



**CITY OF PORTLAND  
APPROVED SITE PLAN**  
Subject to Conditions of Approval  
and Standard Conditions

DATE OF APPROVAL **3-27-2018**

PLANNER Jean Fraser

PROJECT NO. #2017-289

**subject to conditons in the  
PB Approval letter dated  
3.29.18**

# MAINE MEDICAL CENTER

## Construction Projects at the East Tower, Visitor Garage, Central Utility Plant

### Construction Management Plan

**December 15, 2017**

**[Updated for 1/23/18 Planning Board Meeting]**

**(Updated for 2/27/18 Planning Board Meeting)**

**(Updated for 3/27/18 Planning Board Hearing)**

**Prepared for: Portland Department of Planning & Urban Development Department**

**Project Proponent: Colliers International**

**Construction Manager: Turner Construction**

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# **MAINE MEDICAL CENTER**

## **Construction Projects at the East Tower, Visitor Garage, Central Utility Plant**

### **Construction Management**

**Plan 12.15.17  
(Updated 02.07.18)  
(Updated 03.09.18)**

### **Purpose Statement**

The purpose of this document is to provide the details of the first phase of construction for MMC's Master Facility phase IIB. This document outlines Turner Construction's plan for controlling impacts from noise, vibrations, ground movement, truck traffic, and other construction related factors to the surrounding neighborhoods and buildings.

Maintaining normal campus operations and Public Safety are high priorities during the construction phase. This plan will be presented to the appropriate City and Neighborhood representatives. The City of Portland Planning Board will approve the final plan. It has been the goal of these programs to define the construction plan before construction begins. Turner's experience on projects with similar logistic constraints has proven that pre-planning and constant communication are necessary for a smooth efficiently run, incident free project.

It is Turner Construction's full intent to follow the guidelines set by the contract documents.

Turner Construction and its subcontractors shall schedule and conduct operations in a manner that will control the disturbance to the public in areas adjacent to the work and to occupants of buildings in the vicinity.

# 1.0 CONSTRUCTION MANAGEMENT PLAN

This Construction Management Plan (CMP), submitted to the Portland Department of Planning & Urban Development for approval by the City of Portland Planning Board prior to the start of construction, includes specific mitigation measures and staging plans to minimize impacts to surrounding residences and businesses. The Construction Manager and its trade contractors will be bound by the CMP.

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# 2.0 CONSTRUCTION SCHEDULE & WORK HOURS

Construction work hours will be in accordance with Section 17-18 and Section 25-129.

## 2.1 Work Hours

- The construction period for the project is expected to be approximately 19 months in duration.
- It is anticipated that the project is estimated to start on or about Spring 2018 and last through Fall 2019.
- Work hours will be 7AM to 7 PM Monday thru Friday.
- Workers will gather at their workplace from 6 AM to 7AM.
- Saturday work will be required on a periodic basis with hours from 7 AM to 6 PM.

## 2.2 Construction Timeline

### 2.2.1 Spring 2018 to Fall 2019

East Tower – Wescott Street



A new 60,000 SF addition is being added to the top of the East Tower. The addition consists of two new floors, level 6 and 7. On the roof is a helipad system capable of landing two helicopters. The exterior wall at Level 5 is being replaced in conjunction with a new exterior wall for Level 6 and 7. This project is expected to start in April 2018 and complete by October 2019. (19 Months)

### **Visitor Garage- Congress Street**

Three new floors of parking are being added to the Visitor Garage. These floors will be Level 8, 9 and 10. The added floors are precast and are being added to a garage which has been designed for this expansion. The added parking capacity is 220 cars. This project is expected to start in May 2018 and complete in November 2018. (6 Months)

### **Central Utility Plant – Gilman Street**

The Central Utility Plant is increasing cooling capacity by adding one 2,400 ton chiller and two cooling towers. This project is expected to start in November 2018 and complete in March 2019. (5 months)

## **2.3 Construction Updates Reporting**

The following shall be reported to Maine Medical as a part of the CMP.

- Monthly “look ahead” schedules and narrative for upcoming construction activities.
- Schedule of over-size deliveries to the site that will impact roadways
- Schedule of deliveries that will be outside the City of Portland’s allowable work hours.
- Proposed schedule for utility connection or work in public roadways or sidewalks as a part of Turner work.
- Proposed off-hours or Saturday work.
- Construction activities that may impact the neighborhood in an adverse way, including noise, vibration, dust, on-street parking or work outside construction limits or times. Also, proposed options to mitigate these impacts.
- An estimate of the number of vehicles that will require off-site parking.

## **2.4 Construction Logistics**

The proposed Logistics Plans included in the CMP are designed to isolate the construction while providing safe access for pedestrians, hospital operations, and automobiles during normal day-to-day activities and emergencies. Interaction with the public will occur mainly along Congress Street and Gilman Street. The logistics plan define the timeline of construction activities and the location of cranes, gates, access for each Phase. Each site will be secured by an 8-foot high fence with privacy screening in accordance with the Logistics Plan.

**Logistics plans on the construction sequencing for East Tower Overbuild are Slides 2 to 12.**

**Logistics plans on the construction sequencing for Visitor Garage Overbuild are Slides 13 to 26.**

**Logistics plans on the construction sequencing for Central Utility Plant cooling upgrade Slides 27 to 29.**

## **2.5 Perimeter Protection / Public Safety**

- Turner Construction will work to ensure the sidewalk protection minimizes impact to pedestrian and vehicular flow.
- The specific configuration of sidewalk protection and pedestrian access around the site will vary depending on the phase of the work being performed.
- In general, secured fencing will be used to isolate construction areas from pedestrian traffic.
- Flagmen or Police Details will be provided at all active gates when there are deliveries.
- Construction procedures will be designed to meet all OSHA safety standards for specific site construction activities.
- Subcontractors will implement and manage their own Health and Safety Program for the project.
- All Subcontractors are required to wear appropriate personal protective equipment.
- Snow removal and ice treatment will be provided on the surrounding area within the construction fence.
- Sidewalks will be cleaned of trash and debris as needed.
- If existing street lights need to be removed for the duration of the project,
- Concrete washout will be in concrete washout bags, collected and placed in a dumpster. Concrete washout area to be located within the site fence. **(Refer to logistics slide 9)**
- Turner Construction will provide sufficient temporary site lighting to ensure the safety of all pedestrians accessing the sidewalks around the site, including lighting at all covered pedestrian walkways, until permanent street lights are installed Light levels will be tested prior to temporary removal of site lighting.

## **2.6 Worker Parking & Traffic Impacts**

- Turner Construction and its subcontractors shall encourage the use of public transportation by their workers.
- Jobsite personnel will be required to park at an offsite parking area to be determined and will be shuttled to the construction site.
  - Jobsite personnel will be required to park at MMC's Scarborough Campus
  - Jobsite personnel will be shuttled to the construction site
  - Subcontractors who violate this requirement will be fined \$500 per offense
- Worker parking shall not be allowed on site except for company vehicles required to perform the work.

- Turner will qualify subcontractors' with a legitimate reason to park onsite and manage that parking within the construction fence and not impact parking in the surrounding area.
- No personal vehicles will be allowed to park at the project construction site or in the adjacent residential streets.
- The following language will be included in the contracts with all subcontractors
  - *“Parking: There shall be absolutely no parking on the project site, in the Hospital Parking Garages and / or Hospital Surface Parking lots, and on street parking adjacent to the Hospital. This is including but not limited to employee vehicles, company vehicles, delivery trucks, trucks containing equipment such as welders, etc. Turner to provide remote parking location, and transportation to and from the jobsite. Any workers found in violation of this policy will be subject to permanent removal from the project and the subcontractor shall be assessed a \$500 fine per incident.”*

**The number of workers required during the construction will vary by Construction Phase.**

### **Construction Phase - April 2018 to September 2019**

- Spring 2018- Fall 2018 – average workers – 40 to 50
- Fall 2018 – Spring 2019 – average workers – 70 - 90
- Spring 2019- Fall 2019 - average workers – 100

## **2.7 Truck Routes and Volumes – Refer to Site Logistics Plans**

**All street opening and occupancy in the public right of way will be in accordance with Chapter 25 of the City of Portland Land Use Code**

- Truck traffic will vary throughout the construction period, depending on the activity.
- An offsite marshalling area will be utilized for large deliveries such as precast and structural steel.
- All trucks will be brought to the construction areas at MMC by experienced dedicated drivers from the marshalling lot.
- Radio and cell phones will be utilized to coordinate all trucking.
- Trucks routes must be submitted and approved by the City of Portland and the Owner before the start of construction.
- Truck idling will be held to a minimum on-site, to minimize the impact to adjacent properties..
- Turner Construction and its subcontractors shall ensure that haul routes, debris and soil removal, and staging areas are conducted in a manner that minimizes the impacts to the abutting communities,
- Wheel wash stations will be installed and maintained by Turner and its subcontractors as needed and maintained at construction site exit areas.
- Street sweeping / vacuuming of all impacted City streets and sidewalks shall be performed by Turner Construction and its subcontractors on an as-needed basis.

- Streets, sidewalks, bicycle ways or other means of travel may not be obstructed or closed without permission from the Owner and other authorities having jurisdiction. The Owner shall be notified one week in advance of any such obstructions or closures.

### **Construction - East Tower Overbuild – (Refer to Logistics Slides 2 thru 12)**

**All street opening and occupancy in the public right of way will be in accordance with Chapter 25 of the City of Portland Land Use Code**

- All truck traffic will be directed to enter the site in accordance with truck route plans approved by the City of Portland and included in the Logistics plans. **(Refer to Logistics slides 8-12)**
- Turner will install way finding signage at strategic locations identifying no travel zones for construction vehicles.
- All subcontractors and vendors will be provided with pamphlets indicating the appropriate truck routes to access the site.
- Trucks will be brought within the fence line.
- The crane erection requires the closure of Ellsworth for a duration of one long day.
- **Ambulance right of way thru East Tower tunnel is identified on logistics slide 9.**
- Fire Safety requirements are addressed in Turner’s Environmental and Health Safety Program for Maine Medical, pages 34 to 36.
- During crane erection a portion of Ellsworth Street and Wescott will be closed. **(Refer to logistics slide 8)**

### **Construction Visitor Garage Overbuild – (Refer to Logistics Slides 13-26)**

**All street opening and occupancy in the public right of way will be in accordance with Chapter 25 of the City of Portland Land Use Code**

- All truck traffic will be directed to enter the site utilizing Congress Street. The Congress Street road closure is from Monday May 7, 2018 to Thursday June 28, 2018.
- During the erection of the precast pieces, overtime will be used. The work week will consist of 10 hour days –Monday thru Thursday, Friday will be an eight hour day and the plan is to work 8 hours on Saturday for the first two Saturdays. In the event that time is lost during the week due to wind and rain, the contractor will continue to work the remaining Saturdays.
- All detours and paths of travel shown on the Traffic Management Plans will be followed.
- All road signage identified on the Maintenance of Traffic Plans will be installed and maintained by Turner during the Congress Street shutdown and detour.
- Temporary repair patching of Congress Street will completed before Congress Street is reopened. If permanent patching or repairs are required they will be coordinated and completed in accordance with City of Portland timeline requirements.
- There are three places that Weymouth Street resurfacing will occur prior to the Congress Street closure. After the closure is complete, Weymouth Street will be inspected for damage.

- During an eight week period – May 7, 2018 to June 28, 2018 - Congress Street will need to be closed at the location of the Maine Medical Center Visitor Garage. Detailed logistics plans are included to identify the street closure requirements.(refer to logistics slides 18-26)
- This street closing will be in accordance with the City of Portland and Maintenance of Traffic requirements determined during the Site Plan Review process.
- The exterior façade treatment is expected to be coordinated and installed during the construction phase of the new Clinical Tower.
- All subcontractors and vendors will be provided with instructions prior to their project start indicating the appropriate truck routes to access the site.

### **Construction - Central Utility Plant – (Refer to Logistics Slides 27-29)**

**All street opening and occupancy in the public right of way will be in accordance with Chapter 25 of the City of Portland Land Use Code**

- All truck traffic will be directed to the Central Utility Plant utilizing a truck path from Congress Street to Gilman Street, and a departing path from A Street. **(refer to logistics slides 28-29)**
- During a two week period when the chillers and cooling towers are installed, trucks and a crane will be set up on Gilman Street. This is expected to occur in Fall/Winter 2018. Police detail will be fulltime during this period. Gilman Street will remain open.
- Lane closing will be coordinated with the City of Portland.
- All subcontractors and vendors will be provided with instructions prior to their project start indicating the appropriate truck routes to access the site.

## **3.0 Air Quality & Noise**

### **3.1 Dust Control**

To reduce emission of dust and minimize impacts on the local environment, the construction contractor will adhere to a number of strictly enforced mitigation measures. These include:

- Wetting agents will be used regularly to control and suppress dust that may come from the construction materials.
- All trucks used for transportation of construction debris will be fully covered with tarps intended to keep material in the truck.
- Construction practices will be monitored to ensure that unnecessary transfers and mechanical disturbances of loose materials are minimized and to ensure that any emissions of dust are negligible.
- Street and sidewalks will be cleaned to minimize dust accumulations as needed.
- Stock piles of soils will be covered with tarps and/or poly when they are not being utilized for current site activities.

## 3.2 Noise

- The project will require the use of equipment that can be heard from off-site locations. This project will strive to mitigate construction noise impacts, to the extent possible.
- Increased community sound levels are an unavoidable consequence of construction activities.
- Construction will occur during the daytime hours as defined by the City of Portland Code of Ordinances (7:00 a.m. to 6:00 p.m. Monday through Friday).
- In some instances, a second shift, off hour, holiday and weekends may be required.
- When these events arise, all required permits will be obtained and notification will be posted in accordance with section 2.3
- Work shall be performed so as to prevent nuisance noise conditions that are preventable (e.g., unmaintained equipment, brake squeal, etc.).
- Turner Construction and all subcontractors shall use all reasonable efforts to implement noise reduction methods listed below to minimize construction noise emission levels. Noise reduction methods shall include, but not be limited to:

### Use of:

- Tie to local power grid to reduce the use of on-site generators.
- Noise-deadening / isolating surrounds around exceptionally noisy work equipment or operations
- The maintenance of the access roadways to minimize the “bouncing” of construction vehicles within the construction site.
- Enforcing the site logistics plan, so as to maximize to every extent possible the forward movement of construction vehicles. This will, in turn, minimize the sound of back-up alarms.

### Attaching:

- Intake / exhaust mufflers, shields or shrouds
- Noise-deadening material to equipment and fencing

### Maintaining:

- Equipment mufflers and lubrication
- Pre-cast decking and plates to avoid rattling
- Smooth surfaces on construction sites and public ways to prevent unnecessary noise from surface irregularities.

### Minimizing

- Noise from the use of backup alarms, using measures that meet OSHA regulations. The construction logistics has been designed such that the backing-up of equipment and trucks is limited.
- Use of self-adjusting ambient-sensitive backup alarms.
- Manually adjustable alarms on low setting

- Scheduling of activities so that alarm noise is minimized

### **3.3 Air Monitoring**

- Turner Construction and its subcontractor's shall employ dust, odor, and gas control measures to minimize the creation of airborne dusts, odors, and gases during the entire construction process. At a minimum, standard dust control techniques shall be employed where heavy equipment will be traveling, standing, or loading, such as watering down the site or utilizing dust control measures. All measures shall be taken in accordance with Turner's approved Health & Safety plan.
- Turner Construction and its subcontractors shall perform dustless street sweeping / vacuuming for all construction activities including demolition, excavation, slurry / soil mixing operations on an as needed basis.
- An air monitoring plan shall be developed by the Owner based on site contaminants of concern, particulate quantities / densities, and risk-based evaluations. The plan will be developed for off-site impacts. Turner Construction and its subcontractors will be responsible for on-site health and safety issues.
- The Owner will engage a qualified consultant to monitor for dust along the perimeter of the construction site. Results of this monitoring that indicate exceedances of air quality criteria will be immediately brought to the attention of Turner Construction. Turner and its subcontractors shall immediately address the site condition, including terminating construction activities to remedy the site conditions to the satisfaction of the Owner.
- Ductwork snorkels will be installed as required on building air intakes surrounding the job-site to minimize the amount of dust and construction vehicle gases being taken in through the air handling systems. The maintenance of the filters is by the individual building managers.

## **4.0 Communication Strategies**

### **4.1 Communication Plan**

Turner will work with MMC's communications team to deliver a communications plan that encompasses:

- Plans for traffic management during Portland Seadogs games
- Plans for METRO bus stops and routes
- Implementation plans for all aspects of the detours outlined in the Maintenance of Traffic plan completed by Gorrill Palmer
- An emergency contacts list for the City of Portland including specifically Portland Police and Fire Departments.

