

49-A-1

2007-0157

165 Park Ave.

Sea Dogs Clubhouse

City of Portland

on Spreadsheet

- The picnic shelter is proposed as a wood post and truss construction with a metal-seam roof. The roof color will be red. Construction of this shelter is being financially supported by a corporate sponsor that requested a red roof.

3. Score Board

A new score board will be mounted on two support columns located and aligned with the proposed new picnic grandstand seating. The score board is oriented to provide patrons in the left field seating a better view of the score. The score board will have two side panels for advertising. Overall scoreboard dimensions are 33 feet long, 32 inches high and mounted 16 feet high.

4. Picnic Area Grandstand Seating (Phase Two)

- The existing picnic area seating (128 seats) will be replaced by a new grandstand seating area that resembles the existing stadium and will provide better orientation for watching the game. Seating capacity will be 319 seats.
- Materials will be concrete walls, precast concrete deck panels and the same seating found in the existing stadium grandstands. The back of the grandstand will be enclosed, utilizing the same green metal panels used throughout the stadium. Access doors will be located along the back of the grandstand to allow the space to be used for storage.
- Construction of this phase would begin either in the fall of 1997 or 1998, after the playing season ends.

5. Total Impervious Area

The original development plans for Hadlock Stadium approved by the City of Portland included a total impervious area of 24.87 percent of the 29 acre complex that included other city properties. In 1995, when Site Plan approval was granted for new bleacher seating in left field, documentation was provided that demonstrated that the original impervious area had not been maximized. Based upon the 24.87 percent cover, there was still 9,888 square feet of area that was approved and not developed as impervious surface.

The new bleacher seating and associated paving that was completed in 1995 only utilized 5,250 square feet, leaving 4,638 square feet for future development. The difference between the 24.87 percent approved and the 25 percent allowed provides for an additional area of 1,642 square feet or a total area of 6,280 square feet of impervious surface.

The proposed grandstand seating and concession stand improvements do not alter the impervious cover ratio. While the proposed picnic shelter and concrete slab equals 2,520 square feet and the picnic area grandstand seating equals 2900 square feet or a total area of 5,420 square feet, the total area of impervious surfaces remains under the 25 percent allowed.

6. Sewer Capacity

The existing stadium was approved based upon capacity for the proposed 6,100 seats at 5 gpd/seat or a total of 30,500 gallons per day. When the free standing 502 seat bleacher section was added in left field in 1995, approximately 2,520 gpd were added to the system. Discussions with the city engineering department indicates that this 1995 increase was an insignificant impact to the Alms House Sewer interceptor. The net gain in seating capacity with the current proposal including both phases, will only be 178 seats or 890 gpd added to the system. Discussions with John Rague of the city engineering department indicates that this increase would not be a significant impact to the system.

7. Water Availability

The only extension of water service will be a seasonal service extended to the concession stand for sinks. Increased demand in water due to increased seating is negligible and will not have an impact upon water service.

8. Stormwater Management

Stadium Grandstand Seating and Concession:

The entire area of the proposed stadium grandstand seating and concession is presently paved with asphalt, concrete or covered by structure. There will be no increase in impervious cover within this area. Drainage will be accommodated by alteration of grades, raising CB #2 and the use of a new trench drain located within the concession stand. All new grading will maintain or improve drainage flow.

Picnic Shelter:

The introduction of the covered picnic area will not significantly alter the impervious area. Existing concrete pads presently cover approximately 80 percent of the area. Stormwater run-off will continue to sheet flow with the introduction of a trench drain along the existing retaining wall consisting of crushed stone and perforated pipe that will be connected to CB #5.

Picnic Grandstand Seating

Run-off from this structure will be insignificant. Water will be allowed to sheet flow towards the field area.

9. Traffic and Seating Capacity

There will a negligible increase in traffic due to the increased number of seats, net gain of 178 seats, when both phases are completed. The proposed improvements include other modifications that are physical and non-physical. These include the replacement of a section of general admission seats with reserved seats, resulting in a net loss of 47 seats and the adjustment of the actual seating capacity in the general admission area. As we discussed in our meeting last week, when the original traffic study was done for the stadium, eighteen inches per person was allowed. This calculated out to be 2,492 seats. Realistically, each person requires at least 20 inches per seat or 2243 seats. By making this adjustment it reduces the number of general admission seats by 249. The first phase of the proposed project for 1997 will have no increase in seats.

10. Erosion Control

All existing structures within the limit of construction shall be protected from introduction of sediment. A silt fence and or hay bales shall be installed in the vicinity of the picnic shelter where the grade drops significantly at the access to right field.

11. Construction Time Frame

The estimated construction schedule is as follows:

Excavate for foundation and footings	Jan. 6, 1997
Pour footing and foundations	Jan. 10, 1997
Construct grandstand structure	Jan. - April 1, 1997
Construct concession stand	Jan. - April 1, 1997
Construct picnic shelter	Jan. - April 1, 1997

Phase II Picnic Area Grandstand

Commence construction
(Note: subject to change maybe fall of 1998)
Complete project

October 1, 1997

April 1, 1998

PROJECT NARRATIVE

HADLOCK STADIUM AMENDED SITE PLAN

December 19, 1996

1. Project Description

Phase I Grandstand Seating & Concession Stand

- Expand the existing grandstand seating along the first baseline behind the Sea Dogs dugout. This will require the removal of 34 existing seats along the main walkway area and reconfiguration of the access ramp.
- The new grandstand seating will be tied into the existing stadium utilizing the same detail and materials to maintain continuity. Two new speakers on risers will also be added along the rear of this new seating.
- The existing ramp access will be modified by eliminating the second level ramp and providing access from a stairway and an enclosed handicap lift.
- The area below the new grandstand seating (former ramp location) will be enclosed for use as a storage area.
- The existing free standing concession (sausage stand) area is being replaced by a permanent covered area with an enclosed storage kiosk. The materials for this concession stand will be the same masonry block and metal panels used in the stadium.
- The existing fence and parking configuration is being modified slightly to provide better circulation and access to the existing picnic area in right field. A new six foot high fence will be provided that will enhance the appearance of the stadium from Park Avenue. Presently, a tight weave metal fabric fence, green in color is being considered for this location.

2. Picnic Shelter

- The existing picnic area along the right field line will be modified by providing a new 30' x 84' open air picnic shelter. The existing concrete slabs will be reset at closer spacing and new concrete will be placed between these existing concrete slabs to form a continuous slab. The area surrounding the slab will be remulched with bark mulch.

- The picnic shelter is proposed as a wood post and truss construction with a metal-seam roof. The roof color will be red. Construction of this shelter is being financially supported by a corporate sponsor that requested a red roof.

3. Score Board

A new score board will be mounted on two support columns located and aligned with the proposed new picnic grandstand seating. The score board is oriented to provide patrons in the left field seating a better view of the score. The score board will have two side panels for advertising. Overall scoreboard dimensions are 33 feet long, 32 inches high and mounted 16 feet high.

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Phase II Picnic Area Grandstand

Commence construction

October 1, 1997

(Note: subject to change maybe fall of 1998)

Complete project

April 1, 1998

HADLOCK FIELD STADIUM AMENDED SITE PLAN

PROPOSED LAYOUT PLAN

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

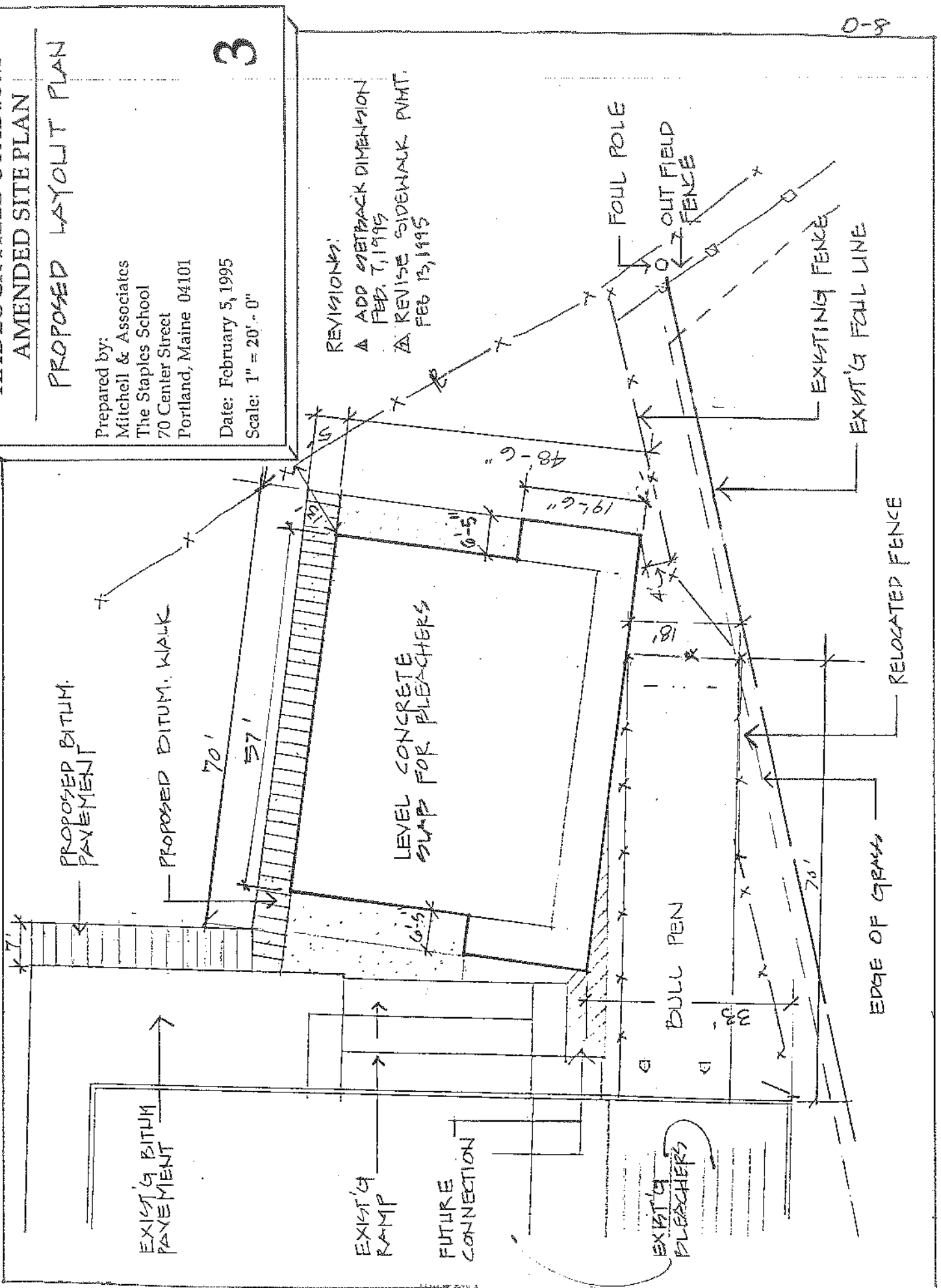
Date: February 5, 1995
Scale: 1" = 20' - 0"

3

0-8

REVISIONS:

- ▲ ADD SETBACK DIMENSION FEB. 7, 1995
- ▲ REVISE SIDEWALK PVMT. FEB 13, 1995



MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

February 6, 1995

Mr. Richard Knowland, Senior Planner
Planning & Urban Development
City of Portland
389 Congress Street
Portland, Maine 04101

RE: HADLOCK FIELD BLEACHER EXPANSION

Dear Rick:

This submission is in regards to documentation requested as part of the approval process for the proposed bleacher expansion at Hadlock Field. Attached for your review are the following exhibits:

Sheet 1 - Existing Conditions (including original approval for paved picnic area)

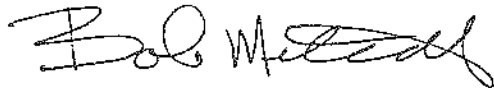
Sheet 2 - Proposed Grading Plan

Sheet 3 - Proposed Layout Plan

Per your request we have calculated the impervious surface ratio for this proposal. The original approval allowed for 24.87% coverage that included the paved picnic area that was to be constructed where the bleachers are now proposed. The total impervious cover for the picnic area was 9,888 square feet. The new bleachers and paved circulation total 4,177 square feet, leaving a net reduction in paving of 5,711 square feet. The reduction in pavement keeps the total impervious area under 25 percent.

Should you have any questions concerning this documentation, please do not hesitate to contact our office.

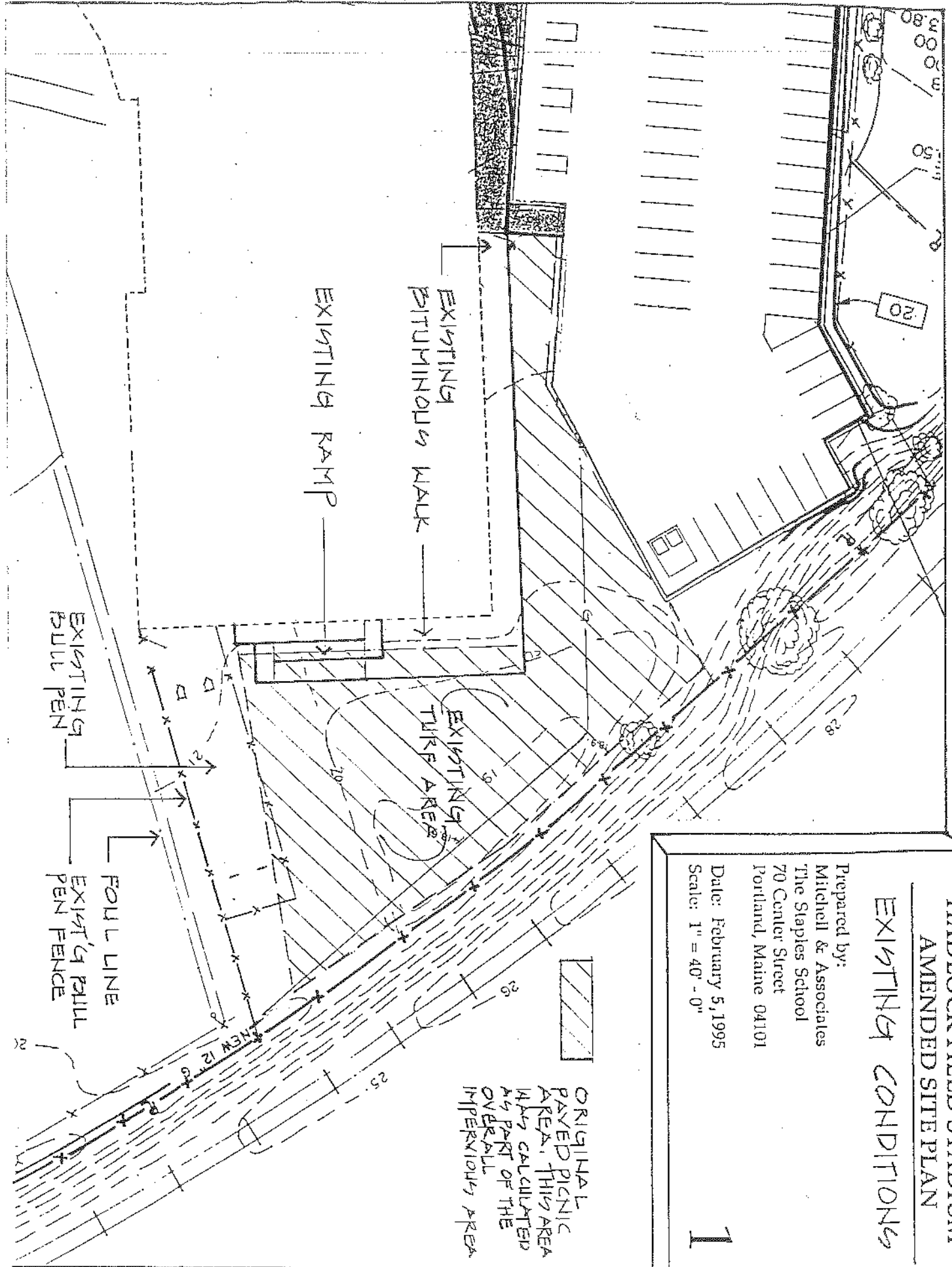
Sincerely,
Mitchell & Associates



Robert B. Metcalf

Enclosure

cc: Sam Hofsus
Mike Fagerson



HADLOCK FIELD STADIUM
 AMENDED SITE PLAN
 EXISTING CONDITIONS

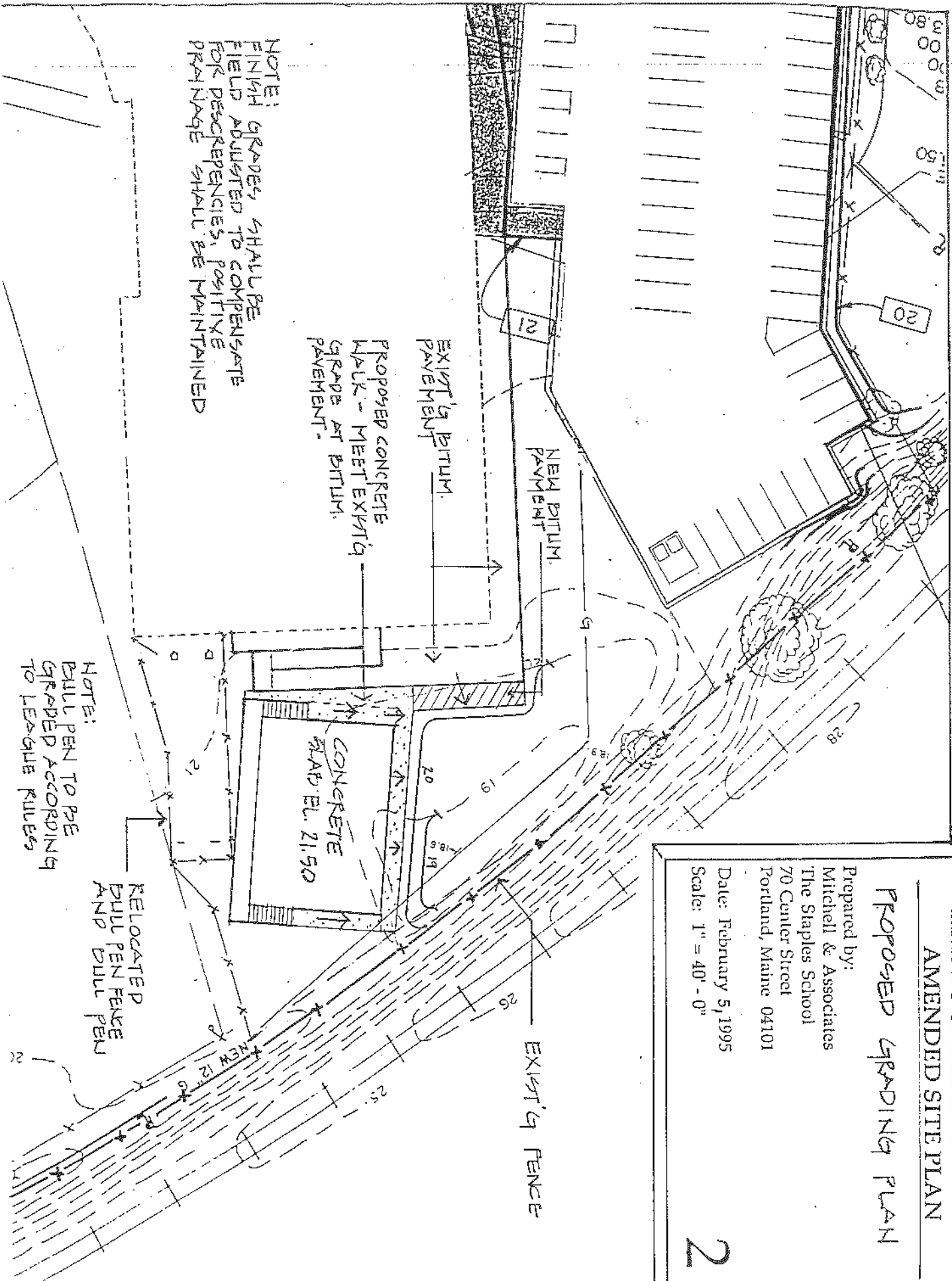
Prepared by:
 Mitchell & Associates
 The Staples School
 70 Center Street
 Portland, Maine 04101

Date: February 5, 1995
 Scale: 1" = 40' - 0"

1

ORIGINAL
 PAVED PICNIC
 AREA. THIS AREA
 HAS CALCULATED
 AS PART OF THE
 OVERALL
 IMPERVIOUS AREA

EXISTING RAMP
 EXISTING BITUMINOUS WALK
 EXISTING TURF AREA
 EXISTING BULL PEN
 EXIST'G BULL PEN FENCE
 FOUL LINE



NOTE: FINISH GRADES SHALL BE FIELD ADJUSTED TO COMPENSATE FOR DISCREPANCIES, POSITIVE DRAINAGE SHALL BE MAINTAINED

PROPOSED CONCRETE WALK - MEET EXIST'g GRADE AT BITUM. PAVEMENT.

EXIST'g BITUM. PAVEMENT.

NEW BITUM. PAVEMENT.

CONCRETE SLAB ELEV. 21.50

EXIST'g FENCE

RELOCATED DULL PEN FENCE AND DULL PEN

NOTE: DULL PEN TO BE GRADED ACCORDING TO LEASING RULES

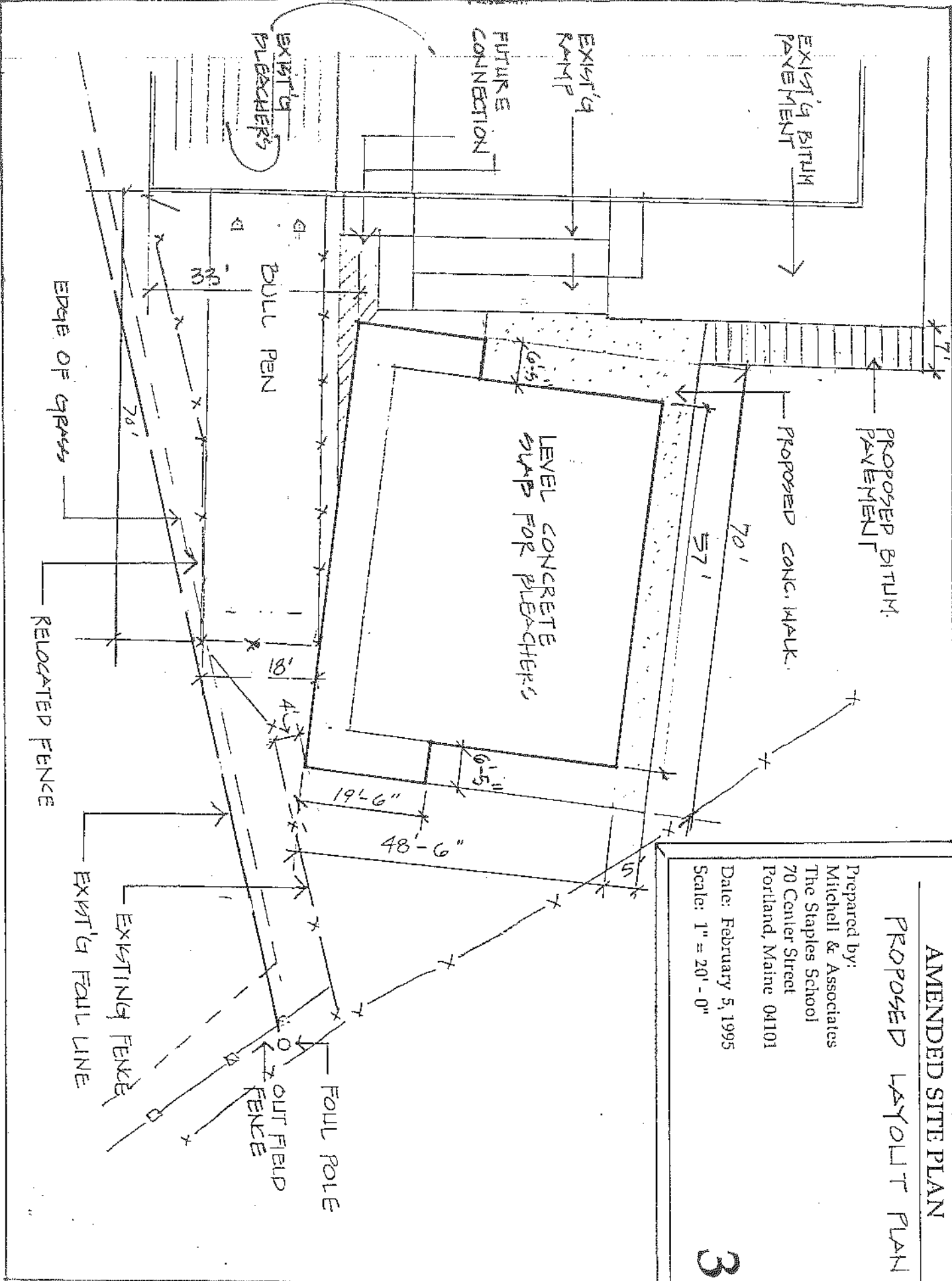
HADLOCK FIELD STADIUM
AMENDED SITE PLAN

PROPOSED GRADING PLAN

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

2



HADLOCK FIELD STADIUM
AMENDED SITE PLAN
PROPOSED LAYOUT PLAN

Prepared by:
 Mitchell & Associates
 The Staples School
 70 Center Street
 Portland, Maine 04101

Date: February 5, 1995
 Scale: 1" = 20' - 0"

3

TO: Richard Knowland, Senior Planner

FROM: Jim Seymour, Acting Development Review Coordinator

DATE: March 6, 1995

SUBJECT: Hadlock Field - Bleacher Addition - Drainage

This morning I met with Bob Metcalf, of Mitchell & Associates, Mike Parrity of White Brothers Construction, and Mike Fagerson with the Portland Sea Dogs, at Hadlock Field. White Brothers had begun removal of the hay bales where the bleachers are proposed for construction. The meeting and site observations produced the following changes to the drainage facilities.

