

90-A-A-1, 2+5
Peaks Island
512 Island Ave.
Peaks Isl. Fuel
~~LEITH IVERS~~

School Advance
Warning Assembly



S1-1



W16-9p

OR



W16-2a

OR



W16-2

School Crosswalk
Warning Assembly



S1-1



W16-7p



W11-2

PEDESTRIAN CROSSING
SIGN

SCHOOL AREA CROSSING
SIGNS

Typical Crosswalk Signage

DATE:
AUGUST 2009

REVISED:

CITY OF PORTLAND, MAINE
TECHNICAL STANDARDS MANUAL

TRANSPORTATION SYSTEMS
AND STREET DESIGN
SECTION I

FIGURE:

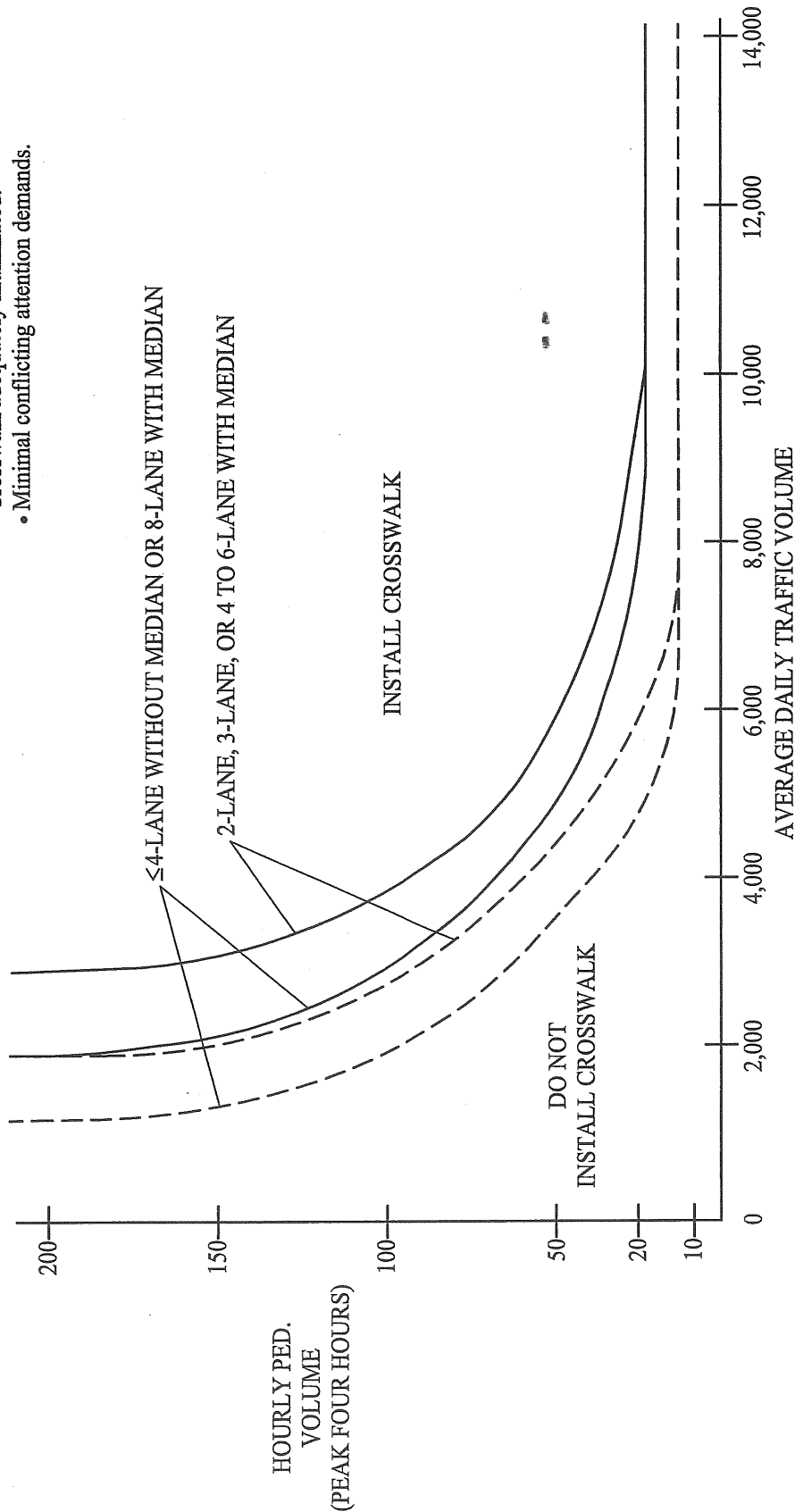
I-24

TYPICAL CROSSWALK SIGNAGE

BASIC CRITERIA

- Speed limit ≤ 45 mi/h.
- Adequate stopping sight distance.
- For midblock, preferred block length $\geq 600'$.
- Crosswalk adequately illuminated.
- Minimal conflicting attention demands.

- - - = Locations with predominately young, elderly or handicapped pedestrians.
- = Other locations



1. If using only the peak hour, threshold must be increased by 1.5.
2. For streets with median, use one-way (directional) ADT volume.

GUIDELINES FOR CROSSWALK INSTALLATION AT UNCONTROLLED INTERSECTIONS AND MID-BLOCK CROSSINGS.

DATE: AUGUST 2009	CITY OF PORTLAND, MAINE TECHNICAL STANDARDS MANUAL	TRANSPORTATION SYSTEMS AND STREET DESIGN SECTION I	FIGURE:
REVISED:	GUIDELINES FOR CROSSWALK INSTALLATION AT UNCONTROLLED INTERSECTIONS AND MID-BLOCK CROSSINGS		I-25 <small>58</small>

FEDERAL STREET CLASSIFICATION

XX XX Other Principal Arterial - Federal & Expressway

Principal Arterial - Interstate

▲▲▲ Other Principal Arterial

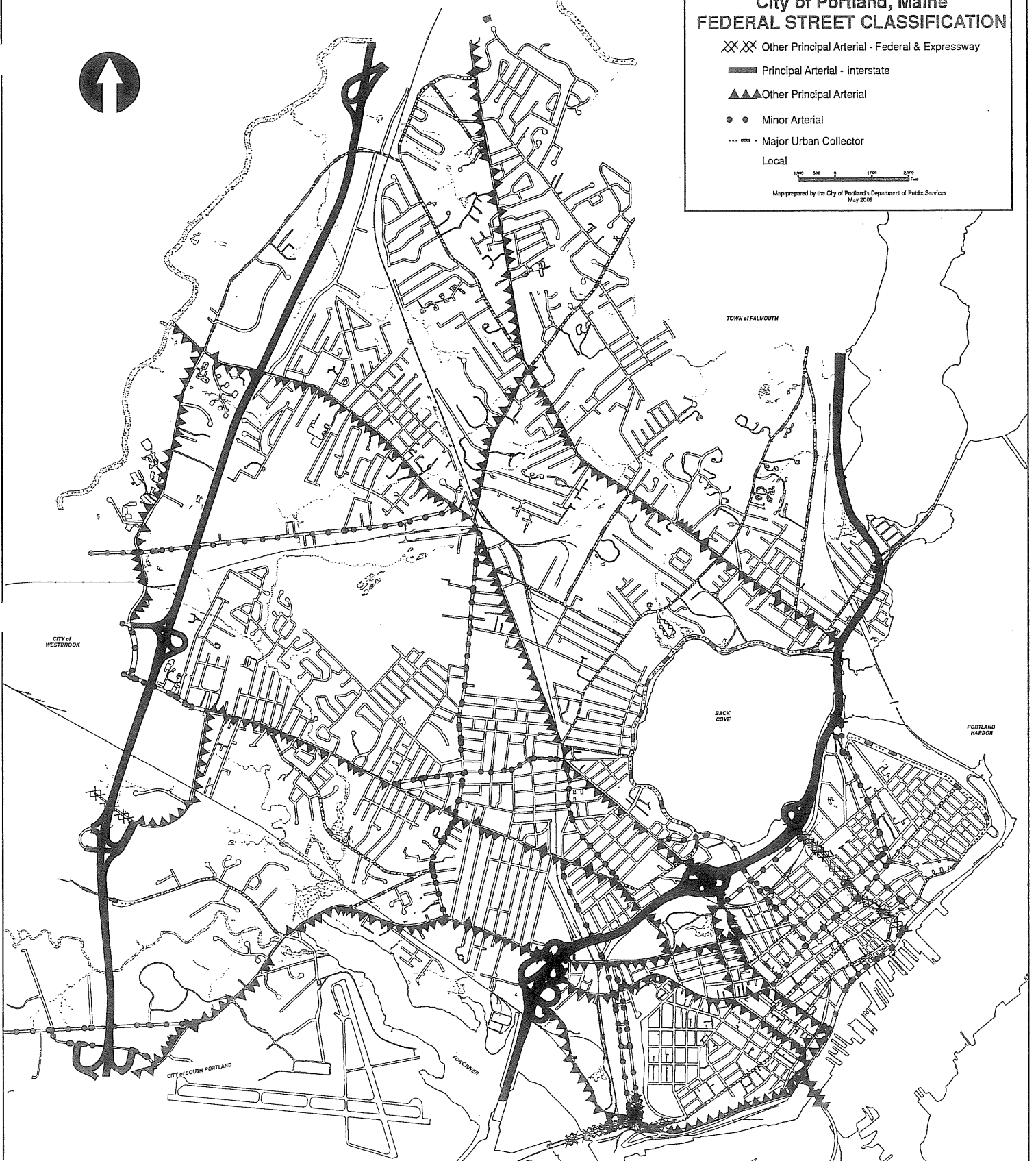
● ● Minor Arterial

Major Urban Collector

Local



Map prepared by the City of Portland's Department of Public Services
May 2009



DATE:
AUGUST 2009

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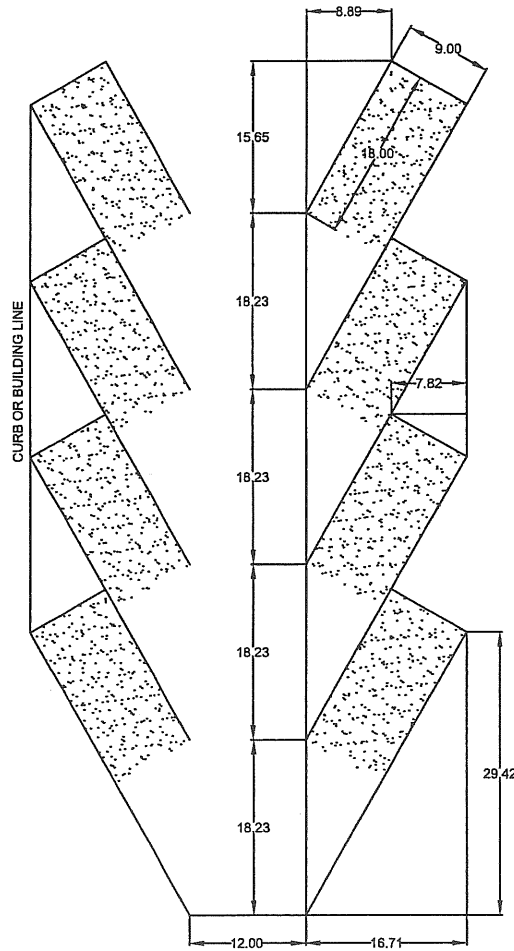
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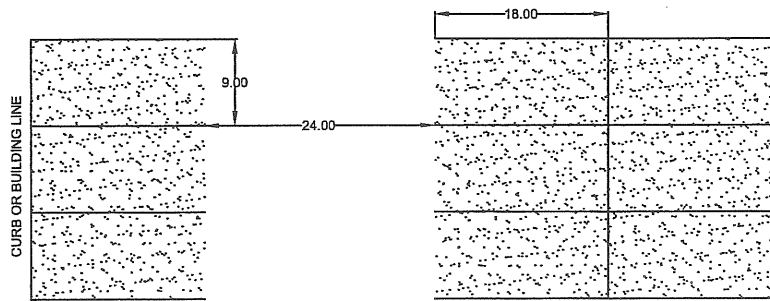
FIGURE:

FEDERAL STREET CLASSIFICATION

I-26



30° PARKING @ 9' x 18'



90° PARKING @ 9' x 18'

STANDARD PARKING SPACES

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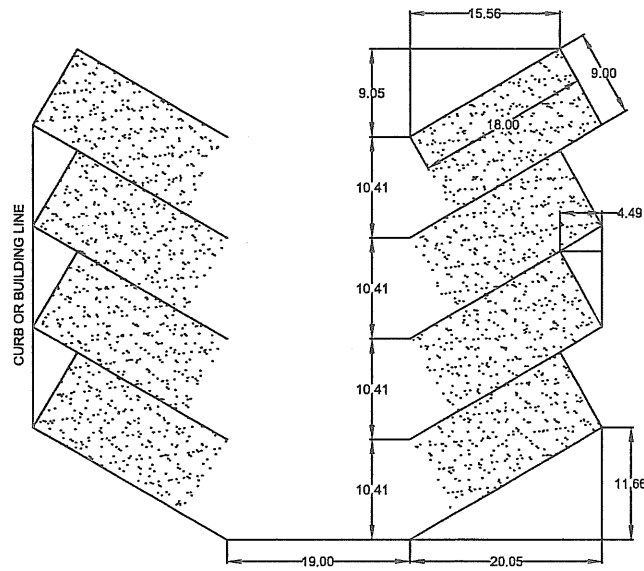
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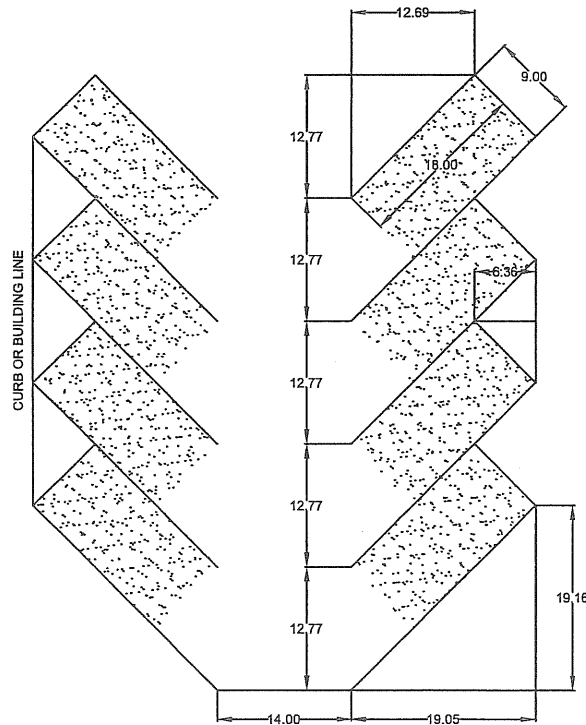
FIGURE:

STANDARD PARKING SPACES

I-27
 60



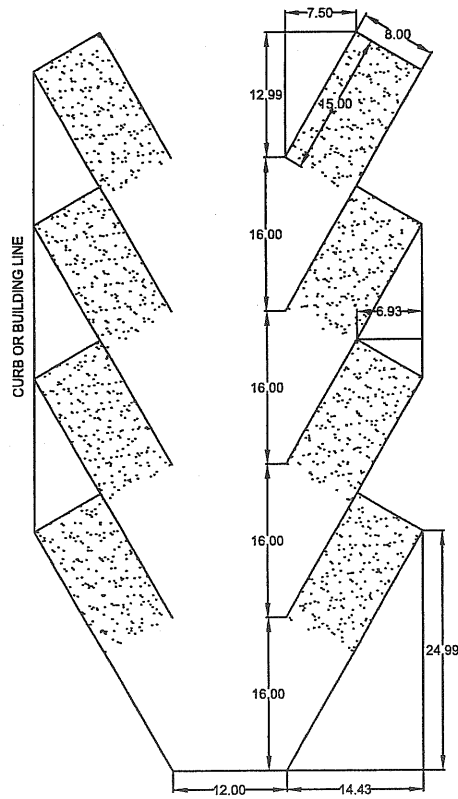
60° PARKING @ 9' x 18'



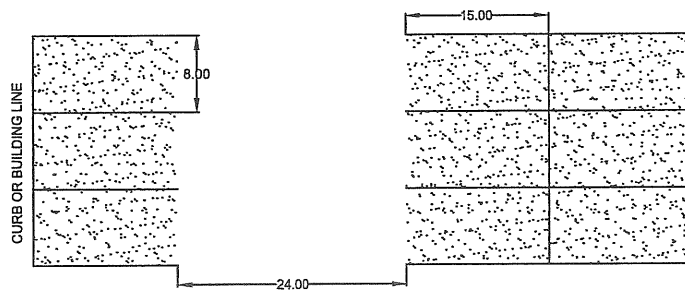
45° PARKING @ 9' x 18'

STANDARD PARKING SPACES

DATE: AUGUST 2009 REVISED:	CITY OF PORTLAND, MAINE TECHNICAL STANDARDS MANUAL	TRANSPORTATION SYSTEMS AND STREET DESIGN SECTION I	FIGURE:
STANDARD PARKING SPACES			I-28 61



30° PARKING @ 8' x 15'



90° PARKING @ 8' x 15'

COMPACT PARKING SPACES

DATE:
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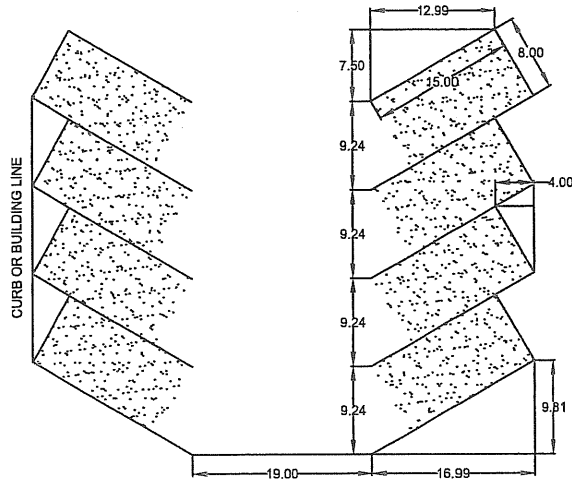
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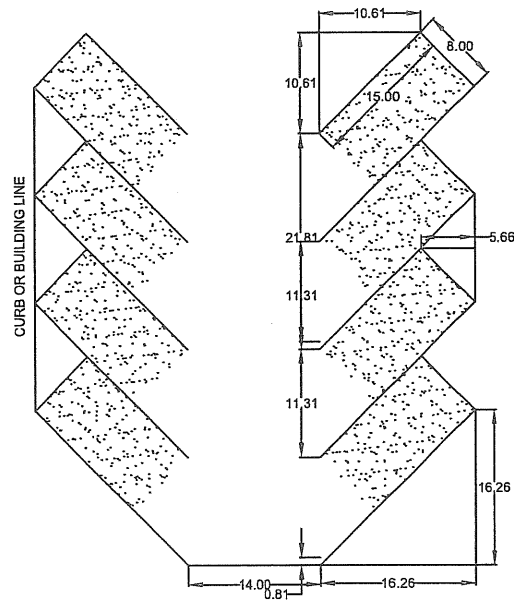
FIGURE:

COMPACT PARKING SPACES

I-29
 62



60° PARKING @ 8' x 15'



45° PARKING @ 8' x 15'

COMPACT PARKING SPACES

DATE:
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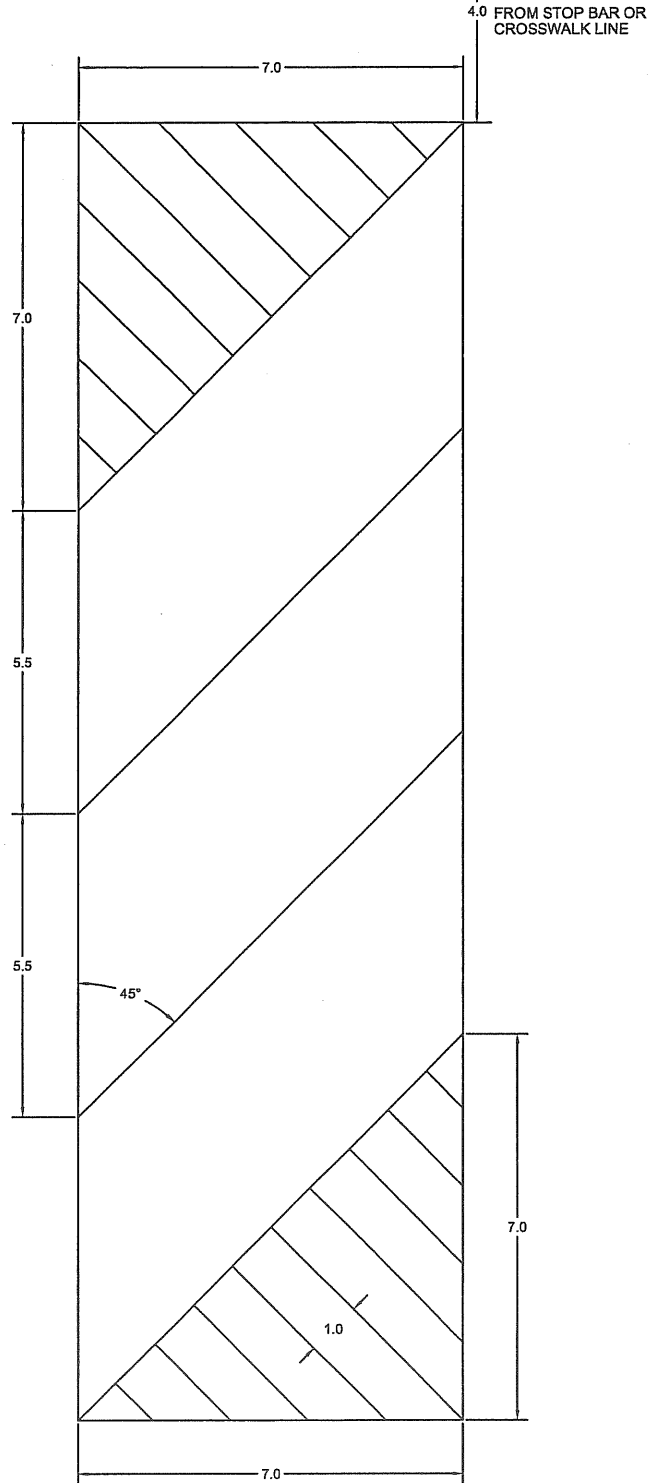
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FIGURE:

COMPACT PARKING SPACES

I-30
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MOTORCYCLE PARKING

*ALL PAVEMENT MARKINGS SHALL BE 4 INCH WIDE WHITE LINES.

