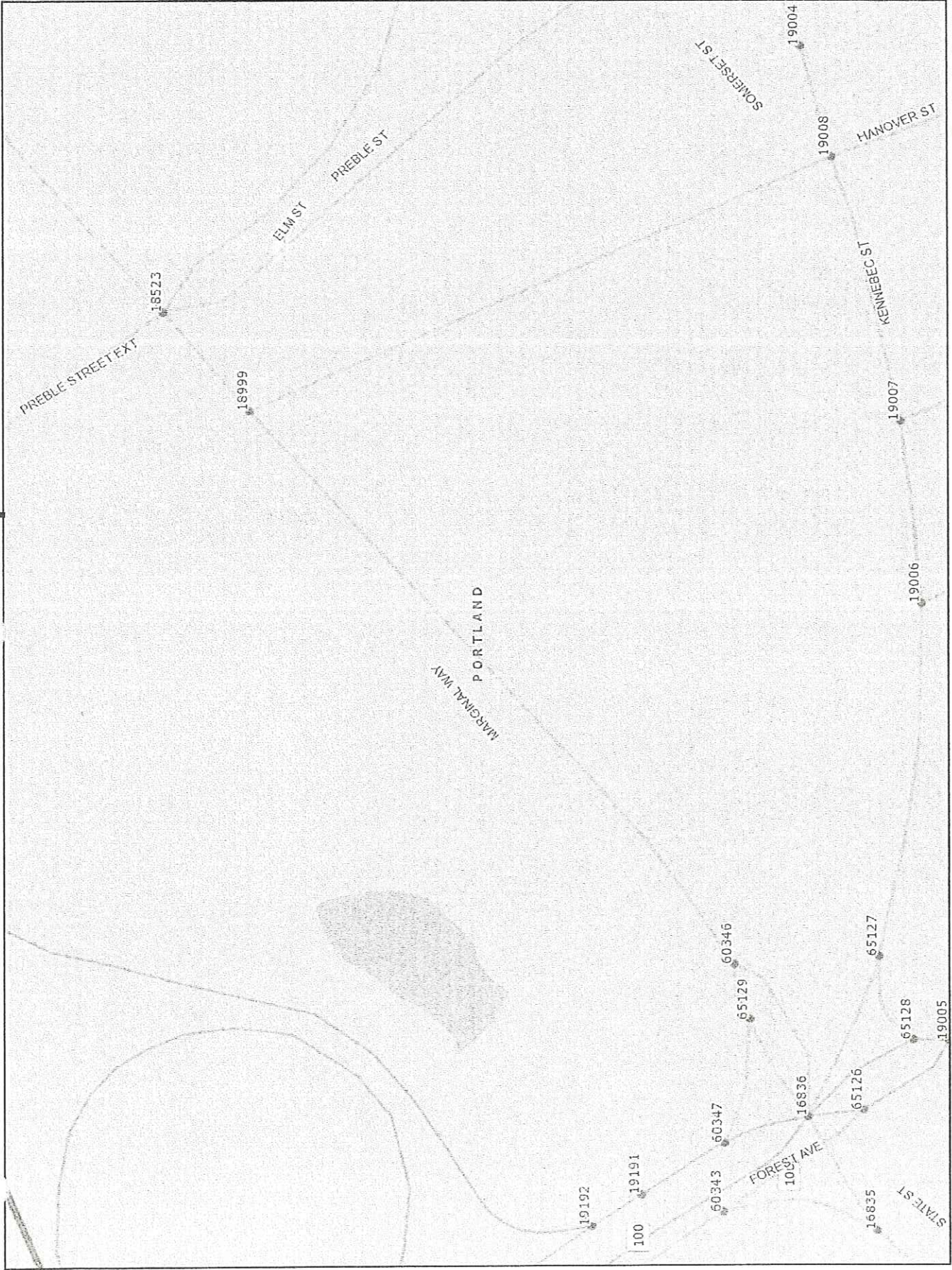
In conclusion, the trip generation for each of the uses is as follows:

**Table 7**  
**Overall Trip Generation Comparison**

	Existing 2,500 sf Automated Car Wash	2,500 sf Dunkin Donuts	2,500 sf Generic Coffee Shop	2,500 sf Drive- In Bank
AM Peak Hour of Generator	106	231	92	33
PM Peak Hour of Generator	118	N/A	49	48
Saturday Peak Hour of Generator	116	N/A	N/A	48
Sunday Peak Hour of Generator	120	N/A	N/A	12
Traffic Movement Permit Required?	N/A	Yes (200+ Trips)	No	No

Given the above information, it is our opinion that a Traffic Movement Permit would only be required for the Dunkin Donuts which would generate 231 AM Peak Hour trips, putting it into the 200+ Traffic Movement Permit category. The remaining two uses all generate below 100 peak hour trips and therefore would not require a Traffic Movement Permit.

# Maine DOT Map



Map Generated on Tuesday, May 05, 2015 10:18:32 AM

The Maine Department of Transportation provides this information only. Reliance upon this information is at user risk. It is subject to revision and may be incomplete depending upon changing conditions. The Department assumes no liability for injuries or damages result from this information. This map is not intended to support emergency dispatch. Road names used on this map may not match official road names.

Map Scale 1:1489

Maine Department Of Transportation - Traffic Engineering, Crash Records Section  
**Crash Summary Report**

**Report Selections and Input Parameters**

REPORT SELECTIONS

Crash Summary I       Section Detail       Crash Summary II       1320 Public       1320 Private       1320 Summary

REPORT DESCRIPTION

Marginal

REPORT PARAMETERS

Year 2012, Start Month 1 through Year 2014 End Month: 12

Route: 0560477

Start Node: 16836  
 End Node: 18523

Start Offset: 0  
 End Offset: 0

Exclude First Node  
 Exclude Last Node

Route: 3201880

Start Node: 60346  
 End Node: 16836

Start Offset: 0  
 End Offset: 0

Exclude First Node  
 Exclude Last Node

Maine Department Of Transportation - Traffic Engineering, Crash Records Section

Crash Summary I

Nodes														
Node	Route - MP	Node Description	U/R	Total Crashes	K	A	B	C	PD	Injury	Percent Annual M Ent-Veh	Crash Rate	Critical Rate	CRF
P16836	0560477 - 0	Int of FOREST AV KENNEBEC ST MARGINAL WY STATE	9	54	0	0	3	11	40	25.9	12.807	1.41	0.98	1.43
A60346	0560477 - 0.03	Non Int MARGINAL WY	2	0	0	0	0	0	0	0.0	0.000	0.00	0.00	0.00
18999	0560477 - 0.16	Int of HANOVER ST MARGINAL WY	2	5	0	0	0	3	2	60.0	4.102	0.41	0.37	1.09
18523	0560477 - 0.18	Int of ELM ST, MARGINAL WY, PREBLE ST, EX	9	25	0	0	1	6	17	29.2	9.923	0.84	1.03	0.00
A65129	3201880 - 0.01	Int of CUT MARGINAL WY	2	0	0	0	0	0	0	0.0	0.000	0.00	0.00	0.00
<b>Study Years: 3.00</b>														
<b>NODE TOTALS:</b>				84	0	0	4	20	59	28.6	26.832	1.04	0.79	1.32

