



PORTLAND MAINE

Strengthening a Remarkable City, Building a Community for Life www.portlandmaine.gov

Planning & Urban Development Department
Penny St. Louis Littell, Director

Planning Division
Alexander Jaegerman, Director

August 3rd, 2010

Mr. William Connolly
Mercy Hospital
144 State Street
Portland, ME 04101

Mr. Stephen R. Bushey, PE
DeLuca Hoffman Associates Inc.
778 Main Street- Suite 8
South Portland, ME. 04106

**RE: Mercy Hospital – Amendment to Approved Site Plan:
Southerly Driveway Modifications to allow Left Turn
Vicinity 175 Fore River Parkway**

Application ID Number: HTE 10-99900004
CBL: 073-A-001-001

Dear Mr. Connelly and Mr. Bushey:

On August 3rd, 2010, the Portland Planning Authority approved the amended site plan for the approved Mercy Hospital Phase I development (approved application #2005-0192) to include modifications to the existing southerly driveway off the Fore River Parkway to allow a left turn (via a new left turn lane) into this driveway. Associated improvements include widening of the Parkway and reduction of the esplanade, modifications to signals and signal poles, relocation of the parking lot entrance and relocation/revisions to drainage and landscape.

The approved amendment is shown on plans prepared by DeLuca-Hoffman: *Roadway Improvement Plan* (Rev. 4 7.28.2010), *Striping and Signage Plan* (Rev. 2 7.28.2010); *Traffic Signal Plan* (Rev 1 7.27.2010) and associated sections and details dated 7.28.2010.

The approval is subject to the following conditions:

- i. That the applicant shall be responsible for incorporating signal equipment modifications, as necessary, such that it will allow for the subject traffic signal to be coordinated with traffic signals associated with the Veterans Memorial Bridge project; and
- ii. That the alignment of the sidewalk ramp and detectable warning device on the northeast corner of the subject driveway shall be modified to better meet ADA standards such that the crossing is constructed to be as close to a perpendicular alignment as reasonably practicable. The change shall be reviewed and approved by the Planning Authority prior to construction; and
- iii. That the applicant shall revise the proposals for the modification of the curbing at the entrance to the internal drop-off area to address traffic control, pavement markings, and pedestrian movements as the curb changes have created a very wide open intersection; these changes shall be reviewed and approved by the Planning Authority prior to construction; and

- iv. That a signage and pavement marking plan shall be provided for the internal intersection that shall be reviewed and approved by the Planning Authority prior to installation /construction; and
- v. That the proposed sign type and associated installation at the new parking lot entry driveway on-site shall be reviewed and approved by the Planning Authority prior to implementation; and
- vi. That the applicant shall add a note to the plan that states that all existing signs for the hospital that are invalid shall be removed by the applicant with approval from the Planning Authority.
- vii. That a detail for the recessed reflectors shall be submitted for review and approval by the Planning Authority prior to construction; and
- viii. That the applicant shall relocate and augment any disturbed planting so that there is no net loss of planting in the vicinity of the improvements, and complete all associated landscaping and planting within six months of the re-opening of the southernmost driveway to traffic access.

The approval is based on the submitted plans. If you need to make any modifications to the approved site plan, you must submit an amended site plan for staff review and approval. This letter also confirms the following:

- a. The Planning Authority has granted a waiver from the City's Technical Standards for the driveway width where it meets the Fore River Parkway.
- b. The Fore River Parkway is a State moratorium road. It is noted that changes in curbing alignment and alterations to system drainage will require cutting into the roadway and the permanent repair may require grinding and full overlay of the disturbed area.

Please note the following provisions and requirements for all site plan approvals:

- 1. Where submission drawings are available in electronic form, final sets of plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (*.dwg), release AutoCAD 2005 or greater.
- 2. A performance guarantee covering the site improvements as well as an agreed inspection fee payment (normally 2.0% of the guarantee amount) and seven (7) final sets of plans must be submitted to and approved by the Planning Division and Public Works prior to the release of the street opening permit. If you need to make any modifications to the approved plans, you must submit a revised site plan application for staff review and approval.
- 3. The site plan amendment approval will be deemed to have expired unless work in the development has commenced within one (1) year of the approval or within a time period agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the expiration date.
- 4. A defect guarantee, consisting of 10% of the performance guarantee, must be posted before the performance guarantee will be released.
- 5. Prior to construction, a pre-construction meeting shall be held at the project site with the contractor, Planning Division's Development Review Coordinator, Public Work's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the pre-construction meeting.

6. If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)
7. The Development Review Coordinator must be notified five (5) working days prior to date required for final site inspection. The Development Review Coordinator can be reached at the Planning Division at 874-8632. Please make allowances for completion of site plan requirements determined to be incomplete or defective during the inspection. This is essential as all site plan requirements must be completed and approved by the Development Review Coordinator prior to the release of the performance guarantee.

If there are any questions, please contact Jean Fraser at 874 8728 or jf@portlandmaine.gov.

Sincerely,



Alexander Jaegerman
Planning Division Director

Attachment: Performance Guarantee Packet

Electronic Distribution:

Penny St. Louis Littell, Director of Planning and Urban Development
Alexander Jaegerman, Planning Division Director
Barbara Barhydt, Development Review Services Manager
Jean Fraser, Planner
Philip DiPierro, Development Review Coordinator
Marge Schmuckal, Zoning Administrator
Tammy Munson, Inspections Division
Gayle Guertin, Inspections Division
Lisa Danforth, Inspections Division
Lannie Dobson, Inspections Division
Michael Bobinsky, Public Services Director
Kathi Earley, Public Services

Bill Clark, Public Services
David Margolis-Pineo, Deputy City Engineer
Greg Vining, Public Services
John Low, Public Services
Jane Ward, Public Services
Keith Gautreau, Fire Prevention
Jeff Tarling, City Arborist
Tom Errico, City Traffic Engineering Reviewer
Dan Goyette, Woodard & Curran
Assessor's Office
Approval Letter File

Hard Copy: Project File



DeLUCA-HOFFMAN ASSOCIATES, INC.
CONSULTING ENGINEERS

778 MAIN STREET
SUITE 8
SOUTH PORTLAND, MAINE 04106
TEL. 207 775 1121
FAX 207 879 0896

- SITE PLANNING AND DESIGN
- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- PERMITTING
- AIRPORT ENGINEERING
- CONSTRUCTION ADMINISTRATION
- LANDSCAPE ARCHITECTURE

August 12, 2010

Mr. Mike Bobinsky
Director of Public Services
55 Portland Street
Portland, Maine 04101

**RE: Mercy Hospital
Southerly Driveway Modifications to Allow Left Turn**

Dear Mike,

On behalf of Mercy Hospital and Reed and Reed Inc. we are seeking your clarification for the completion of the pavement surface restoration along the Fore River Parkway related to Mercy Hospital's proposed left turn lane construction. We currently understand that the City will require the following construction sequence of actions related to the Street Opening Permit that must be issued for the work to occur:

1. The Contractor, R.J. Grondin and Sons must apply for a Street Opening Permit with the Public Services Dept.
2. R.J Grondin will be responsible for all traffic control measures necessary during the course of box cut construction along the Fore River Parkway, generally beginning at Mercy's northerly driveway and extending to just beyond the southerly driveway.
3. As part of the surface restoration measures, a temporary course of HMA Binder will be placed to finish grade prior to the closure of paving (on or about October 15, 2010). This placement will be in addition to the permanent HMA base and binder courses to be placed as part of the new box cut area and as part of any curb removal/resetting.
4. In accordance with the City's Rules and Regulations for Excavation activity within the Public ROW, the temporary binder course will remain in place over 1 winter moratorium cycle and the City will be responsible for completion of final surface restoration (presumably in the spring of 2011) by performing a pavement grind and new surface course pavement placement. The limits of the final surface pavement placement are to extend from the centerline of the Fore River Parkway to the edge of the new box cut section. At this time our approximate measurement of this area is 2,671 SY.
5. We understand the City's normal permanent pavement restoration charge is \$65.00 per square yard. We also understand that the application of this charge and the final amount is subject to determination by the Public Services Director, and you are considering an ultimate fee value that may be more in line with the actual costs to perform the permanent pavement placement. This should result in an overall lower costs than the current

Mr. Mike Bobinsky

August 12, 2010

Page 2

pavement charge of \$65/SY (based on 2,671 SY @ \$65/SY the charge would be \$173,615.00 which is substantially higher than the expected actual costs). For the benefit of Mercy's understanding of their expected costs for the permanent pavement restoration charge, we would appreciate your immediate consideration of the final charge so they may budget accordingly.


6. We note that R.J Grondin and Mercy Hospital will be responsible for all excavation activities including backfilling, materials testing and certifications in compliance with the City's Excavation ordinance. We foresee no issues with meeting these requirements. Per the Maine Department of Transportation requirements, DeLuca-Hoffman Associates, Inc. will provide construction monitoring during the progress of work to observe materials placement and verify that the work is performed in accordance with the approved design. The monitoring work will be performed by Mr. Christopher Earle of our office. Mr. Earle is a retired MDOT employee with years of experience on similar projects.
7. We understand that the permanent pavement restoration charge does not need to be included in the performance guarantee amount to be posted as part of the Site Plan Approval. In this way there will be some reduction in the overall costs associated with the posting of the letter of credit and the Planning Authority inspection fee amount.

As you know, the contractor is seeking to begin construction by next Monday, August 23, 2010, therefore a prompt final resolution to the permanent pavement restoration procedures would be appreciated. Reed and Reed, Inc. is currently trying to schedule the preconstruction meeting as soon as possible so the City's efforts to coordinate the outcome of this issue between the Public Services Department and the Planning Department is appreciated.

If you have any questions regarding this letter please call this office.

Sincerely,

DeLuca-Hoffman Associates, Inc.



Stephen Bushey, PE
Senior Engineer

SRB/jpf/jn2149.07/Bobinsky08-12-10

C: William Connolly, Mercy Hospital
Art Cavanagh, Reed and Reed, Inc.
George Conley, R.J. Grondin and Sons
Jean Fraser, Planning Department
Kathy Earley, Public Services Division.



PORTLAND MAINE

Strengthening a Remarkable City, Building a Community for Life • www.portlandmaine.gov

Public Services Department
Michael J. Bobinsky

July 23, 2010

Mr. Jeff Folsom, PE
MaineDOT
16 State House Station
Augusta, ME 04333

Subject: Fore River Parkway Design Criteria; Mercy Hospital Fore River Entrance- Portland

Dear Jeff,

The City of Portland requests that MaineDOT consider the following design criteria associated with construction of a left turn lane at Mercy Hospital's southerly entrance to the Fore River Parkway, which is classified as an arterial roadway:

- Eleven foot wide left turn lane on the Parkway for traffic turning into the driveway. The desirable lane width is twelve feet with eleven feet being the minimum.
- Eleven foot wide travel lane in the vicinity of the left turn lane for southbound through traffic (northbound would remain twelve feet). The required lane width is twelve feet.
- Five foot wide shoulder in the vicinity of the left turn lane. The required width is eight feet.

As discussed at our meeting on July 23, 2010 that was attended by the Veterans Bridge design team, City staff, Steve Landry, and the public, the reduction in lane widths are context sensitive design elements that would preserve as much of the Parkway green space as possible. Reduction of the lane widths is very important to the City to respect the original design intent of the facility as a "Parkway". We believe the reductions in widths would not be detrimental to the traveling public for the following reasons:

- There is no on street parking in this area, which will limit side friction.
- The City's Fore River Trail crosses the Parkway at this intersection, and reducing crossing widths is desirable for pedestrian safety and as a result also improves efficiency of operation for the traffic signal by allowing a shorter pedestrian clearance time.
- There is limited truck traffic along the corridor.
- The proposed left turn lane will likewise serve limited truck traffic.
- The posted speed limit on the facility is currently 40 mph, but the City requests that MaineDOT consider a 35 mph posting.

8/18 10am Matt. D. Reuening + will get back to Phil

SUBDIVISION/SITE DEVELOPMENT
Cost Estimate of Improvements to be covered by Performance Guarantee

Date: 08/17/10

Name of Project: MERCY HOSPITAL SOUTHERLY DRIVE MODIFICATIONS
 Address/Location: 175 FORE RIVER PARKWAY
 Application ID #: HTE-10-99900084
 Developer: MERCY HOSPITAL
 Form of Performance Guarantee: _____
 Type of Development: Subdivision _____ Site Plan (Major/Minor) _____

TO BE FILLED OUT BY THE APPLICANT:

Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
1. STREET/SIDEWALK						
Road/Parking Areas	5,200SF	8 ⁵⁰ -	44,200-	4,200SF	7 ⁵⁰ -	31,500-
Curbing	210L.F.	26-	5,460-	375L.F.	26-	9,750-
Sidewalks	NA		/	120SF	18 ⁵⁰	2,220-
Esplanades	↑		/	NA		/
Monuments	↓		/	↑		/
Street Lighting	NA		/	↓		/
Street Opening Repairs	?		/	↓		/
Other			/	NA		/
2. EARTH WORK						
Cut	500cy	8-	4,000-	120cy	8-	960-
Fill	450cy	18-	8,100-	100cy	18-	1,800-
3. SANITARY SEWER						
Manholes	NA		/	N.A.		/
Piping	↑		/	↑		/
Connections			/			/
Main Line Piping			/			/
House Sewer Service Piping			/			/
Pump Stations			/			/
Other	↓		/	↓		/
4. WATER MAINS						
	NA		/	N.A.		/
5. STORM DRAINAGE						
Manholes	4EA	500-	2,000	N.A.		/
Catchbasins	3EA	2,500-	7,500-	↑		/
Piping	17L.F.	100-	1,700-			/
Detention Basin	NA		/	↓		/
Stormwater Quality Units	NA		/			/
Other	NA		/	N.A.		/

6. SITE LIGHTING SIGNALS	1 L.S.	59,302	59,302	1 L.S.	20,000-	20,000-
7. EROSION CONTROL						
Silt Fence	900 L.F.	150-	1,350-	NA		
Check Dams	NA					
Pipe Inlet/Outlet Protection	1 L.S.	250-	250-			
Level Lip Spreader	NA					
Slope Stabilization						
Geotextile						
Hay Bale Barriers	NA					
Catch Basin Inlet Protection	3 EA.	150-	450-			
8. RECREATION AND OPEN SPACE AMENITIES	NA			NA		
9. LANDSCAPING - SEE (Attach breakdown of plant materials, quantities, and unit costs)	7 UN	50-	350-	REPLACE EXISTING		2,900-
10. MISCELLANEOUS	1 L.S.	112,430	112,430-	1 L.S.	8,500-	8,500-
TOTAL:						
GRAND TOTAL:		247,092-			77,630-	

INSPECTION FEE (to be filled out by the City)

	PUBLIC	PRIVATE	TOTAL
A: 2.0% of totals:	_____	_____	_____
or			
B: Alternative Assessment:	_____	_____	_____
Assessed by:	_____	_____	_____
	(name)	(name)	



PORTLAND MAINE

Strengthening a Remarkable City. Building a Community for Life • www.portlandmaine.gov

Public Services Department
Michael J. Bobinsky, Director

August 16, 2010

Mr. Stephen Bushey, PE
Senior Engineer
DeLuca-Hoffman Associates, Inc
778 Main Street, Suite #8
South Portland, Maine 04101

Re: Mercy Hospital Southerly Driveway Modifications to Allow Left Turn Lane

Dear Steve:

This letter serves to clarify any outstanding issues associated with the following construction sequence related to the above-referenced project. The comments are itemized and correspond directly to your August 12, 2010 letter addressed to Mike Bobinsky regarding the Street Opening Permit:

1. The City understands that Mercy Hospital has selected R.J. Grondin to construct the improvements associated with the left turn at the Southerly Driveway. As the Contractor, R.J. Grondin will be required to apply for a Street Opening Permit with the Department of Public Services.
2. The City acknowledges and agrees that R.J. Grondin will be responsible for all traffic control measures necessary during the construction of the improvements, generally beginning at Mercy's northerly driveway and extending to just beyond the southerly driveway. A traffic control plan will need to be submitted, reviewed, and accepted before a Street Opening Permit is issued.
3. The City accepts the restoration methods as presented and agreed upon during our site walk on August 16, 2010. Note the contractor is expected to provide pavement markings and striping following the placement of temporary HMA Binder course.
4. The City agrees with the estimated limits of restoration at 2,671 SY. Actual surface restoration work will confirm the final area, but staff feels this figure is accurate for budgeting at this time. Adjustments to the final area and subsequent pricing will be confirmed once the site work is complete.
5. Our normal restoration fee is set at \$65 per square yard, which based on above area equates to \$173,615. However given the nature of the project, Mercy's need for the left turn lane, and the fact that the Veterans Bridge project will have impacts on Mercy's

overall site plans in the future, the Department recognizes the need for a collaborative solution.

The Department will base the restoration fee on our direct cost which is currently \$28.57 per square yard. This unit price results in a Restoration Fee of \$76,310.47. This figure is less than half of what can be assessed through our Street Opening Program. Please note that the final striping quantities total \$2,085.22 and the permit application fee is \$223.

The total Street Opening Permit cost for this project is \$78,618.69.

6. The City would like to be copied on testing results, material submittals, and other pertinent technical information related to the City's Right of Way. Information may be sent electronically to mmd@portlandmaine.gov or a hard copy sent to my attention at 55 Portland Street, Portland, Maine 04101.
7. Per the Planning Department, the permanent pavement restoration charges do not have to be included in the performance guarantee amount to be posted as part of the Site Plan Approval. It is however required to be paid in full before a Street Opening Permit is issued.

Should you have any questions or need further information, please call me at 874-8865.

Sincerely,



Matt Doughty
Field Inspection Coordinator

Cc: Michael Bobinsky, Director of Public Services
Katherine Earley, Engineering Services Manager
David Pineo, Deputy City Engineer
Jean Frasier, Planner

Jean Fraser - Mercy Left Turn Lane

From: Jean Fraser
To: Bushey, Steve
Date: 8/18/2010 1:29 PM
Subject: Mercy Left Turn Lane
CC: Art Cavanagh; DiPierro, Philip; g.conly@grondinconstruction.com

Steve,

1. I am writing to confirm that the Plan Set for this project received yesterday meets all of the conditions of approval (Aug 3 approval letter) and are the final approved set in respect of the Site Plan (amendment) approval. I will be stamping and circulating them; could you please send **4 additional** copies (format as per the ones you sent yesterday) at your convenience to go "on the record" with Assessors etc.

2. The final decision regarding the Performance Guarantee and Inspection Fee amounts (based on review of the 8.17.2010 cost estimates by Alex Jaegerman and Penny Littell) is that they will be as the City normally receives: ie the PG being the contract costs as set out in the cost estimates (returnable in due course); the IF being 2% of that (\$6,514.44). I understand that Phil DiPierro (Planning; 874 8632) will finalize the paperwork re these direct with Grondin.

3. Phil will advise DPS as soon as the PG and IF are paid so that Street Opening Permit may be issued when DPS are ready to do so. Phil and Matt Doughty (DPS) will be the City representatives regarding this project as from now.

Hope that covers all the outstanding issues, but please do not hesitate to call if there is something I have overlooked.

Jean

Jean Fraser, Planner
City of Portland
874 8728

Jean Fraser - Mercy left Turn - final plans

From: Jean Fraser
To: Doughty, Matt
Date: 8/18/2010 2:15 PM
Subject: Mercy left Turn - final plans
CC: DiPierro, Philip; Errico, Thomas; Margolis-Pineo, David

Matt,

Just to confirm that the plans that David brought across to you today (from Steve Bushey late yesterday) are the FINAL APPROVED PLANS in terms of the Site Plan (amendment) approval issued aug 3, 2010- please mark them as such or return to me for "stamping" (I will be circulating some "stamped" copies too when Steve sends across some more copies).

Thanks for your help and collaborative approach in getting this sorted out quickly.

Jean

Jean Fraser - Mercy Hospital Southerly Driveway Modifications

From: Thomas Errico <Thomas.Errico@tylin.com>
To: Jean Fraser <JF@portlandmaine.gov>
Date: 8/18/2010 12:43 PM
Subject: Mercy Hospital Southerly Driveway Modifications
CC: David Margolis-Pineo <DMP@portlandmaine.gov>, Katherine Earley <KAS@port...

Jean – I have reviewed the plans prepared by DeLuca-Hoffman, Inc. dated August 17, 2010 and find them to be acceptable based upon the applicant addressing my previous comments.

If you have any questions or comments, please contact me.

Best regards,

Thomas A. Errico, P.E.
TYLIN INTERNATIONAL

12 Northbrook Drive
Building A, Suite One
Falmouth, ME 04105

207.347.4354 (Direct)
207.781.4721 (Main)
207.781.4753 (Fax)
207.400.0719 (Mobile)

Jean Fraser - Mercy Hospital - Southerly Driveway Modifications

From: Thomas Errico <Thomas.Errico@tylin.com>
To: Jean Fraser <JF@portlandmaine.gov>
Date: 8/13/2010 9:48 AM
Subject: Mercy Hospital - Southerly Driveway Modifications
CC: <mrbushey@maine.rr.com>, Katherine Earley <KAS@portlandmaine.gov>, David...

Jean – I have reviewed the plans and transmittal letter dated August 4, 2010 prepared by DeLuca-Hoffman Associates, Inc. as it relates to traffic comments and find the revised plans to be acceptable with the exception of the following.

- Condition 3 & 4 – I would rather not have paint delineate the travel area, unless vehicle turning templates indicate such a need. The applicant should provide turning template information that justifies the size of the pavement area.

If you have any questions, please contact me.

Best regards,

Thomas A. Errico, P.E.
TYLININTERNATIONAL

12 Northbrook Drive
Building A, Suite One
Falmouth, ME 04105

207.347.4354 (Direct)
207.781.4721 (Main)
207.781.4753 (Fax)
207.400.0719 (Mobile)

Jean Fraser - Mercy Cost Estimate and Site Inspection Fee

From: Philip DiPierro
To: Jaegerman, Alex
Date: 8/18/2010 1:05 PM
Subject: Mercy Cost Estimate and Site Inspection Fee
CC: Fraser, Jean

Hi Alex, Mercy and R.J. Grondin submitted a cost estimate for the work to be completed for the left turn off Fore River Parkway. I have looked at the private section of the estimate and the estimated costs appear to be reasonable and I feel comfortable approving them. I'm still waiting for Public Services to complete their review though. Assuming Public Services approves what was submitted, the site inspection fee will equal \$6,514.44.

Thanks.

Phil



DeLUCA-HOFFMAN ASSOCIATES, INC.
CONSULTING ENGINEERS

778 MAIN STREET
SUITE 8
SOUTH PORTLAND, MAINE 04106
TEL. 207 775 1121
FAX 207 879 0896

- SITE PLANNING AND DESIGN
- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- PERMITTING
- AIRPORT ENGINEERING
- CONSTRUCTION ADMINISTRATION
- LANDSCAPE ARCHITECTURE

August 12, 2010

Mr. Mike Bobinsky
Director of Public Services
55 Portland Street
Portland, Maine 04101

**RE: Mercy Hospital
Southerly Driveway Modifications to Allow Left Turn**

Dear Mike,

On behalf of Mercy Hospital and Reed and Reed Inc. we are seeking your clarification for the completion of the pavement surface restoration along the Fore River Parkway related to Mercy Hospital's proposed left turn lane construction. We currently understand that the City will require the following construction sequence of actions related to the Street Opening Permit that must be issued for the work to occur:

1. The Contractor, R.J. Grondin and Sons must apply for a Street Opening Permit with the Public Services Dept.
2. R.J Grondin will be responsible for all traffic control measures necessary during the course of box cut construction along the Fore River Parkway, generally beginning at Mercy's northerly driveway and extending to just beyond the southerly driveway.
3. As part of the surface restoration measures, a temporary course of HMA Binder will be placed to finish grade prior to the closure of paving (on or about October 15, 2010). This placement will be in addition to the permanent HMA base and binder courses to be placed as part of the new box cut area and as part of any curb removal/resetting.
4. In accordance with the City's Rules and Regulations for Excavation activity within the Public ROW, the temporary binder course will remain in place over 1 winter moratorium cycle and the City will be responsible for completion of final surface restoration (presumably in the spring of 2011) by performing a pavement grind and new surface course pavement placement. The limits of the final surface pavement placement are to extend from the centerline of the Fore River Parkway to the edge of the new box cut section. At this time our approximate measurement of this area is 2,671 SY.
5. We understand the City's normal permanent pavement restoration charge is \$65.00 per square yard. We also understand that the application of this charge and the final amount is subject to determination by the Public Services Director, and you are considering an ultimate fee value that may be more in line with the actual costs to perform the permanent pavement placement. This should result in an overall lower costs than the current

Mr. Mike Bobinsky
August 12, 2010
Page 2

pavement charge of \$65/SY (based on 2,671 SY @ \$65/SY the charge would be \$173,615.00 which is substantially higher than the expected actual costs). For the benefit of Mercy's understanding of their expected costs for the permanent pavement restoration charge, we would appreciate your immediate consideration of the final charge so they may budget accordingly.

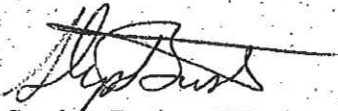
6. We note that R.J Grondin and Mercy Hospital will be responsible for all excavation activities including backfilling, materials testing and certifications in compliance with the City's Excavation ordinance. We foresee no issues with meeting these requirements. Per the Maine Department of Transportation requirements, DeLuca-Hoffman Associates, Inc. will provide construction monitoring during the progress of work to observe materials placement and verify that the work is performed in accordance with the approved design. The monitoring work will be performed by Mr. Christopher Earle of our office. Mr. Earle is a retired MDOT employee with years of experience on similar projects.
7. We understand that the permanent pavement restoration charge does not need to be included in the performance guarantee amount to be posted as part of the Site Plan Approval. In this way there will be some reduction in the overall costs associated with the posting of the letter of credit and the Planning Authority inspection fee amount.

As you know, the contractor is seeking to begin construction by next Monday, August 23, 2010, therefore a prompt final resolution to the permanent pavement restoration procedures would be appreciated. Reed and Reed, Inc. is currently trying to schedule the preconstruction meeting as soon as possible so the City's efforts to coordinate the outcome of this issue between the Public Services Department and the Planning Department is appreciated.

If you have any questions regarding this letter please call this office.

Sincerely,

DeLuca-Hoffman Associates, Inc.



Stephen Bushey, PE
Senior Engineer

SRB/jpf/jn2149.07/Bobinsky08-12-10

C: William Connolly, Mercy Hospital
Art Cavanagh, Reed and Reed, Inc.
George Conley, R.J. Grondin and Sons
Jean Fraser, Planning Department
Kathy Earley, Public Services Division.