### **4.2 Contacts, Distribution Lists**

- Turner Construction shall develop a project contact list (e-mail addresses, office and cell phone numbers), to be posted by MMC on the project web site and supplied to Maine Medical Center (MMC) for 24/7 contacts.
- Turner Construction will request similar contact lists from the neighborhood associations. (e-mail addresses, and phone numbers) of key contacts for the specific project.
- Turner Construction will set-up a project bulletin board for each Phase. These boards will be used as communication tools to the surrounding community. There will be a project website thru MMC, developed that also will communicate construction related activities to the community.
- Any last minute changes to the project schedule that may impact the neighborhood or Maine Medical Center (MMC) community will be communicated per section 2.3.
- Turner Construction will participate in the neighborhood council meetings as needed to discuss progress and issues.
- Turner shall maintain a log of all construction-related concerns, via the MMC website.
- MMC shall communicate final resolution of all complaints and concerns to the neighborhood, Maine Medical Center (MMC) community, and when appropriate, to the City of Portland.

## **5.0 Vibration Monitoring – Not Applicable**





Maine Medical Center  
MaineHealth

# Construction Management Plan

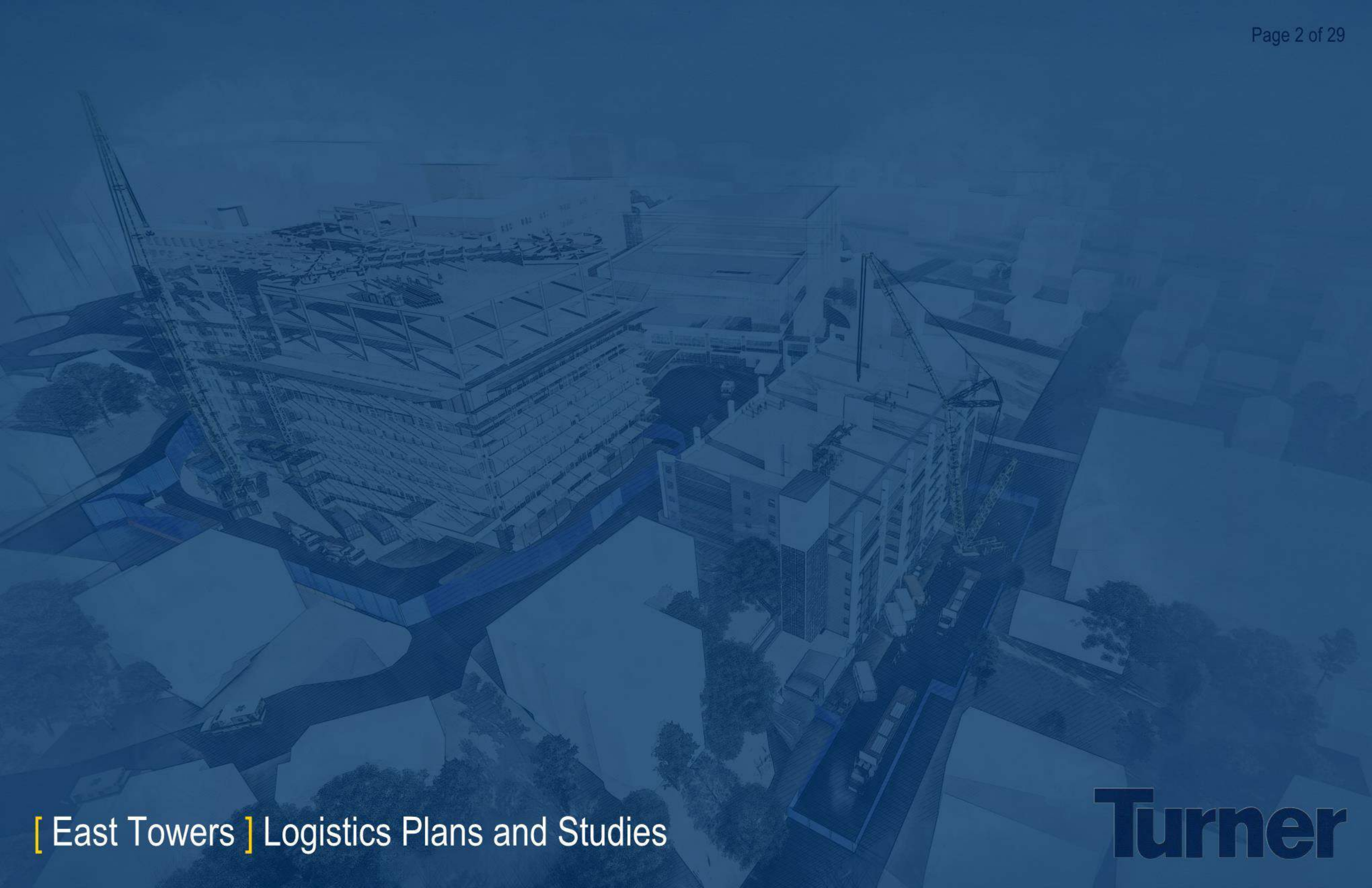
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[ Logistics Plans and Studies ]  
East Towers and Visitors Garage







[ East Towers ] Logistics Plans and Studies

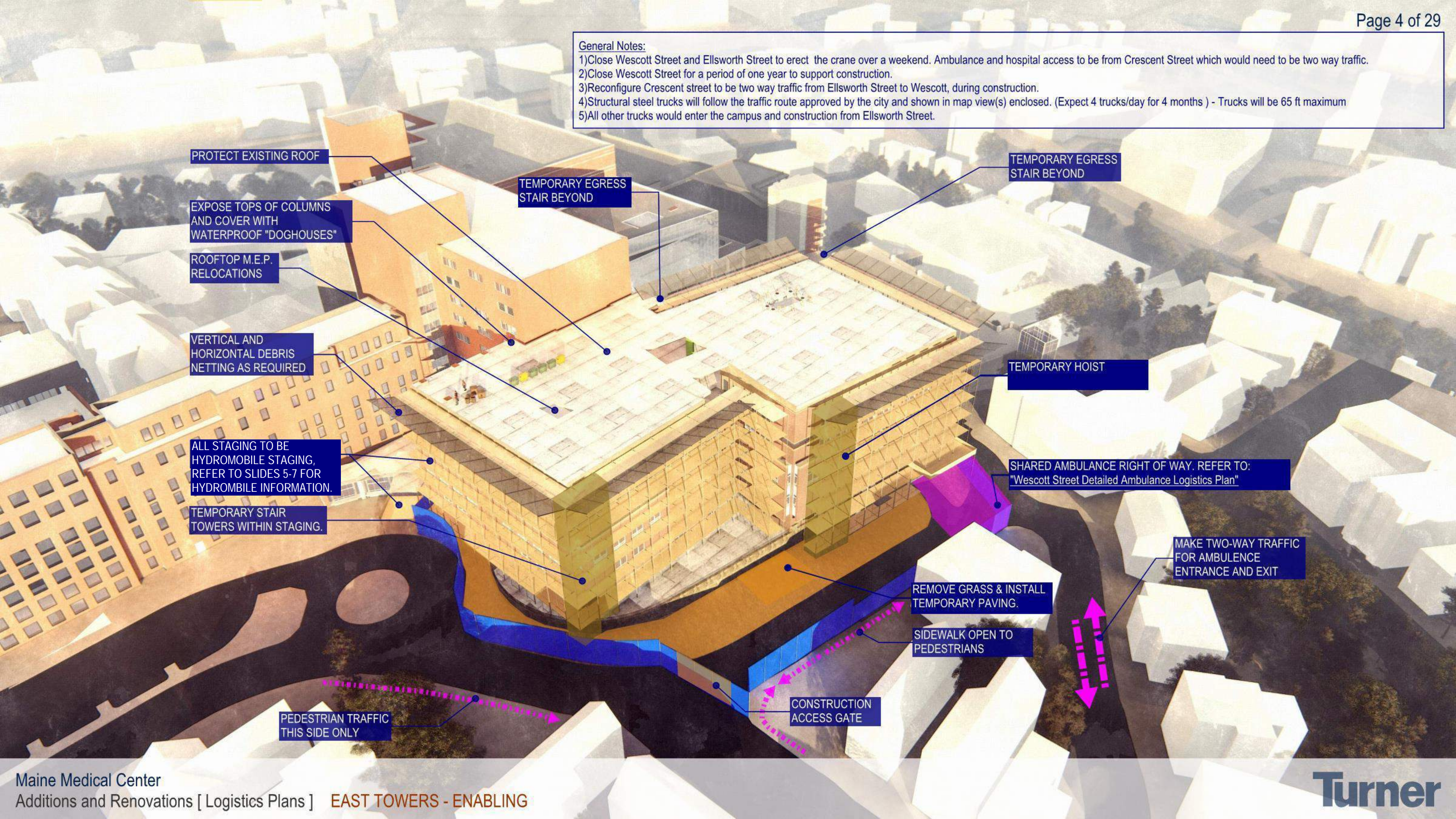








**General Notes:**  
 1) Close Wescott Street and Ellsworth Street to erect the crane over a weekend. Ambulance and hospital access to be from Crescent Street which would need to be two way traffic.  
 2) Close Wescott Street for a period of one year to support construction.  
 3) Reconfigure Crescent street to be two way traffic from Ellsworth Street to Wescott, during construction.  
 4) Structural steel trucks will follow the traffic route approved by the city and shown in map view(s) enclosed. (Expect 4 trucks/day for 4 months) - Trucks will be 65 ft maximum  
 5) All other trucks would enter the campus and construction from Ellsworth Street.



PROTECT EXISTING ROOF

EXPOSE TOPS OF COLUMNS AND COVER WITH WATERPROOF "DOGHOUSES"

ROOFTOP M.E.P. RELOCATIONS

VERTICAL AND HORIZONTAL DEBRIS NETTING AS REQUIRED

ALL STAGING TO BE HYDROMOBILE STAGING, REFER TO SLIDES 5-7 FOR HYDROMOBILE INFORMATION.

TEMPORARY STAIR TOWERS WITHIN STAGING.

PEDESTRIAN TRAFFIC THIS SIDE ONLY

TEMPORARY EGRESS STAIR BEYOND

TEMPORARY EGRESS STAIR BEYOND

TEMPORARY HOIST

SHARED AMBULANCE RIGHT OF WAY. REFER TO: "Wescott Street Detailed Ambulance Logistics Plan"

MAKE TWO-WAY TRAFFIC FOR AMBULANCE ENTRANCE AND EXIT

REMOVE GRASS & INSTALL TEMPORARY PAVING.

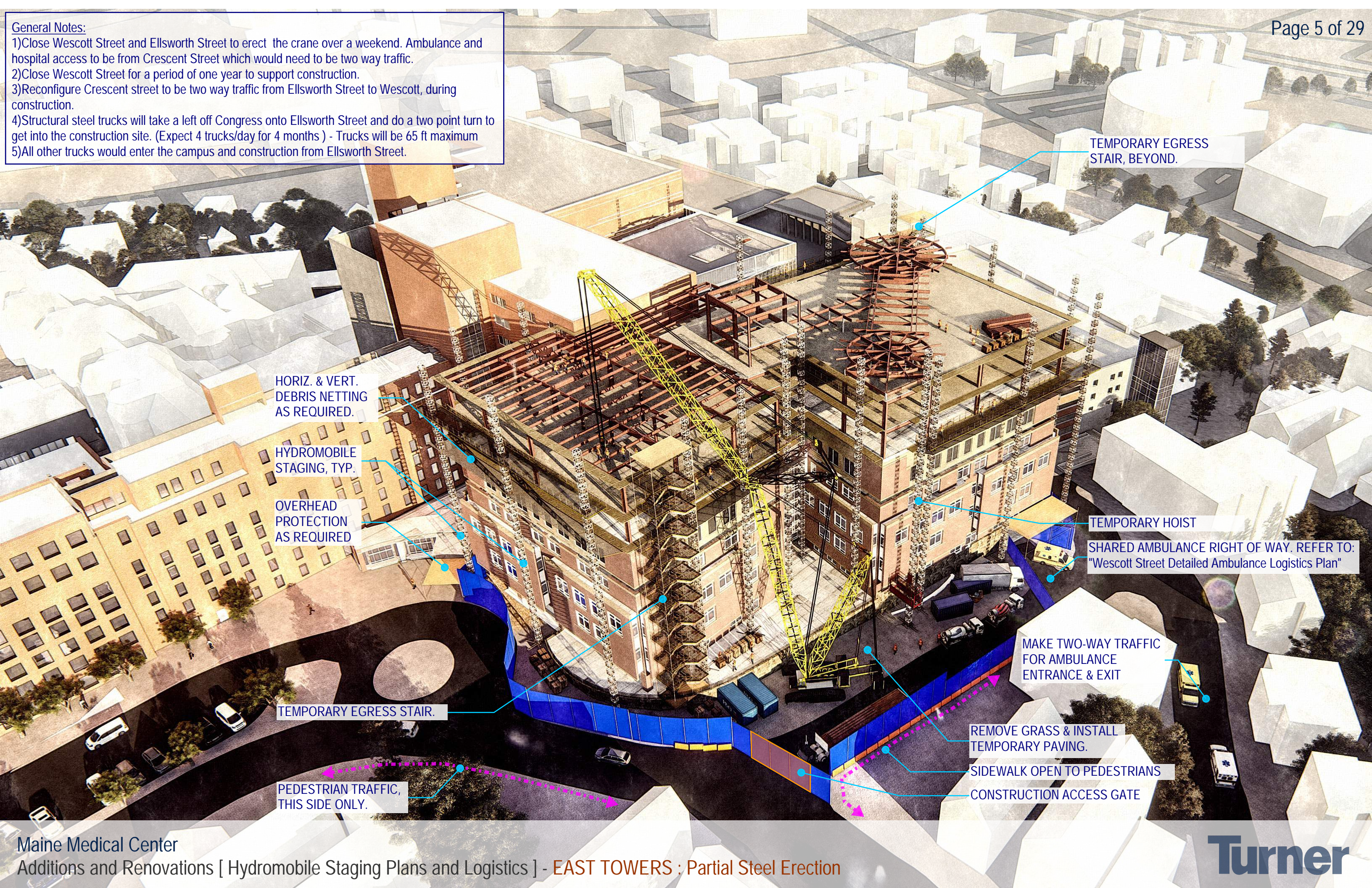
SIDEWALK OPEN TO PEDESTRIANS

CONSTRUCTION ACCESS GATE



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TEMPORARY EGRESS STAIR, BEYOND.

HORIZ. & VERT. DEBRIS NETTING AS REQUIRED.

HYDROMOBILE STAGING, TYP.

OVERHEAD PROTECTION AS REQUIRED

TEMPORARY HOIST

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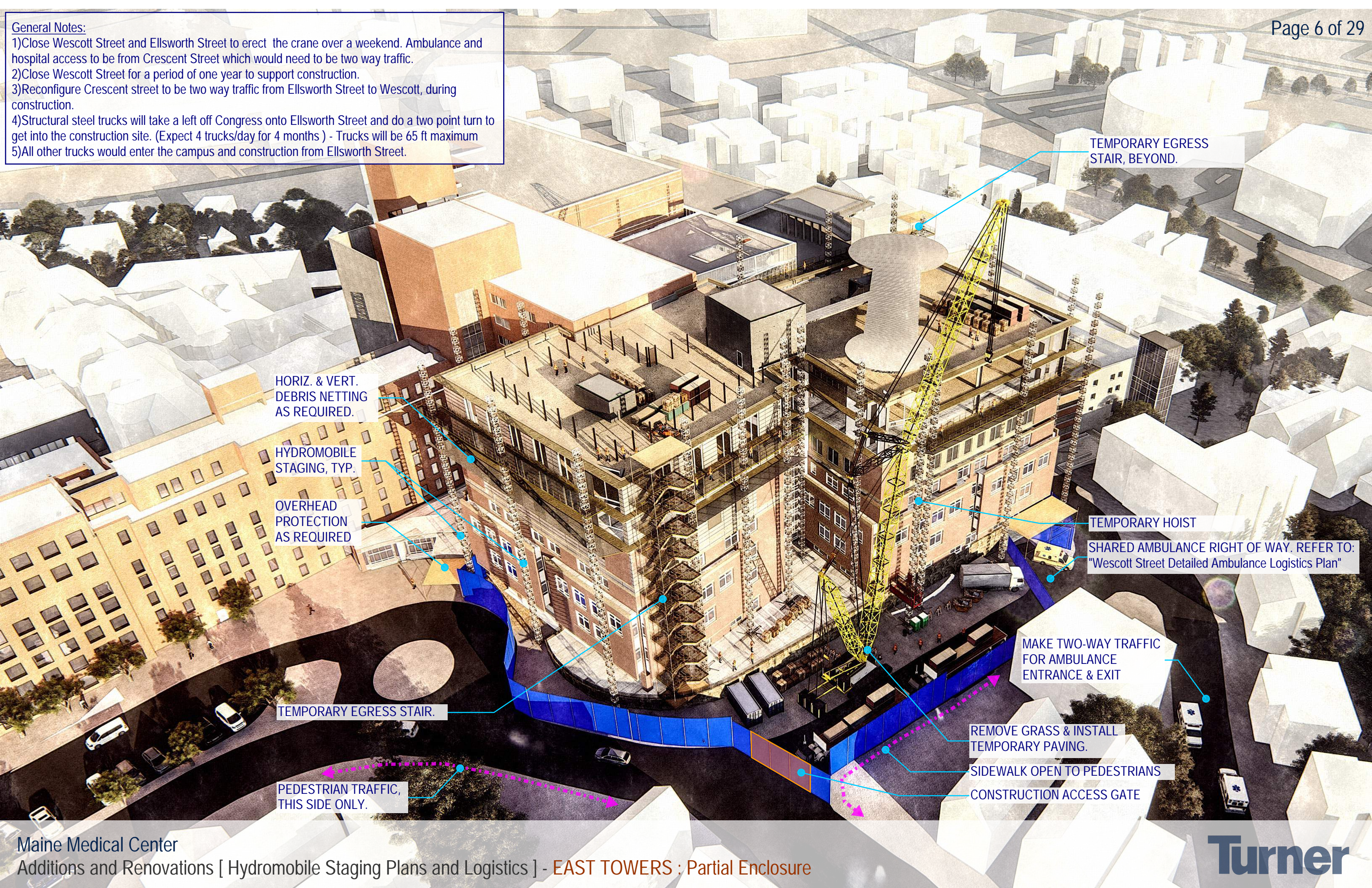
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OVERHEAD PROTECTION AS REQUIRED

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PEDESTRIAN TRAFFIC, THIS SIDE ONLY.