1. The contours on Mitchell & Assoc. plans were not accurate. Field grading between the railroad fence and end of the existing bleachers actually slopes towards the left field corner.
2. Mr. Parrity suggested we relocate the drywell location to avoid a great deal of filling and regrading. I agreed with him and suggested a location just outside the bullpen fence. A grassed area is proposed at this location and would provide a protected area away from fan traffic and avoid any possible problems with pedestrian involvement.
3. Bob Metcalf shall provide revised drawings reflecting the location changes of the drywell structure, and underdrain along the railroad side of the concrete slab.
4. Grading should be sloped toward the drywell structure from the rear of the bleachers and along the side of the bleachers nearest the tracks. I suggested that this only be sloped at 0.5% to allow as much surface runoff to infiltrate into the soils as possible prior to reaching the drywell.
5. Mr. Parrity and Bob Metcalf also made me aware that the City was responsible for final grading, loaming, and seeding. To decrease confusion Bob asked White Bros. to set stakes with final grades for the City. White Bros. should also provide final underdrain inverts to Bob Metcalf who will in turn relay this data to the revised plan for City records.
6. Keith Dalton and his crew from Northern Utilities field located the gas main, and discovered that the main is 2-3 ft. closer to the rear fence than is shown on the plans, which will give White Bros. more of safety buffer to place the concrete slab.

I will be checking construction of the drywell next week and expect that the Development Review Coordinator should follow-up with at least one more quick visit during final grading and loaming. If you have any inquiries or concerns, please contact me soon.

Rich 0-14

Department of Public Works



Nadeen M. Daniels
Assistant City Manager
Director

William J. Bray
Deputy Director
City Engineer

CITY OF PORTLAND

28 February 1995

Linda Kokemuller
DEP Land Quality Control Bureau
312 Canco Road
Portland, Me 04103

RE: Expansion of Hadlock Field - Traffic Impacts

Dear Linda:

The proposed expansion of Hadlock Field will entail the addition of 500 seats, raising total game capacity from 6,000 to 6,500. The attached Hadlock Field Follow-up Review of the 1994 Sea Dogs Parking and Traffic Operations summarizes experience during the 1994 season, and clearly points out the lack of any observed problems with either parking or traffic operations in the vicinity of the field, despite the fact that nearly all of the games at the end of the season were sold out (the original traffic study submitted to DEP in 1993 assumed a "design event" of 75 percent capacity).

Using the estimation procedures developed for the original traffic impact study, the proposed 500 seat expansion would attract a total of 423 additional spectators by automobile (84.65 percent of total new spectators), with new traffic and parking space demand of 141 vehicles (average vehicle occupancy of 3 persons).

The attached report clearly points out that two key parking areas identified in the original traffic study were extremely under utilized. The Maine Medical Center Parking Garage, intended to provide 500± spaces was discontinued in June due to lack of use, and the Bedford Street parking lot at USM was observed to have in excess of 300 vacant spaces during numerous games, including sell out events. Private lots in the vicinity of the field provided much of the needed capacity, as was expected. Given the excess of parking available in the area, parking capacity is expected to more than adequate.

During the 1994 baseball season, a total of 22 accidents were recorded in the study area. Three of these occurred on the dates of home games. A review of data indicated that there was no increase in accident frequency during the baseball season as compared to previous years. In terms of traffic flow, it was observed that game traffic cleared the area within 15 - 20 minutes following a game. Observation of several key intersections in close proximity to the field did not indicate the need to modify traffic signal operations (a potential need identified in the original traffic study). General observations within the study area as a whole did not identify any traffic operational problems, and more detailed assessment was deemed unnecessary.

Overall, traffic and parking operations proceeded efficiently, and minor changes were instituted as needed through the Hadlock Field Parking and Traffic Operations Committee, under the guidance of the procedures established in the Hadlock Field Operations Handbook. Usage of shuttle buses by patrons was higher than was anticipated, and this, combined with a probable higher level of pedestrian trips and higher vehicle occupancy than was predicted resulted in an actual parking and traffic demand that was lower than projected. It is expected that this phenomenon will continue with the proposed expansion, and

any additional parking and traffic demand should be satisfactorily addressed by measures and strategies already in place, as well as additional improvements already planned.

Sincerely,
CITY OF PORTLAND



MARY ANN THERIAULT
Principal Traffic Engineer

MAT/lms
Attachment

- pc: Nadeen Daniels, Assistant City Manager/Director of Public works
- William Bray, Deputy Director
- Bruce Bell, Operations Manager
- Richard Knowland, Planning
- Charlie Eshbach, Sea Dogs Manager

TO: Bob Ganley, City Manager

FROM: Frank LaTorre, Division Director *FLT*

SUBJECT: 1. Changes in the Sea Dogs' construction plans for Hadlock Field
2. Request of the City from Charlie Eshbach regarding this project

DATE: January 16, 1997

1. Yesterday Rick Knowland and I received these last-minute proposed changes to the Sea Dogs plans for Hadlock Field.

While we can find nothing that would stop an approval by the Planning Department, I am still surprised we would be receiving a modification such as the elimination of the concession kiosk the day before construction is to begin.

If you have any problems with the changes please let me know.

2. Charlie has asked the City to help with this project in the following ways:
 - put the fencing (wooden and chain link) back up that had to be taken down to get construction equipment in the picnic area. The fencing will be put back up in March.
 - connect their picnic enclosure roof drainpipe to the drainage system of the stadium, a connection made near the home bullpen - to be done "in a few weeks"
 - have Parks and Recreation remove the plants in back of the reserved seating section of the picnic area then replace these when construction is completed, to be done "as soon as possible"

I'll await your word in the City's commitment to these three items. If you approve them, we can work out coordination with their contractor.

cc: Rick Knowland, Planning Department

MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

January 15, 1997

Mr. Richard Knowland, Senior Planner
City of Portland Planning Department
389 Congress Street
Portland, Maine 04101

RE: HADLOCK STADIUM

Dear Rick:

This correspondence is in regards to our conversation concerning the Portland Sea Dogs request to phase some of the proposed expansion plans recently approved by the City. As you know, the proposed expansion plans are part of a long range plan for future growth of the stadium. Due to those plans, this initial phase must be structurally designed to accommodate those future plans. The development cost for all of the approved work has exceeded the anticipated budget for the 1997 season, therefore the Sea Dogs are requesting to phase in several improvements over the next several seasons. Timing of these phases obviously is dependent upon a capital improvement budget related to season profits.

The following project elements are to be phased:

- **Concession Kiosk** - This structure will be added at a future date which has not been determined at this time.
- **Chain Link Fence** - For immediate plans, the existing wood fence will be relocated along the new fence alignment as shown on the Site Plan and will also enclose the area around the kiosk following the roof line footprint of the proposed concession kiosk. New fence will be considered at the time of construction for the kiosk. The entry gates shall be installed as shown on the plan, however, at this time the original chain link gate assembly will be used.
- **Electric and Air Conditioner Enclosure** - This area will be fenced in as indicated on the plan, however, until the new fence is selected the enclosure will be the same chain link mesh as the entry gates.
- **Enclosed Storage Area** - For the present time the enclosed storage area below the grandstand is to be eliminated from the project and will be included for construction in a later phase.

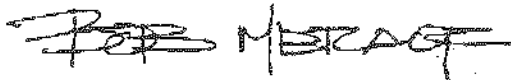
Mr. Richard Knowland

Page 2

- **Concrete Paving** - The new concrete paving in the area below the new grandstand seating will be delayed until future development of the kiosk. The existing bituminous paving will remain and the unpaved portion under the former ramp will receive new bituminous paving. Due to the asphalt plants production season and the opening day schedule, this area will have a stone dust surface until the Sea Dogs are on a spring road trip, then paving will occur.
- **Picnic Shelter** - Due to the winter construction season and the inability to reuse the existing concrete pads as planned, this area will be surfaced with bark mulch for the 1997 season. The concrete slab will be poured during the construction phase for the picnic area grandstand seating.

We have enclosed an updated Site Plan that has been annotated to reflect these changes. Should you have any questions or comments, please do not hesitate to contact our office.

Sincerely,
Mitchell & Associates



Robert B. Metcalf

Enclosure

cc: Charles Esbach
Mike Fagerson
Brian Duffy



CITY OF PORTLAND

April 19, 1995

Ms. Linda Kokemuller
Maine Department of Environmental Protection
Land Quality Control
312 Canco Road
Portland, ME 04103

Dear Linda:

On March 6, 1995 the Portland Planning Authority approved an amendment to the Hadlock Field site plan involving a 502 seat bleacher addition. See Attachments A, B, C and D for municipal notification of final action, approval letter, and a planning staff report of review finding and other background information.

The project involved a 4,200 sq. ft. addition (concrete pad) to the existing Hadlock Field. It is located on the far northwesterly corner of the facility.

Should you have any questions concerning this material, please call me.

Sincerely,

Richard Knowland
Senior Planner

cc: Joseph E. Gray, Jr., Director of Planning and Urban Development
Alexander Jaegerman, Chief Planner

DEPARTMENT OF ENVIRONMENTAL PROTECTION
 Bureau of Land Quality Control
 State House Station 17
 Augusta, Maine 04333
 Tel: (207) 287-2111

FOR DEP USE

#L- _____
 Date Received _____

NOTIFICATION OF FINAL ACTION ON APPLICATION
 MUNICIPAL REVIEW OF DEVELOPMENT
 (38 M.R.S.A. Section 489-A)

This form is to be used by a registered municipality to notify the Department upon the taking of final action by the municipal reviewing authority, pursuant to 38 M.R.S.A. Section 489-A. This form must be received by the Department within 40 working days of the final action. The municipality must also submit one copy of the record of review and basis of decision.

Municipality: CITY OF PORTLAND

Contact Person: RICHARD KNOWLAND, SENIOR PLANNER

Address and Phone: PLANNING DEPT, CITY HALL, 389 CONGRESS ST, PORTLAND, ME, 04101
 574-8300 EXT 8725

Project Applicant: CITY OF PORTLAND / PORTLAND MAINE BASEBALL, INC.

Address and Phone: P.O. BOX 636, PORTLAND, ME, 04104

Title of Project: HADLOCK FIELD *

Date of Final Action By Municipality: MARCH 6, 1995

Please submit as attachments to this form one copy of the record of review and basis of decision. This shall include any and all review comments by either municipal review staff, outside review agents, or consultants who performed a review of any part of the application, and the official record of the municipal proceedings and final action.

Town or City of: PORTLAND

DATE: 4-19-95

By: Richard Knowland

Print Name: RICHARD KNOWLAND

and Title: SENIOR PLANNER

10/93

* NOTE. THIS IS AN AMENDMENT TO A PREVIOUSLY APPROVED PROJECT UNDER THE SITE LOCATION OF DEVELOPMENT LAW.

Planning & Urban Development

Joseph E. Gray Jr.
Director

CITY OF PORTLAND

March 6, 1995

Mr. Charles Eschbach
Portland Maine Baseball Inc.
Portland Sea Dogs
P.O. Box 636
Portland, ME 04104

RE: Hadlock Field, 271 Park Avenue

Dear Mr. Eschbach:

On March 6, 1995 the Portland Planning Authority approved an amendment to the Hadlock Field site plan involving a 502 seat bleacher addition. The approval is subject to the following condition:

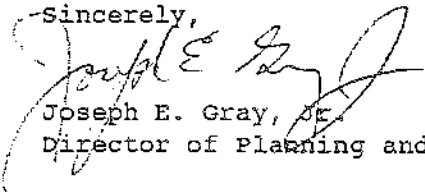
1. That landscaping shall be planted adjacent to the bleacher addition as required by the City Arborist.

This letter approves revisions to the Hadlock Field site plan including local review under the Site Location of Development Law, originally approved by the Planning Board on March 23, 1993.

The approval is based on the submitted site plan. If you need to make any modifications to the approved site plan, you must submit a revised site plan for staff review and approval.

If there are any questions concerning this letter, please contact the Planning Staff.

Sincerely,



Joseph E. Gray, Jr.
Director of Planning and Urban Development

HADLOCK FIELD EXPANSION SITE PLAN REVIEW AND BASIS OF DECISION

The Hadlock Field expansion was reviewed by the Portland Planning Authority for site plan review in accordance with the Site Location of Development Law.

1/2. Traffic

The Hadlock Field Follow-Up Review of the 1994 Sea Dogs Parking and Traffic Operations summarizes experience during the 1994 season and points out the lack of any observed problems with either parking or traffic operations in the vicinity of Hadlock Field, despite the fact that nearly all the games at the end of the season were sold out. This report was previously submitted to the DEP as well as other background information.

Using the standards developed for the original Traffic Impact Study, the proposed 500 seat expansion would attract a total of 423 additional spectators by automobile (84.65 percent of total new spectators), with an overall traffic and parking space demand of 1441 vehicles (average vehicle occupancy of 3 persons) for the entire family.

There is an excess of parking spaces in the vicinity of Hadlock Field. The Maine Medical Center parking garage, intended to provide 500+ spaces was discontinued last June due to lack of use, and the Bedford Street parking lot at USM was observed to have in excess of 300 vacant spaces during numerous games, including sellout events. Private lots, in the vicinity of Hadlock, provided much of the needed capacity, as was expected. Given the excess of parking available in the area, parking capacity is expected to be more than adequate.

A review of traffic data indicated there was no increase in accident frequency during the baseball season as compared to previous years. In terms of traffic flow, it was observed that game traffic cleared the area within 15-20 minutes following a game. Observation of several key intersections in close proximity to Hadlock did not indicate the need to modify traffic signal operations. General observations within the study area as a whole did not identify any traffic operational problems, and more detailed assessment was deemed unnecessary.

3. Bulk, location, height of proposed structure and proposed uses thereof will not cause health or safety problems as to existing uses in the neighborhood

There are no known health and safety problems associated with the proposed use including impacts related to a reduction with light, air, significant wind impact and any significant snow loading on any neighboring structure. The closest off-site building to the addition is approximately 75 feet away which forestalls such concerns.

4. Bulk, location or height of proposed structure minimizes, to the extent feasible, any substantial diminution in the value or utility to neighboring structures

The proposed addition will not cause a substantial diminution in the value or utility to neighboring structures.

The proposed development is located on a large site with existing buildings and fields that function as a major sports complex facility. The addition will be less in height (25 feet) than the existing stadium.

5. Sewers, sanitary and storm drains, water and solid waste disposal

Hadlock Field is already served by existing public water and sewer. People seated in the addition will use water and restroom facilities within the existing stadium. Comments from the Portland Water District and the City of Portland indicate that the addition of 502 seats will have a minimal impact on these utilities.

Any additional solid waste generated will be disposed in an existing dumpster on the site and transported for disposal to the Regional Waste Systems facility. See #8 below for stormwater information.

- 6/7. Landscaping

The proposed site is located on the far northwesterly corner of the site between the existing stadium structure and railroad tracks which limits the view of the proposed bleachers from Park Street. The areas directly adjacent to the bleachers will be loamed and seeded. The site plan was approved with a condition "that landscaping shall be planted adjacent to the bleacher addition as required by the City Arborist."

The site of the proposed bleachers is void of any trees, so that tree preservation is not an issue.

8. Soil, Drainage, Erosion and Sedimentation Control

The original development plans for Hadlock Field called for a large paved picnic area (9,888 sq. ft.) on the site of the proposed bleachers. The picnic area was never built. The proposed bleacher will occupy a foot print of only 4,177 sq. ft., less than half the impervious surface of the picnic area. The stormwater calculations of the original plan factored in the picnic area.

The site of the bleachers is relatively flat. In consultation with the City's Development Review Coordinator, a dry well will be constructed at the low point to accommodate excess stormwater. The actual impact of the impervious area will be less than what was calculated during the initial approval process.

The site will be stabilized with hay after construction of the slab and until such time as the weather permits permanent stabilization.

9. Lighting

There are no new exterior lighting fixtures proposed.

10. Fire

Lt. McDougall of the Fire Department reviewed and approved the plan for fire related concerns.

11. Infrastructure, existing or planned by the City

The proposed project is compatible with the Hadlock Field facility as well as infrastructure, existing or planned by the City.

12. Historic Resources

The development is not located within 100 feet of a historic landmark, historic district or historic landscape district.

13. The proposed development shall have no adverse impact upon the existing natural resources including groundwater quantity and quality, surface water quantity and quality, wetlands, unusual natural areas, and wildlife and fisheries habitats. Stormwater runoff from paved area shall be treated to the extent practical to minimize contaminants.

The Hadlock Field site is an existing developed site in an urban area. The proposed project will have no known adverse impact on existing natural resources. Stormwater runoff from the project site will be directed to a dry well avoiding runoff into the City sewer.

14. The proposed development shall not pose an unreasonable risk that a discharge to a significant groundwater aquifer will occur.

The proposed addition is served by public sewer and water. There are no known discharges emanating from this addition that could affect a groundwater aquifer.

MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

February 22, 1995

Mr. Richard Knowland, Senior Planner
Planning & Urban Development
City of Portland
389 Congress Street
Portland, Maine 04101

RE: HADLOCK STADIUM BLEACHER EXPANSION

Dear Rick:

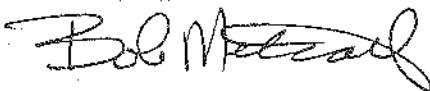
On behalf of the Portland Sea Dogs, we are pleased to submit this Amended Site Plan Application for seating expansion at Hadlock Stadium. The Portland Sea Dogs organization is proposing to construct a new bleacher seating area for five hundred and two (502) general admission seats. This new seating will be located along the third base line in left field adjacent to the existing stadium. Access will be from an extension of the existing walkway that accesses the existing stadium seating.

We have prepared the following documentation for your review:

- Amended Site Plan, dated February 18, 1993, revision date February 22, 1995.
- Amended Site Plan and Elevations, dated February 22, 1995.
- Project Narrative, dated February 22, 1995.

Should you have any questions or comments please do not hesitate to contact our office.

Sincerely,
Mitchell & Associates



Robert B. Metcalf

Enclosure

cc: Michael Fagerson

PROJECT NARRATIVE

HADLOCK STADIUM AMENDED SITE PLAN

February 22, 1995

1. Project Description

The proposed addition to Hadlock Stadium is for the installation of a prefabricated bleacher system to accommodate an additional 502 general admission seats. The new bleachers will be located along the third base line in left field, adjacent to the existing stadium. Orientation of the structure will be rotated slightly to provide proper sight line of the playing field.

The bleacher structure shall be installed on a cast-in-place concrete slab. Proposed construction of the prefabricated bleacher includes cladding the exposed under-structure with the same green seamed metal panels found throughout the stadium. New seating will be the same blue bench type seat with back rest as found in the existing stadium. The existing visitor bull pen will require relocation as shown on the accompanying plans. The bull pen will be reduced in size and will encroach upon the third base warning track. This proposed layout is designed in accordance with league requirements. All disturbed areas will be loamed and seeded after completion of construction.

2. Total Building Area

The original development plans approved by the City of Portland encompassed an impervious surface ratio of 24.87 percent. This included a significant amount of paved area designated for picnicking situated where the proposed bleachers are to be located. The picnic area was never constructed, however, the proposed impervious surface area was equal to 9,888 square feet. The present proposal for the bleacher expansion and necessary circulation is equal to 4,177 square feet. This is a net reduction in impervious cover, thus maintaining the overall percentage of impervious surfaces below the maximum 25 percent allowed.

3. Sewer Capacity

The existing stadium was approved based upon adequate capacity for the proposed 6,100 seats at 5 gpd/seat or a total of 30,500 gallons per day. The proposed expansion of 502 seats will add 2,520 gpd to the system.

Discussions with Mr. William Goodwin of the Portland City Engineering Department, indicates that this increase is insignificant and will not effect the capacity of the Alms House Sewer interceptor.

4. Water Availability

Expansion of the seating capacity will not require any additional plumbing fixtures, and based upon total capacity, will only increase demand by approximately eight percent. Based upon our review of last year's water usage, original estimates were higher than actual consumption, therefore, the addition of 502 seats should have minimal impact on water consumption. Discussions with Glen Hunter of the Portland Water District also concludes that there will be no impacts to water service based upon the proposed expansion.

5. Construction Time Frame

The estimated construction schedule is as follows:

Excavate for slab	Feb 28, 1995
Pour slab	March 7, 1995
Install bleachers	March 15, 1995
Reconstruct bull pen	March 30, 1995
Finish grade, loam & seed	April 1, 1995
Project completion	April 5, 1995

NOTE: Final bituminous paving subject to plant opening.

6. Stormwater Management

The proposed location for the new bleacher expansion is a relatively flat area that slopes toward the railroad right-of-way and the existing parking lot. Presently, the majority of the area is vacant with a sparse cover of grass over a gravelly soil. There have been no drainage problems in this area since the completion of the Hadlock Stadium renovations.

Originally this area was proposed and approved as a paved picnic area. The stormwater calculations for the renovation project included this area as paved. The present proposal, including the paved access walk and ramp to the stadium, covers an area of 4,177 square feet. The proposed coverage is less than 50 percent of what was originally designed for.

Based upon discussions with the Planning Department and their consulting review engineer, a dry well shall be constructed at the designated low point as shown on the Amended Site Plan. In addition, a four inch perforated underdrain shall be installed to aid in draining the relocated bull pen area. The underdrain shall be connected to the drywell.

The area in which the proposed improvements are to be located has been designed to continue to allow stormwater runoff to flow toward the parking lot and existing storm drain structures and toward the railroad right-of-way as it has done prior to improvements. The actual impact of the impervious area will be less than what was calculated for during the initial approval process.

7. Erosion Control

The area of proposed improvements is very flat with the adjacent grade of the railroad sloping toward the site. The extent and duration of the proposed construction will have minimal impacts. The site shall be stabilized with hay after construction of the slab and until such time as the weather permits permanent stabilization.

8. Gas Main

We have discussed the proposed location of the new bleachers with Northern Utilities. The proposed concrete slab for the bleachers will be kept off the twelve inch gas main. Field coordination shall occur between the site contractor and Northern Utilities prior to pouring of the slab.

MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

February 16, 1995

Mr. Richard Knowland, Senior Planner
Planning & Urban Development
City of Portland
389 Congress Street
Portland, Maine 04101

RE: HADLOCK FIELD BLEACHER EXPANSION

Dear Rick:

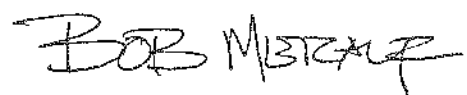
This submission is in response to our meeting at the site on Wednesday February 15th where we discussed the issues concerning stormwater management. At that meeting it was agreed to provide a dry well structure at the low point near the existing access gate to the proposed bleacher area. A six foot diameter by six foot deep dry well will be excavated and filled with one inch crushed rock, topped with a geotextile fabric and four inches of loam. The finished grade will be pitched toward this low point. In addition a four inch diameter perforated PVC underdrain will be installed along the front of the new bleachers and connect to the proposed dry well.

In response to the question concerning existing soils conditions, we have reviewed R.W. Gillespie's geotechnical report for Hadlock Field. Boring B-3 was taken at the corner of the stadium close to the proposed dry well location. Indications are the soils are a fill material consisting of silty sands, silty gravels, rock fragments, charcoal fragments and miscellaneous material. Groundwater was incurred at approximately thirteen feet.