Jean Fraser - Re: Fwd: FW: Sharp scan

From: Matt Doughty
To: Bobinsky, Michael; Margolis-Pineo, David
Date: 8/13/2010 11:37 AM
Subject: Re: Fwd: FW: Sharp scan
CC: Earley, Katherine

Reviewing the letter, I offer the following:

- 1) Dave and I verified the estimated 2,671 SY. Good to go.
- 2) Original \$65/SY Pavement Restoration Fee = \$173,615.00
Current St Opening Restoration Fee is \$28.57/SY which = \$76,310.47 *
* Does not include striping or inspection fees. No signal loops exist per our Traffic Division.
- 3) Just for comparison purposes only, the open 2008 CIP contract would yield an approximate price of \$40,000 + escalation (about \$15/SY.) That doesn't include striping and only an approximate guess.
- 4) Point of possible concern: The grind calls for 2" of removal. St Opening and CIP language / pricing both dictate 1.5". Not sure if that will produce a concern for the St Openings contractor.

I've got an onsite meeting set up for Monday morning between Steve Bushy, Grondin, and myself to review the scope of work and check out any oddball situations that would otherwise complicate this pavement restoration charge. However it shakes out - waiving the \$65/SY in lieu of our contract price is something we occasionally allow and in this case, saves the permittee close to \$100,000.

Jean Fraser - RE: Drive Cut Approval

From: David Margolis-Pineo (David Margolis-Pineo)
To: Thomas Errico
Date: 8/12/2010 8:59 AM
Subject: RE: Drive Cut Approval
CC: Gretel Varney; Jean Fraser; Katherine Earley; Matt Doughty; Steve Bu...

Nothing has been finalized on the pavement restoration of Fore River Parkway. We hope to reach an agreement shortly.

>>> Thomas Errico <Thomas.Errico@tylin.com> 8/12/2010 7:54 AM >>>

Also, Jean would like to get a letter or email from Kathi or Mike regarding the moratorium paving process for Mercy. Is that something you can arrange? Or would like me to request it from them? Thanks

Thomas A. Errico, P.E.

TYLININTERNATIONAL

12 Northbrook Drive
Building A, Suite One
Falmouth, ME 04105

207.347.4354 (Direct)
207.781.4721 (Main)
207.781.4753 (Fax)
207.400.0719 (Mobile)

From: David Margolis-Pineo [mailto:DMP@portlandmaine.gov]
Sent: Thursday, August 12, 2010 7:47 AM
To: Tom Errico
Cc: John Low
Subject: Drive Cut Approval

So, your telling me that the proposed drive cut at 278 Allen Ave is ok'ed with a 1' set back from the property line?

Jean Fraser - Re: MERCY

From: Jean Fraser
To: Conly, George
Date: 8/12/2010 10:02 AM
Subject: Re: MERCY
CC: Cavanagh, Art

George,

1. In answer to your question, the Director of DPS is at this moment making a determination regarding the process surrounding the final overlay paving and once that is clear they can answer your question; DPS will make that call. My understanding is that due to legal constraints the city will need to do the final paving and charge the applicant, but please wait until we get the DPS determination in writing. This Department will make the call re the amount of the Performance Guarantee and Inspection Fee (via Alex Jaegerman, Director).

2. Because it is likely that the Performance Guarantee will NOT include the final overlay that extends beyond the limits of disturbance (ie that the City will do that later and charge a separate fee) could you please prepare and send me another cost estimate that is based on the contract just including the final paving for the area of disturbance. We need the two figures to be sure that the Perf Guarantee amounts and associated Inspection Fee are sensible in the context of all the other issues (I believe Steve Bushey also needs that info in order to work out the costs of the 2 options). Please:

- Reference these to the August 4th set of plans (I think rev 5) and mark this on the cost estimate sheet;
- Make a note somewhere that the cost estimates do not include any extra work in relation to the street moratorium.

Thank you
Jean

<<< George Conly <g.conly@grondinconstruction.com> 8/10/2010 3:06 PM >>>

Jean: Another question- Will your Dept. arrive at the street opening fee or will Carol Merritt @ Public Works be responsible?

George Conly
Project Manager-Estimator
P: (207) 854-1147
F: (207) 854-4315
C: (207) 776-0404

R. J. Grondin & Sons
11 Bartlett Rd.
Gorham, ME. 04038

www.GRONDINCONSTRUCTION.COM

From: Michael Bobinsky
To: Jean Fraser
Date: 8/11/2010 6:33 PM
Subject: Re: fax of MDOT letter re Mercy Left Turn

Sorry for confusion. You may now end up with more copies than you wanted!! By the way I need to catch up with my staff on the street moritorium issue on the Parkway. We focused so much on the details of the left turn lane that we did not cover the street moritorium matter when we all met with you and Alex on the Traffic Movement Permit mod.

-----Original Message-----

From: Jean Fraser
To: Michael Bobinsky <mbobinsky@portlandmaine.gov>

Sent: 8/11/2010 5:30:00 PM
Subject: fax of MDOT letter re Mercy Left Turn

Thank for sending that- sorry to trouble you - I thought you had said that the cc's did not include Alex so that is why I was pressing. Jean



PORTLAND MAINE

Strengthening a Remarkable City. Building a Community for Life * www.portlandmaine.gov

Public Services Department
Michael J. Bobinsky, Director

August 16, 2010

Mr. Stephen Bushey, PE
Senior Engineer
DeLuca-Hoffman Associates, Inc
778 Main Street, Suite #8
South Portland, Maine 04101

Re: Mercy Hospital Southerly Driveway Modifications to Allow Left Turn Lane

Dear Steve:

This letter serves to clarify any outstanding issues associated with the following construction sequence related to the above-referenced project. The comments are itemized and correspond directly to your August 12, 2010 letter addressed to Mike Bobinsky regarding the Street Opening Permit:

1. The City understands that Mercy Hospital has selected R.J. Grondin to construct the improvements associated with the left turn at the Southerly Driveway. As the Contractor, R.J. Grondin will be required to apply for a Street Opening Permit with the Department of Public Services.
2. The City acknowledges and agrees that R.J. Grondin will be responsible for all traffic control measures necessary during the construction of the improvements, generally beginning at Mercy's northerly driveway and extending to just beyond the southerly driveway. A traffic control plan will need to be submitted, reviewed, and accepted before a Street Opening Permit is issued.
3. The City accepts the restoration methods as presented and agreed upon during our site walk on August 16, 2010. Note the contractor is expected to provide pavement markings and striping following the placement of temporary HMA Binder course.
4. The City agrees with the estimated limits of restoration at 2,671 SY. Actual surface restoration work will confirm the final area, but staff feels this figure is accurate for budgeting at this time. Adjustments to the final area and subsequent pricing will be confirmed once the site work is complete.
5. Our normal restoration fee is set at \$65 per square yard, which based on above area equates to \$173,615. However given the nature of the project, Mercy's need for the left turn lane, and the fact that the Veterans Bridge project will have impacts on Mercy's

overall site plans in the future, the Department recognizes the need for a collaborative solution.

The Department will base the restoration fee on our direct cost which is currently \$28.57 per square yard. This unit price results in a Restoration Fee of \$76,310.47. This figure is less than half of what can be assessed through our Street Opening Program. Please note that the final striping quantities total \$2,085.22 and the permit application fee is \$223.

The total Street Opening Permit cost for this project is \$78,618.69.

6. The City would like to be copied on testing results, material submittals, and other pertinent technical information related to the City's Right of Way. Information may be sent electronically to mmd@portlandmaine.gov or a hard copy sent to my attention at 55 Portland Street, Portland, Maine 04101.
7. Per the Planning Department, the permanent pavement restoration charges do not have to be included in the performance guarantee amount to be posted as part of the Site Plan Approval. It is however required to be paid in full before a Street Opening Permit is issued.

Should you have any questions or need further information, please call me at 874-8865.

Sincerely,


Matt Doughty
Field Inspection Coordinator

Cc: Michael Bobinsky, Director of Public Services
Katherine Earley, Engineering Services Manager
David Pineo, Deputy City Engineer
Jean Frasier, Planner

plans arrived 8/17/10

plans arrived
11am 8/11

SUBDIVISION/SITE DEVELOPMENT
Cost Estimate of Improvements to be covered by Performance Guarantee

Date: 08/10/10

Name of Project: MERCY HOSPITAL - SOUTHERLY DRIVE MODIFICATIONS
 Address/Location: 175 FORE RIVER PARKWAY
 Application ID #: HTE-10-99900004
 Developer: MERCY HOSPITAL
 Form of Performance Guarantee: ?
 Type of Development: Subdivision _____ Site Plan (Major/Minor) _____

TO BE FILLED OUT BY THE APPLICANT:

Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
1. STREET/SIDEWALK						
Road/Parking Areas	5,200SF	5-	26,000-	4,200SF	275	11,550-
Curbing	210L.F.	26	5,460-	375L.F.	26-	9,750-
Sidewalks	N.A.		/	1,200SF	185	2,220-
Esplanades	N.A.		/	NA		/
Monuments	N.A.		/			/
Street Lighting	N.A.		/			/
Street Opening Repairs	?		/	NA		/
Other	1L.S.	115				
2. EARTH WORK						
Cut	500cy	8-	4,000-	120cy	8-	960-
Fill	450cy	18-	8,100-	100cy	18-	1,800-
3. SANITARY SEWER						
Manholes	N.A.		/	N.A.		/
Piping	↑		/	↑		/
Connections	↑		/	↑		/
Main Line Piping	↑		/	↑		/
House Sewer Service Piping	↓		/	↓		/
Pump Stations	↓		/	↓		/
Other	NA		/	NA		/
4. WATER MAINS						
	NA		/	NA		/
5. STORM DRAINAGE						
Manholes - ALTER	4EA	500-	2,000-	NA		/
Catchbasins	3EA	2500-	7,500-	↑		/
Piping	17L.F.	100-	1,700-	↑		/
Detention Basin	NA		/	↓		/
Stormwater Quality Units	NA		/	↓		/
Other	NA		/	NA		/

6. SITE LIGHTING & SIGNALS	NA			ICS	79,302	79,302-
7. EROSION CONTROL						
Silt Fence	900L.F.	150	1,350-	NA		
Check Dams	NA			↑		
Pipe Inlet/Outlet Protection	ICS	250	250-			
Level Lip Spreader	NA					
Slope Stabilization	NA					
Geotextile	NA					
Hay Bale Barriers	NA			↓		
Catch Basin Inlet Protection	3 EA	150-	450	NA		
8. RECREATION AND OPEN SPACE AMENITIES	NA.					
9. LANDSCAPING - NEW	7 UN	50-	350-	REPLACE EXISTING		2,900-
(Attach breakdown of plant materials, quantities, and unit costs)			57,160-			108,482-
10. MISCELLANEOUS	ICS		100,000-	ICS		35,000-
TOTAL:			157,160-		143,482	
GRAND TOTAL:						

INSPECTION FEE (to be filled out by the City)

	PUBLIC	PRIVATE	TOTAL
A: 2.0% of totals:	_____	_____	_____
or			
B: Alternative Assessment:	_____	_____	_____
Assessed by:	_____	_____	_____
	(name)	(name)	

plans arr. 4 pm 8/17.

SUBDIVISION/SITE DEVELOPMENT
Cost Estimate of Improvements to be covered by Performance Guarantee

Date: 08/17/10

Name of Project: MERCY HOSPITAL - SOUTHERLY DRIVE MODIFICATIONS

Address/Location: 175 FORE RIVER PARKWAY

Application ID #: HTE-10-99900084

Developer: MERCY HOSPITAL

Form of Performance Guarantee: _____

Type of Development: Subdivision _____ Site Plan (Major/Minor) _____

TO BE FILLED OUT BY THE APPLICANT:

Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
1. STREET/SIDEWALK						
Road/Parking Areas	5,200SF	8 ⁵⁰	44,200 ⁻	4,200SF	7 ⁵⁰	31,500 ⁻
Curbing	210L.F.	26 ⁻	5,460 ⁻	375L.F.	26 ⁻	9,750 ⁻
Sidewalks	NA		/	120SF	18 ⁵⁰	2,220 ⁻
Esplanades	↑		/	NA		/
Monuments	↓		/	↑		/
Street Lighting	NA		/	↓		/
Street Opening Repairs	?		/	↑		/
Other			/	NA		/
2. EARTH WORK						
Cut	500cy	8 ⁻	4,000 ⁻	120cy	8 ⁻	960 ⁻
Fill	450cy	18 ⁻	8,100 ⁻	100cy	18 ⁻	1,800 ⁻
3. SANITARY SEWER						
Manholes	NA		/	N.A.		/
Piping	↑		/	↑		/
Connections			/			/
Main Line Piping			/			/
House Sewer Service Piping			/			/
Pump Stations			/			/
Other	↓		/	↓		/
4. WATER MAINS						
	NA		/	N.A.		/
5. STORM DRAINAGE						
Manholes	4EA	500 ⁻	2,000 ⁻	N.A.		/
Catchbasins	3EA	2,500 ⁻	7,500 ⁻	↑		/
Piping	17L.F.	100 ⁻	1,700 ⁻			/
Detention Basin	NA		/	↓		/
Stormwater Quality Units	NA		/			/
Other	NA		/	N.A.		/

6. SITE LIGHTING SIGNALS	1 L.S.	59,302	59,302	1 L.S.	20,000-	20,000-
7. EROSION CONTROL						
Silt Fence	900 L.F.	150-	1,350-	NA		
Check Dams	NA			↑		
Pipe Inlet/Outlet Protection	1 L.S.	250-	250-			
Level Lip Spreader	NA					
Slope Stabilization	↑					
Geotextile	↓					
Hay Bale Barriers	NA					
Catch Basin Inlet Protection	3 EA.	150-	450-	↓		
8. RECREATION AND OPEN SPACE AMENITIES	NA			NA		
9. LANDSCAPING - SEEDS (Attach breakdown of plant materials, quantities, and unit costs)	7 UN	50-	350-	REPLACE EXISTING		2,900-
10. MISCELLANEOUS	1 L.S.	112,430	112,430-	1 L.S.	8,500-	8,500-
TOTAL:						
GRAND TOTAL:		247,092-			77,630-	

INSPECTION FEE (to be filled out by the City)

	PUBLIC	PRIVATE	TOTAL
A: 2.0% of totals:	_____	_____	_____
or			
B: Alternative Assessment:	_____	_____	_____
Assessed by:	_____	_____	_____
	(name)	(name)	

Jean Fraser - fore River Parkway Street restoration

From: "Steve Bushey" <SBushey@DelucaHoffman.com>
To: "Jean Fraser" <JF@portlandmaine.gov>
Date: 8/10/2010 3:56 PM
Subject: fore River Parkway Street restoration
CC: "Art Cavanagh" <acavanagh@reed-reed.com>, "George Conly" <g.conly@grondi...>

Hi Jean,

Per our telephone discussion we understand that RJ Grondin will be responsible for the final placement of 2" of hot mix asphalt surface pavement for the area commencing at the centerline of the Parkway to the outer edge of the new box cut section/widening. This work will involve grinding off 2" of the existing pavement surface and then placing a new 2" surface layer over the work limit in the Fore River Parkway. This work will be performed in late September or early October and prior to the MDOT paving shut down period which is the first Saturday following Oct. 15th. Further the costs for this part of the work is to be contained within the Performance Guarantee amount posted for the project. We understand that these measures will satisfy the requirements of the City's street opening ordinance and in particular the requirements for Streets still under moratorium.

Please let me know if our interpretation of the requirements is accurate and in line with the staff understanding.

thanks

Stephen Bushey PE
Senior Engineer
DeLuca-Hoffman Associates, Inc.
778 Main Street, Suite 8
South Portland, Maine 04106
207- 775-1121
Fax 207-879-0896
sbushey@delucahoffman.com
www.delucahoffman.com

This message and any attachments are intended for the individual or entity named above and may contain privileged or confidential information. If you are not the intended recipient, please do not forward, copy, print, use or disclose this communication to others; please notify the sender by replying to this message and then delete it from your system.

City of Portland
55 Portland St.
Portland, ME 04101
Phone: (207) 874-8801
Fax: (207) 874-8816

Portland Public Services

Fax

To: Jean Fraiser From: Mike Bobinsky

Fax: 756-8258 Pages: _____ (Incl. Cover Sheet)

Phone: _____ Date: 8-11-10

Re: _____ CC: _____

Urgent For Review Please Comment Please Reply Please Recycle

• Comments:

Extra copy of the MDOT letter entit. the 11' lane
5' shoulder at Mercy Hospital

Alex has copy as well -

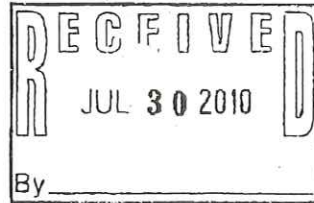


STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE
04333-0016

JOHN ELIAS BALDACCI
GOVERNOR

DAVID A. COLE
COMMISSIONER

July 29, 2010



Michael Bobinsky
Public Services Department
55 Portland St
Portland, Me 04101-2921

Dear Mike,

I have reviewed your request for lane and shoulder narrowing on the Fore River Parkway in the vicinity of Mercy Hospital. The 11 foot lane and 5 foot shoulder requests will be granted. No design exceptions will be necessary as those widths are allowed under current design for rehabilitation of an arterial.

Also, the Department will be lowering the posted speed limit to 35 mph once the Veteran's Bridge work is done. Given the new configuration of the intersections and proximity of the signals, a speed reduction make sense here and also brings the existing crosswalk into compliance.

If you have any further requests on this matter, please feel free to contact me via e-mail at Stephen.landry@maine.gov or by phone at 624-3632.

Sincerely,

Stephen Landry
Assistant State Traffic Engineer

- Cc Joe Gray, City Manager
- Katherine Early, Engineering Services Manager
- Alex Jaegerman, Planning Division Manager
- Lucie Cote, Traffic Operations Coordinator
- Tom Errico, TY Linn, Traffic Engineering Consultant
- Jeff Folsom, Project Manager
- Brian Keezer, Region Traffic Engineer

Barbara + Penny have copies



PRINTED ON RECYCLED PAPER

City of Portland
55 Portland St.
Portland, ME 04101
Phone: (207) 874-8801
Fax: (207) 874-8816

**Portland Public
Services**

Fax

To: Jean Fraiser

From: Mike Bobinsky

Fax: 756 - 8258

Pages: _____ (Incl. Cover Sheet)

Phone: _____

Date: 8-11-10

Re: _____

CC: _____

Urgent For Review Please Comment Please Reply Please Recycle

● Comments:

Extra copy of the MDOT letter auth. the 11' lane
5' shoulder at Mercy Hospital

Alex has copy as well -

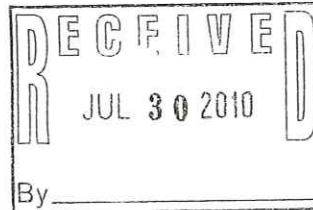


STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE
04333-0016

JOHN ELIAS BALDACCI
GOVERNOR

DAVID A. COLE
COMMISSIONER

July 29, 2010



Michael Bobinsky
Public Services Department
55 Portland St
Portland, Me 04101-2921

Dear Mike,

I have reviewed your request for lane and shoulder narrowing on the Fore River Parkway in the vicinity of Mercy Hospital. The 11 foot lane and 5 foot shoulder requests will be granted. No design exceptions will be necessary as those widths are allowed under current design for rehabilitation of an arterial.

Also, the Department will be lowering the posted speed limit to 35 mph once the Veteran's Bridge work is done. Given the new configuration of the intersections and proximity of the signals, a speed reduction make sense here and also brings the existing crosswalk into compliance.

If you have any further requests on this matter, please feel free to contact me via e-mail at Stephen.landry@maine.gov or by phone at 624-3632.

Sincerely,

Stephen Landry
Assistant State Traffic Engineer

- Cc Joe Gray, City Manager
- Katherine Early, Engineering Services Manager
- ✓ Alex Jaegerman, Planning Division Manager
- Lucie Cote, Traffic Operations Coordinator
- Tom Errico, TY Linn, Traffic Engineering Consultant
- Jeff Folsom, Project Manager
- Brian Keezer, Region Traffic Engineer



PRINTED ON RECYCLED PAPER

Jean Fraser - MERCY

From: George Conly <g.conly@grondinconstruction.com>
To: Jean Fraser <JF@portlandmaine.gov>
Date: 8/10/2010 3:07 PM
Subject: MERCY
CC: Art Cavanagh <acavanagh@reed-reed.com>

Jean: Another question- Will your Dept. arrive at the street opening fee or will Carol Merritt @ Public Works be responsible?

George Conly
Project Manager-Estimator
P: (207) 854-1147
F: (207) 854-4315
C: (207) 776-0404

R. J. Grondin & Sons
11 Bartlett Rd.
Gorham, ME. 04038

www.GRONDINCONSTRUCTION.COM



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE
04333-0016

AUG 2 2010

JOHN ELIAS BALDACCI
SECRETARY

DAVID A. COLE
COMMISSIONER

July 29, 2010

Michael Bobinsky
Public Services Department
55 Portland St
Portland, Me 04101-2921

Dear Mike,

I have reviewed your request for lane and shoulder narrowing on the Fore River Parkway in the vicinity of Mercy Hospital. The 11 foot lane and 5 foot shoulder requests will be granted. No design exceptions will be necessary as those widths are allowed under current design for rehabilitation of an arterial.

Also, the Department will be lowering the posted speed limit to 35 mph once the Veteran's Bridge work is done. Given the new configuration of the intersections and proximity of the signals, a speed reduction make sense here and also brings the existing crosswalk into compliance.

If you have any further requests on this matter, please feel free to contact me via e-mail at Stephen.landry@maine.gov or by phone at 624-3632.

Sincerely,

Stephen Landry
Assistant State Traffic Engineer

Cc Joe Gray, City Manager
Katherine Early, Engineering Services Manager
Alex Jaegerman, Planning Division Manager
Lucie Cote, Traffic Operations Coordinator
Tom Errico, TY Linn, Traffic Engineering Consultant
Jeff Folsom, Project Manager
Brian Keezer, Region Traffic Engineer



PRINTED ON RECYCLED PAPER

Jean Fraser - Question: Mercy Left Turn Performance Guarantee and Moratorium Street fees

From: Jean Fraser
To: Jaegerman, Alex
Date: 8/10/2010 2:19 PM
Subject: Question: Mercy Left Turn Performance Guarantee and Moratorium Street fees
CC: Barhydt, Barbara; DiPierro, Philip
Attachments: MERCY LEFT TURN COST ESTIMATE.pdf

Alex,

I am assuming that the amount the city requests for the Performance Guarantee and the Moratorium Street fee is of political sensitivity (with both Mercy and MDOT) because the cost of the "Mercy Left turn" work is being paid for out of "finite" savings from the MDOT not having to relocate and rebuild the existing slip road access to Mercy.

So if the City adds to the cost of the left turn road works through a high Performance Guarantee (which results in a high inspection fee as its 2% of the PG) and/or through the moratorium street fee, it may impact other negotiations and discussions including yours re the open space/regrading of the slip road for pedestrian access.

Phil has just received the cost estimate (copy attached) for the improvements to be covered by the Performance Guarantee (ie what was shown on Steve Bushey's plans which we just approved) and it totals **\$157,160** for "public" improvements and **\$143,402** for "private" improvements (these figures have not been checked or approved by Phil but at least give an order of magnitude).

I understand that the contractor (Grondin working for Reed and Reed) is being asked to pay the Perf Guarantee and Inspection Fee (I presume this is so it will be paid out of the MDOT purse).

As with other projects, I understand the City may make an alternative assessment and this is the question here.

I have asked David Margolis-Pineo re the Moratorium Street fee and he was going to check- he thinks maybe it would be waived. He has agreed with Steve Bushey (and presumably others are OK with this) that the City will do the final paving over the southern half of the Parkway within the MDOT timetable.

Please advise Phil (and me) as to how to move forward re finalizing the PG/Inspection fee.

This issue is also on the Dev Rev agenda just in case it needs wider discussion.

thanks
Jean

SUBDIVISION/SITE DEVELOPMENT
Cost Estimate of Improvements to be covered by Performance Guarantee

Date: 08/10/10

Name of Project: MERCY HOSPITAL - SOUTHERLY DRIVE MODIFICATIONS

Address/Location: 175 FORE RIVER PARKWAY

Application ID #: HTE-10-99900004

Developer: MERCY HOSPITAL

Form of Performance Guarantee: ?