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SECTION I

FIGURE:

MOTORCYCLE PARKING

I-31

BICYCLE PARKING RACK PLACEMENT

RACK PLACEMENT

RULES:

5' from;
 Fire hydrant
 Crosswalk

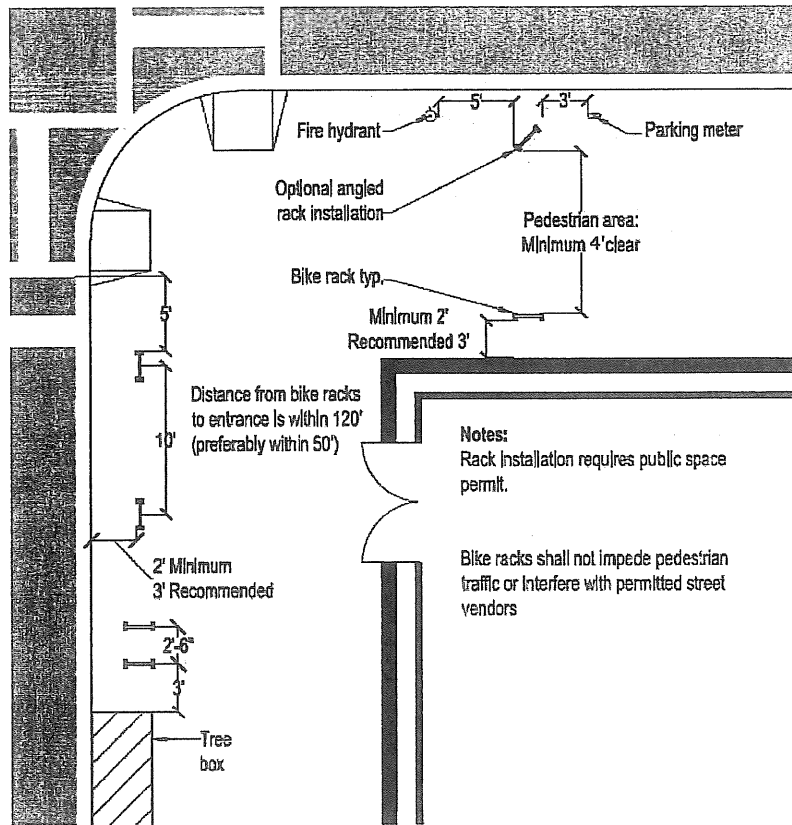
4' from;
 Loading zone
 Bus stop
 Bus shelter
 Bus bench

Min. 2', Rec. 3' from;
 Curb

3' from;
 Parking meter
 Newspaper rack
 US mailbox
 Light pole
 Sign pole
 Driveway
 Tree space
 Trash can
 Utility meter
 Manhole
 Other street furniture
 Other sidewalk obstructions

WALL SETBACKS

For racks set parallel to a wall:
 Min. 24", Rec. 36"
 For racks set perpendicular to a wall:
 Min. 28", Rec. 36"

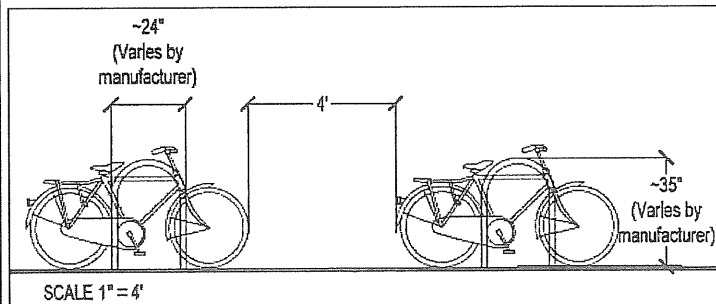


Notes:
 Rack installation requires public space permit.

Bike racks shall not impede pedestrian traffic or interfere with permitted street vendors

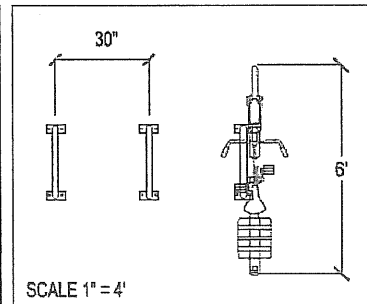
SCALE 1" = 10'

SIDE VIEW



SCALE 1" = 4'

SIDE BY SIDE RACKS:



SCALE 1" = 4'

d.
 District Department of Transportation

District Department of Transportation
 Bicycle Facility Design Guide

REVISED:
 Feb. 2006
 SCALE:
 AS NOTED

27

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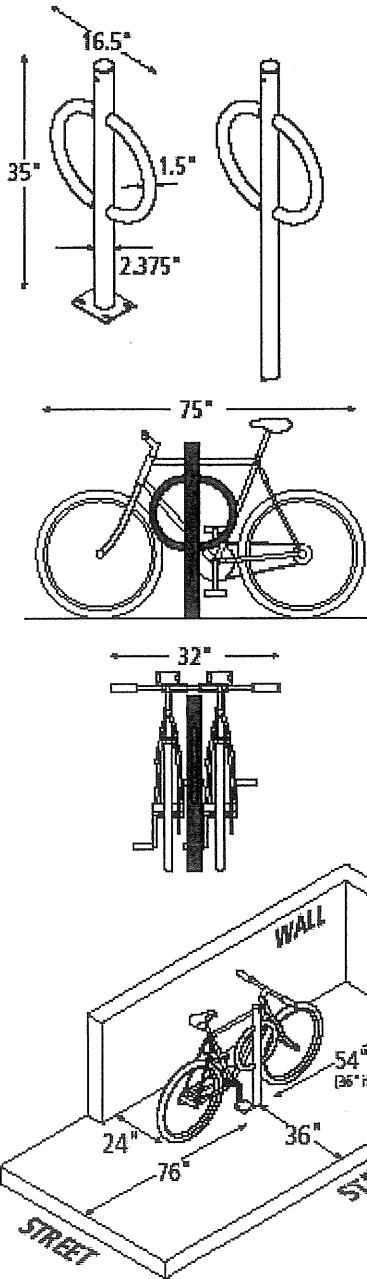
FIGURE:

BICYCLE PARKING RACK PLACEMENT GUIDELINES

I-32
 65

BIKE HITCH

Specifications and Space Use



Product Dero Bike Hitch
As manufactured by Dero Bike Racks

Capacity 2 Bikes

Materials Centerbeam: 2" schedule 40 pipe (2.375" OD)
Ring: 1.5" OD 11 gauge tube

Finishes An after fabrication hot dipped galvanized finish is standard. 250TGIC powder coat colors, a thermoplastic coating and a stainless steel option are also available.

Our powder coat finish assures a high level of adhesion and durability by following these steps:
1. Sandblast
2. Iron phosphate pretreatment
3. Epoxy primer electrostatically applied
4. Final thick TGIC polyester powder coat

Stainless Steel: 304 grade stainless steel material finished in either a high polished shine or a satin finish.

A rubbery PVC Dip is also available

Installation Methods In-ground mount is embedded into concrete base. Surface mount has one 5" x 6" foot which is anchored to the ground with four anchors (included with rack).

Space Use and Setbacks **Wall Setbacks:** For racks set parallel to a wall: Minimum: 12" Recommended: 24"

For racks set perpendicular to a wall: Minimum: 35" (centerline measurement) Recommended: 38" (54" if aisle is needed between bike and wall)

Distance Between Racks: Minimum: 24" Recommended: 38"

Street Setbacks: Minimum: 36"



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FIGURE:

BICYCLE RACK SPECIFICATION - BIKE HITCH

I-33a
66

BIKE HITCH

Installation Instructions - Surface Mount

Tools Needed for Installation

- Tape Measure
- Marker or Pencil
- Masonry Drill Bit
- Drill (Hammer drill recommended)
- Hammer
- Wrench 9/16"
- Level
- Washers (for leveling if necessary)

Recommended Base Materials:

Solid concrete is the best base material for installation. Ask your Dero Rack representative which anchor is appropriate for your application to ensure the proper anchors are shipped with your rack. Be sure nothing is underneath the base material that could be damaged by drilling.

Installation:

3/8" anchors are shipped with the rack. Place the rack in the desired location. Use a marker or pencil to outline the holes of the flange onto the base material. Drill the holes in accordance with the specifications shipped with the anchors. Make sure the holes are at least 6" away from any cracks in the base material.

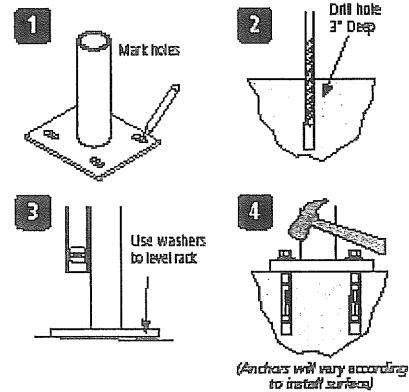
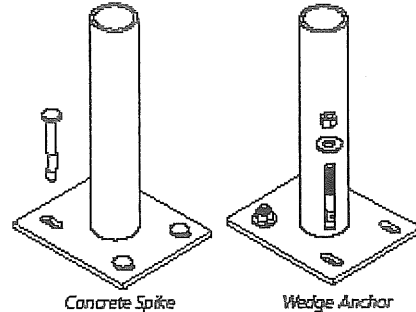
Tamper Resistant Fasteners

The concrete spike is a permanent anchor. The top of the wedge anchor can also be pounded sideways after installation so that it cannot be removed. Other tamper resistant fasteners are also available for purchase.

When using the special tamper resistant nuts, always set and first tighten the anchors. Once the rack is installed, replace two nuts from the bracket (opposite sides from each other) with the tamper resistant fastener. **DO NOT OVERTIGHTEN** the tamper resistant nut.



If you have any questions about installation or other features of the Blue Bike Rack, please call us toll free at 1-800-290-4915



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FIGURE:

BICYCLE RACK SPECIFICATION - BIKE HITCH

I-33b
67

BIKE HITCH

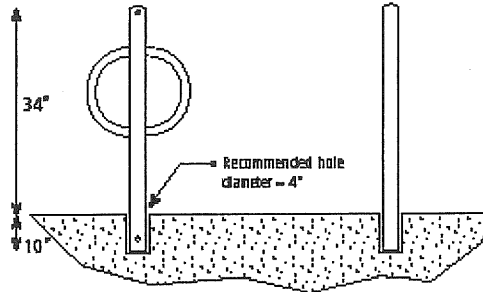
In-ground Installation Instructions

Tools needed for installation

- Level
- Cement mixing tub
- Shovel
- Trowel
- Hole coring machine with 4" bit
- Access to water hose
- Materials to build brace (see "Install Tip" at bottom of page)

Installing into Existing Sidewalk

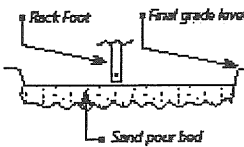
Core holes no less than 3" diameter (4" recommended) and no less than 6" deep into sidewalk. Place Bike Hitch into hole making sure the rack is level. Fill hole with Por-Rok or epoxy grout. 34-36" of the Bike Hitch should remain above the surface. Make sure Hitch is level and held in place until the grout has completely set.



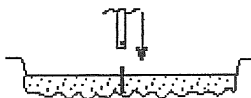
Installing into a New Sidewalk

Stake Method:

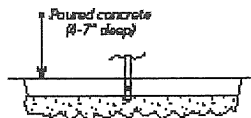
1 Use rack to measure exact location in pour bed.



2 Pound stake into pour bed where end of rack will sit. Slide rack end onto stake. You may need to dig the end of the rack into the sand to make sure the rack sits at least 35" above final grade level. The stake keeps the rack straight while the concrete is being poured.

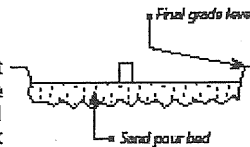


3 Make sure the rack is level and true. Pour concrete around the rack. Make sure the rack is not touched until the concrete has completely set.

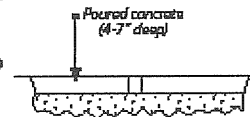


Sleeve Method:

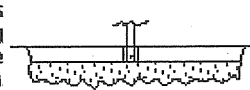
1 Place corrosion resistant sleeve (min. 3" inside diameter) in sand pour bed in exact location where rack will be installed. Make sure top of sleeve is at same level as desired finished concrete surface. Fill sleeve with sand to keep it in place and prevent it from filling with concrete.



2 Pour concrete and allow to cure.

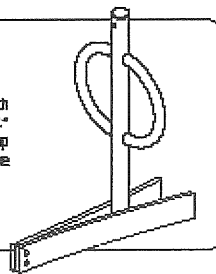


3 After appropriate cure time, dig out sand from sleeves and insert racks, making sure they are level and at the appropriate height. Pour in Por-Rok or epoxy grout and allow to set.

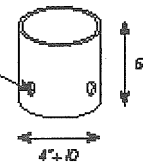


INSTALL TIP

An easy way to brace the Bike Hitch while the grout sets is to bolt two 1x4" boards together at one end and clamp them onto the legs of the Bike Hitch like a clothes pin.



Note: Sleeve should have profile to keep it from coming loose from hardened concrete.



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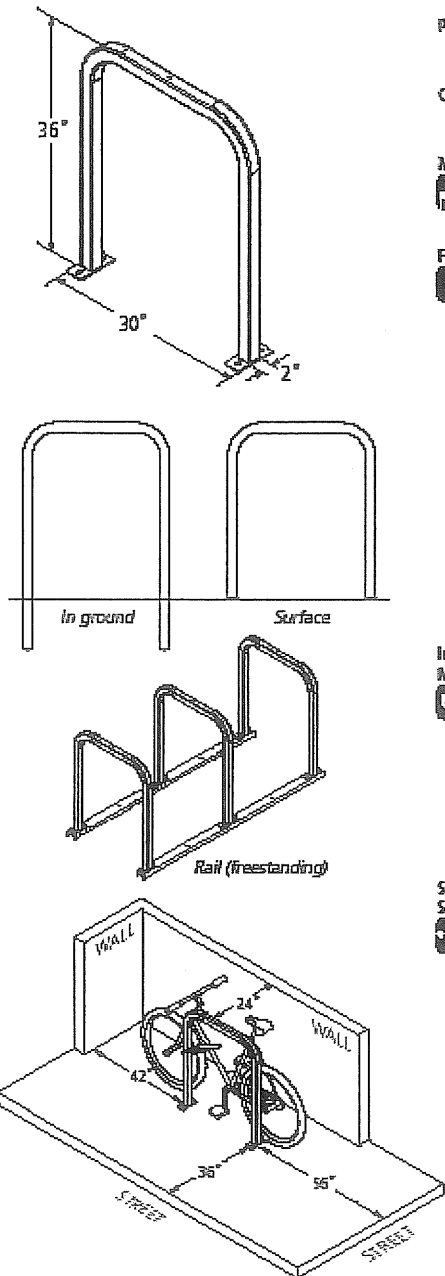
FIGURE:

BICYCLE RACK SPECIFICATION - BIKE HITCH

I-33c
68

 **DOWNTOWN RACK**

Specifications and Space Use



Product Dero Downtown Rack
 As manufactured by Dero Bike Racks

Capacity 2 Bikes

Materials 2" x 2" x 3/16" tube

Finishes An after fabrication hot dipped galvanized finish is standard. 250 TGIC powder coat colors, a thermoplastic coating and a stainless steel option are also available.

Our powder coat finish assures a high level of adhesion and durability by following these steps:

1. Sandblast
2. Iron phosphate pretreatment
3. Epoxy primer electrostatically applied
4. Final thick TGIC polyester powder coat

Stainless Steel: 304 grade stainless steel material finished in either a high polished shine or a satin finish.

A rubbery PVC Dip is also available

Installation Methods In ground mount is embedded into concrete base. Specify In ground mount for this option. Foot Mount has two 2.5"x6"x.25" feet with two anchors per foot. Specify foot mount for this option. Rail Mounted Downtown Racks are bolted to two parallel rails which can be left freestanding or anchored to the ground. Rails are heavy duty 3"x1.4"x3/16" thick galvanized mounting rails. Specify rail mount for this option.

Space Use and Setbacks **Wall Setbacks:** For racks set parallel to a wall: Minimum: 24" Recommended: 36"

For racks set perpendicular to a wall: Minimum: 28" Recommended: 42"

Distance Between Racks: Minimum: 24" Recommended: 36"

Street Setbacks: Minimum: 24" Recommended: 36"



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FIGURE:

BICYCLE RACK SPECIFICATION - DOWNTOWN RACK

I-34a
 69

DOWNTOWN RACK

Installation Instructions - Surface Mount

Tools Needed for Installation

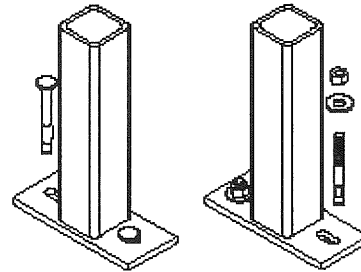
- Tape Measure
- Marker or Pencil
- Masonry Drill Bit
- Drill (Hammer drill recommended)
- Hammer
- Wrench 9/16"
- Level

Recommended Base Materials:

Solid concrete is the best base material for installation. To ensure the proper anchors are shipped with your rack, ask your Dero Rack representative which anchor is appropriate for your application. Be sure nothing is underneath the base material that could be damaged by drilling.

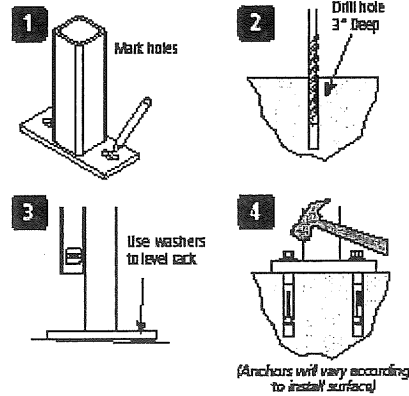
Installation:

3/8" anchors are shipped with the rack. Place the rack in the desired location. Use a marker or pencil to outline the holes of the flange onto the base material. Drill the holes in accordance with the specifications shipped with the anchors. Make sure the holes are at least 3" away from any cracks in the base material. Use washers to level rack if necessary. Tap in anchors and follow your specific anchor instructions provided with the rack.



Concrete Spike

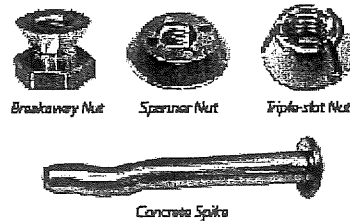
Wedge Anchor




Tamper Resistant Fasteners



The concrete spike is a permanent anchor. The top of the wedge anchor can also be pounded sideways after installation so that it cannot be removed. Other tamper resistant fasteners are also available for purchase.

When using the special tamper resistant nuts, always set and first tighten the anchors. Once the rack is installed, replace two nuts from the bracket (opposite sides from each other) with the tamper resistant fastener. DO NOT OVERTIGHTEN the tamper resistant nut.



 If you have any questions about installation or other features of the Downtown Rack, please call us toll free at 1-800-298-4915



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FIGURE:

BICYCLE RACK SPECIFICATION - DOWNTOWN RACK

I-34b
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DOWNTOWN RACK

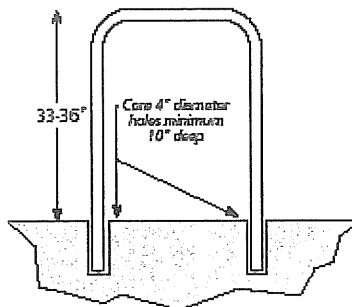
Installation Instructions - In Ground Mount

Tools Needed for Installation

Level	Hole coring machine with 4" bit
Cement mixing tub	Access to water hose
Shovel	Materials to build brace (see "Install Tip" at bottom of page)
Trowel	

Installing into Existing Sidewalk

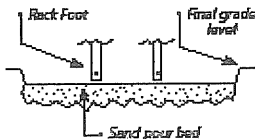
Core holes no less than 3" diameter (4" recommended) and no less than 6" deep into sidewalk. Fill holes with Por-Rok or epoxy grout. Place Downtown Rack into holes, making sure the rack is level. 33"-36" of the Downtown Rack should remain above the surface. If the Downtown Rack is less than 33" high, it will not support the bike adequately. Make sure the rack is level and held in place until the grout has set.



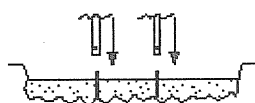
Installing into a New Sidewalk

Stake Method:

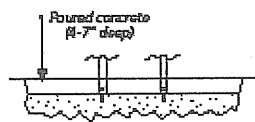
1 Use rack to measure exact location in pour bed.



2 Pound stake into pour bed where end of rack will sit. Slide rack end onto stake. You may need to dig the end of the rack into the sand to make sure the rack sits at least 35" above final grade level. The stake keeps the rack straight while the concrete is being poured.

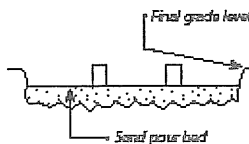


3 Make sure the rack is level and true. Pour concrete around the rack. Make sure the rack is not touched until the concrete has completely set.

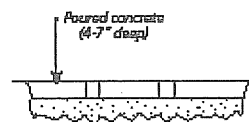


Sleeve Method:

1 Place corrosion resistant sleeve (min. 4" inside diameter) in sand pour bed in exact location where rack will be installed. Make sure top of sleeve is at same level as desired finished concrete surface. Fill sleeve with sand to keep it in place and prevent it from filling with concrete.



2 Pour concrete and allow to cure.

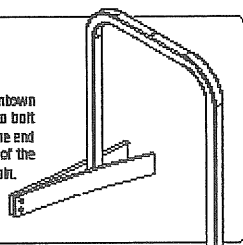


3 After appropriate cure time, dig out sand from sleeves and insert racks, making sure they are level and at the appropriate height. Pour in Por-Rok or epoxy grout and allow to set.

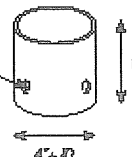


INSTALL TIP

An easy way to brace the Downtown Rack while the grout sets is to bolt two 1x4" boards together at one end and clamp them onto the legs of the Downtown Rack like a clothes pin.



Note: Sleeve should have profile to keep it from coming loose from hardened concrete.