TEMPORARY EGRESS STAIR, BEYOND.

TEMPORARY HOIST

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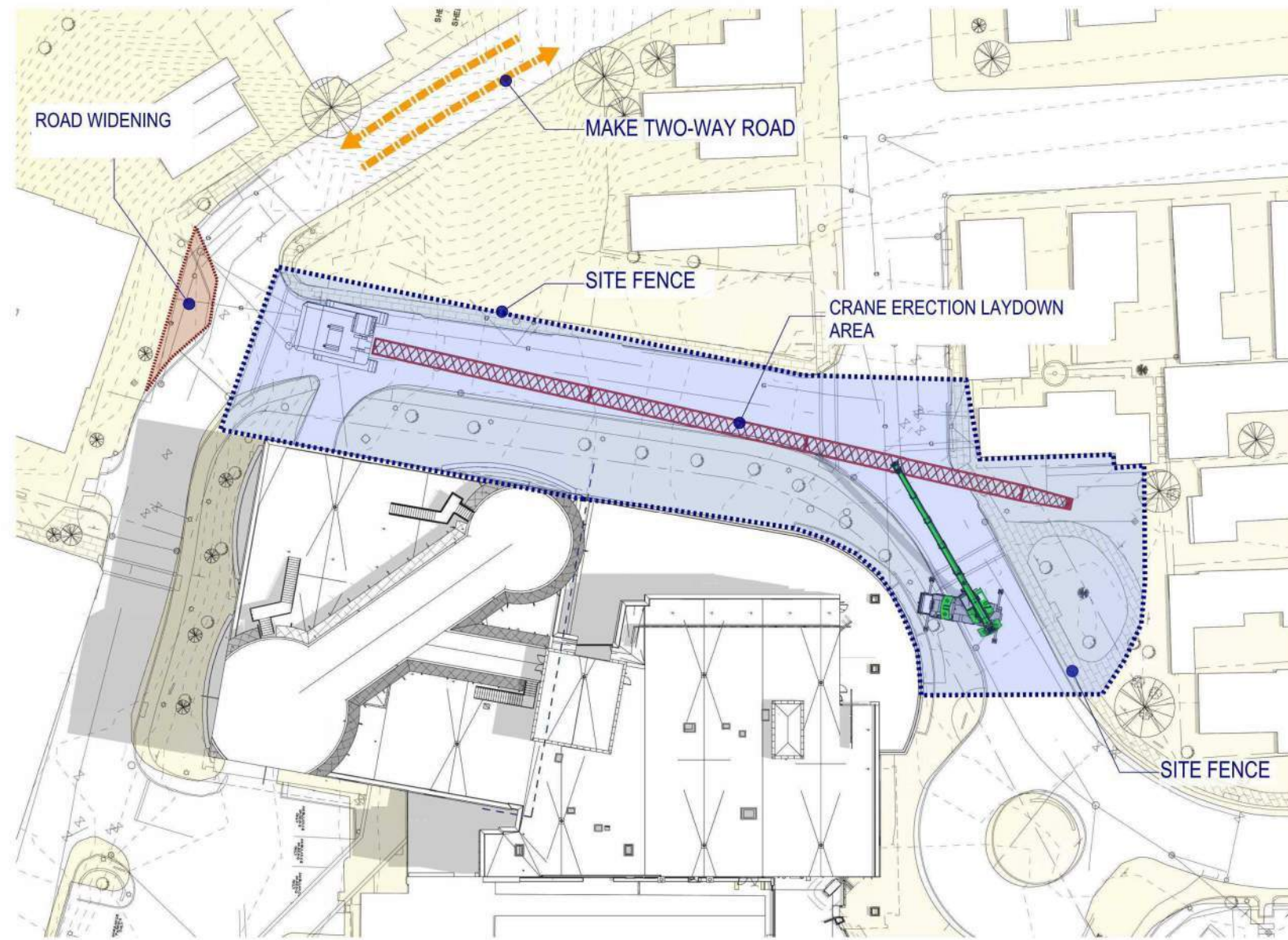
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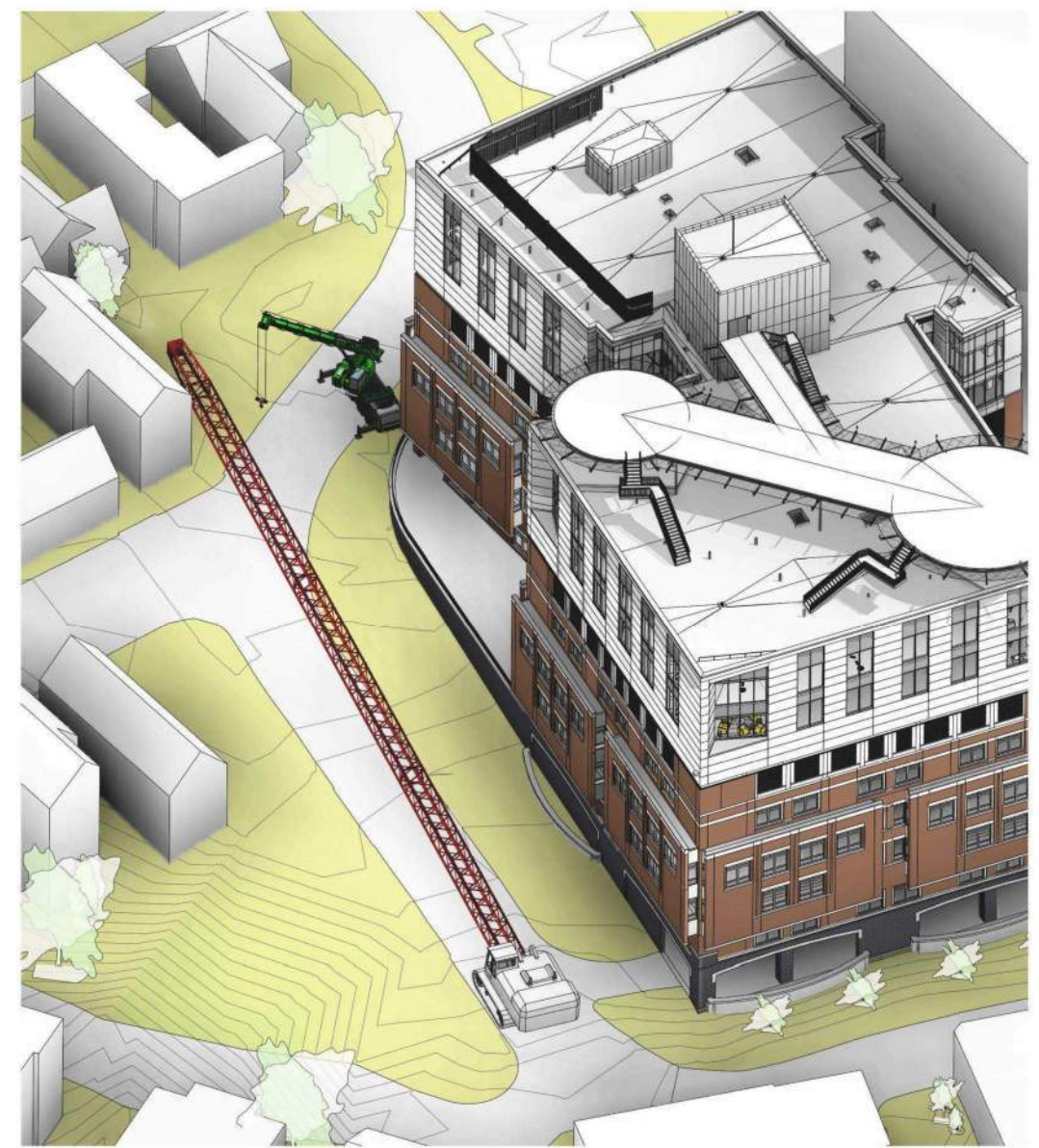
SIDEWALK OPEN TO PEDESTRIANS

CONSTRUCTION ACCESS GATE





1 EAST TOWERS - CRANE LOGISTICS  
3/64" = 1'-0"

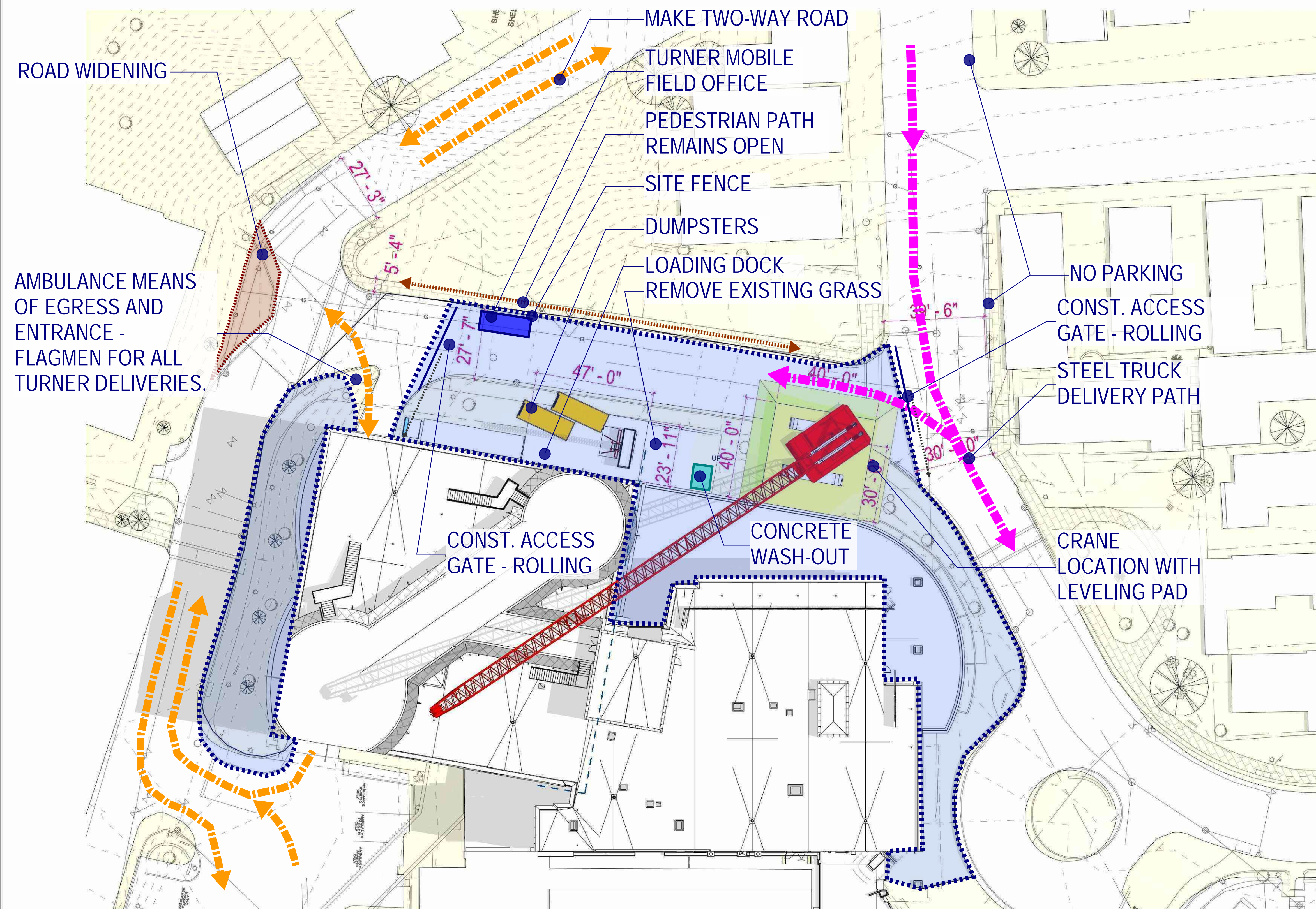


2 EAST TOWERS - CRANE LOGISTICS

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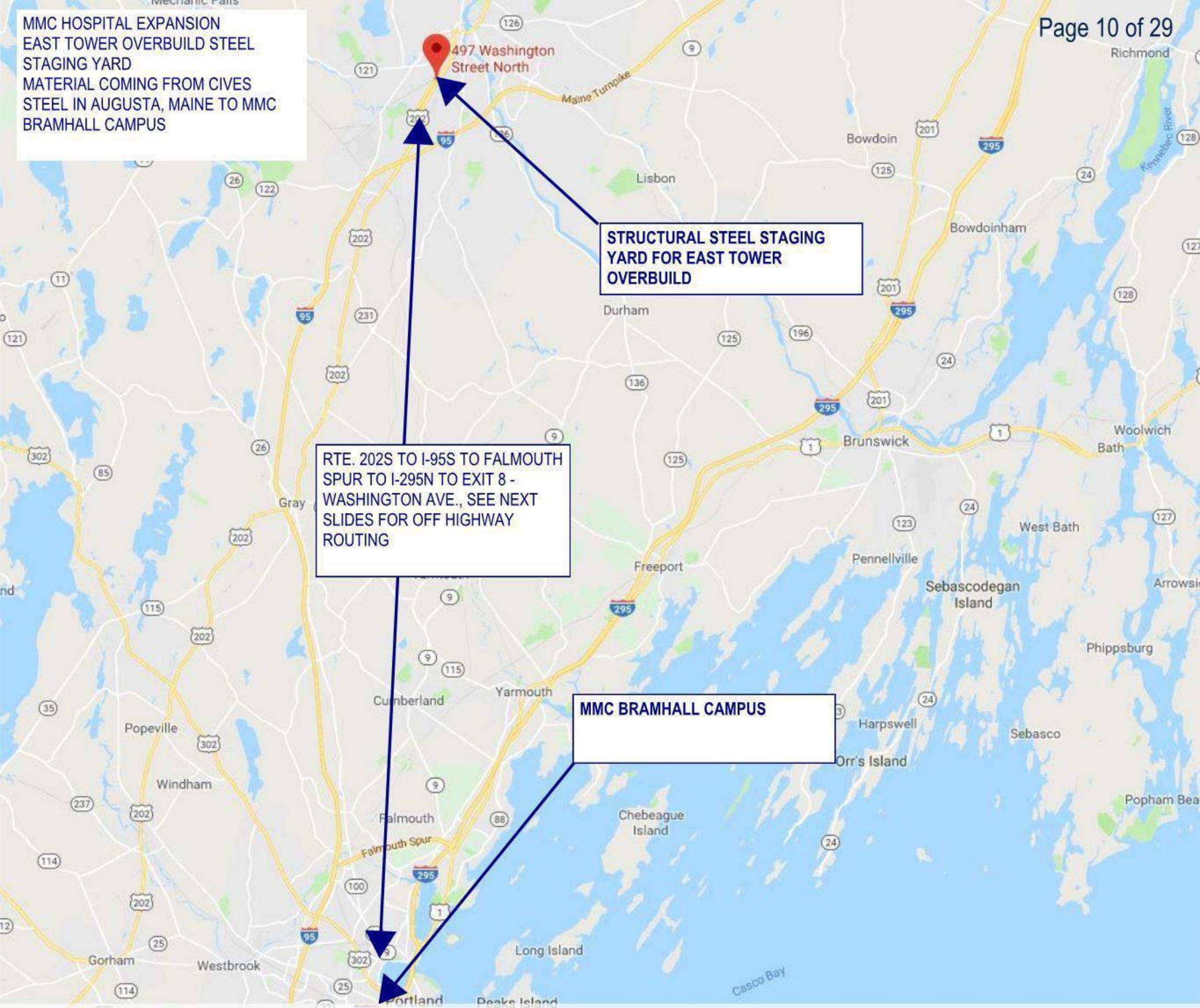
1 EAST TOWERS - CRANE LOGISTICS  
3/8" = 1'-0"