In addition, we have revised the plans to show a bituminous walkway along the rear of the bleachers rather than concrete as originally indicated.

Should you have any questions or concerns regarding this documentation, please do not hesitate to contact our office.

Sincerely,
Mitchell & Associates



Robert B. Metcalf

Enclosure

cc: Mike Fagerson

HADLOCK FIELD STADIUM
AMENDED SITE PLAN

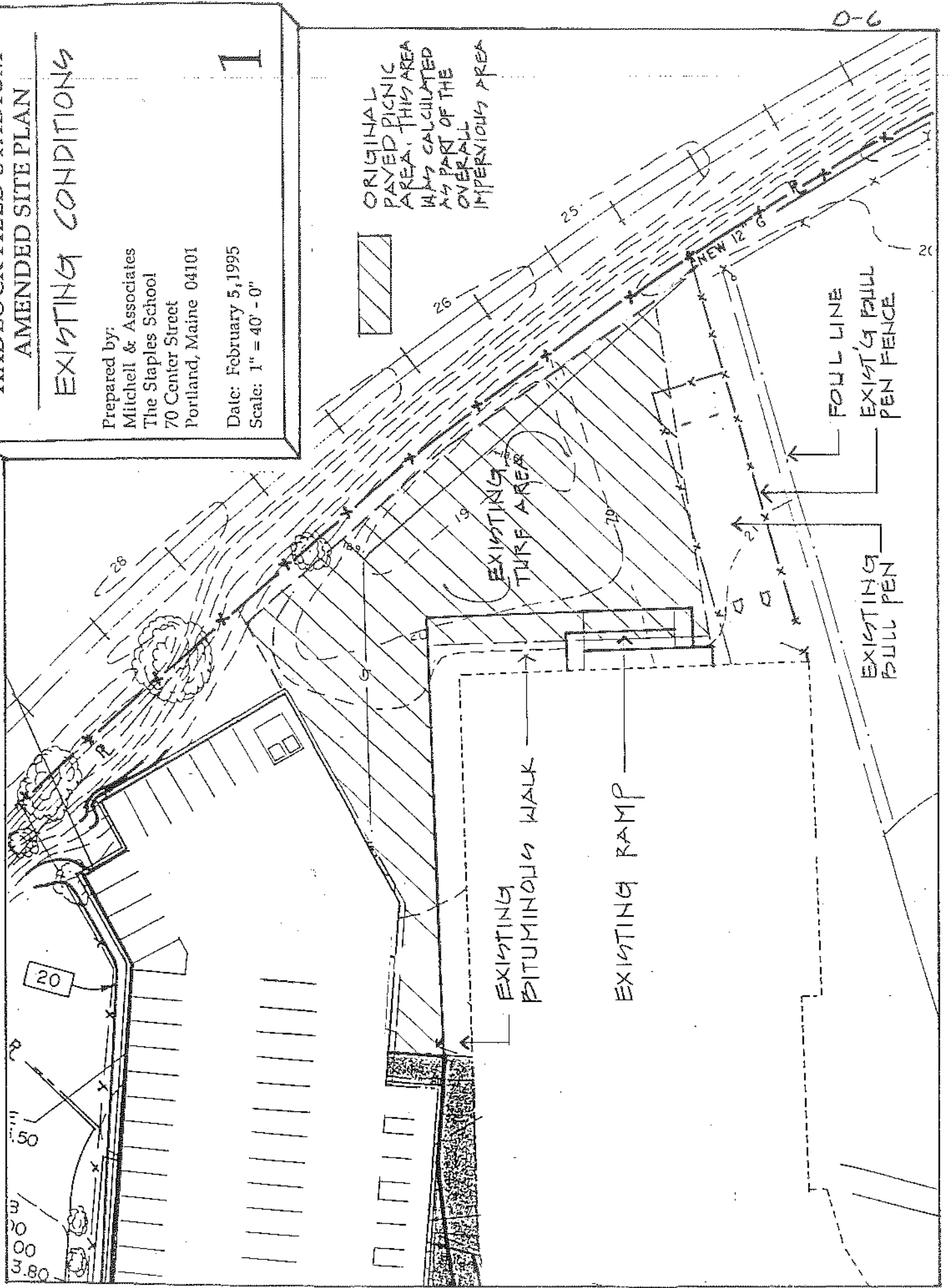
EXISTING CONDITIONS

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

1

ORIGINAL
PAVED PICNIC
AREA. THIS AREA
WAS CALCULATED
AS PART OF THE
OVERALL
IMPERVIOUS AREA



FOUll LINE
EXISTING FULL
PEN FENCE

EXISTING
FULL PEN

EXISTING
BITUMINOUS WALK

EXISTING RAMP

EXISTING
TURF AREA

D-6

HADLOCK FIELD STADIUM
AMENDED SITE PLAN

PROPOSED GRADING PLAN

2

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

REVISIONS:

- A ADD SETBACK DIMENSION FEB. 7, 1995
- Δ PUNT REVISION FEB 13, 1995
- ▲ ADD DRYWELL & UNDERDRAIN FEB 16, 1995

EXIST'G FENCE

15' SETBACK

RELOCATED
BULL PEN FENCE
AND BULL PEN

NOTE:
BULL PEN TO BE
GRADED ACCORDING
TO LEAGUE RULES

NOTE:
FINISH GRADES SHALL BE
FIELD ADJUSTED TO COMPENSATE
FOR DISCREPANCIES. POSITIVE
DRAINAGE SHALL BE MAINTAINED

4" PERFORATED UNDERDRAIN

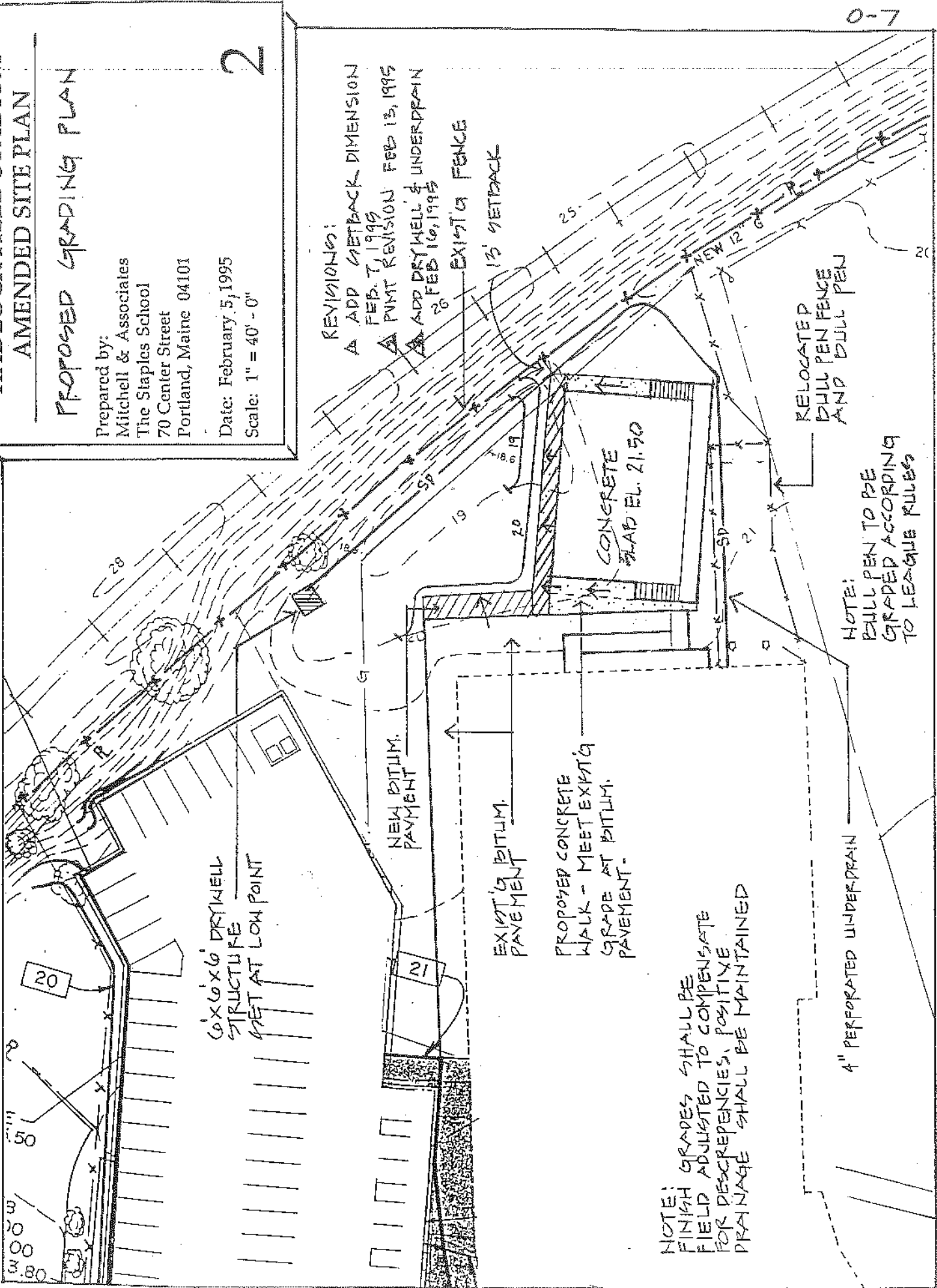
PROPOSED CONCRETE
WALK - MEET EXIST'G
GRADE AT BITUM.
PAVEMENT.

EXIST'G BITUM.
PAVEMENT

NEW BITUM.
PAVEMENT

CONCRETE
SLAB EL. 21.50

6'x6' DRYWELL
STRUCTURE
MET AT LOW POINT



MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

December 19, 1996

Mr. Richard Knowland, Senior Planner
Planning Department
City of Portland
389 Congress Street
Portland, Maine 04101

RE: HADLOCK STADIUM IMPROVEMENTS

Dear Rick:

On behalf of the Portland Sea Dogs, we are pleased to submit the following Amended Site Plan application for Hadlock Stadium. As you are aware, the overwhelming support for the Sea Dogs baseball team has encouraged the Sea Dogs owners to continue to make improvements to the stadium facilities to provide better service to patrons of the ballpark. This proposal includes stadium improvements which are planned as a two stage construction project. The first phase is scheduled to be completed for the 1997 season, the second phase scheduled for completion for the 1998 season.

Phase One improvements include the following:

- A new seating section, 234 seats added to the right field grandstand, including a revised access ramp, an enclosed handicap lift and two new riser pole mounted speakers to replace the existing speakers. This will require the removal of 34 existing seats located along the existing walkway.
- A covered concession stand to replace the present open air sausage concession including new fencing and realignment of the parking.
- A 30 foot by 84 foot picnic shelter to replace the seasonal awning.
- A new score board located behind the existing picnic area seating that will align with the proposed new picnic area grandstand seating.

Phase Two improvements include the following:

- New grandstand seating for the picnic area, 319 seats.

Richard Knowland
Page 2.

Enclosed for your review are the following documents:

Amended Site Plan

Existing Conditions Plan

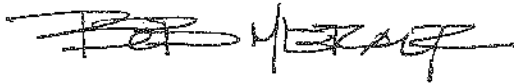
Architectural Perspective

Architectural Plans and Detail Sections

Copy of New Score Board

As you know the Portland Sea Dogs Organization is anxious to begin this project in order to have it completed by the beginning of the 1997 season. As we have discussed, this project has been evolving since the end of August of this year and has developed into the plan before you. Should you have any questions concerning the proposed plans, please do not hesitate to contact me.

Sincerely;
Mitchell & Associates

A handwritten signature in black ink, appearing to read "R. B. METCALF". The signature is stylized with a long horizontal line extending to the right.

Robert B. Metcalf

Enclosure

CC Mike Fagerson
Brian Duffy

PROJECT NARRATIVE

HADLOCK STADIUM AMENDED SITE PLAN

December 19, 1996

1. Project Description

Phase I Grandstand Seating & Concession Stand

- Expand the existing grandstand seating along the first baseline behind the Sea Dogs dugout. This will require the removal of 34 existing seats along the main walkway area and reconfiguration of the access ramp.
- The new grandstand seating will be tied into the existing stadium utilizing the same detail and materials to maintain continuity. Two new speakers on risers will also be added along the rear of this new seating.
- The existing ramp access will be modified by eliminating the second level ramp and providing access from a stairway and an enclosed handicap lift.
- The area below the new grandstand seating (former ramp location) will be enclosed for use as a storage area.
- The existing free standing concession (sausage stand) area is being replaced by a permanent covered area with an enclosed storage kiosk. The materials for this concession stand will be the same masonry block and metal panels used in the stadium.
- The existing fence and parking configuration is being modified slightly to provide better circulation and access to the existing picnic area in right field. A new six foot high fence will be provided that will enhance the appearance of the stadium from Park Avenue. Presently, a tight weave metal fabric fence, green in color is being considered for this location.

2. Picnic Shelter

- The existing picnic area along the right field line will be modified by providing a new 30' x 84' open air picnic shelter. The existing concrete slabs will be reset at closer spacing and new concrete will be placed between these existing concrete slabs to form a continuous slab. The area surrounding the slab will be remulched with bark mulch.

- The picnic shelter is proposed as a wood post and truss construction with a metal-seam roof. The roof color will be red. Construction of this shelter is being financially supported by a corporate sponsor that requested a red roof.

3. Score Board

A new score board will be mounted on two support columns located and aligned with the proposed new picnic grandstand seating. The score board is oriented to provide patrons in the left field seating a better view of the score. The score board will have two side panels for advertising. Overall scoreboard dimensions are 33 feet long, 32 inches high and mounted 16 feet high.

4. Picnic Area Grandstand Seating (Phase Two)

- The existing picnic area seating (128 seats) will be replaced by a new grandstand seating area that resembles the existing stadium and will provide better orientation for watching the game. Seating capacity will be 319 seats.
- Materials will be concrete walls, precast concrete deck panels and the same seating found in the existing stadium grandstands. The back of the grandstand will be enclosed, utilizing the same green metal panels used throughout the stadium. Access doors will be located along the back of the grandstand to allow the space to be used for storage.
- Construction of this phase would begin either in the fall of 1997 or 1998, after the playing season ends.

5. Total Impervious Area

The original development plans for Hadlock Stadium approved by the City of Portland included a total impervious area of 24.87 percent of the 29 acre complex that included other city properties. In 1995, when Site Plan approval was granted for new bleacher seating in left field, documentation was provided that demonstrated that the original impervious area had not been maximized. Based upon the 24.87 percent cover, there was still 9,888 square feet of area that was approved and not developed as impervious surface.

The new bleacher seating and associated paving that was completed in 1995 only utilized 5,250 square feet, leaving 4,638 square feet for future development. The difference between the 24.87 percent approved and the 25 percent allowed provides for an additional area of 1,642 square feet or a total area of 6,280 square feet of impervious surface.

The proposed grandstand seating and concession stand improvements do not alter the impervious cover ratio. While the proposed picnic shelter and concrete slab equals 2,520 square feet and the picnic area grandstand seating equals 2900 square feet or a total area of 5,420 square feet, the total area of impervious surfaces remains under the 25 percent allowed.

6. Sewer Capacity

The existing stadium was approved based upon capacity for the proposed 6,100 seats at 5 gpd/seat or a total of 30,500 gallons per day. When the free standing 502 seat bleacher section was added in left field in 1995, approximately 2,520 gpd were added to the system. Discussions with the city engineering department indicates that this 1995 increase was an insignificant impact to the Alms House Sewer interceptor. The net gain in seating capacity with the current proposal including both phases, will only be 178 seats or 890 gpd added to the system. Discussions with John Rague of the city engineering department indicates that this increase would not be a significant impact to the system.

7. Water Availability

The only extension of water service will be a seasonal service extended to the concession stand for sinks. Increased demand in water due to increased seating is negligible and will not have an impact upon water service.

8. Stormwater Management

Stadium Grandstand Seating and Concession:

The entire area of the proposed stadium grandstand seating and concession is presently paved with asphalt, concrete or covered by structure. There will be no increase in impervious cover within this area. Drainage will be accommodated by alteration of grades, raising CB #2 and the use of a new trench drain located within the concession stand. All new grading will maintain or improve drainage flow.

Picnic Shelter:

The introduction of the covered picnic area will not significantly alter the impervious area. Existing concrete pads presently cover approximately 80 percent of the area. Stormwater run-off will continue to sheet flow with the introduction of a trench drain along the existing retaining wall consisting of crushed stone and perforated pipe that will be connected to CB #5.

Picnic Grandstand Seating

Run-off from this structure will be insignificant. Water will be allowed to sheet flow towards the field area.

9. Traffic and Seating Capacity

There will a negligible increase in traffic due to the increased number of seats, net gain of 178 seats, when both phases are completed. The proposed improvements include other modifications that are physical and non-physical. These include the replacement of a section of general admission seats with reserved seats, resulting in a net loss of 47 seats and the adjustment of the actual seating capacity in the general admission area. As we discussed in our meeting last week, when the original traffic study was done for the stadium, eighteen inches per person was allowed. This calculated out to be 2,492 seats. Realistically, each person requires at least 20 inches per seat or 2243 seats. By making this adjustment it reduces the number of general admission seats by 249. The first phase of the proposed project for 1997 will have no increase in seats.

10. Erosion Control

All existing structures within the limit of construction shall be protected from introduction of sediment. A silt fence and or hay bales shall be installed in the vicinity of the picnic shelter where the grade drops significantly at the access to right field.

11. Construction Time Frame

The estimated construction schedule is as follows:

Excavate for foundation and footings	Jan. 6, 1997
Pour footing and foundations	Jan. 10, 1997
Construct grandstand structure	Jan. - April 1, 1997
Construct concession stand	Jan. - April 1, 1997
Construct picnic shelter	Jan. - April 1, 1997

Phase II Picnic Area Grandstand

Commence construction
(Note: subject to change maybe fall of 1998)
Complete project

October 1, 1997

April 1, 1998

MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

December 31, 1996

Mr. Richard Knowland, Senior Planner
City of Portland
Planning Department
389 Congress Street
Portland, Maine 04101

RE: HADLOCK STADIUM EXPANSION

Dear Rick:

This correspondence is in regards to your recent comments and staff review for the proposed phased improvements for Hadlock Stadium. I have obtained additional information to respond to questions concerning new lighting, new speakers for the P.A. system and materials to be used in the expansion. I also spoke with Steve Bushey of DeLuca-Hoffman concerning his review comments and I have responded to these comments below.

Enclosed for your review are: new lighting information, a statement from Gardner Sound Systems, regarding new speakers for the public address system and Bill Eaton's Traffic Report Letter and a sketch showing the relocation of 3 white pines.

The following comments address specific questions:

New Lighting

New light fixtures will be provided to illuminate the access ramp and walkway beneath the new grandstand seating. The proposed fixtures will be mounted under the grandstand seating and concealed from view. See attached cut sheet and plan view.

New Public Address Speakers

As indicated previously, the existing speaker poles are being relocated to the new grandstand area. In addition, to control the quality and direction of sound, the single higher volume speakers are being replaced by two (per pole) smaller more controllable speakers. See attached letter from Gardner Sound Systems, Inc.

Materials For Proposed Expansion

Proposed Concession Kiosk: The proposed concession kiosk will be constructed utilizing the same materials found in the existing stadium. The block wall will be the same splitface masonry block, color to match the stadium. The roof support will be steel tube columns painted green, and the roof will be the same green seamed metal roofing panels found throughout the stadium.

Proposed Grandstand Seating: The materials used in the construction of the new seating are shown on the section included in our original submission, color and material will match the existing stadium. The back fence along the top row of seating will continue to be the black vinyl chainlink fence used in the stadium. The enclosure for the storage area behind the access ramp will be clad with the same green metal seamed panels.

Proposed New Fence: The proposed new fence for the grandstand and concession area will be a green vinyl clad 1 inch mesh chain link fence. The fence areas include the enclosure around the electric service and HVAC system, the new gates between the existing stadium and new concession kiosk and from the new concession kiosk to the existing wood gates adjacent to the Expo building. A more detailed cut sheet of the fence will be coming under separate cover.

Proposed Picnic Area Grandstand: The materials will be a combination of cast in place as well as precast concrete to match the existing stadium. Seating will match existing stadium seating.

Review Comments from Steve Bushey, Development Review Coordinator

1. The trench drain proposed within the concession kiosk will be a center point drain connected to the existing 15 inch PVC storm drain between CB #2 and CB #3. A wye connection will be used to connect to the existing pipe.
2. Regarding additional spot elevations; to maintain positive drainage to CB #2 and CB #3, a larger scale construction drawing will be prepared to provide sufficient spot elevations for the contractor to construct and maintain positive drainage to all structures in the vicinity of the proposed improvements. A copy of this plan will be provided for your records.
3. Regarding the picnic area drainage; the proposed trench drain and perforated pipe will connect to a solid 8 inch PVC pipe at the end of the existing retaining wall. From the end of the retaining wall the solid 8

Mr. Richard Knowland

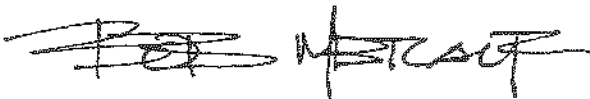
Page 3

inch PVC pipe will be connected into CB #5. This existing structure is part of the drainage system at Hadlock and Fitzpatrick Stadiums. CB #5 connects to the existing Alms House sewer system. No additional drains are proposed within the picnic shelter. Final grades around the picnic shelter will be adjusted to maintain positive flow toward the trench drain or to continue as existing overland flow to CB #5.

4. Regarding the existing white pine to be relocated; the three white pines shall be relocated to the slope area between the existing concrete walk and the fence along the bituminous walkway to the Expo Building. See attached sketch.

I trust this information will satisfy your comments and those of staff. Should you have any additional questions or comments please do not hesitate to contact me.

Sincerely,
Mitchell & Associates

A handwritten signature in black ink, appearing to read "R. B. Metcalf", written in a cursive style.

Robert B. Metcalf

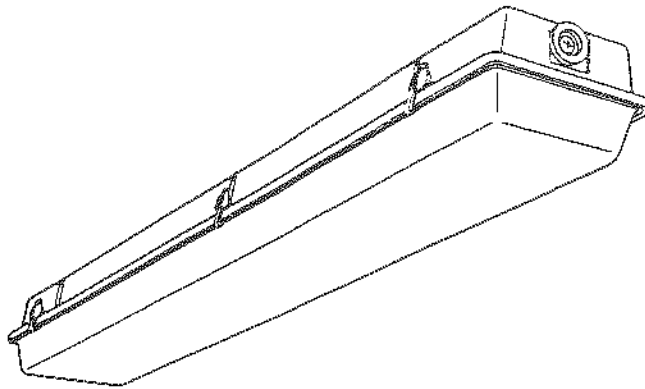
Enclosure

cc: Mike Fagerson
Brian Duffy

DUST/MOISTURE WET LOCATION FIXTURE
4' or 8' LENGTH ■ 2 LAMPS ■ SLIMLINE

DMW 248/296

FEATURES



DMW 248 shown.

- Impact-resistant, UV-resistant, reinforced polyester housing
- Acrylic diffuser standard, vandal-resistant diffuser optional
- Diffuser secured to fully-gasketed housing by captive, cam-action latches, six per 4' unit, ten per 8' unit
- For unit or row installation, surface or suspended mounting
- Wet location fittings standard on ends, optional on top for stem hanging
- UL listed for horizontal mounting in wet locations

SPECIFICATIONS

Ballast

Thermally-protected, resetting, Class P, HPF type 1 ballast standard. Sound rating C, CBM approved in standard combinations. U.L. listed.

Wiring & Electrical

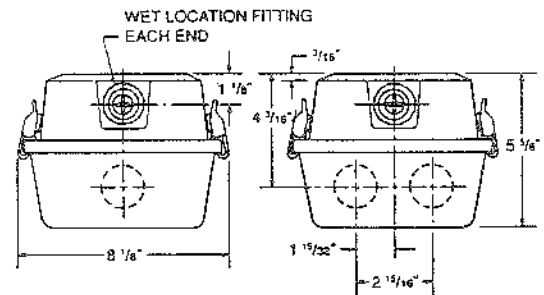
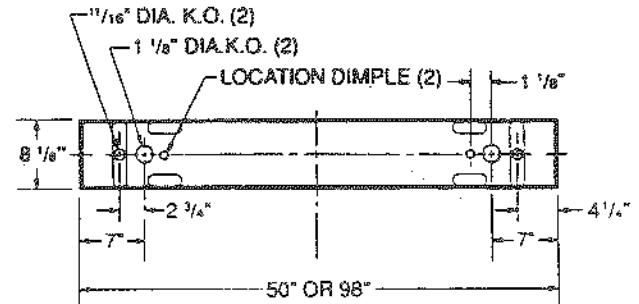
AWM, FTN or THHN wire used throughout, rated for required temperatures. Input watts (standard)(energy-saving) DMW 248 (85)(NA); DMW 296 (153)(143).