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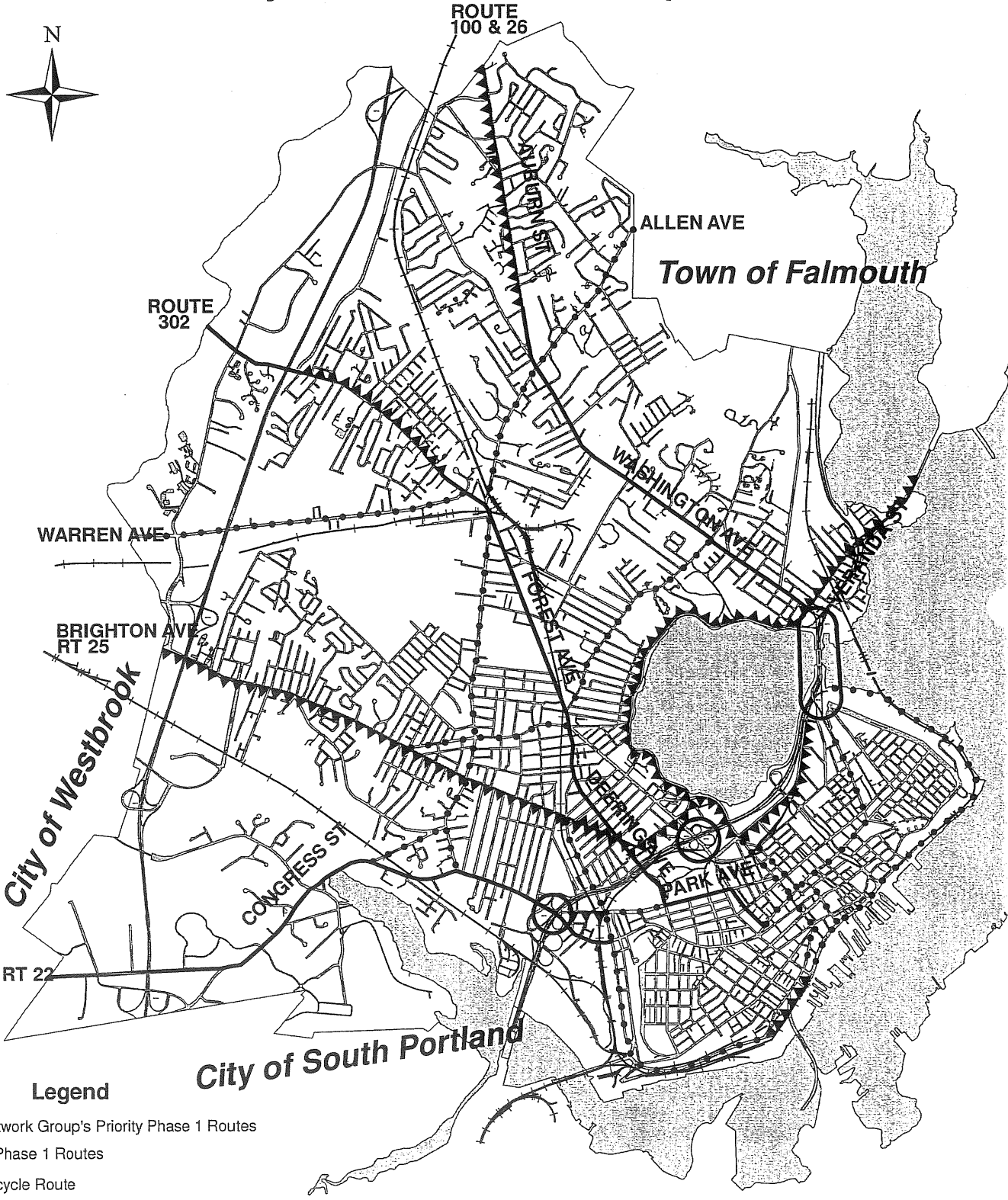
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FIGURE:

BICYCLE RACK SPECIFICATION - DOWNTOWN RACK

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71

City of Portland, Maine Bicycle Network Map



DATE:
 AUGUST 2009
 REVISED:

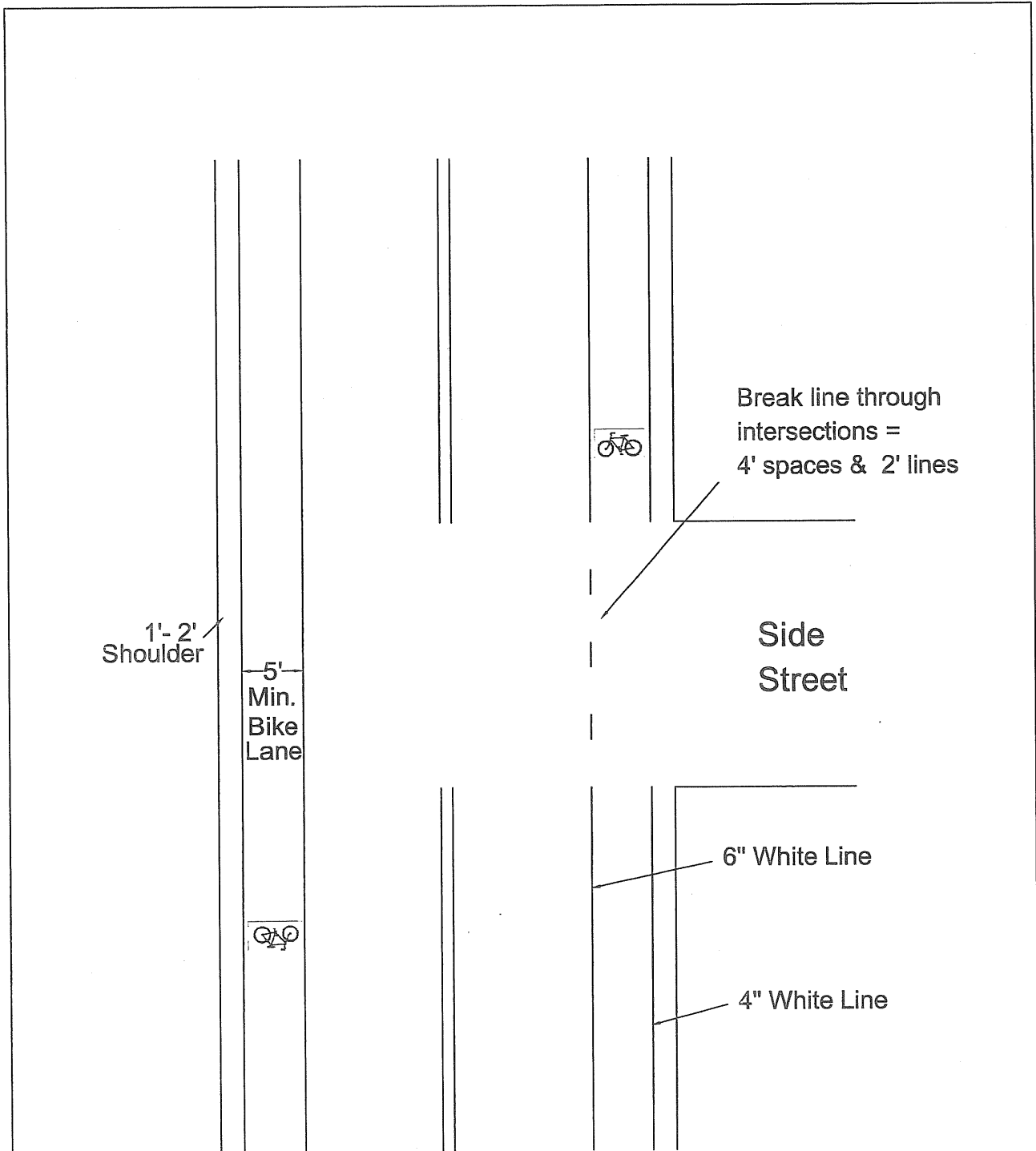
CITY OF PORTLAND, MAINE
 TECHNICAL STANDARDS MANUAL

TRANSPORTATION SYSTEMS
 AND STREET DESIGN
 SECTION I

FIGURE:

BICYCLE NETWORK MAP

I-35



City Street w/ Bike Lanes

DATE:
AUGUST 2009
REVISED:

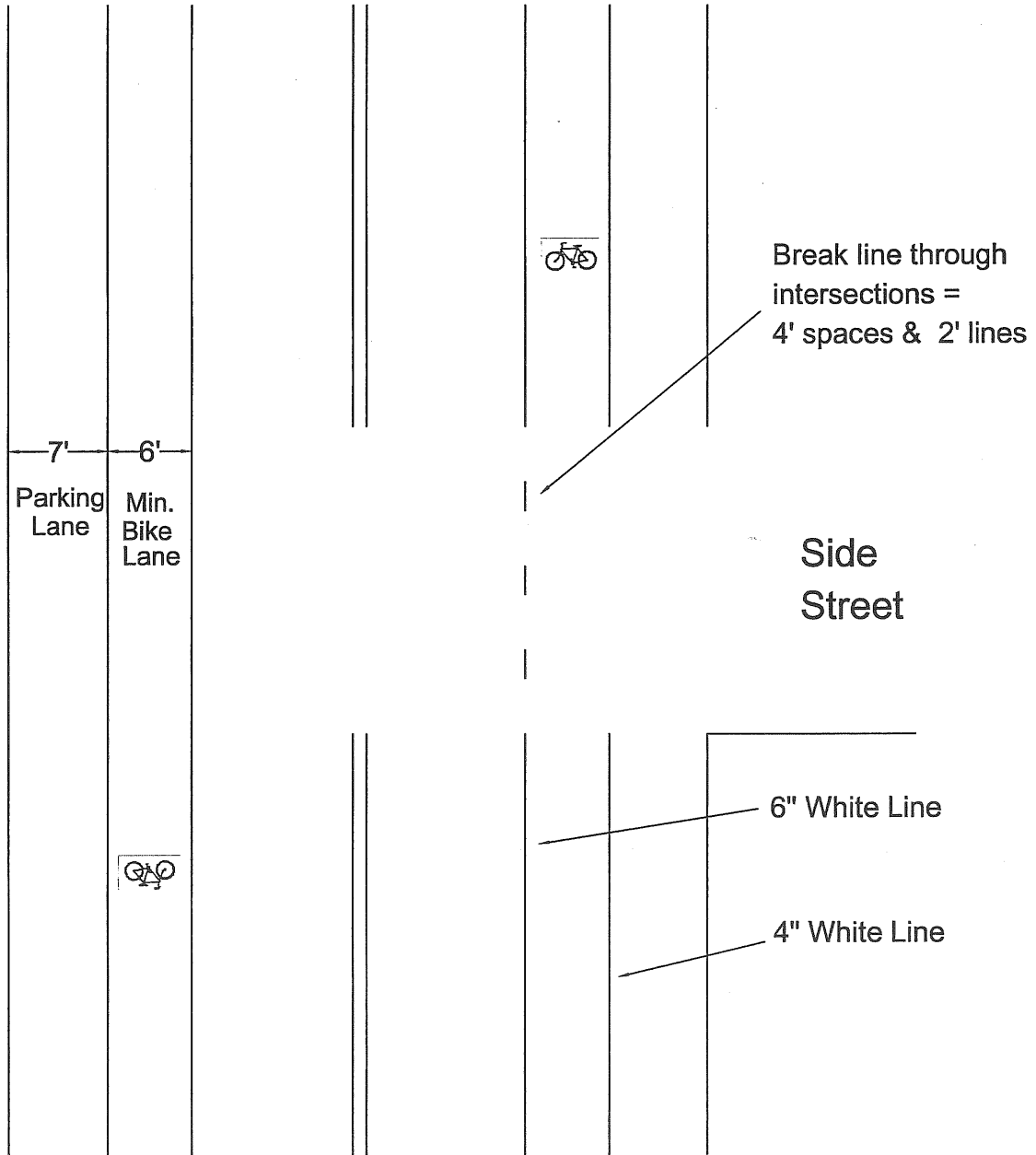
CITY OF PORTLAND, MAINE
TECHNICAL STANDARDS MANUAL

TRANSPORTATION SYSTEMS
AND STREET DESIGN
SECTION I

FIGURE:

CITY STREET W/BIKE LANES

I-36
73



City Street w/ Parking & Bike Lanes

DATE:
AUGUST 2009
REVISED:

CITY OF PORTLAND, MAINE
TECHNICAL STANDARDS MANUAL

TRANSPORTATION SYSTEMS
AND STREET DESIGN
SECTION I

FIGURE:

CITY STREET W/ PARKING & BIKE LANES

I-37
74

TYPICAL BICYCLE LANE PAVEMENT MARKINGS



SHARED USE LANE
SYMBOL



BICYCLE LANE
SYMBOL

TYPICAL BICYCLE ROUTE SIGNAGE



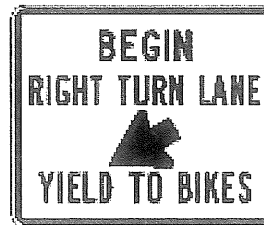
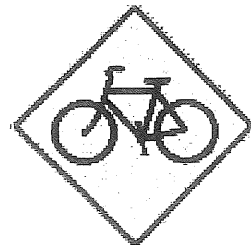
R3-17
30" x 24"



R3-17
30" x 24"



R3-17B
30" x 12"



DATE:
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REVISED:

CITY OF PORTLAND, MAINE
TECHNICAL STANDARDS MANUAL

TRANSPORTATION SYSTEMS
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SECTION I

FIGURE:

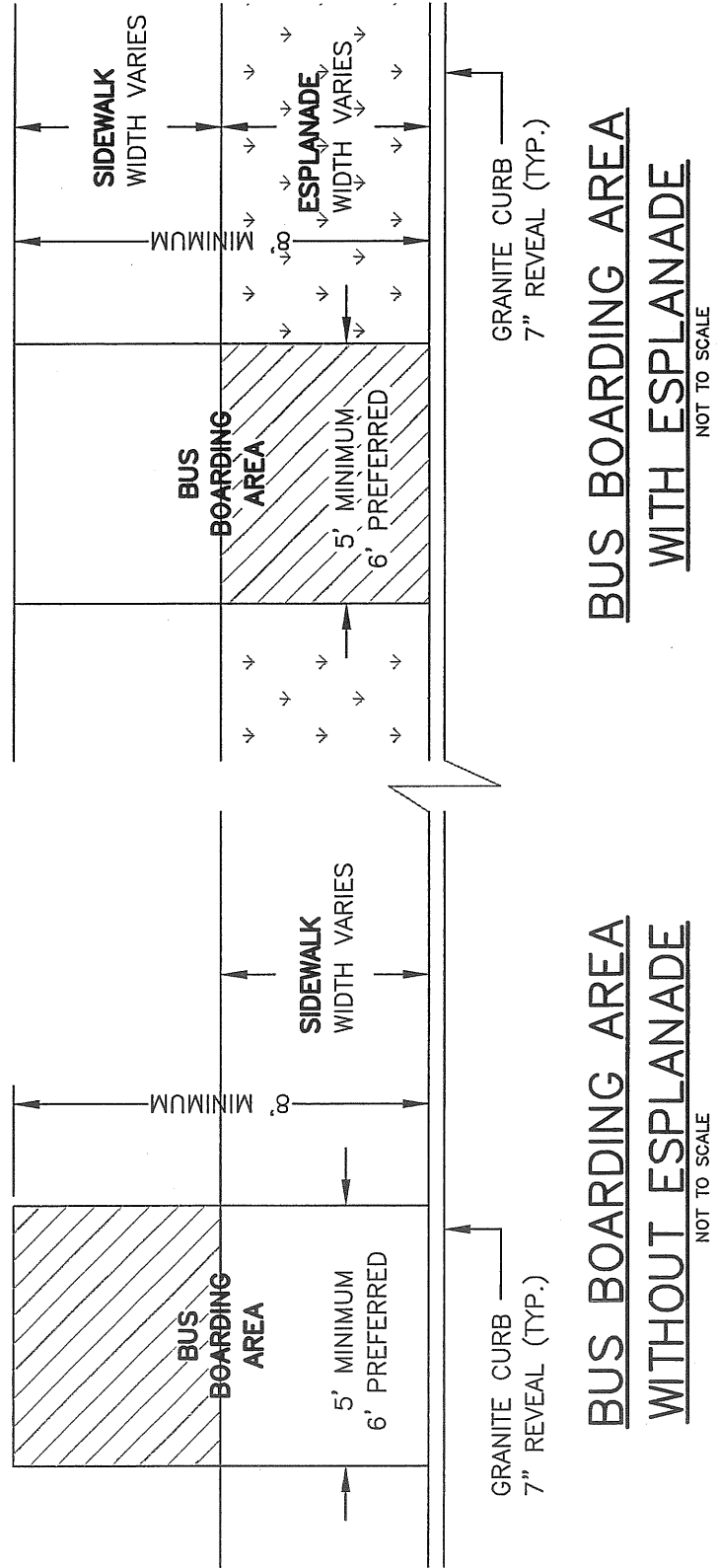
TYPICAL BICYCLE LANE PAVEMENT MARKINGS
AND BICYCLE ROUTE SIGNAGE

I-38
75

NOTES:

1. **SURFACE.** BUS STOP BOARDING AREAS SHALL HAVE A FIRM, STABLE SURFACE.
2. **DIMENSIONS.** BUS STOP BOARDING AREAS SHALL PROVIDE A CLEAR LENGTH OF 8' MINIMUM, MEASURED PERPENDICULAR TO THE CURB OR VEHICLE ROADWAY EDGE, AND A CLEAR WIDTH OF 5' MINIMUM, MEASURED PARALLEL TO THE VEHICLE ROADWAY. THIS AREA SHALL BE CLEAR OF ANY OBSTRUCTIONS, INCLUDING BUT NOT LIMITED TO: BICYCLE RACKS, LIGHT POLES, UTILITY POLES, FIRE HYDRANTS, STREET SIGNS, STREET FURNITURE, NEWSPAPER BOXES OR SIMILAR OBSTACLES.
3. **CONNECTION.** BUS STOP BOARDING AREAS SHALL BE CONNECTED TO STREETS, SIDEWALKS OR PEDESTRIAN PATHS BY AN ADA ACCESSIBLE ROUTE.
4. **SLOPE.** PARALLEL TO THE ROADWAY, THE SLOPE OF THE BUS STOP BOARDING AREA SHALL BE THE SAME AS THE ROADWAY, TO THE MAXIMUM EXTENT PRACTICABLE. PERPENDICULAR TO THE ROADWAY, THE SLOPE OF THE BUS STOP BOARDING AREA SHALL NOT EXCEED 2%.

ADDITIONAL WIDENING,
 AS NEEDED.



DATE:
 MARCH 2011

REVISED:

CITY OF PORTLAND, MAINE
 TECHNICAL STANDARDS MANUAL

TRANSPORTATION SYSTEMS
 AND STREET DESIGN
 SECTION I

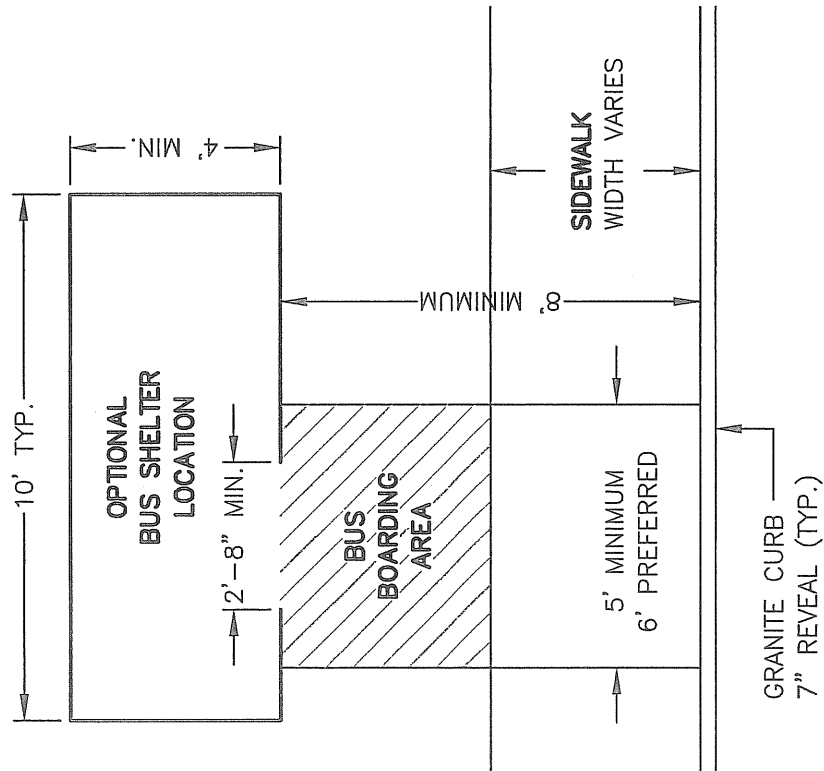
FIGURE:

**BUS STOP BOARDING AREA LAYOUT FOR
 SIDEWALK WITH OR WITHOUT ESPLANADE**

I-39

NOTES:

1. **DIMENSIONS.** MINIMUM CLEAR FLOOR INTERIOR AREA ENTIRELY WITHIN THE PERIMETER OF THE SHELTER IS 2'-6" WIDE BY 4' DEEP TO PERMIT WHEELCHAIR OR MOBILITY AID USER ACCESS. THE MINIMUM SHELTER OPENING FOR WHEELCHAIR ACCESS IS 2'-8".
2. **CONNECTION.** BUS SHELTER OPENINGS WILL BE CONNECTED TO THE BUS BOARDING AREA BY AN ADA ACCESSIBLE ROUTE.
3. **SLOPE.** PARALLEL TO THE ROADWAY, THE SLOPE OF THE BUS BOARDING AREA SHALL BE THE SAME AS THE ROADWAY, TO THE MAXIMUM EXTENT PRACTICABLE. PERPENDICULAR TO THE ROADWAY, THE SLOPE OF THE BUS BOARDING AREA SHALL NOT EXCEED 2%.



**BUS SHELTER AND SHELTER SITING
 WITHOUT ESPLANADE**

NOT TO SCALE

DATE:
 MARCH 2011
 REVISED:

CITY OF PORTLAND, MAINE
 TECHNICAL STANDARDS MANUAL

TRANSPORTATION SYSTEMS
 AND STREET DESIGN
 SECTION I

FIGURE:

BUS SHELTER AND SHELTER SITING LAYOUT
 FOR SIDEWALK WITHOUT ESPLANADE

I-40

0289

AMENDMENT TO PORTLAND CITY
CODE, SECTIONS 14-331, 14-341
(ZONING ORDINANCE) RE: TECHNICAL
STANDARDS FOR OFF-STREET PARKING
(Robert B. Ganley, City Manager)

IN THE CITY COUNCIL

March 20, 19 89

Given first reading.
April 3, 1989 -Removed from the table.
Given second reading and passed, 7 Yeas.

Attest: 
City Clerk.

City of Portland, Maine
IN THE CITY COUNCIL

AMENDMENT TO PORTLAND CITY CODE
§§14-331, 14-341 (ZONING ORDINANCE)
RE: TECHNICAL STANDARDS FOR OFF-STREET PARKING

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF PORTLAND, MAINE, IN CITY COUNCIL ASSEMBLED, AS FOLLOWS:

1. Section 14-331 is hereby amended to read as follows:

Sec. 14-331. Defined.

Off-street parking, either by means of open-air spaces ~~each having an area no less than nine (9) feet wide and nineteen (19) feet long~~ or by garage spaces, which meet the standards set forth in the City of Portland Technical and Design Standards and Guidelines, as hereafter amended, in addition to being a permitted use in certain zones, shall be considered as an accessory use when required or provided to serve conforming uses in any zone.

2. Section 14-341 is hereby amended to read as follows:

Sec. 14-341. Aisles required for six or more spaces.