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MMC HOSPITAL EXPANSION  
EAST TOWER OVERBUILD STEEL  
STAGING YARD  
MATERIAL COMING FROM CIVES  
STEEL IN AUGUSTA, MAINE TO MMC  
BRAMHALL CAMPUS

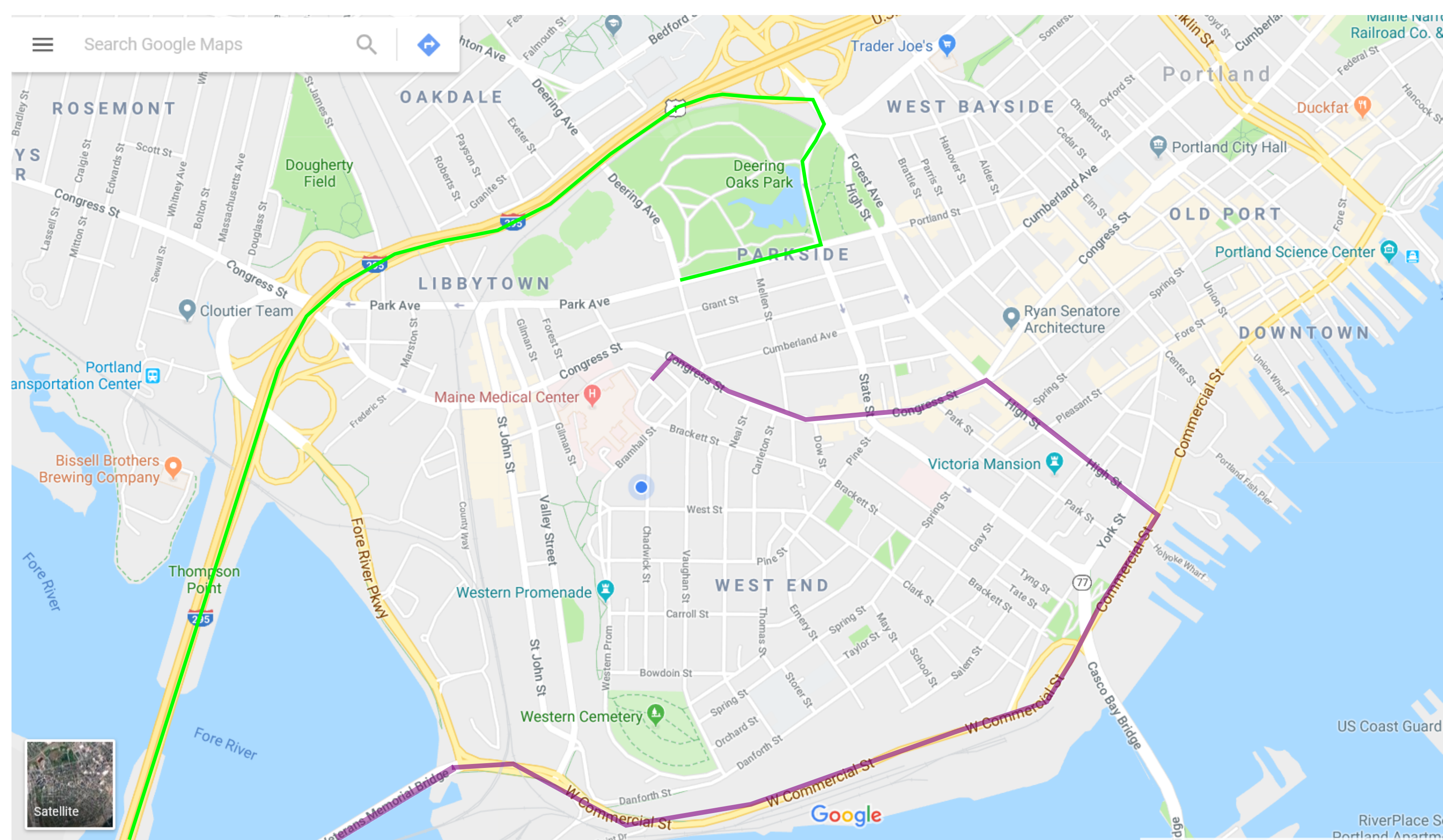


RTE. 202S TO I-95S TO FALMOUTH  
SPUR TO I-295N TO EXIT 8 -  
WASHINGTON AVE., SEE NEXT  
SLIDES FOR OFF HIGHWAY  
ROUTING

STRUCTURAL STEEL STAGING  
YARD FOR EAST TOWER  
OVERBUILD

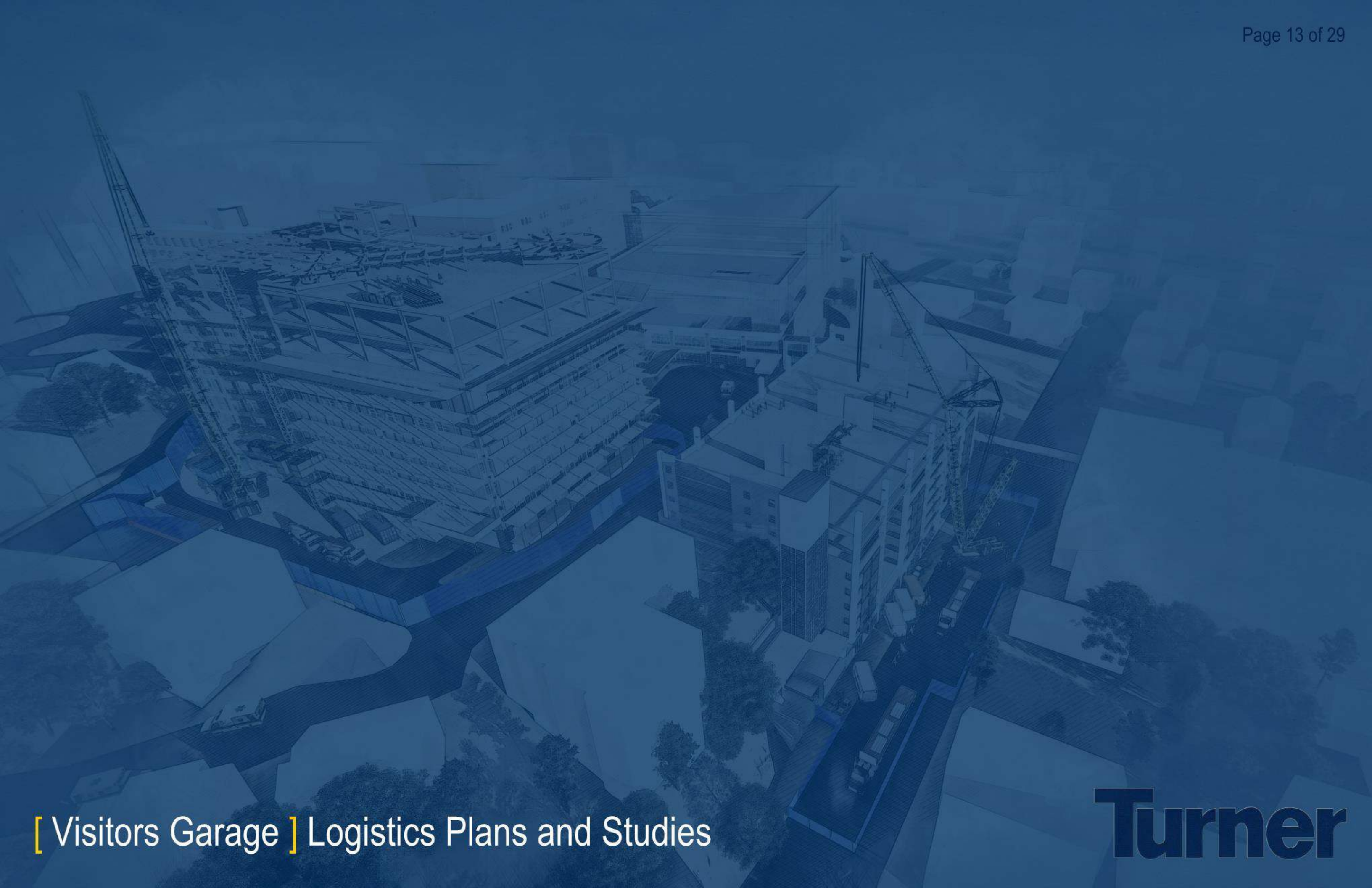
MMC BRAMHALL CAMPUS





STEEL DELIVERY ROUTE





[ Visitors Garage ] Logistics Plans and Studies





## General Notes:

**1)Congress Street has to be closed for a period of 8 weeks. )**

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- f. Visitor Garage west stair tower remains open during the eight weeks.

TOP TWO FLOORS TO BE  
TAKEN OUT OF SERVICE (TO  
BE DISCUSSED WITH MMC)  
MAY 2019



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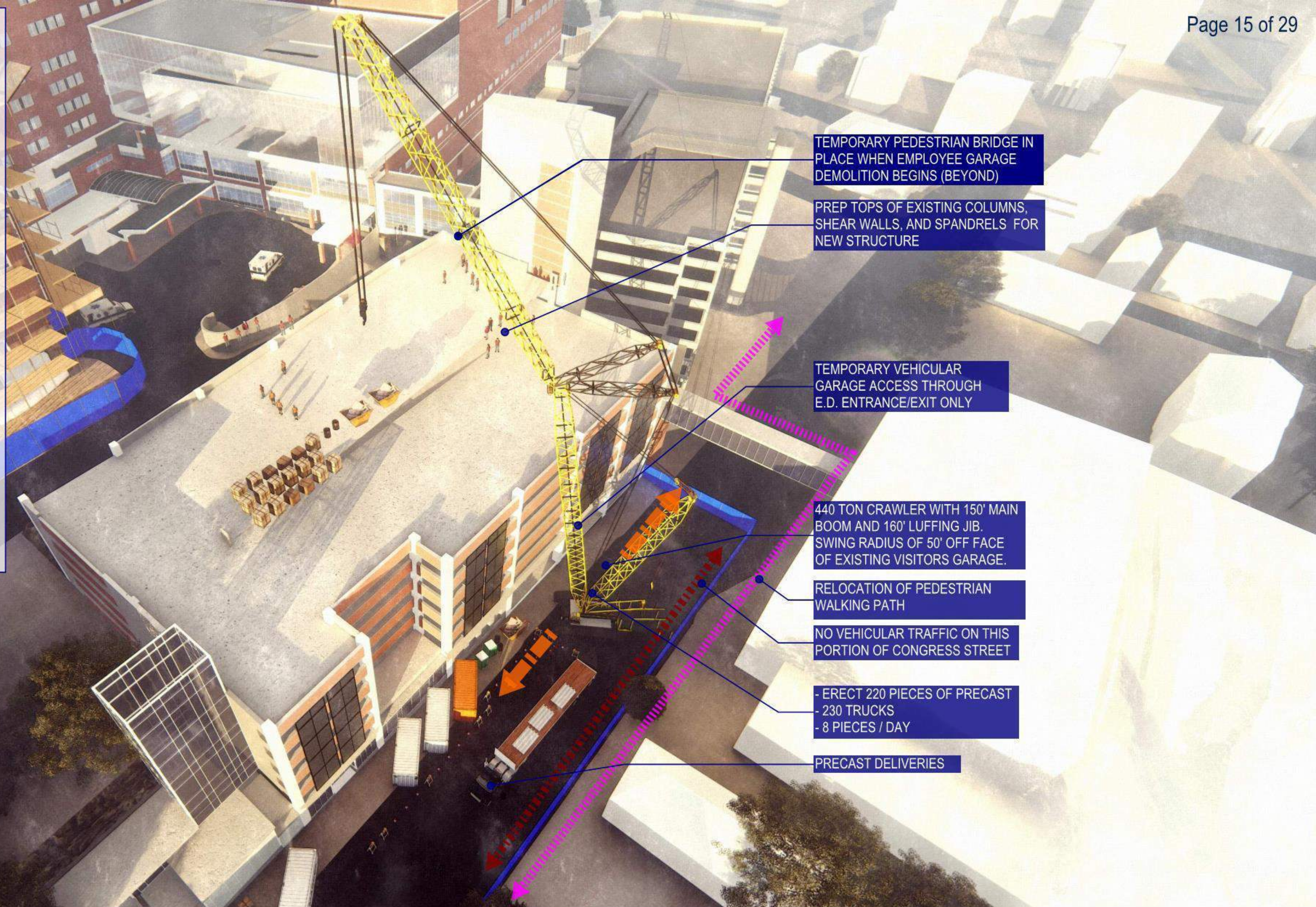
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TEMPORARY PEDESTRIAN BRIDGE IN PLACE WHEN EMPLOYEE GARAGE DEMOLITION BEGINS (BEYOND)

PREP TOPS OF EXISTING COLUMNS, SHEAR WALLS, AND SPANDRELS FOR NEW STRUCTURE

TEMPORARY VEHICULAR GARAGE ACCESS THROUGH E.D. ENTRANCE/EXIT ONLY

440 TON CRAWLER WITH 150' MAIN BOOM AND 160' LUFFING JIB. SWING RADIUS OF 50' OFF FACE OF EXISTING VISITORS GARAGE.

RELOCATION OF PEDESTRIAN WALKING PATH

NO VEHICULAR TRAFFIC ON THIS PORTION OF CONGRESS STREET

- ERECT 220 PIECES OF PRECAST  
- 230 TRUCKS  
- 8 PIECES / DAY

PRECAST DELIVERIES



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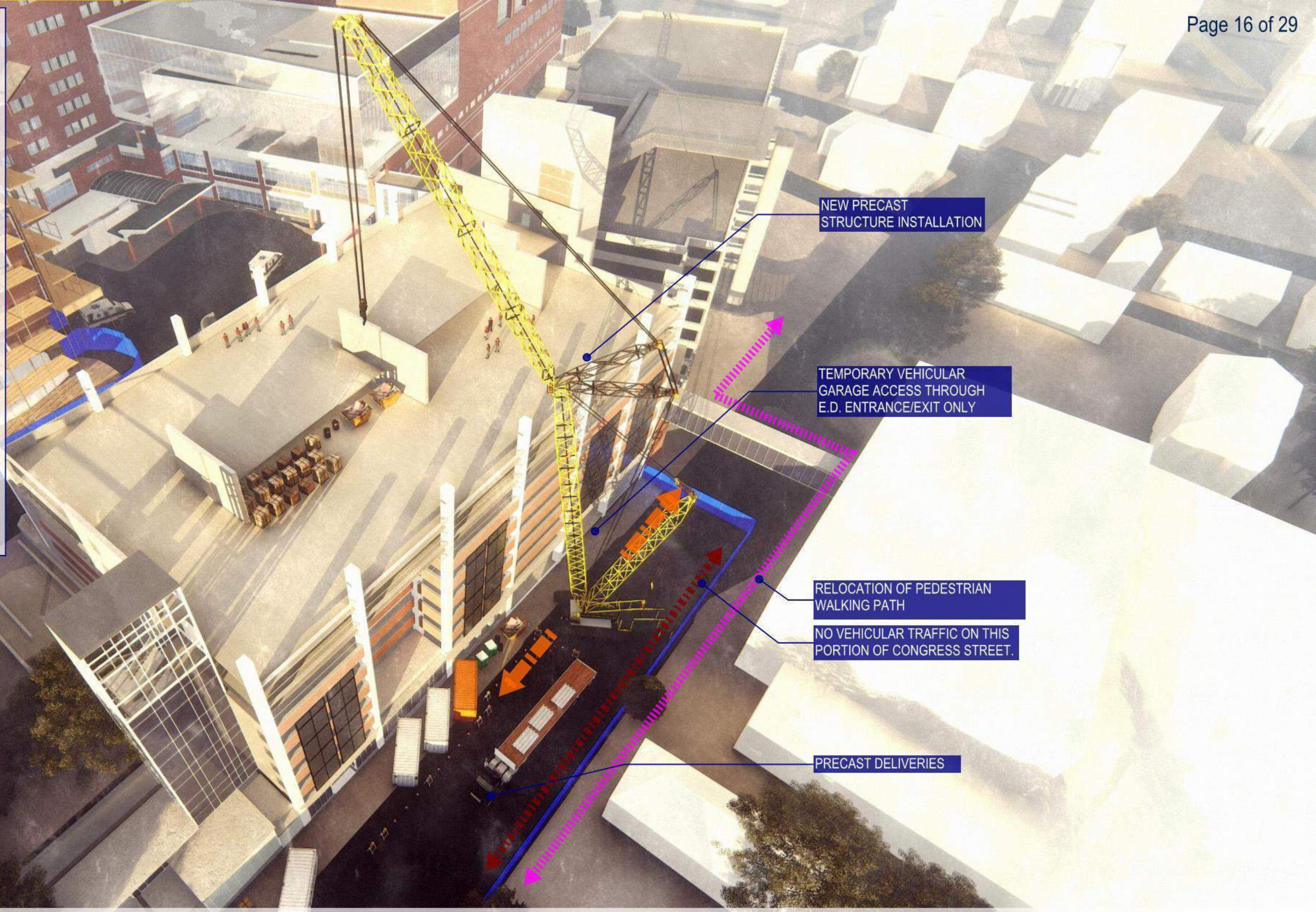
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NEW PRECAST STRUCTURE INSTALLATION