Materials

Metal parts die-formed from code-gauge steel. Standard and optional diffusers feature pebbled interior pattern and smooth exterior.

Finish

Five-stage, iron-phosphate pre-treatment ensures superior adhesion and rust resistance. All painted parts finished with high gloss baked enamel.



UL listed

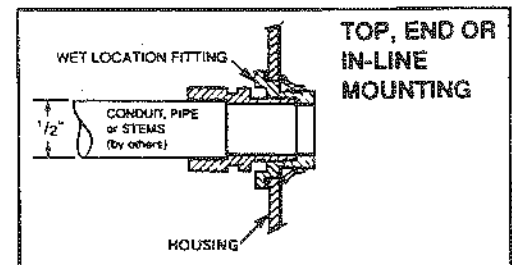
Fixture guaranteed for one year against mechanical defects in manufacture.

Dimensions and specifications subject to change without notice

MOUNTING DATA

SURFACE MOUNTING

Drill holes at location dimples through housing and channel. Attach to surface using fasteners and sealing washers appropriate to ceiling material. See ACCESSORIES on reverse side for hanging devices



Approval

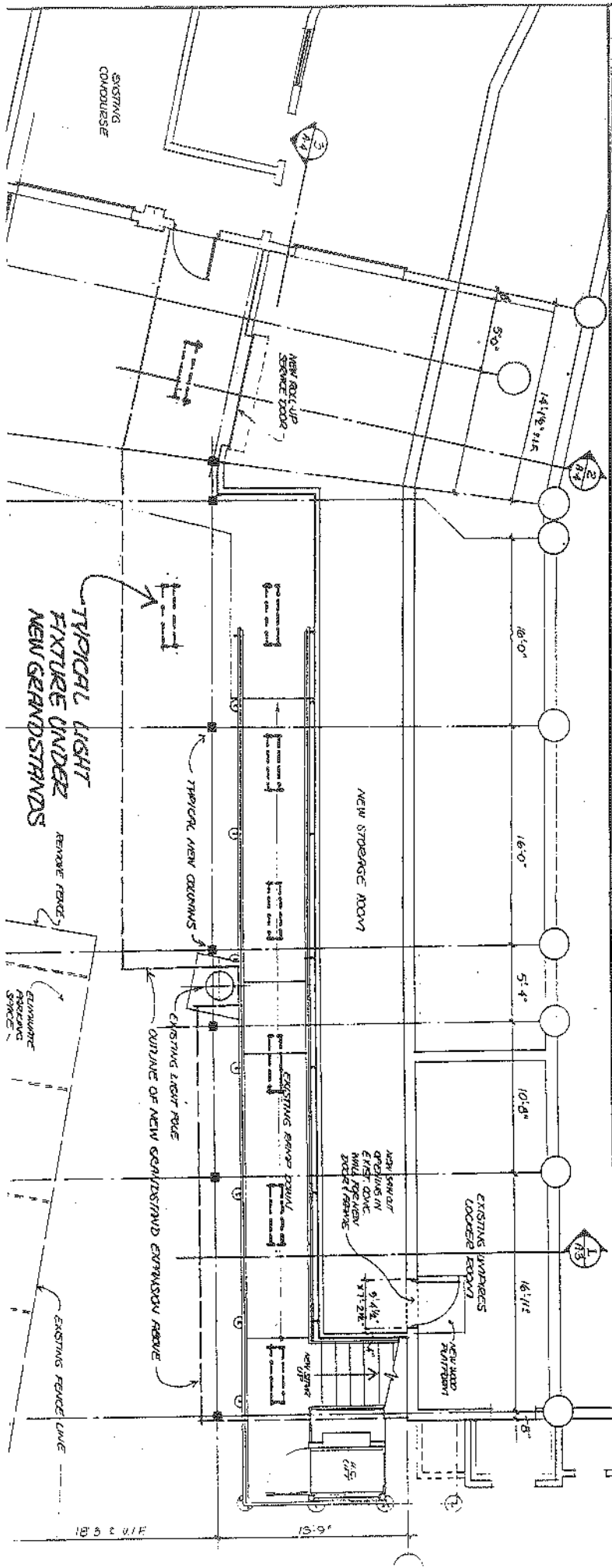
Job Information

PORTLAND SEA DOGS • HADLOCK FIELD

Type _____
 (Specify 120V, (277V))

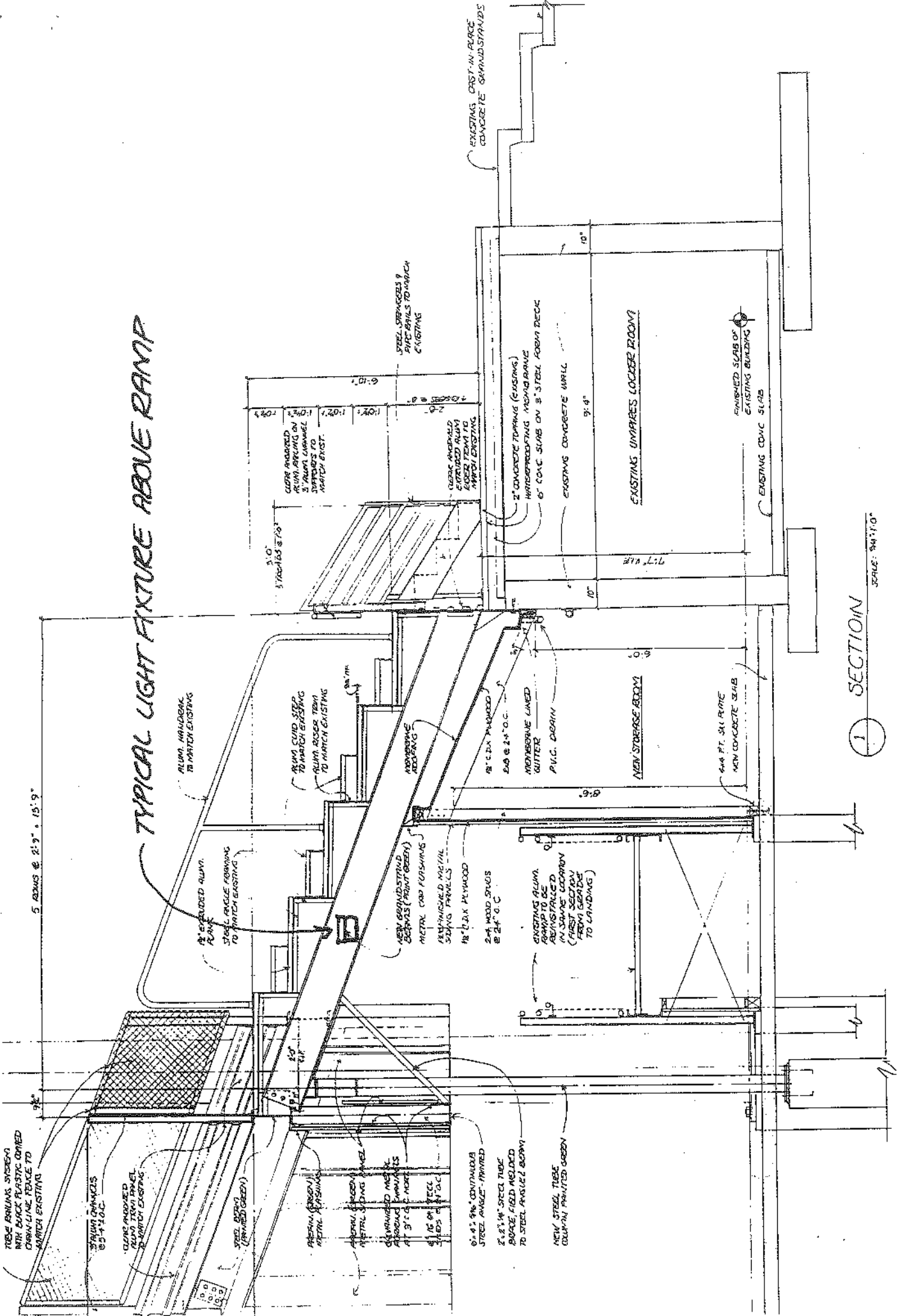
80 TO 100 WATTS EACH





LOWER LEVEL PLAN

PORTLAND SEA DOGS • HADDOCK FIELD



TYPICAL LIGHT FIXTURE ABOVE RAMP

1 SECTION SCALE: 3/4"=1'-0"

PORTLAND SEA DOGS • HADLOCK FIELD

GARDNER SOUND SYSTEMS, INC.

Box 770

Hopkinton, New Hampshire 03229

TEL (603) 224-0229

FAX (603) 746-4662

December 30, 1996

Brian E. Duffy
T.F.H. Architects
100 Commercial Street
Portland, ME 04101

**RE: Relocating P.A. Speakers and Poles
at the Hadlock Baseball Field**

Dear Brian:

This is a brief overview of the proposed new locations for the First Baseline Grandstand P.A. Speakers.

By moving the two existing speaker poles to the top edge of the proposed grandstand expansion (approx. 21' closer to Park Ave. at the 7 row addition and 15' at the 5 row addition), little or no difference should be perceived on Park Avenue. Using two horns per pole, focused in a wide pattern should allow for a reduction in volume (vs. the existing single high volume horns), while offering greater intelligibility for the fans. In addition, these positions will make it possible to focus the horn "hot spots" into the grass of the infield to help keep the sound in the park.

Please feel free to call if you have further questions.

Sincerely,



Chris Gardner

THE 1977 SEASON

PROPOSED COVERED PICNIC SHELTER - REFER TO ARCH. DWG'S.

PROPOSED CRUSHED STONE TRENCH DRAIN & PERF. PIPE

PROPOSED 6" PVC PIPE

INSTALL CONCRETE SLABS AROUND EXISTING CONCRETE PADS

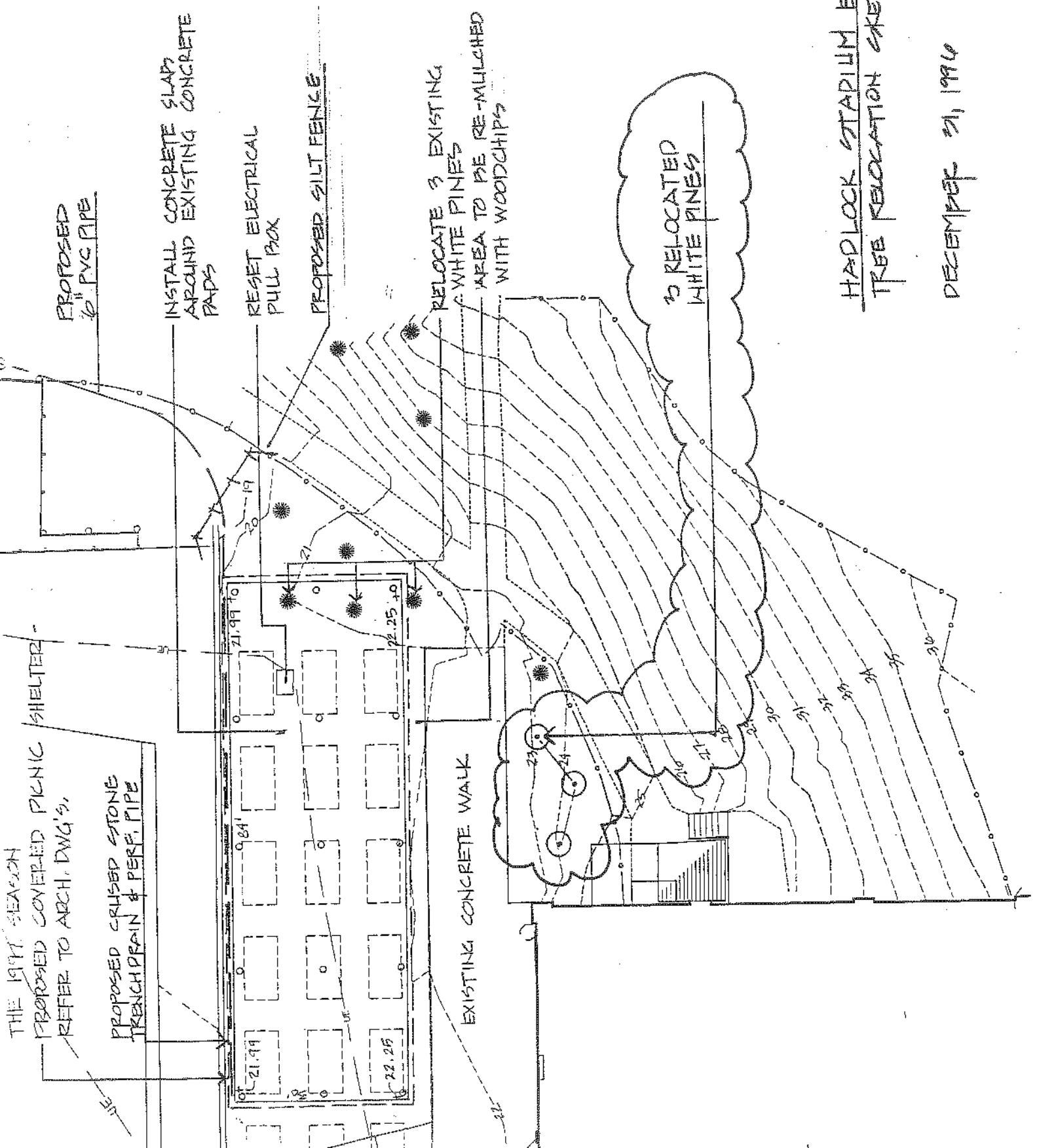
RESET ELECTRICAL PULL BOX

PROPOSED SILT FENCE

RELOCATE 3 EXISTING WHITE PINES AREA TO BE RE-MULCHED WITH WOODCHIPS

3 RELOCATED WHITE PINES

EXISTING CONCRETE WALK



HADLOCK STADIUM EXPANSION
TREE RELOCATION SKETCH

DECEMBER 21, 1996

City of Portland Planning Department

City Hall
389 Congress Street, 4th Floor
Portland, Maine 04101
FAX NUMBER: 756-8258

FAX TRANSMISSION COVER SHEET

To: FRANK LATORRE

Fax #: 874-8130

of Pages: 3

From: R. KNEWLAND

Date: 1/16/97

RE: LOTTER OUTLINING HADLOCK PROVINCE

MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

January 15, 1997

Mr. Richard Knowland, Senior Planner
City of Portland Planning Department
389 Congress Street
Portland, Maine 04101

RE: HADLOCK STADIUM

Dear Rick:

This correspondence is in regards to our conversation concerning the Portland Sea Dogs request to phase some of the proposed expansion plans recently approved by the City. As you know, the proposed expansion plans are part of a long range plan for future growth of the stadium. Due to those plans, this initial phase must be structurally designed to accommodate those future plans. The development cost for all of the approved work has exceeded the anticipated budget for the 1997 season, therefore the Sea Dogs are requesting to phase in several improvements over the next several seasons. Timing of these phases obviously is dependent upon a capital improvement budget related to season profits.

The following project elements are to be phased:

- **Concession Kiosk** - This structure will be added at a future date which has not been determined at this time.
- **Chain Link Fence** - For immediate plans, the existing wood fence will be relocated along the new fence alignment as shown on the Site Plan and will also enclose the area around the kiosk following the roof line footprint of the proposed concession kiosk. New fence will be considered at the time of construction for the kiosk. The entry gates shall be installed as shown on the plan, however, at this time the original chain link gate assembly will be used.
- **Electric and Air Conditioner Enclosure** - This area will be fenced in as indicated on the plan, however, until the new fence is selected the enclosure will be the same chain link mesh as the entry gates.
- **Enclosed Storage Area** - For the present time the enclosed storage area below the grandstand is to be eliminated from the project and will be included for construction in a later phase.

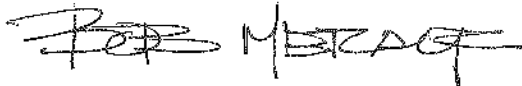
Mr. Richard Knowland

Page 2

- **Concrete Paving** - The new concrete paving in the area below the new grandstand seating will be delayed until future development of the kiosk. The existing bituminous paving will remain and the unpaved portion under the former ramp will receive new bituminous paving. Due to the asphalt plants production season and the opening day schedule, this area will have a stone dust surface until the Sea Dogs are on a spring road trip, then paving will occur.
- **Picnic Shelter** - Due to the winter construction season and the inability to reuse the existing concrete pads as planned, this area will be surfaced with bark mulch for the 1997 season. The concrete slab will be poured during the construction phase for the picnic area grandstand seating.

We have enclosed an updated Site Plan that has been annotated to reflect these changes. Should you have any questions or comments, please do not hesitate to contact our office.

Sincerely,
Mitchell & Associates



Robert B. Metcalf

Enclosure

cc: Charles Esbach
Mike Fagerson
Brian Duffy

RECEIVED

FEB 24 1993

CITY OF PORTLAND, MAINE
ENGINEERING DIVISION
M E M O R A N D U M

PORTLAND PLANNING OFFICE

TO: Melodie Esterberg, Planning Coordinator
FROM: *BAS* Bruce Sherwood, Project Engineer
DATE: February 23, 1993
SUBJECT Hadlock Stadium Drainage Analysis

I have recently completed a stormwater analysis of the impact of the proposed Hadlock Stadium project as shown on the Site Plan dated January 1993. This analysis was done in accordance with the city of Portland Stormwater Management Standards and the TR-55 method for small watersheds, and the calculations are attached. The entire 22 acre site was included in my review and the predeveloped conditions are prior to the ice arena being built. The following assumptions were made in performing the calculations.

1. Very hard compacted gravel used in parking lots is considered an impervious surface.
2. Hinckley (Hlb) and Cut and Fill (Cu) soils are assumed to be Soil Group C instead of the Soil Group A the SCS soil survey classifies them as. This is based on soils analysis recently done for this project.

The results of the calculations revealed a very small increase in runoff and a summary is as follows:

	Predeveloped	Post-developed
2 year storm	7.9 cfs	7.9 cfs
10 year storm	28.2 cfs	29.1 cfs
25 year storm	37.1 cfs	38.0 cfs

Conclusion:

Since the increased runoff from the proposed site is less than 1 cfs, there is no need for any on site storage and the design shown on the Site Plan is adequate.

Also taken into account is the City's Douglass Street Outfall project which is currently underway upstream of the Stadium site. This project, when completed in July, will be removing about 40 cfs of stormwater from the Almshouse sewer. This will significantly reduce the frequency and volume of CSO discharges in the Almshouse sewer.

BAS/jmd
pc: John Rague, Senior Technician

CITY OF PORTLAND

MEMORANDUM

TO: Bob Ganley, City Manager
FROM: John Peverada, Parking Manager J.P.
DATE: July 30, 1997
RE: Sea Dogs Shuttle

When we met late last winter with Charlie Eschbach and Frank LaTorre to discuss the continued operation of the Sea Dogs shuttle bus, we agreed that ridership would have to increase to justify this service. Through the marketing efforts of the Sea Dogs and Metro, we have made great strides in that area, as illustrated in the attached three month comparison of the cost and ridership of the Sea Dogs shuttle bus for April thru June, 1996 vs. 1997. The number of "Revenue Riders" is up 3,510 or 82.7%, and the net costs are \$2,926.00 less for the same period.

We met yesterday to discuss the trials and tribulations of the shuttle thus far in the season, and concurred that overall things are going well. There have been very few complaints from the neighborhood or shuttle passengers; however, an impasse may be approaching. We have heard that the new owners of the Ramada Inn (Double Tree) have plans to expand the hotel onto the lot at the rear of the building and to construct a Wendy's hamburger restaurant at the front of the lot. If these projects are completed, there is a strong possibility that we will have to find another shuttle lot location prior to the start of the next season. Our immediate reaction is to negotiate an agreement with U.S.M. for the Marginal Way lot; however, before we explore new locations, we will be requesting a meeting with you to see what your opinion is. Basically, does the City want to continue to subsidize the shuttle, and if so, to what extent and in what manner?

At the end of the season in September, I will compile a final report, the committee will review the data, the situation with Ramada and probably briefly sit down with you to discuss the future direction of the shuttle.

cc: Charlie Eschbach, General Manager, Portland Sea Dogs
Mike Gillogly, Director of Sales and Marketing, Portland Sea Dogs
Frank LaTorre, Director, Public Assembly Facilities
John Tibbetts, Operations Manager, METRO
Philip Chin, Marketing Director, METRO
Joseph Gray, Director of Planning & Urban Development
Gloria Thomas, Department Head



CITY OF PORTLAND

January 13, 1997

Mr. Robert Metcalf
Mitchell and Associates
70 Center Street
Portland ME 04101

Re: Hadlock Field, 271 Park Avenue

Dear Mr. Metcalf:

On January 13, 1997 the Portland Planning Authority granted approval of an amendment to the Hadlock Field site plan. The approval is subject to the following conditions:

1. The final design and material of all fencing shall be reviewed and approved by the Planning Department.
2. Prior to site construction of the new picnic area grand stand, the existing vegetation requiring removal shall be transplanted at the expense of the Sea Dogs at a locations determined by the City Arborist.
3. The three white pines adjacent to the picnic area shall be transplanted at the expense of the Sea Dogs at a location determined by the City Arborist.

The approved amendments to the site plan include expansion of existing grand stand seating along the first base line behind the Sea Dogs dug out, construction of a kiosk, construction of a picnic shelter along the right field line, and an expansion of picnic area grandstand seating (from 128 seats to 319 seats) and other revisions specified in the submitted site plans.

This letter approves revisions to the Hadlock Field site plan including local review under the site location of development law, originally approved by the Planning Board on March 23, 1993.

Please note the following provisions and requirements for all site plan approvals:

1. The site plan approval will be deemed to have expired unless work in the development has commenced within one (1) year of the approval or within a time period agreed upon in writing by the City and the applicant. A one year extension may be granted by this department if requested by the applicant in writing prior to the expiration date of the site plan.

O:\PLANDEVRE\PROJECTS\272PRKAV\APPLTR.JMD

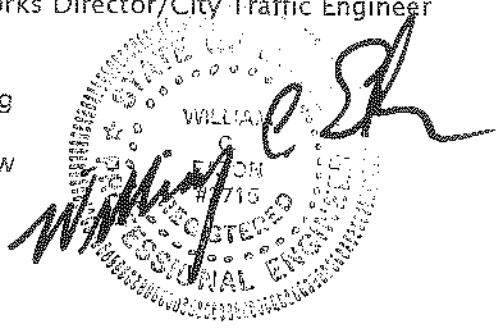


To: William J. Bray, P.E., Portland Deputy Public Works Director/City Traffic Engineer
 Mary Ann Theriault, Traffic Project Engineer

Fm: William C. Eaton, P.E., Eaton Traffic Engineering

Re: Hadlock Field Traffic/Parking Study Peer Review

Dt: February 11, 1993



Per your request, I have reviewed the Hadlock Field Traffic Study (January 1993) and the Hadlock Field Parking Study (August 1992), prepared by the City of Portland Public Works Department and Traffic Engineering Division. My findings with regard to both studies are summarized as follows:

Hadlock Field Traffic Study

Approach and Scope of Study

The traffic study defines the primary impact area of the proposed Ballpark as being the intersections of Park Avenue @ Deering Avenue and Park Avenue @ St. John Street. At the request of the Portland Planning Board, the intersection of Congress Street @ St. John Street has been added to the "impact area". The rationale implicit in the report for identifying these locations as the primary impact area is based upon the fact that both original locations are in immediate proximity to both Hadlock Field and primary parking areas available to spectators. In addition to this basic consideration, there are several other factors to be considered:

- The arrival and departure times for traffic associated with ballgames will occur during "off-peak" traffic flow periods - for weekday games traffic will arrive at 6:00 - 7:00 PM, and depart after 8:00 PM. Along Park Avenue, weekday peak traffic flows generally begin at 3:00 - 3:15 PM and decline after 5:00PM. On weekends, peak traffic volumes overall tend to be less pronounced than that of weekdays since much of the work-trip traffic associated with weekday peak traffic flow periods is not present. Because the peak traffic generation associated with the ballpark occurs in off-peak periods the traditional approaches used for traffic impact analyses are not necessary, and professional judgement should be used to define the appropriate impact area. In this case it appears that selection of intersections in the immediate vicinity of the ballpark (and its associated parking areas) that are known to be heavily loaded during "normal" peak traffic periods as the primary impact area is appropriate. If analysis of these locations indicate potential problems, this would justify expansion of the primary impact area - if not, this would indicate that the probability of problems further from the site is extremely unlikely;
- The location of Hadlock Field and associated parking facilities within the highway system of the City of Portland provides a multitude of access routings. Because of this condition, traffic approaching and departing the area will essentially be diffused and not concentrate on any single facility. Given the

location of the parking supply for the Field, it appears clear that any concentration of traffic that does occur will be on Park Avenue in the vicinity of Deering Avenue and St. John Street;

Traffic counts performed by the City of Portland at the intersections of Park Avenue @ Deering Avenue and Park Avenue @ St. John Street on two Saturdays in November - one representing conditions with a University of Maine football game at Fitzpatrick Field and one without any special event - indicate that 30 - 50 percent of the vehicles associated with the game passed through these two intersections after the game (when concentration of demand is most likely). The remainder of the traffic associated with the game had numerous route alternatives and is unlikely to have concentrated on any single facility to the extent measured on Park Avenue.