# Crash Summary I

Sections																	
Start Node	End Node	Element	Offset Begin - End	Route - MP	Section U/R Length	Total Crashes	K	A	B	C	PD	Percent Injury	Annual HMVM	Crash Rate	Critical Rate	CRF	
16836	60346	3115192	0 - 0.03	0560477 - 0 RD INV 05 60477	0.03	2	0	0	0	0	0	0.0	0.00016	0.00	770.05	0.00	
Int of FOREST AV, KENNEBEC ST, MARGINAL WY, STATE ST EXT																	
60346	18999	3115193	0 - 0.13	0560477 - 0.03 RD INV 05 60477	0.13	2	2	0	0	1	1	50.0	0.00468	142.59	454.26	0.00	
Non Int MARGINAL WY																	
18523	18999	3106676	0 - 0.02	0560477 - 0.16 RD INV 05 60477	0.02	2	1	0	0	1	0	100.0	0.00071	472.50	725.73	0.00	
Int of ELM ST, MARGINAL WY, PREBLE ST, PREBLE ST EXT																	
60346	65129	3123766	0 - 0.01	3201880 - 0 RD INV 3201880	0.01	2	0	0	0	0	0	0.0	0.00031	0.00	817.46	0.00	
Non Int MARGINAL WY																	
65129	16836	3139747	0 - 0.02	3201880 - 0.01 RD INV 3201880	0.02	2	0	0	0	0	0	0.0	0.00028	0.00	819.99	0.00	
Int of CUT, MARGINAL WY																	
<b>Study Years:</b>				3.00	<b>Section Totals:</b>	0.21	3	0	0	2	1	66.7	0.00613	163.16	424.75	0.38	
					<b>Grand Totals:</b>	0.21	87	0	0	4	22	60	29.9	0.00613	4731.57	588.58	8.04

# Crash Summary

## Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	K	A	B	C	PD	Crash Report	Crash Date	Crash Mile Point	Injury Degree
16836	60346	3115192	0 - 0.03	0560477 - 0	0	0	0	0	0	0				
60346	18999	3115193	0 - 0.13	0560477 - 0.03	2	0	0	0	1	1	2012-26376	04/11/2012	0.06	PD
18523	18999	3106676	0 - 0.02	0560477 - 0.16	1	0	0	0	1	0	2012-29895	06/07/2012	0.13	C
60346	65129	3123766	0 - 0.01	3201880 - 0	0	0	0	0	0	0	2014-32637	11/25/2014	0.17	C
65129	16836	3139747	0 - 0.02	3201880 - 0.01	0	0	0	0	0	0				
<b>Totals:</b>					<b>3</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>1</b>				



Maine Department Of Transportation - Tra. Engineering, Crash Records Section  
**Crash Summary II - Characteristics**

**Crashes by Day and Hour**

Day Of Week	Hour of Day												Un	Tot												
	AM						PM																			
	12	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6	7	8	9	10	11		
SUNDAY	1	0	0	0	0	0	0	0	2	1	1	1	0	0	0	0	1	0	0	0	0	1	0	0	0	8
MONDAY	0	0	0	0	0	0	0	0	1	1	0	1	1	2	1	1	0	1	1	0	1	0	0	0	0	11
TUESDAY	0	0	0	0	0	0	0	1	2	1	1	0	1	2	1	2	4	2	1	2	0	0	0	0	0	20
WEDNESDAY	0	0	0	0	0	0	0	1	2	1	0	2	2	0	1	0	2	2	3	2	1	0	0	0	0	19
THURSDAY	0	1	0	0	0	0	0	1	0	0	1	2	2	2	2	2	2	2	2	0	0	0	0	1	0	18
FRIDAY	0	0	0	0	0	0	0	0	1	1	0	0	0	1	1	1	1	0	1	1	0	0	0	0	0	8
SATURDAY	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	1	0	0	0	3
<b>Totals</b>	<b>1</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>8</b>	<b>5</b>	<b>3</b>	<b>5</b>	<b>7</b>	<b>7</b>	<b>6</b>	<b>6</b>	<b>10</b>	<b>7</b>	<b>8</b>	<b>5</b>	<b>2</b>	<b>2</b>	<b>0</b>	<b>1</b>	<b>87</b>	

**Vehicle Counts by Type**

Unit Type	Total	Unit Type	Total
1-Passenger Car	115	23-Bicyclist	2
2-(Sport) Utility Vehicle	28	24-W/fitness	7
3-Passenger Van	4	25-Other	3
4-Cargo Van (10K lbs or Less)	2	<b>Total</b>	<b>183</b>
5-Pickup	12		
6-Motor Home	0		
7-School Bus	0		
8-Transit Bus	2		
9-Motor Coach	1		
10-Other Bus	0		
11-Motorcycle	1		
12-Moped	0		
13-Low Speed Vehicle	0		
14-Autocycle	0		
15-Experimental	0		
16-Other Light Trucks (10,000 lbs or Less)	1		
17-Medium/Heavy Trucks (More than 10,000 lbs)	2		
18-ATV - (4 wheel)	0		
20-ATV - (2 wheel)	0		
21-Snowmobile	0		
22-Pedestrian	3		

Maine Department Of Transportation - Traffic Engineering, Crash Records Section  
**Crash Summary II - Characteristics**

**Crashes by Driver Action at Time of Crash**

Driver Action at Time of Crash	Dr 1	Dr 2	Dr 3	Dr 4	Dr 5	Other	Total
No Contributing Action	56	31	1	0	0	0	88
Ran Off Roadway	0	0	0	0	0	0	0
Failed to Yield Right-of-Way	10	6	0	0	0	0	16
Ran Red Light	1	3	0	0	0	0	4
Ran Stop Sign	0	0	0	0	0	0	0
Disregarded Other Traffic Sign	1	1	0	0	0	0	2
Disregarded Other Road Markings	2	1	0	0	0	0	3
Exceeded Posted Speed Limit	0	0	0	0	0	0	0
Drove Too Fast For Conditions	1	3	0	0	0	0	4
Improper Turn	2	4	0	0	0	0	6
Improper Backing	1	0	0	0	0	0	1
Improper Passing	0	0	0	0	0	0	0
Wrong Way	0	1	0	0	0	0	1
Followed Too Closely	5	22	2	0	0	0	29
Failed to Keep in Proper Lane	2	2	0	0	0	0	4
Operated Motor Vehicle in Erratic, Reckless, Careless, Negligent or Aggressive Manner	0	0	0	0	0	0	0
Swerved or Avoided Due to Wind, Slippery Surface, Motor Vehicle, Object, Non-Motorist in Roadway	0	0	0	0	0	0	0
Over-Correcting/Over-Steering	0	0	0	0	0	0	0
Other Contributing Action	2	7	0	0	0	0	9
Unknown	1	0	0	0	0	0	1
<b>Total</b>	<b>84</b>	<b>81</b>	<b>3</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>168</b>

**Crashes by Apparent Physical Condition And Driver**

Apparent Physical Condition	Dr 1	Dr 2	Dr 3	Dr 4	Dr 5	Other	Total
Apparently Normal	84	78	3	0	0	5	170
Physically Impaired or Handicapped	0	0	0	0	0	0	0
Emotional/Depressed, Angry, Disturbed, etc.)	0	0	0	0	0	0	0
Ill (Sick)	0	2	0	0	0	0	2
Asleep or Fatigued	0	0	0	0	0	0	0
Under the Influence of Medications/Drugs/Alcohol	0	0	0	0	0	0	0
Other	0	1	0	0	0	0	1
<b>Total</b>	<b>84</b>	<b>81</b>	<b>3</b>	<b>0</b>	<b>0</b>	<b>5</b>	<b>173</b>

**Driver Age by Unit Type**

Age	Driver	Bicycle	Snow/Mobile	Pedestrian	ATV	Total
09-Under	1	0	0	0	0	1
10-14	0	0	0	0	0	0
15-19	8	0	0	0	0	8
20-24	16	0	0	0	0	16
25-29	28	0	0	0	0	28
30-39	37	0	0	0	0	37
40-49	23	0	0	0	0	23
50-59	27	0	0	0	0	27
60-69	19	0	0	0	0	19
70-79	7	0	0	0	0	7
80-Over	2	0	0	0	0	2
Unknown	3	2	0	3	0	8
<b>Total</b>	<b>171</b>	<b>2</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>176</b>

Maine Department of Transportation - Traffic Engineering, Crash Records Section  
**Crash Summary II - Characteristics**