Type of Development: Subdivision _____ Site Plan (Major/Minor) _____

TO BE FILLED OUT BY THE APPLICANT:

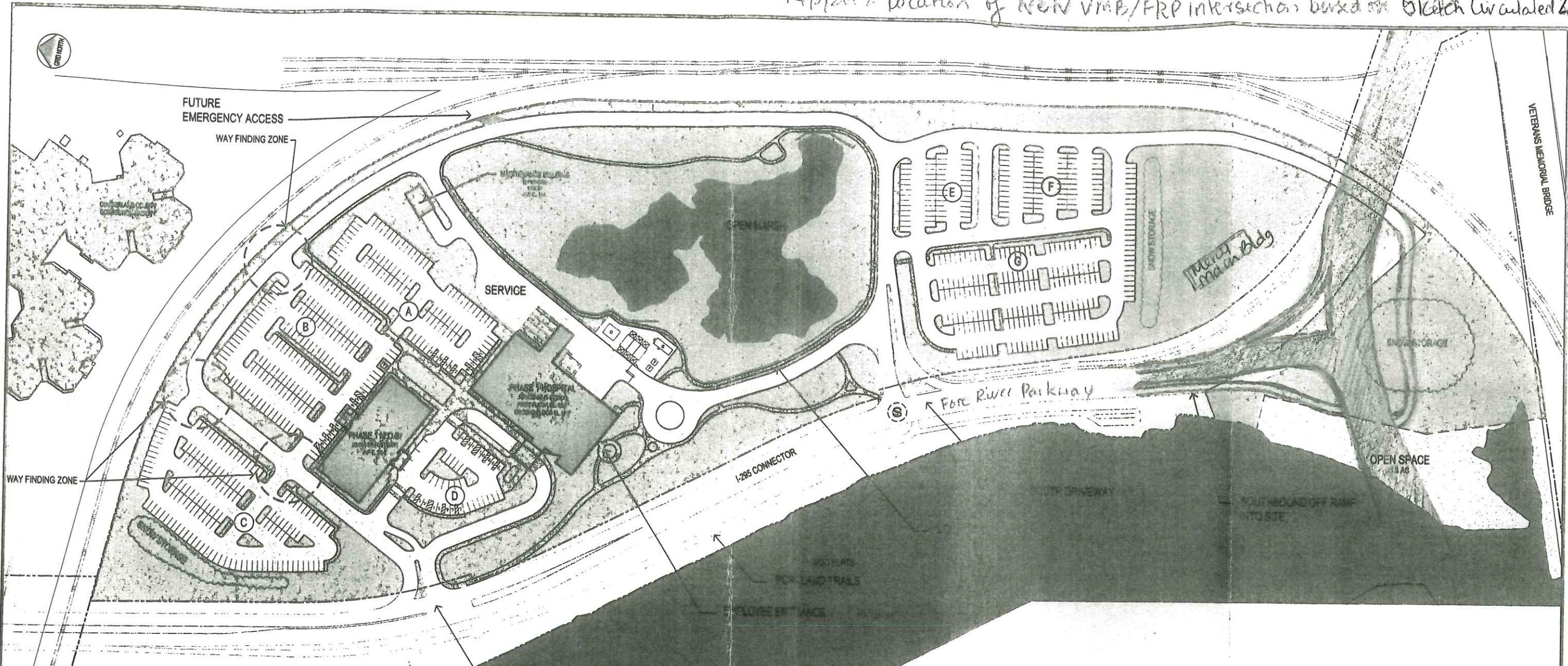
Item	PUBLIC			PRIVATE		
	Quantity	Unit Cost	Subtotal	Quantity	Unit Cost	Subtotal
1. STREET/SIDEWALK						
Road/Parking Areas	<u>5,200SF</u>	<u>5-</u>	<u>26,000-</u>	<u>4,200SF</u>	<u>275</u>	<u>11,550-</u>
Curbing	<u>210L.F.</u>	<u>26</u>	<u>5,460-</u>	<u>375L.F.</u>	<u>26-</u>	<u>9,750-</u>
Sidewalks	<u>N.A.</u>		<u>/</u>	<u>1,200SF.</u>	<u>185</u>	<u>2,220-</u>
Esplanades	<u>N.A.</u>		<u>/</u>	<u>NA</u>		<u>/</u>
Monuments	<u>N.A.</u>		<u>/</u>			<u>/</u>
Street Lighting	<u>N.A.</u>		<u>/</u>			<u>/</u>
Street Opening Repairs	<u>?</u>		<u>/</u>	<u>NA</u>		<u>/</u>
Other	<u>1L.S.</u>	<u>11-</u>	<u>/</u>			<u>/</u>
2. EARTH WORK						
Cut	<u>500cy</u>	<u>8-</u>	<u>4,000-</u>	<u>120cy</u>	<u>8-</u>	<u>960-</u>
Fill	<u>450cy</u>	<u>18-</u>	<u>8,100-</u>	<u>100cy</u>	<u>18-</u>	<u>1,800-</u>
3. SANITARY SEWER						
Manholes	<u>N.A.</u>		<u>/</u>	<u>N.A.</u>		<u>/</u>
Piping	<u>↑</u>		<u>/</u>	<u>↑</u>		<u>/</u>
Connections	<u>↑</u>		<u>/</u>	<u>↑</u>		<u>/</u>
Main Line Piping	<u>↑</u>		<u>/</u>	<u>↑</u>		<u>/</u>
House Sewer Service Piping	<u>↓</u>		<u>/</u>	<u>↓</u>		<u>/</u>
Pump Stations	<u>↓</u>		<u>/</u>	<u>↓</u>		<u>/</u>
Other	<u>NA</u>		<u>/</u>	<u>↓</u>		<u>/</u>
4. WATER MAINS	<u>NA</u>		<u>/</u>	<u>NA</u>		<u>/</u>
5. STORM DRAINAGE						
Manholes - ALTER	<u>4EA</u>	<u>500-</u>	<u>2,000-</u>	<u>NA</u>		<u>/</u>
Catchbasins	<u>3EA</u>	<u>2500-</u>	<u>7,500-</u>	<u>↑</u>		<u>/</u>
Piping	<u>17L.F.</u>	<u>100-</u>	<u>1,700-</u>	<u>↑</u>		<u>/</u>
Detention Basin	<u>NA</u>		<u>/</u>	<u>↑</u>		<u>/</u>
Stormwater Quality Units	<u>NA</u>		<u>/</u>	<u>↓</u>		<u>/</u>
Other	<u>NA</u>		<u>/</u>	<u>NA</u>		<u>/</u>

6. SITE LIGHTING & SIGNALS	NA			1CS	79,302	79,302-
7. EROSION CONTROL						
Silt Fence	900L.F.	150	1,350-	NA		
Check Dams	NA			↑		
Pipe Inlet/Outlet Protection	1CS	250	250-	↓		
Level Lip Spreader	NA					
Slope Stabilization	NA					
Geotextile	NA					
Hay Bale Barriers	NA					
Catch Basin Inlet Protection	3 EA	150	450	NA		
8. RECREATION AND OPEN SPACE AMENITIES	NA.					
9. LANDSCAPING - NEW	7 UN	50	350-	REPLACE EXISTING		2,900-
(Attach breakdown of plant materials, quantities, and unit costs)			57,160-			108,482-
10. MISCELLANEOUS	1CS		100,000-	1CS		35,000-
TOTAL:			157,160-		143,482	
GRAND TOTAL:						

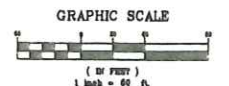
INSPECTION FEE (to be filled out by the City)

	PUBLIC	PRIVATE	TOTAL
A: 2.0% of totals:	_____	_____	_____
or			
B: Alternative Assessment:	_____	_____	_____
Assessed by:	_____	_____	_____
	(name)	(name)	

Approx. location of NEW VMB/FRP intersection based on Sketch Circulated 2/10



PARKING SUMMARY		
AREA	QUANTITY	DESCRIPTION
A	107	HOSPITAL PATIENT / VISITORS / PHYSICIANS
B	184	MOB PATIENT / VISITORS
C	181	MOB STAFF / VISITORS
D	38	HOSPITAL / MOB PATIENTS
E	63	HOSPITAL STAFF
F	70	HOSPITAL STAFF
G	202	HOSPITAL / MOB STAFF
TOTAL	783	



7.25.06	4	REVISED AND RESUBMITTED TO PLANNING AUTHORITY
5.31.06	3	REVISED AND RESUBMITTED TO PLANNING AUTHORITY
12.23.05	2	REVISED AND RESUBMITTED TO PLANNING AUTHORITY
12.14.05	1	REVISED AND RESUBMITTED TO PLANNING AUTHORITY

DH
 DeLuca-Hoffman Associates, Inc.
 778 MAIN STREET, SUITE 8
 SOUTH PORTLAND, ME 04106
 207.775.1121
 WWW.DELUCAHOFFMAN.COM

DRAWN:	DED	DATE:	12.07.05
DESIGNED:	-	SCALE:	AS NOTED
CHECKED:	SRB	JOB NO.	2149.01
FILE NAME:	2149.01-MP3		

**MERCY HOSPITAL
 FORE RIVER PROJECT**

MASTER PLAN - PHASE 1
 (copy of plan from Adopted July 2006 Master Plan Mercy)
 FIGURE
1 of 6

Jean Fraser - Approval letter for the Mercy Left Turn

From: Jean Fraser
To: Bushey, Steve; Connolly, William
Date: 8/3/2010 4:24 PM
Subject: Approval letter for the Mercy Left Turn
Attachments: 175 Fore River Parkway Mercy Hospital.pdf

Steve and Bill

Please find a pdf copy of the signed letter attached. The paper copies will go out in the mail tomorrow.

We appreciate there seem to be alot of conditions but we thought it better to get the approval letter out rather than go through another round of revisions. As I mentioned, these are based on Tom Errico's comments and I assume that he can elaborate if anything is unclear.

Please call if any questions.

Jean

Jean Fraser, Planner
City of Portland
874 8728



PORTLAND MAINE

Strengthening a Remarkable City, Building a Community for Life www.portlandmaine.gov

Planning & Urban Development Department
Penny St. Louis Littell, Director

Planning Division
Alexander Jaegerman, Director

August 3rd, 2010

Mr. William Connolly
Mercy Hospital
144 State Street
Portland, ME 04101

Mr. Stephen R. Bushey, PE
DeLuca Hoffman Associates Inc.
778 Main Street- Suite 8
South Portland, ME. 04106

**RE: Mercy Hospital – Amendment to Approved Site Plan:
Southerly Driveway Modifications to allow Left Turn
Vicinity 175 Fore River Parkway**

**Application ID Number: HTE 10-99900004
CBL: 073-A-001-001**

Dear Mr. Connelly and Mr. Bushey:

On August 3rd, 2010, the Portland Planning Authority approved the amended site plan for the approved Mercy Hospital Phase I development (approved application #2005-0192) to include modifications to the existing southerly driveway off the Fore River Parkway to allow a left turn (via a new left turn lane) into this driveway. Associated improvements include widening of the Parkway and reduction of the esplanade, modifications to signals and signal poles, relocation of the parking lot entrance and relocation/revisions to drainage and landscape.

The approved amendment is shown on plans prepared by DeLuca-Hoffman: *Roadway Improvement Plan* (Rev. 4 7.28.2010), *Striping and Signage Plan* (Rev. 2 7.28.2010); *Traffic Signal Plan* (Rev 1 7.27.2010) and associated sections and details dated 7.28.2010.

The approval is subject to the following conditions:

- i. That the applicant shall be responsible for incorporating signal equipment modifications, as necessary, such that it will allow for the subject traffic signal to be coordinated with traffic signals associated with the Veterans Memorial Bridge project; and
- ii. That the alignment of the sidewalk ramp and detectable warning device on the northeast corner of the subject driveway shall be modified to better meet ADA standards such that the crossing is constructed to be as close to a perpendicular alignment as reasonably practicable. The change shall be reviewed and approved by the Planning Authority prior to construction; and
- iii. That the applicant shall revise the proposals for the modification of the curbing at the entrance to the internal drop-off area to address traffic control, pavement markings, and pedestrian movements as the curb changes have created a very wide open intersection; these changes shall be reviewed and approved by the Planning Authority prior to construction; and

- iv. That a signage and pavement marking plan shall be provided for the internal intersection that shall be reviewed and approved by the Planning Authority prior to installation /construction; and
- v. That the proposed sign type and associated installation at the new parking lot entry driveway on-site shall be reviewed and approved by the Planning Authority prior to implementation; and
- vi. That the applicant shall add a note to the plan that states that all existing signs for the hospital that are invalid shall be removed by the applicant with approval from the Planning Authority.
- vii. That a detail for the recessed reflectors shall be submitted for review and approval by the Planning Authority prior to construction; and
- viii. That the applicant shall relocate and augment any disturbed planting so that there is no net loss of planting in the vicinity of the improvements, and complete all associated landscaping and planting within six months of the re-opening of the southernmost driveway to traffic access.

The approval is based on the submitted plans. If you need to make any modifications to the approved site plan, you must submit an amended site plan for staff review and approval. This letter also confirms the following:

- a. The Planning Authority has granted a waiver from the City's Technical Standards for the driveway width where it meets the Fore River Parkway.
- b. The Fore River Parkway is a State moratorium road. It is noted that changes in curbing alignment and alterations to system drainage will require cutting into the roadway and the permanent repair may require grinding and full overlay of the disturbed area.

Please note the following provisions and requirements for all site plan approvals:

1. Where submission drawings are available in electronic form, final sets of plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (*.dwg), release AutoCAD 2005 or greater.
2. A performance guarantee covering the site improvements as well as an agreed inspection fee payment (normally 2.0% of the guarantee amount) and seven (7) final sets of plans must be submitted to and approved by the Planning Division and Public Works prior to the release of the street opening permit. If you need to make any modifications to the approved plans, you must submit a revised site plan application for staff review and approval.
3. The site plan amendment approval will be deemed to have expired unless work in the development has commenced within one (1) year of the approval or within a time period agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the expiration date.
4. A defect guarantee, consisting of 10% of the performance guarantee, must be posted before the performance guarantee will be released.
5. Prior to construction, a pre-construction meeting shall be held at the project site with the contractor, Planning Division's Development Review Coordinator, Public Work's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the pre-construction meeting.

6. If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)
7. The Development Review Coordinator must be notified five (5) working days prior to date required for final site inspection. The Development Review Coordinator can be reached at the Planning Division at 874-8632. Please make allowances for completion of site plan requirements determined to be incomplete or defective during the inspection. This is essential as all site plan requirements must be completed and approved by the Development Review Coordinator prior to the release of the performance guarantee.

If there are any questions, please contact Jean Fraser at 874 8728 or jf@portlandmaine.gov.

Sincerely,



Alexander Jaegerman
Planning Division Director

Attachment: Performance Guarantee Packet

Electronic Distribution:

Penny St. Louis Littell, Director of Planning and Urban Development
Alexander Jaegerman, Planning Division Director
Barbara Barhydt, Development Review Services Manager
Jean Fraser, Planner
Philip DiPierro, Development Review Coordinator
Marge Schmuckal, Zoning Administrator
Tammy Munson, Inspections Division
Gayle Guertin, Inspections Division
Lisa Danforth, Inspections Division
Lannie Dobson, Inspections Division
Michael Bobinsky, Public Services Director
Kathi Earley, Public Services

Bill Clark, Public Services
David Margolis-Pineo, Deputy City Engineer
Greg Vining, Public Services
John Low, Public Services
Jane Ward, Public Services
Keith Gautreau, Fire Prevention
Jeff Tarling, City Arborist
Tom Errico, City Traffic Engineering Reviewer
Dan Goyette, Woodard & Curran
Assessor's Office
Approval Letter File

Hard Copy: Project File

Jean Fraser - Mercy Hospital Left-turn Lane Application - Traffic Comments

From: Thomas Errico <Thomas.Errico@tylin.com>
To: Jean Fraser <JF@portlandmaine.gov>
Date: 7/21/2010 3:03 PM
Subject: Mercy Hospital Left-turn Lane Application - Traffic Comments
CC: Katherine Earley <KAS@portlandmaine.gov>, David Margolis-Pineo <DMP@port...

Jean – The following summarizes my detailed comments regarding the provision of a left-turn lane on Fore River Parkway at the southerly Mercy entrance. I would note that the analyses indicate that left-turn movements will operate at an excellent level of service with little delay and queuing. Overall the intersection will also operate at an acceptable level of service. The left-turn lane is being designed to be very conservative, in terms of the storage capacity, and spillback of vehicles into the through lane is highly unlikely. I would note that these conclusions are based upon a full-build out of the Mercy Campus and traffic volume levels on Fore River Parkway projected to the year 2030. Lastly, I would note that the implementation of the left-turn lane will help to reduce confusion on access/egress routing for Mercy's customers. Current conditions are confusing and illegal movements and general complaints are frequent.

Signal Plan

- A note shall be added that indicates the traffic signal shall meet City of Portland specifications.
- A note shall be added that notes existing signal equipment not used shall be retained by the City of Portland and shall be delivered to a location as directed by the City.
- Note 10 should indicate that conflicting markings shall be grinded via a method as approved by the City.
- An overlap right-turn phase should be added for right-turns from the Mercy Driveway.
- It should be noted that the City will be requesting a speed limit change to 35MPH. Accordingly, the design should account for this change, as appropriate.
- It is preferred that the Walk time be 7 seconds.
- The proposed entry driveway seems to be excessively wide. The plan indicates a width of approximately 48 feet. An Auto-turn analysis should be provided supporting the proposed width.

General Plan

- Based upon the condition of the stamped pavement crosswalk and the amount of disturbed area, it is recommended that the entire crosswalk be replaced.
- The painted arrows entering the site (a total of five) should be deleted.
- The painted arrow of Fore River Parkway downstream departing the intersection to the south should be deleted.
- The left-turn lane should have additional painted arrows and "ONLY" symbols.
- A post-mounted lane assignment sign should be installed at the beginning of the left-turn lane.
- The plan should include the removal of all existing signs that will no longer be relevant due to the removal of the slip lane. The plan should also include any desired signs for advising motorist of the new Mercy entrance.
- A detail should be provided on the construction of the recessed reflectors.
- A detail should be provided for the stamped pavement.
- The STOP painted symbol on Fore River Parkway should be deleted from the plan.
- The plans illustrate conflicting information on the width of the left-turn lane (11 or 12 feet). The applicant should provide clarification.

- A proposed sign is noted on the plan at the new parking lot entry driveway on-site. The sign type should be clarified.
- The modification of the curbing at the internal drop-off area needs to address traffic control, pavements markings, and pedestrian movements. The curb changes have created a very wide open intersection.

If you have any questions, or comments, please contact me.

Best regards,

Thomas A. Errico, P.E.

TYLININTERNATIONAL

12 Northbrook Drive
Building A, Suite One
Falmouth, ME 04105

207.347.4354 (Direct)
207.781.4721 (Main)
207.781.4753 (Fax)
207.400.0719 (Mobile)

Jean Fraser - Mercy Hospital - Southerly Driveway Modifications

From: Thomas Errico <Thomas.Errico@tylin.com>
To: Barbara Barhydt <BAB@portlandmaine.gov>
Date: 7/30/2010 1:32 PM
Subject: Mercy Hospital - Southerly Driveway Modifications
CC: Katherine Earley <KAS@portlandmaine.gov>, David Margolis-Pineo <DMP@port...

Barbara – The following presents a status update on the project according to revised plans transmitted by Deluca-Hoffman Associates, Inc. on July 28, 2010

Signal Plan

- A note shall be added that indicates the traffic signal shall meet City of Portland specifications.
Status: A note has been added and I have no further comment.
- A note shall be added that notes existing signal equipment not used shall be retained by the City of Portland and shall be delivered to a location as directed by the City.
Status: A note has been added and I have no further comment.
- Note 10 should indicate that conflicting markings shall be grinded via a method as approved by the City.
Status: The note has been revised and I have no further comment.
- An overlap right-turn phase should be added for right-turns from the Mercy Driveway.
Status: The plan has been revised and I have no further comment.
- It should be noted that the City will be requesting a speed limit change to 35MPH. Accordingly, the design should account for this change, as appropriate.
Status: No comment necessary.
- It is preferred that the Walk time be 7 seconds.
Status: The plan has been revised accordingly.
- The proposed entry driveway seems to be excessively wide. The plan indicates a width of approximately 48 feet. An Auto-turn analysis should be provided supporting the proposed width.
Status: I have reviewed the revised design plans and find the design to be acceptable. The driveway width exceeds the maximum width noted in the City's Technical Standards and I support a waiver from this standard.

General Plan

- Based upon the condition of the stamped pavement crosswalk and the amount of disturbed area, it is recommended that the entire crosswalk be replaced.
Status: The plan has been revised and I have no further comment.
- The painted arrows entering the site (a total of five) should be deleted.
Status: The plan has been revised and I have no further comment.

- The painted arrow on Fore River Parkway downstream departing the intersection to the south should be deleted.

Status: The plan has been revised and I have no further comment.

- The left-turn lane should have additional painted arrows and "ONLY" symbols.

Status: The plan has been revised and I have no further comment.

- A post-mounted lane assignment sign should be installed at the beginning of the left-turn lane.

Status: A sign has been added and I have no further comment.

- The plan should include the removal of all existing signs that will no longer be relevant due to the removal of the slip lane. The plan should also include any desired signs for advising motorists of the new Mercy entrance.

Status: A note should be added to the plan that states all existing signs for the hospital that are invalid shall be removed by the applicant with approval from the City.

- A detail should be provided on the construction of the recessed reflectors.

Status: A condition of approval should be the provision of a detail for the recessed reflectors for review and approval by the City.

- A detail should be provided for the stamped pavement.

Status: A detail has been provided and I have no further comment.

- The STOP painted symbol on Fore River Parkway should be deleted from the plan.

Status: The plan has been revised and I have no further comment.

- The plans illustrate conflicting information on the width of the left-turn lane (11 or 12 feet). The applicant should provide clarification.

Status: The plans have been revised and two 11-foot southbound travel lanes will be constructed. It is my understanding that MaineDOT has approved the width and a copy of their decision should be obtained from Mike Bobinsky. I have no further comment.

- A proposed sign is noted on the plan at the new parking lot entry driveway on-site. The sign type should be clarified.

Status: This comment is outstanding. I would include a condition of approval that the sign installation shall be reviewed and approved by the City.

- The modification of the curbing at the internal drop-off area needs to address traffic control, pavement markings, and pedestrian movements. The curb changes have created a very wide open intersection.

Status: No changes or response has been provided. I would include a condition of approval that a signage and pavement marking plan be provided for the internal intersection that shall be reviewed and approved by the City.

Additional Comments

- The alignment of the sidewalk ramp and detectable warning device on the northeast corner of the subject driveway shall be modified to better meet ADA standards such that the crossing is constructed to be as close to a perpendicular alignment as reasonably practicable. The change shall be reviewed

and approved by the City.

- **The applicant shall be responsible for incorporating signal equipment modifications, as necessary, such that it will allow for the subject traffic signal to be coordinated with traffic signals associated with the Veterans Memorial Bridge project.**

If you have any questions, or comments, please contact me.

Best regards,

Thomas A. Errico, P.E.

TYLININTERNATIONAL

12 Northbrook Drive
Building A, Suite One
Falmouth, ME 04105

207.347.4354 (Direct)
207.781.4721 (Main)
207.781.4753 (Fax)
207.400.0719 (Mobile)

Mercy L - turn

ZONING ADMINISTRATOR – MARGE SCHMUCKAL

JULY 23, 2010

This proposal is concerned with traffic changes and does not include anything that would be reviewable under zoning. No zoning comment is included here.



PORTLAND MAINE

Strengthening a Remarkable City, Building a Community for Life • www.portlandmaine.gov

Public Services Department

Michael J. Bobinsky

July 23, 2010

Mr. Jeff Folsom, PE
MaineDOT
16 State House Station
Augusta, ME 04333

Subject: Fore River Parkway Design Criteria; Mercy Hospital Fore River Entrance- Portland

Dear Jeff,

The City of Portland requests that MaineDOT consider the following design criteria associated with construction of a left turn lane at Mercy Hospital's southerly entrance to the Fore River Parkway, which is classified as an arterial roadway:

- Eleven foot wide left turn lane on the Parkway for traffic turning into the driveway. The desirable lane width is twelve feet with eleven feet being the minimum.
- Eleven foot wide travel lane in the vicinity of the left turn lane for southbound through traffic (northbound would remain twelve feet). The required lane width is twelve feet.
- Five foot wide shoulder in the vicinity of the left turn lane. The required width is eight feet.

As discussed at our meeting on July 23, 2010 that was attended by the Veterans Bridge design team, City staff, Steve Landry, and the public, the reduction in lane widths are context sensitive design elements that would preserve as much of the Parkway green space as possible. Reduction of the lane widths is very important to the City to respect the original design intent of the facility as a "Parkway". We believe the reductions in widths would not be detrimental to the traveling public for the following reasons:

- There is no on street parking in this area, which will limit side friction.
- The City's Fore River Trail crosses the Parkway at this intersection, and reducing crossing widths is desirable for pedestrian safety and as a result also improves efficiency of operation for the traffic signal by allowing a shorter pedestrian clearance time.
- There is limited truck traffic along the corridor.
- The proposed left turn lane will likewise serve limited truck traffic.
- The posted speed limit on the facility is currently 40 mph, but the City requests that MaineDOT consider a 35 mph posting.

Mercy - Left Hand Turn
"Scoping" Meeting
Fri. July 23, 2010.

S. Landry
A.J. / J.F. / T.E. / M.B.
S. Bushey / B. Connelly
Reed, Reed
Anne Pringle

1. Alex in hrs + introductions
2. Steve Bushey described ex. context eq slip lane and proposal -
capacity is good for L turn.
described physical work involved
3. Alex - clar. S-bd. on PK Way runs w/ L turn signal here would be cool w/ new Vet Br. Sigs
4. Anne Pringle - asked if timing slips for peak ans yes.
- ? re mercy shift pattern
ans. mercy moved to 12 hr. shift 7-7
so on edge of peak
5. Tom Emico - expl. assumptions re forecasting traffic
- robust re assumption of growth.
- wd have detectors for those turning left
6. S. Landry - N-bound flow low in AM.
so opposing vol low for the 500 wanting to turn left in AM.
7. Mike B - do we need more equipment

8. AP - loss of esplanade on west side
SB now 18-35' - losing 12'
moving trees?
9. SL - doesn't req imp amend. as folded into VMB
know Portland resp.
10. MB - no major adverse impact
make sure equipment piece near. to
cov. of signals possible
ADElectric - familiar with their work.
11. TE - re typical section - is 12' lane for both.
taper min 5' shoulder.
12. AP - concerned re losing 12' to allow for
12' lanes as this is a Parkway w/
limited access / speeds.
why can't have 10' lane for stacking lane
13. Sh - min. 11 ft. for trucks maneuvering
(Mercy does have delivery trucks)
Mercy - this would be ambulance entrance
as campus expands.
14. Mercy plans - 20' S Emer Rm. to rear of
existing hospital. so S. drive would
be ambulance access. + metro.
involve 2 pkg structures, 1 next ex MOB; 1 on ^{site of} S _{1st} pkg _{lot}

15. AP - any poss. shifting to save 12' ; trees are close to impacted.

16. TE - would like to keep 5' bike lane all along

17. SL - OK w/ 11' turn lane if City wants

AJ - noted cyclists can use trail.

these both would result in reduction of loss of esplanade.

18. TE - these changes would affect geometry of intersection but SB OK with this

19. AP - qn. to Mercy - poss. pkg area for ppl ~~using~~ wanting to walk onto the Bridge to look at things. [discussion - could park re R+K new park but access up to bridge via Mercy property]

20. Timescales: Summer 2012 new bridge open
Winter 2012 old bridge down
before then Mercy pkg deck on S. parking area

21. MB - qn. re snow storage short/long term

22. Mercy looking at snow melter for upper decks of parking

MB said City also looking at this technology.

described: boiler (biodiesel / nat gas) +

then filter etc → catch basin.

* cost
\$100,000's +
man.

23. AJ unkd closing comments

Art Cal - RR prop. temp L turn ^{thru} agreed.
need L-turn completed by Oct 15
need 8 weeks to construct

24. BK - Mercy concerned that temp L turn doesn't allow through traffic during construction so pref. to get perm. turn lane in new

25. AJ - net loss of 8' ft along Parkway
if slip road goes away may be benefits
+ access

26. MB to send letter MDOT requesting 11' turn
lane, and ^{11' S-bd. lane, and} 5' shoulder

27. TE - for at-grade crossing near Fred St. need
red. in speed to 35 mph., ^{so 11' lane prob} OK.

SL - shortened up Parkway so totally
diff animal.

