

DISPLAY THIS CARD ON PRINCIPAL FRONTAGE OF WORK

CITY OF PORTLAND

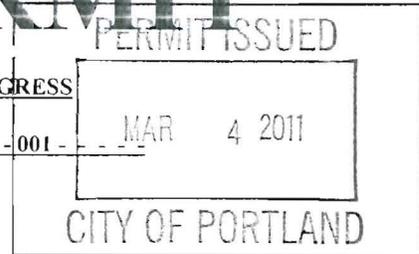
BUILDING PERMIT



This is to certify that MGO PROPERTIES LLC/M&M INDUST Located At 696 CONGRESS

Job ID: 2010-12-176-OPB

CBL: 055 - - E - 032 - 001 -



has permission to Repair to Int & Ext from Vehicle Collison, add egress to 1st floor front unit for compliance provided that the person or persons, firm or corporation accepting this permit shall comply with all of the provisions of the Statues of Maine and of the Ordinances of the City of Portland regulating the construction, maintenance and use of the buildings and structures, and of the application on file in the department.

Notification of inspection and written permission procured before this building or part thereof is lathed or otherwise closed-in. 48 HOUR NOTICE IS REQUIRED.

A final inspection must be completed by owner before this building or part thereof is occupied. If a certificate of occupancy is required, it must be

Sub pe. KG-

Fire Prevention Officer

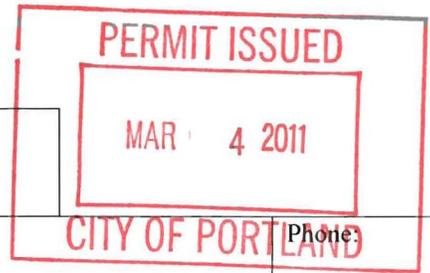
Cam [Signature] 3/3/11

Code Enforcement Officer / Plan/Reviewer

THIS CARD MUST BE POSTED ON THE STREET SIDE OF THE PROPERTY.
PENALTY FOR REMOVING THIS CAR

City of Portland, Maine - Building or Use Permit Application

389 Congress Street, 04101 Tel: (207) 874-8703, FAX: (207) 8716



Job No: 2010-12-176-OPB	Date Applied: 12/29/2010	CBL: 055 - - E - 032 - 001 - - - - -	
Location of Construction: 696 CONGRESS	Owner Name: PROPERTIES LLC MGO	Owner Address: PMB 299 50 MARKET ST SOUTH PORTLAND, ME - MAINE 04106	Phone:
Business Name:	Contractor Name: M & M Industries, Mark	Contractor Address: 14 Kirkland Ave SOUTH PORTLANDMAINE04106	Phone: 4875
Lessee/Buyer's Name:	Phone:	Permit Type: BLDG - Building	Zone: B-2b
Past Use: Four Family	Proposed Use: Four Family – repairs to interior/exterior due to vehicle collision – egress changes per PFD	Cost of Work: 20000.00	CEO District:
		Fire Dept: <input checked="" type="checkbox"/> Approved <i>w/conditions</i> <input type="checkbox"/> Denied <input type="checkbox"/> N/A	Inspection: Use Group: R-2 Type: 3B <i>DOB-2009</i> Signature: <i>[Signature]</i>
Proposed Project Description: Mixed Use 696 Congress St		Pedestrian Activities District (P.A.D.)	

Permit Taken By:	Zoning Approval		
<p>1. This permit application does not preclude the Applicant(s) from meeting applicable State and Federal Rules.</p> <p>2. Building Permits do not include plumbing, septic or electrical work.</p> <p>3. Building permits are void if work is not started within six (6) months of the date of issuance. False informatin may invalidate a building permit and stop all work.</p>	Special Zone or Reviews <input type="checkbox"/> Shoreland <input type="checkbox"/> Wetlands <input type="checkbox"/> Flood Zone <input type="checkbox"/> Subdivision <input type="checkbox"/> Site Plan <input type="checkbox"/> Maj ___ Min ___ MM Date: <i>12/29/10</i>	Zoning Appeal <input type="checkbox"/> Variance <input type="checkbox"/> Miscellaneous <input type="checkbox"/> Conditional Use <input type="checkbox"/> Interpretation <input type="checkbox"/> Approved <input type="checkbox"/> Denied Date:	Historic Preservation <input type="checkbox"/> Not in Dist or Landmark <input type="checkbox"/> Does not Require Review <input type="checkbox"/> Requires Review <input type="checkbox"/> Approved <input checked="" type="checkbox"/> Approved w/Conditions <input type="checkbox"/> Denied Date: <i>2/15/11</i> <i>[Signature]</i>
	CERTIFICATION		

I hereby certify that I am the owner of record of the named property, or that the proposed work is authorized by the owner of record and that I have been authorized by the owner to make this application as his authorized agent and I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in the application is issued, I certify that the code official's authorized representative shall have the authority to enter all areas covered by such permit at any reasonable hour to enforce the provision of the code(s) applicable to such permit.

SIGNATURE OF APPLICANT ADDRESS DATE PHONE

RESPONSIBLE PERSON IN CHARGE OF WORK, TITLE DATE PHON



General Building Permit Application

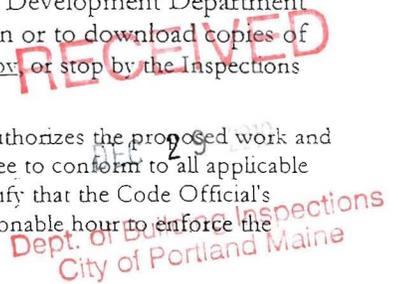
If you or the property owner owes real estate or personal property taxes or user charges on any property within the City, payment arrangements must be made before permits of any kind are accepted.

Location/Address of Construction: <u>696 CONGRESS STREET</u>		
Total Square Footage of Proposed Structure/Area <u>1,814 sq ft</u>	Square Footage of Lot <u>0.78 ACRES</u>	Number of Stories <u>3</u>
Tax Assessor's Chart, Block & Lot Chart# <u>55</u> Block# <u>E</u> Lot# <u>32</u>	Applicant * <u>must be owner, Lessee or Buyer</u> * Name <u>MGO PROPERTIES LLC</u> Address <u>PMB 299 SO MARKET ST</u> City, State & Zip <u>S. PORTLAND 04106</u>	Telephone: <u>207-799-7502</u>
Lessee/DBA (If Applicable)	Owner (if different from Applicant) Name Address City, State & Zip	Cost Of Work: \$ <u>20,000⁰⁰</u> C of O Fee: \$ _____ Total Fee: \$ <u>220⁰⁰</u>
Current legal use (i.e. single family) <u>MIXED / OFFICE</u> Number of Residential Units <u>FOUR</u>		
If vacant, what was the previous use? _____		
Proposed Specific use: <u>SAME</u>		
Is property part of a subdivision? _____ If yes, please name _____		
Project description: <u>REPAIRS TO INTERIOR / EXTERIOR DUE TO VEHICLE COLLISION. EGRESS CHANGES PER PFD.</u>		
Contractor's name: <u>M & M INDUSTRIES</u>		
Address: <u>14 KIRKLAND AVE</u>		
City, State & Zip <u>SOUTH PORTLAND, ME 04106</u>		Telephone: <u>207-329-4875</u>
Who should we contact when the permit is ready: <u>MARU</u>		Telephone: <u>207-329-4875</u>
Mailing address: <u>SAME</u>		

Please submit all of the information outlined on the applicable Checklist. Failure to do so will result in the automatic denial of your permit.