In considering the above factors, the selection of the two most proximate intersections to Hadlock Field and its parking supply is appropriate for analysis of impacts associated with games at the proposed ballpark.

When considering the traffic impacts associated with an activity such as a baseball game, it must be remembered that traffic is generated to and from the parking supply associated with the park, not the park itself. It is sometimes difficult to project which parking supplies will be used, and what routes will be used to access the parking supply. The Hadlock Field Traffic Study essentially used a University of Maine football game, which is estimated to generate about 2/3 the traffic and parking demand of a maximum attendance ballgame, as a "model" to determine projected traffic and parking demand. The use of such an approach allows direct observation of individual driver selection of travel route and parking area, and as such, is to be preferred over the normal judgement and logic based assignment of traffic and parking demand that is typically employed in such studies (and where the opportunity for such an approach is not present). The Hadlock Field study essentially expands upon the traffic and parking demand associated with the University of Maine football game by increasing the incremental impact over "normal" (i.e. non-game) conditions by 50± percent. This approach seems both logical and reasonable in terms of predicting the impact of a maximum attendance ballgame.

Finally, it must be considered that traffic impact studies look at worst case or "design" conditions in terms of traffic and parking impacts, on the logic that if the design condition can be accommodated reasonably, then there should be no problem addressing a normal "less than design" condition. The importance of this consideration is that the impacts predicted for the design condition will not occur during "normal" conditions, and that the design condition will occur only 5 - 6 times during the season.

Mobility Assessment

Based upon the experience of other minor league ballparks, the Hadlock Field Traffic Study assumes that the "design" condition will reflect traffic generated for 95% of the available seating in the park (5700 persons). This condition involves a demand of 1693± vehicles added to the normal traffic flow on the streets in the vicinity of the site, and a demand for 1693± parking spaces. As noted in the previous section, the traffic demand at the intersections evaluated was estimated by factoring the traffic increases observed before and after the University of Maine football game by 50± percent, and adding this increase to "normal", non-game traffic. Capacity analysis was performed for both pre- and post-game conditions, with acceptable conditions (Level of Service "D" or better) predicted for both pre- and post-game

traffic flow - with some modification to signal operation. In reviewing the analyses for pre- and post-game conditions it was felt that the projected pre-game assessment of intersection Level of Service was reasonable, because spectators will tend to start to arrive one hour before the game to get good seats, find a convenient parking space, et cetera. After the game, however, the departure of spectators is not likely to be as leisurely. The parking study associated with this proposed ballpark indicated that all parked vehicles exited the lots very quickly. This indicates that basing the analysis of the intersections in the primary impact area on a peak one hour traffic demand may understate the impact of the "surge" of traffic that would be expected immediately after the game. Accordingly, the analyses performed in the study for the post-game condition was re-evaluated assuming that a "surge" of traffic would occur at both of the intersections in the primary impact area. Essentially this analysis doubles the peak demand impact associated with the ballpark in terms of its effect on operations and Level of Service at the Park Avenue @ Deering and Park Avenue @ St. John Street intersections. A summary of the results of this re-evaluation is shown below:

COMPARISON OF LEVEL OF SERVICE FOR DEPARTING TRAFFIC

LOCATION	LEVEL OF SERVICE / DELAY (SECONDS/VEHICLE)			
	WEEKDAY HADLOCK STUDY	WEEKDAY RE-EVALUATION	WEEKEND HADLOCK STUDY	WEEKEND RE-EVALUATION
PARK @ DEERING	C / 16.3	C / 19.1	C / 18.2	C / 23.3
PARK @ ST. JOHN	C / 21.2	C / 21.3	D / 25.9	D / 37.6

As can be seen, the impact of the "surge" is minimal on the weekday condition, but is more pronounced on the weekend - particularly for the Park Avenue @ St. John Street intersection. At the Park/St. John intersection, the most recent timing of the traffic signal was used, which includes a more efficient, semi-actuated operation which is easily modified by the City Traffic Engineering Division. Overall, the operation of both intersections during the "surge" of traffic immediately following the game should be within acceptable limits. It should be noted that there may be short periods of several minutes duration during the surge that will reflect demand that exceeds even the surge volumes used in the evaluation. This is not unusual and in fact occurs on a relatively normal basis at most intersections during peak flow periods. This condition is simply one that will cause a short term period of congestion that will disappear as normal flow is established.

At the request of the Portland Planning Board, the intersection of Congress Street @ St. John Street was included in the primary impact area. The City of Portland Traffic Engineering Division conducted traffic counts at this location for a Friday evening and Saturday afternoon, and performed capacity analysis for both projected pre- and post-game traffic conditions. The results of this analysis indicate a projected Level of Service "B" condition, with average vehicle delay in the range of 10.9 to 12.8 seconds. Re-evaluation of the Level of Service for the post-game period surge condition indicates a projected Level of Service "C" condition, with average vehicle delay of 15.0 seconds on a design weekday (Friday evening), and 18.1 seconds on a design weekend (Saturday afternoon).



Alternate Traffic Modes

While the mobility assessment review in the previous section assumes, as a worst case scenario, that all of the 5700 spectators attending a "design" event will arrive by automobile, it is very likely that a significant proportion of the attendance will arrive on foot from the surrounding residential neighborhoods, by bicycle, or by Charter or Metro Bus. The Hadlock Field Study indicates that bus drop-off areas will be provided in the vicinity of the site and bike racks will be available on-site. A key consideration is the fact that all persons attending a game, with the exception of those who park in Hadlock Field Parking Lot "B" or arrive by bus, will approach the ballpark on foot. Pedestrian traffic considerations are thus quite important. The presence of a wide sidewalk on Park Avenue addresses much of the pedestrian need, and it is felt that the area in the immediate vicinity of the entrance is the main concern. At this location, vehicles (autos and buses), bicycles and pedestrians will converge, and it is critical that a relatively high degree of control and direction be exercised. Attendants will be stationed in this area to direct traffic and pedestrians to minimize any congestion. In addition to these measures, it is recommended that actions be taken to positively control pedestrian crossings of Park Avenue in the vicinity of the field. The report notes that pedestrian signals and timing are provided at the signalized intersections along Park Avenue. The nearest traffic signal to the field entrance is Park Avenue @ St. John Street. It seems unlikely that persons taking advantage of private parking and/or the Maine Medical Center parking garage in the Gilman Street area will travel to that intersection to cross Park Avenue. Accordingly it is felt that pedestrians travelling to the field from the south side of Park Avenue should be kept on the sidewalk on that side of the street and crossing should be limited to a single crosswalk directly in front of the main entrance to the field. Use of this crosswalk should be controlled by a crossing guide. The crosswalk itself may be temporary in nature, using traffic cones and movable signs, or may be permanent. The crosswalk area should also be well lighted to alert drivers on Park Avenue of crossing activity, since most weekday games will end after sundown.

Safety

The Hadlock Field Traffic Study provides a detail review of safety conditions in the vicinity of the Field, and recommends a number of hazard mitigation actions to address problems, a number of which have already been programmed or implemented. Review of these recommendations indicates that all are appropriate. Two areas where improvements are not recommended are Deering Avenue in the vicinity of Cumberland Avenue, and Marston Street at Congress Street and Park Avenue. In the former case, most of the accidents (11 of 18) occur during poor environmental conditions (snowy or icy streets) and involve collisions with parked vehicles. The ability to correct this problem is severely limited. At the latter locations, the extremely heavy volume of traffic exiting from I-295 northbound and wishing to travel west on Congress Street is a major contributing factor to the accident problem, and as the Study indicates, is best addressed as part of an I-295 corridor study, which may find better ways to address this travel pattern demand.

Hadlock Field Parking Study

Parking demand for a "design" event of 95 percent attendance is projected to be 1693 spaces. This projection is based upon observation of parking usage during a University of Maine football game in November 1992 which had attendance of approximately 2/3 of the projected attendance at a "design" condition baseball game. Corroboration of this projection uses statistics provided by the Professional Baseball Association, which indicate that 85 percent of attendees will arrive by automobile, with an average vehicle occupancy of 3 persons. Compari-

**TRANSMITTAL
LETTER**

AIA DOCUMENT G810

PROJECT: *RIGHT FIELD PLATFORM
PORTLAND SEA DOGS*

ARCHITECT'S
PROJECT NO: *9701*

DATE: *3.20.97*

TO: *RICK KNOWLAND
PLANNING DEPT.
ATTN: PORTLAND CITY HALL*

If enclosures are not as noted, please inform us immediately.

If checked below, please:

- Acknowledge receipt of enclosures.
- Return enclosures to us.

WE TRANSMIT:

- herewith under separate cover via _____
- in accordance with your request _____

FOR YOUR:

- approval distribution to parties information
- review & comment record
- use _____

THE FOLLOWING:

- Drawings Shop Drawing Prints Samples
- Specifications Shop Drawing Reproducibles Product Literature
- Change Order _____

COPIES	DATE	REV. NO.	DESCRIPTION	ACTION CODE
<i>4</i>	<i>3.19.97</i>	<i>2</i>	<i>PROPOSED RIGHT FIELD PLATFORM</i>	<i>E</i>

ACTION CODE A. Action indicated on item transmitted D. For signature and forwarding as noted below under REMARKS
 B. No action required E. See REMARKS below
 C. For signature and return to this office

REMARKS *RICK, THIS IS THE PLATFORM I DISCUSSED WITH YOU.
 CHARUE ESHBACH OF THE SEA DOGS WILL MEET
 WITH BOB SANLEY & FRANK LATORRE TO REVIEW
 THIS PLAN. PLEASE CALL ME WITH ANY QUESTIONS*

COPIES TO: (with enclosures)

PORTLAND SEA DOGS

-
-
-
-
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BY: *Brian E. Duffy*

MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

3/15/97
MCA
MCA

May 15, 1997

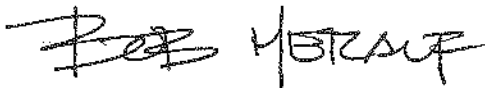
Mr. Michael Claus, P.E.
City of Portland Public Works
55 Portland Street
Portland, Maine 04101

RE: HADLOCK STADIUM IMPROVEMENTS

Dear Mike,

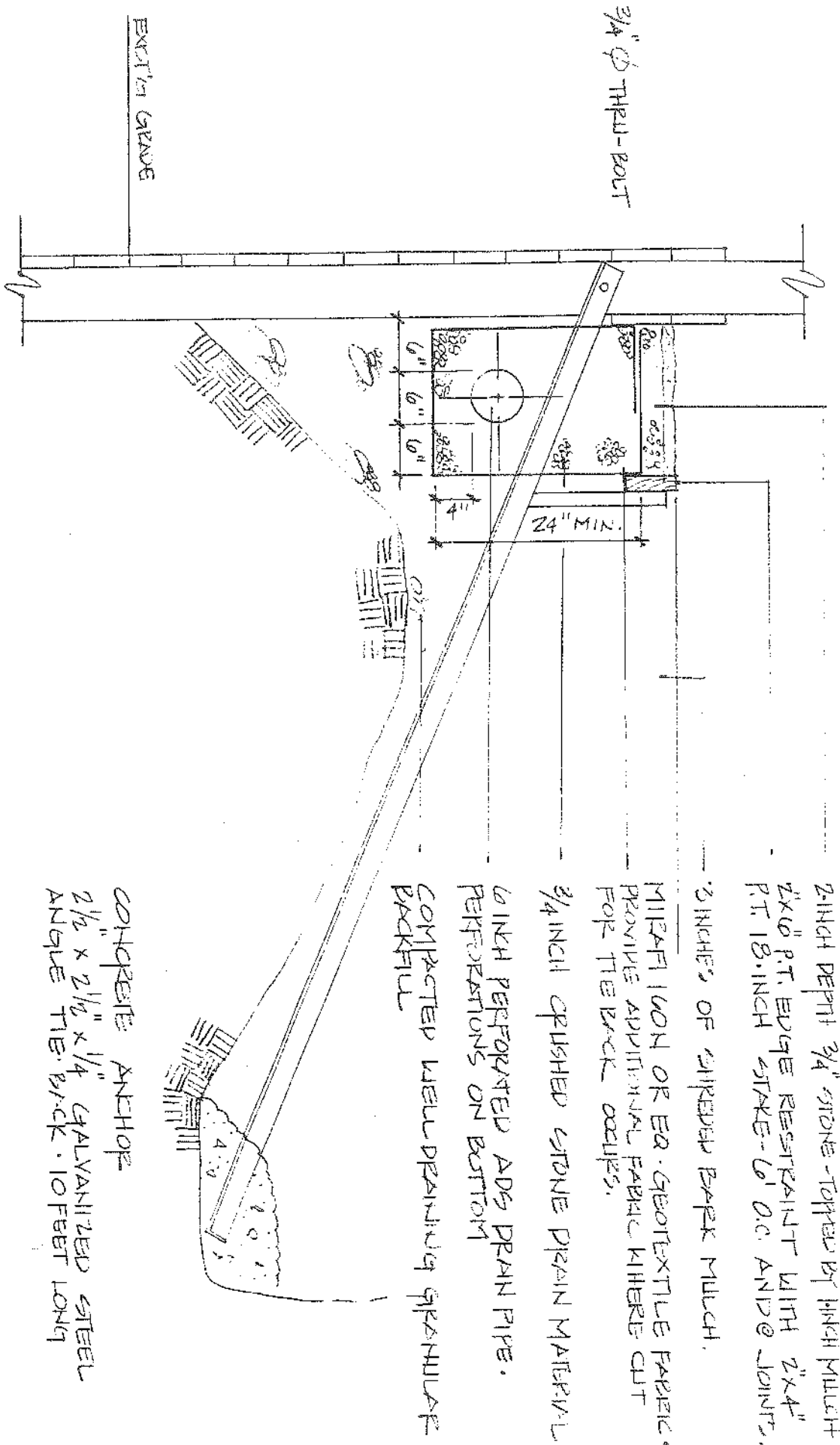
At your request we have prepared a series of sketches and details to construct the extension of the retaining wall at Hadlock Stadium for your review. As we discussed, I spoke with Rick Knowland, Senior Planner in the City Planning Department concerning these improvements. Rick indicated they would require staff review, however he did not see this being a major issue or time requirement. Please review these sketches and comment on any changes you may wish to make. Once you have approved the plans we can submit them to the Planning Department for staff review. Upon staff approval you may proceed with the improvements. Should you have any questions, please do not hesitate to call me.

Sincerely
Mitchell & Associates



Robert Metcalf

Enclosure



SECTION THRU RETAINING WALL
 NOT TO SCALE

2-INCH DEPTH 3/4" STONE-TOPPED BY 1-INCH MULCH
 2X4" PT. EDGE RESTRAINT WITH 2X4"
 PT. 18-INCH STAKE-6' O.C. AND @ JOINTS.

3-INCHES OF SHREDED BARK MULCH.

MIXED LION OR EQ. GEOTEXTILE FABRIC.
 PROVIDE ADDITIONAL FABRIC WHERE CUT
 FOR THE BACK OCCURS.

3/4 INCH CRUSHED STONE DRAIN MATERIAL

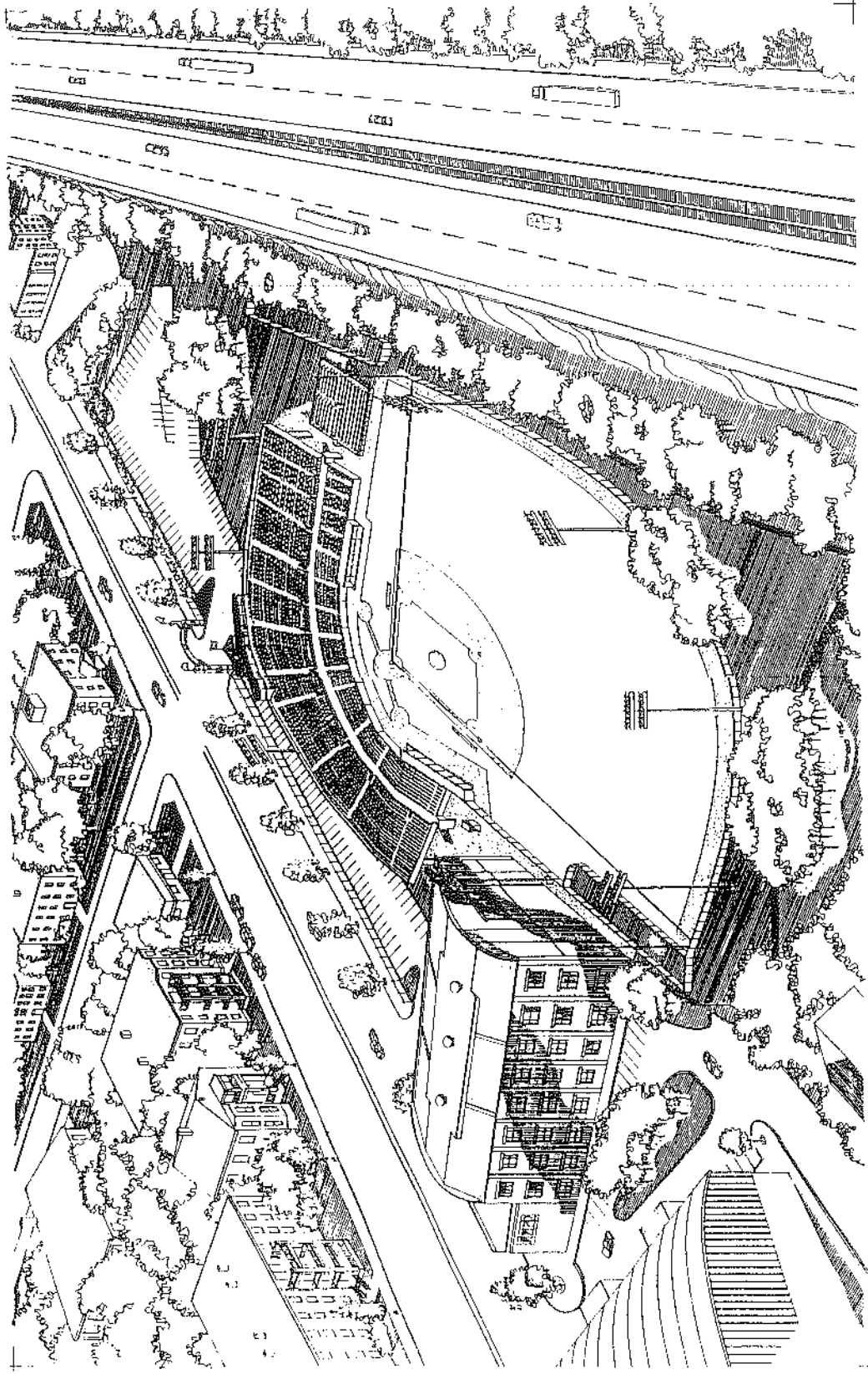
1/4 INCH PERFORATED ADS DRAIN PIPE.
 PERFORATIONS ON BOTTOM

COMPACTED WELL DRAINING GRANULAR
 BACKFILL

CONCRETE ANCHOR
 2 1/2" x 2 1/2" x 1/4" GALVANIZED STEEL
 ANGLE TIE-BACK-10 FEET LONG

PERIMETER DRAIN & TIE-BACK
HADLOCK FIELD

MITCHELL & ASSOCIATES,
 MAY 15, 97



AERIAL PERSPECTIVE
AT HEIGHT OF 1000 FEET

PROPOSED STADIUM FOR HADLOCK FIELD
PORTLAND, MAINE

William E. Whited, Inc.
Professional Engineer
Registered Architect

One Union Wharf
P.O. Box 127 - Portland, Maine 04112

(207) 774 2135

WILLIAM E. WHITED, INC., P.E., P.A., MEMBER
PORTLAND, MAINE

ERNO A. SANDER, AIA, REGISTERED
SEABROOK, MAINE

SCALE: 1" = 40' DIST. 1/4" = 100' DATE: 11/15/74 SHEET NO. 1 OF 1

3573

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12. Miscellaneous
13. Traffic Memorandum
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CITY OF PORTLAND

MEMORANDUM

TO: PORTLAND CITY COUNCIL AUGUST 14, 1992

FROM: ROBERT G. GANLEY, CITY MANAGER

SUBJECT: PROPOSED AA BASEBALL FRANCHISE FOR PORTLAND

The City Council will be meeting on Monday, August 17th at 7:30 p.m. to review in workshop the proposed AA baseball franchise at Hadlock Field. Public Hearing notice has also been given for Monday, August 24th at 7:30 p.m. to consider the option of spending up to \$1.5 million to make the required improvements to secure this franchise.

OVERVIEW

At the beginning of June, 1992 the City of Portland was approached by Mr. Dan Burke who was applying for one of the newly created AA minor league franchises. Mr. Burke indicated to the Mayor and City Manager that he was interested in locating the team in Portland and subsequent discussions began regarding that possibility. The City Council met with Mr. Burke and subsequently passed a resolution indicating interest in having the franchise located in the City and asking the City Manager to provide more information before a final commitment was made. We are now at the point where the information is being returned to the City Council by the City Manager and a definitive position needs to be taken by the City in advance of the selection process being made at the beginning of September.

HADLOCK FIELD

The requirements of the Eastern League call for a 6,000 seat facility. Presently there are about 1,000 seats at Hadlock Field and the estimates that have been put in place before the City Council call for expanding that capacity to 6,000. There are also requirements for concession areas, locker room facilities, office space and obviously a high quality baseball field. It is my determination in discussing this particular issue with League officials, Mr. Burke, municipal officials in other communities that have ball teams, private engineers and my own staff that Hadlock Field can be renovated and expanded to meet the requirements of the Eastern League for a total cost of \$1.5 million. In the report provided to the City Council, a detailed cost breakdown has been provided.

In order to make these improvements for the least amount of money, I am proposing that the City forces over the next 18 months construct many of the ancillary facilities such as restrooms, press box and concession stands and that the City hire through private contract the grandstand seating, handicapped accessibility and other major components of the project. It's also necessary to utilize

the existing facilities of the Portland Exposition building for clubhouse and locker rooms during the season. The major facility improvement would be the expansion of the seating capacity. It is anticipated that there will be approximately 1,400 box seats and approximately 3,800 grandstand bleacher seats and that the existing bleacher seats would be relocated to fill out the requirement for 6,000 seat capacity. The field itself is in excellent condition and would require very little upgrading. The lighting for the facility is in our estimation very close to what would be required for the Eastern League, however it may require some changes after it has been analyzed by League officials. The concession, restroom and ticket areas ~~would be constructed under the grandstand and would be included as part of the facility.~~ The concession area would be constructed by the City but would be equipped by the team. City crews would construct the media facilities and the existing locker rooms in the exposition building would be used by the ballclubs for a period from approximately April 1st to September 15th. Any additional scoreboard fencing or private boxes would be the responsibility of the team and are not included in this cost estimate.

FINANCING

It is of primary importance that the City, the private sector and the ballclub work to finance as much of this stadium renovation as possible and that the impact on the taxpayer of the City of Portland be minimized. I am providing to the City Council two different scenarios which it will want to consider in its deliberations. The proposal that is being forwarded for financing would incorporate the Hadlock Field renovations into the 1993 capital improvement program and would keep that capital improvement program within its budgeted and program amount.