In parking facilities containing six (6) or more parking spaces, there shall be provided vehicular access by one or more aisles. ~~Where parking spaces are arranged at angles of sixty (60) degrees or less, aisle widths shall be not less than eighteen (18) feet; where parking spaces are arranged at angles greater than sixty (60) degrees, but not more than seventy five (75) degrees, aisle widths shall be not less than twenty (20) feet; and aisle widths for all other parking space arrangements shall be not less than twenty four (24) feet.~~ Aisle widths shall be in conformance with the standards set forth in the City of Portland Technical and Design Standards and Guidelines, as hereafter amended.

BE IT FURTHER ORDAINED and determined by the City Council that the above amendments are necessary to correct current inconsistencies in parking requirements and shall apply to all pending proceedings, applications, and petitions and to all projects previously approved by the planning board or planning authority.

CITY OF PORTLAND, MAINE
MEMORANDUM

TO: Robert B. Ganley, City Manager

FROM: Joseph E. Gray, Jr., Director of Planning and Urban Development

DATE: March 8, 1989

SUBJECT: Parking Definition Zoning Amendment - Request for Council Item

Recent developments have uncovered a discrepancy between existing zoning requirements for parking stall dimensions and aisle widths compared with the standards for such dimensions contained in the adopted City of Portland Technical Design Standards and Guidelines. A careful reading of current zoning results in a prohibition of any open air compact car spaces; a result which is unworkable and inefficient given current practices in the design of parking lots. We are therefore requesting that the attached zoning amendment to the definition of parking be brought to the City Council as soon as possible to clarify this situation. The amendment would make the Technical Standards, which were recently revised to reflect state of the art parking layout design, the definitive reference for parking space and aisle width requirements. At present the more restrictive zoning requirements prevail, which is creating problems for site plan review.

This issue was brought to the Planning Board's attention during workshop review of several site plans. While no formal vote was taken by the Board, all members agreed at the workshop that an immediate correction is needed, and endorsed the direct action on this amendment by the City Council. The members do not believe that the Planning Board needs to further review the proposed amendment or hold a public hearing on this item. The amendment would, in effect, restore the practice of parking layout to the way it was understood before the restrictive nature of the present ordinance was clearly recognized.

In addition to the standards listed above, staff has reviewed the plan for compliance with 2) *Landscaping and Landscape Preservation* of the Environmental Quality Standards and 2) *Public Safety and Fire Prevention* of the Public Infrastructure and Community Safety Standards in order to minimize or abate any potential adverse impact upon adjacent private property to the extent feasible.

Preliminary Plan Staff Review Comments

A. Zoning

Zoning Administrator Marge Schmuckal, August 18, 2011 (Attachment 2)

The applicant, Mr. Ivers, is showing a vehicle parking lot for seven (7) vehicles located at 512 Island Avenue, Peaks Island. Currently a single family house is located on one portion of the lot. The proposed parking area will be located on another portion of the lot and has sufficient space to park four 2,800 gallon fuel trucks and three passenger-vehicle sized service vehicles. The vehicles are all to be actively used by Mr. Ivers in his heating and fuel oil business. The proposed parking area is shown entirely within the I-B zone.

Section 14-223(f) indicates that "off-street parking" is a permitted use in the I-B island business zone.

Section 14-331 defines "off-street parking" as parking "either by use of open-air spaces or by garage spaces which meet the standards set forth in the City of Portland Technical Manual, as hereafter amended...."

The above sections do not limit the allowable "off-street parking" to any particular type of vehicle or only allow parking as an accessory use. Instead, the I-B zone allows off-street parking as a specific permitted use and the definition makes clear that the parking spaces can either be open-air or garage spaces that meet the standards in the City's Technical Manual. The proposed parking area is meeting the standards in the City's Technical Manual. As a result, I have determined that the parking lot described in Mr. Iver's application is permitted under the City Code.

It is important to note that I have reviewed the definition for a truck terminal. A "truck terminal" is defined in the City's Land Use Zoning Ordinance as:

"a building and premises devoted to handling and temporary warehousing of goods, which may include facilities for the maintenance and repair (except body repairs, frame straightening and painting), fueling and storage of trucks or tractor-trailer combinations".

Mr. Iver's proposed parking lot is not a truck terminal. This is because his trucks are not warehoused or stored on the site. The trucks are also not filled, fueled and no product will be dispensed on the site. Instead, the trucks are just parked on this site for active use as needed in Mr. Ivers' propane and oil delivery business. The other vehicles that will be parked on the site are also for active use with Mr. Ivers' heating repair business. Mr. Ivers' business has been active through four generations and has garnered many clients on Peaks Island.

You have the right to appeal my decision. If you wish to exercise your right to appeal, you have thirty days from the date of this letter in which to appeal. If you should fail to do so, my decision is binding and not subject to appeal. Please contact this office for the necessary paperwork that is required to file an appeal.

B. Division 20. Off-Street Parking

Planner Eric Giles

Under Sec. 14-339. When located adjacent to a street or a residential use. Where off-street parking for more than six (6) vehicles is required or provided on a lot in any business zone, the following requirements shall be met:

(b) Where such off-street parking shall be on a lot in a residence zone or a lot in residential use, a chain link, picket or sapling fence, not less than forty-eight (48) inches in height, shall be provided and maintained between such off-street parking and that part of the lot line involved.

This standard is being met with the proposed 6' solid board fence shown on the Landscape Plan. The City Arborist has made the following recommendation with regards to fencing below.

Fencing - In addition to the proposed fencing to screen the parking lot, I would recommend a simple wooden split-rail fence that would run from about the timber retaining wall along the entrance drive following the property line. This fencing would be to demarcate the property line and serve as a safety for children or other residential visitors from venturing into the proposed parking area. This could be an option with agreement from the neighbor.

Stormwater Engineer David Senus, P.E.

Gravel Driveway & Parking Area:

The applicant proposes a compacted gravel driveway and parking lot area, the only exception being a curbed concrete pad for spill containment for four parking spaces on the east side of the lot. The detail of the gravel section includes 15" of MDOT Type D gravel subbase and 6" of MDOT Type A gravel surface. The use of a gravel parking lot surface is allowable under Division 20 - Off Street Parking of Chapter 14 - Land Use of the City Code of Ordinances (Sec. 14-340 *Construction requirements when more than six vehicles parked.* (b)). The section meets the City's requirements for base and subbase gravel depths for Collector and Arterial roadways, and exceeds the City's requirements for base and subbase gravel depths for Local roadways and driveways.

Under the Site Plan Standards (14-526) of the City's Land Use Code, parking lots "shall be constructed of a permanent and durable hard surface that is not subject to ponding or erosion" (14-526.(a).4.a.(v)). The parking lot areas and some of the driveway, as proposed, meet the requirements of this standard, the exception being the steeper driveway area (8% slope). Channelized flow and erosion of surface gravels may occur in this steeper driveway area. We recommend the applicant propose an alternate surface material in driveway areas that exceed a 5% slope.

Given the use and location of the site, and considering the depth and type of gravel noted on the applicant's plans, we have no concern with the use of a gravel parking lot or gravel driveway surface for areas with 5% or less slope.

C. Transportation

Traffic Engineer Tom Errico, P.E.

I have reviewed the site plan prepared by Terradvn Consultants, LLC and I have the following comments.

- The proposed commercial driveway does not meet City standards for width. Based upon low traffic usage, repeat driver use, and the fact that customers will not be accessing the site, I support a waiver for the construction of a 12-foot wide driveway.

- The applicant should define the location of the driveway as it relates to abutting driveways and whether it meets the City's driveway separation standard. I'm not concerned about this issue, based upon traffic volumes on Island Avenue and traffic entering and exiting relevant driveways, but want to note whether a standard waiver is required.
- I support a waiver from the City's technical standards regarding the truck parking stall dimensions (12' x 22') and the parking aisle width (28') to assist with on-site circulation movements. I do not support the provision of the 10' x 20' parking stalls and would suggest that they be reduced to meet city standards (9' x 18').
- The applicant should provide information on the adequacy of sight distance exiting the proposed driveway.
- In my professional opinion the proposed project will not significantly impact safety or traffic mobility in the area of the project.

Deputy Engineer David Margolis-Pineo

The Department has the following comments.

- Per Section 1.8.1 Driveway Aprons of the City's Technical Manual:
 "Any driveway, or section thereof, located within any public street right-of-way shall be designed and built with a permanent, erosion resistant, surface, such as hot mix asphalt pavement or brick as illustrated in Figures I-10 through I-12."

The applicant may choose to use concrete for this purpose. If so and since oil trucks will be using this apron, the apron shall be a minimum of six inches in thickness, reinforced, a mix of 4,000 psi.

- Due to the 8% grade, the applicant is requested to present a concept to stabilize the proposed gravel drive to avoid erosion of the gravels.
- Due to the high cost of Type A gravels, the applicant may wish to use Type B gravels which is acceptable.

Planner Eric Giles

Under Sec. 14-526 (a) Transportation Standards (c) Sidewalks, all proposed developments shall provide sidewalks along all frontages in accordance with Sections 14-498 and 14-499 of the City Code, installed to City specifications as described in Section 1 of the Technical Manual. An applicant may request a waiver from curb and sidewalk installation requirements if they meet applicable waiver criteria listed in Section 14-506 (b) of the City Code.

The waiver criterion has been submitted as Attachment 3 to this letter.

D. Environmental Quality

Stormwater Engineer David Senus, P.E.

Woodard & Curran has reviewed the Level I Site Alteration submission for 512 Island Avenue.

Comments

- The Applicant has submitted a Spill Prevention, Control and Countermeasures Plan to the MaineDEP Bureau of Remediation and Waste Management. The proposed system will require continuous maintenance to ensure that the containment area on the slab is drained following every rain event. The site grading should be further detailed to restrict stormwater from running onto the concrete parking spaces from adjacent areas.

- The Applicant has requested a waiver from the flooding standard. The Applicant should provide calculations for the pre-development and post-development peak flows at the project site. We would likely support a waiver from meeting the flow requirements of the flooding standard if the post-development flow is found to be higher than the pre-development flow, given the proximity to ocean waters; however we need to see the changes in flow to evaluate the impact to the offsite conveyance ditch.
- Additional spot grades should be provided at the northeast corner of the parking lot to ensure that stormwater will be directed to the ditch turnout buffer.
- The Grading and Erosion Control Plan shows a culvert crossing the proposed gravel driveway. Based on the pipe's invert elevations and the proposed surface grade, there appears to be 1.25 feet of cover over this pipe. The Applicant should specify the type of pipe that will be used and verify that the amount of cover is acceptable. A typical pipe installation detail should be provided.
- Inlet and outlet erosion control protection should be provided for the proposed 12" diameter culvert pipe.
- The plans should note a location for snow storage. The snow storage location should be sited outside of existing and proposed drainage courses.

City Arborist Jeff Tarling

I have reviewed the proposed project at 512 Island Avenue and offer the following comments:

- Plant sizes - Shade and ornamental trees proposed did not meet the city standard sizes, shade trees such as Red Maple should be 2.5" caliper, ornamental trees 2" caliper, and evergreens 5-6' in height minimum. The shrub sizes proposed did meet standards.
- Additional screening recommendation - I would recommend an additional 5 lilac shrubs to screen the adjacent residential property, this screening can follow the note on the plan to be coordinated between the project team and neighbor.

Overall, the landscape plan saves existing trees, plants additional mix of shade and evergreen trees along with shrubs to help buffer the project.

E. Public Infrastructure and Community Safety Standards

Fire Capt. Chris Pirone

Fire is all set with the proposed use and parking layout. No comments or changes required.

Please submit a complete set of final plans to address staff comments. Upon receipt of the revised material, the City of Portland will review the additional plans and information for conformance with applicable ordinances. If you have any questions, feel free to contact me at 874-8723 or by email at egiles@portlandmaine.gov

Sincerely,

Eric Giles

Eric Giles
Planner

Attachments

- 1: Peaks Island Fuel SPCC Plan – MDEP Review 8/23/2011
- 2: Sidewalk and Curb Waiver Criterion

Electronic Distribution:

Barbara Barhvd. Development Review Services Manager
Danielle West-Chuhta. Associate Corporation Counsel
Marge Schmuckal. Zoning Administrator
David Margolis-Pineo. Deputy City Engineer
Chris Pirone. Fire
Jeff Tarling. City Arborist
Tom Errico. Wilbur Smith Consulting Engineers
David Senus. Woodard and Curran

Attachment 1: MDEP SPCC Review

From: "Bowie, Butch C" <Butch.C.Bowie@maine.gov>
To: "Eric Giles" <EGILES@portlandmaine.gov>
CC: "Jeff Amos" <jeff@terradyvconsultants.com>
Date: 8/23/2011 12:58 PM
Subject: Peaks Island Fuel SPCC Plan

Dear Mr. Giles.

In 2002, the Maine Legislature enacted 38 MRSA § 570-K(5), giving the Maine DEP authority to oversee compliance with the federal SPCC requirements for aboveground oil storage facilities that exceed the federal 1.320 gallon aggregate storage capacity threshold and are used to market and distribute oil.

An SPCC plan lists the containment equipment and structures used to prevent spills from reaching ground water or surface water, and it identifies the inspection, monitoring and oil transfer procedures that will be followed to prevent a spill. If a spill occurs, a well-developed Oil SPCC plan will identify whom to call, and will specify steps, or "countermeasures," to contain the spill and minimize environmental impacts. The specific SPCC requirements for oil storage facilities are found in federal regulation, 40 CFR Part 112. A qualified professional engineer must examine the plan and attest that it has been prepared in accordance with good engineering practices.

Based on a review of the draft SPCC plan prepared by Jeffrey D. Amos, P.E. and submitted for review on August 15, 2011 for Peaks Island Fuel Company, the plan appears to adequately address the requirements of 40 CFR Part 112.

At some point, I would like to schedule a follow up site visit to ensure that all aspects of the plan have been fully implemented at the facility.

Sincerely,

Butch Bowie
Environmental Specialist
Bureau of Remediation and Waste Management
Division of Technical Services
(207) 287-4804



PORTLAND MAINE

Strengthening a Remarkable City, Building a Community for Life - www.portlandmaine.gov

*Penny St. Louis - Director of Planning and Urban Development
Marge Schmuckal, Zoning Administrator*

512 Island Avenue, Peaks Island 90-
AA-1. 2. 5 – IR-2/I-B Zone

August 18, 2011

The applicant, Mr. Ivers, is showing a vehicle parking lot for seven (7) vehicles located at 512 Island Avenue, Peaks Island. Currently a single family house is located on one portion of the lot. The proposed parking area will be located on another portion of the lot and has sufficient space to park four 2,800 gallon fuel trucks and three passenger-vehicle sized service vehicles. The vehicles are all to be actively used by Mr. Ivers in his heating and fuel oil business. The proposed parking area is shown entirely within the I-B zone.

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delivery business. The other vehicles that will be parked on the site are also for active use with Mr. Ivers' heating repair business. Mr. Ivers' business has been active through four generations and has garnered many clients on Peaks Island.

You have the right to appeal my decision. If you wish to exercise your right to appeal, you have thirty days from the date of this letter in which to appeal. If you should fail to do so, my decision is binding and not subject to appeal. Please contact this office for the necessary paperwork that is required to file an appeal.

Very truly yours,

Marge Schmuckal
Zoning Administrator

Cc: Pennv St. Louis, Director of Planning and Urban Development
Alex Jaegerman, Division Director of Planning
Barbara Barhvdtd, Development Review Service Manager
Eric Giles, Planning
Danielle West-Chuhta, Corporation Counsel
Mike Murray, Island/Neighborhood Liaison

Sec. 14-506. Modifications.

(a) Except for the requirements set forth in sections 14-498 and 14-499 pertaining to the provision and construction of curbs and sidewalks, the Planning Board if it finds that extraordinary conditions exist or that undue hardship may result from strict compliance with these regulations may vary the regulations so that substantial justice may be done and the public interest secured: provided that such variation will not have the effect of nullifying the intent and purpose of the land development plan and the regulations of this article.

(b) Where the Planning Board or planning authority finds that, for each of the requirements listed below, two or more of the conditions exist with respect to compliance with the requirements set forth in sections 14-498 and 14-499 pertaining to the provision and construction of curbs and/or sidewalks, it may waive, in whole or in part, the regulations so that substantial justice may be done and the public interest secured:

Sidewalks-

1. There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site.
2. There is no sidewalk in existence or expected within 1000 feet and the construction of sidewalks does not contribute to the development of a pedestrian oriented infrastructure.
3. A safe alternative-walking route is reasonably and safely available, for example, by way of a sidewalk on the other side of the street that is lightly traveled.
4. The reconstruction of the street is specifically identified and approved in the first or second year of the current Capital Improvement Program or has been funded through an earlier CIP or through other sources.
5. The street has been constructed or reconstructed without sidewalks within the last 24 months.
6. Strict adherence to the sidewalk requirement would result in the loss of significant site features related to landscaping or topography that are deemed to be of a greater public value.

Curbing-

1. The cost to construct the curbing, including any applicable street opening fees, is in excess of 5% of the overall project cost.
2. The reconstruction of the street is specifically identified and approved in the first or second year of the current Capital Improvement Program or has been funded through an earlier CIP or through other sources.
3. The street has been rehabilitated without curbing in the last 60 months.
4. Strict adherence to the curb requirement would result in the loss of significant site features related to landscaping or topography that are deemed to be of a greater public value.
5. Runoff from the development site or within the street does not require curbing for stormwater management.

August 8, 2011

City of Portland Maine Planning Department
c/o Mr. Erick Giles, Planner
389 Congress Street
Portland, ME 04101-3509

**Site Alteration Application – Peaks Island Fuel
Island Avenue, Peak’s Island, Maine**

On behalf of Peaks Island Fuel, we are pleased to submit information pertaining to the Site Alteration Application for their new parking lot. The property is shown as Lots 1, 2 & 5 on the City of Portland Tax Map 90 and is located in the Island Residential 2 (IR2) & Island Business (IB) Districts. The proposed parking area and storage building are located entirely within the IB district.

The proposed improvements consist of an access drive, parking area and potential future 180 SF storage building. The 12' wide access drive will provide a single point of access from Island Ave. to the parking area. The parking area has space to park 4 - 2,800 gallon fuel trucks and three passenger-vehicle-sized service vehicles. The access drive and service vehicle parking will be gravel. The fuel trucks will be parked on a concrete pad.

This project does require a Spill Prevention, Control and Countermeasures Plan. We are working directly with the MDEP – Bureau of Remediation and Waste Management to finalize the details of that plan. We will forward the approved SPCC Plan under a separate cover. The influence of the SPCC Plan regulations can be readily seen on the site plan. As previously mentioned, the oil delivery trucks will be parked on a concrete pad. The pad will be surrounded on three sides by a water-tight curb dike. The Dike will serve as secondary containment for any spills or leaks from the oil trucks. Secondary containment measures need to be sized to contain a volume equal to the largest likely spill volume. In this case, where there is no transfer of materials from one tank to another – the most likely spills would be caused either from a leaky valve or from a leak in the delivery truck’s engine. The secondary containment provides a storage volume well in excess of the required amount.

Mohr & Seredin, Landscape Architects provided the landscaping plan. They also made key contributions to the site plan that will result in a more attractive site. Some key elements of their plan are as follows:

- A curved entrance drive will allow for trees and shrubs (to be planted on the inside of the curve) to block the view of the fuel trucks from Island Avenue.
- The storage building has been placed to block the view of the service vehicles from Island Ave. as well as partially blocking the view of the fuel trucks from those viewing the site from the south and west.

- A 6' high wooden fence will be added around the south and east sides of the delivery truck parking area.
- New trees, bushes and shrubs will be added to the site to beautify and screen the site from neighboring properties.
- Existing vegetation will be preserved as much as possible to provide mature screening. The lower end of the site will be preserved as a wooded buffer. This buffer will be used to filter storm-water runoff.

The applicant is not proposing any signage or lighting at this time.

The site plan standards call for a permanent paved service that is not subject to ponding. The access drive and service vehicle parking area are proposed to be gravel. Water will flow to a swale at the lower corner of the drive aisle that will drain to a ditch turnout buffer. Water will not pond in this portion of the parking area. The oil & propane delivery trucks will be parked on a curb-dike lined concrete pad as part of the site specific SPCC Plan. The pad has been designed to drain toward the 6"-7" high curb-dike. The dike will cause the water to pond to the full depth of the dike. The water within the dike will be inspected prior to release to ensure that no petroleum leakage has occurred. The dike/parking area will be drained via a threaded and capped PVC pipe. Both surfaces are stable and compact and will ultimately drain to a wooded buffer in the rear of the site. A paved parking lot would not provide any additional benefit in terms of functionality or quality of storm-water runoff.

We request a waiver of the following items:

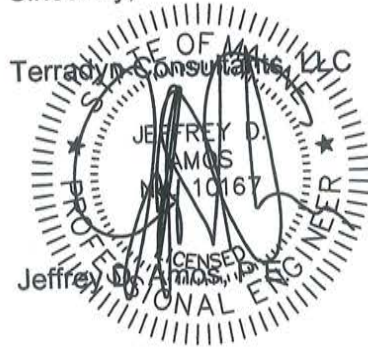
- Bicycle parking – The parking lot is not for public use. There will be no vehicular or pedestrian traffic into the parking area except for employees of Peak's Island Fuel
- Typical driveway width – We are proposing a 12' wide access drive into the parking area. The smaller access drive is a key component of our efforts to better screen the parking area from the surrounding area. Additionally, a narrower entrance drive will help to increase the available buffer between the gravel surface and the adjacent abutter. At this time, Peak's Island Fuel only has one full time driver and one part time driver. There will not be opposing vehicles on the access drive at any time.

The following items are attached to this submittal:

- Civil Plan Set (including Site Plan, Grading & Erosion Control Plan and Construction Details)
- Landscaping Plan prepared by Mohr & Seredin, Landscape Architects
- Stormwater Management Report
- Agenda from the July 28th Peak's Island Council meeting where public meeting was held to discuss the project.
- Letters from Keith Ivers outlining both the public hearing process; specifics about the Peaks Island Fuel operations; as well as a waiver request and a further information on the use of the proposed parking area.

We hope that you find the enclosed information to be helpful in your review of this project. Please call me if you have any questions as you review the enclosed plans and information.