28. AP - settlement at RR - ~~needs~~ will be
repaired

29. AP - VMB - Valley St take more traffic
TE suggests would prob. be less.

30. MB/
TE to add in re signal equipment.

31. SL - timings are conjecture - don't know ^{new patterns &}
idea road past - Budget assessment/tweaking

30. SL - have modelled but not sure reality will be same so need post - const. review.

31. MB - ^{need to} assess hardware

32. AP - concerned re location of boxes
TE confirmed are shown on plan so look these located so can adjust while viewing actual signals

33. - Process going forward
Resubmission of plans to City
SL needs to review City's reg re 11' lanes
Mike B to send letter to MDOT Jeff ^{5'} → Steve
Alex promised S. Parnood App. by end of next week.

Jean Fraser - Re: Clr Notice re mercy amend- new Left Turn

From: Jean Fraser
To: Barhydt, Barbara
Date: 7/15/2010 9:38 AM
Subject: Re: Clr Notice re mercy amend- new Left Turn

Jenn told me you were doing the usual notice to the Ward councilor regarding the application to amend the Mercy Site Plan; so I just wanted you to tell the councilor about the Scoping meeting on 7.23 and gave you the info to pass on to that Clr.

>>> Barbara Barhydt 7/14/2010 4:55 PM >>>
I am sorry, but I don't understand what you want me to do.

Barbara

>>> Jean Fraser Wednesday, July 14, 2010 4:53 PM >>>
It went ut Monday; the postcards should have gone out yesterday.

>>> Barbara Barhydt 7/14/2010 4:05 PM >>>
Has this already gone out or do you want to ask Jennifer to forward it to the interested citizens list?

Barbara

>>> Jean Fraser Wednesday, July 14, 2010 2:50 PM >>>
Barbara,

Please be sure to include a reference to the TMP scoping meeting as per the notice (below) that has gone to the interested parties (as per Penny/Alex instruction; agreed with Alex on Monday).

thanks
Jean

NOTICE:

IMPORTANT NOTICE FROM THE CITY OF PORTLAND PLANNING DIVISION TO INTERESTED PARTIES

RE: FORE RIVER PARKWAY; MERCY SOUTHERLY DRIVE ENTRANCE

The Planning Division has received a site plan amendment application to create a southbound left turn access into Mercy Hospital from the Fore River Parkway at the southerly signaled drive entrance. The project creates a new left turn lane, alterations to the existing signals, a relocated entrance to Mercy's southern parking lot, and associated improvements. The proposed left turn would eliminate the existing southbound slip road access to Mercy Hospital.

The new left turn represents a modification to the MDOT's 2002 Mercy Hospital Traffic Movement Permit (TMP) for access to/from the Fore River Parkway, which is now being substantially redesigned at the southern end to incorporate the Veteran's Bridge Intersection redesign. The site plan amendment will be reviewed administratively.

A **Scoping Meeting** with the MDOT and applicant will take place on:

Friday, July 23

9:00 am

Room 209, City Hall.

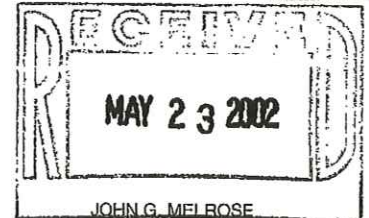
Members of the public are welcome to attend and comment.

Plans are available in the Portland Planning Division, 4th Floor, City Hall. For further information or if you wish to submit comments, contact Jean Fraser, Planner at 874-8728 or jf@portlandmaine.gov



ANGUS S. KING, JR.
GOVERNOR

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE
04333-0016



2149/47

Developer: Mercy Health Systems of Maine
Location: Between the Fore River and St. Johns Street on the proposed I-295 connector in Portland
Project: Ambulatory Care Unit, Hospital and Medical Offices
Identification Number: Div. 06-00035-A-N

Pursuant to the provision of 23 M.R.S.A. § 704-A and Chapter 305 of the Department's Regulations, the Department of Transportation has considered the application of Mercy Health Systems of Maine with supportive data, agency review and other related materials on file.

Project Description

The applicant proposes to construct a 60,000 square foot Ambulatory Care Unit, 75,000 square foot Medical Office Building, and a 300,000 square foot hospital with 250 beds. The development is expected to generate 784 passenger car equivalent trips in the a.m. peak hour of adjacent street, 687 passenger car equivalent trips in the p.m. peak hour of adjacent street and 990 passenger care equivalent trips in the p.m. peak hour of generator.

Findings

Based on the findings of fact, the Department approves the Traffic Movement Permit application Mercy Health Systems of Maine, subject to the following conditions:

MITIGATION

On Site Mitigation

- A. The project will provide two driveways onto the proposed Commercial Street Extension (CSE). The northerly most driveway will provide two entrance lanes and a single right turn only exit lane with divisional island to channelize the right turning traffic from the site. The southerly driveway will provide a single right turn in only entrance lane with channelizing divisional island and separate left and right turn exit lanes. A third driveway will be provided toward the rear of the site for emergency and service vehicles only. An existing access drive to the correctional facility toward the back of the site will be relocated to align with the third proposed driveway.
- B. Mercy shall provide and erect the necessary signs at each of the two driveways to convey the prohibited turning movements.

PRINTED ON RECYCLED PAPER

C. A right turn lane 100 feet in length plus appropriate taper shall be provided on the CSE for right turning vehicles into the southerly site driveway.

D. A left turn lane 200 feet in length plus appropriate taper and deceleration length shall be provided on the CSE to provide for queued left turning vehicles into the site at the northerly driveway.

E. Underground conduit shall be installed at the intersection of the CSE / southerly site driveway for potential future signalization.

F. Provide a dual use pole with mast arm over the CSE at the southerly site driveway to support intersection lighting, a pedestrian actuated flasher, and a NO LEFT TURN symbol sign for southbound vehicles. Mercy shall be responsible for providing and installing the entire assembly with associated pedestrian actuated push button.

G. Mercy shall provide overhead lighting at the CSE / northerly site driveway intersection and at the emergency entrance at the rear of the site.

Off Site Mitigation

H. An off ramp to the site will be provided on the westerly side of the CSE to provide access to the site for southbound vehicles which would typically be accommodated via a left turn movement on the CSE at the southerly driveway. Left turns into the site off the Connector Road at the southerly driveway will be prohibited.

I. Provide overhead lighting at the intersection of the CSE and the southbound Mercy off ramp.

J. Underground conduit shall be installed at the intersection of the CSE / I-295 Northbound Off Ramp for potential future signalization.

K. Mercy Medical Facilities shall place \$150,000 in an escrow account for traffic signals and potential signal system interconnection for the intersections of the CSE with the southerly driveway and the I-295 northbound off ramp. These escrow accounts shall be provided prior to construction of each development phase and held until a signal warrant analysis is completed by Mercy 6 months after the opening of each phase. If the escrow money is not used within 12 months of the last development phase or ten years of the escrow agreement date (whichever comes first), the money and all accrued interest shall be returned to Mercy Health Systems.

L. Stevens Avenue (at the intersection with Congress Street) shall be restriped to reflect a left and a left/through/right turn lane. Congress Street shall be restriped to provide for receiving the Stevens Avenue dual left turn lanes. The City recognizes and accepts the responsibility for improvements necessary at this intersection to allow the intersection to operate at acceptable levels of service. This work shall be done within three years of the hospital opening.

M. The applicant shall provide the support necessary, such as signal equipment and/or monetary support, to ensure the timing and phasing indicated in the Traffic Impact Study, dated December 2001 and with associated supplements, can be realized in the field at the study area intersections.

N. The applicant shall place \$ 83,000 dollars in escrow to be applied toward unspecified future improvements for the highway overlap section of Route 22 and Route 114 in the towns of Scarborough and Gorham. If the escrow money is not used within ten years of the escrow agreement date, the money and all accrued interest shall be returned to Mercy Health Systems.


O. Mercy Health Systems of Maine shall provide a \$ 5,000 contribution to the City for construction of a traffic signal at the intersection of Commercial Street and Park Street. The City has recognized forecast queues may exceed existing roadway lengths and has accepted responsibility for future improvements if required. All details associated with this payment are between the City of Portland and Mercy Health Systems and shall be built within within three years of development opening if warranted.

Signage (ground and overhead) and striping shall be in accordance with the Manual on Uniform Traffic Control Devices (MUTCD) and the Maine Department of Transportation Highway Design Guide.

Because the proposed project effects the state highway and drainage systems and requires improvement to that system, the applicant must obtain approval of the design plans and coordinate work through MeDOT's State Traffic Engineer, who can be reached at (207)-287-3775 in Augusta.

624-3620 (SB)

By:



Bruce A. Ibarquer, P.E.
State Traffic Engineer

Date: 5-13-02



DeLUCA-HOFFMAN ASSOCIATES, INC.
CONSULTING ENGINEERS

778 MAIN STREET
SUITE 8
SOUTH PORTLAND, MAINE 04106
TEL. 207 775 1121
FAX 207 879 0896

- SITE PLANNING AND DESIGN
- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- PERMITTING
- AIRPORT ENGINEERING
- CONSTRUCTION ADMINISTRATION

July 28, 2010

Ms. Barbara Barhydt
City of Portland Planning Authority
4th Floor, City Hall
389 Congress Street
Portland, ME 04101

**Subject: Mercy Hospital – Fore River Project
 Southerly Driveway Modifications**

Dear Barbara:

In accordance with our discussions at the Public Meeting on July 23, 2010, we are submitting revised plans related to Mercy's request to modify their existing southerly driveway off the Fore River Parkway. The plan revisions address comments that we received from Tom Errico as well as from the Department of Transportation. The revisions include the following:

- Development of an auxiliary 11' wide left turn lane beginning just south of Mercy's northerly driveway along the Parkway.
- The auxiliary lane work will involve widening along the Parkway basically between the two existing driveways. We have revised the plans to include an 11' wide left turn lane, an 11' wide thru lane and a 5' wide shoulder in response to comments made by the public at last weeks meeting. We understand from an email exchange with Mr. Steve Landry of the MDOT that the Department has approved the City's request to allow the two 11' wide lanes and 5' shoulder configuration. As you know, the existing road section consists of an approximately 12-foot wide travel lane and an 8-foot wide paved shoulder in each direction. The widening effort to create the auxiliary lane would involve widening of the Parkway by approximately 7-feet for a distance of approximately 1,000 LF along the west side of the Parkway. The widening will consist of a box section to match the existing. This consists of 24" of gravel and 6 1/2" of bituminous asphalt.
- The existing raised curbed island just south of the Mercy's northerly driveway will be modified to create a tapered and fully shadowed approach to the left turn lane, while the existing through lane will continue uninterrupted through to the proposed Veteran's Bridge intersection. A mountable raised concrete median island on the south side of the signaled intersection will provide separation between the travel lanes along the Parkway.
- Modifications to Mercy's driveway including widening and installation of an additional entering lane for the left turning movements. The current driveway configuration contains only a right turn entering lane movement and separate left and right turn exiting lanes. The

Ms. Barbara Barhydt
July 28, 2010
Page 2

left and right turn existing lanes will be shifted to allow for the new entering lane. We have chosen to minimize the reconfiguration of the existing landscaped island in Mercy's driveway as it currently contains the signal controller box, pedestrian signals, the crosswalk landing and drainage measures. For this reason the width of the overall Mercy driveway is somewhat wider than normal.

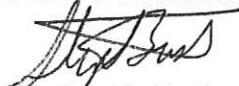
- The work will also involve modifications to the existing signal poles within the signalized intersection. Gorrill-Palmer Consulting Engineers has made the required modifications to their Traffic Signal Plan to address Mr. Errico's comment. The signal modifications include:
 1. Relocation of the existing Mast Arm Pole on the west side of the Parkway;
 2. Addition of a new mast arm pole on the east side of the Parkway for signal head(s) directing the Northbound traffic movements;
 3. Relocation of the two existing pedestal poles located on the west side of the Parkway just north of the Fore River Trail;
 4. Addition of audible pedestrian signals and countdown heads.

As you know, the Mercy and Reed & Reed team have held ongoing discussions with City representatives regarding this proposal. As this work is also part of the design-build VMB process we have been coordinating with DOT officials regarding the design parameters. We will continue to work with Department officials as the Reed & Reed team move forward with the VMB project. If approved by the City, Reed & Reed has scheduled the left turn lane work to begin within the next few weeks so that finish paving can be completed prior to mid October.

We have prepared seven copies of the revised plans at 11" x 17" for your distribution to City staff. A full size set of drawings will be delivered to Tom Errico directly. We look forward to your prompt consideration of this request. If you require any further information please call this office.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.



Stephen R. Bushey, P.E.
Senior Engineer

SRB/jpf/JN2149.06/Barhydt-07-28-10

c: Bill Connolly, Mercy Hospital
Art Cavanaugh, Reed & Reed
Tom Errico, T.Y. Lin
Jeff Folsom, MDOT

Attachments: A - Plans

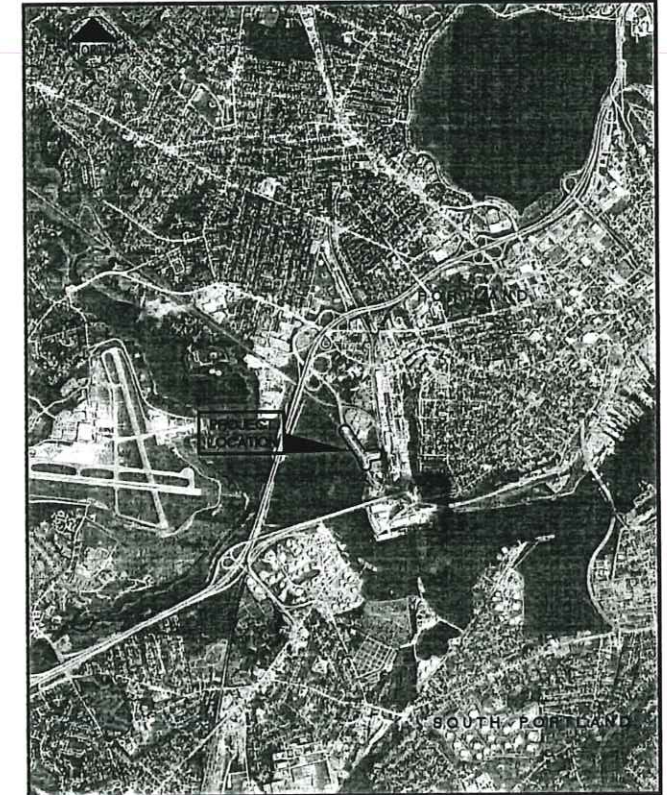
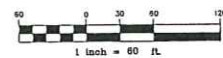
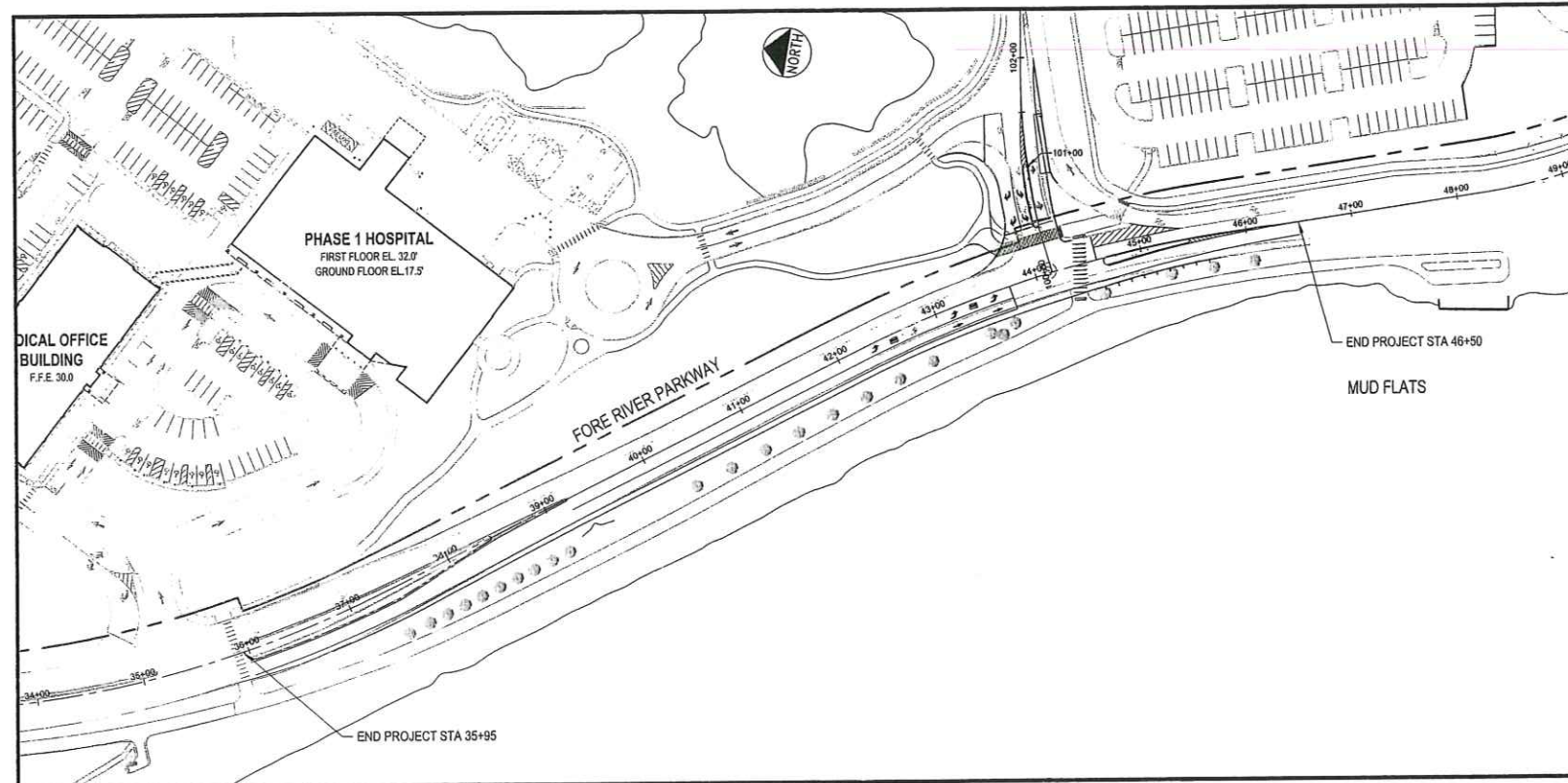
extra

FORE RIVER PARKWAY

PORTLAND, MAINE

A GRADING, SIGNAL, AND PAVEMENT WIDENING PROJECT

PROJECT LENGTH: 0.20 MILES



LOCATION MAP
R.E.S.

NOTES:

1. ALL WORK CONTEMPLATED UNDER THIS CONTRACT SHALL BE GOVERNED BY AND IN CONFORMITY WITH THE MDT STANDARD SPECIFICATIONS (REVISION OF DECEMBER 2002) FOR HIGHWAYS, AND SUPPLEMENTALS THERETO, EXCEPT AS MODIFIED ON THE PLANS AND IN THE SPECIAL PROVISIONS. NO WORK SHALL BE PERFORMED WITHIN THE MDT R.O.W. UNTIL WRITTEN CONSTRUCTION APPROVAL HAS BEEN ISSUED BY THE DEPARTMENT.
2. MAINTENANCE OF TRAFFIC TO BE DONE PER THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES - LATEST VERSION (MUTCD).
3. STATE OF MAINE DEPARTMENT OF TRANSPORTATION BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL REV. FEB. 2008 ARE TO BE USED AS THE MINIMUM STANDARD FOR EROSION AND SEDIMENT CONTROL ON THIS PROJECT.

INDEX

1. COVER SHEET
 2. GENERAL NOTES AND LEGEND
 3. TYPICAL SECTIONS AND SPECIAL DETAILS
 4. EXISTING CONDITIONS PLAN
 5. SITE PLAN
 6. STRIPING AND SIGNING PLAN
 7. TRAFFIC SIGNAL PLAN
- S1-S4. CROSS SECTIONS

UTILITIES

WATER
PORTLAND WATER DISTRICT
ATTN: RICO SPUGNARDI
225 DOUGLASS STREET
PORTLAND, ME 04104
207.774.5991

MUNICIPAL
CITY OF PORTLAND
ATTN: MIKE BOBINSKY
PUBLIC SERVICE DIVISION
389 CONGRESS STREET
PORTLAND, ME 04101

ELECTRIC
CENTRAL MAINE POWER CO.
ATTN: TOM ATWOOD
162 CANCO ROAD
PORTLAND, ME 04103
207.828.2860

TELEPHONE
FAIRPOINT
ATTN: MARTY PEASE
S DAVIS FARM ROAD
PORTLAND, ME 04103
207.797.1786

CABLE TV
TIME WARNER CABLE
ATTN: MARK PELLETIER
118 JOHNSON ROAD
PORTLAND, ME 04102
207.253.2291

CALL BEFORE YOU DIG
DIGSAFE 1.888.DIG.SAFE
(1.888.344.7233)

DESIGNED BY:

DeLUCA-HOFFMAN ASSOCIATES, INC.
778 Main Street Suite 8
South Portland, Maine 04106
207.775.1121

GORRILL-PALMER CONSULTING ENGINEERS

15 Shaker Road
Gray, Maine 04039
207.657.6910

PREPARED FOR:

MERCY HOSPITAL
IN ASSOCIATION WITH
REED & REED, INC.

THESE ROADWAY IMPROVEMENT PLANS HAVE NOT BEEN APPROVED BY THE MDT. CHANGES TO THESE PLANS MAY BE MADE TO ADDRESS THEIR COMMENTS. THE ROADWAY IMPROVEMENTS DEPICTED ON THESE PLANS SHALL NOT BE CONSTRUCTED UNTIL AUTHORIZED BY THE MDT AND THE CITY OF PORTLAND.

0:13:49:00 Concept/10/13/10 08:00:00 COV.dwg COVER, 7/28/2010 12:56:47 PM, dsaw

PROJECT		FORE RIVER PARKWAY LEFT TURN LANE	
SHEET TITLE		COVER SHEET	
CLIENT		MERCY HOSPITAL AND REED & REED, INC.	
DESIGNED BY	DATE	SCALE	DATE
DESIGNED	5.17.10	1" = 50'	5.17.10
CHECKED BY	SRB	JOB NO.	2148.06
FILE NAME	2148.06-COV	SHEET	1

GENERAL NOTES:

- Granular borrow used to backfill muck excavation or in low wet areas to 1' above water level or old ground shall meet requirements for granular borrow - underwater backfill.
- No existing drainage shall be abandoned, removed or plugged without prior approval of the Engineer.
- The culvert sizes shown on the plans and cross sections are smoothlined pipes unless otherwise noted.
- Unless otherwise noted, Seeding Method No. 1 shall be utilized on all lawns and developed areas.
- Mulch shall be applied in areas seeded by Seeding Method No. 1.
- Loam salvage shall be stockpiled at a designated site for placement on the sideslopes.
- Excavations accomplished as part of this project shall be constructed in accordance with Subpart P of 29 CFR Part 1926.850-852. (Construction Standards for Excavations). Additional excavation for the Contractor's convenience or to comply with backstopping requirements will not be paid for directly but will be considered incidental to the related drainage items.
- The contractor shall provide all layout necessary to complete the work. Coordinate layout data for centerline and improvement layout will be provided to the contractor prior to construction.
- Existing contours are based on NGVD 29 datum.

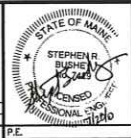
CONTRACTOR'S NOTES:

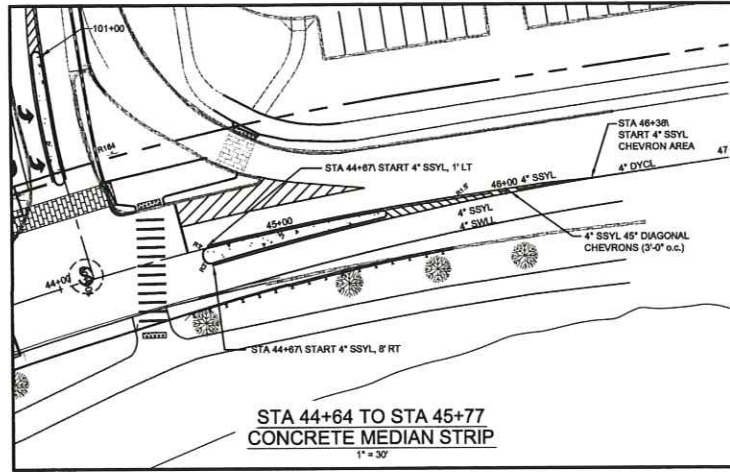
- Prior to making any highway improvements, THE CONTRACTOR shall obtain all necessary permits required to construct such improvements.
- Prior to making any highway improvements, THE CONTRACTOR shall submit a plan to control traffic during the period of construction to the DEPARTMENT for approval which conforms to the Federal Highway Administration's "Manual on Uniform Traffic Control Devices For Streets and Highways".
- Prior to making any highway improvements, THE CONTRACTOR shall comply with all necessary requirements as outlined under Section 103.08 in the DEPARTMENT'S "Standard Specifications for Highways and Bridges" (hereafter Standard Specifications). Minimum insurance requirements shall include at least Workers' Compensation Insurance, Commercial General Liability and Automobile Liability Insurance as defined therein. Additional insurance may be required where applicable. THE CONTRACTOR shall furnish to the DEPARTMENT satisfactory proof of such insurance so required. In the event that such insurance is terminated or canceled without being replaced with comparable insurance, the DEPARTMENT may suspend or terminate all highway construction in the progress at the time of such termination or cancellation.
- THE CONTRACTOR shall not make any highway improvements without the express written approval of the DEPARTMENT. THE CONTRACTOR shall provide the DEPARTMENT with a schedule of work for constructing such improvements prior to receiving such approval.
- The contractor shall call Dig Safe (1-888-DIG-SAFE) before performing any excavation in association with the proposed Highway improvements.
- All proposed highway improvements shall be constructed as shown on the Final Plans in accordance with the Standard Specifications.
 - The DEPARTMENT shall be allowed to inspect such construction and test any materials used therein at any and all times as necessary to assure compliance with the Final Plans or Standard Specifications.
 - The DEPARTMENT may reject any work or materials which do not conform to the Final Plans or Standard Specifications.
 - THE CONTRACTOR shall allow or arrange for the DEPARTMENT, its inspectors, agents, employees, contractors or invited guests, to enter upon any land outside of the highway right-of-way which may be used for construction of the highway improvements at any and all times and for any and all purposes necessary or incidental to such inspection or testing.
 - The placement of bituminous paving materials shall be subject to all of the weather and seasonal limitations outlined under Section 401.07 in the Standard Specifications.
 - All traffic shall be controlled during the period of construction in accordance with the traffic control plan approved by the DEPARTMENT.
- THE CONTRACTOR shall indemnify and hold harmless the DEPARTMENT and its inspectors, officers, employees and agents, against all claims arising from any injury or death to any of its officers, employees, workers, trespassers, licensees, invitees and all other persons in, on or extending from the construction area of the highway improvements or from any damage to or loss of property of any third party as a result of any act, omission, neglect or default of THE CONTRACTOR or any subcontractor thereunder, its and their officers, agents and employees, including reasonable attorney's fees. This indemnification provision shall not apply to any injury or death of any person or to any damage to or completion and acceptance of such work which is caused by an injury occurring prior thereto because of any act, omission, neglect or default of THE CONTRACTOR or any subcontractor thereunder, its and their officers, agents and employees, shall not be excluded from this indemnification provision simply because such death occurred after the completion and acceptance of such work. Acceptance of such work by the DEPARTMENT shall not relieve THE CONTRACTOR of any legal liability to the DEPARTMENT for any defect or negligence in the design and construction of the highway improvements. Notwithstanding the foregoing, neither the DEPARTMENT nor THE CONTRACTOR waives any statutory or common-law defenses available to them.
- The DEPARTMENT shall reserve the right to revoke its approval to construct any or all of the highway improvements upon notice for cause at any time that it determines such action appropriate and in the best interest of the public because of any failure by THE CONTRACTOR to comply with the Final Plans or Standard Specifications or any of the other terms or conditions outlined herein. However, in the interest of public safety, nothing in this provision shall prevent the DEPARTMENT from immediately suspending work for cause pending any necessary correction by THE CONTRACTOR.
- Upon completion of construction, the DEPARTMENT shall conduct a final inspection of all highway improvements to determine acceptability. The DEPARTMENT shall notify THE CONTRACTOR if any additional work is necessary to conform to the Final Plans or Standard Specifications. Upon acceptance, the DEPARTMENT shall notify THE CONTRACTOR of such acceptability in writing.
- All right, title and interest in and to all equipment, materials and supplies which become a part of the highway improvements shall vest in the DEPARTMENT upon attachment to the highway.
- Upon completion of all highway improvements, THE CONTRACTOR shall furnish the owner a set of redline markups with all asbuilt information. This shall include but not be limited to grade & location data of all drainage structures, ditches, as well as information to confirm the roadway was constructed to line & grade. The owner shall be responsible for submitting to the DEPARTMENT a set of reproducible "as-built" plans of archival quality suitable for permanent filing showing all of the highway improvements as constructed.
- Other notes associated with the work within the right-of-way are contained on the individual drawings.