Most Harmful Event		Total
1-Overturn / Rollover	0	0
2-Fire / Explosion	0	12
3-Immersion	0	0
4-Jackknife	0	0
5-Cargo / Equipment Loss Or Shift	0	169
6-Fell / Jumped from Motor Vehicle	0	
7-Thrown or Falling Object	0	
8-Other Non-Collision	0	
9-Pedestrian	0	
10-Pedalcycle	1	
11-Railway Vehicle - Train, Engine	0	
12-Animal	0	
13-Motor Vehicle in Transport	156	
14-Parked Motor Vehicle	0	
15-Struck by Falling, Shifting Cargo or Anything Set in Motion by Motor Vehicle	0	
16-Work Zone / Maintenance Equipment	0	
17-Other Non-Fixed Object	0	
18-Impact Attenuator / Crash Cushion	0	
19-Bridge Overhead Structure	0	
20-Bridge Pier or Support	0	
21-Bridge Rail	0	
22-Cable Barrier	0	
23-Culvert	0	
24-Curb	0	
25-Ditch	0	
26-Embankment	0	
27-Guardrail Face	0	
28-Guardrail End	0	
29-Concrete Traffic Barrier	0	
30-Other Traffic Barrier	0	
31-Tree (Standing)	0	
32-Utility Pole / Light Support	0	
33-Traffic Sign Support	0	
34-Traffic Signal Support	0	
35-Fence	0	
36-Mailbox	0	
37-Other Post Pole or Support	0	

Injury Data		
Severity Code	Injury Crashes	Number Of Injuries
K	0	0
A	0	0
B	4	4
C	22	23
PD	60	0
<b>Total</b>	<b>86</b>	<b>27</b>

Road Character		Total
Road Grade		
1-Level		87
2-On Grade		0
3-Top of Hill		0
4-Bottom of Hill		0
5-Other		0
<b>Total</b>		<b>87</b>

Traffic Control Devices		Total
1-Traffic Signals (Stop & Go)	70	
2-Traffic Signals (Flashing)	1	
3-Advisory/Warning Sign	0	
4-Stop Signs - All Approaches	0	
5-Stop Signs - Other	1	
6-Yield Sign	11	
7-Curve Warning Sign	0	
8-Officer, Flagman, School Patrol	0	
9-School Bus Stop Arm	0	
10-School Zone Sign	0	
11-R.R. Crossing Device	0	
12-No Passing Zone	0	
13-None	3	
14-Other	1	
<b>Total</b>		<b>87</b>

Light Condition		Total
1-Daylight	62	
2-Dawn	0	
3-Dusk	6	
4-Dark - Lighted	19	
5-Dark - Not Lighted	0	
6-Dark - Unknown Lighting	0	
7-Unknown	0	
<b>Total</b>		<b>87</b>

Maine Department Of Transportation - Traffic Engineering, Crash Records Section  
**Crash Summary II - Characteristics**

Crashes by Year and Month

Month	2012	2013	2014	Total
JANUARY	0	1	1	2
FEBRUARY	7	5	4	16
MARCH	3	0	4	7
APRIL	3	1	4	8
MAY	3	2	4	9
JUNE	2	2	1	5
JULY	0	1	3	4
AUGUST	4	1	4	9
SEPTEMBER	1	2	4	7
OCTOBER	1	1	1	3
NOVEMBER	3	4	3	10
DECEMBER	3	1	3	7
<b>Total</b>	<b>30</b>	<b>21</b>	<b>36</b>	<b>87</b>

Report is limited to the last 10 years of data.

# Crash Summary II - Characteristics

## Crashes by Crash Type and Type of Location

Crash Type	Straight Road	Curved Road	Three Leg Intersection	Four Leg Intersection	Five or More Leg Intersection	Driveways	Bridges	Interchanges	Other	Parking Lot	Private Way	Cross Over	Railroad Crossing	Total
Object in Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rear End / Sideswipe	1	0	23	19	25	1	0	0	0	0	0	0	0	69
Head-on / Sideswipe	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Intersection Movement	0	0	1	2	9	0	0	0	0	0	0	0	0	12
Pedestrians	0	0	0	1	0	1	0	0	0	0	0	0	0	2
Train	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Went Off Road	0	0	0	1	1	0	0	0	0	0	0	0	0	2
All Other Animal	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bicycle	0	0	0	2	0	0	0	0	0	0	0	0	0	2
Other	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Jackknife	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rollover	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Fire	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Submersion	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Thrown or Falling Object	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bear	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Deer	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Moose	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Turkey	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>Total</b>	<b>1</b>	<b>0</b>	<b>24</b>	<b>25</b>	<b>35</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>87</b>

Maine Department of Transportation - Traffic Engineering, Crash Records Section  
**Crash Summary II - Characteristics**

**Crashes by Weather, Light Condition and Road Surface**

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
<b>Blowing Sand, Soil, Dirt</b>												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
<b>Blowing Snow</b>												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
<b>Clear</b>												
Dark - Lighted	12	0	0	0	0	0	0	0	0	0	2	14
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	44	0	0	0	0	0	0	0	0	0	1	45
Dusk	2	0	0	0	0	0	0	0	0	0	0	2
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
<b>Cloudy</b>												
Dark - Lighted	1	0	0	0	0	0	0	0	0	0	1	2
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	8	0	0	0	0	0	0	0	0	0	3	11
Dusk	1	0	0	0	0	0	0	0	0	0	0	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

Maine Department of Transportation - Traffic Engineering, Crash Records Section  
**Crash Summary II - Characteristics**

**Crashes by Weather, Light Condition and Road Surface**

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
<b>Fog, Smog, Smoke</b>												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
<b>Other</b>												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
<b>Rain</b>												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	1	1
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	3	3
Dusk	0	0	0	0	0	0	0	0	0	0	3	3
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
<b>Severe Crosswinds</b>												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

Maine Department of Transportation - Traffic Engineering, Crash Records Section  
**Crash Summary II - Characteristics**

**Crashes by Weather, Light Condition and Road Surface**

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
<b>Sleet, Hail (Freezing Rain or Drizzle)</b>												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
<b>Snow</b>												
Dark - Lighted	0	0	0	0	0	0	0	2	0	0	0	2
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	3	0	0	0	3
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
<b>TOTAL</b>	<b>68</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>5</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>87</b>



**ATTACHMENT 4**  
**EROSION AND**  
**SEDIMENTATION CONTROL**

**EROSION AND SEDIMENTATION CONTROL**  
**BASIC STANDARDS**

**1.1 Overview**

This Exhibit demonstrates the developer has made adequate provision for controlling erosion and sedimentation.

**1.2 Introduction**

Gorrill Palmer has been retained by Bangor Savings Bank to prepare an Erosion and Sedimentation Control Report for a proposed bank/office building at 20 Marginal Way in Portland, Maine. The redevelopment of the site is anticipated to include a 2,100 sf footprint three story building and 24 space parking lot. Figure 1 is a map showing the project location. Gorrill Palmer has prepared an Erosion and Sedimentation Control Plan for the proposed development. This narrative contains the general erosion and sedimentation control measures, which are appropriate for the construction of the project.