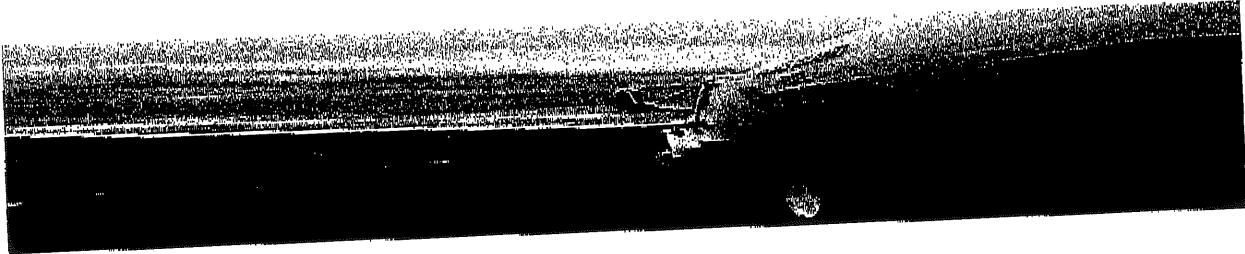
Sincerely,



PEAKS ISLAND FUEL

P.O. BOX 104 PEAKS ISLAND, ME 04108

207-766-5700



Mr. Giles,

This letter is to inform you of last weeks open meeting that was held at the Peaks Island Community Center. A public announcement was sent out through an email list that a large number of the Islanders are part of, it is also the same mailing list that has been used to circulate peoples opinions about my proposed site plan. This email list is what has been used by myself and the oposition since the very first day of the site review plan and that is why it was used to invite everyone to this public meeting.

The meeitng was held on July 28th at 6:30 pm and it was put on by the Peaks Island Council. I have enclosed a copy of the email as it was sent out. I made one hundred (100) copies of the revised site plan and handed them out to everyone in the room and then I proceeded to the front of the room to explain in detail my ideas. I spent about half an hour explaining the plan and my situation in great detail to the packed room of sixty (60) plus people, after which the room was open to questions. Eighteen (18) people raised there hands and seventeen (17) were in support for the site plan. One person wasn't against it but would still like to see it elsewhere but had nothing other that asthetic concerns. A show of unanomouse support followed with applause and cheers and even a few tears. The coucil was unanimous in their support and made the comment that this was the largest crowd they had seen since the secession movement.

I am not a confrontational person by nature but that night I put my fears aside with hopes that certain neighbors, who have plagued my with opposition in every shape and form, would have questions loaded and I would be able to address them. Especially Fred O'Keefe who has said I haven't addressed all of my neighbors. At 6:15 pm he drove past me as I was leaving to head down to the meeting and I thought that he to was on his way, then another couple who has been brought up in opposition drove by as well. Not one of them or anyone else that has sent emails in opposition showed up that night. The council even stated that Mr. O'Keefe usually attends every council meeting. I was a little discouraged especially when I found out that playing cards at the local pub was more important to him than the concern he has for the neighborhood and his property values. But not to single him out, I was discouraged because the council was nice enough to change their agenda and to host this meeting and not one person that has shown opposition, aside from Mr. Haykal, thought the meeting was worth attending. I will give Mr. Haykal credit for showing up and speaking up and I'm glad he did. He has always been a good friend and I don't want to jepordize that but I have to look out for my family and what's right. I feel I have been

more than accomodating towards him and have spent over a thousand dollars having the plans changed over and over again in trying to make him happy but the plan we have now I feel is absolutly brilliant. Mr. Haykal is coming over for diner tonight and a resolution that works for us both will be reached.

My Engineer has worked around the clock on this and he has done a fantastic job to say the least. The final touches were added by the generous donation from my fellow islanders of Mohr & Seredin Landscape Architects, Inc., along with a lot of input from neighbors and other islanders.

If you have any further questions about this public meeting please contact me. Thank you.

Best Regards,

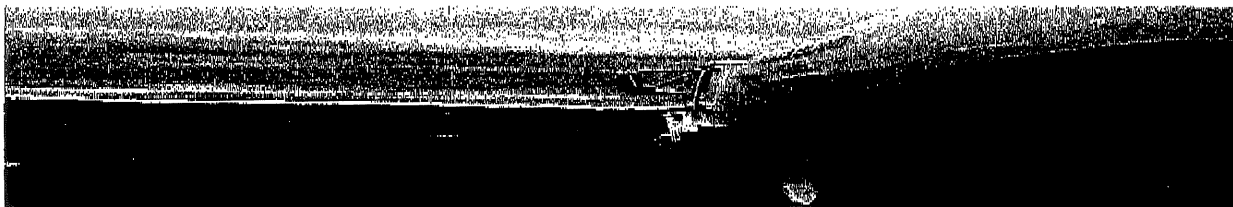


Keith Ivers

PEAKS ISLAND FUEL

P.O. BOX 104 PEAKS ISLAND, ME 04108

207-766-5700



Dear Mr. Giles:

Thank you for your time and consideration of my proposed 4,200 square foot parking area. The area will be used for parking of my fuel oil trucks which hold up to 2,800 gallons of fuel and up to three smaller service vehicles. I currently own four oil trucks but one of which is up for sale and will not be parked on the lot, I also would have one propane truck parked on the lot making a total of three fuel oil trucks and one propane truck. The service vehicles include a single rear wheel service van, service pick up and service flat bed truck none of which store any fuel oil in or on them. As a small business I would be responsible for the management of the lot and as I have only one seasonal driver, who is also a family member, and the lot will be for business use only. The majority of my business takes place from October thru April and that is when the fuel trucks will be on the road the most but no more then they currently are. During the summer months the oil trucks are on the street less than eight hours or one day a week and the same goes for the propane truck. When a fuel truck leaves the lot it is out for the day and only comes back when the deliveries are done and then will be parked so there will not be traffic in and out of the lot all day. My normal operating hours are from eight to five Monday through Friday.

Currently I park the trucks in rented space on Welch St., just above where the boat docks. It is the busiest and most congested street on the island not only in the summer but year round as well. There is no security for my trucks and the lot is often used by the public which not only creates an unsafe situation but it makes it very difficult of operate my business efficiently. Unlike the proposed use of my property, the lot I use now is much closer to the water and it is also a steep grade to the ocean which not only makes it difficult and unsafe in the winter but if a spill was ever to happen it would be much tougher to contain. In the fourteen years I have been with the company there has never been an accident with a vehicle or a fuel spill. On any given day there are thousands of people up and down Welch St. and the front of the island and it would be much safer to have the trucks away from the traffic and down at my own residence. Whether I am pulling out of Welch St. to go to work or if I leave from my own house I am going to and have been traveling past the proposed parking area so there will be no increase in traffic of the area it will simply be where the day ends for the trucks. For the past two years that I have lived at the residence I always drive one of the vehicles home after work and it is parked in front of my house, as there is no off street parking. It is parked there each night until I leave for work in the morning and I have had no problems or complaints so the only difference will be that the trucks will

be off the street and out of the way and out of sight. The original plan had shown two drive ways but if a waiver could be granted I would rather only have one drive way coming in from Island Ave and not access Trefethren St. The trucks would be secure on my lot and as the landscape plans show it will be near impossible for them to be seen and natural screening using trees and shrubs will keep noise down as well. The addition of a naturally raised berm with trees and shrubs planted on top will give added height to the vegetation and create better screening. Also adding trees and or shrubs along the south side of the parking area would block view of the lot when traveling down Island Ave towards Trefethren and from the neighbors up on Island Ave. and Oak Lawn.

I understand that change is tough for everyone and that is why I am committed to making sure every safety precaution is taken and making sure the entire site will become something people come to admire. Honestly, the people in opposition of the project have never seen one set of plans, I have offered and been met with resistance each time. Finally I am having an open meeting in hopes that something constructive can come from it and I can hopefully get some positive feedback from the opposition. I have had nothing but total support from hundreds of people out here, some living directly next door and across the street from me and the proposed sight. Why would I want to build something that was unsafe, ugly and a nuisance to everyone when my family and I are going to be living on the same piece of land? Financially it is the only choice, I currently spend over \$10,000.00 a year to rent the 30' by 50' lot that I am also required to maintain. I feel I'm held captive as it is the only commercially zoned space large enough to house my trucks but it is unsecure and my equipment is treated with total disrespect daily and especially each weekend as I am in the same lot that is used for Reggae Sunday all summer long.

I hope this answers your questions and concerns about my project. All in all it is a small site alteration and all I want to do is park my vehicles in a safe place that I own and this is the only chance I am going to have to be able to do so. Currently there is nothing for sale on the Island that is business zoned and even when there was the least expensive property was over \$500,000. Lastly, I have been in the house for two years and have been bringing these vehicles back and forth daily for work without a single complaint, now I want to move them out of sight when I'm done work and I am met with resistance?

If you have any further questions on the operation of my business please ask as I am always available to talk about this matter. Thank you for your time and consideration.

Best Regards,

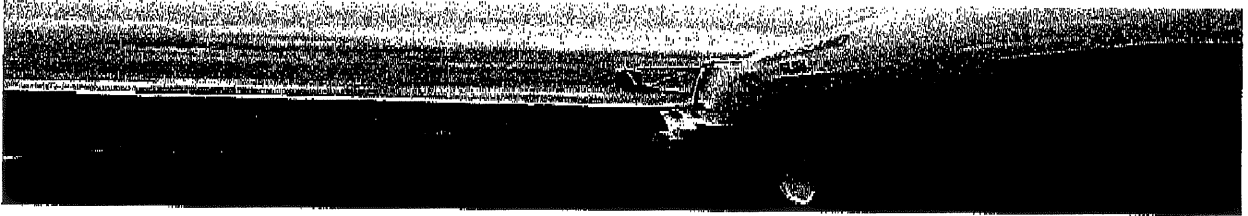

Keith Ivers

Owner/President of Peaks Island Fuel

PEAKS ISLAND FUEL

P.O. BOX 104 PEAKS ISLAND, ME 04108

207-766-6700



Dear Mr. Giles,

This letter is in request for a waiver of the two (2) bicycle spaces required for every zero (0) to ten (10) parking spaces found under 14-526 (a) 4 (b) ii. The seven (7) spaces I have requested are strictly for private use as is the whole lot and in no way will allow for public bicycle access. Thank you for your consideration.

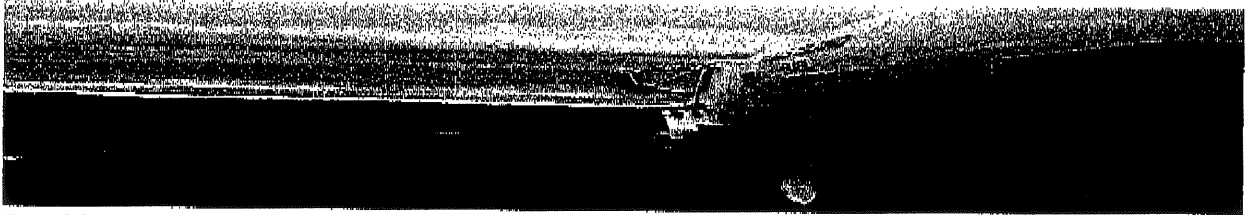
Best Regards,


Keith Ivers

PEAKS ISLAND FUEL

P.O. BOX 104 PEAKS ISLAND, ME 04108

207-766-5700



Dear Marge,

I have read and reviewed sections 14-339, 14-340 and 14-341 in order to supply you with further information on the use of the proposed parking area at 512 Island Ave.

In regards to section 14-339, since the proposed lot is for (7) spaces but does not allow for any of the vehicles to be parked within ten (10) feet of any street line in any direction. The parking lot itself will not abut a residential zone or a lot in residential use but the I-B zone that the proposed lot will be on does on the South side of the lot where it abuts my residence and on the East side where it abuts Mr. Haykals lot. As shown on the plans a sapling fence will extend between the proposed parking area and the abutting lots. The sapling fence will be no less than forty-eight inches in height and well maintained by Peaks Island Fuel. I have a very reputable and local landscape architect who has prepared the landscape plans for me.

In regards to section 14-340, since the proposed parking area is for seven (7) spaces I have taken into consideration for the following: there are no public sidewalks that will need to be crossed. The plans show the appropriate area that the lot will allow for maneuvering the vehicles on the proposed lot and where the driveway from the lot to the street will be. No artificial lighting will be installed on the lot. And the surface of the lot and its construction are all detailed on the refined engineering plans which also show the landscaping additions.

In regards to section 14-341, since the proposed parking area is for seven (7) spaces the aisle area for access to these spaces will be from one side and in excess of the total vehicle length to be parked there. These dimensions can be seen on the engineering plan as well.

I hope this answers your questions so you can make a determination on zoning compliance for the proposed lot. This lot is strictly for private use by my business and I hope that if you have any further questions you will let me know. I have provided you with answers that were carefully thought out and researched as my families home is on the line for this project. Thank you again for all your help and understanding on this project.

Best regards,


Keith Ivers

From: Peggie Peretti <peggieperetti@hotmail.com>
To: joyce doane <jed364pi@aol.com>
Subject: FW: Peaks Island Council meeting, Thursday, 6:30 pm
Date: Wed, Jul 27, 2011 8:25 pm

Subject: Fwd: Peaks Island Council meeting, Thursday, 6:30 pm
From: choppin@aol.com
Date: Wed, 27 Jul 2011 16:42:51 -0400

-----Original Message-----

From: Rusty Foster <rusty@kuroshin.org>
To: Carol I. Eisenberg <CEisenberg@rwlb.com>; Chris Hoppin <CHOPPIN@aol.com>
Sent: Wed, Jul 27, 2011 12:24 pm
Subject: PI Lists: Peaks Island Council meeting, Thursday, 6:30 pm

**Tomorrow night (Thursday, not Wednesday),
July 28th, 6:30 pm
The Community Center**

**The Peaks Island Council
will meet**

To be discussed:

- ~ The application by Peaks Island Fuel to build a parking lot on commercially zoned property near the TEIA club, and the subsequent harassment of Keith Ivers and his family by a small group of opponents to this plan**
- ~ Progress on the sewer inspection, repair, and extension plans**
- ~ Report on Island Transportation Fund expenditures, including the monthly and annual pass discounts, which are now officially in effect**
- ~ A report on what we currently know about public beach**

access laws

~ And a mention, at least, of what the deal is with the airport approach routing, or at least when we will have more clarity on that.

Do join us. It'll be a hootenanny.

STORMWATER MANAGEMENT PLAN

Peak's Island Fuel Peak's Island, Maine

The following Stormwater Management Plan has been prepared for Peak's Island Fuel to evaluate stormwater runoff and erosion control for the proposed parking lot and future storage building to be located off Island Avenue in Peak's Island, Maine.

Site Calculations

Total Property Area	0.78 Ac (+/-) (34,171 SF)
Total New Impervious Area	0.11 Ac (4,907 SF)
Total Disturbed Area	0.28 Ac (12,500 SF)

Existing Conditions

The development property is approximately 0.78 AC and contains a single family home, a lawn and is wooded in the rear of the property. The lower end of the site is between 150'-200' away from Casco Bay and has frontage on the north side of Island Avenue, the west side of Trefethen Avenue and the east side of the unimproved Brimmer Street right-of-way. The property surrounds a small single family house lot that is located on the northwest corner of the Island Avenue/Trefethen Avenue intersection.

The property generally drains from a high point at the Island Avenue/Trefethen Avenue intersection to the lowpoint in the northwest corner of the lot. The top half of the lot contains slopes that are generally between 8%-10%. The lower half is generally between 3%-5%. The property drains to a ditch that runs down the Brimmer Street right-of-way and flows into Casco Bay. A copy of the U.S.G.S. Quadrangle Map (Portland East) is attached to this submittal.

Proposed Development

Peak's Island Fuel is proposing to add a parking lot, gravel access drive, future storage building and landscaping to the property. The new parking lot will be able to fit 4 – 2,800 gallon fuel trucks as well as 3 passenger-car-sized service vehicles. The drive aisle has been sized to allow for easy maneuverability. The fuel trucks will be parked on a curb-lined concrete pad. The curbing joints will be grouted to provide a watertight seal and the pad will serve as secondary containment dike for any potential fuel leaks. The containment area will be drained via a threaded 2" pvc pipe. The containment dike offers enough storage to fully contain all small storm events. The gravel parking area and access drive will drain to a new ditch turnout level spreader/buffer area. The runoff from the neighboring residence as well as portions of Island Avenue will be intercepted

by a 12" pipe in order to limit the size of the watershed that will drain to the level spreader/buffer system.

Flooding

The development area is not located within an area of flood hazard according to the Federal Insurance Rate Map 230051 0015 B. See attached map.

Water Quantity

We are requesting a waiver of the flooding standards. We believe this waiver to be appropriate due to the close proximity of Casco Bay (approximately 150' downstream of the site) as well as the design of the proposed stormwater management system.

Although we propose to provide no formal peak flow rate calculations, we believe that the proposed stormwater management system is not likely to cause an increase in the peak flow rate when compared to the existing condition. Nearly all of the new parking area will be collected and directed to a ditch turnout buffer. The buffer contains the gentlest slopes on the entire property and is located on the only wooded portion of the site. The time of concentration for the existing property would be over 150' of lawn area with an average slope of approximately 10%. This would equate to a $T_c=7.4$ minutes. The developed area will be routed through a 150' wooded buffer with an average slope of approximately 5%. This would equate to a $T_c=37$ minutes. The increase in the T_c would result in smaller peak flows. Furthermore, the concrete slab contains a water-tight curb along the down gradient side that will store a significant amount of runoff – thereby further reducing the post development flow rate.

Onsite Soils

The soils were delineated from the Cumberland County Medium Intensity Soil Survey, as shown (See attached map). The soil survey reports the onsite soils are as summarized below:

A copy of the Medium Intensity Soil Survey has been included with this submittal.

Soil Type Summary Table		
Soil Symbol	Soil Name	HSG
BuB	Buxton	D
HIC	Hinkley	A
HrC	Hollis	C/D
W	Water	

Water Quality (BMP Standard)

Best Management Practices (BMPs) will be implemented to reduce the impacts of the proposed site development on downstream water quality. A ditch turnout level spreader/buffer has been designed to provide the necessary water quality treatment. The impervious and disturbed treatment percentages are detailed below:

New Impervious Area: Approximately 4,907 SF of new impervious area will be created. According to our calculations, 4,767 SF of new impervious area and 268 SF of existing impervious area will flow to the ditch turnout buffer. Taking 50% credit for the treatment of the existing impervious area leaves us with:

$$[4,767 + (268/2)] / 4,907 = 4,901 / 4,907 = 0.9987$$

% of Treatment of the New Impervious Area = 99.9% (95% required)

Project Developed Area: The existing project area is currently developed as lawn. For the purpose of these calculations the developed area is assumed to be the area required to build the access drive, parking area and future storage building plus the lawn area required to match into existing ground at a 3:1 slope. (Note: the total disturbed area encircled all of the potential landscaped areas located along Trefethen Avenue.) Approximately 8,863 SF of developed area will be created including 4,907 SF of new impervious area and 3,956 SF of grassed area. According to our calculations, 8,226 SF of disturbed area will flow the ditch turnout level spreader/buffer. $8,226 / 8,863 = 0.928$

% of Treatment of the Disturbed Area = 92.8% (80% required)

Level Spreader/Buffer Sizing

Ditch Turnout Buffer:

Development Type	
Impervious	5,035 SF
Pervious	3,191 SF

Required Berm length for a forested buffer (from Table 5-4 of BMP Manual) for Soil Group D non wetland:

Per acre of impervious area: 150'

Per acre of lawn: 45'

$$L = (5,035 / 43,560) \times 150' + (3,191 / 43,560) \times 45'$$
$$L = (0.116)(150) + (0.073)(45) = 17.4' + 3.3' = 20.7'$$

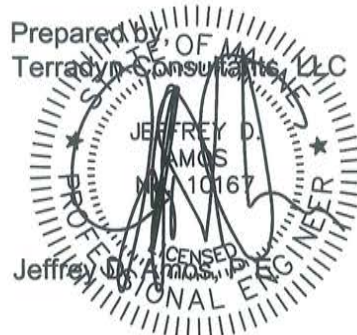
Required Length=20.7'

Proposed Length=21'

Required Length of Flow Path through Buffer = 150' (from Table 5-4 of BMP Manual)
Flow Path Provided = 180'

Summary

Based on the results of this evaluation, the proposed stormwater design is not expected to cause flooding, erosion or other significant adverse effects downstream of the site.





SHEET DESCRIPTION

U.S.G.S. QUADRANGLE MAP
PORTLAND EAST

PREPARED FOR

PEAK'S ISLAND FUEL
P.O. BOX 6
PEAK'S ISLAND, MAINE 04108



Civil Engineering - Land Planning - Stormwater Design - Environmental Permitting

P.O. Box 339
111 Elderberry Lane
New Gloucester, ME 04260
Office: (207) 926-5111
Fax: (207) 221-1317
www.terradyconsultants.com

JOB NO.

1128

DATE

7/28/11

SCALE

1"=2,000'

SHEET

1

OF

1

Hydrologic Soil Group—Cumberland County and Part of Oxford County, Maine
(Peak's Island Fuel)



MAP INFORMATION

Map Scale: 1:1,490 if printed on A size (8.5" x 11") sheet.
The soil surveys that comprise your AOI were mapped at 1:24,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for accurate map measurements.

Source of Map: Natural Resources Conservation Service
Web Soil Survey URL: <http://websoilsurvey.nrcs.usda.gov>
Coordinate System: UTM Zone 19N NAD83

















This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Cumberland County and Part of Oxford County, Maine
Survey Area Data: Version 7, Jan 8, 2009

Date(s) aerial images were photographed: Data not available.

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

MAP LEGEND

Area of Interest (AOI)	
	Area of Interest (AOI)
Soils	
	Soil Map Units
Soil Ratings	
	A
	A/D
	B
	B/D
	C
	C/D
	D
	Not rated or not available
Political Features	
	Cities
Water Features	
	Streams and Canals
Transportation	
	Rails
	Interstate Highways
	US Routes
	Major Roads
	Local Roads

Hydrologic Soil Group

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
BuB	Buxton silt loam, 3 to 8 percent slopes	D	3.1	57.7%
HIC	Hinckley gravelly sandy loam, 8 to 15 percent slopes	A	0.7	12.6%
HrC	Hollis fine sandy loam, 8 to 15 percent slopes	C/D	1.4	26.5%
W	Water		0.2	3.2%
Totals for Area of Interest			5.4	100.0%

Description

Hydrologic soil groups are based on estimates of runoff potential. Soils are assigned to one of four groups according to the rate of water infiltration when the soils are not protected by vegetation, are thoroughly wet, and receive precipitation from long-duration storms.

The soils in the United States are assigned to four groups (A, B, C, and D) and three dual classes (A/D, B/D, and C/D). The groups are defined as follows:

Group A. Soils having a high infiltration rate (low runoff potential) when thoroughly wet. These consist mainly of deep, well drained to excessively drained sands or gravelly sands. These soils have a high rate of water transmission.

Group B. Soils having a moderate infiltration rate when thoroughly wet. These consist chiefly of moderately deep or deep, moderately well drained or well drained soils that have moderately fine texture to moderately coarse texture. These soils have a moderate rate of water transmission.

Group C. Soils having a slow infiltration rate when thoroughly wet. These consist chiefly of soils having a layer that impedes the downward movement of water or soils of moderately fine texture or fine texture. These soils have a slow rate of water transmission.