LEGEND

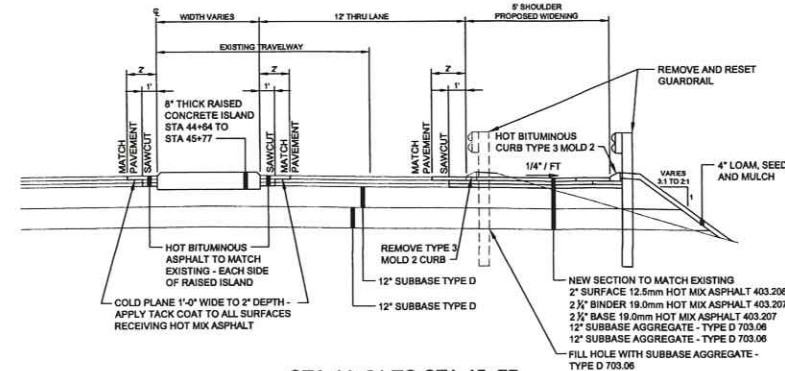
EXISTING	DESCRIPTION	PROPOSED
---	RIGHT OF WAY	
---	BASELINE	
●	SURVEY CONTROL POINT	
---	NOW OR FORMERLY	
---	EDGE OF PAVEMENT	
---	ADA SIDEWALK RAMP	
---	CURB - TYPE AS NOTED ON PLAN	
---	LIMIT OF GRADING CUT LINE	
---	LIMIT OF GRADING FILL LINE	
●	TREE	
●	MANHOLE (S=SEWER, D=DRAIN, E=ELECTRIC, T=TELEPHONE, W=WATER)	
●	UTILITY POLE	
●	GUY WIRE	
●	LIGHT	
●	CATCH BASIN	■ CB 1
●	SIGN	↑
---	GRADING CONTOUR LINE	
---	CHAIN LINK FENCE	
---	STORM DRAIN	12"SD
---	UNDERDRAIN	8"UD
---	CONTROLLER AND CABINET	⊠
---	MAST ARM POLE	⊙
---	MAST ARM	
---	SIGNAL HEAD	→ A3
---	LOOP DETECTOR AND LEAD IN	⊠ L2
---	PULL BOX (JUNCTION BOX)	■
---	MAST ARM MOUNTED SIGN	↑
---	MICROWAVE DETECTOR	→ P1
---	PEDESTRIAN SIGNAL HEAD	●
---	PEDESTRIAN BUTTON	●
---	SIGNAL CONDUIT	---

G:\2149.06\Concept\dwg\2149.06-COV.dwg, GEN NOTES, 7/28/2010 11:17:58 PM, dwgformat

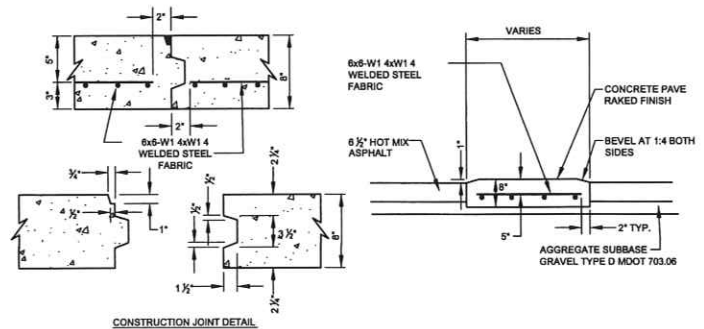
		PROJECT FORE RIVER PARKWAY LEFT TURN LANE	DeLUCA-HOFFMAN ASSOCIATES, INC. 700 MAIN STREET, SUITE 110 SOUTH PORTLAND, ME 04106 WWW.DELUCAHOFFMAN.COM
SHEET TITLE GENERAL NOTES AND LEGEND		DRAWN: DED DATE: 5.17.10 DESIGNED: SRB SCALE: 1" = 30' CHECKED: SRB JOB NO. 2149.06 FILE NAME: 2149.06-COV	CLIENT MERCY HOSPITAL AND REED & REED INC.
REV # DATE DESCRIPTION REVISIONS P.E. LIC. #	2 07.28.10 REVISED PER CITY / DOT COMMENTS AND RELEASED FOR REVIEW 1 07.14.10 RELEASED FOR REVIEW	SHEET 2	



STA 44+64 TO STA 45+77
CONCRETE MEDIAN STRIP
1" = 30'

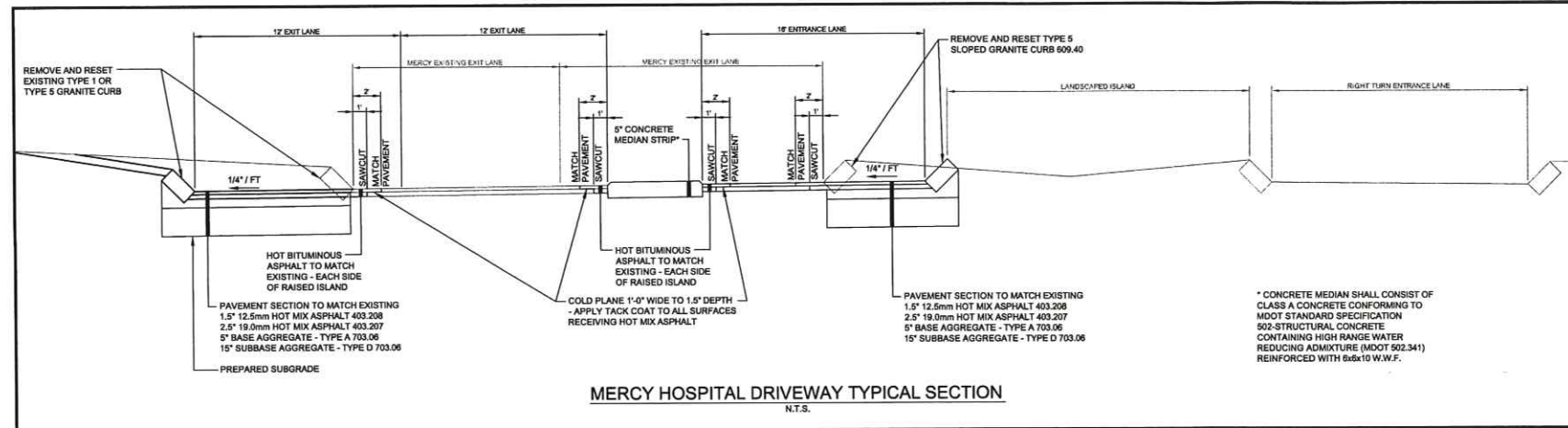


STA 44+64 TO STA 45+77
CONCRETE MEDIAN STRIP TYPICAL SECTION
N.T.S.

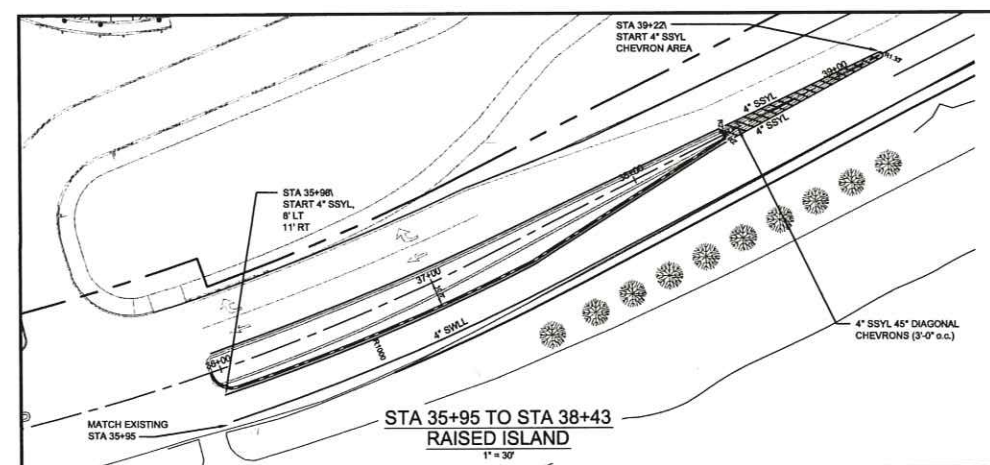


STA 44+64 TO STA 45+77
CONCRETE MEDIAN STRIP TYPICAL SECTION
N.T.S.

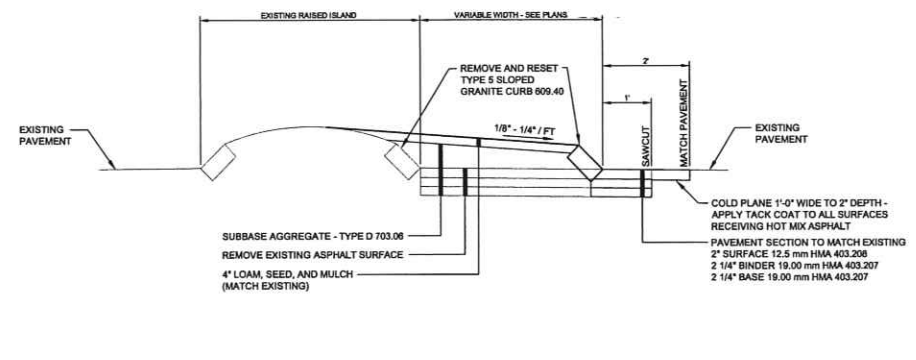
- NOTES:**
1. ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.
 2. SAWCUT VERTICALLY & REMOVE THE EXISTING PAVEMENT AS DIRECTED BY THE RESIDENT.
 3. APPLY A LIGHT COAT OF ASPHALT TO ACT AS A BOND BREAKER BETWEEN THE CONCRETE AND ADJACENT PAVEMENT.
 4. CONSTRUCT CONCRETE MEDIAN WITH CLASS A CONCRETE CONFORMING TO STANDARD SPECIFICATIONS SECTION 902 - STRUCTURAL CONCRETE CONTAINING HIGH-RANGE WATER REDUCING ADMIXTURE (SUBSECTION 701.0401).
 5. FILL ALL EXPANSION JOINTS AND SEAL WITH AN APPROVED JOINT SEALANT.
 6. ALL WORK DESCRIBED, INCLUDING REINFORCEMENT, WILL BE CONSIDERED TO BE INCLUDED IN ITEM 902.341 STRUCTURAL CONCRETE - ROADWAY MEDIAN, UNLESS OTHERWISE NOTED.
 7. CONSTRUCT EXPANSION JOINTS AT 10 FT SPACING.



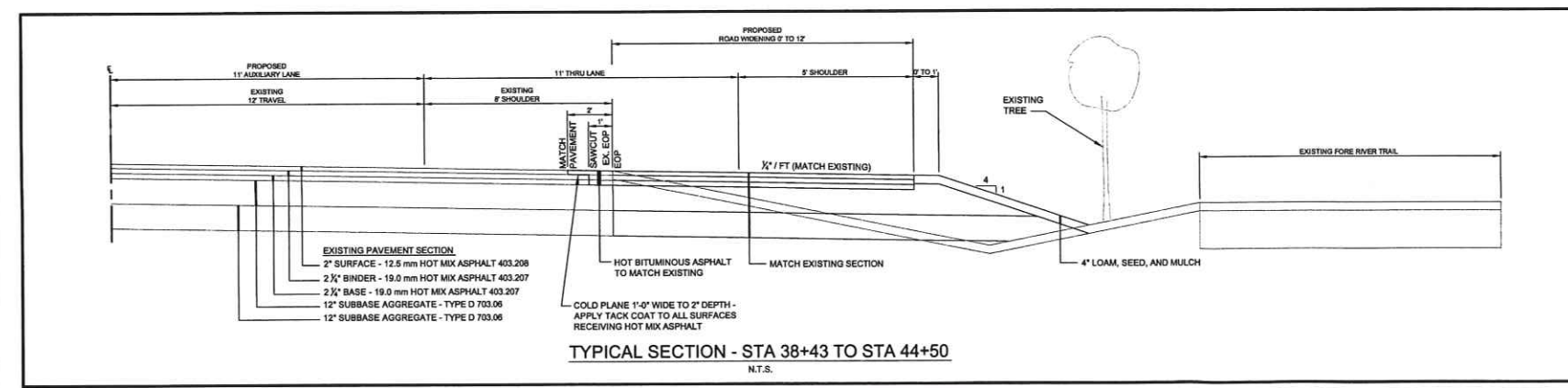
MERCY HOSPITAL DRIVEWAY TYPICAL SECTION
N.T.S.



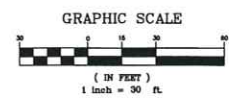
STA 35+95 TO STA 38+43
RAISED ISLAND
1" = 30'



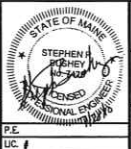
STA 35+95 TO STA 38+43
MODIFICATIONS TO RAISED ISLAND
N.T.S.



TYPICAL SECTION - STA 38+43 TO STA 44+50
N.T.S.



REV	DATE	DESCRIPTION
2	07.28.10	REVISED PER CITY / DOT COMMENTS AND RELEASED FOR REVIEW
1	07.14.10	RELEASED FOR REVIEW

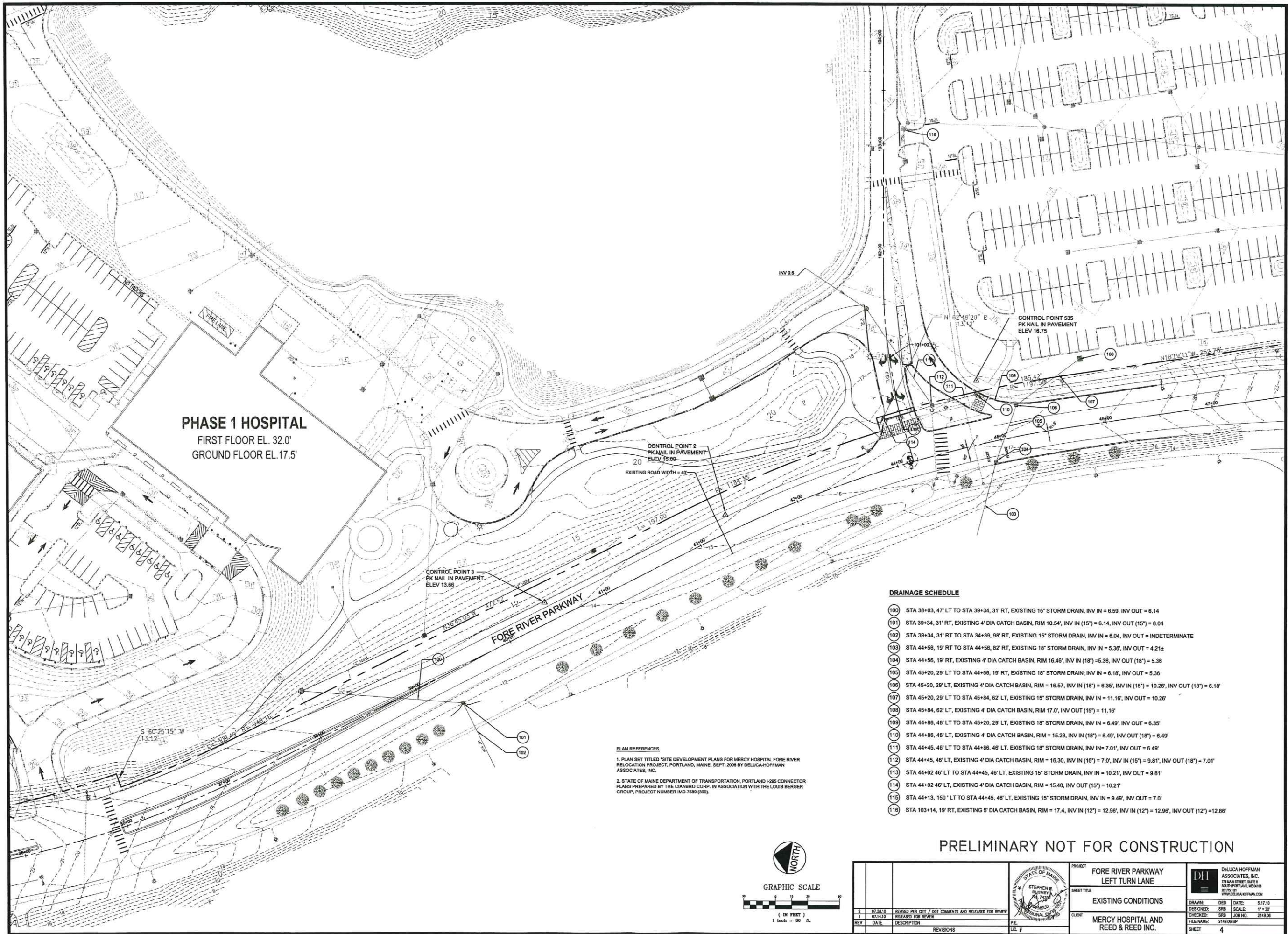


PROJECT: FORE RIVER PARKWAY LEFT TURN LANE
SHEET TITLE: TYPICAL SECTIONS AND DETAILS
CLIENT: MERCY HOSPITAL AND REED & REED INC.

DeLuca-Hoffman Associates, Inc.
77 MAIN STREET, SUITE 3 SOUTH PORTLAND, ME 04106
207.776.1100
WWW.DELUCAHOFFMAN.COM

DRAWN: OED DATE: 5.17.10
SCALE: 1" = 30'
CHECKED: SRB
JOB NO. 2149.06
FILE NAME: 2149.06-DETAILS2
SHEET 3

G:\2149.06\Concept\dwg\2149.06-DETAILS2.dwg, DETAILS, 7/26/2010 1:01:00 PM, chvntamr



PHASE 1 HOSPITAL
 FIRST FLOOR EL. 32.0'
 GROUND FLOOR EL. 17.5'

CONTROL POINT 3
 PK NAIL IN PAVEMENT
 ELEV 13.66

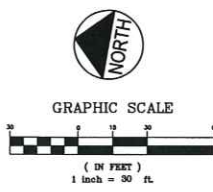
CONTROL POINT 2
 PK NAIL IN PAVEMENT
 ELEV 15.99

CONTROL POINT 535
 PK NAIL IN PAVEMENT
 ELEV 16.75

DRAINAGE SCHEDULE

100	STA 38+03, 47' LT TO STA 39+34, 31' RT, EXISTING 15" STORM DRAIN, INV IN = 6.59, INV OUT = 6.14
101	STA 39+34, 31' RT, EXISTING 4" DIA CATCH BASIN, RIM 10.54', INV IN (15') = 6.14, INV OUT (15') = 6.04
102	STA 39+34, 31' RT TO STA 34+39, 98' RT, EXISTING 15" STORM DRAIN, INV IN = 6.04, INV OUT = INDETERMINATE
103	STA 44+56, 19' RT TO STA 44+56, 82' RT, EXISTING 18" STORM DRAIN, INV IN = 5.36', INV OUT = 4.21±
104	STA 44+56, 19' RT, EXISTING 4" DIA CATCH BASIN, RIM 16.46', INV IN (18') = 5.36, INV OUT (18') = 5.36
105	STA 45+20, 29' LT TO STA 44+56, 19' RT, EXISTING 18" STORM DRAIN, INV IN = 6.18', INV OUT = 5.36
106	STA 45+20, 29' LT, EXISTING 4" DIA CATCH BASIN, RIM = 16.57, INV IN (18') = 6.35', INV IN (15') = 10.26', INV OUT (18') = 6.18'
107	STA 45+20, 29' LT TO STA 45+84, 62' LT, EXISTING 15" STORM DRAIN, INV IN = 11.16', INV OUT = 10.26'
108	STA 45+84, 62' LT, EXISTING 4" DIA CATCH BASIN, RIM 17.0', INV OUT (15') = 11.16'
109	STA 44+86, 46' LT TO STA 45+20, 29' LT, EXISTING 18" STORM DRAIN, INV IN = 6.49', INV OUT = 6.35'
110	STA 44+86, 46' LT, EXISTING 4" DIA CATCH BASIN, RIM = 15.23, INV IN (18') = 6.49', INV OUT (18') = 6.49'
111	STA 44+45, 46' LT TO STA 44+86, 46' LT, EXISTING 18" STORM DRAIN, INV IN = 7.01', INV OUT = 6.49'
112	STA 44+45, 46' LT, EXISTING 4" DIA CATCH BASIN, RIM = 16.30, INV IN (15') = 7.0', INV IN (15') = 9.81', INV OUT (18') = 7.01'
113	STA 44+02 46' LT TO STA 44+45, 46' LT, EXISTING 15" STORM DRAIN, INV IN = 10.21', INV OUT = 9.81'
114	STA 44+02 46' LT, EXISTING 4" DIA CATCH BASIN, RIM = 15.40, INV OUT (15') = 10.21'
115	STA 44+13, 150' LT TO STA 44+45, 46' LT, EXISTING 15" STORM DRAIN, INV IN = 9.49', INV OUT = 7.0'
116	STA 103+14, 19' RT, EXISTING 5" DIA CATCH BASIN, RIM = 17.4, INV IN (12') = 12.96', INV IN (12') = 12.96', INV OUT (12') = 12.86'

PLAN REFERENCES
 1. PLAN SET TITLED 'SITE DEVELOPMENT PLANS FOR MERCY HOSPITAL FORE RIVER RELOCATION PROJECT, PORTLAND, MAINE, SEPT. 2008 BY DELUCA-HOFFMAN ASSOCIATES, INC.
 2. STATE OF MAINE DEPARTMENT OF TRANSPORTATION, PORTLAND I-295 CONNECTOR PLANS PREPARED BY THE CHAMBERLAIN CORP. IN ASSOCIATION WITH THE LOUIS BERGER GROUP, PROJECT NUMBER MD-7989 (500).