**1.3 Narrative**

**1.3.1 Existing Conditions and Soil Types**

The site is approximately 17,862 sf in size and is currently developed with a car wash. Abutting land uses include:

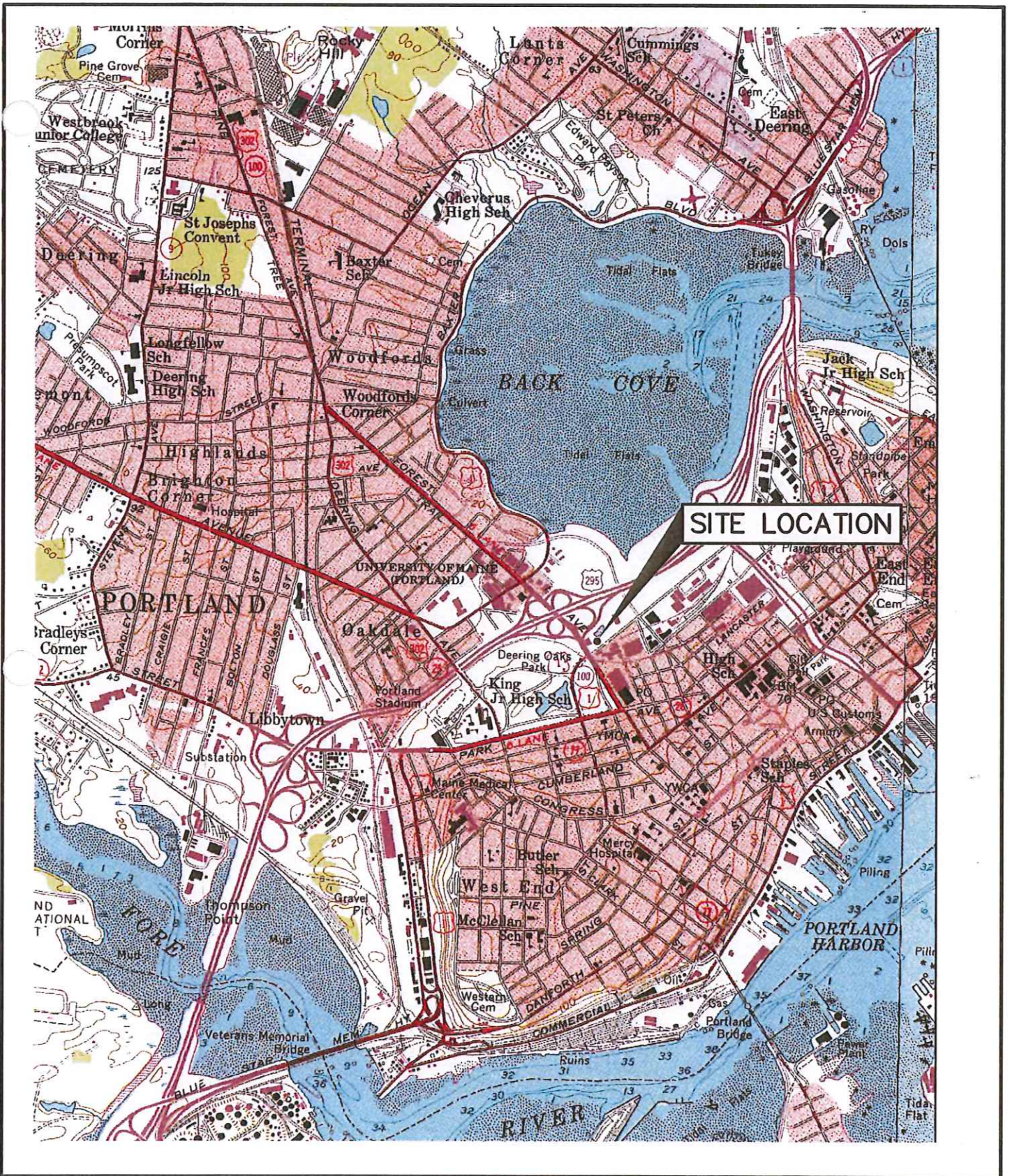
- North – I-295
- East - Commercial
- South - Commercial
- West — CMP station/Forest Ave

The front of the existing site slopes towards Marginal Way with a slope of approximately 1%. The rear of the site slopes toward the I-295 Right-of-Way with a slope of approximately 1.5 %.

The Medium Intensity Soil Survey for Cumberland County as prepared by the Natural Resources Conservation Service was utilized in identifying the on-site soils. The soil report for this vicinity follows this page. The susceptibility of soils to erosion is indicated on a relative "K" scale of values over a range of 0.02 to 0.69. The higher values are indicative of the more erodible soils. The following table lists the soils found on site and their K values:

K VALUE		
Type	Subsurface	Substratum
Cut and Fill	-	-

The existing soils do not have listed K values and will be considered susceptible to erosion.



**U.S.G.S. Location Map**  
 Bangor Savings Bank - Portland, Maine  
 U.S.G.S. Portland-East, Maine-7.5 Minute Series (Topographic)

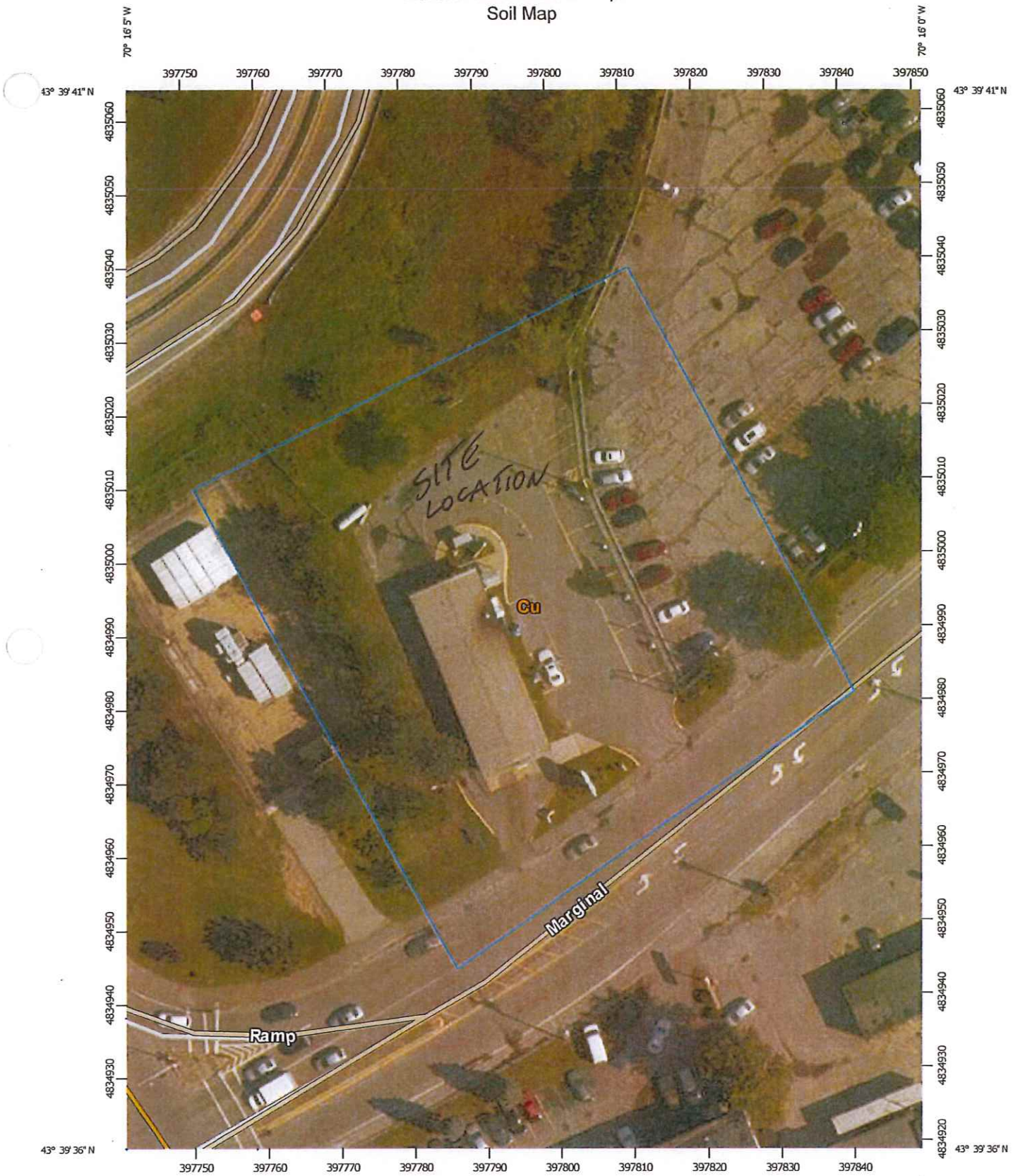
Design: JWA	Date: may 2015
Draft: CG	Job No.: 2970
Checked: AMP	Scale: None
File Name: 2970-LOCATION.dwg	



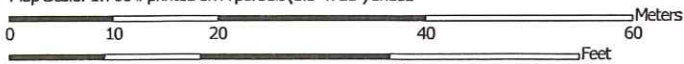
Relationships. Responsiveness. Results.  
[www.gorrillpalmer.com](http://www.gorrillpalmer.com)  
 207.657.6910

Figure  
**1**

Custom Soil Resource Report  
Soil Map



Map Scale: 1:700 if printed on A portrait (8.5" x 11") sheet.