As the City Council knows, the City projects its capital spending program over a ten year period and bases its budgetary decisions on the individual spending in any particular year. The 1993 projections call for the City to spend \$9.5 million in total capital improvements. My proposal calls for this particular facility to be partially financed within that budgeted amount, approximately \$600,00 to \$800,000, depending on which scenario the City Council chooses. Therefore, while the project standing alone may have some debt service impact on the City taxpayer, I am entirely positive that the City will spend \$9.5 million in capital improvements next year, regardless of whether or not we renovate Hadlock Field or have a baseball team. Therefore, this proposal can be viewed as whether the glass is half full or half empty. It can be argued that this facility would have a net cost to the City of approximately \$60,000 per year over a ten year period and significantly less than that if the projections are factored beyond the ten year period. However, it can also be argued that there is no additional cost because the City would incur additional debt on other projects to stay within the \$9.5 million capital improvement program in 1993 and therefore it is not costing the taxpayer any more than they would normally be spending in next year's debt service budget.

The scenarios for financing the improvements would call for a \$200,000 private sector contribution, allocating \$500,000 of surplus from closed out CIP projects in the 1990-1992 timeframe, and utilizing \$600,000 to \$800,000 from the 1993 CIP. The difference in the two scenarios is that Mr. Burke has indicated he would pre-pay approximately four years worth of

rent on the facility if that would help finance the project. Therefore one of the scenarios calls for a \$200,000 contribution from Mr. Burke and the other one doesn't. Obviously the cash flow will vary in the two scenarios because then the facility would be rent free for the first four years. The City Council will note from the financial section of this report that there really is not a whole lot of difference in either of the two scenarios over the ten year period.

LEASE AND TEAM RESPONSIBILITIES

The proposed arrangement calls for the ballclub to enter into a ten year lease for the facility and that lease could not be broken regardless of the ownership situation. As we have discussed with Mr. Burke, he is willing to stipulate in the lease that in the event of his or his wife's demise that if he did not leave the team to his immediate children, the City would have one year to put together local ownership to purchase the ballclub. In any event, whoever owned the club would have to live with the requirements of the lease for ten years. It is my opinion that the best way to ensure that the baseball team stays in the community is to require that a strong and hard lease be negotiated, and that is what keeps most sports teams in place. Teams usually do not or cannot move until their lease is expired. For example, the New England Patriots would have left Foxboro, Massachusetts in the past few years if they could have gotten out of the lease on the stadium. The San Francisco Giants are only leaving San Francisco in 1993 because the lease on the facility has run its course and they are now free to leave. The Giants have been threatening to leave for years, but could not get out of the lease they had on Candlestick Park.

The ballclub would be responsible for all of the ballpark operations, including but not limited to security, ushers, clean up, ticket takers, and everything but maintenance of the field. Mr. Burke feels that the City crews do a wonderful job on the field and has increased his proposed rent from \$50,000 to \$65,000 in acknowledgement of the City maintaining the baseball field. It is our understanding from researching other communities that most of them make some contribution towards maintenance or at least acknowledge that upkeep of the field is not solely the ballclub's responsibility, especially with other uses that are allowed on it. There is no standard for how rental fees are determined. Some communities go with flat fees and the ballclub keeps all of the revenues, and other communities have gone with lower fees and a sharing of the revenues over a certain threshold. Again, it is my opinion in approaching this that we ought to keep it as simple as possible and I would prefer to have a stable rental fee year in and year out and not be as subject to the revenue fluctuations depending on attendance, team performance, and weather. The lease would also call for the facility to be utilized by the City when the ballclub is not using it. It is clearly stipulated that the Portland High School baseball team will have access to that facility and the only inconvenience or relocation that I can see being required would be the amount of time the facility was available for practice time.

EXPOSITION BUILDING

The Exposition Building is crucial to our being able to bring this project in at a reasonable cost and the facilities there are equal to or better than most of the facilities that our inspection team visited. Because of the timing of the Eastern League schedule, the conflict with the sports teams from the high schools will be minimized. Frank LaTorre, who is the Director of the Expo, feels that we can utilize the portion of the locker room facilities that would be necessary without damaging his operation. We will have to work very closely in scheduling to minimize the parking problems in the area, however, July and August are the slow periods of time for the Expo and in fact the Expo is generally closed during August for maintenance. It is my opinion that we really need to focus on the period from mid April until Memorial Day that needs the schedule coordination. The baseball club would need exclusive use of the home team locker room and clubhouse from April until early September and the visiting team would only need use of the locker room while it was in town. There are 3-4 other locker rooms in that area and Mr. LaTorre feels that the needs of the baseball team could be accomplished, especially since school is out during the bulk of the baseball schedule.

SUMMARY

It is impossible in a memorandum to outline all of the information that is included in the attached report. Therefore, I will not try to do that. I leave it to the City Council to peruse the report and ask the appropriate questions that the information provokes. I would also be happy to provide any additional reports that are necessary. My role in this has been to provide the information requested by the City Council and it is up to the City Council whether or not we have a baseball team. I will provide answers to the questions that the information provokes. I would also be happy to provide any additional reports that are necessary. My role in this has been to provide the information requested by the City Council and it is up to the City Council whether or not we have a baseball team.

Hadlock Field

Cost Estimates - Contract

Total Cost

Foundation	\$ 58,450
Superstructure	

Steel	109,035	
Deck	213,335	
Precast	90,810	
Railings, Sealant	65,140	
Dugouts	19,575	497,895

Grandstand Seating

3,844 seats w/backs	204,065	
1,432 box seats	84,465	288,530

Elevator	87,180
Stair Lift	14,745
A/E	<u>131,825</u>

Sub total \$1,078,625

Cost Estimates - City Construction

Media Booths	
Restrooms	
Concession Stands	
First Aid	
Security	
Ticket Booths	
Site Work	\$ 300,000

Contingency	<u>121,375</u>
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Total \$1,500,000

Maintenance costs for the field, provided by the City, are estimated to be \$35,000 per year. It is difficult to exactly quantify this number due to the other uses and normal upkeep not associated with AA baseball.

8-14-92

Funding Sources

Scenario One:

CIP 1993	\$ 800,000
90-92 CIP Surplus - Anticipated	500,000
Private Fundraising	<u>200,000</u>
	\$1,500,000

Scenario Two:

CIP 1993	\$ 600,000
90-92 CIP Surplus - Anticipated	500,000
Private Fundraising	200,000
Pre-paid Lease - 4 years	<u>200,000</u>
	\$1,500,000

Lease Provisions

Scenario One:

10 year lease

\$65,000 annual payment towards cost of renovation plus groundskeeping

Scenario Two:

10 year lease

\$15,000 annual payment years 1-4

\$200,000 pre-paid

\$65,000 payment years 5-10

14-Aug-92

11:10 AM

B6K

FINANCIAL PROJECTION
AA BASEBALL PROPOSAL
SCENARIO # 1

NEW BONDS ISSUED: 800,000

DATE	PRINCIPAL	INTEREST	BALANCE	DEBT SERVICE FISCAL YR	EST MAINT COST	10 YEAR LEASE PAYMENTS	NET COST TO CITY
01-Jan-93			800,000				
01-Jul-93		30,000	800,000				
01-Jan-94	40,000	30,000	760,000	100,000	35,000	65,000	70,000
01-Jul-94		28,500	760,000				
01-Jan-95	40,000	28,500	720,000	97,000	36,225	65,000	68,225
01-Jul-95		27,000	720,000				
01-Jan-96	40,000	27,000	680,000	94,000	37,493	65,000	66,493
01-Jul-96		25,500	680,000				
01-Jan-97	40,000	25,500	640,000	91,000	38,805	65,000	64,805
01-Jul-97		24,000	640,000				
01-Jan-98	40,000	24,000	600,000	88,000	40,163	65,000	63,163
01-Jul-98		22,500	600,000				
01-Jan-99	40,000	22,500	560,000	85,000	41,569	65,000	61,569
01-Jul-99		21,000	560,000				
01-Jan-2000	40,000	21,000	520,000	82,000	43,024	65,000	60,024
01-Jul-2000		19,500	520,000				
01-Jan-2001	40,000	19,500	480,000	79,000	44,530	65,000	58,530
01-Jul-2001		18,000	480,000				
01-Jan-2002	40,000	18,000	440,000	76,000	46,088	65,000	57,088
01-Jul-2002		16,500	440,000				
01-Jan-2003	40,000	16,500	400,000	73,000	47,701	65,000	55,701
01-Jul-2003							
TOTALS	400,000	465,000		865,000	410,599	650,000	625,599

ASSUMPTIONS:	FURTHER ASSUMPTIONS:
PRINCIPAL AMOUNT 800,000	BONDS WILL BE GENERAL OBLIGATION
INTEREST RATE, ANNUAL 7.50%	BONDS WILL BE FULLY TAX EXEMPT
TERM, YEARS 20	BOND RATING e Aa1
	COSTS OF ISSUANCE NOT INCLUDED
	STABLE BOND MARKET CONDITIONS
ISSUE DATE: 01-Jan-93	ANNUAL MAINTENANCE COST: 35,000
	INFLATION ON MAINT. COSTS, ANNUALLY: 3.50%
	NO PREPAID LEASE INVOLVED

14-Aug-92

11:10 AM

D6K

FINANCIAL PROJECTION
AA BASEBALL PROPOSAL
SCENARIO # 2

NEW BONDS ISSUED: 600,000

DATE	PRINCIPAL	INTEREST	BALANCE	BEST SERVICE FISCAL YR	EST MAINT COST	10 YEAR LEASE PAYMENTS	NET COST TO CITY
01-Jan-93			600,000				
01-Jul-93		22,500	600,000				
01-Jan-94	30,000	22,500	570,000	75,000	35,000	15,000	95,000
01-Jul-94		21,375	570,000				
01-Jan-95	30,000	21,375	540,000	72,750	36,225	15,000	93,975
01-Jul-95		20,250	540,000				
01-Jan-96	30,000	20,250	510,000	70,500	37,493	15,000	92,993
01-Jul-96		19,125	510,000				
01-Jan-97	30,000	19,125	480,000	68,250	38,805	15,000	92,055
01-Jul-97		18,000	480,000				
01-Jan-98	30,000	18,000	450,000	66,000	40,163	65,000	41,163
01-Jul-98		16,875	450,000				
01-Jan-99	30,000	16,875	420,000	63,750	41,569	65,000	40,319
01-Jul-99		15,750	420,000				
01-Jan-2000	30,000	15,750	390,000	61,500	43,024	65,000	39,524
01-Jul-2000		14,625	390,000				
01-Jan-2001	30,000	14,625	360,000	59,250	44,530	65,000	38,780
01-Jul-2001		13,500	360,000				
01-Jan-2002	30,000	13,500	330,000	57,000	46,088	65,000	38,088
01-Jul-2002		12,375	330,000				
01-Jan-2003	30,000	12,375	300,000	54,750	47,701	65,000	37,451
TOTALS	300,000	348,750		648,750	410,599	450,000	609,349

ASSUMPTIONS:

PRINCIPAL AMOUNT 600,000
INTEREST RATE, ANNUAL 7.50%
TERM, YEARS 20

FURTHER ASSUMPTIONS:

BONDS WILL BE GENERAL OBLIGATION
BONDS WILL BE FULLY TAX EXEMPT
BOND RATING @ Aa1
COSTS OF ISSUANCE NOT INCLUDED
STABLE BOND MARKET CONDITIONS

ANNUAL MAINTENANCE COST:

ISSUE DATE: 01-Jan-93 35,000

INFLATION ON MAINT. COSTS, ANNUALLY:

3.50%

PREPAID LEASE APPLIED TO
CONSTRUCTION COSTS

1992
CITY OF PORTLAND
TEN YEAR PROJECTED CIP BUDGET
(000's)

FISCAL YEAR	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	TOTAL	% TOTAL
TRANSPORTATION	3,292	2,355	1,500	1,500	3,700	4,400	4,500	2,500	4,800	4,700	33,247	29.2%
UTILITY/INFRASTRUCTURE	3,524	2,037	1,000	1,500	2,900	2,000	3,900	4,000	2,700	1,500	25,061	22.0%
PUBLIC FACILITIES												
A. buildings and garages	3,447	740	500	0	2,000	500	1,000	1,000	800	600	10,587	9.3%
B. schools	440	2,135	6,000	7,500	500	500	800	1,000	1,000	1,400	21,275	18.7%
C. jetport and waterfront	200	100	500	0	1,000	1,000	1,000	1,000	1,000	1,400	7,200	6.3%
PARKS												
A. parks	50	250	250	0	1,000	1,000	1,000	1,000	1,000	1,500	7,050	6.2%
B. golf	0	0	300	0	500	400	0	400	500	400	2,500	2.2%
EQUIPMENT	725	640	380	0	600	500	600	500	600	500	5,045	4.4%
DOWNTOWN IMPROVEMENTS	0	1,200	100	0	100	100	100	100	100	100	1,900	1.7%
reallocated funds	-3,638											
TOTAL	8,040	9,457	10,530	10,500	12,300	10,400	12,900	11,500	12,500	12,100	113,865	100.0%

**rounded to the nearest 1,000

AUG 11 1992

BOB GANLEY

CITY OF PORTLAND, MAINE
PARKS & PUBLIC WORKS DEPARTMENT

M E M O R A N D U M

TO: William J. Bray, Deputy Director

FROM: John P. Rague, Senior Technician

SUBJECT: Hadlock Field Site Plan

DATE: August 11, 1992

Please find attached a colored rendition of the proposed site plan for Hadlock Field.

This plan was developed by placing the grandstands sixty feet (60') from homeplate pursuant to league rules. The site plan maximizes onsite parking, segregates parking for ball players, umpires and club officials, provides fifteen (15) onsite handicapped parking spaces adjacent to the proposed elevator and further provides access and parking for the visiting club's bus.

I have included a "picnic area" per the recommendations of the Harrisburg and Reading club officials. The entrance to the park is defined by brick and block cavity wall structures accessible from both parking lots and with direct access from Park Avenue. My thoughts were to centrally control foot traffic into the park through an inner concourse which would house concessions, novelty stands, restrooms, beer concessions and the like. Included in Parking Lot "B", adjacent to the concession areas, would be areas where patrons could sit to eat and/or drink beverages.

Along the perimeter of the outfield fence, I have shown "billboard advertising" per your request.

I changed the grandstand layout slightly from that which Bill Whited had originally produced by relocating an entire "bay" of general admission seats from the rightfield line to the leftfield line due to sight constraints and then added two (2) new bays of "box seats" behind the right field dugout.

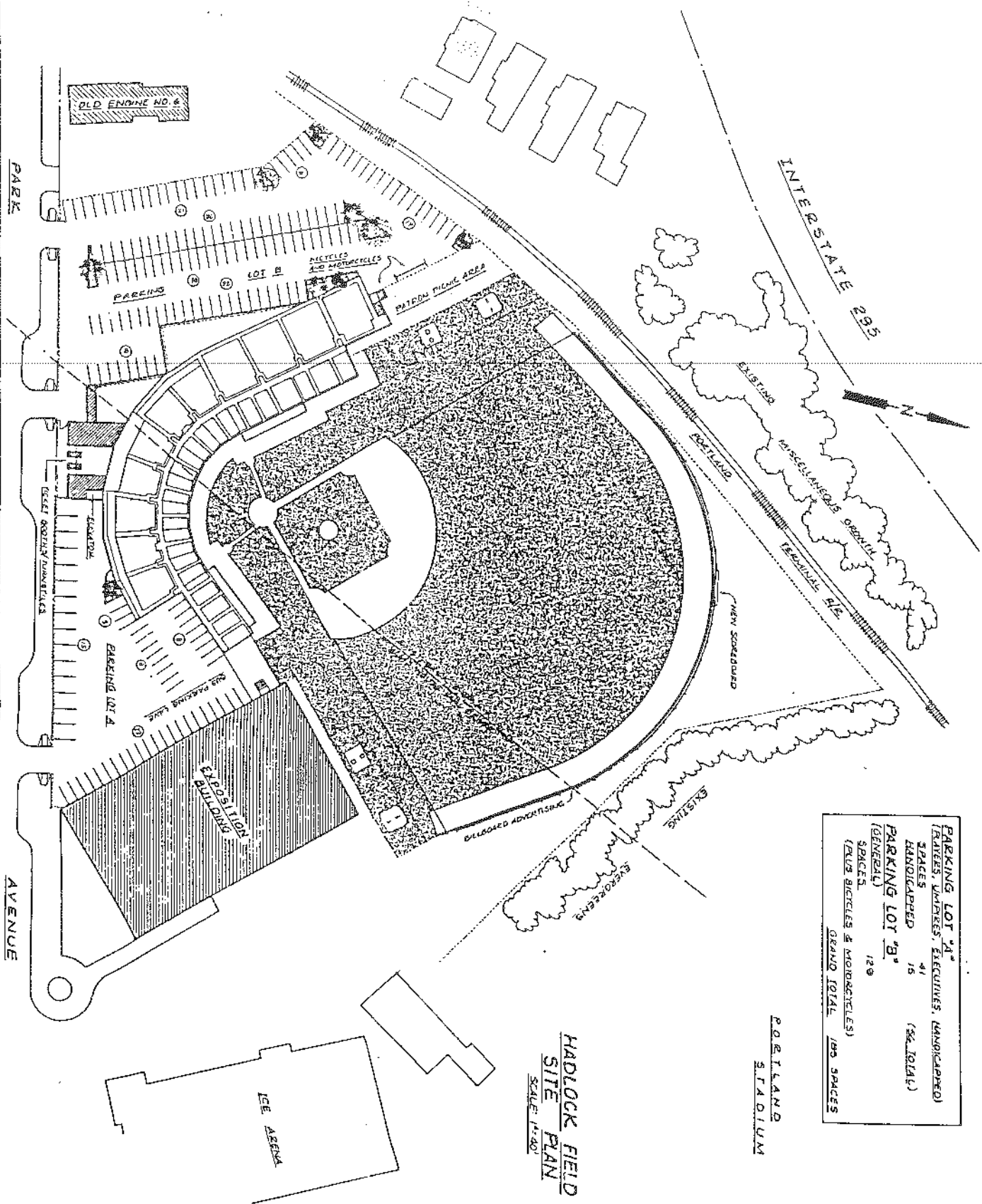
The site plan shows a covered (or otherwise enclosed) walkway from beneath the grandstands to the existing entrance into the Exposition Building which leads to the proposed visitor's locker room. This provides direct access from the locker room to the dugout while controlling fan access to the locker room and to the players.

Last friday, his first day back from vacation, Mary Theriault and I met with Bill Whited to discuss my site plan, his proposed "under the grandstands facilities" layout and how we are to coordinate the two. Bill seemed in complete agreement with my suggestions and site plan layouts. He indicated no concerns with the changes I made from his original concepts and suggested the changes were for the best.

Bill, I strongly believe, as does Bill Whited, the time has come for someone to initiate coordination meetings with the Manager's office. Such a meeting would afford Bill Whited and myself a closer working relationship without conflicts of future thoughts and planning. Bill Whited is enjoyable to work with, however, I feel we need Bob Ganley's direction at this point.

Thank you. Should there be anything further I can do to assist you, please let me know.

cc: Robert B. Ganley, City Manager
Nadeen M. Daniels, Assistant City Manager
George A. Flahery, Director Parks & Public Works
William S. Boothby, Director of Engineering
Mary A. Theriault, Traffic



PARKING LOT "A"	41	(HANDICAPPED)
(FARES, LIMOS, ETC.)	16	(% TOTAL)
PARKING LOT "B"	120	(GENERAL)
(TRUCKS, BICYCLES & MOTORCYCLES)		
GRAND TOTAL	177	SPACES

**HADLOCK FIELD
SITE PLAN
SCALE: 1"=40'**

CITY OF PORTLAND, MAINE
PARKS AND PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION

APPROVED _____
DIRECTOR OF ENGINEERING

**SITE PLAN
HADLOCK FIELD
PROPOSED
RENOVATIONS**

PROJECT NO.	DESIGNED BY JOHN RAQUE
DATE AUGUST 1982	EXAMINER BY JOHN RAQUE
SCALE AS NOTED	CHECKED BY

REFERENCE	DESCRIPTION	DATE

Sec. 14-323. Off-street parking.

Any off-street parking in a R-P resource protection zone is required as provided in division 20 of this article. (Code 1968, § 602.13A.B; Ord. No. 499-74, § 6, 8-19-74)

Secs. 14-324–14-330. Reserved.

DIVISION 20. OFF-STREET PARKING**Sec. 14-331. Defined.**

Off-street parking, either by means of open-air spaces or by garage spaces which meet the standards set forth in the City of Portland Technical and Design Standards and Guidelines, as hereafter amended, in addition to being a permitted use in certain zones, shall be considered as an accessory use when required or provided to serve conforming uses in any zone. (Code 1968, § 602.14.A; Ord. No. 272-77, 5-16-77; Ord. No. 389-89, § 1, 4-3-89)

Cross reference—Definitions and rules of construction generally, § 1-2.

Sec. 14-332. Uses requiring off-street parking.

In all zones where off-street parking is required, the following minimum off-street parking requirements shall be provided and maintained in the case of new construction, alterations which increase the number of units, and changes of use:

- (1) *Residential structures:*
 - a. For new construction, two (2) parking spaces for each dwelling unit, plus one (1) additional parking space for every six (6) units or fraction thereof.
 - b. For alterations or changes of use in existing structures, which create new or additional dwelling units in such structures, one and one-half (1½) additional parking spaces for each such unit. Existing parking spaces shall not be used to meet the parking requirements of this paragraph, unless the existing parking spaces exceed one (1) space for each dwelling unit.
- (2) *Tourist home, tourist court, motel, cabins:* One (1) parking space for each sleeping room.
- (3) *Hotels:* One (1) parking space for each four (4) guest rooms.
- (4) *Schools providing instruction for students up to and including those fifteen (15) years of age:* One (1) parking space for each room used for purposes of instruction.
- (5) *Schools providing instruction for students sixteen (16) years of age and over:* One (1) parking space for each ten (10) seats or major fraction thereof, used for purposes of instruction; if no fixed seats, one (1) parking space for each one hundred (100) square feet or major fraction thereof used for purposes of instruction.

- (6) *Hospitals*: One (1) parking space for each five hundred (500) square feet or major fraction thereof, of floor area, exclusive of cellar.
- (7) *Auditoriums, theaters, assembly halls, funeral homes*: One (1) parking space for each five (5) seats or for each one hundred (100) square feet, or major fraction thereof, of assemblage space if no fixed seats.
- (8) *Retail stores*: One (1) parking space for each two hundred (200) square feet of first floor area in excess of two thousand (2,000) square feet not used for bulk storage and one (1) parking space for each seven hundred (700) square feet, or major fraction thereof, for each floor above the first floor not used for bulk storage.
- (9) *Restaurants or establishments constructed and intended for the dispensing of food and drink as the principal activity*: One (1) parking space for each one hundred fifty (150) square feet, or major fraction thereof, of floor area not used for bulk storage or food preparation.
- (10) *Offices; professional and public buildings*: One (1) parking space for each four hundred (400) square feet or major fraction thereof, of floor area exclusive of cellar not used for bulk storage.
- (11) *Church and accessory uses located on same or contiguous lots*: One (1) parking space for each five (5) fixed seats; or if no fixed seats, one (1) parking space for each twenty-five (25) square feet, or major fraction thereof, of area in sanctuary or principal place of assemblage for worship in the church.
- (12) *For that part of every business, manufacturing, and industrial building not catering to retail trade and with floor area over three thousand (3,000) square feet*: One (1) parking space for each one thousand (1,000) square feet of floor area, or major fraction thereof.
- (13) *Beds*: One (1) parking space for each eight (8) beds, or major fraction thereof.
- (14) *Longterm, extended care and intermediate care facilities*: One (1) parking space for each five (5) beds, or major fraction thereof, plus one (1) parking space per each employee normally present during one (1) weekday morning shift.
- (15) *Lodging houses*: One (1) parking space for each five (5) rooming units, except in the R-5 zone; in the R-5 zone, one (1) parking space for every two (2) rooming units.
- (16) *Sheltered care group homes and emergency shelters*: One (1) parking space for every two (2) employees.
- (17) *Congregate care facilities*: One (1) parking space for every three (3) living units.
- (18) *Special needs independent living units*: One (1) parking space per every four (4) living units, plus one (1) parking space for each staff member, if any, normally present at any one time.

(19) [Exception:] Notwithstanding the preceding provisions of this section, the parking requirement for any new structure in excess of fifty thousand (50,000) square feet shall be as established by the planning board pursuant to section 14-526(a)(2).
 (Code 1968, § 602.14.B; Ord. No. 268-77, 5-16-77; Ord. No. 431-82, § 2, 2-22-82; Ord. No. 575-86, §§ 1, 2, 5-19-86; Ord. No. 65-87, 11-2-87; Ord. No. 230-90, § 2, 3-5-90; Ord. No. 33-91, § 14, 1-23-91; Ord. No. 243-91, § 1, 3-11-91; Ord. No. 33A-91, § 8, 4-17-91)

Sec. 14-333. To be located on lot with principal use in residence zones; exceptions.