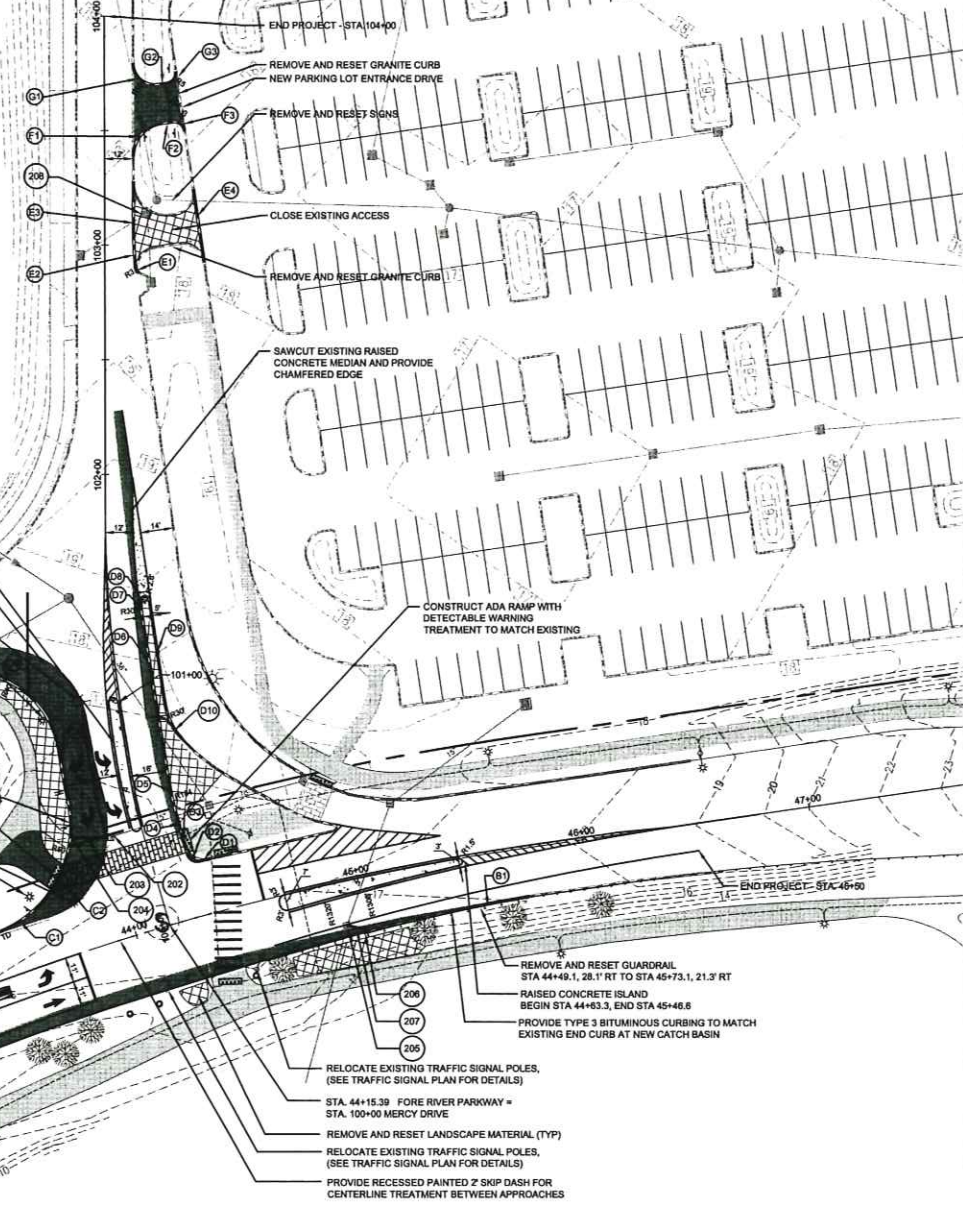
PRELIMINARY NOT FOR CONSTRUCTION

		PROJECT: FORE RIVER PARKWAY SHEET TITLE: EXISTING CONDITIONS CLIENT: MERCY HOSPITAL AND REED & REED INC.	DLUCA-HOFFMAN ASSOCIATES, INC. 307 MAIN STREET, SUITE # 207 PORTLAND, ME 04101 WWW.DLUCAHOFFMAN.COM
1. 07.28.10 REVISED PER CITY / DOT COMMENTS AND RELEASED FOR REVIEW 2. 07.14.10 RELEASED FOR REVIEW	REVISIONS REV. DATE DESCRIPTION	DATE: 5.17.10 SCALE: 1" = 30' JOB NO.: 2149.08 FILE NAME: 2149.08-SP SHEET: 4	

CURB LAYOUT TABLE						
CURB ID	STATION OFFSET FROM	STATION OFFSET TO	LENGTH (FT)	RADIUS (FT)	CURB TYPE	COMMENTS
A1	38+94.8, 0.2' RT	36+04.7, 10.4' RT	15.8	10	5	REMOVE AND RESET EXISTING
A2	36+04.7, 10.4' RT	37+44.8, 12' RT	141.4	1000	5	REMOVE AND RESET EXISTING
A3	37+44.8, 12' RT	38+42.4, 2.2' RT	58.3		5	REMOVE AND RESET EXISTING
A4	38+42.4, 2.2' RT	38+42.2, 2.2' LT	7.1	2.3	5	REMOVE AND RESET EXISTING
B1	44+97.8, 25.3' RT	45+97.7, 22.8' RT	100	1119.9	3M00Z	MATCH EXISTING CURB
C1	100+17.85, 73.3' LT	100+29.0, 45.4' LT	30.6	50	5	REMOVE AND RESET EXISTING
C2	100+29.0, 45.4' LT	100+37.8, 35.8' LT	12.1	50	1	REMOVE AND RESET EXISTING FLUSH GRANITE CURB AT RAMP
C3	100+37.8, 35.8' LT	100+61.3, 28.3' LT	28.4	50	5	REMOVE AND RESET EXISTING
C4	100+61.3, 28.3' LT	100+83.8, 28.7' LT	22.2	342.5	5	REMOVE AND RESET EXISTING
C5	100+83.8, 28.7' LT	101+18.5, 68.2' LT	78.7	40	1	REMOVE AND RESET EXISTING INCLUDING 18' TRANSITION PIECE
D1	100+22.4, 24.2' RT	100+22.8, 18.1' RT	4.8		5	REMOVE AND RESET EXISTING
D2	100+22.8, 18.1' RT	100+27.8, 15.8' RT	7.6	5	5	REMOVE AND RESET EXISTING
D3	100+27.8, 15.8' RT	100+38.4, 18.8' RT	19		5	REMOVE AND RESET EXISTING FLUSH GRANITE CURB AT RAMP
D4	100+38.4, 18.8' RT	100+44.8, 15.8' RT	6.6		5	REMOVE AND RESET EXISTING
D6	100+44.8, 15.8' RT	100+68.8, 17.1' RT	20.1	184	5	REMOVE AND RESET EXISTING
D6	100+68.8, 17.1' RT	101+38.1, 14.8' RT	67.9		5	NEW
D7	101+38.1, 14.8' RT	101+43.4, 13.8' RT	7.1	300	5	NEW
D8	101+43.4, 13.8' RT	101+43.8, 18.3' RT	8.2	2.8	5	NEW
D9	101+43.8, 18.3' RT	100+98.2, 20.8' RT	44.8		5	NEW
D10	100+98.2, 20.8' RT	100+80.2, 25.7' RT	17.8	30	5	REMOVE AND RESET EXISTING
E1	102+87.8, 14.8' RT	102+90.0, 12.8' RT	3.2		5	REMOVE AND RESET EXISTING
E2	102+90.0, 12.8' RT	103+08.2, 12.8' RT	18.3		5	REMOVE AND RESET EXISTING
E3	103+08.2, 12.8' RT	103+27.8, 12.8' RT	21.3		5	REMOVE AND RESET EXISTING
E4	103+27.8, 12.8' RT	103+24.3, 39.3' RT	32		5	REMOVE AND RESET EXISTING
F1	103+38.7, 12.8' RT	103+53.8, 24.8' RT	21.4	10	5	REMOVE AND RESET EXISTING
F2	103+53.8, 24.8' RT	103+54.3, 29.8' RT	5.1		5	REMOVE AND RESET EXISTING
F3	103+54.3, 29.8' RT	103+60.1, 38.8' RT	7.8	5	5	REMOVE AND RESET EXISTING
G1	103+78.4, 13.2' RT	103+89.8, 23.4' RT	17.2	10	5	REMOVE AND RESET EXISTING
G2	103+89.8, 23.4' RT	103+70.1, 27.8' RT	4.2		5	REMOVE AND RESET EXISTING
G3	103+70.1, 27.8' RT	103+76.8, 31.8' RT	7.8	5	5	REMOVE AND RESET EXISTING

NOTE: ADDITIONAL NEW TYPE 1 OR TYPE 5 CURB SHALL BE PROVIDED AS NECESSARY TO REPLACE DAMAGED CURB OR TO MEET THE PROPOSED LENGTH REQUIREMENTS.

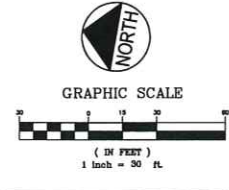
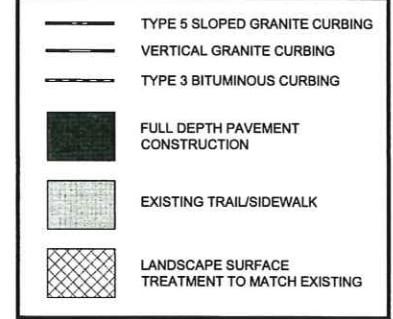
PHASE 1 HOSPITAL
FIRST FLOOR EL. 32.0'
GROUND FLOOR EL. 17.5'



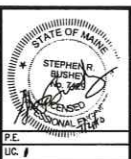
DRAINAGE SCHEDULE

- 200 STA 39+34.0, 40.3' RT
INSTALL NEW CATCH BASIN B2
RIM: MATCH EXISTING GRADE
INVERTS: FIELD VERIFY - CUT EXISTING PIPE TO ALLOW INSERTION OF NEW STRUCTURE
- 201 ALTER EXISTING CATCH BASIN TO MANHOLE
RIM: 10.52 ADJUST RIM TO NEW GRADE: 11.50
- 202 STA 100+44.2, 13.9' LT
ALTER EXISTING CATCH BASIN TO MANHOLE
RIM: 15.28
- 203 STA 100+44.2, 13.9' LT TO STA 100+48.5, 28.2' LT
INSTALL 12 LF OF 18" SD
INV. IN: 10.37, INV. OUT: 10.31
SLOPE: 0.006 R/R
- 204 STA 100+48.5, 28.2' LT
INSTALL NEW CATCH BASIN TYPE B2
RIM: 15.93
INVERT OUT: 10.37
- 205 STA 44+91.0, 19.4' RT
ALTER EXISTING CATCH BASIN TO MANHOLE
RIM: 18.48
CORE HOLE FOR NEW 18" SD AT INV. 11.50
- 206 STA 44+98.8, 22.5' RT
INSTALL NEW CATCH BASIN TYPE B2
RIM: 18.28
INVERT OUT: 12.0
- 207 STA 44+91.0, 19.4' RT TO 44+98.9, 23.1' RT
INSTALL 5 LF OF 18" SD
INV. IN: 12.0, INV. OUT: 11.60
SLOPE: 0.100 R/R
- 208 STA 103+14.0, 18.3' RT
ALTER EXISTING CATCH BASIN TO MANHOLE
RIM = 17.9

CURB LEGEND



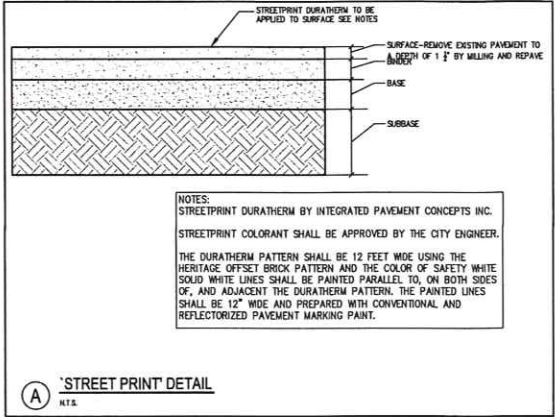
REV	DATE	DESCRIPTION	REVISIONS
4	07.28.10	REVIEW PER CITY / DOT COMMENTS AND RELEASED FOR REVIEW	
3	7.14.10	RELEASED FOR REVIEW	
2	7.08.10	SITE PLAN AMENDMENT APPLICATION TO CITY OF PORTLAND / PROCESS PERMITS RELAY TO MOOT	
1	5.12.10	RELEASED FOR REVIEW	



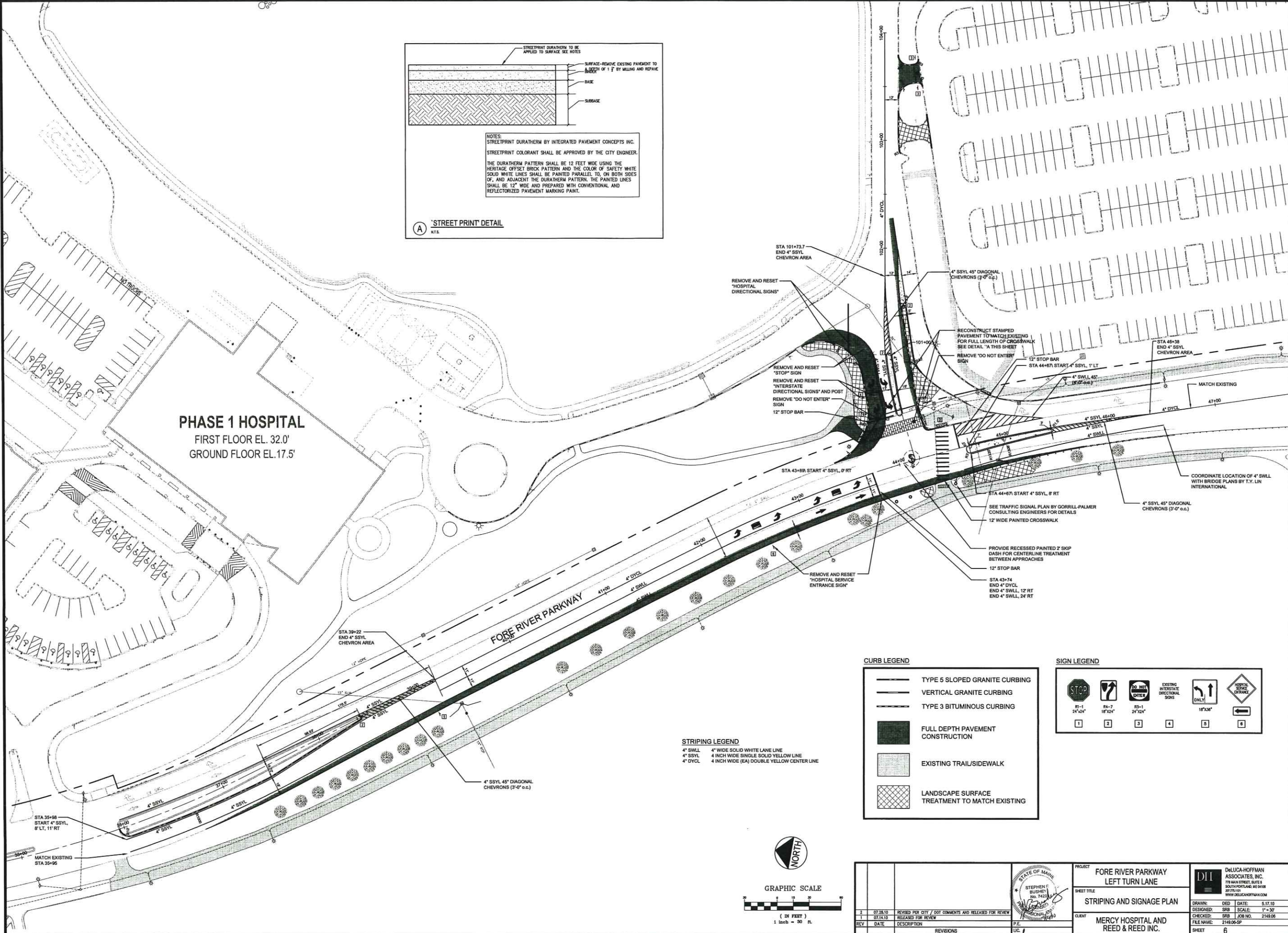
PROJECT: FORE RIVER PARKWAY LEFT TURN LANE
SHEET TITLE: ROADWAY IMPROVEMENT PLAN
CLIENT: MERCY HOSPITAL AND REED & REED INC.

DATE: 5.17.10
SCALE: 1" = 30'
JOB NO.: 2149-06
SHEET: 5

C:\149\06\Concept\dwg\2149_06_Sp_Amp_STE_7/28/2010 11:53:30 PM c:\winramide



PHASE 1 HOSPITAL
FIRST FLOOR EL. 32.0'
GROUND FLOOR EL. 17.5'



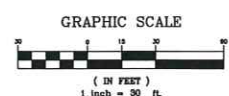
STRIPING LEGEND
4" SWLL 4" WIDE SOLID WHITE LANE LINE
4" SSYL 4 INCH WIDE SINGLE SOLID YELLOW LINE
4" DYCL 4 INCH WIDE (EA) DOUBLE YELLOW CENTER LINE

CURB LEGEND

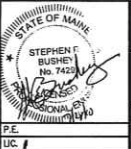
- TYPE 5 SLOPED GRANITE CURBING
- VERTICAL GRANITE CURBING
- TYPE 3 BITUMINOUS CURBING
- FULL DEPTH PAVEMENT CONSTRUCTION
- EXISTING TRAIL/SIDEWALK
- LANDSCAPE SURFACE TREATMENT TO MATCH EXISTING

SIGN LEGEND

RT-1 24"x24"
R4-7 18"x24"
R5-1 24"x24"
EXISTING INTERSTATE DIRECTIONAL SIGN
ONLY 18"x24"
TRUCK HEADWAY AHEAD 18"x24"



REV	DATE	DESCRIPTION	REVISIONS
2	07.28.10	REVISED PER CITY / DOT COMMENTS AND RELEASED FOR REVIEW	
1	07.14.10	RELEASED FOR REVIEW	

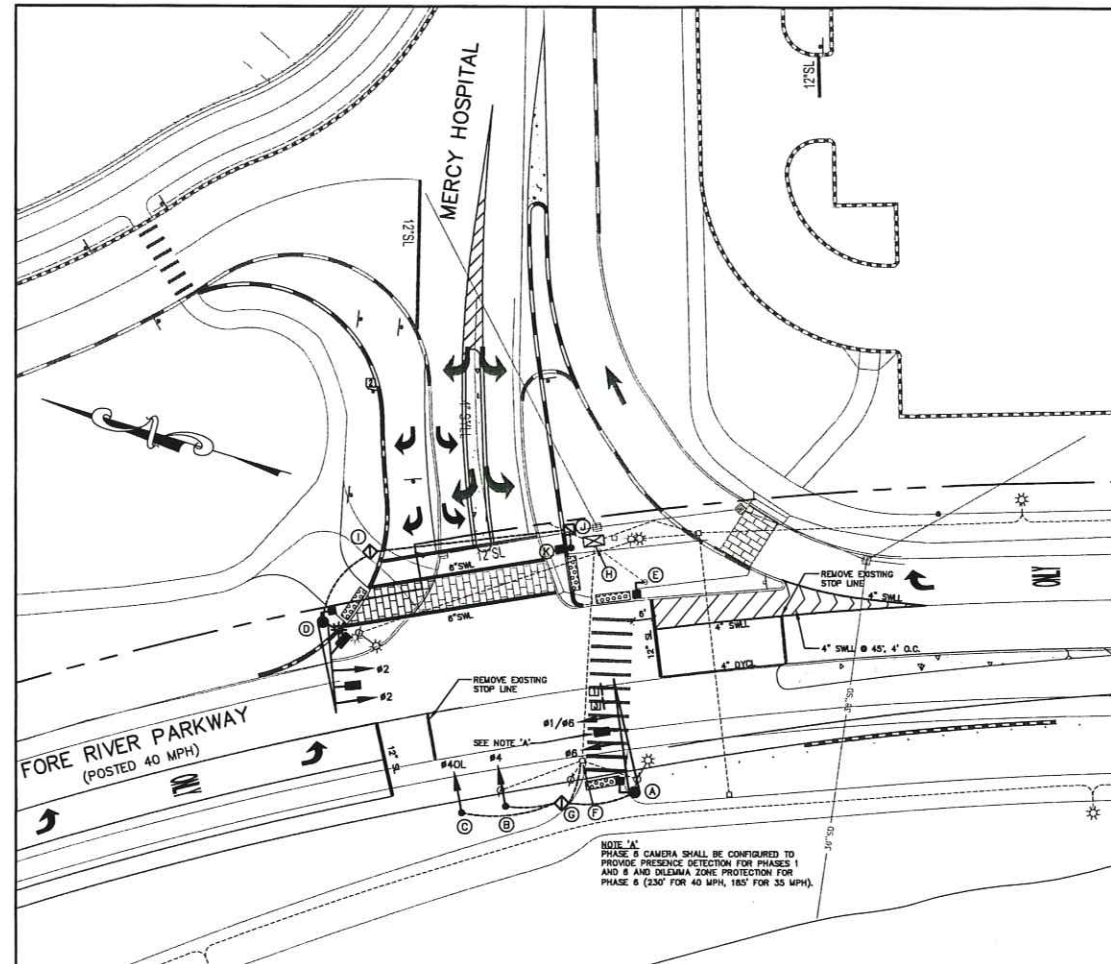


PROJECT: FORE RIVER PARKWAY LEFT TURN LANE
SHEET TITLE: STRIPING AND SIGNAGE PLAN
CLIENT: MERCY HOSPITAL AND REED & REED INC.

DeLUCA-HOFFMAN ASSOCIATES, INC.
77 MAIN STREET, SUITE 8 SOUTH PORTLAND, ME 04106
207.778.1010 WWW.DELUCAHOFFMAN.COM

DRAWN: DEB DATE: 5.17.10
DESIGNED: SRB SCALE: 1" = 30'
CHECKED: SRB JOB NO. 2149.06
FILE NAME: 2149.06-SF
SHEET 6

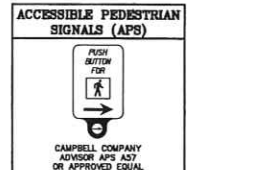
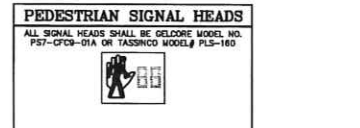
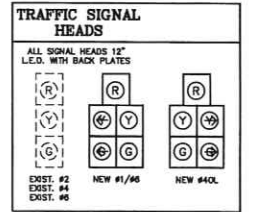
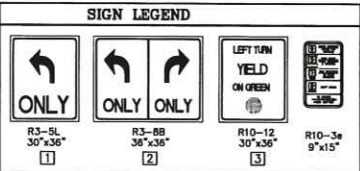
G:\3149\06\Concept\dwg\3149.06_Sf.dwg STRIPING AND SIGNAGE, 7/28/2010 11:59:35 AM, dwhiteman



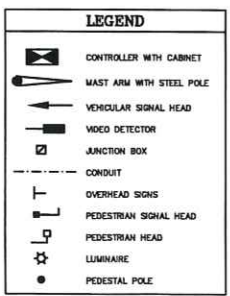
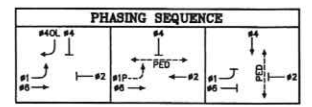
EQUIPMENT SCHEDULE			
DESIGNATION	ITEM	FATIGUE CATEGORY	DESCRIPTION
A	RELOCATED EXISTING MAST ARM POLE		EXISTING LUMINAIRE, SIGNAL HEADS, NEW COUNTDOWN PEDESTRIAN HEAD AND SIGN. INSTALL NEW APS BUTTON AND SIGN (R10-3a) AND VIDEO DETECTOR
B	RELOCATED PEDESTAL POLE		EXISTING SIGNAL HEAD
C	RELOCATED PEDESTAL POLE		EXISTING SIGNAL HEAD
D	NEW 30' MAST ARM POLE		25' ARM, RELOCATE STREET LIGHT TO THIS POLE, VEHICLE SIGNAL HEADS, COUNTDOWN PEDESTRIAN SIGNAL HEAD. INSTALL NEW APS BUTTON AND SIGN (R10-3a) AND VIDEO DETECTOR.
E	EXISTING PEDESTAL POLE		REMOVE EXISTING SIGNAL HEADS, CAP TOP OF POLE. INSTALL NEW COUNTDOWN PEDESTRIAN SIGNAL HEAD, APS BUTTON AND SIGN (R10-3a)
F	EXISTING JUNCTION BOX		REMOVE BOX AND EXTEND CONDUIT TO NEW JUNCTION BOX. REMOVE CONDUIT TO OTHER APPLIANCE THAT ARE BEING RELOCATED.
G	NEW CONCRETE JUNCTION BOX		INSTALL OUTSIDE PAVED LIMITS OF PATH
H	EXISTING CONTROLLER CABINET TO REMAIN		MODIFY CONTROLLER FOR NEW TIMING, PHASING AND VIDEO DETECTION
I	NEW CONCRETE JUNCTION BOX		
J	NEW CONCRETE JUNCTION BOX		
K	NEW PEDESTAL POLE		INSTALL NEW COUNTDOWN PEDESTRIAN SIGNAL HEAD, APS BUTTON AND SIGN (R10-3a)

CONDUIT SCHEDULE		
Run	Center to Center (Linear Feet)	Comments
A-G	12'	2 IN PVC
B-G	40'	2 IN PVC
C-G	20'	2 IN PVC
F-G	12'	3 IN PVC
D-I	22'	2 IN PVC
I-J	80'	3 IN PVC
J-K	6'	EXISTING
I-K	4'	2 IN PVC

ALL CONDUIT SHALL BE SCHEDULE 80 PVC



SIGNAL TIMING SCHEDULE - PLAN 1								
ITEM / PHASE	1	2	3	4	5	6	7	8
MINIMUM INTERVAL	4	4	-	4	-	4	-	-
PERMISSIVE TIME	3	3	-	3	-	3	-	-
MAXIMUM GREEN	8	30	-	32	-	31	-	-
MAXIMUM GREEN 2	-	-	-	-	-	-	-	-
YELLOW	3.8	3.8	-	3	-	3.8	-	-
ALL RED	1.7	1.7	-	2	-	1.7	-	-
WALK	-	7	-	7	-	-	-	-
PEDESTRIAN CLEARANCE	-	18	-	18	-	-	-	-
FLASH	DARK	Y	-	8	-	Y	-	-
PHASE CALL	NONE	ONMAX	-	NONE	-	ONMAX	-	-
MEMORY	NO LINK	-	-	-	-	-	-	-
DETECTOR WOOD	PRESENCE	PRESENCE	-	PRESENCE	-	PRESENCE	-	-
DETECTOR DELAY	-	-	-	25	-	-	-	-



**PRELIMINARY
NOT FOR CONSTRUCTION**

- NOTES:**
- MAST ARM AND POLE D SHALL BE HOT DIPPED AND GALVANIZED.
 - MAST ARM POLES A AND D SHALL BE ON 36 INCH DIAMETER FOUNDATION AND EXTEND 10 FEET BELOW EXISTING GRADE OR PROPOSED GRADE, WHICHEVER IS LOWER, AS A MINIMUM, IN ACCORDANCE WITH SECTION 628 OF MAINE DOT STANDARD DETAILS, DECEMBER 2002.
 - ALL NEW SIGNAL SECTIONS SHALL HAVE LED LENSES 12 INCHES IN DIAMETER WITH BACK PLATES.
 - ALL SIGNAL HEADS SHALL BE MOUNTED TO MAST ARMS WITH ASTROBRACKETS.
 - ALL SPLICES WILL BE MADE IN THE CABINET MEETING MANHOLE SPECIFICATIONS.
 - THE BOTTOM OF THE HOUSING OF THE SIGNAL FACE SHALL BE AT LEAST 18 FEET BUT NOT MORE THAN 19 FEET ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
 - ALL MATERIALS AND WORK COMPLETED UNDER THIS CONTRACT SHALL CONFORM TO THE MAINE DOT STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS AND STANDARD DETAILS AND WITH THE FEDERAL HIGHWAY ADMINISTRATION MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, 2003 EDITION.
 - TWO COPIES OF AS-BUILT PLANS SHALL BE LEFT IN THE CONTROLLER CABINET.
 - CONTRACTOR SHALL PROVIDE AND INSTALL ALL JUNCTION BOXES.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETING STRIPING AS INDICATED AND FOR REMOVING ANY STRIPING THAT CONFLICTS WITH THE PROPOSED STRIPING. MARKINGS SHALL BE GROUND OFF USING A METHOD APPROVED BY THE CITY.
 - UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY AND THE CONTRACTOR IS RESPONSIBLE FOR FINDING EXACT LOCATIONS OF UTILITIES PRIOR TO CONSTRUCTION. CONTRACTOR SHALL CONTACT DIG SAFE AT 1-800-DIG-SAFE.
 - CONTRACTOR SHALL CONTACT UTILITIES UPON AWARD OF THE CONTRACT. CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES SO THAT THE TRAFFIC SIGNAL INSTALLATION IS DONE AFTER THE UTILITIES HAVE BEEN RELOCATED BY THE RESPECTIVE UTILITY COMPANIES.
 - CONTRACTOR IS RESPONSIBLE FOR OBTAINING MAINE DOT AND CITY OPENING PERMIT, IF NECESSARY.
 - IT IS THE INTENT OF THIS WORK TO HAVE A COMPLETE OPERATIONAL, TESTED AND ACCEPTED TRAFFIC SIGNAL UPON COMPLETION OF THIS CONTRACT.
 - THE PROPOSED TRAFFIC SIGNAL TIMING AND PHASING PLAN SHALL BE INPUT BY THE CONTRACTOR UNDER OBSERVATION BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE 48 HOURS NOTICE TO THE ENGINEER.
 - ALL PEDESTRIAN HEADS SHALL BE L.E.D. AND INCLUDE THE COUNTDOWN FEATURE.
 - ALL SIGNAL HEADS AND LANE USE SIGNS SHALL BE CENTERED FOR THE APPROACHING LANES AS PRACTICABLE. PHASE 1 SIGNAL HEAD SHALL BE PLACED ON THE EXTENSION OF THE LANE LINE FOR THE LEFT/THRU LANES.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY TRAFFIC SIGNALS IF REQUIRED.
 - ALL TRAFFIC SIGNAL EQUIPMENT AND INSTALLATION METHODS SHALL BE IN ACCORDANCE WITH THE CITY OF PORTLAND REQUIREMENTS.
 - ANY EXISTING SIGNAL EQUIPMENT NOT REUSED SHALL BE DELIVERED TO THE CITY OF PORTLAND AS DIRECTED BY THE CITY.

u:\2407 - Mercy Hospital Fore River Entrance - CAD\Drawings\SIGNAL.dwg 7/27/2010 1:31 PM

Rev.	Date	Revision
1	7/27/10	REVISED PER MAINE DOT & CITY COMMENTS

Issued For	Date	By

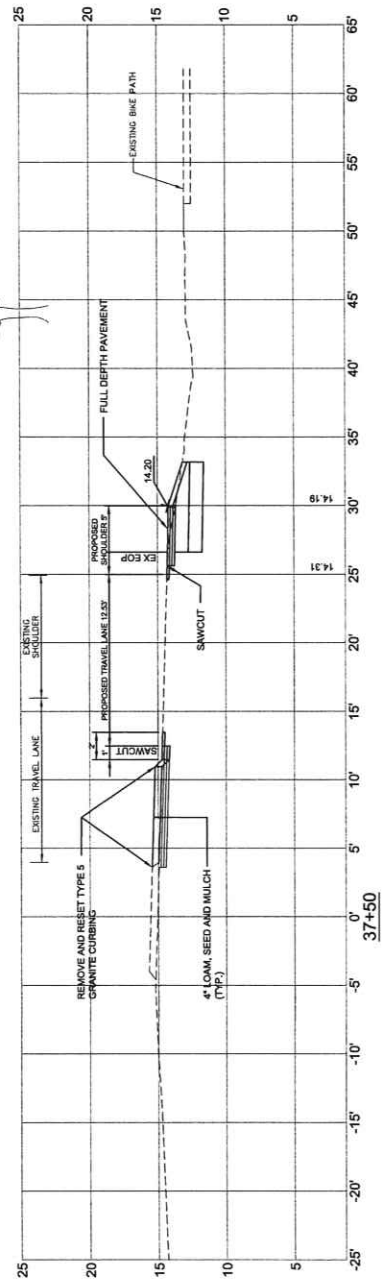
Design: PAM Draft: DB Date: JUL 2010
 Checked: RED Scale: 1"=20' Job No: 2407
 File Name: 2407_SIGNAL.dwg
 This plan shall not be modified without written permission from Gorrill-Palmer Consulting Engineers, Inc.(GPCE). Any alterations, authorized or otherwise, shall be at the user's sole risk and without liability to GPCE.