Map projection: Web Mercator Corner coordinates: WGS84 Edge ties: UTM Zone 19N WGS84

## MAP LEGEND

- Area of Interest (AOI)
- Soils
- Soil Map Unit Polygons
- Soil Map Unit Lines
- Soil Map Unit Points
- Special Point Features**
  - Blowout
  - Borrow Pit
  - Clay Spot
  - Closed Depression
  - Gravel Pit
  - Gravelly Spot
  - Landfill
  - Lava Flow
  - Marsh or swamp
  - Mine or Quarry
  - Miscellaneous Water
  - Perennial Water
  - Rock Outcrop
  - Saline Spot
  - Sandy Spot
  - Severely Eroded Spot
  - Sinkhole
  - Slide or Slip
  - Sodic Spot
- Water Features**
  - Streams and Canals
- Transportation**
  - Rails
  - Interstate Highways
  - US Routes
  - Major Roads
  - Local Roads
- Background**
  - Aerial Photography
- Spoil Area
- Stony Spot
- Very Stony Spot
- Wet Spot
- Other
- Special Line Features

## MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:24,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service  
 Web Soil Survey URL: <http://websoilsurvey.nrcs.usda.gov>  
 Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Cumberland County and Part of Oxford County, Maine  
 Survey Area Data: Version 9, Sep 13, 2014

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Jul 31, 2013—Aug 11, 2013

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

## Map Unit Legend

Cumberland County and Part of Oxford County, Maine (ME005)			
Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
Cu	Cut and fill land	1.1	100.0%
Totals for Area of Interest		1.1	100.0%

## Map Unit Descriptions

The map units delineated on the detailed soil maps in a soil survey represent the soils or miscellaneous areas in the survey area. The map unit descriptions, along with the maps, can be used to determine the composition and properties of a unit.

A map unit delineation on a soil map represents an area dominated by one or more major kinds of soil or miscellaneous areas. A map unit is identified and named according to the taxonomic classification of the dominant soils. Within a taxonomic class there are precisely defined limits for the properties of the soils. On the landscape, however, the soils are natural phenomena, and they have the characteristic variability of all natural phenomena. Thus, the range of some observed properties may extend beyond the limits defined for a taxonomic class. Areas of soils of a single taxonomic class rarely, if ever, can be mapped without including areas of other taxonomic classes. Consequently, every map unit is made up of the soils or miscellaneous areas for which it is named and some minor components that belong to taxonomic classes other than those of the major soils.

Most minor soils have properties similar to those of the dominant soil or soils in the map unit, and thus they do not affect use and management. These are called noncontrasting, or similar, components. They may or may not be mentioned in a particular map unit description. Other minor components, however, have properties and behavioral characteristics divergent enough to affect use or to require different management. These are called contrasting, or dissimilar, components. They generally are in small areas and could not be mapped separately because of the scale used. Some small areas of strongly contrasting soils or miscellaneous areas are identified by a special symbol on the maps. If included in the database for a given area, the contrasting minor components are identified in the map unit descriptions along with some characteristics of each. A few areas of minor components may not have been observed, and consequently they are not mentioned in the descriptions, especially where the pattern was so complex that it was impractical to make enough observations to identify all the soils and miscellaneous areas on the landscape.

The presence of minor components in a map unit in no way diminishes the usefulness or accuracy of the data. The objective of mapping is not to delineate pure taxonomic classes but rather to separate the landscape into landforms or landform segments that have similar use and management requirements. The delineation of such segments on the map provides sufficient information for the development of resource plans. If intensive use of small areas is planned, however, onsite investigation is needed to define and locate the soils and miscellaneous areas.

### 1.3.2 Existing Erosion Problems

Gorrill Palmer is not aware of any existing erosion problems onsite.

### 1.3.3 Critical Areas

The construction will take place within the existing development footprint, and is not anticipated to impact any critical areas.

### 1.3.4 Protected Natural Resources

The site has been previously developed and does not contain wetlands. Based upon the FEMA maps, the site is not located within a Zone A 100-year floodplain.

### 1.3.5 Erosion Control Measures and Site Stabilization

The primary emphasis of the erosion/sedimentation control plan, which will be implemented for this project, is as follows:

- ◆ Development of a careful construction sequence.
- ◆ Rapid revegetation of denuded areas to minimize the period of soil exposure.
- ◆ Rapid stabilization of drainage paths to avoid rill and gully erosion.
- ◆ The use of on-site measures to capture sediment (hay bales/ stone check dams/silt fence, etc.)

The following temporary and permanent erosion and sediment control devices will be implemented as part of the site development. These devices shall be installed as indicated on the plans or as described within this report. For further reference, see the latest edition of the Maine Erosion and Sediment Control BMPS.

#### A. Dewatering

Water from construction trench dewatering shall pass first through a filter bag or secondary containment structure (e.g. hay bale lined pool) prior to discharge. The discharge site shall be selected to avoid flooding, icing, and sediment discharges to a protected resource. In no case shall the filter bag or containment structure be located within 50 feet of a protected natural resource.

## B. Inspection and Monitoring

Maintenance measures shall be applied as needed during the entire construction season. After each rainfall, snow storm or period of thawing and runoff, the site contractor shall perform a visual inspection of all installed erosion control measures and perform repairs as needed to insure their continuous function. Following the temporary and/or final seeding and mulching, the contractor shall in the spring inspect and repair any damages and/or unestablished spots. Established vegetative cover means a minimum of 90% of areas vegetated with vigorous growth.

## C. Temporary Erosion Control Measures

The following measures are planned as temporary erosion/sedimentation control measures during construction:

1. Crushed stone-stabilized construction entrance shall be placed at the entrance along Marginal Way.
2. Siltation fence or wood waste compost berms shall be installed downstream of any disturbed areas to trap runoff-borne sediments until grass areas are revegetated. The silt fence and/or wood waste compost berms shall be installed per the details provided in this package and inspected at least once a week and before and immediately after a storm event of 0.5 inches or greater, and at least daily during prolonged rainfall. Repairs shall be made if there are any signs of erosion or sedimentation below the fence or berm line. If there are signs of undercutting at the center or the edges, or impounding of large volumes of water behind the fence or berm, the barrier shall be replaced with a stone check dam. Wood waste compost berms are not to be used adjacent to wetland areas that are not to be disturbed.
3. Straw or hay mulch including hydroseeding is intended to provide cover for denuded or seeded areas until revegetation is established. Mulch placed between April 15th and October 15th on slopes of less than 15 percent shall be anchored by applying water; mulch placed on slopes of equal to or steeper than 15 percent shall be covered by a fabric netting and anchored with staples in accordance with manufacturer's recommendation. Fabric netting and staples shall be used on disturbed areas within 50' of lakes, streams, and wetlands regardless of the upstream slope. Mulch placed between October 15th and April 15th on slopes equal to or steeper than 8 percent shall be covered with a fabric netting and anchored with staples in accordance with the manufacturer's recommendations. Slopes steeper than 3:1 and equal to or flatter than 2:1, which are to be revegetated, shall receive curlex blankets by American Excelsior or equal. Slopes steeper than 2:1 shall receive riprap as noted on the plans. The mulch application rate for both temporary and permanent seeding is 75 lbs per 1000 sf as identified in Attachment A of this section. Mulch shall not be placed over snow.
4. Temporary stockpiles of stumps, grubblings, or common excavation will be protected as follows:
  - a) Temporary stockpiles shall not be located within 50 feet of any wetlands which will not be disturbed and shall be located away from drainage swales.



b) Stockpiles shall be stabilized within 7 days by either temporarily seeding the stockpile by a hydroseed method containing an emulsified mulch tackifier or by covering the stockpile with mulch, such as hay, straw, or erosion control mix.

c) Stockpiles shall be surrounded by sedimentation barrier at the time of formation.