In order to be sure the City fully understands the full scope of the project, the Planning and Development Department may request additional information prior to the issuance of a permit. For further information or to download copies of this form and other applications visit the Inspections Division on-line at www.portlandmaine.gov or stop by the Inspections Division office, room 315 City Hall or call 874-8703.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Code Official's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.



Signature: [Signature] Date: 12/21/10

This is not a permit; you may not commence ANY work until the permit is issued



PORTLAND MAINE

Strengthening a Remarkable City, Building a Community for Life • www.portlandmaine.gov

Director of Planning and Urban Development
Penny St. Louis

Job ID: 2010-12-176-OPB

Located At: 696 CONGRESS

CBL: 055 - - E - 032 - 001 - - - -

Conditions of Approval:

Zoning

1. This permit is being approved on the basis of plans submitted. Any deviations shall require a separate approval before starting that work.
2. ANY exterior work requires a separate review and approval thru Historic Preservation. This property is located within an Historic District.
3. This property shall remain a four family dwelling. Any change of use shall require a separate permit application for review and approval.
4. This is NOT an approval for an additional dwelling unit. You SHALL NOT add any additional kitchen equipment including, but not limited to items such as stoves, microwaves, refrigerators, or kitchen sinks, etc. Without special approvals.

Historic

1. Contractor to prepare concrete samples for review and approval by Historic Preservation staff prior to pouring concrete steps.
2. Concrete to match as closely as possible the color of existing curb in front of planting bed.
3. Obsolete sign above entry to be removed.

Fire

1. Fire Conditions
2. All smoke detectors and smoke alarms shall be photoelectric.
3. Hardwired Carbon Monoxide alarms with battery back up are required in the dwelling units.
4. This permit is being approved on the basis of the plans submitted. Any deviation from the plans would require amendments and approval.
5. All construction shall comply with City Code Chapter 10.
6. The entire structure shall comply with NFPA 101 "Existing Apartments" Compliance shall be insured prior to the issuance of a Certificate of Occupancy.
7. All means of egress to remain accessible at all times.

Building

1. Permit approved based on the plans submitted and reviewed w/owner/contractor, with additional information as agreed on and as noted on plans.
2. Application approval based upon information provided by applicant. Any deviation from approved plans requires separate review and approval prior to work.
3. Separate permits are required for any electrical, plumbing, sprinkler, fire alarm, HVAC systems, heating appliances, including pellet/wood stoves, commercial hood exhaust

systems and fuel tanks. Separate plans may need to be submitted for approval as a part of this process.

4. This permit is approved under IBC Sec. 3404 Alterations, which allow the alteration or replacement of existing stairs to not meet new construction dimensions when the space is restricted. Guardrails and handrails, stair rise/run profile and continuity shall comply with new codes to the fullest extent possible. IBC Sec. 3409, Historic Buildings exempts meeting the full extent of the code in the case of the pipe rail at the side steps instead of a full guardrail.

BUILDING PERMIT INSPECTION PROCEDURES

Please call 874-8703 or 874-8693 (ONLY)

or email: buildinginspections@portlandmaine.gov

With the issuance of this permit, the owner, builder or their designee is required to provide adequate notice to the city of Portland Inspections Services for the following inspections. Appointments must be requested 48 to 72 hours in advance of the required inspection. The inspection date will need to be confirmed by this office.

- **Please read the conditions of approval that is attached to this permit!! Contact this office if you have any questions.**
- **Permits expire in 6 months. If the project is not started or ceases for 6 months.**
- **If the inspection requirements are not followed as stated below additional fees may be incurred due to the issuance of a "Stop Work Order" and subsequent release to continue.**

1. Foundation/Rebar
2. Final at completion of work

The project cannot move to the next phase prior to the required inspection and approval to continue, REGARDLESS OF THE NOTICE OF CIRCUMSTANCES.

IF THE PERMIT REQUIRES A CERTIFICATE OF OCCUPANCY, IT MUST BE PAID FOR AND ISSUED TO THE OWNER OR DESIGNEE BEFORE THE SPACE MAY BE OCCUPIED.

Mark is requesting a meeting this week for this property, guard/handrail detail and the side egress for the 1st floor that currently missing, but there is a door.

He is available after 12pm on Tues and Wed and after 2:30 on Thurs.
Are you available for any of these?
Thanks

>>> "Mark Mawhinney" <mamjam@maine.rr.com> 9/28/2010 6:57 AM >>>
We were able to reset the stairs and install temporary guard rails with a handrail.

Thank you,

Mark Mawhinney
M & M industries
207-329-4875

*might make from back
only need 30w so.
1/2 build hand steps
concrete - make
- sign*







19

350

3497

1200 37



691
Cana

Surveyed by

~~W. J. ...~~

MAY - 3 1924

(Remarks on other Side)

05-E-32



709

35 LN

2-22
1-21

047 C029
707

047 C030
705

047 C031
701

699
047 C032

047 C033
691

2-20
1-19

708 - 712
709 - 711

696 - 706
697 - 707

CONGRESS ST

684 - 694
685 - 695

680 - 682
689 - 089

714

10189

055 D016

055 D014

9

6300

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13

3103

055 D012

15

3104

2-16
1-15

HORTON PL

1975
055 E022
10
48
055 E021
1480
12
1480
055 E020
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055 E040
768
36 85