GP Gorrill-Palmer Consulting Engineers, Inc.
 Engineering Excellence Since 1998
 PO Box 1237 207-657-6910
 15 Shaker Road FAX: 207-657-6912
 Gray, ME 04039 E-Mail: mail@gorrillpalmer.com

Drawing Name: **Traffic Signal Plan**
 Project: **Mercy Hospital Fore River Entrance**
 Portland, Maine
 Client: **DeLuca-Hoffman Associates, Inc.**
 778 Main Street, Suite B, South Portland, ME 04106

Drawing No. **7**

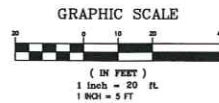
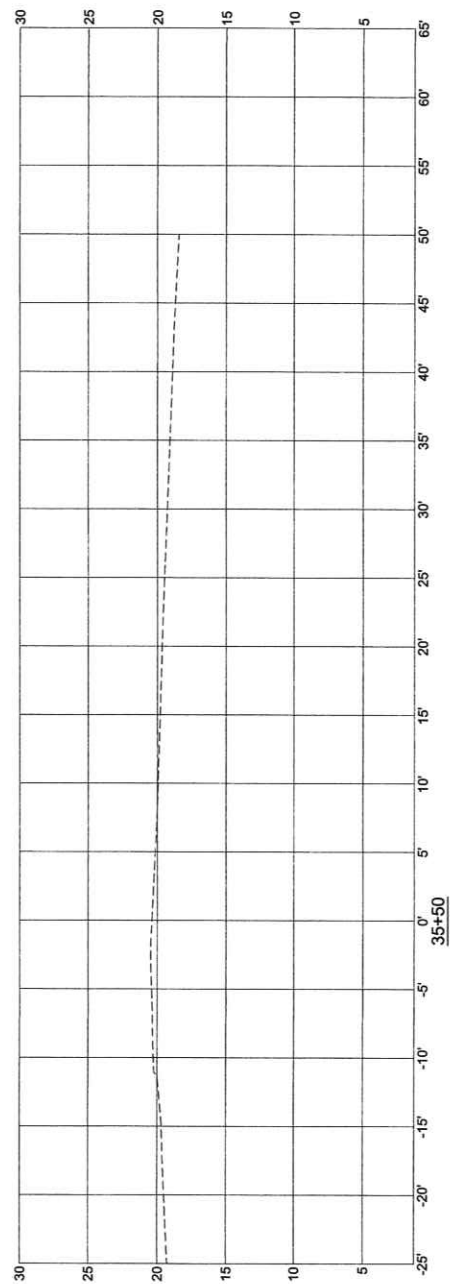
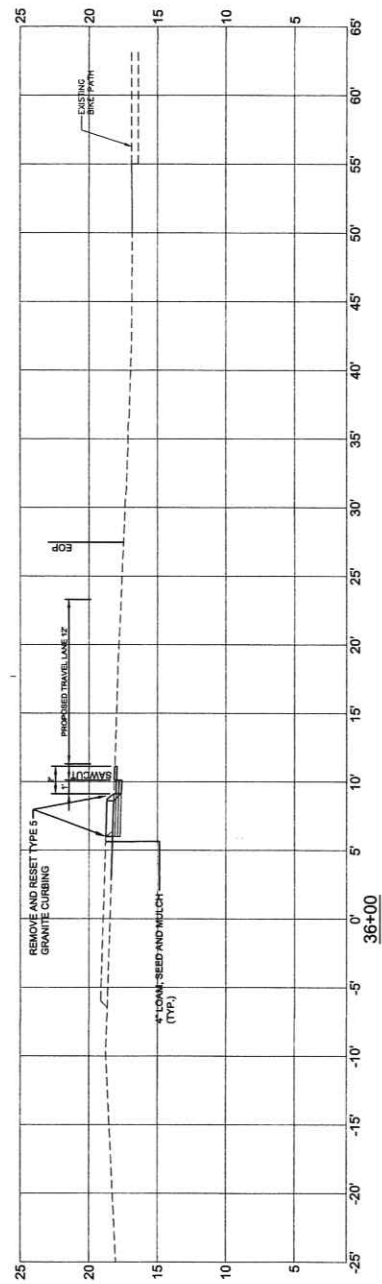
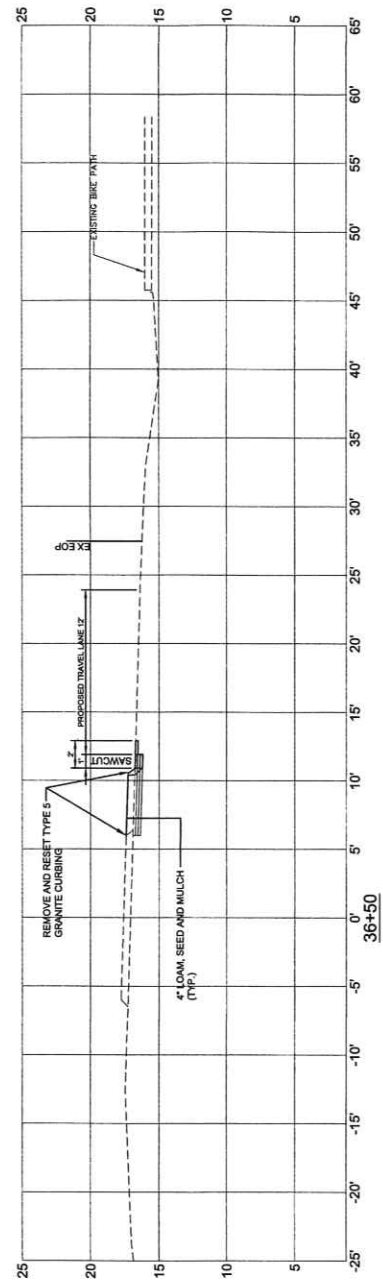
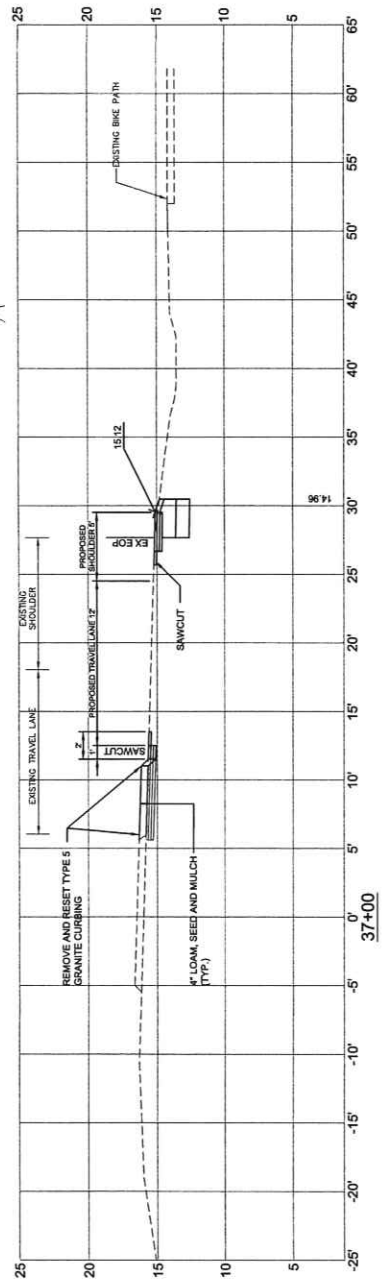
FORE RIVER PARKING
LEFT TURN AUXILIARY LANE
PORTLAND, ME



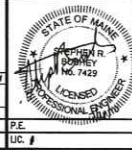
STA. 37+94
44' R. DECID.
TREE TO REMAIN

STA. 37+75
44' R.
TREE TO REMAIN

STA. 37+58
44' R.
TREE TO REMAIN



REV	DATE	DESCRIPTION
3	07.28.10	REVISED FOR CITY / DOT COMMENTS AND RELEASED FOR REVIEW
2	7.09.10	SITE PLAN AMENDMENT APPLICATION TO CITY OF PORTLAND / PROCESS PRINT RELEASED TO MOVT
1	5.17.10	RELEASED FOR REVIEW



PROJECT: FORE RIVER PARKWAY
LEFT TURN LANE

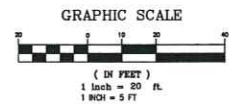
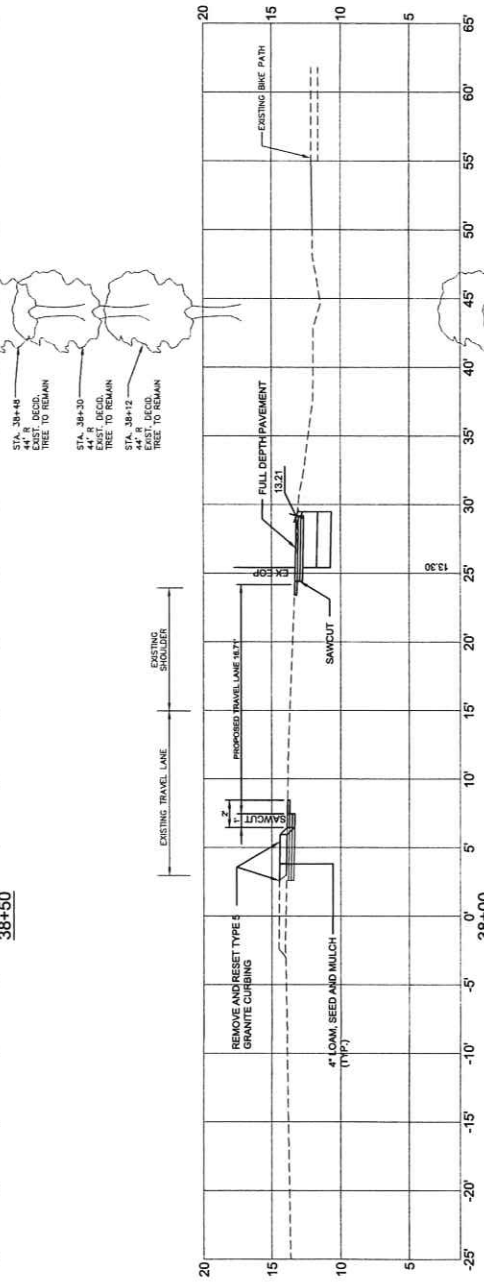
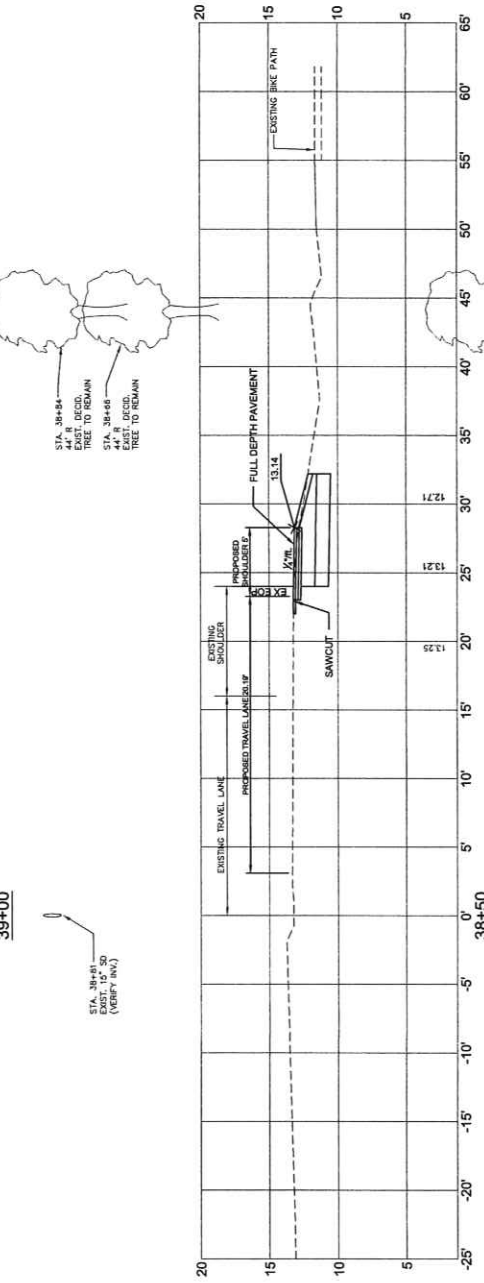
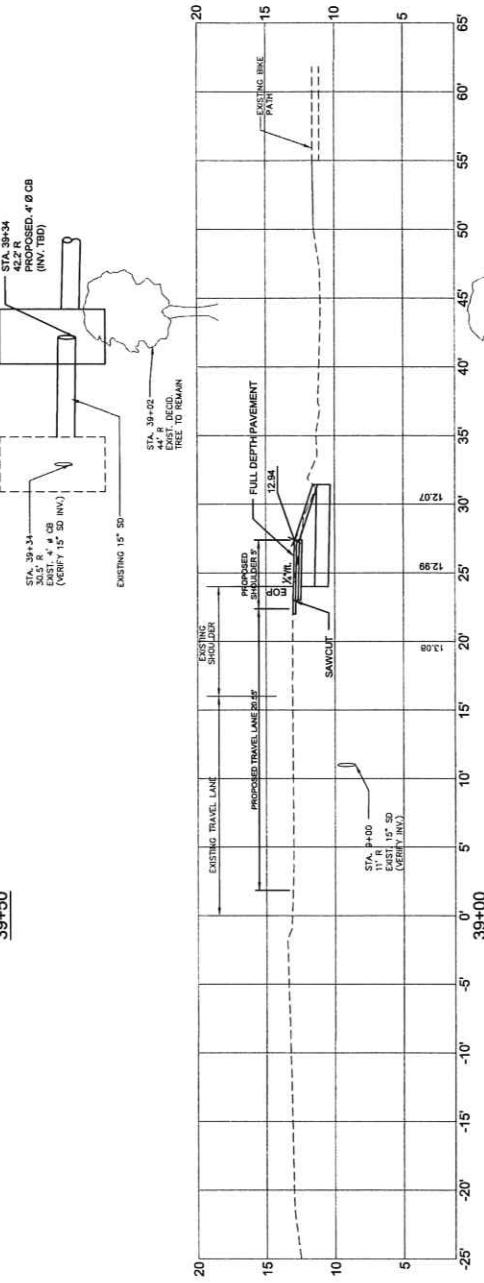
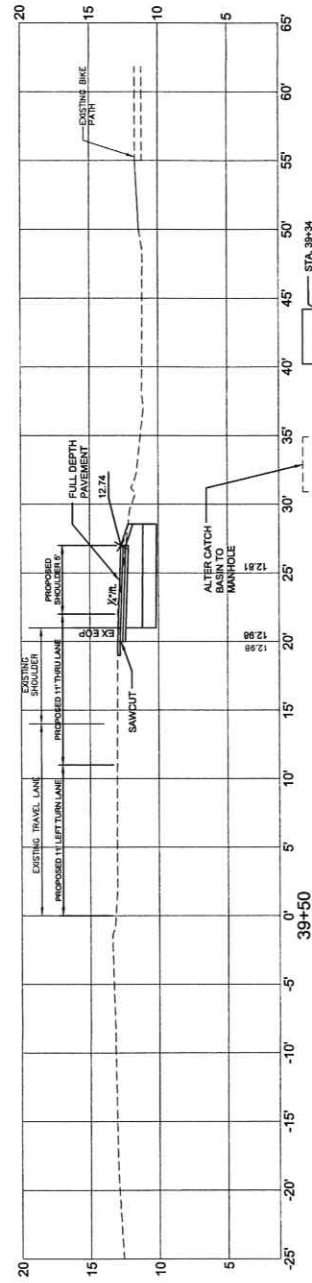
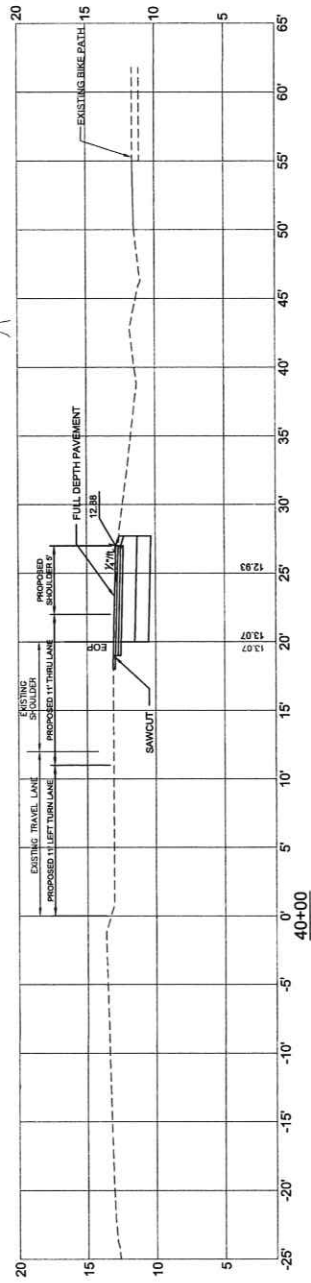
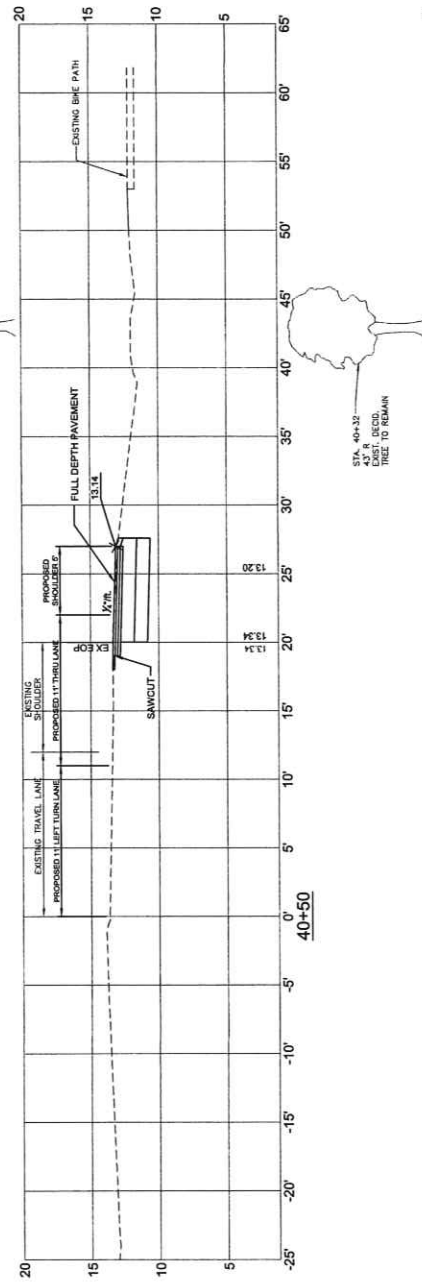
SHEET TITLE: SECTIONS
STA. 35+50 TO 37+50

CLIENT: MERCY HOSPITAL AND
REED & REED INC.