TEMPORARY VEHICULAR GARAGE ACCESS THROUGH E.D. ENTRANCE/EXIT ONLY

RELOCATION OF PEDESTRIAN WALKING PATH

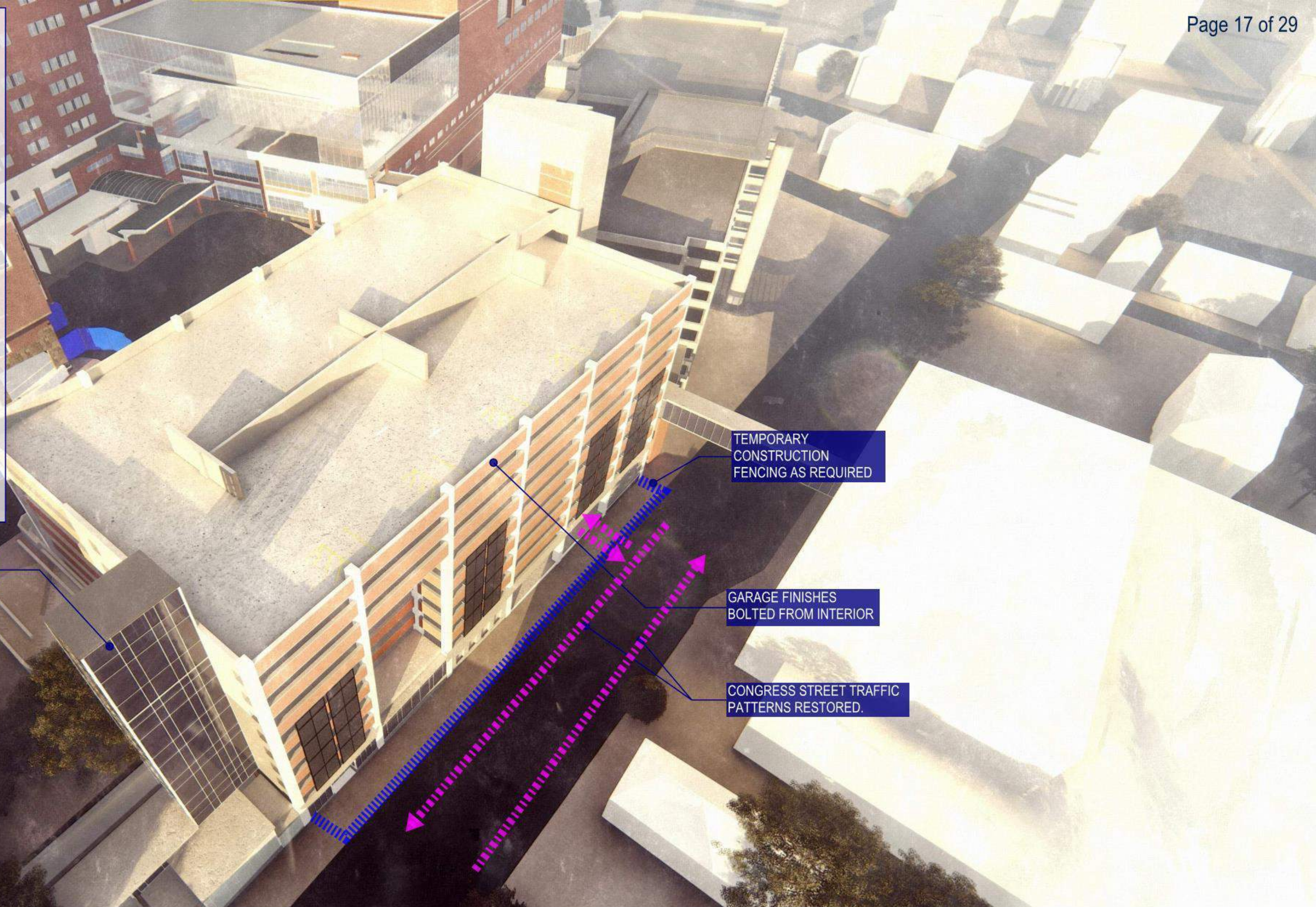
NO VEHICULAR TRAFFIC ON THIS PORTION OF CONGRESS STREET.

PRECAST DELIVERIES



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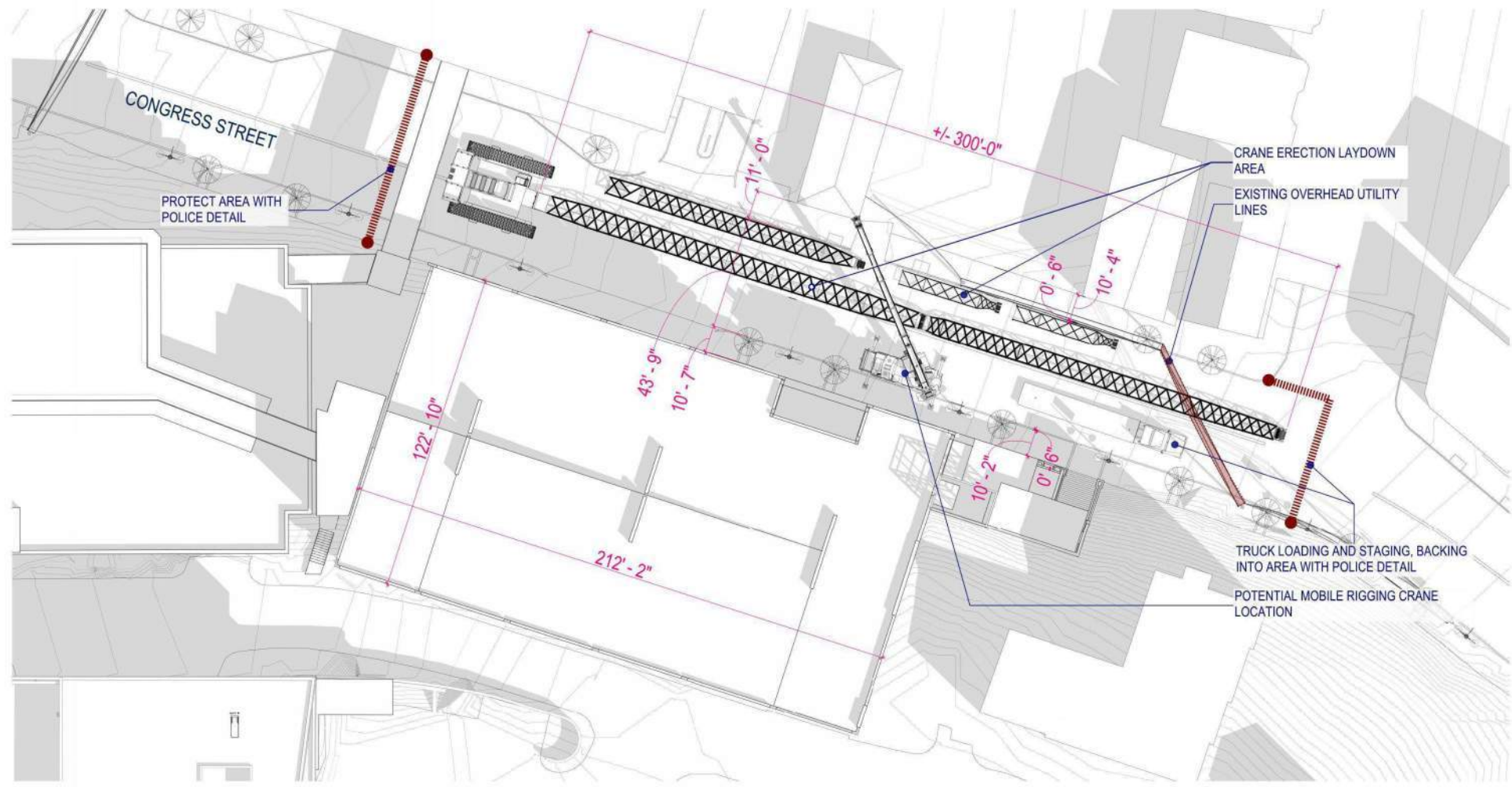
NEW STAIR TOWER EXTENSION

TEMPORARY CONSTRUCTION FENCING AS REQUIRED

GARAGE FINISHES BOLTED FROM INTERIOR

CONGRESS STREET TRAFFIC PATTERNS RESTORED.



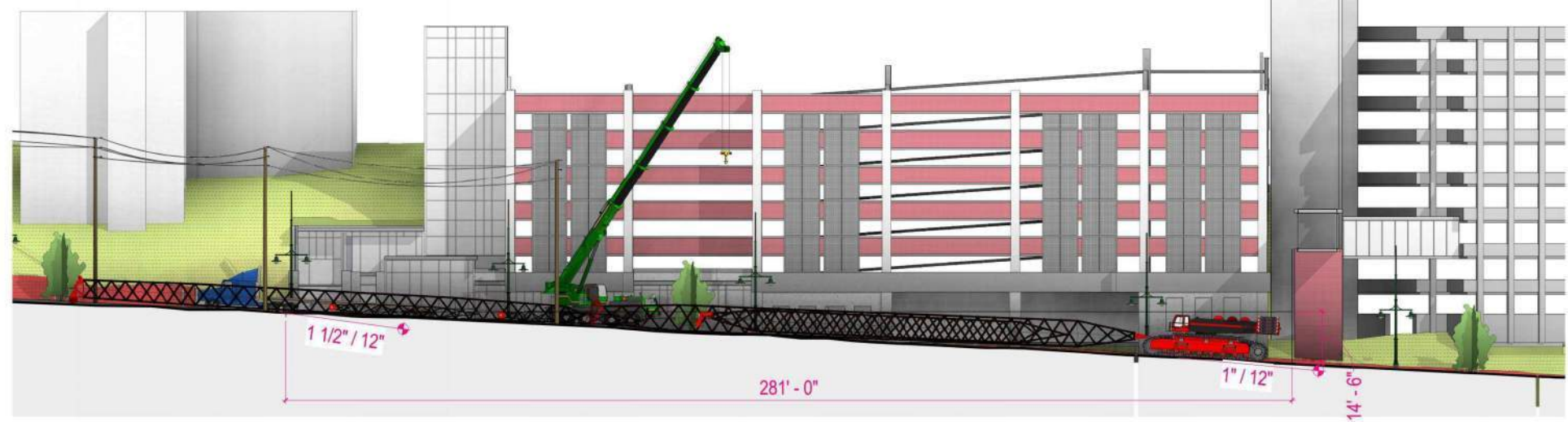


1 CONGRESS STREET - CRANE PICK PLAN  
3/64" = 1'-0"

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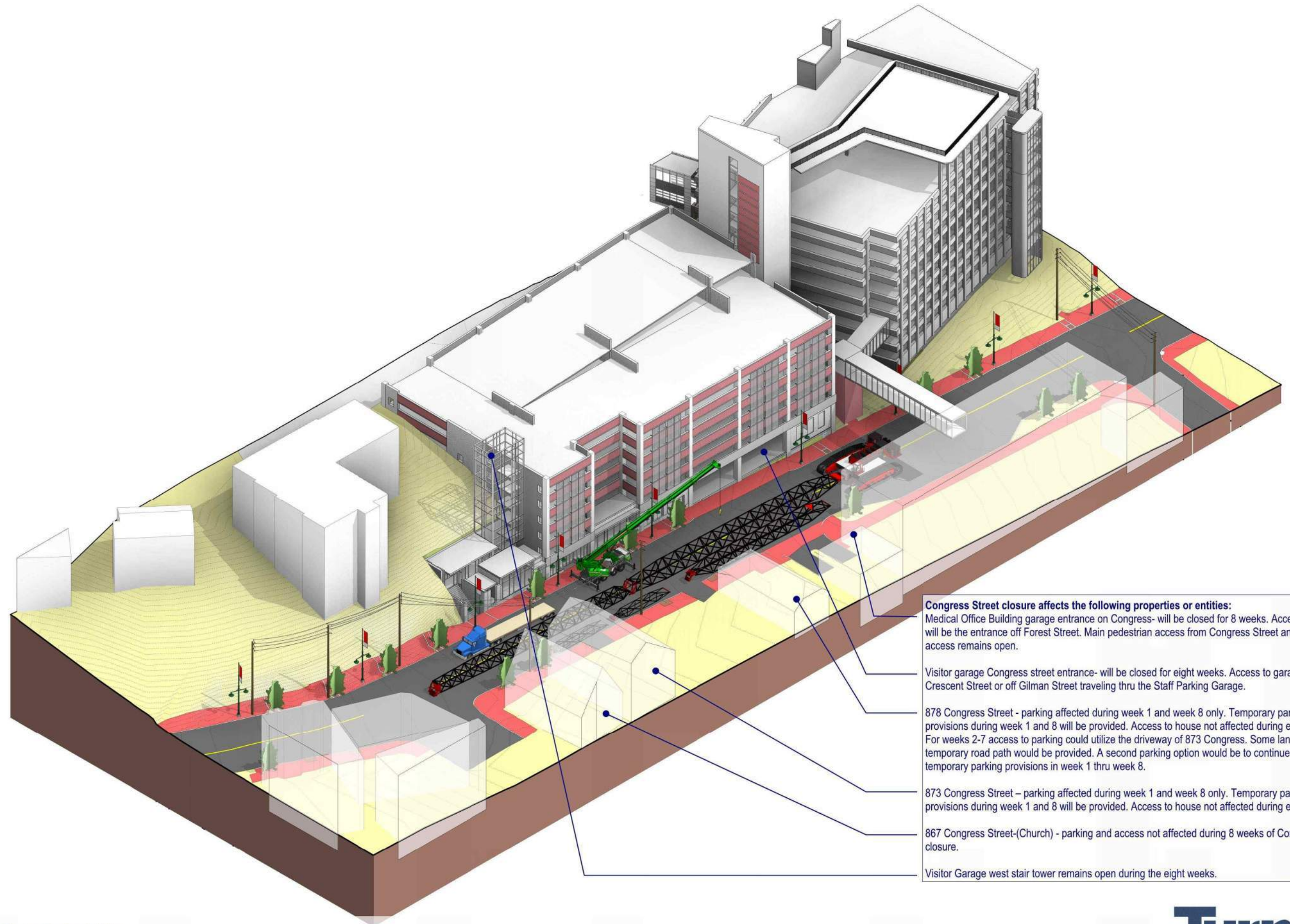