5. All denuded areas that are within 50 feet of an undisturbed wetland, which have been rough graded and are not located within a building pad, parking area, or access drive subbase area, shall receive mulch or erosion control mesh fabric within 48 hours of initial disturbance of soil. All areas within 100 feet of an undisturbed wetland shall be mulched prior to any predicted rain event regardless of the 48 hour window. In other areas, the time period may be extended to 7 days.

6. For work, which is conducted between October 15<sup>th</sup> and April 15<sup>th</sup> of any calendar year, all denuded areas, shall be covered with hay mulch or erosion control mix, applied at twice the normal application rate and anchored with a fabric netting. The time period for applying mulch shall be limited to 2 days for all areas.

7. Marginal way shall be swept to control mud and dust as necessary.

8. During grubbing operations stone check dams shall be installed at any evident concentrated flow discharge points and as directed on the Erosion Control Plans.

9. Silt fencing with a minimum stake spacing of 6 feet shall be used, unless the fence is supported by wire fence reinforcement of minimum 14 gauge and with a maximum mesh spacing of 6 inches, in which case stakes may be spaced a maximum of 10 feet apart. The bottom of the fence shall be anchored.

10. Wood waste compost/bark berms may be used in lieu of siltation fencing. Berms shall be removed and spread in a layer not to exceed 3" thick once upstream areas are completed and a 90% catch of vegetation is attained.

11. Water and/or calcium chloride shall be furnished and applied in accordance with MDOT specifications – Section 637 – Dust Control.

12. Loam and seed is intended to serve, as the primary permanent revegetative measure for all denuded areas not provided with other erosion control measures, such as riprap. Application rates are provided in Attachment A of this section. Seeding shall not occur over snow.

#### D. Permanent Erosion Control Measures

The following permanent erosion control measures have been designed as part of the Erosion/Sedimentation Control Plan:

1. All areas disturbed during construction, but not subject to other restoration (paving, riprap, etc.) will be loamed, limed, fertilized, mulched, and seeded. Fabric netting, anchored with staples, shall be placed over the mulch in areas as noted in Temporary Erosion Control Measures paragraph 3 of this report. All areas within 50 feet of an undisturbed wetland shall be mulched prior to any predicted rain event regardless of the 48 hour window. Native topsoil shall be stockpiled and reused for final restoration when it is of sufficient quality.

#### I.4 Implementation Schedule

The following construction sequence shall be required to insure the effectiveness of the erosion and sedimentation control measures are optimized:

It is anticipated that construction of the project will commence in the Summer of 2015 and be completed by Spring of 2016.

Note: For all grading activities, the contractor shall exercise extreme caution not to overexpose the site, this shall be accomplished by limiting the disturbed area.

1. Install stabilized construction entrance at the intersection of the access drive and Marginal Way.
2. Install perimeter silt fence and/or wood waste berms prior to commencement of demolition.
3. Perform demolition of existing site elements.
4. Foundation preparation area shall be excavated for installation of the building foundation. Building work will be on going through the remainder of the project.
5. Commence installation of drainage appurtenances.
6. Commence earthwork and grading to subgrade.
7. Commence installation of water and sewer lines.
8. Continue earthwork and grading to subgrade as necessary for construction.
9. Complete installation of underground utilities to within 5' of the buildings.
10. Install light pole foundations and light poles.
11. Complete remaining earthwork operations.
12. Install sub-base and base gravel within parking fields, walkways, and all driveways.
13. Install curbing in parking fields, driveways, and along the streets as needed.

14. Install base course paving for access drive and parking area as well as concrete surfaces.
15. Loam, lime, fertilize, seed and mulch disturbed areas and complete all landscaping.
16. Install surface course paving for access drive and parking areas. Stripe per plan.
17. Once the site is stabilized and a 90% catch of vegetation has been obtained, remove all temporary erosion control measures.
18. Touch up loam and seed.

Note: All denuded areas not subject to final paving, riprap, or gravel shall be revegetated.

Prior to construction of the project, the contractor shall submit to the owner a schedule for the completion of the work, which will satisfy the following criteria:

1. The above construction sequence should generally be completed in the specified order; however, several separate items may be constructed simultaneously. Work must also be scheduled or phased to reduce the extent of the exposed areas as specified below. The intent of this sequence is to provide for erosion control and to have structural measures such as silt fence and construction entrances in place before large areas of land are denuded.
2. The work shall be conducted in sections which shall:
  - a) Limit the amount of exposed area to those areas in which work is expected to be undertaken during the proceeding 30 days.
  - b) Revegetate disturbed areas as rapidly as possible. All areas shall be permanently stabilized within 7 days of final grading or before a storm event; or temporarily stabilized within 48 hours of initial disturbance of soil for areas within 50 feet of an undisturbed wetland and 7 days for all other areas. Areas within 50 feet of an undisturbed wetland shall be mulched prior to any predicted rain event regardless of the 48 hour window.
  - c) Incorporate planned inlets and drainage system as early as possible into the construction phase. The ditches shall be immediately lined or revegetated as soon as their installation is complete.

#### 1.5 Erosion, Sedimentation and Stabilization Control Plan

The Erosion Control information is included in the plan set.

#### 1.6 Details and Specifications

The Erosion Control details and specifications are included in the plan set.

#### 1.7 Winter Stabilization Plan

The winter construction period is from November 1 through April 15. If the construction site is not stabilized with pavement, a road gravel base, 75% mature vegetation cover or riprap by November 15

then the site needs to be protected with over-winter stabilization. An area considered open is any area not stabilized with pavement; vegetation, mulching, erosion control mats, riprap or gravel base on a road.

Winter excavation and earthwork shall be completed such that any area left exposed can be controlled by the contractor. Limit the exposed area to those areas in which work is expected to be under taken during the proceeding 15 days and that can be mulched in one day prior to any snow event.

All areas shall be considered to be denuded until the subbase gravel is installed in roadway/parking areas or the areas of future loam and seed have been loamed, seeded and mulched. Hay and straw mulch rate shall be a minimum of 150 lbs./1,000 s.f. (3 tons/acre) and shall be properly anchored.

The contractor shall install any added measures which may be necessary to control erosion/sedimentation from the site dependent upon the actual site and weather conditions. Continuation of earthwork operations on additional areas shall not begin until the exposed soil surface on the area being worked has been stabilized, in order to minimize areas without erosion control protection.

#### 1. Soil Stockpiles

Stockpiles of soil or subsoil shall be mulched for over winter protection with hay or straw at twice the normal rate or at 150 lbs/1,000 s.f. (3 tons per acre) or with a four-inch layer of woodwaste erosion control mix. This shall be done within 24 hours of stocking and re-established prior to any rainfall or snowfall. Any soil stockpile shall not be placed (even covered with hay or straw) within 50 feet from any natural resources.

#### 2. Natural Resource Protection

Any areas within 50 feet from any natural resources, if not stabilized with a minimum of 75% mature vegetation catch, shall be mulched by December 1 and anchored with plastic netting or protected with erosion control mats. During winter construction, a double line of sediment barriers (i.e. silt fence backed with hay bales or erosion control mix) shall be placed between any natural resource and the disturbed area. Projects crossing the natural resource shall be protected a minimum distance of 50 feet on either side from the resource. Existing projects not stabilized by December 1 shall be protected with the second line of sediment barrier to ensure functionality during the spring thaw and rains.

#### 3. Sediment Barriers

During frozen conditions, sediment barriers shall consist of woodwaste filter berms as frozen soil prevents the proper installation of hay bales and sediment silt fences.