055 E026

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684

055 X003

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055 E018

055 E019

19

39.86
73.86

51.4
39.8
39.75
48
32
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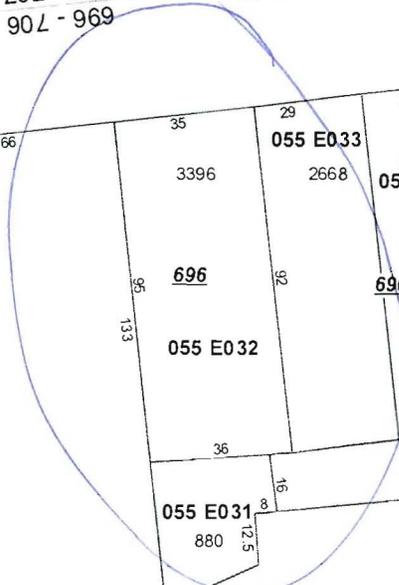
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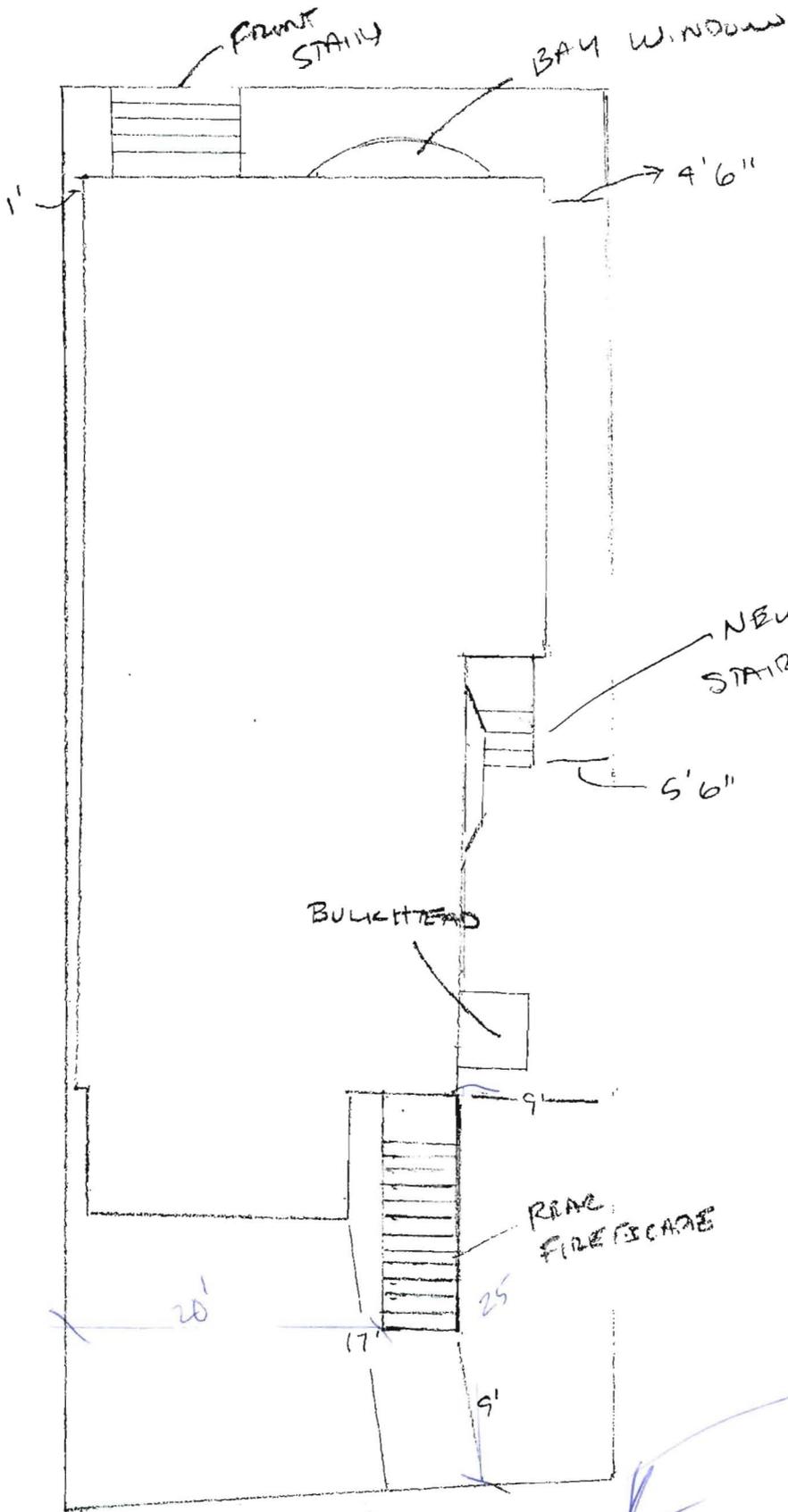
35
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16
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690