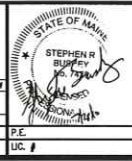
DELUCA-HOFFMAN
ASSOCIATES, INC.
778 HAN STREET, SUITE 8
SOUTH PORTLAND, ME 04106
207.761.1110
WWW.DELUCAHOFFMAN.COM

DRAWN: DEB DATE: 5.17.10
DESIGNED: SRB SCALE: 1" = 30'
CHECKED: SRB JOB NO. 2149.06
FILE NAME: 2149.06-SP
SHEET: S1

FORE RIVER PARKING
LEFT TURN AUXILIARY LANE
PORTLAND, ME



REV	DATE	DESCRIPTION
3	07.28.10	REVISED PER CITY / DOT COMMENTS AND RELEASED FOR REVIEW
2	7.08.10	SITE PLAN AMENDMENT APPLICATION TO CITY OF PORTLAND / PROCESS PRINT RELEASED TO MOOT
1	5.17.10	RELEASED FOR REVIEW

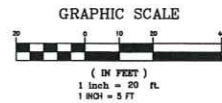
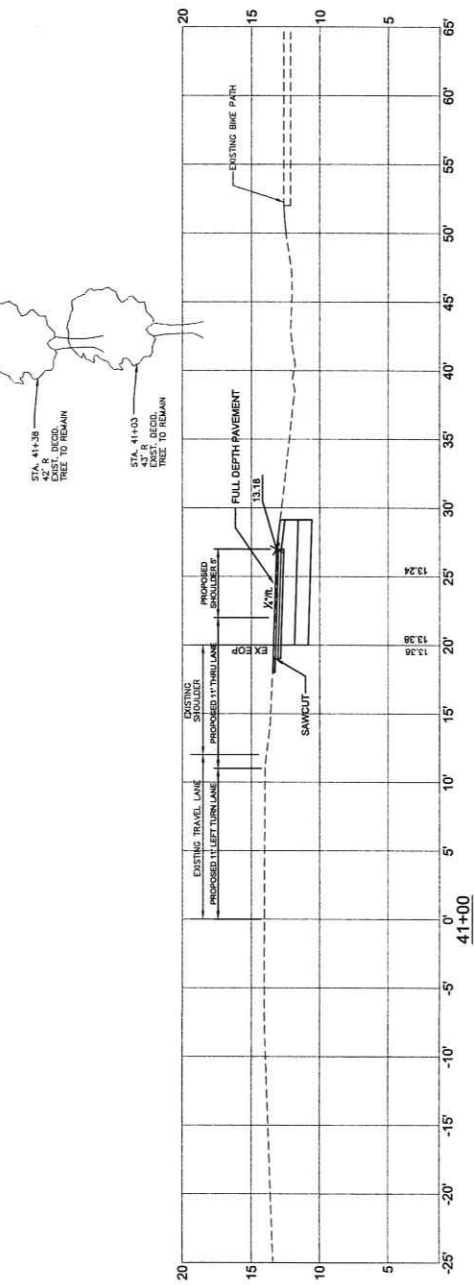
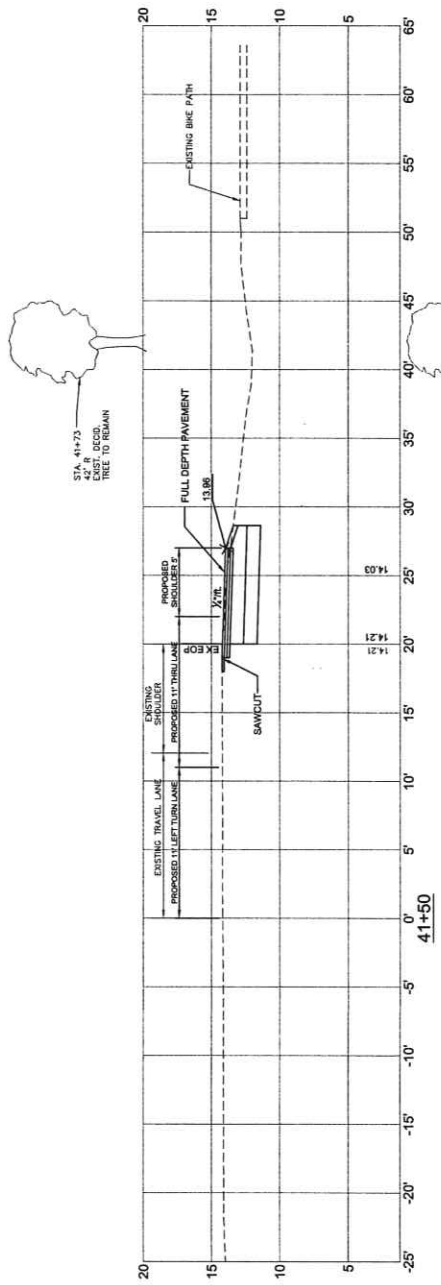
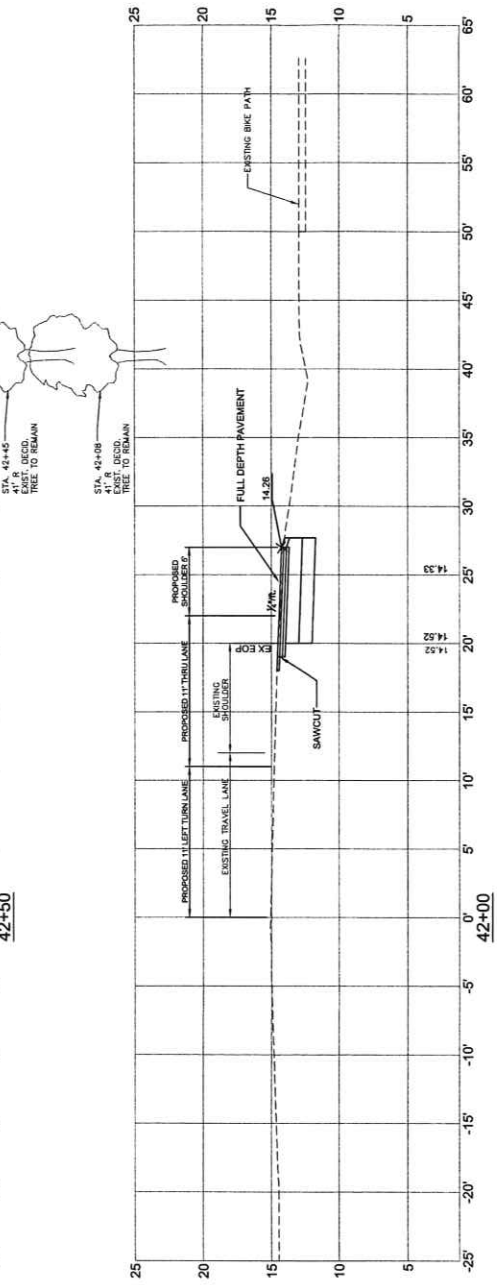
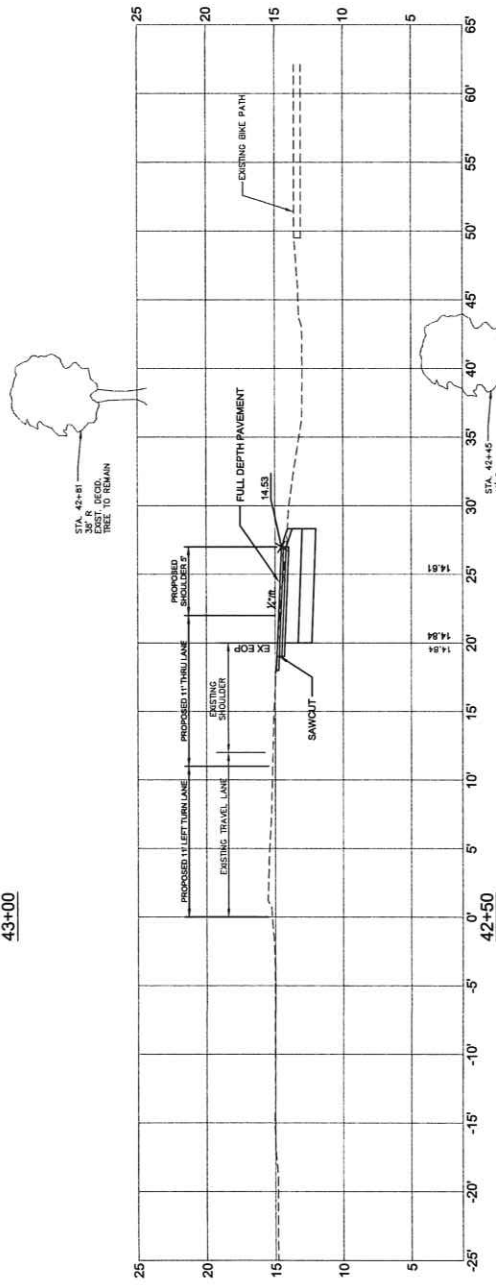
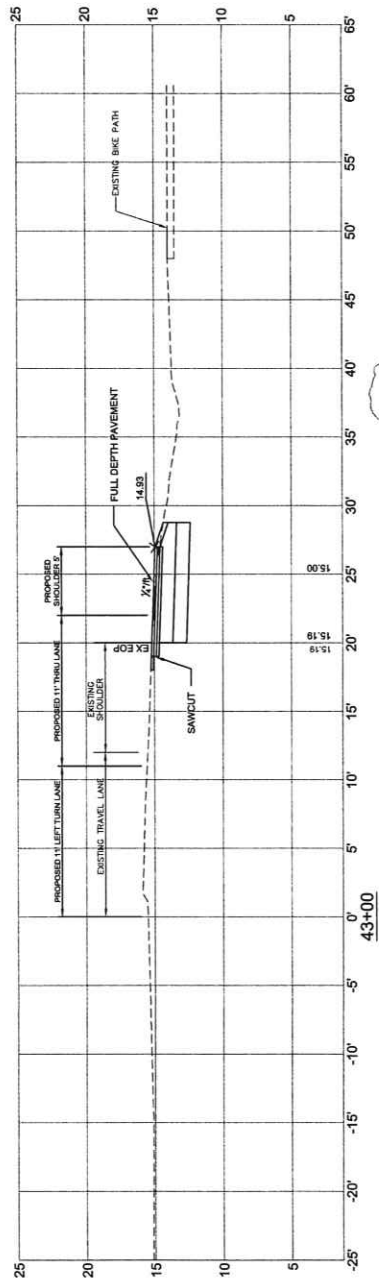


PROJECT	FORE RIVER PARKWAY LEFT TURN LANE
SHEET TITLE	SECTIONS STA. 38+00 TO 40+50
CLIENT	MERCY HOSPITAL AND REED & REED INC.

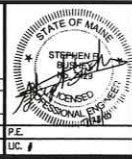
DRAWN:	DED	DATE:	5.17.10
CHECKED:	SPB	SCALE:	1" = 30'
FILE NAME:	2148.06-SP	JOB NO.:	2148.06
SHEET	S2		

D&L DALLUCA-HOFFMAN ASSOCIATES, INC.
78 WASHINGTON ST. SUITE 4
SOUTH PORTLAND, ME 04106
WWW.DALLUCA-HOFFMAN.COM

FORE RIVER PARKING
LEFT TURN AUXILIARY LANE
PORTLAND, ME



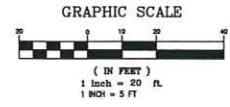
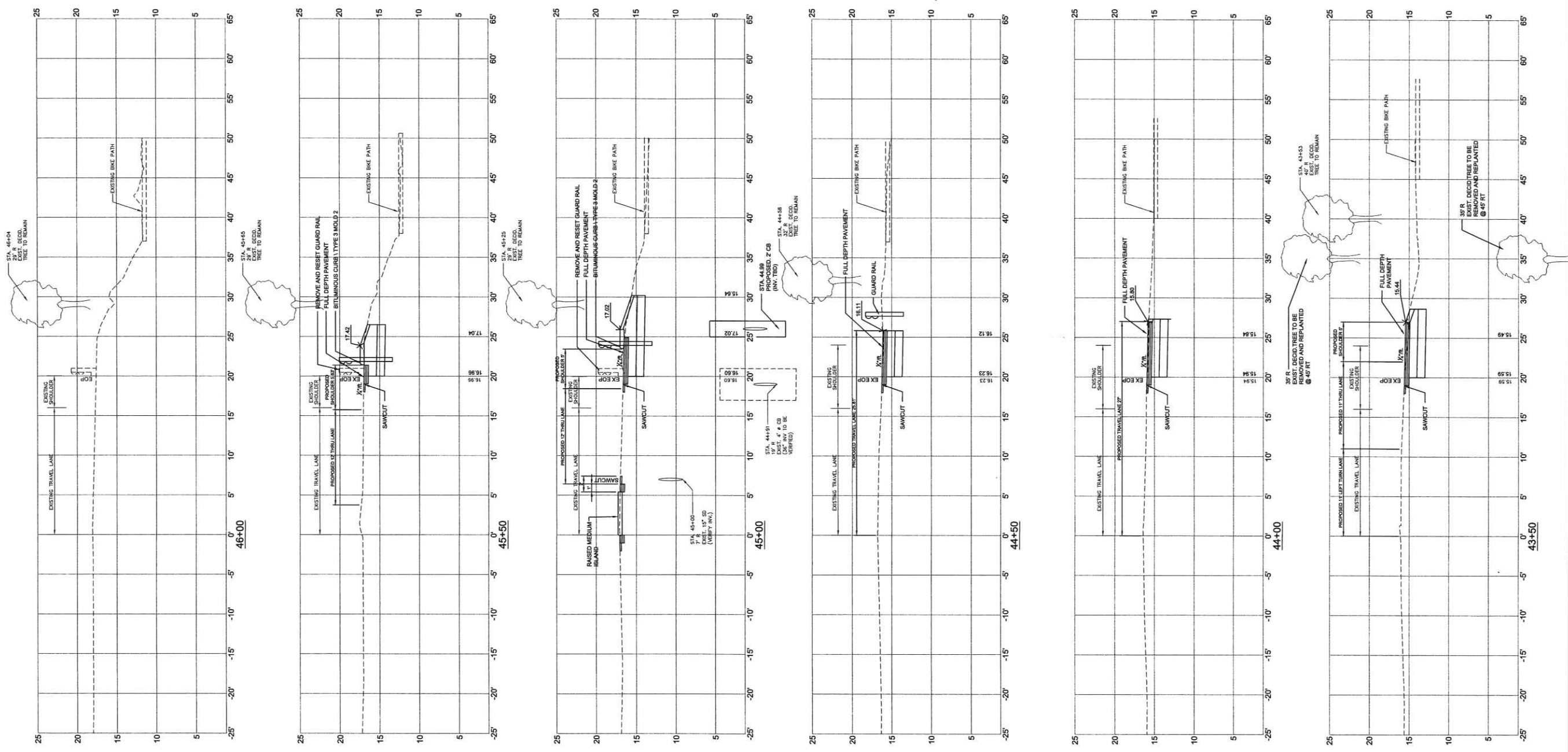
REV	DATE	DESCRIPTION
3	07.28.10	REVISED PER CITY / DOT COMMENTS AND RELEASED FOR REVIEW
2	7.06.10	SITE PLAN AMENDMENT APPLICATION TO CITY OF PORTLAND / PROCESS POINT RELEASED TO MOU
1	5.17.10	RELEASED FOR REVIEW



PROJECT: FORE RIVER PARKWAY
LEFT TURN LANE
SHEET TITLE: SECTIONS
STA. 41+00 TO 43+00
CLIENT: MERCY HOSPITAL AND REED & REED INC.

DESIGNED: SRB	CHECKED: SRB	DATE: 5.17.10	SCALE: 1" = 30'
FILE NAME: 2149.06-SP	SHEET: S3	DELUCA-HOFFMAN ASSOCIATES, INC. 78 WASHINGTON STREET, SUITE 8 SOUTH PORTLAND, ME 04108 WWW.DELUCA-HOFFMAN.COM	

FORE RIVER PARKING
LEFT TURN AUXILIARY LANE
PORTLAND, ME



REV	DATE	DESCRIPTION
3	07.28.10	REVISED PER CITY / DOT COMMENTS AND RELEASED FOR REVIEW
2	7.08.10	SITE PLAN AMENDMENT APPLICATION TO CITY OF PORTLAND / PROCESS PRINT RELEASED TO HOVY
1	5.17.10	RELEASED FOR REVIEW

PROJECT: FORE RIVER PARKWAY
LEFT TURN LANE
SECTIONS
STA. 43+50 TO 46+00

CLIENT: MERCY HOSPITAL AND REED & REED INC.

DESIGNED: DED DATE: 5.17.10
CHECKED: SRB SCALE: 1" = 32'
FILE NAME: 2149.06-SP
SHEET: S4



DeLUCA-HOFFMAN ASSOCIATES, INC.
CONSULTING ENGINEERS

778 MAIN STREET
SUITE 8
SOUTH PORTLAND, MAINE 04106
TEL. 207 775 1121
FAX 207 879 0896

- SITE PLANNING AND DESIGN
- ROADWAY DESIGN
- ENVIRONMENTAL ENGINEERING
- PERMITTING
- AIRPORT ENGINEERING
- CONSTRUCTION ADMINISTRATION
- LANDSCAPE ARCHITECTURE

August 4, 2010

Ms. Jean Fraser
City of Portland Planning Authority
4th Floor, City Hall
389 Congress Street
Portland, ME 04101

**Subject: Mercy Hospital – Fore River Project
 Southerly Driveway Modifications
 Response to Site Plan Approval Conditions
 HTE-10-99900004**

Dear Jean:

DeLuca-Hoffman Associates, Inc. has received and reviewed the Amendment to the Site Plan approval letter issued by the City on August 3, 2010 and we offer the following additional supporting information and modified plans to address the Conditions of Approval. Each of the Conditions of Approval are stated below with our response and/or supporting evidence for satisfying each condition.

Condition 1:

The applicant shall be responsible for incorporating signal equipment modifications, as necessary, such that it will allow for the subject traffic signal to be coordinated with traffic signals associated with the Veterans Memorial Bridge project.

Response:

The applicant, Reed and Reed Inc. and A.D. Electric will coordinate all signal equipment modifications to assure that the proposed Mercy Driveway signal systems are coordinated with the new systems to be installed at the new Veteran's Bridge/Fore River Parkway intersection and other intersections in the project vicinity.

Condition 2:

The alignment of the sidewalk ramp and detectable warning device on the northeast corner of the subject driveway shall be modified to better meet ADA standards such that the crossing is constructed to be as close to a perpendicular alignment as reasonably practicable. The change shall be reviewed and approved by the City.

Response:

The project Site Improvement Drawing has been modified to make the alignment of the ADA detectable surface as near to perpendicular as possible to the path of travel. A copy of the revised drawing accompanies this letter.

Condition 3 & 4:

That the applicant shall revise the proposals for the modification of the curbing at the entrance to the internal drop-off area to address traffic control, pavement markings, and pedestrian movements as the curb changes have created a very wide open intersection.

Response:

The signage and striping plan has been modified to include information pertaining to the existing signage and pavement markings in the area of the proposed onsite intersection. We have also added a yellow chevron striping along the modified curb radius approach so that the intersection appearance will be a bit more confined without impacting maneuvering area for exiting vehicles. The proposed drive aisle improvements are not expected to impact the existing cross walks and pedestrian access patterns in the vicinity.

Condition 5:

That the proposed sign type and associated installation at the new parking lot entry driveway onsite shall be reviewed and approved by the Planning Authority prior to implementation;

Response:

These signs are "Do Not Enter" signs that have been noted on the Signage and Striping plan. The proposed driveway is intended to allow only entering movements from the main drive into the parking lot. Vehicles exiting the lot must exit from the drive located at the southeast corner of the lot.

Condition 6:

That the applicant shall add a note to the plan that states that all existing signs for the hospital that are invalid shall be removed by the applicant with approval from the Planning Authority.

Response:

A note has been added to the Signage and Striping plan that requires the removal of any signs that are no longer valid based on the reconfiguration of the intersection and elimination of the slip lane entrance. A copy of the revised plan accompanies this letter.

Condition 7:

That a detail for the recessed reflectors shall be submitted for review and approval by the Planning Authority prior to construction.

Response:

Basically, a detail has been added to the signage and striping plan that shows a 4-inch wide pavement grind to a depth of approximately 2x the paint thickness. This detail has been prepared as advised by DOT representatives.

Ms. Jean Fraser
August 4, 2010
Page 3

Condition 8:

That the applicant shall relocate and augment any disturbed planting so that there is no net loss of planting in the vicinity of the improvements and complete all associated landscaping and planting within six months of the re-opening of the southernmost driveway to traffic access.

Response:

The applicant will coordinate with their contractor, R.J Grondin and Sons, and their landscaping provider, Seabreeze Property Management, to insure that the landscaping is adequately handled. We note that the landscaping beds on the Mercy site will likely not be fully restored until next spring's planting period due to the lateness in the season, and timing for the completed improvements this fall.

We trust that the accompanying revised plans and supporting information contained in this letter adequately address the Conditions of Approval. Please be advised that Reed and Reed, Inc. and R.J. Grondin will be providing the performance guarantee materials to you in the next few days and they will be seeking to conduct a pre-construction meeting next week. At the pre-construction meeting they will provide a detailed construction schedule and traffic control plans related to the upcoming work along the Fore River Parkway.

Sincerely,

DeLUCA-HOFFMAN ASSOCIATES, INC.



Stephen R. Bushey, P.E., C.P.E.S.C.
Senior Engineer

SRB/jpf/JN2149.07/Fraser 8-4-10

C: William Connolly, Mercy Hospital
Art Cavanagh, Reed and Reed, Inc.
Jeff Folsom, Maine DOT

CURB LAYOUT TABLE						
CURB ID	STATION OFFSET	STATION OFFSET	LENGTH (FT)	RADIUS (FT)	CURB TYPE	COMMENTS
	FROM	TO				
A1	35+94.8, 0.2' RT	36+04.7, 10.4' RT	10.0	10	5	REMOVE AND RESET EXISTING
A2	36+04.7, 10.4' RT	37+44.6, 12' RT	14.0	1000	5	REMOVE AND RESET EXISTING
A3	37+44.6, 12' RT	38+24.2, 12.2' RT	8.0		5	REMOVE AND RESET EXISTING
A4	38+24.2, 12.2' RT	38+42.2, 2.2' LT	7.0	2.5	5	REMOVE AND RESET EXISTING
B1	44+97.8, 25.2' RT	45+97.7, 22.2' RT	100	1119.9	3M000	MATCH EXISTING CURB
C1	100+17.66, 73.3' LT	100+29.0, 45.4' LT	30.8	50	5	REMOVE AND RESET EXISTING
C2	100+29.0, 45.4' LT	100+37.8, 36.8' LT	12.1	50	1	REMOVE AND RESET EXISTING
C3	100+37.8, 36.8' LT	100+61.3, 28.7' LT	23.4	50	5	FLUSH GRANITE CURB AT RAMP
C4	100+61.3, 28.7' LT	100+83.6, 28.7' LT	22.3	342.6	5	REMOVE AND RESET EXISTING
C5	100+83.6, 28.7' LT	101+18.5, 68.2' LT	78.7	40	1	REMOVE AND RESET EXISTING
D1	100+22.4, 24.2' RT	100+22.4, 18.1' RT	4.8		5	INCLUDING EX TRANSITION PIECE
D2	100+22.4, 18.1' RT	100+27.8, 15.8' RT	7.6	5	5	REMOVE AND RESET EXISTING
D3	100+27.8, 15.8' RT	100+38.4, 16.0' RT	10		5	REMOVE AND RESET EXISTING
D4	100+38.4, 16.0' RT	100+44.9, 16.0' RT	6.5		5	FLUSH GRANITE CURB AT RAMP
D5	100+44.9, 16.0' RT	100+68.9, 17.1' RT	24.1	184	5	REMOVE AND RESET EXISTING
D6	100+68.9, 17.1' RT	101+36.1, 14.8' RT	67.2		5	REMOVE AND RESET EXISTING
D7	101+36.1, 14.8' RT	101+43.4, 13.8' RT	7.3	300	5	NEW
D8	101+43.4, 13.8' RT	101+43.8, 19.2' RT	5.4	2.8	5	NEW
D9	101+43.8, 19.2' RT	100+98.2, 20.8' RT	44.8		5	NEW
D10	100+98.2, 20.8' RT	100+80.2, 28.8' RT	17.8	30	5	REMOVE AND RESET EXISTING
E1	102+87.6, 14.8' RT	102+90.0, 12.8' RT	3.2		5	REMOVE AND RESET EXISTING
E2	102+90.0, 12.8' RT	103+06.2, 12.8' RT	16.3		5	REMOVE AND RESET EXISTING
E3	103+06.2, 12.8' RT	103+27.8, 12.8' RT	21.3		5	REMOVE AND RESET EXISTING
E4	103+27.8, 12.8' RT	103+24.3, 28.3' RT	3.2		5	REMOVE AND RESET EXISTING
F1	103+28.7, 12.8' RT	103+53.8, 24.8' RT	25.1	10	5	REMOVE AND RESET EXISTING
F2	103+53.8, 24.8' RT	103+54.3, 29.8' RT	5.1		5	REMOVE AND RESET EXISTING
F3	103+54.3, 29.8' RT	103+55.1, 29.8' RT	7.8	5	5	REMOVE AND RESET EXISTING
G1	103+78.4, 12.8' RT	103+89.5, 23.4' RT	17.2	10	5	REMOVE AND RESET EXISTING
G2	103+89.5, 23.4' RT	103+70.1, 27.8' RT	4.2		5	REMOVE AND RESET EXISTING
G3	103+70.1, 27.8' RT	103+78.8, 21.8' RT	7.8	5	5	REMOVE AND RESET EXISTING

NOTE: ADDITIONAL NEW TYPE 1 OR TYPE 5 CURB SHALL BE PROVIDED AS NECESSARY TO REPLACE DAMAGED CURB OR TO MEET THE PROPOSED LENGTH REQUIREMENTS.

PHASE 1 HOSPITAL
FIRST FLOOR EL. 32.0'
GROUND FLOOR EL. 17.5'

FORE RIVER PARKWAY
EXISTING ROAD WIDTH = 40'

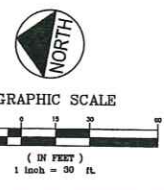
DRAINAGE SCHEDULE

- 200 STA 39+34.0, 40.3' RT
INSTALL NEW CATCH BASIN B2
RIM: MATCH EXISTING GRADE
INVERTS: FIELD VERIFY - CUT EXISTING PIPE TO ALLOW INSERTION OF NEW STRUCTURE
- 201 ALTER EXISTING CATCH BASIN TO MANHOLE
RIM: 10.62 ADJUST RIM TO NEW GRADE: 11.50
- 202 STA 100+44.2, 13.8' LT
ALTER EXISTING CATCH BASIN TO MANHOLE
RIM: 15.28
- 203 STA 100+44.2, 13.8' LT TO STA 100+48.5, 28.2' LT
INSTALL 12 LF OF 15" SD
INV. IN: 10.37, INV. OUT: 10.31
SLOPE: 0.005 R/L
- 204 STA 100+48.5, 28.2' LT
INSTALL NEW CATCH BASIN TYPE B2
RIM: 15.93
INVERT OUT: 10.37
- 205 STA 44+91.0, 19.4' RT
ALTER EXISTING CATCH BASIN TO MANHOLE
RIM: 18.48
CORE HOLE FOR NEW 15" SD AT INV. 11.50
- 206 STA 44+88.8, 22.8' RT
INSTALL NEW CATCH BASIN TYPE B2
RIM: 16.28
INVERT OUT: 12.0
- 207 STA 44+91.0, 19.4' RT TO 44+98.9, 23.1' RT
INSTALL 8 LF OF 15" SD
INV. IN: 12.0, INV. OUT: 11.50
SLOPE: 0.100 R/L
- 208 STA 103+14.0, 18.3' RT
ALTER EXISTING CATCH BASIN TO MANHOLE
RIM = 17.9

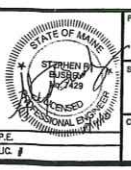
CURB LEGEND

- TYPE 5 SLOPED GRANITE CURBING
- VERTICAL GRANITE CURBING
- TYPE 3 BITUMINOUS CURBING
- FULL DEPTH PAVEMENT CONSTRUCTION
- EXISTING TRAIL/SIDEWALK
- LANDSCAPE SURFACE TREATMENT TO MATCH EXISTING

- PLANNING BOARD CONDITIONS OF APPROVAL**
1. THE APPLICANT SHALL RELOCATE AND AUGMENT ANY DISTURBED PLANTING SO THAT THERE IS NO NET LOSS OF PLANTING IN THE VICINITY OF THE IMPROVEMENTS AND COMPLETE ALL ASSOCIATED LANDSCAPING AND PLANTING WITHIN SIX MONTHS OF THE RE-OPENING OF THE SOUTHERNMOST DRIVEWAY TO TRAFFIC ACCESS.
 2. ALL ACTIVITIES WITHIN THE FORE RIVER PARKWAY SHALL BE COMPLETED IN ACCORDANCE WITH THE CITY OF PORTLAND RULES AND REGULATIONS FOR EXCAVATION ACTIVITY WITHIN THE CITY RIGHT-OF-WAY.



REV	DATE	DESCRIPTION	REVISIONS
5	06.04.18	REVISED FOR CONDITIONS OF APPROVAL AND RESUBMITTED TO CITY	
4	07.28.18	REVISED PER CITY DOT COMMENTS AND RELEASED FOR REVIEW	
3	7.14.18	RELEASED FOR REVIEW	
2	7.08.18	SITE PLAN AMENDMENT APPLICATION TO CITY OF PORTLAND/ PROGRESS PRINT RELEASED TO MOOT	
1	5.17.18	RELEASED FOR REVIEW	



PROJECT: FORE RIVER PARKWAY LEFT TURN LANE
SHEET TITLE: ROADWAY IMPROVEMENT PLAN
CLIENT: MERCY HOSPITAL AND REED & REED INC.

DR: DELUCA-HOFFMAN ASSOCIATES, INC.
700 W. PINE STREET
SOUTH PORTLAND, ME 04106
207.875.1111
WWW.DELUCAHOFFMAN.COM

DRAWN: DED DATE: 5.17.18
DESIGNED: SRB SCALE: 1" = 30'
CHECKED: SRB JOB NO.: 2148.05
FILE NAME: 2148.05-SP
SHEET: 5

C:\2148\05\Comp\p1\18.05.18\SP.dwg, SITE, 04/20/18 4:57:27 PM, c:\deluca