#### 4. Mulching

An area shall be considered denuded until areas of future loam and seed have been loamed, seeded and mulched. Hay and straw mulch shall be applied at a rate of 150 lb. per 1,000 square feet or 3 tons/acre (twice the normal accepted rate of 75-lbs./1,000 s.f. or 1.5 tons/acre) and shall be properly anchored. Mulch shall not be spread on top of snow. The snow shall be removed down to a one-inch depth or less prior to application. After each day of final grading, the area shall be properly stabilized with anchored hay or straw or erosion control matting. An area shall be considered to have been stabilized when exposed surfaces have been either mulched with straw or hay at a rate of 150 lb. per

1,000 square feet (3 tons/acre) and adequately anchored that ground surface is not visible through the mulch.

Between the dates of November 1 and April 15, all mulch shall be anchored by peg line, mulch netting, asphalt emulsion chemical, or wood cellulose fiber. When ground surface is not visible through the mulch then cover is sufficient. After November 1<sup>st</sup>, mulch and anchoring of all bare soil shall occur at the end of each final grading workday.

#### 5. Mulching on Slopes and Ditches

Slopes shall not be left exposed for any extended time of work suspension unless fully mulched and anchored with peg and netting or with erosion control blankets. Mulching shall be applied at a rate of 230 lbs/1,000 s.f. on all slopes greater than 8%.

Mulch netting shall be used to anchor mulch in all drainage ways with a slope greater than 3% for slopes exposed to direct winds and for all other slopes greater than 8%. Erosion control blankets shall be used in lieu of mulch in all drainage ways with slopes greater than 8%. Erosion control mix can be used to substitute erosion control blankets on all slopes except ditches.

#### 6. Seeding

Between the dates of October 15 and April 1<sup>st</sup>, loam or seed will not be required. During periods of above freezing temperatures finished areas shall be fine graded and either protected with mulch or temporarily seeded and mulched until such time as the final treatment can be applied. If the date is after November 1<sup>st</sup> and if the exposed area has been loamed, final graded with a uniform surface, then the area may be dormant seeded at a rate of 3 times higher than specified for permanent seed and then mulched. Dormant seeding may be selected to be placed prior to the placement of mulch and fabric netting anchored with staples. If dormant seeding is used for the site, all disturbed areas shall receive 4" of loam and seed at an application rate of 5 lbs/1,000 s.f. All areas seeded during the winter shall be inspected in the spring for adequate catch. All areas insufficiently vegetated (less than 75% catch) shall be revegetated by replacing loam, seed and mulch. If dormant seeding is not used for the site, all disturbed areas shall be revegetated in the spring.

#### Standards for Timely Stabilization of Construction Sites During Winter

1. Standard for the timely stabilization of ditches and channels -- The applicant shall construct and stabilize all stone-lined ditches and channels on the site by November 15. The applicant shall construct and stabilize all grass-lined ditches and channels on the site by September 1.

2. Standard for the timely stabilization of disturbed slopes -- The applicant shall construct and stabilize stone-covered slopes by November 15. The applicant shall seed and mulch all slopes to be vegetated by September 1. The department shall consider any area having a grade greater than 15% to be a slope.

3. Standard for the timely stabilization of disturbed soils -- By September 15 the applicant shall seed and mulch all disturbed soils on areas having a slope less than 15%. If the applicant fails to stabilize these soils by this date, then the applicant shall take one of the following actions to stabilize the soil for late fall and winter.

## 1.8 Maintenance of facilities

The stormwater facilities will be maintained by the Applicant, Bangor Savings Bank or their assigned heirs. The contract documents will require the contractor to designate a person responsible for maintenance of the sedimentation control features during construction as required by the Erosion Control Report. Long-term operation/maintenance recommended for the stormwater facilities is presented below.

The responsible party may contract with such professionals, as may be necessary in order to comply with this provision and may rely on the advice of such professionals in carrying out its duty hereunder, provided, that the following operation and maintenance procedures are hereby established as a minimum for compliance with this section. A maintenance log of the inspections shall be kept by the responsible party.

### **Inspection and Maintenance Frequency and Corrective Measures:**

The following areas, facilities, and measures will be inspected and the identified deficiencies will be corrected. Clean-out must include the removal and legal disposal of any accumulated sediments and debris.

### **Vegetated Areas:**

Inspect slopes and embankments early in the growing season to identify active or potential erosion problems. Replant bare areas or areas with sparse growth. Where rill erosion is evident, armor the area with an appropriate lining or divert the erosive flows to on-site areas able to withstand the concentrated flows. The facilities will be inspected after major storms and any identified deficiencies will be corrected.

**Roadways and Parking Surfaces:** Clear accumulations of winter sand in parking lots and along roadways at least once a year, preferably in the spring. Accumulations on pavement may be removed by pavement sweeping. Accumulations of sand along road shoulders may be removed by grading excess sand to the pavement edge and removing it manually or by a front-end loader. Repair potholes and other roadway obstructions and hazards. Plowing and sanding of paved areas shall be performed as necessary to maintain vehicular traffic safety.

### **Housekeeping**

The following procedures are hereby established as a minimum for compliance with this section. For further information on the procedures listed below, refer to MDEP Chapter 500 rules – Appendix C.

#### **Spill Prevention:**

Appropriate spill prevention, containment, and response planning/implementation shall be used to prevent pollutants from being discharged from materials on site.

#### **Groundwater Protection:**

During construction, hazardous materials with the potential to contaminate groundwater shall not be stored or handled in areas of the site which drain to an infiltration area.

**Fugitive Sediment and Dust:**

Appropriate measures shall be taken to ensure that activities do not result in noticeable erosion of the soils and water and/or calcium chloride shall be used to ensure that activities do not result in fugitive dust emissions during or after construction.

**Debris and Other Materials:**

Litter, construction debris, and chemicals exposed to stormwater must be prevented from becoming a pollutant source.

**Trench or Foundation De-watering:**

Water collected through the process of trenching and/or de-watering must be removed from the ponded area, and must be spread through natural wooded buffers or other areas that are specifically designed to collect the maximum amount of sediment possible.

**Non-stormwater Discharges:**

Identify and prevent contamination by non-stormwater discharges.

Conclusion

The Applicant has provided temporary and permanent erosion control measures as well as specifying a sequence of construction as measures to minimize erosion and sedimentation.

Attachments

- Attachment A - Seeding Plan
- Attachment B - Inspection Report

ATTACHMENT A  
Seeding Plan



SEEDING PLAN

Project: Bangor Savings Bank

Site Location: Portland, ME

Permanent Seeding                       Temporary Seeding

1. Instruction on preparation of soil: Prepare a good seed bed for planting method used.
2. Apply lime as follows: \_\_\_\_\_ # / acres, OR 138 # /M Sq. Ft.
3. Fertilize with \_\_\_\_\_ pounds of \_\_\_\_\_ N-P-K/ac. OR 13.8 pounds of 10-10-10 N-P-K/M Sq. Ft.
4. Method of applying lime and fertilizer: Spread and work into the soil before seeding.
5. Seed with the following mixture:  
    50% Winter Rye  
    50% Annual Rye
6. Mulching instructions: Apply at the rate of \_\_\_\_\_ per acre, OR 75 pounds per M. Sq. Ft.

	<u>Amount</u>	<u>Unit # Tons. Etc.</u>
7. TOTAL LIME	138	#/1000 sq. ft.
8. TOTAL FERTILIZER	13.8	#/1000 sq. ft.
9. TOTAL SEED	1.03	#/1000 sq. ft.
10. TOTAL MULCH	75	#/1000 sq. ft.
11. TOTAL other materials, seeds, etc.	_____	
12. REMARKS		

Spring seeding is recommended; however, late summer (prior to September 1) seeding can be made. Permanent seeding should be made prior to August 5 or as a dormant seeding after the first killing frost and before the first snowfall. If seeding cannot be done within these seeding dates, temporary seeding and mulching shall be used to protect the site. Permanent seeding shall be delayed until the next recommended seeding period.