29
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35.67

51
3
45
40
4050





B2b egress doesn't need to be enclosed

NEW PLATFORM PER HISTORIC & PFD

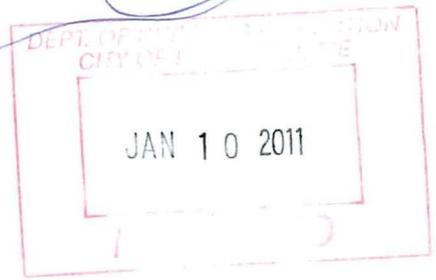
5' side yd Reg ok
5'6" shown

696 CONGRESS ST.
PLOT PLAN

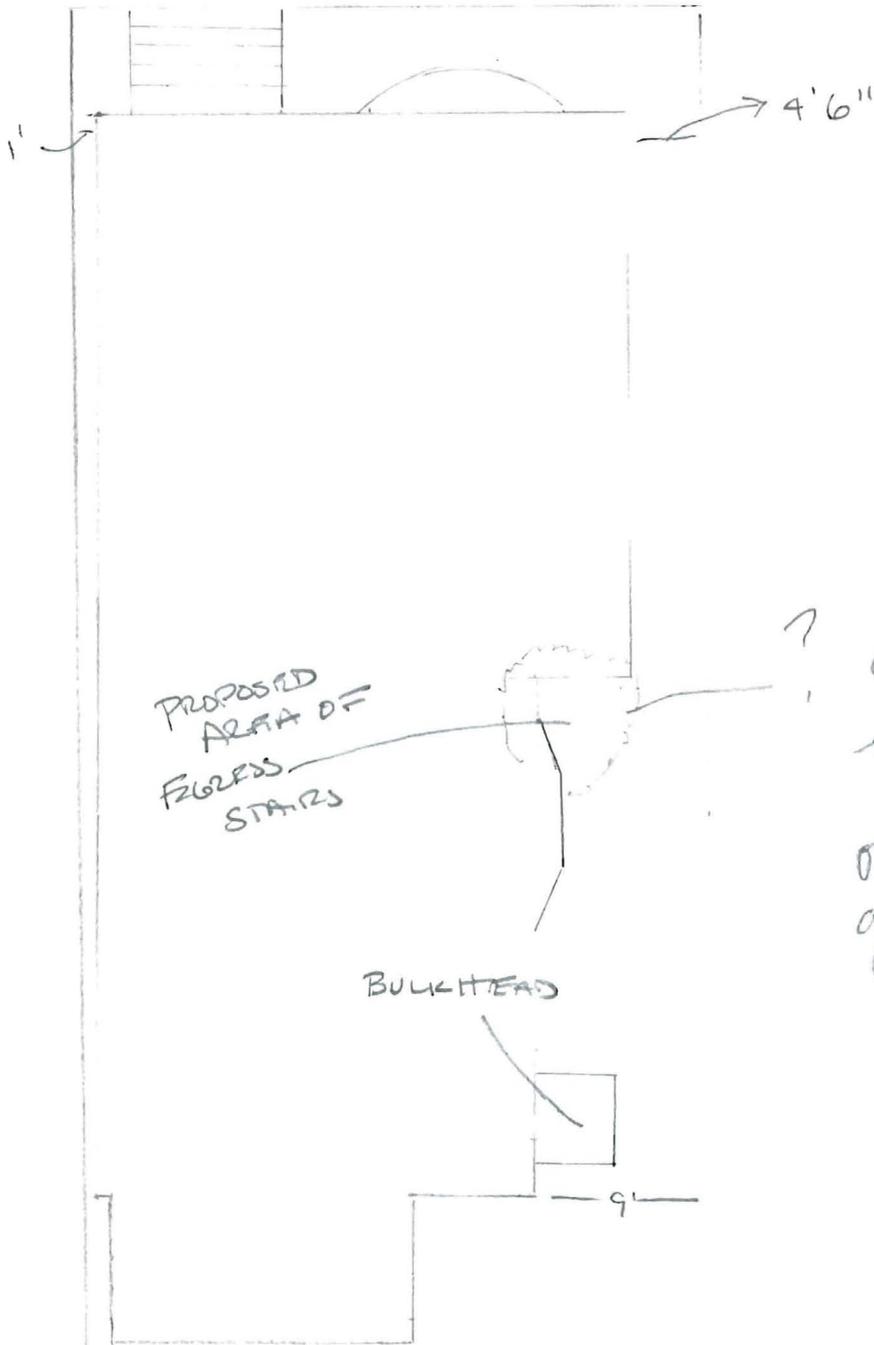
$3/32 = 1'$

90% in previous Reg
(3396[#] per Assessors)
339.6[#] min previous

$20 \times 17 = 340\#$
 $95 \times 9 = 225\#$



ok



CALL 12/30/10
 TALKED TO MARK —
 on Monday he will
 get me a revised plot
 plan showing dimensions
 to new →

696 CONGRESS ST.
 PLOT PLAN
 $\frac{3}{32} = 1'$

M & M Industries
14 Kirkland Avenue
South Portland, Me 04106
207-329-4875

December 14, 2010

City of Portland
389 Congress Street
Portland, Maine 04106

Re: 696 Congress Construction /Repairs due to Accident.

#1 Masonry Repairs:

Repoint foundation and replace bricks as necessary (any broken bricks will be replaced with bricks from the rear, by the new stairs. Foundation by the bow window will be taken apart and bricks will be shifted to make the accident caused distortion smaller. Masonry will be pointed to meet Historical District standards.

The front steps will be rebuilt according to engineering plans, and the railings, stored in the basement, will be re-installed.

#2 Window/glass Repairs:

Installed new curved glass. Reglaze and paint cracked window glazing.

#3 Basement Repairs:

Install new insulation and sheet rock over removed areas.

#6 Insulation:

Cut out broken insulation and re-spray. Cover insulation with fire retardant paint.

#7 Interior Repairs:

Install buttons and patch interior hall walls. .

#8 Install pad and new rear stairs as per city direction.

The egress issues were discussed in meetings with the Fire Department and Inspections Services. The rear pad will be built according to these meetings and an additional meeting with the Historic Preservation Board

The front stairs are to be completed by agreement with the Historic Preservation Board, Fire Department and Building Inspections.

Sincerely yours,

Mark A. Mawhinney

**L & L STRUCTURAL
ENGINEERING SERVICES, INC.**

Six Q Street
South Portland, ME 04106
Phone: (207) 767-4830
Fax: (207) 799-5432

September 29, 2010

Dan Houghman
50 Market Street
PMB 299
South Portland, Maine 04106

Subject: Building located at 696 Congress Street, Portland, Maine
Damage to Building Resulting from Automobile Accident

Dear Mr. Houghman,

As per your request, we visited the existing multi-family residential apartment building located at 696 Congress Street in Portland, Maine to review the damage to building resulting from a recent automobile accident. Our review of the building was limited to the damage caused by the automobile that collided with the building the weekend prior to our visit on Monday September 13, 2010. We are not reviewing the building and/or structure for compliance with current codes unless the automobile collision affected that particular component in the building. Our analysis and review of the structure was performed utilizing the 2003 International Building Code (IBC) adopted by the City of Portland. The analysis and review considered the Building Code Requirements for Wood Construction (NDS-latest edition) published by the National Forest Products Association and criteria specified by the Brick Institute of America (BIA). In addition, the building is located in a historic district. Hence, replacement and/or repair of any building components shall conform to the standards of the Maine Historic Society unless the standards are waived in the interest of compliance with the building codes or safety requirements.

The existing rectangular shaped building structure consists of dimensional timber framed floor and roof systems supported on interior timber bearing walls supported on timber beams and steel lally columns in the basement beneath the first floor level. There is an exterior two- or three-wythe (depending on level of the building being considered) brick bearing wall at the perimeter of the building. The automobile collision occurred at the right side of the building near the front right corner. The collision imposed a significant force large enough to transmit energy through the base of the front brick wall just beneath the first floor level and cause distortion of the brick on the opposite (left) side of the building. The imposition of the force shifted the front wall sideways including the bay window structure causing damage to components in the front wall at the first and second floor level as well as the concrete entry stair structure at the front of the building. In addition, the force shifted the first and second floor systems (horizontal diaphragms) causing distortion of the floors indicated in cosmetic damage to plaster walls and ceilings. The force also compressed the bay window structure laterally which caused the bay window and wall system to distort or bulge toward the front of the building pulling away from the floor structure indicated by the approximately 1" gap between the base of the first floor bay wall and the first floor system.

The existing building is not in imminent danger of collapse. However, the existing building has experienced significant structural and cosmetic damage as a result of the automobile collision. The damage that has occurred shall be repaired as soon as possible to curtail additional damage and potential structural failure caused by exposure of the components to the elements especially during the winter months. The specific components that have been damaged as a result of the automobile collision to the building are as follows:

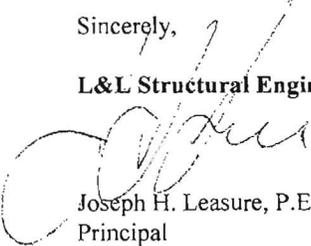
1. The base of the front brick wall and both side brick walls near the two front corners of the building has been compromised. The right side wall near the front right corner of the building where the impact occurred has distorted inward in a local area approximately 3 feet square. The left side wall near the front left corner of the building has distorted outward in an area approximately 2 feet square. The bricks in these areas have been cracked, defaced and mortar joints that have opened. The front wall has shifted laterally causing cracking of some bricks, shifting and opening of mortar joints, and large gaps/openings of the mortar joints in some areas. It would be time consuming and relatively expensive to repair the existing brick walls completely plumb and true by removing portions of the brick wall that have distorted and replacing the bricks and mortar to align plumb and true with the adjacent portions of the brick wall. You may decide to pursue that option to insure the brick walls are returned to the condition which they existed prior to the collision. However, at a minimum, the compromised brick walls shall be repaired by replacing cracked and defaced bricks and re-mortaring existing joints between the bricks that have shifted laterally (sheared) and opened up gaps between the bricks that are not currently sealed with mortar. This repair shall be implemented as soon as possible to avoid exposure to the elements during the winter months and curtail additional damage and potential structural failure. The individual bricks shall be replaced with bricks that match the original bricks in conformance with the Maine Historic Society. In addition, the compromised mortar joints shall be gouged out and cleaned free of debris, sealed with mortar conforming with the historic mortar according to the Maine Historic Society, and the mortar joints shall be implemented by "raking" the joint in conformance with the standard of the Maine Historic Society.
2. The existing concrete stair and steel railings at the front of the building has been compromised. Some of the concrete treads and portions of the concrete side walls have been cracked, completely broken or shifted laterally or rotated from the original position. In addition, the steel side rails have been bent and dislodged from the concrete embedment that structurally supported the railings. The original concrete stair and steel railings do not meet the current building code requirements for an egress stair entry. The entire stair system shall be either replaced or the effected concrete treads, concrete side walls and steel railings shall be repaired or replaced. In either case, the reconstruction of the stair to meet the current building code requirements would violate the standards of the Maine Historic Society and force the stair system to be located over the property line into the existing sidewalk. This is obviously unacceptable. Hence, a variance from the City of Portland shall be pursued to repair and/or replace the stair system to match the original condition. In addition, the repair and/or replacement of the "historic" steel handrails on both sides of the stair, which do not currently meet the building code requirements, may potentially be modified to enhance the safety by installing a bottom rail and balusters which will violate the standards of the Maine Historic Society. This shall be pursued with the City of Portland Building Department and the Maine Historic Society. The replacement of the entire stair system shall be implemented utilizing a reinforced concrete stair and side wall construction that matches the original stair to be designed upon your request. Otherwise, the compromised concrete treads shall be replaced with concrete treads reinforced with 3-#5 reinforcing bars longitudinally (one located near the stair nosing and two located at the center of the depth) and doweled into the repaired concrete side walls at both ends with 2-#4 hooked L-shaped bars drilled and grouted vertically 6" minimum into the repaired concrete side walls and embedded 15" minimum into the center of the new concrete stair tread. In addition, 2-#4 U-shaped reinforcing bars shall be installed at the end of the treads where the repaired steel vertical posts of the railings shall be embedded. The U-shaped bars shall be installed at the top and bottom of the treads at the ends wrapped around the vertical steel post (with 2" minimum concrete cover around the vertical steel post and around the reinforcing bar on all sides) and extend 15" minimum into the tread longitudinally. The steel side rails shall be repaired and /or replaced to match the original rails, potentially modified as previously discussed, and anchored as previously anchored.

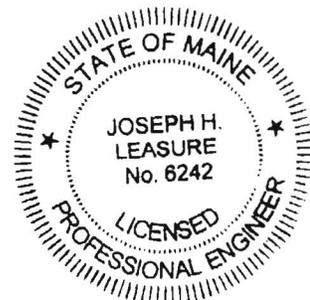
3. The existing bay window and wall system at the front of the building has distorted laterally and distorted (bulged) toward the front of the building pulling away from the first floor structure indicated by the approximately 1" gap between the base of the first floor bay wall and the first floor. There is various trim around the windows that has moved and windows that have shattered that require repair/replacement including caulking around windows and trim to insure that the existing system is well sealed. In addition, the wall has pulled away approximately 1" from the floor joists. We need to expose the underside of the first floor joists to verify that the floor joists have adequate support or it may be necessary to reinforce and/or re-support the existing floor joists. The gap in the first floor created by the movement shall be covered with floor sheathing at a minimum.
4. The front wall adjacent to the bay window system and the first and second floor systems has distorted laterally causing significant cosmetic damage to plaster walls and ceilings. The first and second floor systems (horizontal diaphragms) attempted to resist the lateral force and transferred the force to internal shear walls causing excessive distortion of the floors and walls which resulted in cosmetic damage to plaster walls and ceilings. The cosmetic damage of the finishes is clearly noticed by the relatively recent plaster cracking in the front entry wall, the plaster ceiling cracking above the first floor front entry at the front and rear of the interior stair well, the basement plaster ceiling cracking adjacent to the front wall where some pieces are falling away from the structure, the trim and plaster wall around the bay window, the foam insulation that has cracked and been compromised along the front wall especially adjacent to the gas meter, and the windows that have cracked adjacent to the front entry doors. There may be additional damage to the wall and ceiling plaster in the second floor unit adjacent to the front wall which was inaccessible at the time we visited the building. The wall and ceiling plaster shall be repaired by removing all the loose plaster and either re-finishing the walls and ceilings that have been compromised with new plaster and lath or installing gypsum wall board as long as the same fire separations are achieved where required. The windows that have been compromised shall be replaced and re-sealed as required and the foam insulation shall be replaced where it has been compromised.

If you have any questions or require any additional technical assistance, please do not hesitate to call.

Sincerely,

L&L Structural Engineering Services, Inc.


Joseph H. Leasure, P.E.
Principal
File



GENERAL NOTES:

1. The notes on the drawings are not intended to replace specifications. In addition to general notes. See specifications for requirements
2. Structural drawings shall be used in conjunction with job specifications and architectural, mechanical, electrical, plumbing, and site drawings. Consult, openings, chases, inserts, reglets, sleeves, depressions, and other details not shown on structural drawings.
3. All dimensions and conditions must be verified in the field. Any discrepancies shall be brought to the attention of the engineer before proceeding with the affected part of the work.
4. Do not scale plans.
5. Sections and details shown on any structural drawings shall be considered typical for similar conditions.
6. All proprietary products shall be installed in accordance with the manufacturers written instructions.
7. The structure is designed to be self supporting and stable after the erection is complete. It is the contractor's sole responsibility to determine erection procedures and sequencing to ensure the safety of the building and its components during erection. This includes the addition of necessary shoring, sheeting temporary bracing, guys or tiedowns. Such material shall remain the property of the contractor after completion of the project.
8. All applicable federal, state, and municipal regulations shall be followed, including the federal department of labor occupational safety and health act.

CONCRETE NOTES:

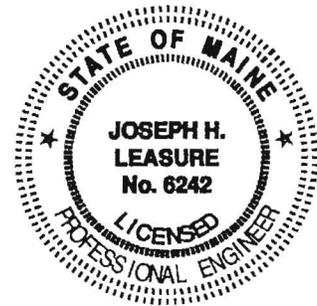
1. All concrete work shall conform to ACI 318—Latest Edition.
2. Concrete strength at 28 days shall be 4000 psi
3. All concrete shall be air entrained 4% to 6% per the specifications.
4. Concrete shall not be placed in water or on frozen ground.
5. Provide PVC sleeves where pipes pass through concrete walls or slabs.
6. Reinforcing bars shall conform to ASTM A615 Grade 60 deformed bars, and shall be detailed, fabricated and erected in accordance with ACI 315—Latest edition.
7. Welded wire fabric shall be provided in flat sheets.
8. Fiber reinforced concrete shall conform to ASTM C-1116.
9. Splices of reinforcing bars shall be in accordance with ACI 318. Splices of WWF shall be 6" minimum.
10. Concrete finishes: Broom finish for stair treads per owner's requirements.
11. Anchor bolts shall conform to ASTM A307 hot dipped galvanized unless noted otherwise on plan.
12. The general contractor shall be responsible for coordination of door bondout locations, slab depression & other required bondouts. Coordinate location of bondouts with Architectural, Mechanical & Plumbing, Electrical and kitchen equipment vendors as necessary to properly install each specific item.