2 CONGRESS STREET - SITE SECTION  
1/16" = 1'-0"



3 CONGRESS STREET - AXON VIEW

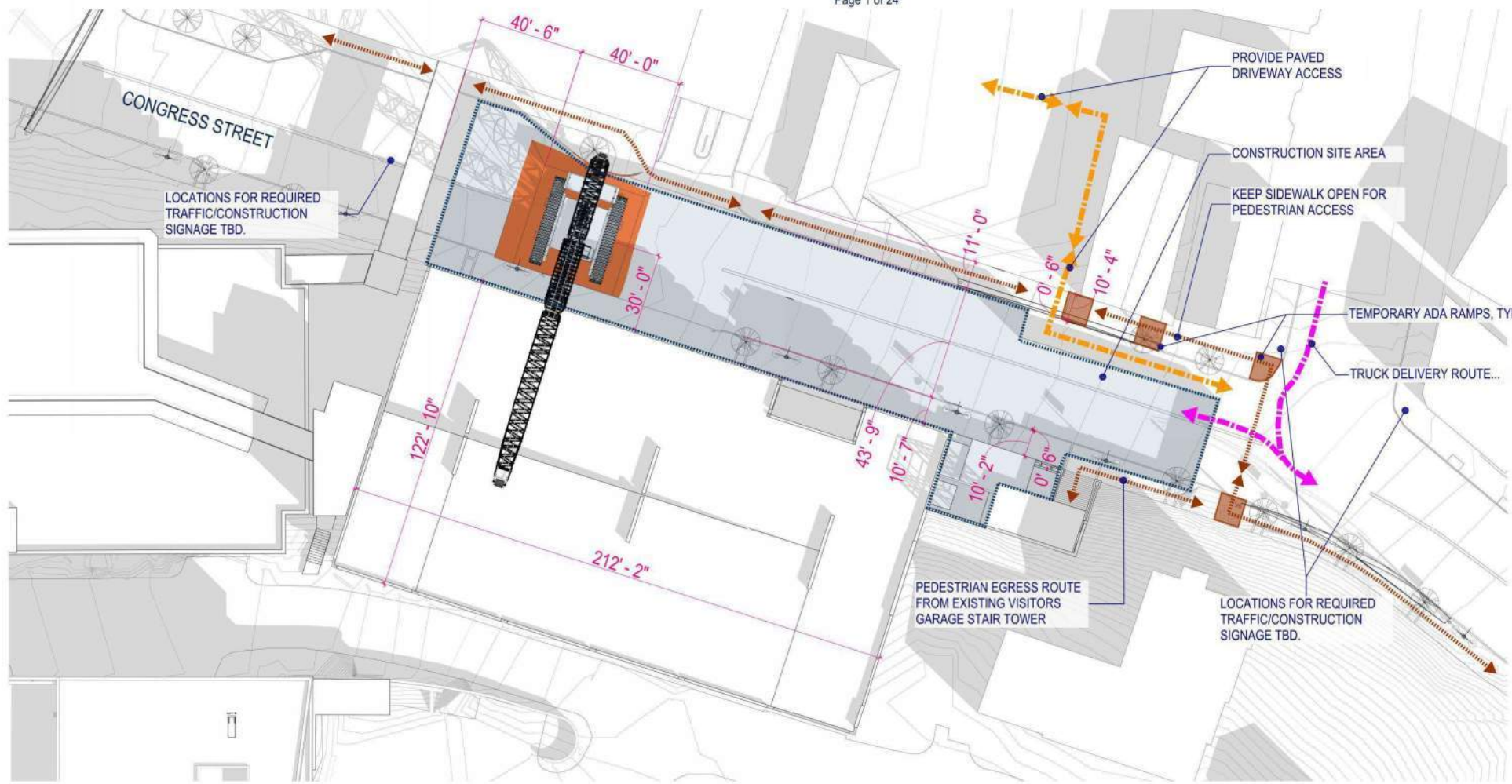




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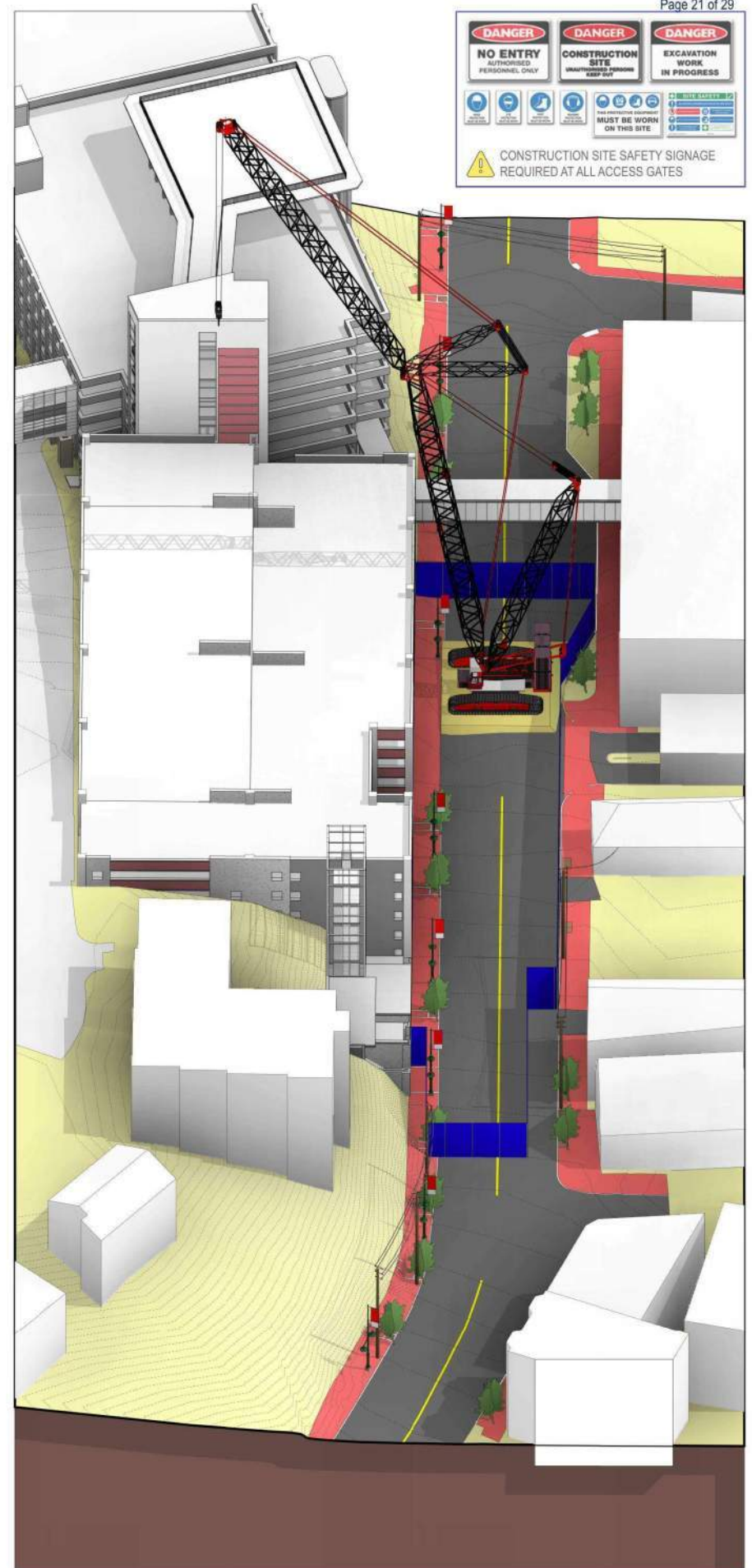
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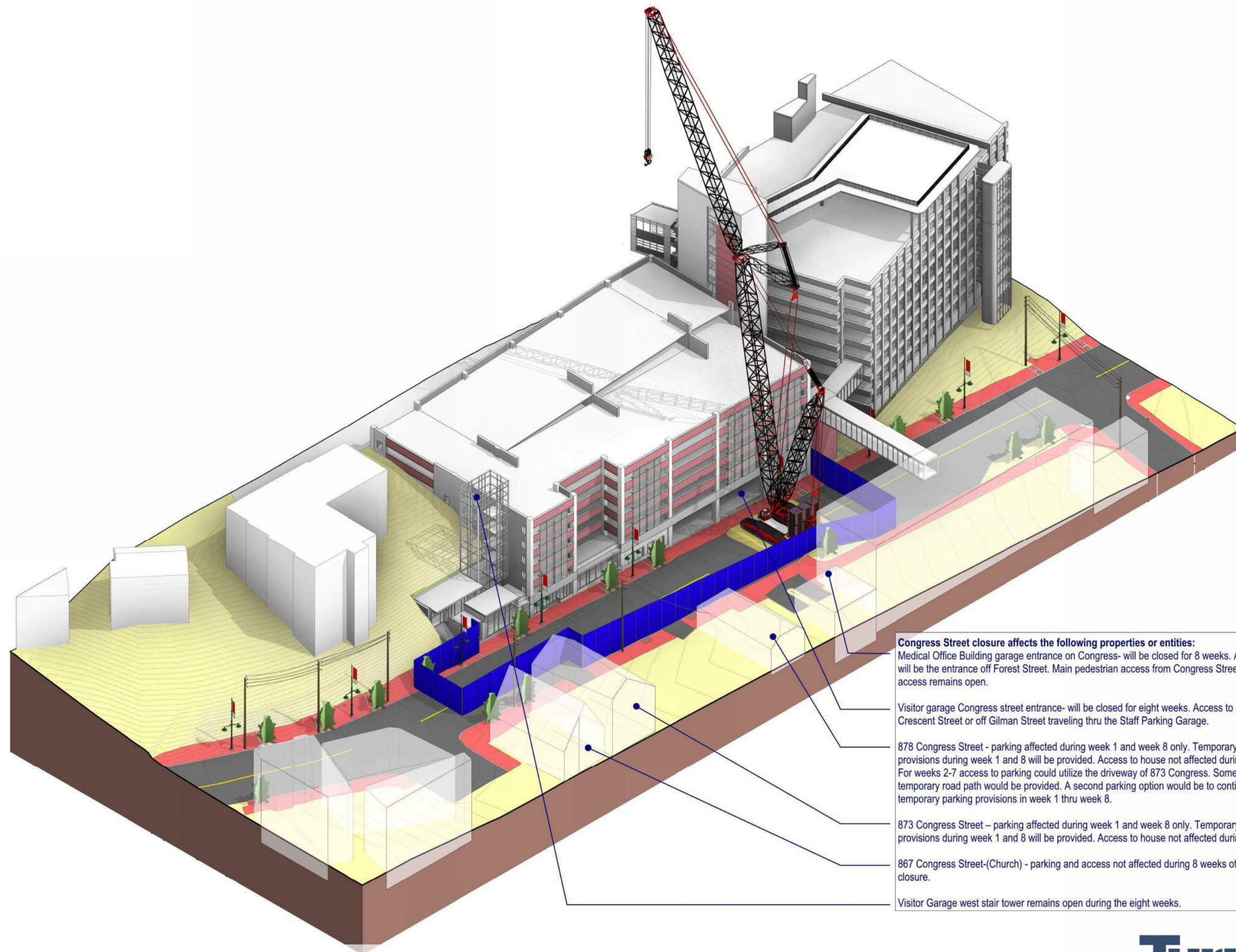


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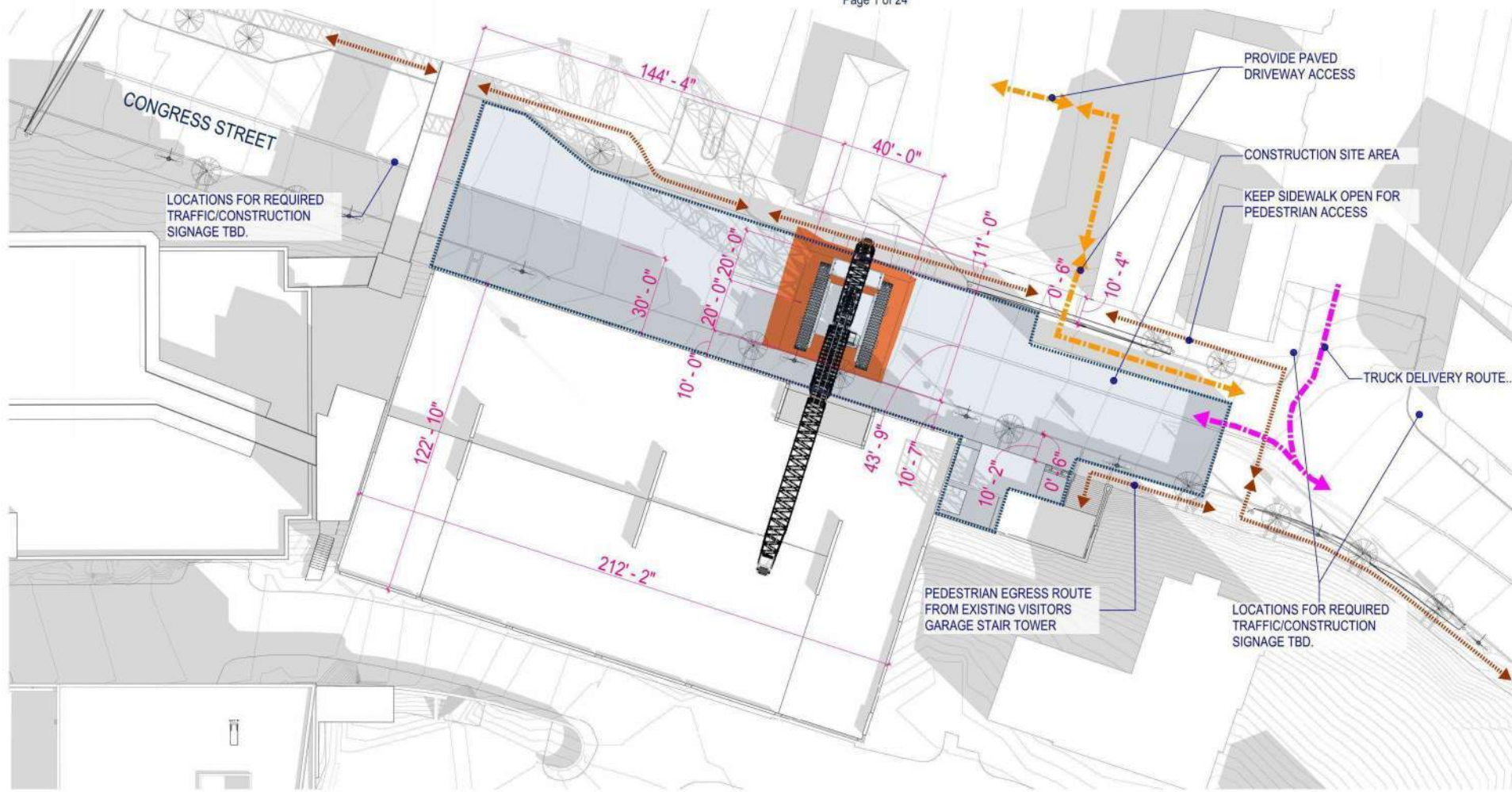




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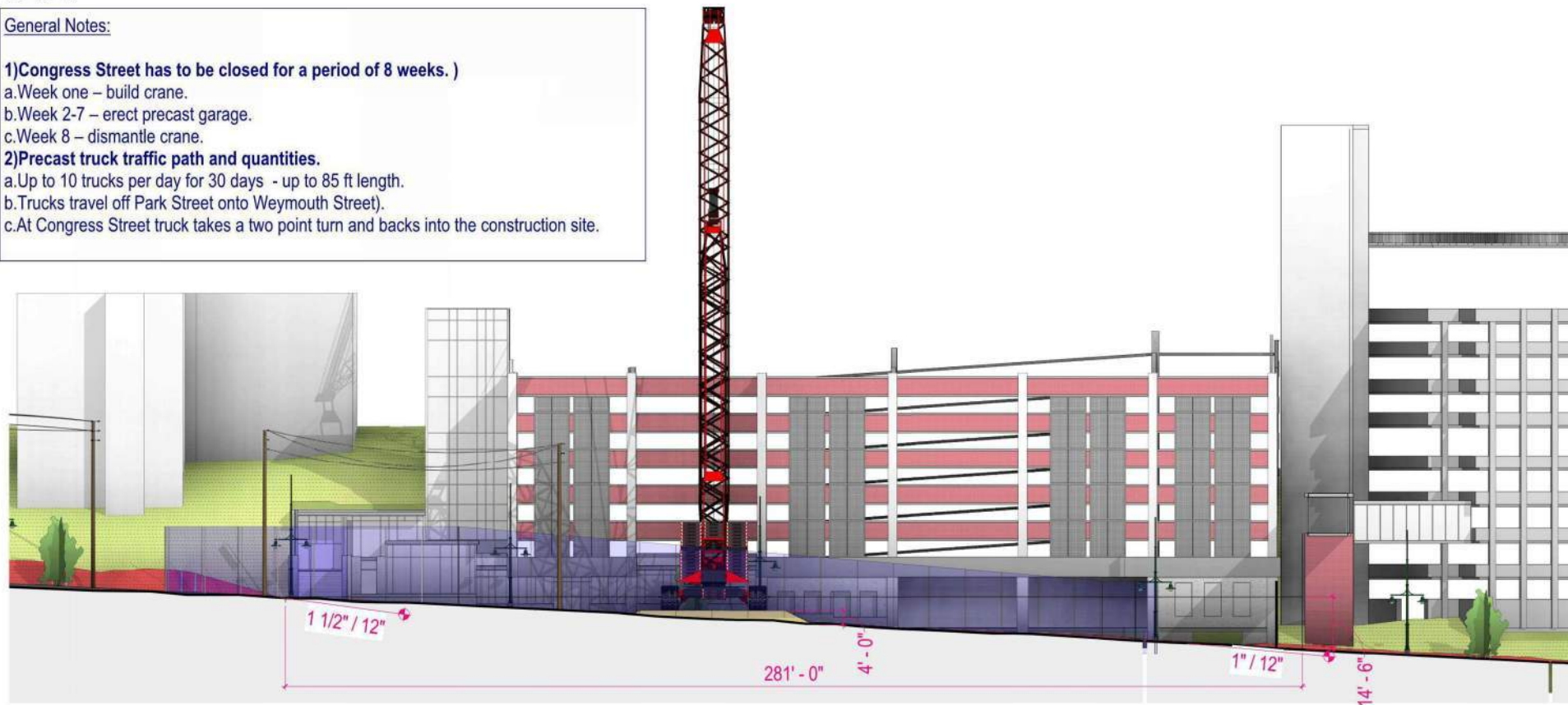