SEEDING PLAN

Project: Bangor Savings Bank

Site Location: Portland, ME

Permanent Seeding                       Temporary Seeding

1. Instruction on preparation of soil: Prepare a good seed bed for planting method used.
2. Apply lime as follows: \_\_\_\_\_ # / acres, OR 138 # /M Sq. Ft.
3. Fertilize with \_\_\_\_\_ pounds of \_\_\_\_\_ N-P-K/ac. OR 18.4 pounds of 10-20-20 N-P-K/M Sq. Ft.
4. Method of applying lime and fertilizer: Spread and work into the soil before seeding.
5. Seed with the following mixture:  
40% Creeping Red Fescue  
30% Charger II Perennial Ryegrass  
20% KenBlue Kentucky Bluegrass  
10% Tiffany Chewings Fescue
6. Mulching instructions: Apply at the rate of \_\_\_\_\_ per acre, OR 75 pounds per M. Sq. Ft.

	<u>Amount</u>	<u>Unit # Tons. Etc.</u>
7. TOTAL LIME	138	#/1000 sq. ft.
8. TOTAL FERTILIZER	18.4	#/1000 sq. ft.
9. TOTAL SEED	1.03	#/1000 sq. ft.
10. TOTAL MULCH	75	#/1000 sq. ft.
11. TOTAL other materials, seeds, etc.		
12. REMARKS		

Spring seeding is recommended, however, late summer (prior to September 1) seeding can be made. Permanent seeding should be made prior to August 5 or as a dormant seeding after the first killing frost and before the first snowfall. If seeding cannot be done within these seeding dates, temporary seeding and mulching shall be used to protect the site. Permanent seeding shall be delayed until the next recommended seeding period.

**ATTACHMENT B**  
**Inspection Report**

**STORMWATER POLLUTION PREVENTION PLAN**

**INSPECTION REPORT**

PROJECT INFORMATION

Project Name: Bangor Savings Bank

Address: 20 Marginal Way  
Portland, Maine

CONTRACTOR/SUBCONTRACTOR INFORMATION

Inspector Name: \_\_\_\_\_

Firm: \_\_\_\_\_

Title: \_\_\_\_\_

Qualifications: \_\_\_\_\_

INSPECTION SUMMARY

Date of Inspection: \_\_\_\_\_

Major Observations: \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

THE FACILITY IS IN COMPLIANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN WITH THE FOLLOWING EXCEPTIONS:

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

## Master Signage Plan

### Bangor Savings Bank and Office Building

20 Marginal Way, Portland, Maine

In accordance with Portland Downtown Urban Design Guidelines for the B-7 Mixed Use Urban District, attached is the Master Signage Plan for the proposed Bangor Savings Bank and Office Building for 20 Marginal Way, Portland, Maine. The Mater Signage Plan adheres to the Guidelines as follows:

#### B. Design:

- The design of signage is respectful of the building and has been carefully designed to fit the given facades complementing the building's architectural features.
- The design of signage is oriented and scaled to reflect the scale and character of movement of people around the building, with an emphasis primarily on the pedestrian and slow-moving traffic. To emphasize the two pedestrian entrances into the building an architectural overhang will be installed over both entrances. Back halo-illuminated signs will be installed on each overhang to inform pedestrians along Marginal Way and from the rear parking lot to the appropriate entrance.
- The size of the proposed signs is compatible with the scale of the overall building.
- The size relates comfortably in size and scale to the pedestrians moving about in the vicinity.
- The signage is simple and limited to the primary tenant of the property.

- The lettering typefaces and words are simple, easy to read, and scaled appropriately for both the signs and the building.
- The colors on the signage have been selected to complement the character and color pattern of the building.
- The illumination of the signs is very compatible and consistent with the character of illumination already existing on the surrounding buildings and on existing appropriate signs in the vicinity. The proposed upper wall signs are internally illuminated block letters which is the style of signs for the vast majority of wall signs along Marginal Way, including but not limited to Gorham Savings Bank, Drummond Woodson, Planet Fitness, Walgreens, WinxNet, Trader Joes, Eastern Mountain Sports and West Marine. The signs over the pedestrian entrances are back halo-illuminated signs which is a character of illumination appropriate for pedestrian areas.

#### C. Placement

- The placement of the signage on all building elevations has been carefully considered, taking into account the scale, character and design of the building.
- The placement of the signage does not visually obscure any architecturally significant features of the building.

#### D. Number of Signs

- Building signs are limited to one per building street frontage for each tenant. Bangor Savings will occupy the retail bank branch on the 1<sup>st</sup> floor of the building and the 2<sup>nd</sup> and 3<sup>rd</sup> floors with banking service offices. In keeping with the above guideline, maintaining a simple signage message, Bangor Savings has decided not to uniquely sign the service office uses. Bangor Savings is proposing wall signs on each of three street frontages (Marginal Way, Forest Avenue and I-295).
- The signs have a common pattern and character.

- Standard E-16 limits the number of upper level signs to two and one sign per storefront at the Pedestrian level. Since the proposed building occupies the entire Marginal Way frontage except for the entrance drive and the building is located on the front property line there is no room for a pylon sign. Bangor Savings is requesting a third upper level sign to be located on the Marginal Way elevation in place of a pylon sign. To properly inform pedestrians of the two pedestrian entrances, Bangor Savings is requesting a second pedestrian level sign to be located above the Marginal Way entrance door.

- The total sign area proposed in the Master Signage Plan is approximately 50% of the sign area allowed for the proposed building.

**From:** David Latulippe <dlatulip@aol.com>  
**To:** <JF@portlandmaine.gov>  
**CC:** <dcm.danmiller@gmail.com>, <jason.donovan@bangor.com>  
**Date:** 7/8/2015 10:57 AM  
**Subject:** Fwd: Bangor Savings Bank - Marginal Way  
**Attachments:** image003.jpg; Sidewalk Rendering 6-28-15.JPG

Jean,

We have investigated the transparency of the proposed glazing for the Bangor Savings project. In accordance with the B-7 Design Principles, Bangor Savings has aggressively incorporated "green" design and sustainable technology into the architectural design. The proposed glazing is very comparable to the Intermed building which has the Apothecary by Design on the first floor which is very visible and provides an active streetscape feel. The front of the Bangor Savings building faces south which is ideal for the solar panels being installed on the roof but subjects the façade to solar heat gain. In order to meet the transparency standard, the AC system would have to be substantially increased resulting in significantly higher energy consumption. This is an example of where the standards conflicts.

The proposed storefront exceeds the design standards in quantity of glass and is located directly in front to the proposed new 8 foot wide brick sidewalk. The attached rendering portrays how pedestrian oriented the façade of the building will be. Bangor Savings' strong preference is to install the high performance glass and maintain the current energy efficient design. We request a waiver to the transparency standard, similar to the one granted to the Intermed building.

Should you have any questions or need any additional information, please do not hesitate to contact me.

Sincerely,

David Latulippe

-----Original Message-----

**From:** Johnson, Troy <tjohnson@sigcoinc.com>  
**To:** 'David Latulippe' <dlatulip@aol.com>; dcm.danmiller <dcm.danmiller@gmail.com>  
**Sent:** Wed, Jul 8, 2015 9:59 am  
**Subject:** Bangor Savings Bank - Marginal Way

Hi David,

Confirming our telephone conversation:

The Intermed building is glazed with product similar to the proposed BSB product, but the BSB product is much higher performing.

The vision glass on Intermed has a VLT of 61%, with a SHGC of 0.45.

The proposed vision glass for BSB has a VLT of 56%, with a SHGC of 0.32.

Simply put, the proposed glass for BSB allows approximately 30% less solar heat gain, while reducing the visible light transmitted by only 5%.

Please let me know if you would like to discuss this further or if you require additional product literature or



samples.

Thank you,

Troy

Troy Johnson, CSI, CCPR

Vice President Sales

48 Spiller Drive

Westbrook, ME 04092

Tel 207-775-2691

[www.sigcoinc.com](http://www.sigcoinc.com)