DESIGN LOADS:

1. Building code: IBC (2003) International Building Code.
2. Design Live Loads:
Stairs & exit ways 100 PSF

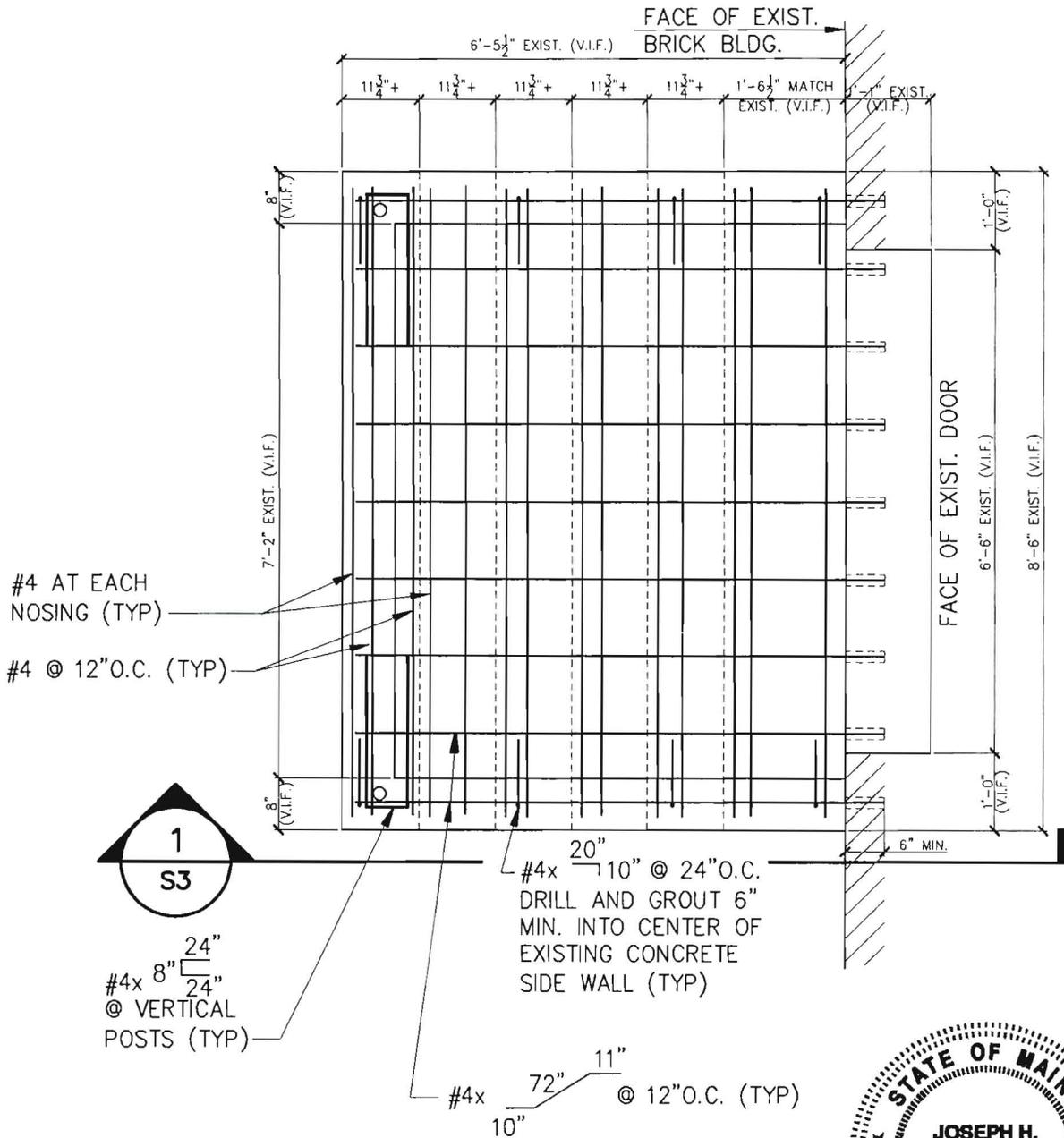
FOUNDATION NOTES:

1. Foundations have been designed with a presumptive soil bearing capacity of 2000 psf to be verified by the general contractor in the field.
2. Interior spread footings and exterior strip footings shall be founded on undisturbed native soil or compacted structural fill.
3. Exterior strip and spread footings shall be founded a minimum of 4'-0" below finished site grade or insulated as indicated on the drawings.
4. Slabs on grade shall bear on a minimum of 12" of compacted structural fill or compacted 3/4" crushed stone. If loose or undesirable fills are encountered at the slab subgrade level, they shall be over excavated to the surface of the natural soil and replaced with structural fill. Refer to drawings and specifications for vapor barrier requirements. Moist cure slabs in accordance with ACI.
5. Exterior concrete slabs on grade, shall be underlain by at least 4 feet of structural fill meeting gradation and compaction requirements noted above. Reinforce top of slabs with #4 @ 12" each way at center of slab.



2010-129

designed by: JHL	696 CONGRESS STREET PORTLAND, MAINE FRONT ENTRY STAIR REPAIR AND NEW REAR ENTRY STAIR GENERAL NOTES	L & L STRUCTURAL ENGINEERING SERVICES, INC. SIX Q STREET SOUTH PORTLAND, MAINE 04106 PHONE: (207) 767-4830 FAX: (207) 799-5432	S1
drawn by: AKB			
checked by: JHL			
scale: NONE			
date: 10-28-10			



FRONT ENTRY STAIR-CONCRETE PLAN

1/2" = 1'-0"



2010-129

designed by:	JHL
drawn by:	AKB
checked by:	JHL
scale:	1/2" = 1'-0"
date:	10-28-10