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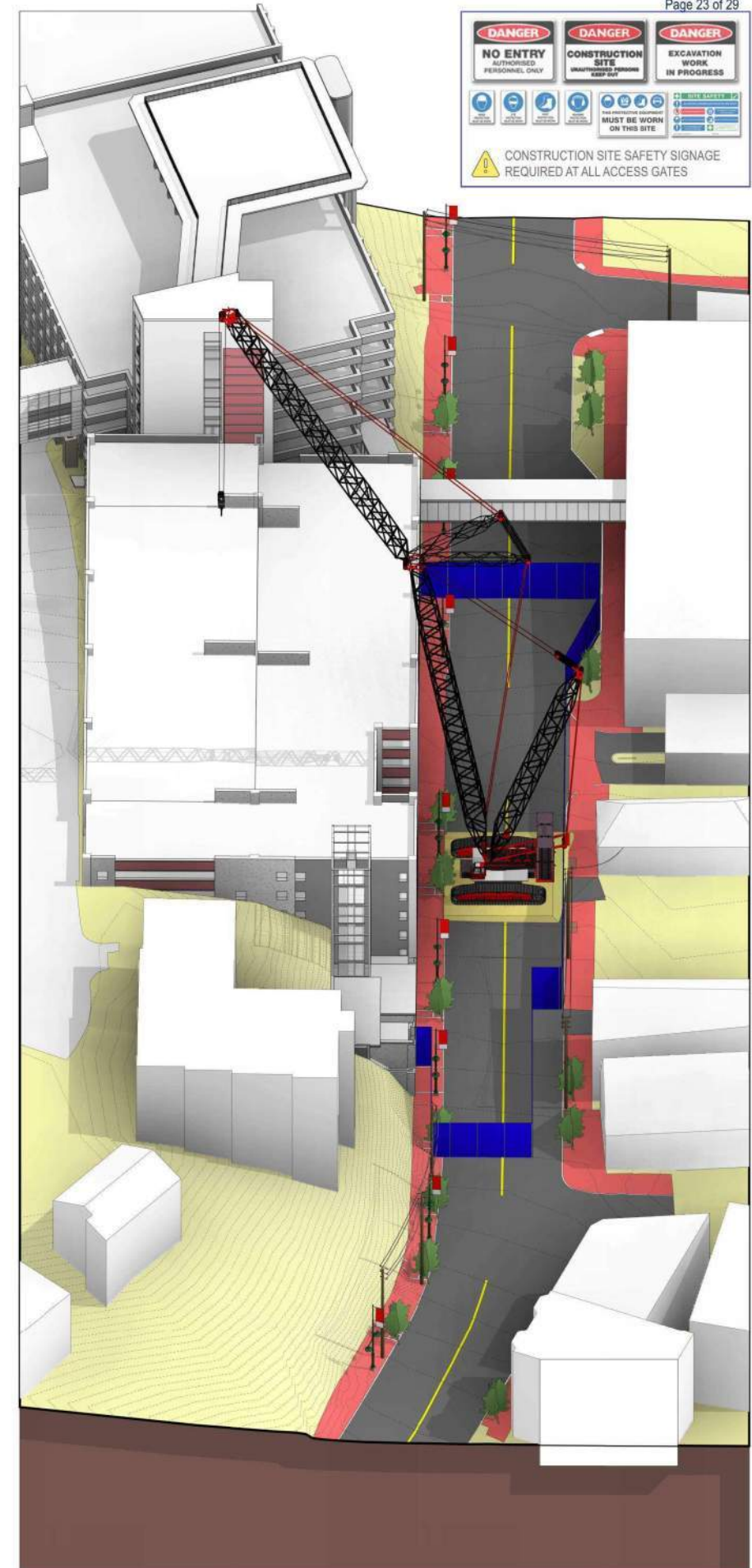
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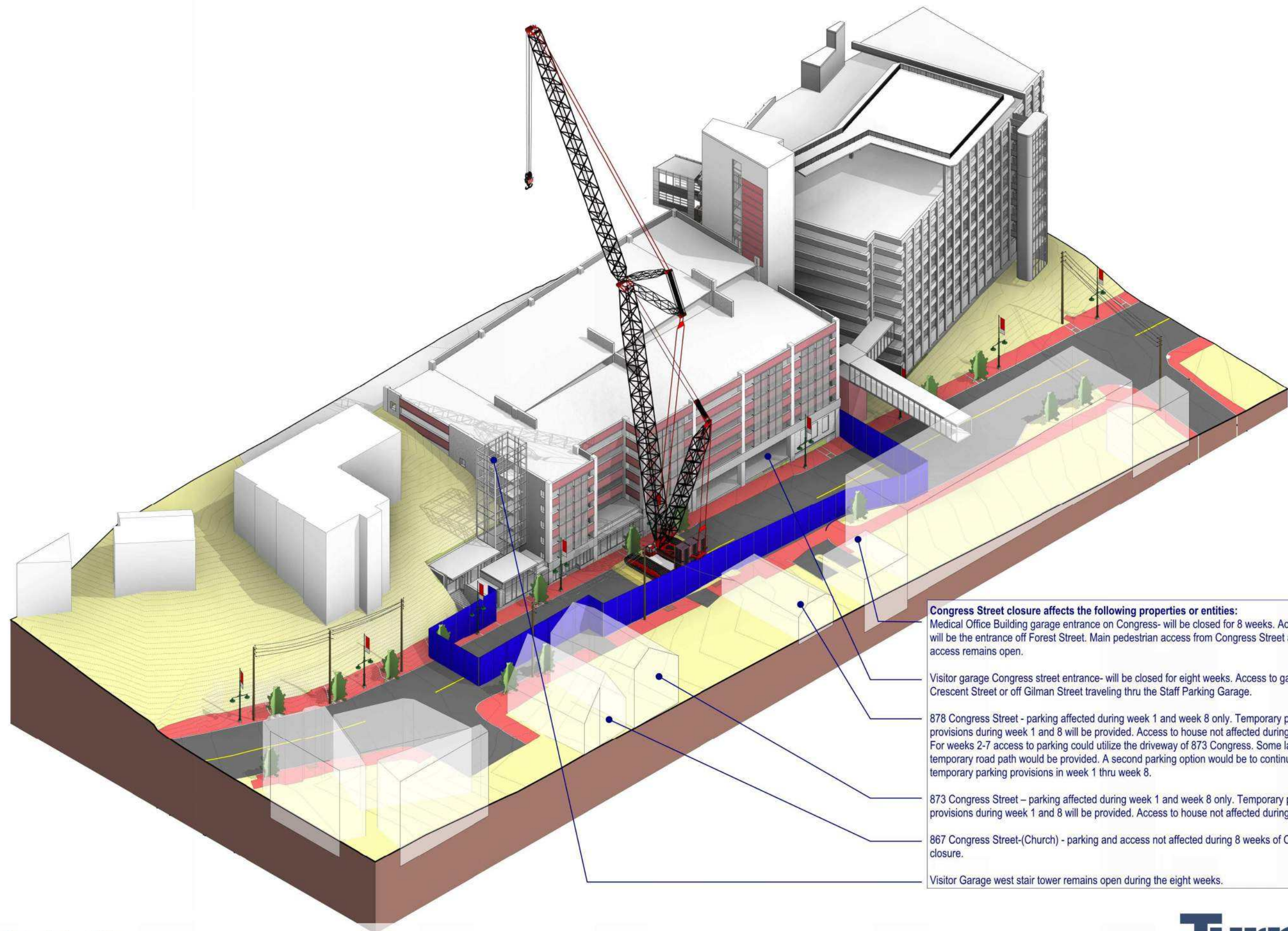


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887 Delivery / Drop off area



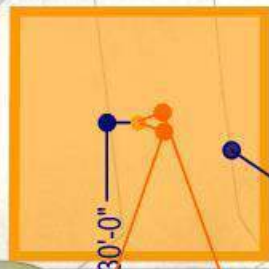
Do Not Block Loading Area Signage

NO VEHICULAR TRAFFIC ON THIS PORTION OF CONGRESS STREET FOR 8 WEEKS.

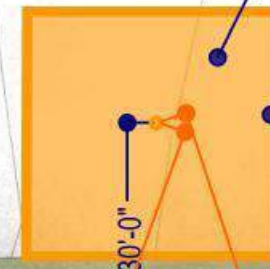
RELOCATION OF PEDESTRIAN WALKING PATH

440 TON CRAWLER WITH 150' MAIN BOOM AND 160' LUFFING JIB. SWING RADIUS OF 50' OFF FACE OF EXISTING VISITORS GARAGE.

EXISTING OVERHEAD POWER LOCATION



CRAWLER CRANE LOCATION 1



CRAWLER CRANE LOCATION 2

TEMPORARY VEHICULAR GARAGE ACCESS THROUGH E.D. ENTRANCE/EXIT ONLY

887 Drop Off

Truck and Delivery turn around

Truck Turn Around Signage

121'-0"

160'-0"

160'-11"

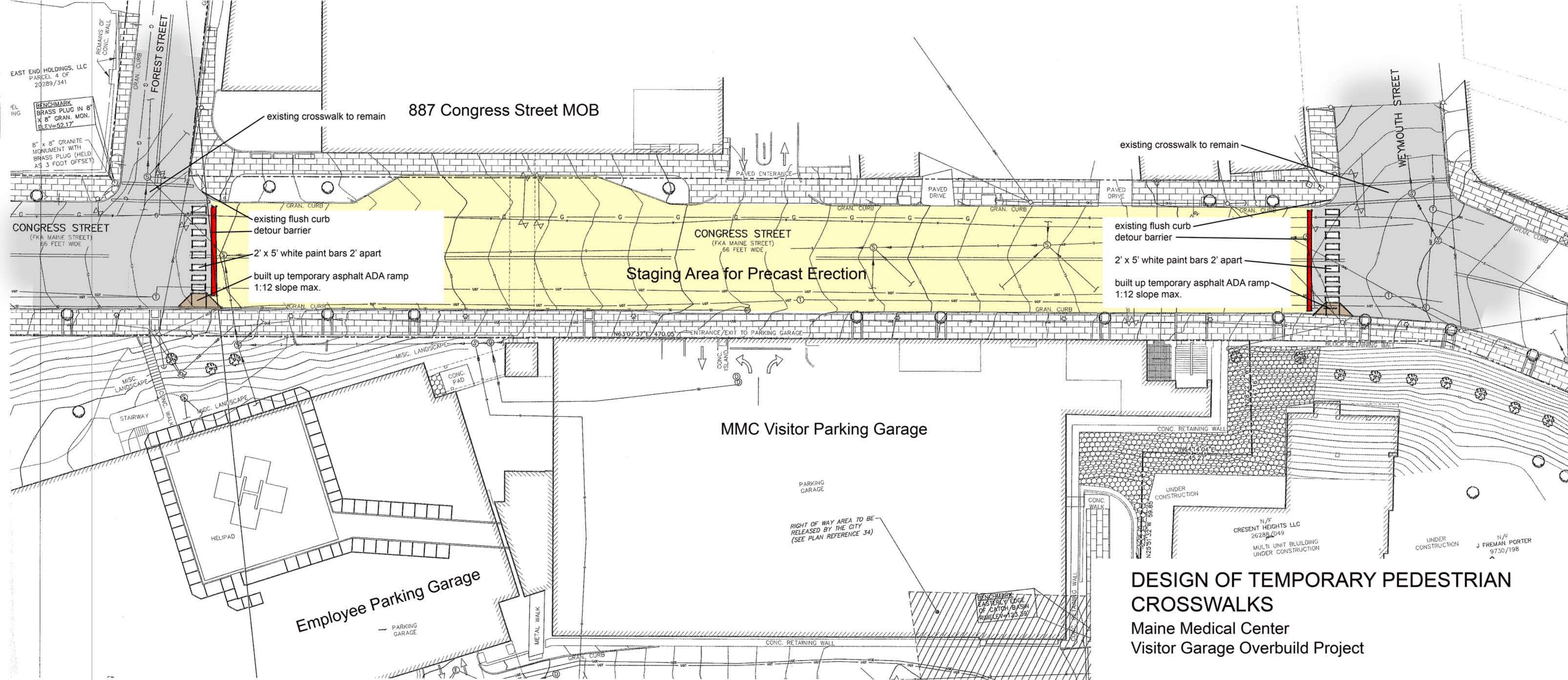
159'-10"

160'-0"

210'-2"

REFER TO "CONGRESS STREET CRANE LOCATION" DRAWINGS FOR MORE INFORMATION AND DETAILS.





887 Congress Street MOB

Staging Area for Precast Erection

MMC Visitor Parking Garage

Employee Parking Garage

**DESIGN OF TEMPORARY PEDESTRIAN CROSSWALKS**  
 Maine Medical Center  
 Visitor Garage Overbuild Project

EAST END HOLDINGS, LLC  
 PARCEL 4 OF  
 20289/341

BENCHMARK  
 BRASS PLUG IN 8"  
 X 8" GRAN. MON.  
 ELEV=52.17'

8" x 8" GRANITE  
 MONUMENT WITH  
 BRASS PLUG (HELD  
 AS 3 FOOT OFFSET)

CONGRESS STREET  
 (FKA MAINE STREET)  
 66 FEET WIDE

STAIRWAY

MISC. LANDSCAPE

MISC. LANDSCAPE

MISC. LANDSCAPE

HELIPAD

METAL WALK

existing crosswalk to remain

existing flush curb  
 detour barrier

2' x 5' white paint bars 2' apart

built up temporary asphalt ADA ramp  
 1:12 slope max.

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existing flush curb  
 detour barrier

2' x 5' white paint bars 2' apart

built up temporary asphalt ADA ramp  
 1:12 slope max.

RIGHT OF WAY AREA TO BE  
 RELEASED BY THE CITY  
 (SEE PLAN REFERENCE 34)

BENCHMARK  
 EASTERLY EDGE  
 OF CATCH BASIN  
 RMC ELEV=123.39'