Required off-street parking in all residence zones and accessory off-street parking in R-1 through R-5 zones shall be located on the same lot with the principal building or use, except that the board of appeals may permit such off-street parking to be located at a distance of not more than three hundred (300) feet from the principal building or use, measured along lines of public access where it can not reasonably be provided on the same lot if the premises to be used for parking are held under the same ownership or lease as the building or use served and if said premises are located in the same or a less restricted zone as the building or use served. Evidence of such control, either deed or lease, shall be required. (Code 1968, § 602.14.C)

Sec. 14-334. To be located on lot with principal use in nonresidential zones; exceptions.

Required off-street parking in all nonresidential zones shall be located on the same lot with the principal building or use, or within one hundred (100) feet measured along lines of public access, except that where off-street parking cannot be provided within these limits, the board of appeals may permit such off-street parking to be located a reasonable distance from the principal building or use measured along lines of public access if the premises to be used for parking are held under the same ownership or lease. Evidence of such control, either deed or lease, shall be required.
 (Code 1968, § 602.14.D; Ord. No. 430-83, § 1, 4-25-83)

02
40
75

Sec. 14-335. Off-street parking restricted.

Off-street parking shall not include:

- (1) More than one (1) commercial motor vehicle in any residence zone, the R-P zone or any B-1 zone;
- (2) More than six (6) commercial motor vehicles in any B-2 zone;
- (3) Loading, sales, dead storage, repair, or servicing of any kind, except when customarily incidental or accessory to a conforming principal building or use when located in an I-2, I-2b, I-3 zone and I-3b zone;
- (4) Except in the case of a car dealer, more than one (1) unregistered motor vehicle stored outside for a period in excess of thirty (30) days in any residence zone, the R-P zone or any business zone;

- (5) Notwithstanding (1) above, any truck body, commercial trailer or similar commercial vehicles in any residence zone or the R-P zone.

(Code 1968, § 602.14.E; Ord No. 298-88, 5-31-88)

Sec. 14-336. Location in residence zones for six or fewer vehicles.

(a) Where off-street parking for six (6) or fewer vehicles is required or provided in any residence zone, it shall not be located closer than fifty (50) feet to any street line if less than five (5) feet from any lot line and shall not be closer to any street line than the required depth of the front yard for the same lot, except on a corner lot where the minimum depth from the line of the side street shall be the minimum width of the side yard on the side street.

(b) Parking shall be prohibited in the front yard of lots containing two (2) or more dwelling units, except within one (1) driveway on the lot. "Driveway," as used in this paragraph, shall not include any turnaround area. (Code 1968, § 602.14.F; Ord. No. 231-90, § 1, 3-5-90)

Sec. 14-337. Location in residence zones for more than six vehicles.

Where off-street parking for more than six (6) vehicles is required or provided for nonresidential uses in residence zones, it shall not be located closer than twenty-five (25) feet to any residential structure on an adjoining lot.

(Code 1968, § 602.14.G)

Sec. 14-338. When located within required open yard areas in residence zones.

(a) Where off-street parking for more than six (6) vehicles is required or provided on a lot in a residence zone and vehicles are to be or may be parked within the area otherwise required to be kept open and unoccupied for front, side and rear yards in the zone in which such parking is located, the following requirements shall be met:

- (1) A continuous curb guard, rectangular in cross-section, at least six (6) inches in height and permanently anchored, shall be provided and maintained at least five (5) feet from the street or lot line between such off-street parking and that part of the street or lot line involved; or a continuous bumper guard of adequate strength, the top of which shall be at least twenty (20) inches in height, shall be provided and maintained between such off-street parking and that part of the street or lot line involved so that bumpers of vehicles cannot project beyond its face toward the street or lot line involved, either above or below the impact surface.
- (2) Where such off-street parking shall abut a lot in residential use or an unoccupied lot which is located in a residence zone, a chain link, picket or sapling fence, not less than forty-eight (48) inches in height, shall be provided and maintained between such off-street parking and that part of the lot line involved.

(b) Notwithstanding the provisions of subsection (a) of this section, parking shall be prohibited in the front yard of lots containing two (2) or more dwelling units, except within one (1) driveway on the lot. "Driveway," as used in this paragraph, shall not include any turnaround area. (Code 1968, § 602.14.H; Ord. No. 231-90, § 2, 3-5-90)

Supp. No. 23

Sec. 14-339. When located adjacent to a street for a residential use.

Where off-street parking for more than six (6) vehicles is required or provided on a lot in any business zone, the following requirements shall be met:

- (1) Where vehicles are to be or may be parked within ten (10) feet of any street line, a continuous curb guard, rectangular in cross-section, at least six (6) inches in height and permanently anchored, shall be provided and maintained at least five (5) feet from the street line between such off-street parking and that part of the street line involved; or a continuous bumper guard of adequate strength, the top of which shall be at least twenty (20) inches in height, shall be provided and maintained between such off-street parking and that part of the street line involved so that bumpers of vehicles cannot project beyond its face toward the street line involved either above or below the impact surface.
- (2) Where such off-street parking shall abut a lot in a residence zone or a lot in residential use, a chain link, picket or sapling fence, not less than forty-eight (48) inches in height, shall be provided and maintained between such off-street parking and that part of the lot line involved. (Code 1968, § 602.14.I)

Sec. 14-340. Construction requirements when more than six vehicles parked.

Where off-street parking for more than six (6) vehicles is required or provided, the following construction requirements shall apply:

- (1) Appropriate driveways from streets or alleys, as well as maneuvering areas, shall be provided. Location and width of approaches over public sidewalks shall be approved by the traffic engineer.
- (2) The surface of driveways, maneuvering areas and parking areas shall be uniformly graded with a subgrade consisting of gravel or equivalent materials at least six (6) inches in depth, well compacted, and with a wearing surface equivalent in quantities of compaction and durability to fine gravel.
- (3) A system of surface drainage shall be provided in such a way that the waste run-off shall not run over or across any public sidewalk or street.
- (4) Where artificial lighting is provided, it shall be shaded or screened so that no light source shall be visible from outside the area and its access driveways.
(Code 1968, § 602.14.J; Ord No. 96-88, § 1, 7-19-88)

Editor's note—Ord. No. 96-88, § 1, adopted July 19, 1988, amended subsection (1) of this section to read as herein set out. See also the editor's note to Art. III of this chapter for additional provisions relative to Ord. No. 96-88.

Sec. 14-341. Aisles required for six or more spaces.

In parking facilities containing six (6) or more parking spaces, there shall be provided vehicular access by one (1) or more aisles. Aisle widths shall be in conformance with the

standards set forth in the City of Portland Technical and Design Standards and Guidelines, as hereafter amended. (Code 1968, § 602.14.A; Ord. No. 272-77, 5-16-77; Ord. No. 389-89, § 2, 4-3-89)

Sec. 14-342. Reserved.

Editor's note—Section 1 of Ord. No. 354-85, adopted Jan. 7, 1985, repealed § 14-342, relative to a reduction of requirements by the board of appeals, which derived from Code 1968, § 602.14.K.

Sec. 14-343. Board of appeals may approve joint use.

The board of appeals may approve the joint use of a parking facility by two (2) or more principal buildings or uses where it is clearly demonstrated that the parking facility will substantially meet the intent of the requirements by reason of variation in the probable time of maximum use by patrons or employees among such establishments. This section shall apply to residential uses in the B-3 zone which propose joint use of a parking facility with another principal use in the B-3 zone.

(Code 1968, § 602.14.L; Ord No. 243-91, § 2, 3-11-91)

Sec. 14-344. Board of appeals may authorize parking in certain residence zones.

In R-3 through R-5 zones, the board of appeals may permit off-street parking for passenger cars only accessory to a use located in and conforming with the provisions of a nearby business or industrial zone (except B-1 zones) if the lot on which the use is proposed is located wholly within three hundred (300) feet, measured along lines of public access, of the principal building of the use to which the proposed use would be accessory and provided further that:

- (1) The lot where the parking use is proposed shall be under the control of the owner of the use to which the parking use would be accessory. Evidence of such control by deed or lease shall be required before the certificate of occupancy is issued. If such control

should be abrogated, the parking use thus allowed shall automatically revert to a nonconforming use in violation of this article and shall be terminated forthwith.

- (2) No such appeal shall be in order for hearing before the board of appeals until the planning board shall have reviewed the site plan accompanying the application for building permit or certificate of occupancy for such use and shall have submitted its recommendations with respect thereto.
- (3) The board of appeals may impose such conditions as deemed necessary to insure development compatible with that of the immediate neighborhood notwithstanding the provisions of any other section of this article and may at its discretion limit the period of such use. (Code 1968, § 602.14.M; Ord. No. 541-84, 5-7-84)

Secs. 14-345—14-350. Reserved.

DIVISION 21. OFF-STREET LOADING

Sec. 14-351. Minimum loading bays or loading berths.

In those zones where off-street loading is required, the following minimum off-street loading bays or loading berths shall be provided and maintained in the case of new construction, alterations and changes of use:

- (1) Office buildings and hotels with a gross floor area of more than one hundred thousand (100,000) square feet: One (1) bay.
- (2) Retail, wholesale and industrial operations with a gross floor area of more than five thousand (5,000) square feet:
 - a. 5,000 to 40,000: 1 bay;
 - b. 40,001 to 100,000: 2 bays;
 - c. 100,001 to 160,000: 3 bays;
 - d. 160,001 to 240,000: 4 bays;
 - e. 240,001 to 320,000: 5 bays;
 - f. 320,001 to 400,000: 6 bays;
 - g. Each 90,000 over 400,000 square feet: 1 additional bay.
- (3) Hospitals and nursing and convalescent homes: Two (2) off-street loading areas shall be provided whereby one (1) service area for ambulance and other emergency vehicles shall be separate from one (1) service area accommodating supply vehicles, and whereby both off-street loading areas shall be separate from parking and entrance locations. (Code 1968, § 602.15.A; Ord. No. 49-74, 1-7-74)

Sec. 14-352. Dimensions.

Each loading bay shall have minimum dimensions of fifty (50) feet by fourteen (14) feet and may be located either within a building or outside and adjoining an opening in the building, except that in the case of hospitals and nursing homes and convalescent homes, the

total parking provided on the site may be located between the principal structure and the front yard.

- (5) *Signs*: Signs shall be subject to the provisions of division 22 of this article.
- (6) *Exterior storage*: There shall be no exterior storage with the exception of receptacles for solid waste disposal. Such receptacles shall be shown on the approved site plan.
- (7) *Storage of vehicles*: Storage of vehicles is subject to the provisions of section 14-335.
- (8) *Shoreland and flood plain management regulations*: If the lot is located in a shoreland zone or in a flood hazard zone, the requirements of division 26 and/or division 26.5 apply. (Ord. No. 291-88, 4-4-88)

Sec. 14-151. External effects.

Every use in a R-P zone shall be subject to the following requirements:

- (1) *Enclosed structure*: The use shall be operated within a completely enclosed structure, except for those customarily operated in the open air.
- (2) *Noise*: The volume of sound, measured by a sound level meter with frequency weighting network (manufactured according to standards prescribed by the American Standards Association), generated shall not exceed fifty-five (55) decibels on the A scale, on impulse (less than one (1) second), at lot boundaries, excepting air raid sirens and similar warning devices.
- (3) *Vibration and heat*: Vibration inherently and recurrently generated and heat shall be imperceptible without instruments at lot boundaries.
- (4) *Glare, radiation or fumes*: Glare, radiation or fumes shall not be emitted to an obnoxious or dangerous degree beyond lot boundaries.
- (5) *Smoke*: Smoke shall not be emitted at a density in excess of twenty (20) percent opacity level, as classified in Method 9 (Visible Emissions) of the Opacity Evaluation System of the U.S. Environmental Protection Agency.
- (6) *Materials or wastes*: No materials or wastes shall be deposited on any lot in such form or manner that they may be transferred beyond the lot boundaries by natural causes or forces. All material which might cause fumes or dust, or constitute a fire hazard if stored out-of-doors, shall be only in closed containers. Areas attracting large numbers of birds, rodents or insects are prohibited. (Ord. No. 291-88, 4-4-88)

Sec. 14-152. Reserved.

DIVISION 8.5. R-OS RECREATION AND OPEN SPACE ZONE

Sec. 14-153. Purpose.

- (a) The purpose of this division is:
 - (1) To preserve and protect open space as a limited and valuable resource;

- (2) To permit the reasonable use of open space, while simultaneously preserving and protecting its inherent open space characteristics to assure its continued availability for public use as scenic, recreation, and conservation or natural resource area, and for the containment and structuring of urban development; and
- (3) To coordinate with and carry out federal, state, regional, and city recreation and open space plans.

(b) The recreation open space zone may include major parcels (over two (2) acres) of public property, and private property legally restricted from intensive use or development through deed, covenant, or otherwise. (Ord. No. 232-81, § 602.7B.1, 11-16-81)

Sec. 14-154. Permitted uses.

The following uses are permitted uses within the recreation and open space zone, subject to the development standards contained herein:

- (1) Municipal parks, public open spaces, picnic areas, playgrounds and playlots;
- (2) Cemeteries;
- (3) Arboretums;
- (4) Golf courses, excluding miniature golf;
- (5) Boat landings, beaches, and marinas for public uses;
- (6) Outdoor ballfields and public athletic fields;
- (7) Swimming pools and tennis courts;
- (8) Picnic groves and areas;
- (9) Natural parks and scenic overlooks;
- (10) Hiking, walking, bicycling or cross-country ski trails;
- (11) Community gardens for cultivation by and for city residents;
- (12) Accessory uses, including structures or buildings of less than two thousand five hundred (2,500) square feet of floor area. (Ord. No. 232-81, § 602.7B.2, 11-16-81)

Sec. 14-155. Conditional uses.

The following uses are conditional uses in the recreation and open space zone, subject to approval by the board of appeals.

- (1) Accessory uses with structures or buildings of two thousand five hundred (2,500) square feet or more of floor area;
- (2) Other recreational facilities and uses that are open to the public;
- (3) Water pumping stations, sewage pumping stations and sewage treatment facilities. (Ord. No. 232-81, § 602.7B.3, 11-16-81; Ord. No. 67-89, § 1, 8-7-89)

Sec. 14-156. Standards for conditional uses.

In addition to the criteria listed in section 14-474(c), the board of appeals shall consider the following criteria when reviewing conditional uses in the recreation and open space zone:

- (1) The use shall be in conformity with or satisfy a deficiency identified in a federal, state, regional, or city recreation and open space plan, including but not limited to the state comprehensive outdoor recreation plan, as such plans may from time to time be created or revised.
- (2) Buildings and structures shall not obstruct significant scenic views presently enjoyed by nearby residents, passersby, or users of the site.
- (3) Indoor recreation or nonrecreational uses shall serve a significant public purpose that cannot reasonably be accommodated outside of the recreation and open space zone. (Ord. No. 232-81, § 602.7B.4, 11-16-81)

Sec. 14-157. Space and bulk requirements.

No building or structure of a permanent nature shall be erected, altered, enlarged, rebuilt, or used unless it meets the following requirements:

- (1) *Minimum front yard:*
 - a. Principal buildings or structures: Twenty-five (25) feet.
 - b. Accessory buildings or structures: Twenty-five (25) feet.
- (2) *Minimum rear yard:*
 - a. Principal buildings or structures: Twenty-five (25) feet.
 - b. Accessory buildings or structures: Twenty-five (25) feet.
- (3) *Minimum side yard:*
 - a. Principal buildings or structures: Twelve (12) feet.
 - b. Accessory buildings or structures: Twelve (12) feet.
- (4) *Minimum lot size:* Two (2) acres, except that sewage treatment facilities are not required to meet this standard.
- (5) *Maximum building height:* Thirty-five (35) feet.
- (6) *Maximum coverage of lot by buildings, structures and other impervious site improvements such as paved sidewalks, drives and parking lots:* Twenty-five (25) percent of lot area, except that sewage treatment facilities are not required to meet this standard.
- (7) *Maximum floor area ratio:* Two-tenths (0.2). (Ord. No. 232-81, § 602.7B.5, 11-16-81; Ord. No. 67-89, § 2, 8-7-89)

Sec. 14-158. Development standards for recreation and open space zone.

All development in the recreation and open space zone shall comply with the following development standards, which shall be reviewed by the planning board in conjunction with the site plan review:

Supp. No. 19

- (1) All ground areas not used for parking, loading, vehicular or pedestrian areas and not left in their natural state shall be suitably landscaped.
- (2) Natural features, such as mature trees and natural surface drainageways, shall be preserved to the greatest possible extent consistent with the uses of the property.
- (3) Loading areas shall be screened and parking areas shall be screened and landscaped so as to avoid a large continuous expanse of paved area.
- (4) Buildings and structures shall be sited to avoid obstructing significant scenic views presently enjoyed by nearby residents, passersby, and users of the site.
- (5) Storage of commodities and equipment shall be completely enclosed within buildings or provided with screening by a fence, wall, or landscaping.
- (6) The outer perimeter of playfields, playlots, and other active recreational areas shall be screened, or shall be located a reasonable distance from any residential use.
- (7) Off-street parking shall conform to the requirements of division 20 of this article, where applicable. Otherwise, off-street parking adequate to serve projected employee and visitor needs shall be provided. Parking needs projections provided by the applicant or the planning department should be considered in the review. (Ord. No. 232-81, § 602.7B.6, 11-16-81)

Sec. 14-159. Shoreland regulations.

No building or structure shall be erected, altered, enlarged, rebuilt, or used, and no premises shall be used within the area situated between the shoreland zone line and the normal high water mark of the waters of the Stroudwater River, Presumpscot River, Fore River, Portland Harbor, Back Cove, and the bays, coves, sounds, inlets, and open waters of Casco Bay, as shown on the city zoning map and on all land areas of all islands not having a shoreland zone line on the city zoning map, unless it meets the requirements of division 26 of this article. (Ord. No. 232-81, § 602.7B.7, 11-16-81)

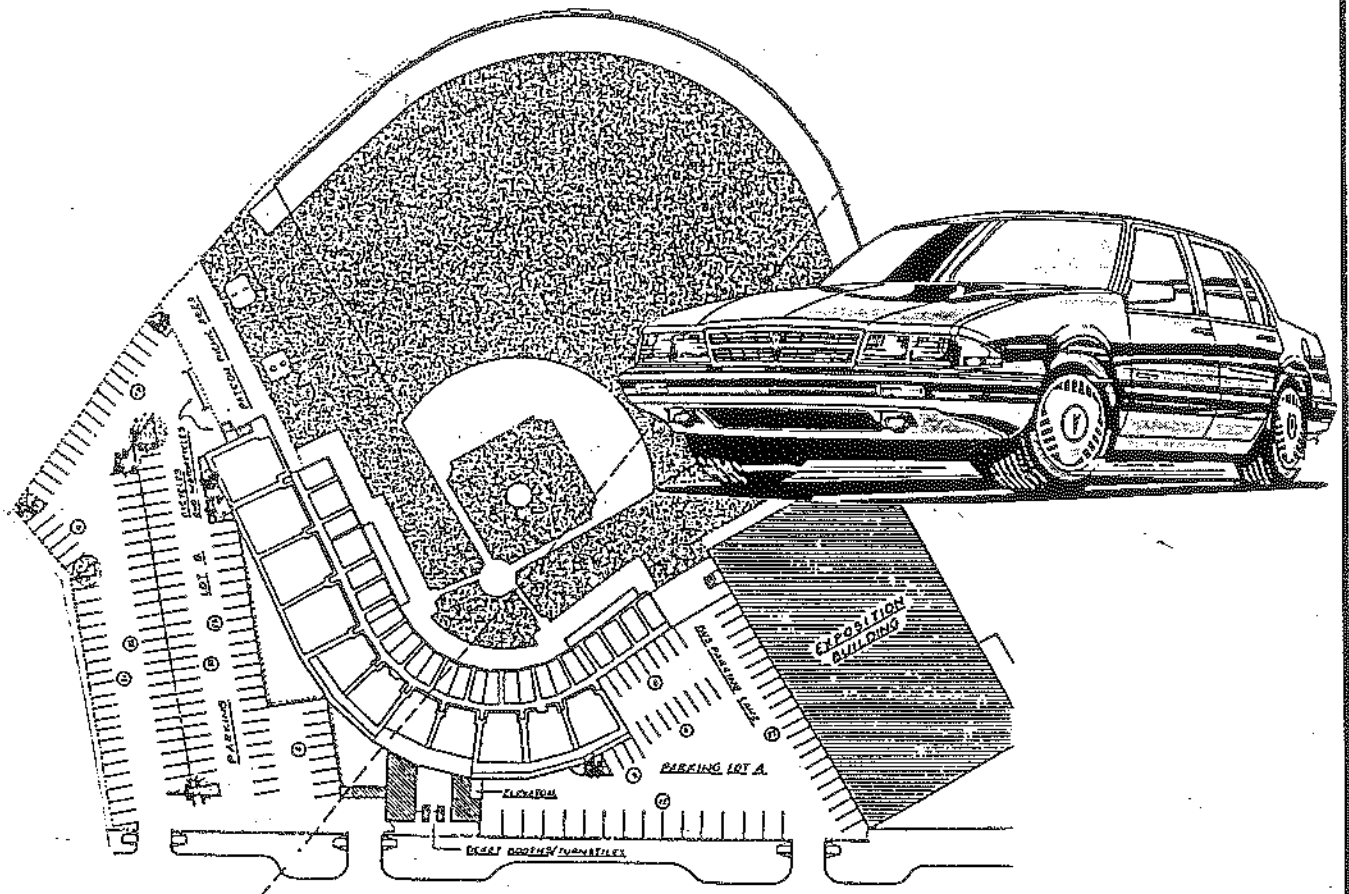
Sec. 14-160. Reserved.

DIVISION 9. B-1 NEIGHBORHOOD BUSINESS ZONE*

Sec. 14-161. Purpose.

The purpose of the B-1 neighborhood business zone is to provide limited areas for the location of small-scale commercial establishments intended to serve a local market. (Ord. No. 292-88, 4-4-88)

*Editor's note—Ord. No. 292-88, adopted Apr. 4, 1988, with an effective date of July 1, 1988, repealed §§ 14-161—14-167 of Div. 9, B-1 Business Zone, of this article and enacted in lieu thereof similar new provisions as set out in §§ 14-161—14-167. Formerly, such sections derived from §§ 602.8.A—602.8.G of the city's 1968 Code and from Ord. No. 74-72, adopted Mar. 6, 1972, and Ord. No. 499-74, § 4, adopted Aug. 19, 1974.



HADLOCK FIELD PARKING STUDY

12/2/97
REC'D
FCA
2006-01-06

CHECK NO. 1007522
52-36
112

Fleet Bank
PORTLAND, MAINE

MAINE MEDICAL CENTER FOUNDATION

22 BRAMHALL STREET
PORTLAND, MAINE 04102
PAY TO THE ORDER OF
CITY OF PORTLAND

DATE OF CHECK

01/02/97

AMOUNT OF CHECK

\$300.00

THREE HUNDRED AND NO/100

VOID AFTER 90 DAYS

William S. Long
VICE PRESIDENT FINANCE/TREASURER
Michael Silman
ASSOCIATE VICE PRESIDENT FINANCE

⑆ 1007522⑆ ⑆ 011200365⑆ 000 679 3290⑆

C



CITY OF PORTLAND

November 15, 1994

Maine Department of Environmental Protection
ATTN: Jeff Madore
State House Station 17
Augusta, ME. 04333

RE: Hadlock Field Baseball Project

Dear Jeff:

Enclosed is our follow-up report for the parking and traffic operations during the first season of baseball at Hadlock Field. Overall, the program was very successful with minimal resident complaints, numerous parking alternatives and minimal traffic delays. The report includes the committee's recommendations for the upcoming year.

Please let me know if you need any further documentation for next season. I can be reached at 874-8894; however, I will be away on a transportation tour from November 28th to December 15th. Thank you for your continued cooperation.

Sincerely,

A handwritten signature in cursive script, appearing to read "Mary Ann Theriault".