Group D. Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clays that have a high shrink-swell potential, soils that have a high water table, soils that have a claypan or clay layer at or near the surface, and soils that are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

If a soil is assigned to a dual hydrologic group (A/D, B/D, or C/D), the first letter is for drained areas and the second is for undrained areas. Only the soils that in their natural condition are in group D are assigned to dual classes.

Rating Options

Aggregation Method: Dominant Condition

Component Percent Cutoff: None Specified

Tie-break Rule: Higher



APPROXIMATE SCALE



NATIONAL FLOOD INSURANCE PROGRAM

FIRM
FLOOD INSURANCE RATE MAP

CITY OF
PORTLAND, MAINE
CUMBERLAND COUNTY

PANEL 15 OF 17
(SEE MAP INDEX FOR PANELS NOT PRINTED)

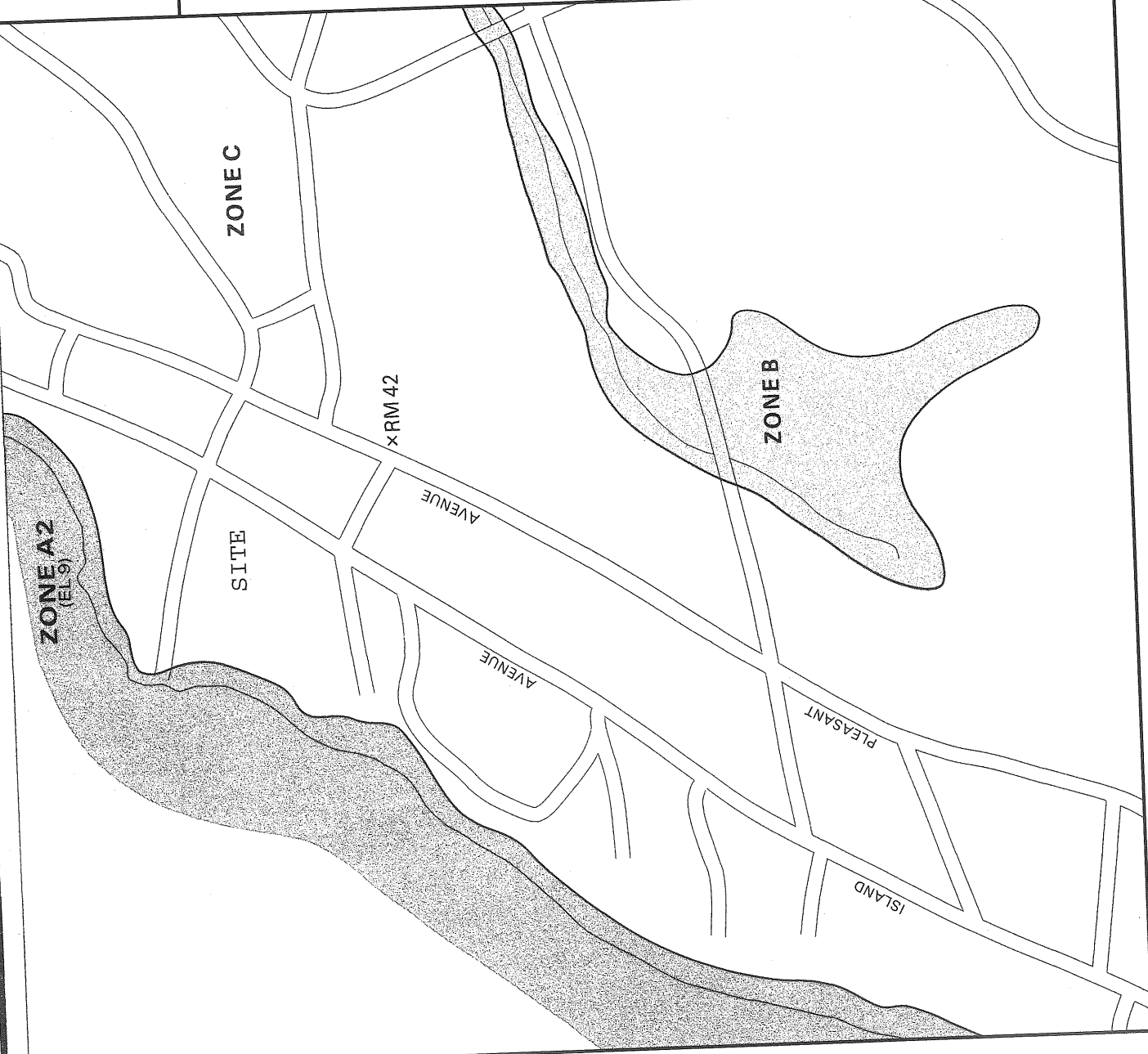
COMMUNITY-PANEL NUMBER
230051 0015 B

EFFECTIVE DATE:
JULY 17, 1986



Federal Emergency Management Agency

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at www.msc.fema.gov



**MAINTENANCE PLAN OF STORMWATER MANAGEMENT FACILITIES
FOR:
Peak's Island Fuel
Peak's Island Fuel, Maine**

Land Owner: Mr. Keith Ivers

Project Developer: Mr. Keith Ivers / Peak's Island Fuel
P.O. Box 6
Peak's Island Fuel, ME 04108

Responsible Party: Peak's Island Fuel

List of Stormwater Measures:

Vegetated Areas
Conveyance & Distribution System (Stormwater Channels & Culverts)
Level Spreaders
Buffers

Inspection & Maintenance Tasks:

Although not required by the MDEP Chapter 500 or the City of Portland stormwater regulations, we recommend that the project developer follow the following inspection and maintenance guidelines.

Conveyance & Distribution Systems: (Stormwater Channels & Culverts, etc.)

- 1. Mowing:** Grass should not be trimmed extremely short, as this will reduce the filtering effect of the swale (MPCA, 1989). The cut vegetation should be removed to prevent the decaying organic litter from adding pollutants to the discharge from the swale. The mowed height of the grass should be 2-4 inches taller than the maximum flow depth of the design water quality storm. A minimum mow height of 6 inches is generally recommended (Galli, 1993).
- 2. Routine Maintenance and Inspection:** The area should be inspected for failures following heavy rainfall and repaired as necessary for newly formed channels or gullies, reseeding/sodding of bare spots, removal of trash, leaves and/or accumulated sediments, the control of woody or other undesirable vegetation and to check the condition and integrity of the check dams.
- 3. Aeration:** The buffer strip may require periodic mechanical aeration to restore infiltration capacity. This aeration must be done during a time when the area can be reseeded and mulched prior to any significant rainfall.
- 4. Erosion:** It is important to install erosion and sediment control measures to stabilize this area as soon as possible and to retain any organic matter in the bottom of the trench.
- 5. Fertilization:** Routine fertilization and/or use of pesticides is strongly discouraged. If complete re-seeding is necessary, half the original recommended rate of fertilizer should be applied with a full rate of seed.
- 6. Sediment Removal:** The level of sediment deposition in the channel should be monitored regularly, and removed from grassed channels before permanent damage is done to the

grassed vegetation, or if infiltration times are longer than 12 hours. Sediment should be removed from riprap channels when it reduces the capacity of the channel.

Level Spreaders:

Long term maintenance of the level spreader is essential to ensure its continued effectiveness. The following provisions should be followed. In the first year the level spreader should be inspected semi annually and following major storm events for any signs of channelization and should be immediately repaired. After the first year, annual inspection should be sufficient. Vegetated level spreaders may require periodic mowing. Spreaders constructed of wood, asphalt, stone or concrete curbing also require periodic inspection to check for damage and to be repaired as needed.

- 1. Inspections:** At least once a year, the level spreader pool should be inspected for sand accumulation and debris that may reduce its capacity.
- 2. Maintenance Access:** Level spreaders should be sited to provide easy access for removal of accumulated sediment and rehabilitation of the berm.
- 3. Sediment Removal:** Sediment build-up within the swale should be removed when it has accumulated to approximately 25% of design volume or channel capacity. Dispose of the sediments appropriately.
- 4. Debris:** As needed remove debris such as leaf litter, branches and tree growth from the spreader.
- 5. Mowing:** Vegetated spreaders may require mowing.
- 6. Snow Storage:** Do not store snow removed from the street and parking lot within the area of the level spreader.
- 7. Level Spreader Replacement:** The reconstruction of the level spreader may be necessary when sheet flow from the spreader becomes channeled into the buffer.

Buffers:

1. Inspect resource and treatment buffers at least once a year for evidence of erosion, concentrating flow, and encroachment by development.
2. Management of a buffer's vegetation must be consistent with the requirements in any deed restrictions for the buffers.
3. Wooded buffers must remain fully wooded and have no disturbance to the duff layer.
4. Vegetation in non-wooded buffers must be cut no more than three times per year and no shorter than six inches.
5. Erosion within a buffer must be repaired as soon as practicable. If flows are concentrating within the buffer, site grading, level spreaders, or ditch turn-outs must be used to ensure a more even distribution of flow into the buffer.
6. Check downslope of all spreaders and turn-outs for erosion. If erosion is present, adjust or modify the spreader's or turnout's lip to ensure a better distribution of flow into the buffer.
7. Clean-out any accumulation of sediment within the spreader bays or turn-out pools.

Task Frequency:

Table 11-1 Long-Term Inspection & Maintenance Plan				
	Spring	Fall or Yearly	After a Major Storm	Every 2-5 Years
Vegetated Areas				
Inspect all slopes and embankments	X		X	
Replant bare areas or areas with sparse growth	X		X	
Armor areas with rill erosion with an appropriate lining or divert the ero-sive flows to on-site areas able to withstand concentrated flows. See Appendix A(5) of Rule.	X		X	
Stormwater Channels				
Inspect ditches, swales and other open stormwater channels	X	X	X	
Remove any obstructions and accumulated sediments or debris	X	X		
Control vegetated growth and woody vegetation		X		
Repair any erosion of the ditch lining		X		
Mow vegetated ditches		X		
Remove woody vegetation growing through riprap		X		
Repair any slumping side slopes		X		
Replace riprap where underlying filter fabric or underdrain gravel is showing or where stones have dislodge		X		
Culverts				
Remove accumulated sediments and debris at the inlet, at the outlet, and within the conduit	X	X	X	
Repair any erosion damage at the culvert's inlet and outlet	X	X	X	
Roadways and Parking Surfaces				
Clear accumulated winter sand in parking lots and along roadways	X			
Sweep pavement to remove sediment	X			
Grade road shoulders and remove excess sand either manually or by a front-end loader	X			
Grade gravel roads and gravel shoulders	X			
Clean-out the sediment within water bars or open-top culverts	X			
Ensure that stormwater is not impeded by accumulations of material or false ditches in the shoulder	X			

**Table 11-1
Long-Term Inspection & Maintenance Plan**

	Spring	Fall or Yearly	After a Major Storm	Every 2-5 Years
Buffers				
Inspect treatment buffers for evidence of erosion, concentrated flow, or encroachment by development		X		
Manage the buffer's vegetation with the requirements in any deed restrictions		X		
Mow vegetation in non-wooded buffers no shorter than six inches and less than three times per year		X		
Repair any sign of erosion within a buffer		X		
Inspect and repair down-slope of all spreaders and turn-outs for erosion		X		
Install more level spreaders, or ditch turn-outs if needed for a better distribution of flow		X		
Clean-out any accumulation of sediment within the spreader bays or turnout pools		X		
Stormwater Detention and Retention Facilities				
Inspect the embankments for settlement, slope erosion, internal piping, and downstream swamping. A professional engineer must review these immediately.		X	X	
Mow the embankment to control woody vegetation		X		
Inspect the outlet control structure for broken seals, obstructed orifices, and plugged trash racks		X	X	
Remove and dispose of sediments and debris within the control structure		X		
Repair any damage to trash racks or debris guards		X		
Mow vegetated spillways to control woody vegetation and replace any dislodged stone in riprap spillways		X		
Remove and dispose of accumulated sediments within the impoundment and forebay				X
Runoff Infiltration Facilities				
Inspect and clean-out any pre-treatment measures that collect sediment and hydrocarbons entering an infiltration measure	X	X		
Provide for the removal and disposal of accumulated sediments within the infiltration area				X
Renew the infiltration measure if it fails to drain within 72 hours after a rainfall of one-half inch or more				X
Till and replant the soil of vegetated infiltration basins				X
Reconstruct rock-lined basins or stone-filled trenches by removing the stones, replacing new underlying filter fabric, and tilling or removing the underlying soil				X
Other Practices and Measures				
Contact the department for appropriate inspection and maintenance requirements for other drainage control and runoff treatment measures.				

HOUSEKEEPING PERFORMANCE STANDARDS

FOR:

Peak's Island Fuel
Peak's Island Fuel, Maine

Land Owner: Mr. Keith Ivers

Project Developer: Mr. Keith Ivers
Peak's Island Fuel
P.O. Box 6
Peak's Island Fuel, ME 04108

Responsible Party: Peak's Island Fuel

Introduction:

The contractor shall be responsible for maintaining proper housekeeping standards throughout the construction phase of the project. After the construction phase has been completed, the owner or operator of the project and the homeowners association will be responsible.

Standards:

In accordance with the housekeeping performance standards required by MDEP chapter 500 stormwater regulations, the following standards shall be met:

- 1. Spill prevention.** Controls must be used to prevent pollutants from being discharged from materials on site, including storage practices to minimize exposure of the materials to stormwater, and appropriate spill prevention, containment, and response planning and implementation.
- 2. Groundwater protection.** During construction, liquid petroleum products and other hazardous materials with the potential to contaminate groundwater may not be stored or handled in areas of the site draining to an infiltration area. An "infiltration area" is any area of the site that by design or as a result of soils, topography and other relevant factors accumulates runoff that infiltrates into the soil. Dikes, berms, sumps, and other forms of secondary containment that prevent discharge to groundwater may be used to isolate portions of the site for the purposes of storage and handling of these materials.
- 3. Fugitive sediment and dust.** Actions must be taken to ensure that activities do not result in noticeable erosion of soils or fugitive dust emissions during or after construction. Oil may not be used for dust control.

Operations during wet months that experience tracking of mud off the site onto public roads should provide for sweeping of road areas at least once a week and prior to significant storm events. Where chronic mud tracking occurs, a stabilized construction entrance should be provided. Operations during dry months, that experience fugitive dust problems, should wet down the access roads once a week or more frequently as needed.

- 4. Debris and other materials.** Litter, construction debris, and chemicals exposed to stormwater must be prevented from becoming a pollutant source.

To prevent these materials from becoming a source of pollutants, construction and post-construction activities related to a project may be required to comply with

applicable provision of rules related to solid, universal, and hazardous waste, including, but not limited to, the Maine solid waste and hazardous waste management rules; Maine hazardous waste management rules; Maine oil conveyance and storage rules; and Maine pesticide requirements.

- 5. Trench or foundation de-watering.** Trench de-watering is the removal of water from trenches, foundations, coffer dams, ponds, and other areas within the construction area that retain water after excavation. In most cases the collected water is heavily silted and hinders correct and safe construction practices. The collected water must be removed from the ponded area, either through gravity or pumping, and must be spread through natural wooded buffers or removed to areas that are specifically designed to collect the maximum amount of sediment possible, like a cofferdam sedimentation basin. Avoid allowing the water to flow over disturbed areas of the site. Equivalent measures may be taken if approved by the department.
- 6. Non-stormwater discharges.** Identify and prevent contamination by non-stormwater discharges.



PORTLAND MAINE

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Planning and Urban Development Department
Penny St. Louis, Director

Planning Division
Alexander Jaegerman, Director

JULY 15, 2011

PEAKS ISLAND FUEL
KEITH IVERS
BOX 6
PEAKS ISLAND, ME 04108

TERRADYN CONSULTANTS, LLC
JEFFERY D. AMOS, P.E.
111 ELDERBERRY LANE
NEW GLOUCSTER, ME 04260

RE: Review Comments for Preliminary Plan – Level I: Site Alteration Application

Project Name:	PEAKS ISLAND FUEL	Project ID:	2011-287
Address:	512 ISLAND AVE.	CBL:	112-H-1
Applicant:	KEITH IVERS		
Planner:	ERICK GILES		

Dear Mr. Ivers:

Thank you for submitting a site alteration application for the Peaks Island Fuel parking lot at 512 Island Ave. Our meetings with you both have been productive and we have provided some guidance as to how the plan might be further developed to meet applicable standards. This letter serves to aid you in preparing a complete submission and reiterates some of the review issues we have noted in our meetings that are specific to this highly visible project.

This proposal is being reviewed as a preliminary plan requiring an administrative review and subject to the standards of Site Plan Ordinance, Article V of Portland's Land Use Code. The applicable site plan standards for a Level I: Site Alteration Plan include the following:

Section 14-526

(a) Transportation Standards: *1) Impact on Surrounding Street Systems; 2) Access and Circulation, and 4) Parking.*

(b) Environmental Quality Standards: *1) Preservation of Significant Natural Features, and 3 Water Quality, Stormwater Management and Erosion Control.*

(c) Public Infrastructure and Community Safety Standards: *1) Consistency with City Master Plans.*

(d) Site Design Standards: 5) Historic Resources 6) Exterior Lighting, 8) Signage and Wayfinding, and 9) Zoning Related Design Standards

Preliminary Plan Staff Review Comments

A. Zoning Administrator Marge Schmuckal

This is a proposal to create a small, gravel parking lot for up to 7 vehicles. Currently there is a single family dwelling located along Island Avenue. The lot consists of three separate parcels. The lot is divided by two separate zones: the IR-2 zone and the I-B zone. A portion of the outer edge of the 250' Shoreland Zone also extends into the property. The dwelling is located in the IR-2 zone and the proposed parking lot is located within the I-B zone.

Currently up to seven (7) vehicles are proposed to be parked upon the lot within the I-B zone. The applicant should review sections 14-339 and 14-340 and 14-341 to supply further information before I can make a final determination on zoning compliance. It is my understanding that there will be some refinements to the application.

B. Planner Erick Giles

The proposal is for a 4,700 square foot gravel parking area in the Island Business (I-B) zone with two access driveways. Based upon a review of the plan submitted and our site visit on July 1, 2011, we are seeking additional information and offer the following comments:

1. Please provide a project description that outlines the number and types of trucks to be parked at this facility. Please include management information about the use, time of activity, seasonal variations, and other relevant information pertaining to this application.
2. A grading and drainage plan prepared by a licensed professional engineer is required. The plan shall show the following:
 - a. Existing and proposed grading and contours. Include the amount of fill that may be proposed for this project.
 - b. Proposed Stormwater Management and erosion controls. The drainage plan must address the Chapter V Stormwater Management requirements of Portland's Technical Manual. David Senus, Consulting Civil Engineer, provides additional guidance below.
 - c. Total area and limits of proposed land disturbance.
 - d. Provide information on the erosion and sedimentation control measures proposed for installation during construction.
3. A landscape plan is required that addresses the preservation of existing vegetation and proposed site landscaping fencing, and buffering of the parking area. You

should give careful thought to screening to minimize the visual impact from abutting neighbors and streets.

4. The proposed gravel parking lot is shown with two curb cuts: one on Island Avenue; and one on Trefethren.
 - a. According to public services, the winter maintenance of Trefethren is a lower priority than other streets on the Island. We recommend that you consider one entrance on Island Avenue. A waiver of the typical driveway width in the Technical Manual could be considered, since this is not a parking lot for public use.
 - b. The site plan standards call for a permanent paved service that is not subject to ponding. Please address how the gravel lot will meet this standard.
 - c. The site plan ordinance requires bicycle parking and calls for two bicycle spaces for every 10 required spaces. You may include a written request for a waiver from this standard, which is found under 14-526 (a) 4 (b) ii.
5. The design detail and photometrics for any proposed exterior lighting must be submitted for review. Full cut-off fixtures are required and the lighting standards are found in Portland Technical Manual, Chapter 12.
6. If you are considering any signage on the site, please include the dimensions, location and content of any proposed signs.
7. Please submit evidence of all required state and or federal approvals. Specifically, submit business management and project design information regarding compliance with Maine Department Environmental Protection – Bureau of Remediation and Waste Management rules. This should include a written determination of applicability from the Spill Prevention, Control and Countermeasures Program. In addition, any safeguards or management practices that you will employ to avoid and respond to leaks or spills should be described.

C. Stormwater Engineer Dave Senus, Woodard and Curran

Woodard & Curran has reviewed the Level I Site Alteration submission for 512 Island Avenue.

Documents Provided

- “Plan Showing a Standard Boundary and Topographic Survey,” dated May 29, 2011, prepared by Austin Land Surveying on behalf of Keith Ivers.

Comments

At this time, the Applicant has not provided a stormwater management plan and engineering design plans meeting the requirements of the Basic, General and Flooding Standards (Section 5 of the City of Portland Technical Manual).

- The project is required to meet the Basic, General and Flooding standards outlined in Section 5 of the Technical Manual (City of Portland Land Use Code, Article V. Site Plan, Section 14-524(a)2.d.(ii)(b), Level 1 Site Alteration Plans are required to conform with certain site plan standards contained in Section 14-526, including Environmental Quality Standards outlined in Section 14-526(b) 3 – Water Quality, Stormwater Management and Erosion Control).
- A Level I Site Plan Submission is required to include proposed stormwater management control and a soil erosion control plan (Article V. Site Plan, Section 14-527(b) (12) and (13))

We await the submittal of engineering design plans and a stormwater management plan to perform our review relative to water quality standards.

Revised Plans

Please submit one hard copy and upload a digital set of the revised plans and documents to address staff comments. Upon receipt of the revised material, the City of Portland will review the additional plans and information for conformance with applicable ordinances. If you have any questions, feel free to contact me at 874-8723 or by email at egiles@portlandmaine.gov

Sincerely,

Erick Giles
Planner

Electronic Distribution:

Penny St. Louis, Director, Department of Planning and Urban Development
 Alexander Jaegerman, Planning Division Director
 Barbara Barhydt, Development Review Services Manager
 Danielle West-Chuhta, Associate Corporation Counsel
 Marge Schmuckal, Zoning Administrator
 David Margolis-Pineo, Deputy City Engineer
 Chris Pirone, Fire Department
 Jeff Tarling, City Arborist
 Tom Errico, Wilbur Smith Consulting Engineers
 David Senus, Woodard & Curran

City of Portland
 Development Review Application
 Planning Division Transmittal form

Application Number: 2011-277 **Application Date:** 6/6/2011 12:00:00 AM
CBL: 90-AA-1
Project Name: Site Alteration
Address: 512 Island Avenue, Peaks Island, ME

Project Description: Site Alteration
Zoning:
Other Reviews Required:
Review Type: Level 1 Site Alteration

Distribution List:

<input type="checkbox"/> Planner	Erick Giles	<input type="checkbox"/> Parking	John Peverada
<input type="checkbox"/> Zoning Administrator	Marge Schmuckal	<input type="checkbox"/> Design Review	Alex Jaegerman
<input type="checkbox"/> Traffic	Tom Errico	<input type="checkbox"/> Corporation Counsel	Danielle West-Chuhta
<input type="checkbox"/> Stormwater	Dan Goyette	<input type="checkbox"/> Sanitary Sewer	John Emerson
<input type="checkbox"/> Fire Department	Keith Gautreau	<input type="checkbox"/> Inspections	Tammy Munson
<input type="checkbox"/> City Arborist	Jeff Tarling	<input type="checkbox"/> Historic Preservation	Deb Andrews
<input type="checkbox"/> Engineering	David Margolis-Pineo	<input type="checkbox"/> Outside Agency	
		<input type="checkbox"/> DRC Coordinator	Phil DiPierro

Preliminary Comments needed by: June 22, 2011

Final Comments needed by: June 29, 2011

300 DATA/SAR/11/2010
874-8699

Effective August 18, 2010



Level I – Site Alteration Development Review Application Portland, Maine

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Level I: Site Alteration site plan.