696 CONGRESS STREET
 PORTLAND, MAINE

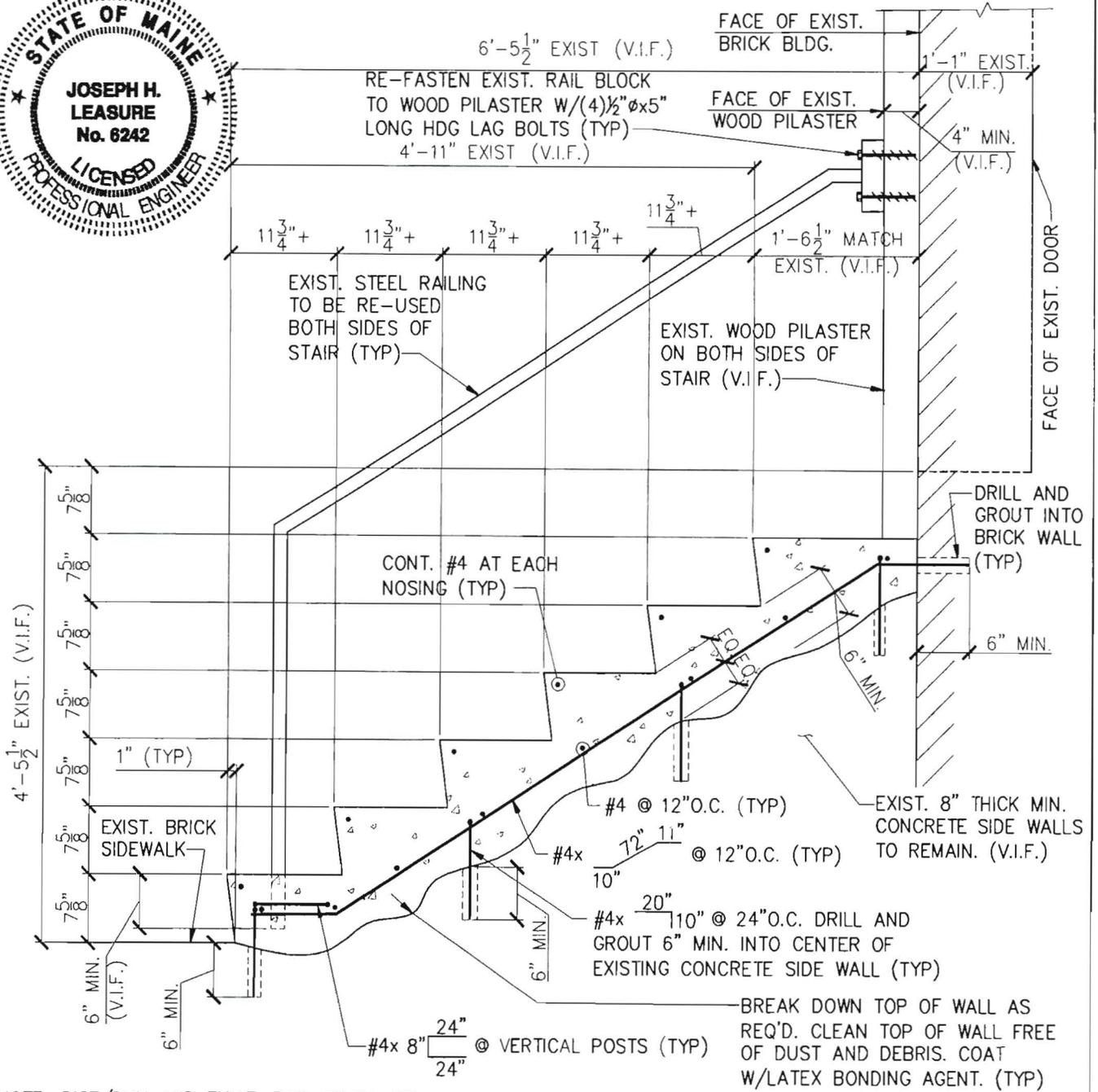
FRONT ENTRY STAIR REPAIR AND
 NEW REAR ENTRY STAIR

FRONT ENTRY STAIR-CONCRETE PLAN

L & L STRUCTURAL
 ENGINEERING SERVICES, INC.
 SIX Q STREET
 SOUTH PORTLAND, MAINE 04106

PHONE: (207) 767-4830
 FAX: (207) 799-5432

S2

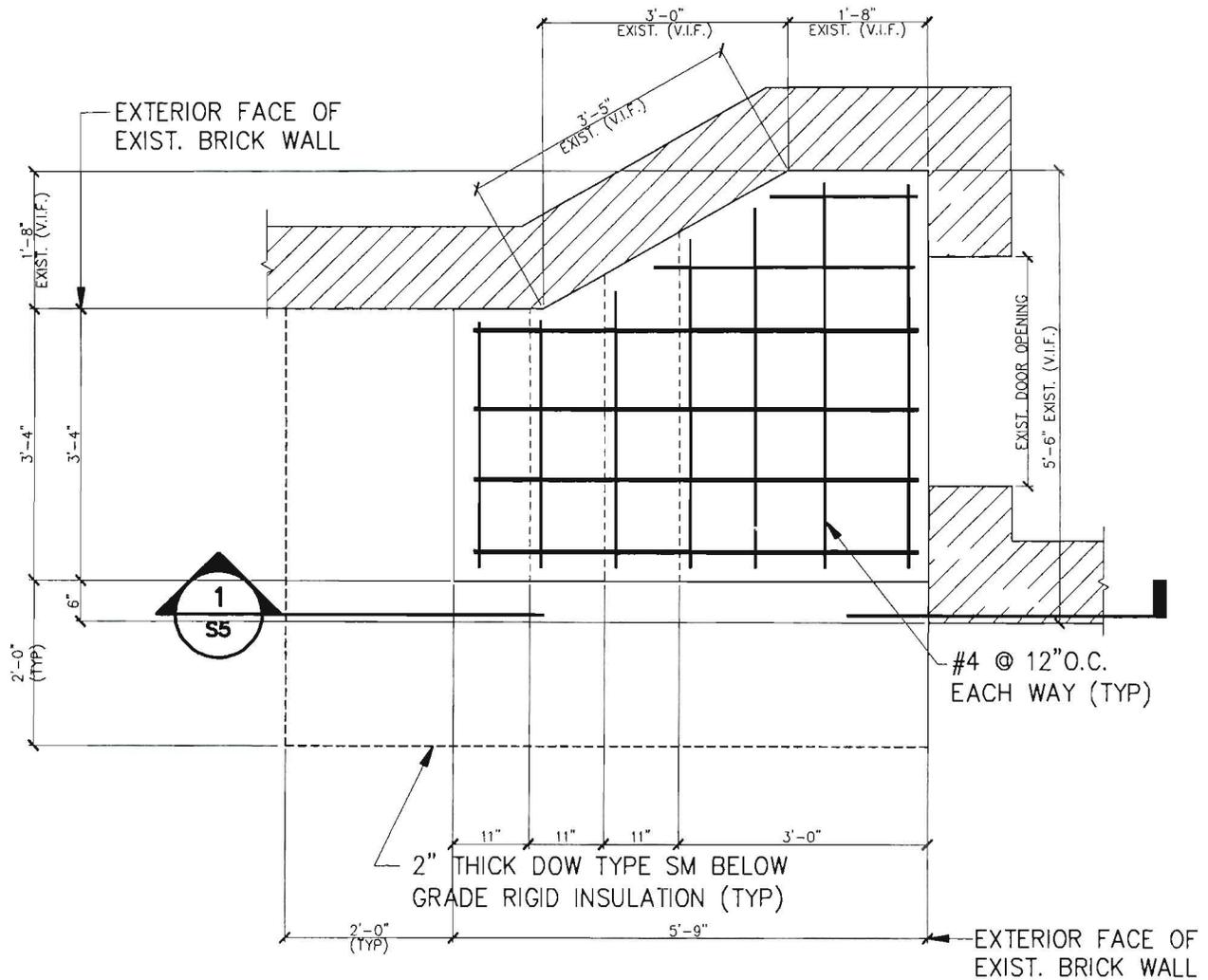


NOTE: RISE/RUN AND EXIST. RAIL SHALL BE RE-CONSTRUCTED OR RE-USED TO MATCH EXIST. CONDITIONS DUE TO HISTORIC BUILDING REQUIREMENTS. BUILDING CODE REQUIREMENT HAVE BEEN WAIVED ON RISE/RUN AND RAIL REQUIREMENTS BY THE CITY OF PORTLAND TO ALLOW CONFORMANCE WITH THE HISTORIC REQUIREMENTS.