N/F  
 CRESENT HEIGHTS LLC  
 26288/049  
 MULTI UNIT BUILDING  
 UNDER CONSTRUCTION

UNDER  
 CONSTRUCTION  
 N/F  
 J FREMAN PORTER  
 9730/198

ENTRANCE/EXIT TO PARKING GARAGE

PARKING GARAGE

CONC. RETAINING WALL

CONC. RETAINING WALL

UNDER CONSTRUCTION

CONC. WALK

CONC. RETAINING WALL

BLOCK RETAINING WALL

CONC. RETAINING WALL

UNDER CONSTRUCTION

CONC. WALK

CONC. RETAINING WALL

PAVED ENTERANCE

PAVED DRIVE

PAVED DRIVE

WEYMOUTH STREET

FOREST STREET

GRAN. CURB

GRAN. CURB

GRAN. CURB

GRAN. CURB

GRAN. CURB

GRAN. CURB

GRAN. CURB

GRAN. CURB

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MISC. LANDSCAPE

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MISC. LANDSCAPE

MISC. LANDSCAPE

CONC. PAD

CONC. ISLAND

CONC. ISLAND

CONC. ISLAND

CONC. ISLAND

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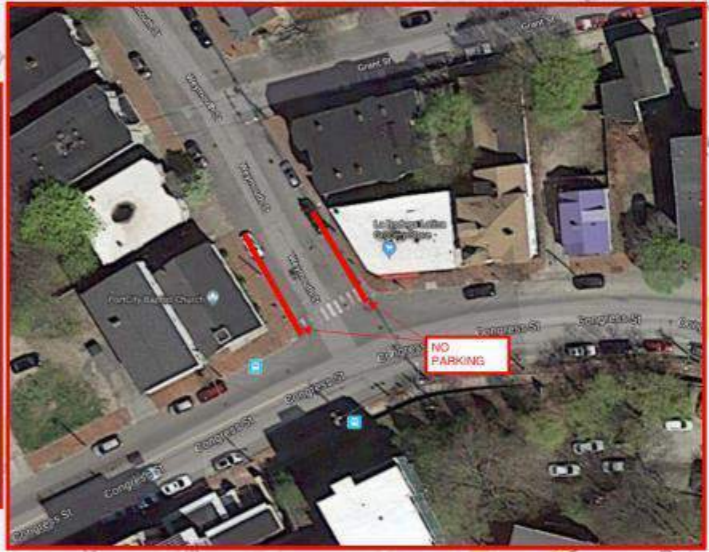
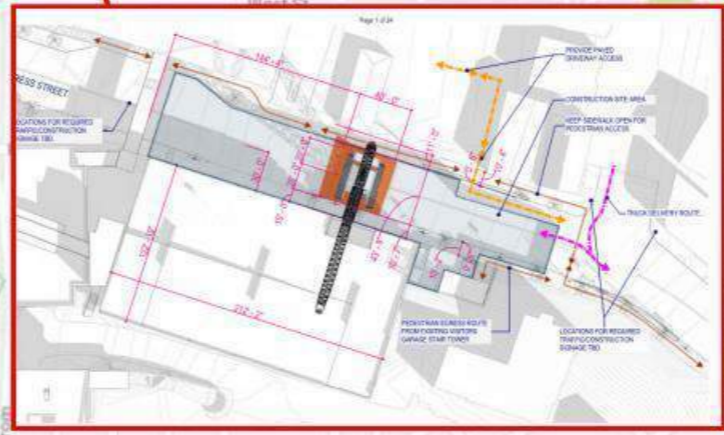
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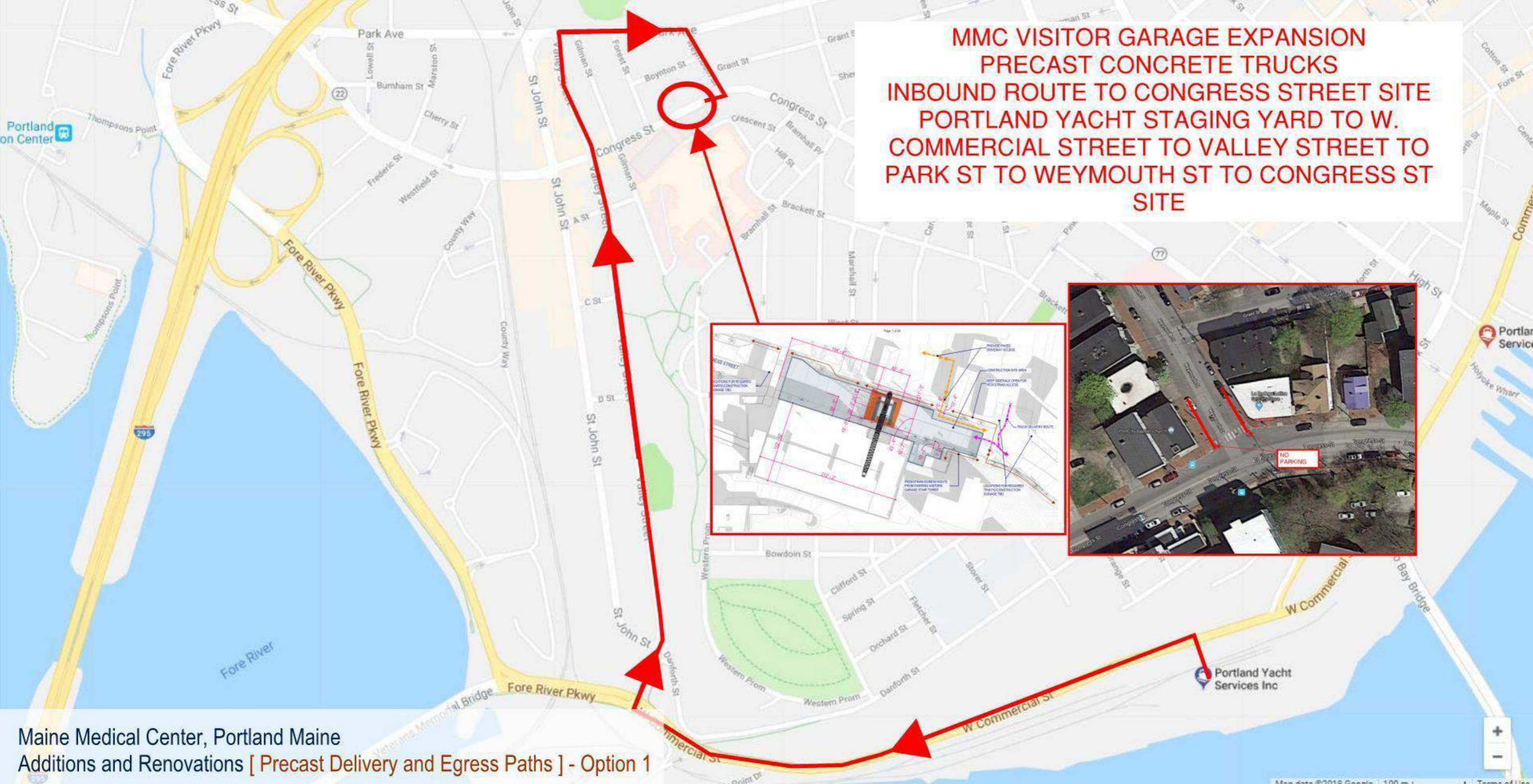
CONC. ISLAND



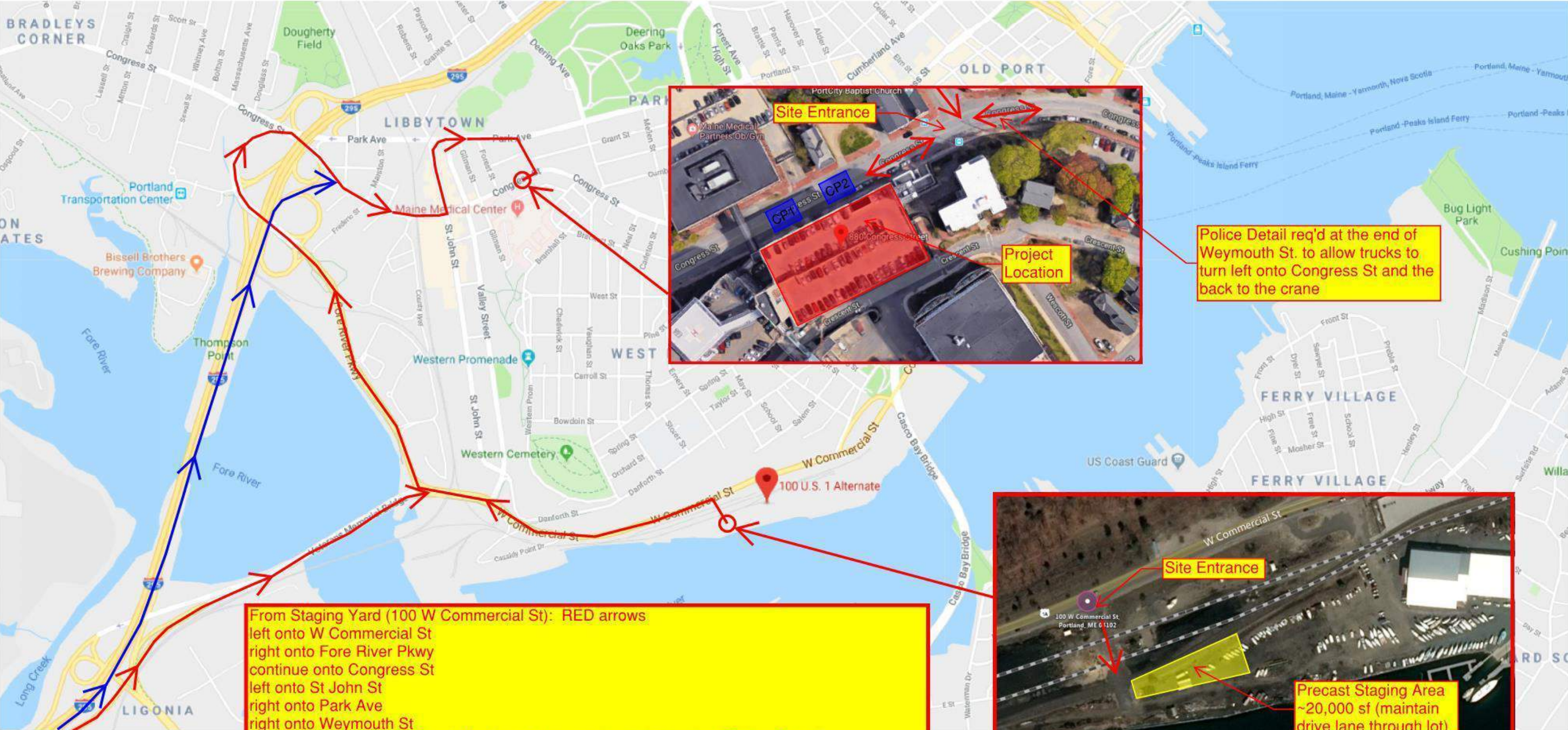
**MMC VISITOR GARAGE EXPANSION  
PRECAST CONCRETE TRUCKS  
INBOUND ROUTE TO CONGRESS STREET SITE  
PORTLAND YACHT STAGING YARD TO W.  
COMMERCIAL STREET TO VALLEY STREET TO  
PARK ST TO WEYMOUTH ST TO CONGRESS ST  
SITE**



Maine Medical Center, Portland Maine  
Additions and Renovations [ Precast Delivery and Egress Paths ] - Option 1







From Staging Yard (100 W Commercial St): RED arrows  
left onto W Commercial St  
right onto Fore River Pkwy  
continue onto Congress St  
left onto St John St  
right onto Park Ave  
right onto Weymouth St  
turn left onto Congress St and then back into project site limits (need Police Detail)

Direct to crane: BLUE arrows  
from I-295 N  
take exit 5 to Congress St  
follow same directions as noted above

Oversize load travel restrictions on I-95: No travel 7am to 9am and 3pm to 5pm

Staging for 15-20 loads to be at staging yard.

Police Detail req'd at the end of Weymouth St. to allow trucks to turn left onto Congress St and the back to the crane



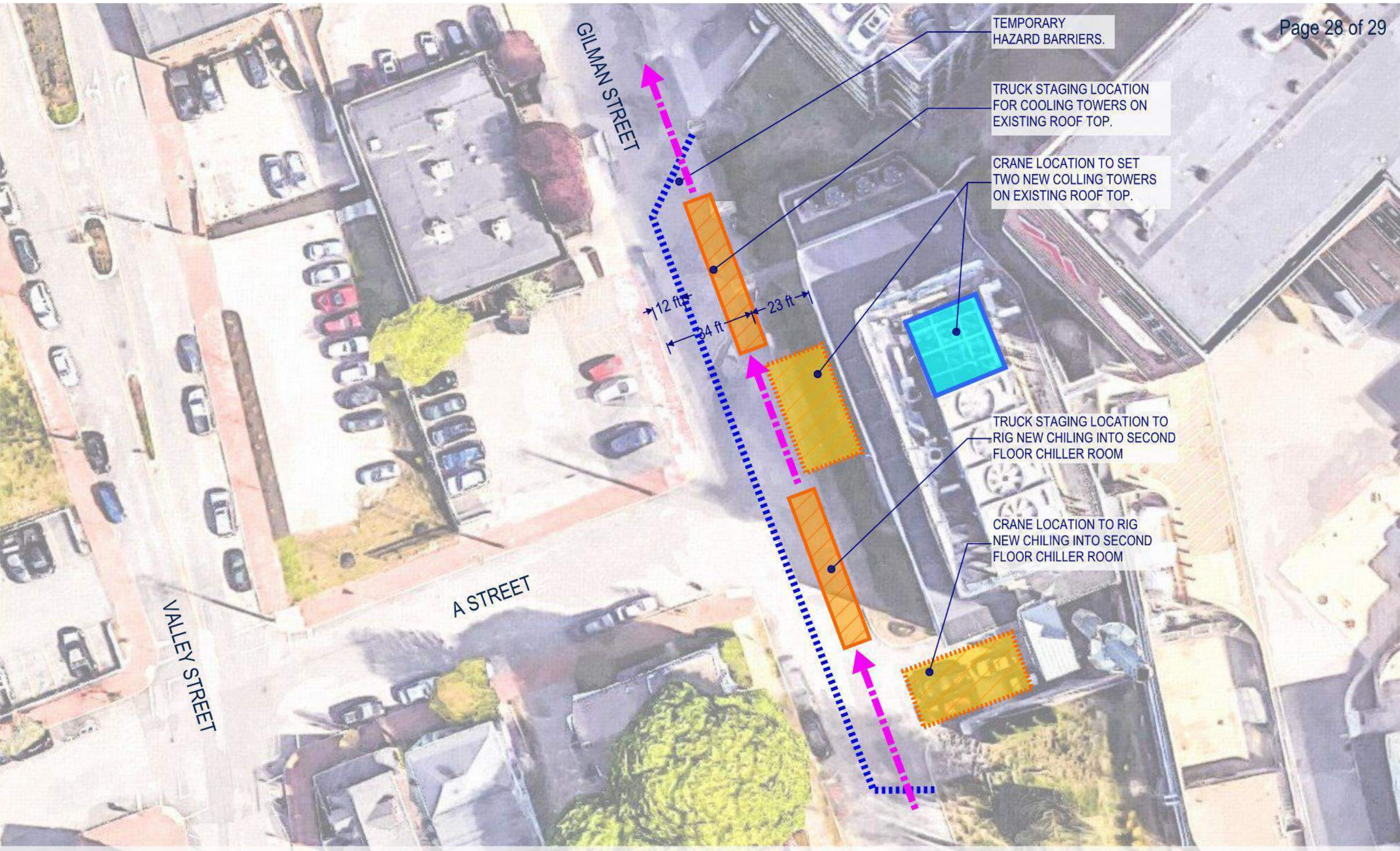




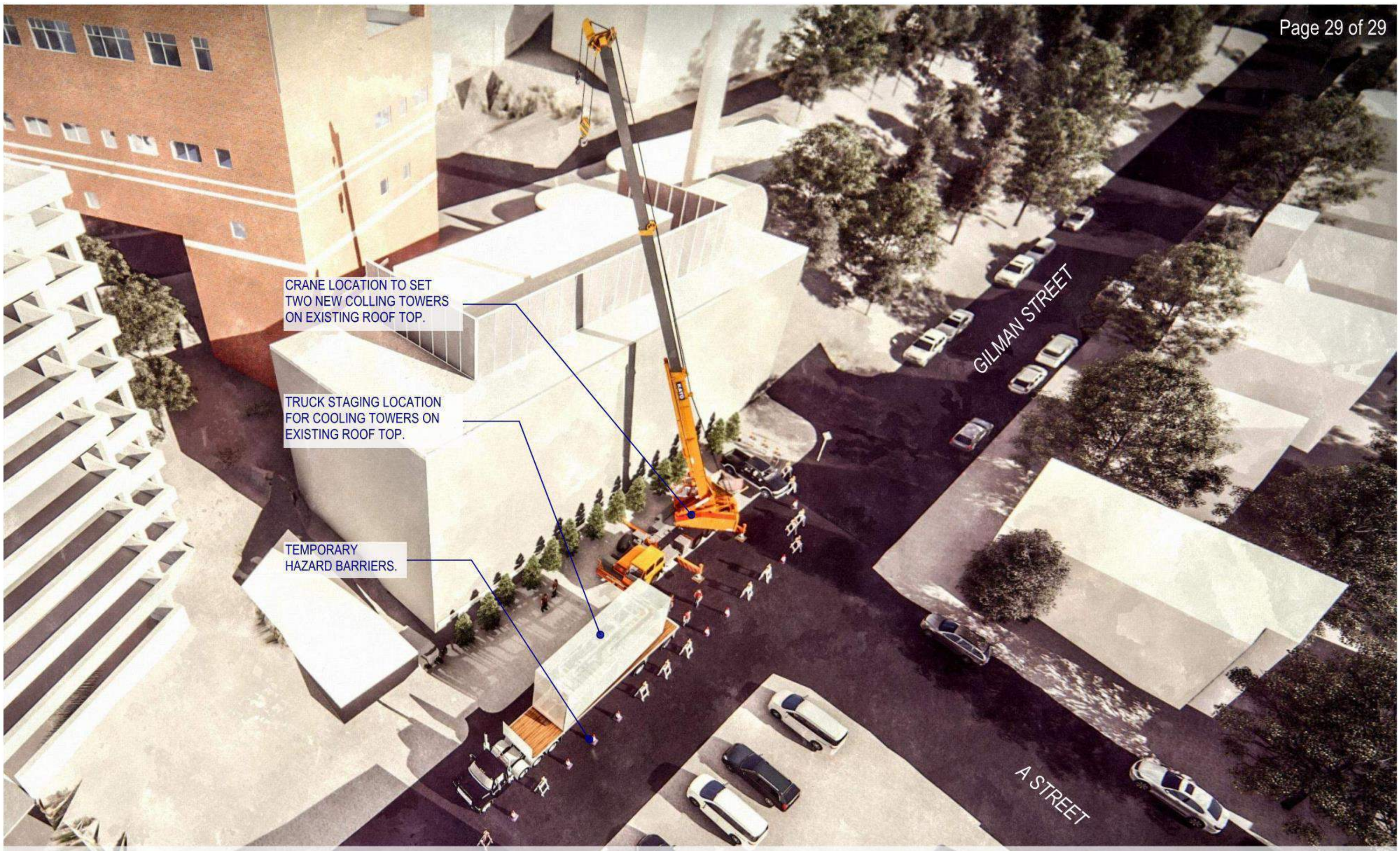
[ Central Utility Plant ] Logistics Plans and Studies











CRANE LOCATION TO SET TWO NEW COLLING TOWERS ON EXISTING ROOF TOP.

TRUCK STAGING LOCATION FOR COOLING TOWERS ON EXISTING ROOF TOP.

TEMPORARY HAZARD BARRIERS.

GILMAN STREET

A STREET