Level I: Site Alteration Development includes:

- Alteration of a watercourse or wetland as defined in Section 14-47 of the City Code.
- Alteration of a site. The disturbance of land areas of less than one (1) acre that are stripped, graded, grubbed, filled or excavated. The Planning Authority shall exempt from review the loam and seeding of lawns and the cumulative placement of less than fifteen (15) cubic yards of fill on any lot provided such loaming or placement does not alter a drainage course, swale, wetland or redirect water onto adjoining property and does not violate any other provision of the Portland City Code or state or federal law. "Disturbed area" does not include routine maintenance, but does include re-development and new impervious areas.
- The construction of any temporary or permanent parking area, paving of existing unpaved surface parking areas between 1,000 and 7,500 square feet, or creation of other impervious surface areas between 1,000 and 7,500 square feet.
- The rehabilitation or reconstruction, but not new construction, of piers, docks, wharves, bridges, retaining walls, and other structures located within the shoreland zone.
- A site alteration in which vehicle access is proposed from more than one (1) street;

The Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division
Fourth Floor, City Hall
389 Congress Street
(207) 874-8721

Office Hours
Monday thru Friday
8:00 a.m. – 4:30 p.m.

PROJECT NAME: PEAKS ISLAND FUEL

PROPOSED DEVELOPMENT ADDRESS:
512 ISLAND AVE, PEAKS ISLAND, ME 04108

PROJECT DESCRIPTION:
UTILIZE COMMERCIAL PROPERTY FOR PARKING OF DELIVERY AND SERVICE TRUCKS, FILL IN SITE AND ADD TWO DRIVEWAYS

CHART/BLOCK/LOT: MAP 90, BLOCK AA, LOTS 1, 2 AND 5

CONTACT INFORMATION:

Applicant – must be owner, Lessee or Buyer Name: <u>KEITH IVERS</u> Business Name, if applicable: <u>PEAKS ISLAND FUEL</u> Address: <u>BOX G</u> City/State: <u>PEAKS ISLAND, ME</u> Zip Code: <u>04108</u>	Applicant Contact Information Work # <u>207-749-5700</u> Home# <u>207-749-5700</u> Cell # <u>207-749-5700</u> Fax# <u>207-766-2969</u> e-mail: <u>VAS92@AOL.COM</u>
Owner – (if different from Applicant) Name: Address: City/State : Zip Code:	Owner Contact Information Work # Home# Cell # Fax# e-mail:
Agent/ Representative Name: Address: City/State : Zip Code:	Agent/Representative Contact information Work # Cell # e-mail:
Billing Information Name: <u>KEITH IVERS</u> Address: <u>BOX G</u> City/State: <u>PEAKS ISLAND, ME</u> Zip Code: <u>04108</u>	Billing Information Work # <u>207-749-5700</u> Cell # Fax# <u>207-766-2969</u> e-mail: <u>VAS92@AOL.COM</u>

Engineer		Engineer Contact Information	
Name:		Work #	
Address:		Cell #	Fax#
City/State :	Zip Code:	e-mail:	
Surveyor		Surveyor Contact Information	
Name:	BILL AUSTIN	Work #	
Address:	82 ELDERBERRY LANE	Cell #	207-632-2959
City/State :	NEW BRUNSWICK, ME	Fax#	
	Zip Code: 04260	e-mail:	ALS2174@GMAIL.COM

e-plan contact

APPLICATION FEES:

Check all reviews that apply. Payment may be made by Check or Cash addressed to the City of Portland.

<p>Level I Site Alteration Site Plan</p> <p><input checked="" type="checkbox"/> Application Fee (\$200.00)</p> <p>The City invoices separately for the following:</p> <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) <p>Third party review is assessed separately.</p>	<p>Fees Paid (office use)</p> <p>—</p>
<p>Performance Guarantee: A performance guarantee is required to cover all public and private site improvements.</p>	<p>Required</p>
<p>Inspection Fee: An inspection fee of 2% of the performance guarantee is due prior to the release of permits</p>	<p>2% of the performance guarantee</p>

Application Check List


Refer to the application checklist for a detailed list of submittal requirements.

As of December 1, 2010, all site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521).

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Site Plan review only, a Performance Guarantee, Inspection Fee, Building Permit Application and associated fees will be required prior to construction.

Signature of Applicant: 	Date: 5 - - 2011
--	---------------------

Please refer to Article V, Site Plan of the City of Portland Land Use Code for detailed information concerning the City's site plan review process, thresholds and standards. Should you have any questions regarding the submittal requirements or any other aspect of the site plan review process, please contact the Planning Division.

PROJECT DATA

The following information is required where applicable, in order complete the application

Total Site Area	39093 total 22310 (UNIMETRICAL) sq. ft.
Proposed Total Disturbed Area of the Site	4200-4500 sq. ft.
IMPERVIOUS SURFACE AREA	
• Proposed Total Paved Area / GRAVEL	4200 sq. ft.
• Existing Total Impervious Area	0 sq. ft.
• Proposed Total Impervious Area	4200 sq. ft.
• Proposed Impervious Net Change	4200 sq. ft.
PARKING SPACES	
• Existing Number of Parking Spaces	0
• Proposed Number of Parking Spaces	7
TOTAL Number of Parking Spaces	7

General Submittal Requirements – Level I Site Alteration

Applicant Checklist	Planner Checklist	Number of Paper Copies	Submittal Requirement
<input type="checkbox"/>	<input checked="" type="checkbox"/>	1	Completed application form.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	1	Application fees.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written description of project.
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of right, title and interest.
<input type="checkbox"/>	<input type="checkbox"/>	1	Copies of required state and/or federal permits.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of zoning.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written description of existing and proposed easements or other burdens.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written requests for waivers from individual site plan and/or technical standards.
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of financial and technical capacity.

Site Plans and Boundary Survey Requirements – Level I Site Alteration

Applicant Checklist	Planner Checklist	Number of Copies	Submittal Requirement
<input type="checkbox"/>	<input checked="" type="checkbox"/>	1	Boundary Survey meeting the requirements of Section 13 of the City of Portland Technical Manual.
<input type="checkbox"/>	<input type="checkbox"/>	1	Site Plan including the following:
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Existing structures with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone)
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas.
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Location and details of proposed infrastructure improvements (e.g. - curb and sidewalk improvements, utility connections, roadway improvements).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Identification of and proposed protection measures for any significant natural features on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code.
<input type="checkbox"/>	<input type="checkbox"/> N/A		▪ Details of proposed pier rehabilitation (Shoreland areas only).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing utilities.
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Existing and proposed grading and contours.
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Proposed stormwater management and erosion controls.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Total area and limits of proposed land disturbance.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing vegetation to be preserved and proposed site landscaping.
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Existing and proposed easements or public or private rights of way.

Site Plan Standards for Review of Level I: Site Alteration

Level I: Site alteration plans shall only be subject to the following site plan standards, as applicable, as contained in section 14-526:

- (a) Transportation standards
 - 1, 2, and 4.
- (b) Environmental quality standards
 - 1. and 3.
- (c) Public infrastructure and community safety standards.
 - 1.
- (d) Site design standards
 - 5, 6, 8 and 9.

Except as provided in article III, or to conditions imposed under section 14-526(e) only, or to those submission requirements set forth in section 14-527 as relate solely thereto.



PORTLAND FIRE DEPARTMENT SITE REVIEW FIRE DEPARTMENT CHECKLIST



A separate drawing[s] shall be provided to the Portland Fire Department for all site plan reviews.

1. Name, address, telephone number of applicant.
2. Name address, telephone number of architect
3. Proposed uses of any structures [NFPA and IBC classification]
4. Square footage of all structures [total and per story]
5. Elevation of all structures
6. Proposed fire protection of all structures
 - **As of September 16, 2010 all new construction of one and two family homes are required to be sprinkled in compliance with NFPA 13D. This is required by City Code. (NFPA 101 2009 ed.)**
7. Hydrant locations



PEAKS ISLAND FUEL

P.O. BOX 104

PEAKS ISLAND, ME 04108

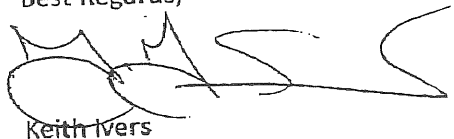
207-766-5700 OFFICE

207-766-2969 FAX

To whom it may concern;

This letter is to describe the proposed project for 516 Island ave on Peaks Island, ME 04108. The current site is relatively flat with a slightly sloping terrain. Aside from two small pieces of vegetation it is all grass at the moment. The plan is to slightly raise a small portion of the yard and form a small parking area for up to seven service vehicles. This will include having two driveways put in for access to the property, one from Island ave and the other coming in from Trefethen ave. The lots height will begin close to the original grade of the lot towards Trefethen ave and carry appropriate pitch towards Brimmer st for approx 82' and towards the northwest of the property for approx 50'. The total disturbed area will be around 4,200 square feet. The top soil will be removed and appropriate fill brought in to form a suitable sub base for the lot. Remaining fill will be composed of crushed stone and topped off with gravel for a parking surface. Culverts will be put in place at each location were there would be a possibility for water to catch so it can continue on its natural path. The proposed are has been surveyed and a plan has been provided to show its location in the Commercial zone portion of the lot and that it is designed for minimal disruption of the area. The main objective of the project is to consolidate my business to one area instead of renting three different areas . It will give my family's business a permanent foundation so we can continue the business for many years to come. Thank you.

Best Regards,



Keith Ivers

Owner/ President Peaks Island Fuel

Exhibit A - Property Description

Certain lots or parcels of land, situated on Peaks Island in Portland, County of Cumberland and State of Maine, on the northwesterly side of Island Avenue, and bounded and described as follows:

Beginning at a point on the northwest sideline of Island Avenue, which point is ninety (90) feet southwest of the intersection of Island Avenue and Trefethen Avenue; thence northwest along land now or formerly of Donald S. Webber, a distance of sixty (60) feet to a point; thence northeast along said Webber land, a distance of ninety (90) feet, to an iron monument set in the ground on the southwest sideline of Trefethen Avenue. Said iron monument being sixty (60) feet northwest of the intersection formed by the northwest sideline of Island Avenue and Trefethen Avenue; thence northwest along the southwest sideline of Trefethen Avenue, a distance of one hundred and fifty-eight (158) feet, to a point; thence turning and running southwest along land now or formerly of Lawrence Burke, a distance of one hundred two and seven-tenths (102.7) feet, to a point; thence South 45° 47' West, along land now or formerly of Edgar B. Allen, a distance of one hundred and sixteen hundredths (100.16) feet, to a point on the north sideline of Brimmer Street; thence South 57° 33' East, along said sideline of Brimmer Street a distance of one hundred ninety-four and seventy-three hundredths (194.73) feet, to a point, which point is the intersection of the northeast corner of Brimmer Street and the northwest side of Island Avenue; thence North 50° 20' East, along the northwest sideline of Island Avenue, a distance of ninety-nine and forty-four hundredths (99.44) feet, to the point of beginning.

Meaning and intending to convey by this deed the lot numbered 1 on a plan by Raymond J. Mercer for Wendell P. Wright dated June 2, 1964, and recorded in the Cumberland County Registry of Deeds in Plan Book 69, Page 36, to which plan reference is hereby made for a more accurate description.

Reference may also be made to a deed from Howard to Wright, dated March 31, 1964, in said Registry of Deeds in Book 2812, Page 161. This deed herein conveyed also includes land conveyed to Grantors from D. Webber, said deed dated July 12, 1978, in said Registry of Deeds in Book 4259, Page 76.

1957 DEC 31 AM 9:42

CUMBERLAND COUNTY

John B. Coburn

Offer to Purchase Real Estate
THIS OFFER TO PURCHASE REAL ESTATE dated this 9th day of May, 2011
IS MADE BY:

Keith Ivers/ Peaks Island Fuel Co.
OF THE FIRST PART

TO

Nancy L. Wright and Joseph D. Wright
OF THE SECOND PART

BACKGROUND

The Buyer wishes to submit an offer to purchase a certain completed home from the Seller under the terms stated below.

IN CONSIDERATION OF and as a condition of the Seller selling the Property and the Buyer purchasing the Property (collectively the "Parties") and other valuable consideration the receipt of which is hereby acknowledged, the Parties to this Offer to Purchase Real Estate agree as follows:

Real Property

1. The legal description of the Property is as follows: City of Portland Real Estate Assessed Property Description 90-AA-1-2-5

Recorded at the Cumberland County Registry of Deeds

Deed Number 75916
Book 11103
Page 239

Certain lots or parcels of land situated at Peaks Island in Portland Maine on the Northwestern side of Island Avenue and bounded and described as follows:
Beginning at a point on the northwest sideline of Island Avenue, which point is NINETY (90) Feet Southwest of the intersection of Island Avenue and Trefesthen Avenue; thence Northwest along land now or formally of Donald S. Webber, a distance of SIXTY (60) Feet, to a point; thence Northeast along said Webber land a distance of NINETY (90) Feet to an iron monument set in the ground on the Southwest sideline of Trefethen Avenue. Said iron monument being SIXTY (60) Feet Northwest
of the intersection formed by the Northwest sideline of Island Avenue and Trefethen Avenue; thence Northwest along the Southwest sideline of Trevethen Avenue a distance of ONE HUNDRED and FIFTY-EIGHT (158) Feet, to a point; thence turning and running Southwest along land now or formerly of Lawrence Burke a distance of ONE HYNDRED TWO and SEVEN-TENTHS (102.7) Feet, to a point; thence South 45 Degrees 47 Degrees West along land now or formerly

of Edgar B. Allen, a distance of ONE HUNDRED and SIXTEEN-HUNDRETHS (100.16) Feet, to a point on the North Sideline of Brimmer Street a distance of ONE HUNDRED NINETY-FOUR and SEVENTY-THREE HUNDRETHS (194.3) Feet, to a point, which point is the intersection of the Northeast corner of Brimmer Street and the Northwest side of Island Avenue; thence North 50 Degrees 20 Degrees East along the Northwest sideline of Island Avenue a distance of NINET-NINE and FOURTY-FOUR HUNDRETHS (99.44) Feet, to the point of beginning.

Sales Price

2. The total purchase price of \$280,000.00 (the "Purchase Price") that is to be paid for the Property by the Buyer is payable as follows:
 - a. The initial earnest money deposit (the "Deposit") accompanying this offer is \$5,000.00. The Deposit will be paid by cash on or before June 1, 2011. The Deposit will not be held in escrow and is nonrefundable after June 1st, 2011. The deposit will be credited to the Buyer.
 - b. The balance of the Purchase Price will be paid in cash or equivalent in financing at closing unless otherwise provided in this Offer. The balance will be subject to adjustments.

Closing & Possession

3. The Closing Date will be on or before prior to August 31, 2011, at which point the Buyer will take possession of the Property.

Conditions

4. The Buyer's obligation to purchase the Property is contingent upon: The Buyer obtaining third party financing. The application for this loan will be completed with a lender acceptable to the Buyer within 30 days of the acceptance of this Offer.

Waiver

5. All contingencies under the header entitled "Conditions" in this Offer may be waived if the Parties sign a written waiver indicating their intention.

Notices

6. All notices pursuant to this Offer must be written and signed by the respective party or its agent and all such correspondence will be effective upon it being mailed, delivered, or transmitted by facsimile as follows:

Buyer

Address: 512 Island Avenue Peaks Island, Maine 04108

Seller

Address: 146 Pond Road Bridgton, Maine 04009

Severability

7. If any terms or provision of this Offer will, to any extent, be determined to be invalid or unenforceable by a court of competent jurisdiction, the remainder of this Offer will not be affected and each unaffected term and provision of this Offer will remain valid and be enforceable to the fullest extent permitted by law.

Interpretation

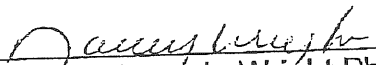
8. Headings are inserted for the convenience of the Parties only and are not to be considered when interpreting this Offer. Words in the singular mean and include the plural and vice versa. Words in the masculine gender mean and include the feminine gender and vice versa. Words importing persons include firms and corporations and vice versa.

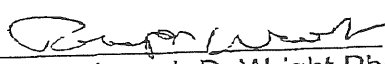
Time of Essence


9. Time is of the essence in this Offer. Every calendar day except Saturday, Sunday or US national holiday will be deemed a business day and all relevant time periods in this Offer will be calculated in business days. Performance will be due the next business day, if any deadline falls on a Saturday, Sunday or US national holiday. A business day ends at 5 p.m. local time in the time zone in which the Property is situated.

Buyer's Offer

10. This is an offer to purchase the Property on the above terms and conditions. The Seller has the right to continue to offer the Property for sale and to accept any other offer at any time

Seller:  Date: 5/17/11
Seller's Name: Nancy L. Wright Phone: (207) 650-6457
Address: 146 Pond Road Bridgton, Maine 04009
Marital status: Single Married Separated Divorced Co-ownership
Social security #: 2028

Seller:  Date: 5/17/11
Seller's Name: Joseph D. Wright Phone: (207) 766-2773
Address: 498 Island Avenue Peaks Island, Maine 04108
Marital status: Single Married Separated Divorced Co-ownership
Social security #: 2828

Buyer(s):  Date: 5/19/11
Buyer's Name: Keith Ivers
Address: 512 Island Avenue Peaks Island, Maine 04108

Marital status: Single ___ Married ___ Separated ___ Divorced ___ Co-ownership

Social security #: 007-76-4094

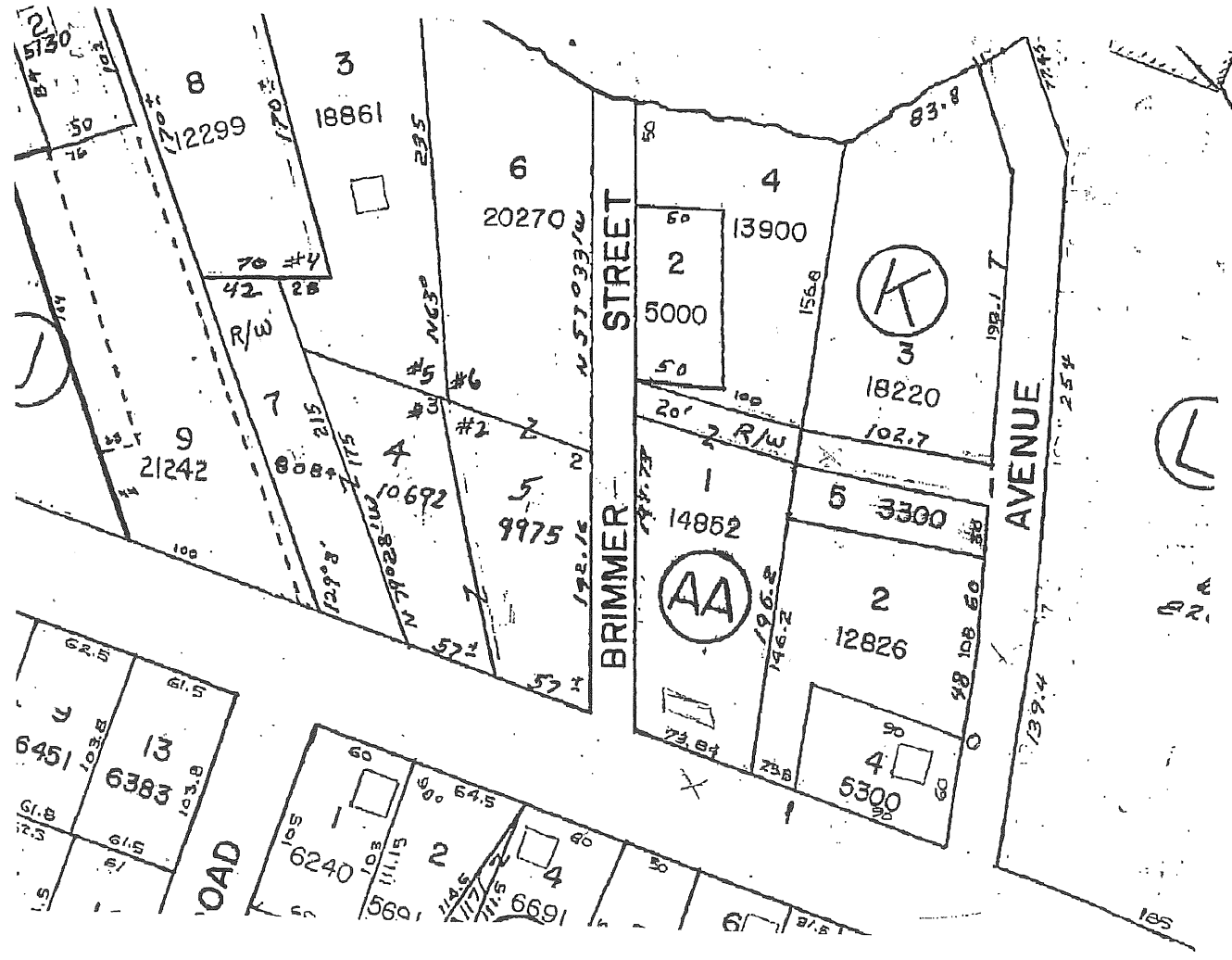
No 90

PEAKS ISLAND



CITY OF PORTLAND
ASSESSORS PLAN
SCALE 1" = 100'
DRAWN 6-76

MND 090





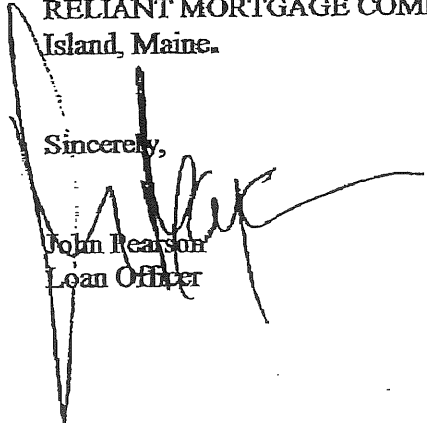
2320 Congress Street, Suite C, Portland, ME 04102 Ph: (207) 775-4200 Toll Free: (888) 775-4200