FRONT ENTRY STAIR-SECTION
 $\frac{3}{4}'' = 1'-0''$

1
 S2

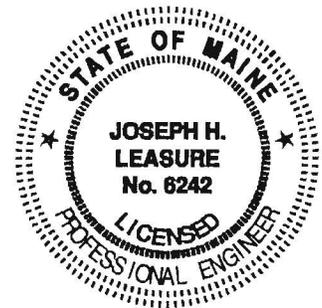
2010-129	696 CONGRESS STREET PORTLAND, MAINE		L & L STRUCTURAL ENGINEERING SERVICES, INC. SIX Q STREET SOUTH PORTLAND, MAINE 04106 PHONE: (207) 767-4830 FAX: (207) 799-5432
designed by: JHL	FRONT ENTRY STAIR REPAIR AND		
drawn by: AKB	NEW REAR ENTRY STAIR		
checked by: JHL	FRONT ENTRY STAIR-SECTION		
scale: $\frac{3}{4}'' = 1'-0''$			
date: 10-28-10			S3



NOTE: GUARD RAILS AND HANDRAILS TO BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH CODE BY OTHERS (TYP)

REAR ENTRY STAIR-CONCRETE PLAN

1/2" = 1'-0"

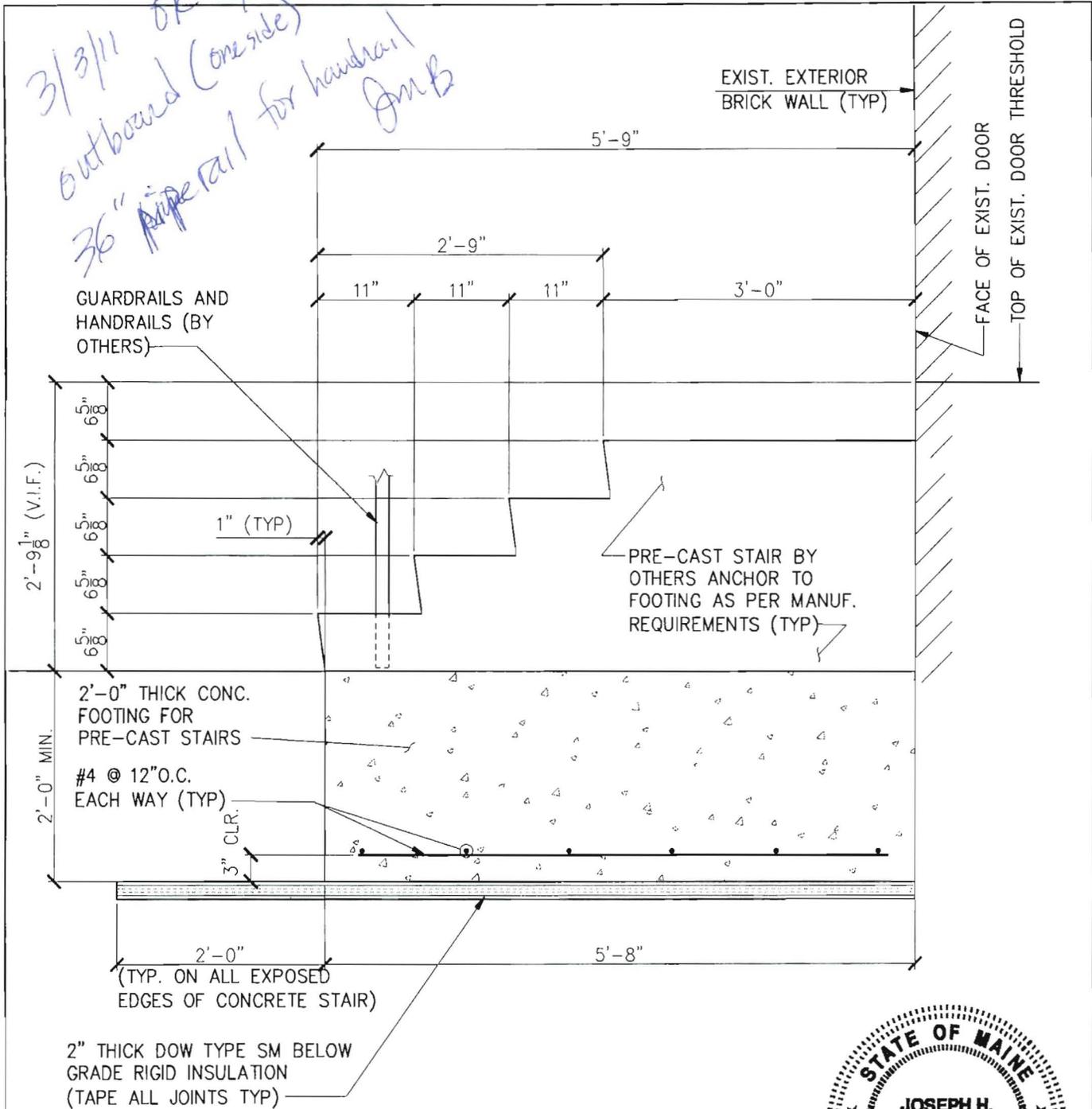


2010-129

designed by: JHL	696 CONGRESS STREET PORTLAND, MAINE FRONT ENTRY STAIR REPAIR AND NEW REAR ENTRY STAIR REAR ENTRY STAIR-CONCRETE PLAN	L & L STRUCTURAL ENGINEERING SERVICES, INC. SIX Q STREET SOUTH PORTLAND, MAINE 04106 PHONE: (207) 767-4830 FAX: (207) 799-5432
drawn by: AKB		
checked by: JHL		
scale: 1/2" = 1'-0"		
date: REV. 11-1-10		

S4

*3/3/11 OK w/ Deb A.
outboard (one side)
36" Pipe rail for handrail
Am B*



REAR ENTRY STAIR-SECTION 1
 3/4" = 1'-0" S5



2010-129	designed by: JHL	696 CONGRESS STREET PORTLAND, MAINE	L & L STRUCTURAL ENGINEERING SERVICES, INC. SIX Q STREET SOUTH PORTLAND, MAINE 04106
drawn by: AKB	FRONT ENTRY STAIR REPAIR AND		
checked by: JHL	NEW REAR ENTRY STAIR		
scale: 3/4" = 1'-0"	REAR ENTRY STAIR-SECTION		
date: REV. 11-1-10			
			PHONE: (207) 767-4830 FAX: (207) 799-5432
			S5



CITY OF PORTLAND, MAINE

Department of Building Inspections

Original Receipt

_____ 12 29 20 10 _____

Received from M. M. Jones

Location of Work 696 Congress

Cost of Construction \$ _____ Building Fee: 220

Permit Fee \$ _____ Site Fee: _____

Certificate of Occupancy Fee: _____

Total: 220

Building (IL) Plumbing (I5) Electrical (I2) Site Plan (U2)

Other _____

CBL: SS-E-32

Check #: 3957 Total Collected \$ 220 1/10

**No work is to be started until permit issued.
Please keep original receipt for your records.**

Taken by: [Signature]

WHITE - Applicant's Copy
YELLOW - Office Copy
PINK - Permit Copy