TO: Whom it may concern
DATE: 06/09/2011

FROM: John Pearson
RE: Keith Ivers

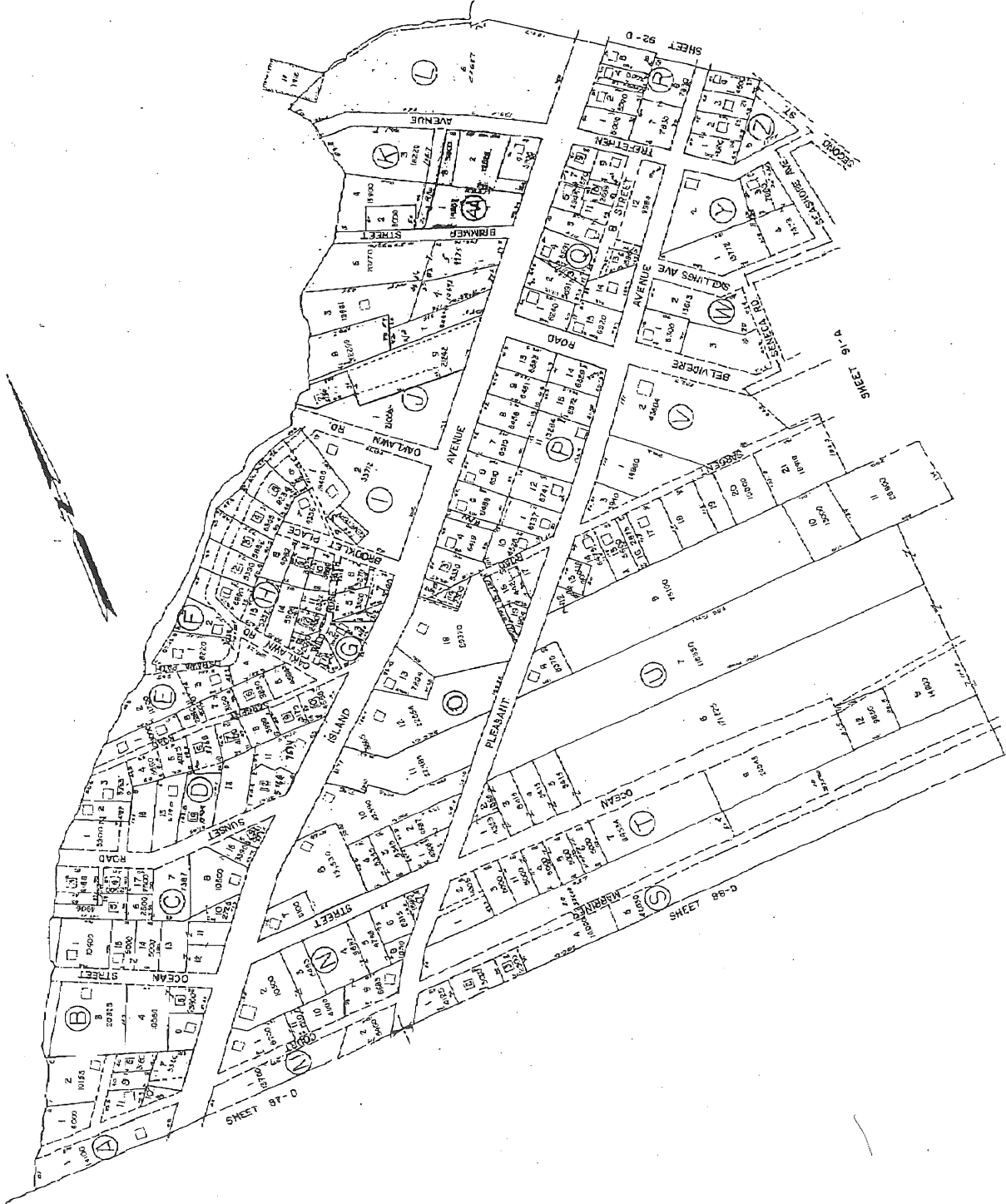
Please be advised that the above mentioned borrower has made application with
RELIANT MORTGAGE COMPANY for the purchase of, 512 Island Avenue, Peaks
Island, Maine.

Sincerely,


John Pearson
Loan Officer

ATTN: BARBRA

No 90
PEAKS ISLAND



CITY OF PORTLAND
ASSESSORS PLAN
SCALE 1" = 100'
DRAWN 6-76

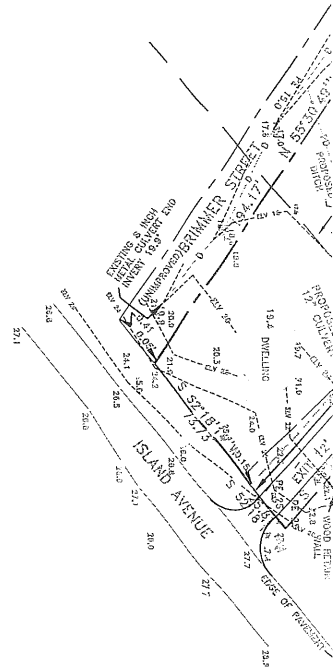
NOTES:

- 1.) DEED REFERENCE: DONALD W. WRIGHT TO JOSEPH D. WRIGHT AND NANCY L. WRIGHT DATED DECEMBER 24, 1997 AND RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS IN BOOK 13525, PAGE 259 AND DONALD S WEBBER TO NANCY L. WRIGHT AND JOSEPH D. WRIGHT DATED JULY 6, 1978 AND RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS IN BOOK 4259, PAGE 76.
- 2.) PLAN REFERENCE: " PLAN OF PROPERTY FOR WENDELL WRIGHT", DATED JUNE 2, 1964 BY RAYMOND MERCER AND RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS IN PLAN BOOK 69, PAGE 36.
- 3.) ELEVATIONS REFER TO N.G.V.D. 1929 DATUM.
- 4.) BEARINGS REFER TO MAGNETIC FOR THE YEAR 2010.
- 5.) LOT IS SHOWN ON PORTLAND TAX ASSESSORS MAP 90, BLOCK AA, LOTS 1, 2, AND 5.

SYMBOLS:

- IRON PIPE OR ROD FOUND
- 1/2 INCH IRON ROD SET WITH PLASTIC CAP MARKED "AUSTIN 2174"
- PROPOSED EDGE OF PAVEMENT
- 16.8 SPOT ELEVATION
- — — — OVERHEAD UTILITY LINES
- CATCH BASIN
- UTILITY POLE
- BOUNDARY LINE
- LARGE TREE
- FE 22.0 PROPOSED ELEVATION
- EXISTING CONTOUR

WILLIAM G. AUSTIN
STATE OF MAINE
PROFESSIONAL LAND SURVEYOR 2174



BARBARA ISAKHAYAT
874-8699

Effective August 18, 2010



Level I – Site Alteration Development Review Application Portland, Maine

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Level I: Site Alteration site plan.

Level I: Site Alteration Development includes:

- Alteration of a watercourse or wetland as defined in Section 14-47 of the City Code.
- Alteration of a site. The disturbance of land areas of less than one (1) acre that are stripped, graded, grubbed, filled or excavated. The Planning Authority shall exempt from review the loam and seeding of lawns and the cumulative placement of less than fifteen (15) cubic yards of fill on any lot provided such loaming or placement does not alter a drainage course, swale, wetland or redirect water onto adjoining property and does not violate any other provision of the Portland City Code or state or federal law. "Disturbed area" does not include routine maintenance, but does include re-development and new impervious areas.
- The construction of any temporary or permanent parking area, paving of existing unpaved surface parking areas between 1,000 and 7,500 square feet, or creation of other impervious surface areas between 1,000 and 7,500 square feet.
- The rehabilitation or reconstruction, but not new construction, of piers, docks, wharves, bridges, retaining walls, and other structures located within the shoreland zone.
- A site alteration in which vehicle access is proposed from more than one (1) street;

The Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division
Fourth Floor, City Hall
389 Congress Street
(207) 874-8721

Office Hours
Monday thru Friday
8:00 a.m. – 4:30 p.m.

PROJECT NAME: PEAKS ISLAND FUEL

PROPOSED DEVELOPMENT ADDRESS:
512 ISLAND AVE, PEAKS ISLAND, ME 04108

PROJECT DESCRIPTION:
UTILIZE COMMERCIAL PROPERTY FOR PARKING OF DELIVERY AND SERVICE TRUCKS, FILL IN SITE AND ADD TWO DRIVEWAYS

CHART/BLOCK/LOT: MAP 90, BLOCK AA, LOTS 1, 2 AND 5

CONTACT INFORMATION:

Applicant – must be owner, Lessee or Buyer Name: <u>KEITH IVERS</u> Business Name, if applicable: <u>PEAKS ISLAND FUEL</u> Address: <u>BOX G</u> City/State: <u>PEAKS ISLAND, ME</u> Zip Code: <u>04108</u>	Applicant Contact Information Work # <u>207-749-5700</u> Home# <u>207-749-5700</u> Cell # <u>207-749-5700</u> Fax# <u>207-766-2969</u> e-mail: <u>VAS92@AOL.COM</u>
Owner – (if different from Applicant) Name: Address: City/State : Zip Code:	Owner Contact Information Work # Home# Cell # Fax# e-mail:
Agent/ Representative Name: Address: City/State : Zip Code:	Agent/Representative Contact information Work # Cell # e-mail:
Billing Information Name: <u>KEITH IVERS</u> Address: <u>BOX G</u> City/State: <u>PEAKS ISLAND, ME</u> Zip Code: <u>04108</u>	Billing Information Work # <u>207-749-5700</u> Cell # Fax# <u>207-766-2969</u> e-mail: <u>VAS92@AOL.COM</u>

Engineer Name: Address: City/State : Zip Code:	Engineer Contact Information Work # Cell # Fax# e-mail:
Surveyor Name: <u>BILL AUSTIN</u> Address: <u>82 ELDERBERRY LANE</u> City/State: <u>NEW GLOUCESTER, ME</u> Zip Code: <u>04260</u>	Surveyor Contact Information Work # <u>207-632-2959</u> Cell # Fax# e-mail: <u>ALS2174@GMAIL.COM</u>

e-plan contact

APPLICATION FEES:

Check all reviews that apply. Payment may be made by Check or Cash addressed to the City of Portland.

Level I Site Alteration Site Plan <input checked="" type="checkbox"/> Application Fee (\$200.00) The City invoices separately for the following: <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) Third party review is assessed separately.	Fees Paid (office use) —
Performance Guarantee: A performance guarantee is required to cover all public and private site improvements.	Required
Inspection Fee: An inspection fee of 2% of the performance guarantee is due prior to the release of permits	2% of the performance guarantee

Application Check List


Refer to the application checklist for a detailed list of submittal requirements.

As of December 1, 2010, all site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521).

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Site Plan review only, a Performance Guarantee, Inspection Fee, Building Permit Application and associated fees will be required prior to construction.

Signature of Applicant: 	Date: 5 - - 2011
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Please refer to Article V, Site Plan of the City of Portland Land Use Code for detailed information concerning the City's site plan review process, thresholds and standards. Should you have any questions regarding the submittal requirements or any other aspect of the site plan review process, please contact the Planning Division.

PROJECT DATA

The following information is required where applicable, in order complete the application

Total Site Area	39093 total 22310 Commercial sq. ft.
Proposed Total Disturbed Area of the Site	4200-4500 sq. ft.
IMPERVIOUS SURFACE AREA	
• Proposed Total Paved Area / GRAVEL	4200 sq. ft.
• Existing Total Impervious Area	0 sq. ft.
• Proposed Total Impervious Area	4200 sq. ft.
• Proposed Impervious Net Change	4200 sq. ft.
PARKING SPACES	
• Existing Number of Parking Spaces	0
• Proposed Number of Parking Spaces	7
TOTAL Number of Parking Spaces	7

General Submittal Requirements – Level I Site Alteration

Applicant Checklist	Planner Checklist	Number of Paper Copies	Submittal Requirement
<input type="checkbox"/>	<input checked="" type="checkbox"/>	1	Completed application form.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	1	Application fees.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written description of project.
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of right, title and interest.
<input type="checkbox"/>	<input type="checkbox"/>	1	Copies of required state and/or federal permits.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of zoning.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written description of existing and proposed easements or other burdens.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written requests for waivers from individual site plan and/or technical standards.
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of financial and technical capacity.

Site Plans and Boundary Survey Requirements – Level I Site Alteration

Applicant Checklist	Planner Checklist	Number of Copies	Submittal Requirement
<input type="checkbox"/>	<input checked="" type="checkbox"/>	1	Boundary Survey meeting the requirements of Section 13 of the City of Portland Technical Manual.
<input type="checkbox"/>	<input type="checkbox"/>	1	Site Plan including the following:
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Existing structures with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone)
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas.
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Location and details of proposed infrastructure improvements (e.g. - curb and sidewalk improvements, utility connections, roadway improvements).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Identification of and proposed protection measures for any significant natural features on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Details of proposed pier rehabilitation (Shoreland areas only).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing utilities.
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Existing and proposed grading and contours.
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Proposed stormwater management and erosion controls.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Total area and limits of proposed land disturbance.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing vegetation to be preserved and proposed site landscaping.
<input type="checkbox"/>	<input checked="" type="checkbox"/>		▪ Existing and proposed easements or public or private rights of way.

Site Plan Standards for Review of Level I: Site Alteration

Level I: Site alteration plans shall only be subject to the following site plan standards, as applicable, as contained in section 14-526:

- (a) Transportation standards
 - 1, 2, and 4.
- (b) Environmental quality standards
 - 1. and 3.
- (c) Public infrastructure and community safety standards.
 - 1.
- (d) Site design standards
 - 5, 6, 8 and 9.

Except as provided in article III, or to conditions imposed under section 14-526(e) only, or to those submission requirements set forth in section 14-527 as relate solely thereto.



PORTLAND FIRE DEPARTMENT SITE REVIEW FIRE DEPARTMENT CHECKLIST



A separate drawing[s] shall be provided to the Portland Fire Department for all site plan reviews.

1. Name, address, telephone number of applicant.
2. Name address, telephone number of architect
3. Proposed uses of any structures [NFPA and IBC classification]
4. Square footage of all structures [total and per story]
5. Elevation of all structures
6. Proposed fire protection of all structures
 - **As of September 16, 2010 all new construction of one and two family homes are required to be sprinkled in compliance with NFPA 13D. This is required by City Code. (NFPA 101 2009 ed.)**
7. Hydrant locations



2320 Congress Street, Suite C, Portland, ME 04102

Ph: (207) 775-4200

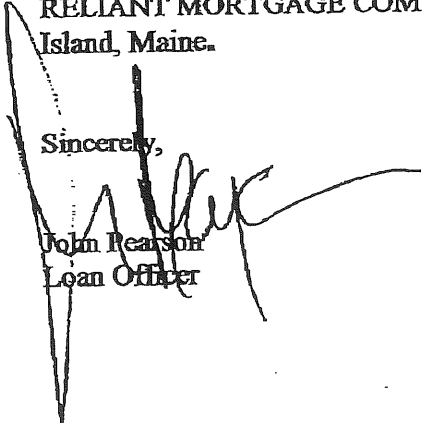
Toll Free: (888) 775-4200

TO: Whom it may concern
DATE: 06/09/2011

FROM: John Pearson
RE: Keith Ivers

Please be advised that the above mentioned borrower has made application with
RELIANT MORTGAGE COMPANY for the purchase of, 512 Island Avenue, Peaks
Island, Maine.

Sincerely,


John Pearson
Loan Officer

ATTN: BARBRA



PEAKS ISLAND FUEL

P.O. BOX 104

PEAKS ISLAND, ME 04108

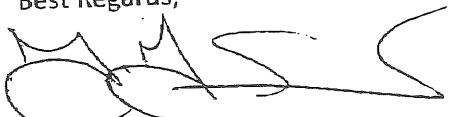
207-766-5700 OFFICE

207-766-2969 FAX

To whom it may concern;

This letter is to describe the proposed project for 516 Island ave on Peaks Island, ME 04108. The current site is relatively flat with a slightly sloping terrain. Aside from two small pieces of vegetation it is all grass at the moment. The plan is to slightly raise a small portion of the yard and form a small parking area for up to seven service vehicles. This will include having two driveways put in for access to the property, one from Island ave and the other coming in from Trefethen ave. The lots height will begin close to the original grade of the lot towards Trefethen ave and carry appropriate pitch towards Brimmer st for approx 82' and towards the northwest of the property for approx 50'. The total disturbed area will be around 4,200 square feet. The top soil will be removed and appropriate fill brought in to form a suitable sub base for the lot. Remaining fill will be composed of crushed stone and topped off with gravel for a parking surface. Culverts will be put in place at each location were there would be a possibility for water to catch so it can continue on its natural path. The proposed are has been surveyed and a plan has been provided to show its location in the Commercial zone portion of the lot and that it is designed for minimal disruption of the area. The main objective of the project is to consolidate my business to one area instead of renting three different areas . It will give my family's business a permanent foundation so we can continue the business for many years to come. Thank you.

Best Regards,



Keith Ivers

Owner/ President Peaks Island Fuel

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1997 DEC 31 AM 9:42

CUMBERLAND COUNTY
John E. O'Brien

Offer to Purchase Real Estate
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Keith Ivers/ Peaks Island Fuel Co.
OF THE FIRST PART
TO
Nancy L. Wright and Joseph D. Wright
OF THE SECOND PART

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Book 11103
Page 239

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Beginning at a point on the northwest sideline of Island Avenue, which point is NINETY (90) Feet Southwest of the intersection of Island Avenue and Trefesthen Avenue; thence Northwest along land now or formally of Donald S. Webber, a distance of SIXTY (60) Feet, to a point; thence Northeast along said Webber land a distance of NINETY (90) Feet to an iron monument set in the ground on the Southwest sideline of Trefethen Avenue. Said iron monument being SIXTY (60) Feet Northwest
of the intersection formed by the Northwest sideline of Island Avenue and Trefethen Avenue; thence Northwest along the Southwest sideline of Trevethen Avenue a distance of ONE HUNDRED and FIFTY-EIGHT (158) Feet, to a point; thence turning and running Southwest along land now or formerly of Lawrence Burke a distance of ONE HYNDRED TWO and SEVEN-TENTHS (102.7) Feet, to a point; thence South 45 Degrees 47 Degrees West along land now or formerly

of Edgar B. Allen, a distance of ONE HUNDRED and SIXTEEN-HUNDRETHS (100.16) Feet, to a point on the North Sideline of Brimmer Street a distance of ONE HUNDRED NINETY-FOUR and SEVENTY-THREE HUNDRETHS (194.3) Feet, to a point, which point is the intersection of the Northeast corner of Brimmer Street and the Northwest side of Island Avenue; thence North 50 Degrees 20 Degrees East along the Northwest sideline of Island Avenue a distance of NINET-NINE and FOURTY-FOUR HUNDRETHS (99.44) Feet, to the point of beginning.

Sales Price

2. The total purchase price of \$280,000.00 (the "Purchase Price") that is to be paid for the Property by the Buyer is payable as follows:
 - a. The initial earnest money deposit (the "Deposit") accompanying this offer is \$5,000.00. The Deposit will be paid by cash on or before June 1, 2011. The Deposit will not be held in escrow and is nonrefundable after June 1st, 2011. The deposit will be credited to the Buyer.
 - b. The balance of the Purchase Price will be paid in cash or equivalent in financing at closing unless otherwise provided in this Offer. The balance will be subject to adjustments.

Closing & Possession

3. The Closing Date will be on or before prior to August 31, 2011, at which point the Buyer will take possession of the Property.

Conditions

4. The Buyer's obligation to purchase the Property is contingent upon: The Buyer obtaining third party financing. The application for this loan will be completed with a lender acceptable to the Buyer within 30 days of the acceptance of this Offer.

Waiver

5. All contingencies under the header entitled "Conditions" in this Offer may be waived if the Parties sign a written waiver indicating their intention.

Notices

6. All notices pursuant to this Offer must be written and signed by the respective party or its agent and all such correspondence will be effective upon it being mailed, delivered, or transmitted by facsimile as follows:

Buyer

Address: 512 Island Avenue Peaks Island, Maine 04108

Seller

Address: 146 Pond Road Bridgton, Maine 04009

Severability

7. If any terms or provision of this Offer will, to any extent, be determined to be invalid or unenforceable by a court of competent jurisdiction, the remainder of this Offer will not be affected and each unaffected term and provision of this Offer will remain valid and be enforceable to the fullest extent permitted by law.

Interpretation

8. Headings are inserted for the convenience of the Parties only and are not to be considered when interpreting this Offer. Words in the singular mean and include the plural and vice versa. Words in the masculine gender mean and include the feminine gender and vice versa. Words importing persons include firms and corporations and vice versa.

Time of Essence

9. Time is of the essence in this Offer. Every calendar day except Saturday, Sunday or US national holiday will be deemed a business day and all relevant time periods in this Offer will be calculated in business days. Performance will be due the next business day, if any deadline falls on a Saturday, Sunday or US national holiday. A business day ends at 5 p.m. local time in the time zone in which the Property is situated.

Buyer's Offer

10. This is an offer to purchase the Property on the above terms and conditions. The Seller has the right to continue to offer the Property for sale and to accept any other offer at any time

Seller: Nancy L. Wright Date: 5/17/11
Seller's Name: Nancy L. Wright Phone: (207) 650-6457
Address: 146 Pond Road Bridgton, Maine 04009
Marital status: Single Married Separated Divorced Co-ownership
Social security #: 2028

Seller: Joseph D. Wright Date: 5/17/11
Seller's Name: Joseph D. Wright Phone: (207) 766-2773
Address: 498 Island Avenue Peaks Island, Maine 04108
Marital status: Single Married Separated Divorced Co-ownership
Social security #: 3528

Buyer(s): Keith Ivers Date: 5/19/11
Buyer's Name: Keith Ivers
Address: 512 Island Avenue Peaks Island, Maine 04108

Marital status: Single Married Separated Divorced Co-ownership

Social security #: 007-76-4094



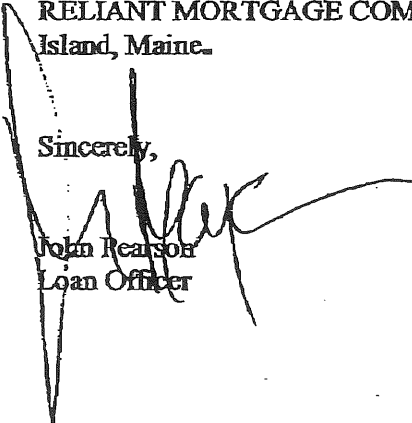
2320 Congress Street, Suite C, Portland, ME 04102 Ph: (207) 775-4200 Toll Free: (888) 775-4200

TO: Whom it may concern
DATE: 06/09/2011

FROM: John Pearson
RE: Keith Ivers

Please be advised that the above mentioned borrower has made application with RELIANT MORTGAGE COMPANY for the purchase of, 512 Island Avenue, Peaks Island, Maine.

Sincerely,


John Pearson
Loan Officer

ATTN: BARBRA