Mary Ann Theriault
Traffic Project Engineer

cc: Linda Kokemuller, MDEP, Portland
Paul Minor, MDOT, Augusta
William Bray, Deputy Director PW (memo only)
Alex Jaegerman, Planning (memo only)

1995 Portland Sea Dogs Home Schedule

Thursday, April 6	Binghamton
Friday, April 7	Binghamton
Tuesday, April 18	Canton
Wednesday, April 19	Canton
Thursday, April 20	Canton
Friday, April 21	Bowie
Saturday, April 22	Bowie
Sunday, April 23	Bowie
Friday, May 5	Binghamton
Saturday, May 6	Binghamton
Sunday, May 7	Binghamton
Monday, May 8	Harrisburg
Tuesday, May 9	Harrisburg
Wednesday, May 10	Harrisburg
Thursday, May 11	Harrisburg
Friday, May 12	Trenton
Saturday, May 13	Trenton
Sunday, May 14	Trenton
Monday, May 22	Reading
Tuesday, May 23	Reading
Wednesday, May 24	Reading
Thursday, May 25	New Haven
Friday, May 26	New Haven
Saturday, May 27	New Haven
Sunday, May 28	New Haven
Monday, May 29	New Britain
Tuesday, May 30	New Britain
Wednesday, May 31	New Britain
Friday, June 9	Harrisburg
Saturday, June 10	Harrisburg
Sunday, June 11	Harrisburg
Monday, June 12	Norwich
Tuesday, June 13	Norwich
Wednesday, June 14	New Britain
Thursday, June 15	New Britain
Friday, June 23	Trenton
Saturday, June 24	Trenton
Sunday, June 25	Trenton
Monday, June 26	Trenton
Saturday, July 1	New Haven
Sunday, July 2	New Haven
Monday, July 3	New Haven
Tuesday, July 4	Norwich
Wednesday, July 5	Norwich
Thursday, July 6	Norwich
Friday, July 7	Norwich
Saturday, July 8	New Britain
Sunday, July 9	New Britain
Monday, July 17	Canton
Tuesday, July 18	Canton
Wednesday, July 19	Canton
Thursday, July 20	Canton
Friday, July 21	Norwich
Saturday, July 22	Norwich
Sunday, July 23	Norwich

Friday, August 4	Bowie
Saturday, August 5	Bowie
Sunday, August 6	Bowie
Monday, August 7	Bowie
Tuesday, August 8	Binghamton
Wednesday, August 9	Binghamton
Monday, August 21	Binghamton
Tuesday, August 22	Binghamton
Wednesday, August 23	New Britain
Thursday, August 24	New Britain
Wednesday, August 30	New Haven
Thursday, August 31	New Haven
Friday, September 1	Reading
Saturday, September 2	Reading
Sunday, September 3	Reading
Monday, September 4	Reading

FOR MORE INFORMATION
CALL (207) 879-9500

- Binghamton Mets (New York Mets)
- Bowie Bay Sox (Baltimore Orioles)
- Canton-Akron Indians (Cleveland Indians)
- Harrisburg Senators (Montreal Expos)
- New Britain Twins (Minnesota Twins)
- New Haven Ravens (Colorado Rockies)
- Norwich Navigators (New York Yankees)
- Portland Sea Dogs (Florida Marlins)
- Reading Phillies (Philadelphia Phillies)
- Trenton Thunder (Boston Red Sox)



ATTACH 7

FY95

MODE: ACT

CITY OF PORTLAND
CITY EXPENDITURE REVIEWING SYSTEM

DATE: 10-12-94
TIME: 12:18:26

FUND: 95101 CITY EXPENDITURES - FISCAL YEAR 95
ACCOUNT: 13 02 03 BASEBALL PARKING

ODE	ACCOUNT TITLE	APPROPRIATION	TOTAL EXPENDED	BALANCE	PCT
300	TEMPORARY HELP	0.00	2,132.95	2,132.95-	0
500	TERMINATION PAYMENTS	0.00	0.00	0.00	0
600	WORKERS COMPENSATION	0.00	0.00	0.00	0
200	TEMPORARY SERVICES	0.00	3,712.68	3,712.68-	0
531	HIRED EQUIPMENT	0.00	39,882.90	39,882.90-	0
710	ADVERTISING EXPENSE	0.00	1,623.00	1,623.00-	0
230	WEARING APPAREL	0.00	319.20	319.20-	0
LL	ACCOUNT 13 02 03	0.00	47,670.73	47,670.73-	0

ACCOUNT: 13 03 00 ELM STREET GARAGE

210	HOURLY PAYMENTS	128,392.00	42,560.65	85,831.35	33
300	TEMPORARY HELP	5,000.00	637.77	4,362.23	13

CONTINUE: []

37 441.52

85,112.25

F444

MODE: ACT

CITY OF PORTLAND
CITY EXPENDITURE REVIEWING SYSTEM

DATE: 10-12-94
TIME: 12:10:49

FUND: 94101 CITY EXPENDITURES - FISCAL YEAR 94
ACCOUNT: 13 02 03 BASEBALL PARKING

CODE	ACCOUNT TITLE	APPROPRIATION	TOTAL EXPENDED	BALANCE	PCT
1300	TEMPORARY HELP	0.00	3,277.38	3,277.38-	0
2200	TEMPORARY SERVICES	0.00	260.00	260.00-	0
2531	HIRED EQUIPMENT	0.00	6,500.00	6,500.00-	0
2620	LOCAL TRANS BUS	0.00	20,986.08	20,986.08-	0
2710	ADVERTISING EXPENSE	0.00	3,972.00	3,972.00-	0
2740	PRINTING EXPENSE	0.00	2,359.75	2,359.75-	0
3160	MINOR EQUIPMENT	0.00	86.31	86.31-	0
ULL	ACCOUNT 13 02 03	0.00	<u>37,441.52</u>	37,441.52-	0

ACCOUNT: 13 03 00 ELM STREET GARAGE

1210	HOURLY PAYMENTS	139,397.00	144,112.15	4,715.15-	103
1300	TEMPORARY HELP	4,410.00	9,412.16	5,002.16-	213

CONTINUE: []

DISCOVERY WEEKEND DATES SET

MCREF continues its 14-year sponsorship of Discovery Weekend, a seminar for cancer patients and their loved ones. Weekends will be held during the coming year on: September 30 - October 2, 1994, January 27-29, 1995, and May 12-14, 1995. During the weekend, participants explore new skills and insights for dealing with cancer. Trained professionals share their expertise and present information on communication, processing a medical diagnosis, family concerns, stress, and spirituality. A panel of other cancer patients share their knowledge and experience. Dr. Jake Hannemann is Medical Director. For more information, contact the Foundation at 773-2533 or Jude Hannemann at 767-2494.

Maine Cancer Research & Education Foundation News is published periodically to inform concerned citizens about past successes and future plans of the Foundation.

In our 1994-95 editions, specific cancers will be addressed in each issue. The focus of this issue is breast cancer. The Winter 1995 issue will discuss prostate cancer.

Address correspondence to:
Jayne A. Soles, Executive Director, PO Box 553 DTS, Portland, Maine 04112-0553, 207-773-2533.

FOR YOU FOR LIFE - BREAST HEALTH EDUCATION

Maine Cancer Research and Education Foundation joined other health organizations in sponsoring a breast health information booth near Filene's at the Maine Mall during National Breast Cancer Awareness Month in October. Educational literature about breast self-exams, mammograms, breast cancer, and advances in research were available. Volunteers and professionals from the sponsoring organizations staffed the booth during the first week of October. *For You For Life* was sponsored by: Maine Medical Center, Maine Mall, Maine Cancer Research & Education Foundation, Maine Center for Cancer Medicine and Maine Breast Cancer Coalition.



-
- * The Maine Cancer Research and Education Foundation appreciates the efforts of the following volunteers who helped with Foundation activities and Discovery Weekend during the 1993 Fiscal Year:
 - *
 - * Dave & Alice Anderson
 - * Mary Lou Carroll
 - * Peter Fitzpatrick
 - * Arlene Hamilton
 - * Patty Jackson
 - * Aleita Kilborn
 - * Fr. Bob MacDonald
 - * Suzanne McWilliam
 - * Grace Moulton
 - * Betty Norton
 - * The Rev. Dick Sheasley
 - * Pam Szalajski
 - *
 - * Barbara Wood
 - * Carole Bruce
 - * Debbie Corveau
 - * Joan & Leslie Gordon
 - * Jake & Jude Hannemann
 - * Dick & Betty Kennedy
 - * Theresa King
 - * Shirley Mason
 - * Paul Marin
 - * Tom Noll
 - * Joe Richards
 - * Pat Sweatt
 - * Jim Tomney
 - *

SEA DOGS BENEFIT PARKING RAISES \$3000 FOR CANCER PROGRAMS

Summer days and baseball season are behind us now, but over \$3000 raised by Maine Cancer Research and Education Foundation at their benefit parking at the Portland Sea Dogs home games will continue to provide support for Maine cancer research and education programs. With the generous use of the parking lot at the Maine Center for Cancer Medicine on Park Avenue, Jim Stanhope and volunteer Pat Sweatt collected benefit parking fees from hundreds of baseball fans. MCREF sincerely appreciates the loyal customers who returned numerous times for convenient parking and support of cancer research at the same time. We hope to see you again next year!

MAINE MEDICAL CENTER

June 30, 1994

Mary Ann Theriault
Traffic Principal Engineer
City of Portland
Traffic Division
65 Hanover Street
Portland, Me. 04101

Dear Mary,

Dave Young has informed me that the City has made the decision not to use the Maine Medical Center Garage for Sea Dog parking. This is understandable given the small number of fans that have chosen to use the garage.

I want to thank you and the members of the City Traffic Division that worked with us on this project. The team of city employees that worked on the garage did an outstanding job in accommodating both the fans and MMC employees.

It has been a pleasure to work with you in putting this together even though our participation in the parking plan has only been for a short time. Please let us know if we can be of service in the future.

Sincerely,



Mike Swan, AVP Administrative Services
Maine Medical Center

SEADOGS SHUTTLE
Total 1994 Passengers and Bus Mileage

Month	Games	F.C. PARK	SONESTA	TOTAL	MARGINAL MAY	RAMADA INN	TOTAL ALL ROUTES	GAME ATTEND.	% MILEAGE
April	6	239	280	379	1,036	1,638	3,053	33,386	9.1
May	16	175	166	341	1,143	2,348	3,832	70,336	5.6
June	13	110	173	283	794	2,079	3,156	69,844	4.6
July	17	265	164	429	1,489	3,378	5,296	89,463	5.3
Aug.	14	171	58	229	1,106	2,730	4,065	84,111	4.8
Sept.	4	22	9	31	188	667	894	19,995	4.5
	70	882	650	1,692	6,764	12,840	20,296	377,135	5.4

Less tickets rec'd: 441

19,855

PIERCE, ATWOOD, SCRIBNER,
ALLEN, SMITH & LANCASTER

ATTORNEYS AT LAW

ONE MONUMENT SQUARE PORTLAND, MAINE 04101-1110
207/773-6411 FAX 207/773-3419

M E M O R A N D U M

TO: City of Portland/Hadlock Field Improvements File
FROM: TRDoyle *Tom*
RE: Telephone Conversation with Martha Kirkpatrick, Esq.
of DEP
DATE: January 15, 1993

Yesterday afternoon I spoke with Martha Kirkpatrick about the proposed improvements at Hadlock Field and the status of DEP action with respect to "delegation" (aka "registration") of Site Law authority to the City.

I told Martha I understood after speaking with Bob Ganley, that Commissioner Marriott had given his commitment to having the DEP take favorable action on its "review" of the City Planning Board's decision on the Hadlock Field improvements the day after they occur. The Commissioner did say that this commitment is premised on the City and MDOT having "worked out" all traffic issues. I told Martha that given this, though the City still does not believe that the entire Hadlock Field improvements qualify as a "building", the City has agreed to settle for this compromise position, provided Board action

delegating authority to the City occurs at the January 27 meeting as she had indicated previously.

Martha told me she had not spoken with Commissioner Marriott, but what I had stated was completely consistent with her view and the last discussion we had on this matter. She stated that if the traffic issue is resolved, DEP can "review the Planning Board decision in one day." She stated just have the City "keep us involved and informed as the application moves its way through the Planning Board process."

Martha also told me that "delegation" of Site Law authority to the City is on the Board of Environmental Protection's agenda for Wednesday, January 27, and is listed as a "unanimous consent item." The delegation order is conditioned upon the City revising its traffic standards to require an applicant to "trace" traffic to the intersection identified in Chapter 374 § 3(B)(2)(b) of the DEP Traffic Rules under the Site Law. Martha promised to fax me a copy of staff's draft order on this item, which is to be included in the DEP's packet for the January 27 meeting.

cc: Robert Ganley
William Bray

P.S. Draft Order is attached. Bill Bray, please note pages 2, 4 and 5, particularly Condition #4. Order requires Planning Board to revise traffic standards as described above "prior to review of any proposed development pursuant to this Order." (Emphasis supplied).

V. BUDGET

The detailed costs of the Hadlock Field Parking and Traffic Operations were \$37,442.00 for FY94 and \$47,671.00 for FY95 for a total of \$85,113.00 (per expenditure listings dated 10-12-94, Attachments # 5, 6).

The following chart breaks down the total costs:

\$67,775.00	Shuttle Program including Maine Line net costs (The breakdown of METRO costs is shown on Attachment #1, 2)
\$ 5,595.00	Portland Press Advertising
\$ 2,360.00	Printing Coupon Booklets
\$ 5,410.00	Payroll Costs - Maine Med. Parking Attendants/PCO's
<u>\$ 3,973.00</u>	<u>Maine Med. Security Charges</u>
<u>\$85,113.00</u>	<u>TOTAL</u>

VI. CONCLUSIONS/RECOMMENDATIONS FOR 1995

1. Repeat similar shuttle service at the Ramada Inn and USM-Marginal Way Lot. Review the continuation of the Downtown Shuttle run and make a decision as soon as possible. Prepare a bid for the back-up shuttle services.
2. Continue to utilize the shuttle coordinators at each Shuttle Lot as well as the shuttle dispatch at Hadlock Field.
3. More pro-active advance shuttle ticket sales to be coordinated with the Shuttle provider and the Sea Dogs staff possibly utilizing mascots in schools for April/May/June.
4. If the Deering Oaks Festival will be utilizing the Marginal Way Lot (for the next 5 years???), secure a back-up use of the MDOT newly, expanded rideshare lot for weekends/nights.
5. Continue with a similar Marketing Plan maintaining the same format for consistency. Possibly include the parking/traffic ad in the 1995 PW Calendar to be distributed in June 1995???
6. The Traffic Division should proceed with the budget request for installation of an overhead sign directly over the entrance crosswalk on Park Avenue (poles/foundations/sign needed).
7. Continue with the free Shuttle Coupon Booklet at the cost of \$25.00 per participant to cover printing/paper costs.
8. Continue to include the Parking & Traffic Plan/Ad in the Sea Dogs program for next year. Include shuttle information with advance game ticket sales both purchased at the ticket office and through the mail.
9. Provide a Police Officer dedicated to crossing the pedestrians at Park Avenue/Hadlock Entrance for before and after every game. Also, the use of barrels and warning signs are recommended for the centerline locations before and after the Police Officer.
10. Provide the Police Officer on duty with a Sea Dogs radio for improved radio coordination.
11. Continue to meet randomly with the Operations Committee to address and resolve any changes in the Parking & Traffic Operations, as needed.
12. Revert shuttle lane on Park Avenue, between Lot A & B, to "unrestricted parking" for the off-season months, Sept. 15 to April 1 for residential use.

REVENUES AND COSTS PER SEADOGS SHUTTLE ROUTES
April Through September 1994

	P.O. Park Sonoma	Marginal Way	Rancho Loma	Total	Comments
COSTS					
Labor and Fringe:					
Drivers	7,814	10,872	16,288	33,974	
Dispatchers	3,537	3,538	3,538	10,613	Not Contingent on % of Routes
Coordinators	6,833	2,918	2,918	11,666	
Supervision	180	180	180	540	Initial Costs
	17,364	17,506	21,922	56,792	
Bus Mileage	2,858	6,173	9,269	19,299	
Masterlist and Supplies	732	738	738	2,213	One Time Costs
TOTAL COSTS	21,997	24,478	32,010	79,295	
REVENUES	1,592	5,764	12,840	20,296	Passengers One-Way
COSTS OVER REVENUES	20,305	18,714	19,170	57,999	
TOTAL PASSENGERS	3,384	11,528	26,680	40,692	
COSTS OVER REVENUES PER PASSENGER	6.00	1.62	0.75	1.43	

Assumptions for Labor and Fringe:

P.O. Park/Sonoma: 1 Driver, 2 Coordinators
 Marginal Way (Does Not Include Maine Line): 1 Driver, 1 Coordinator
 Rancho Loma: 2 Drivers, 1 Coordinator
 Sonoma: 2 Drivers, 1 Coordinator

Assumptions for Bus Mileage:

P.O. Park/Sonoma: 25 Miles Per Game
 Marginal Way: 40 Miles Per Game (Does Not Include Maine Line)
 Rancho Loma: 60 Miles Per Game

Hadlock, page 2...

Pedestrian Operations

Pedestrians arrived at the game in large numbers from both ends of Park Avenue. The pedestrian signals were continuously checked for proper operations. No problems were documented by the Traffic Division crews or the general public. With this current intracity location, it was very difficult to monitor pedestrian traffic. Were they simply walking from home or the various remote parking sites? Our committee estimates that many of the intown patrons walked to the game which is partially the reason for the "underutilized" downtown shuttle run.

Shuttle Program

A total of 25,370 (20,296 plus an estimated 25% for under 12 free) baseball fans rode on the Sea Dogs shuttle buses!

A total of 50,740 (40,592 plus the estimated 25%) passenger trips were completed by METRO for the first season; an average of 363 (290 plus the estimated 25%) two way riders per game. In March 1994, we had estimated 225 two way riders per game. The actual totals exceeded the estimation by 62%!

The Ramada Inn Lot was the most successful with the net cost per passenger of \$0.75; the Marginal Way Lot was second at \$1.62. The Sonesta/PO Park Route was the most underutilized run with a high cost of \$6.00 per passenger. The breakdown on the costs from METRO is enclosed as Attachment #1, 2.

The Maine Line Bus Service backed up METRO services with (3) buses on the weekdays, then was quickly narrowed down to (1) bus on the weekday games. Their service was equal to METRO in professionalism, cleanliness and promptness. All parties involved were pleased with their back-up service throughout the season.

III. PARKING

Due to the variety of choices, baseball fans had very little problems finding available parking in and around Hadlock Field. Randomly throughout the season, the parking sites were observed by the Traffic Division as well as other committee members. The following comments summarize our observations which are also part of the monitoring program:

On-Street

Citywide, we received three minor complaints and no additional request for one hour parking due to ballpark operations. Therefore, the Traffic Division staff felt it was unnecessary to perform a detailed parking count for the city streets (on overtime) as suggested in the monitoring program. The parking enforcement was minimal. Further detail can be provided by the Parking Division, if needed. On Deering Avenue, we had to add signs stating "Available Parking" before patrons utilized this unrestricted parking area. State Street Extension did not get utilized; however, St. James Street was highly used by baseball patrons. The bus parking was slightly reduced to allow for customer parking for the Carvel Company located at the corner of St. James Street and Park Avenue.

Hadlock, page 3...

On-Street, continued...

St. John Street, from Park Avenue to Fairmouth Street, was also a prime location for on-street parking; however, the local residents did not call with complaints or request any parking changes.

Off-Street- Public

Hadlock Field, Lot A & B were filled to capacity+ every game. Lot B was expanded to include twice as many handicapped spaces (8 to 16) as well as provide a taxi drop off area. King School was not available for baseball parking due to the ongoing construction on the middle school facility. Fitzpatrick Parking Lot usually had 1-5 spaces available per game. Evidently, most patrons expected this lot to fill up first and didn't bother checking for available free parking.

Off-Street - Private

The USM lot was very underutilized on Bedford Street except during the Deering Oaks Festival/SeaDog Game dual event nights. Consequently, this dual event and others co-existed without any significant upset to the neighborhoods or traffic operations. Also, the USM-Marginal Way Lot and the Ramada Inn Lot were highly utilized as part of a successful shuttle program.

The Maine Medical Center parking facility was closed early in the season due to low volumes. The staff costs were evaluated and the committee recommended closing the garage. A letter from MMC complimented the City Staff and supported future coordination for special parking needs in this area (Attachment #3).

Private Enterprise

As we had hoped, the "private-enterprise" parking opportunities flourished for the area businesses. Approximately thirteen businesses participated on a regular basis throughout the first season. The Maine Center for Cancer Medicine, 180 Park Avenue, recently published a very positive article stating their success and continued support for the next year (Attachment #4). Approximately half of the parking was managed as donations which added a positive light to the private parking alternative.

IV. MARKETING

The marketing plan was very successful in respect to the lack of traffic and parking problems. The patrons were well-informed and their parking choices were easy to understand. Random ads in the Portland Press were updated throughout the season while maintaining the same basic format. The Sea Dogs program was also a regular source for the parking alternatives.

The Portland Downtown District and Economic Development are planning to survey the (90+/-) coupon book participants through a follow-up mailing. The committee had suggested that this mailing hold a dual purpose of surveying the utilization of the coupons, as well as formal, sign up for next year. The committee is recommending that this booklet be paid by the merchants at a cost/materials rate of approximately \$25.00 each. The shuttle patrons were excited about the free booklets with the free upcoming Sea Dogs tickets being a big hit.

HADLOCK FIELD

FOLLOW - UP REVIEW

of the

1994 Sea Dogs

PARKING & TRAFFIC OPERATIONS

I. Executive Summary

II. Traffic

III. Parking

IV. Marketing

V. Budget

VI. Conclusions/Recommendation for 1995

*Prepared By: Hadlock Field Parking & Traffic Operations Committee
Mary Ann Theriault, Principal Traffic Engineer/Chairman*

November 1994

I. EXECUTIVE SUMMARY

As John Boles of the Florida Marlins stated in his press conference on April 18th, "...*Everything that was promised, was delivered.*" Following the season's opening game, Mr. Boles was very impressed with the fan support and overall operations. This supportive statement was a very good omen for the Hadlock Field Parking & Traffic Operations Committee.

During the entire premier baseball season, the City of Portland and the Portland Sea Dogs received *repeated positive comments* concerning the Parking & Traffic Operations for Hadlock Field. Throughout the season, a total of three minor complaints were handled by the Traffic Division: (1) a resident's request for no parking at his driveway, (2) a businesses request to slightly reduce the bus zone on St. James Street to allow for customer parking, and (3) the manager for Sonesta requested more frequent shuttle operations from their facility on High Street. The first two concerns were addressed and resolved immediately. The request from Sonesta was discussed in detail; however, the low ridership did not result in more bus service downtown (See Attachment # 1, 2).

Hadlock Field and the EXPO combined received 1 MILLION visitors over a course of a year (Sept. 1993-Sept. 1994) with virtually no significant parking and traffic problems. The entire committee continuously updated and fine-tuned the operations throughout the season. By mid-June, substantial base operations were in successful working order... thanks to all the participants hard work...

Overall, the committee feels successful in accomplishing their primary goal of "providing safe and efficient pedestrian, bicycle and vehicular operations" for the baseball season.

II. TRAFFIC

The following information details the finding of the random monitoring program as defined in the Hadlock Field Operations Handbook, pages 14 & 15:

Accident Data

The Portland Police Department researched a total of (22) accidents occurring within the study area from April 1 to September 5, 1994: (4) Park/High, (3) Park/Deering and (15) I-295/Forest Avenue. Only three of these accidents occurred on baseball game dates and all three were located at the I-295 off-ramp on Forest Avenue. This location has been identified as a high accident location to be corrected by MDOT in the Hadlock Field Traffic Study.

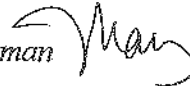
Signal Timing/Operations

The vehicular traffic arrived randomly 60-90 minutes prior to gametime and departed within minutes after the game. As anticipated, traffic cleared within 15-20 minutes; therefore, the Traffic Division did not deem it necessary to program any special timing for the area traffic signals. The bicycle traffic was very low due to the nighttime hours of most ballgames.

Memorandum - Hadlock Field Parking & Traffic Operations Committee

*Mary Ann Theriault, Chairman, Principal Traffic Engineer PW
John Peverada, Parking Manager, Portland Parking Division
John Brennan, Deputy Police Chief, Portland Police Department
Frank LaTorre, Facilities Manager EXPO/Hadlock Field
Charlie Eschbach, General Manager Sea Dogs
Sarah Dedoes, General Manager METRO
Liz Darling, Economic Development*

*To: All Committee Members
From: Mary Ann Theriault, Traffic Principal Engineer/Chairman
Date: November 14, 1994
Subject: End of the Season Report*



Congratulations and Thank You for a very successful Parking & Traffic Program for the Seadogs first season in Portland, Maine!!! I sincerely appreciated the time and effort each and every one of you contributed to our program.

Enclosed is a copy of the "End of the Season" report for Parking & Traffic for Hadlock Field. As we discussed at our September 30th meeting, this report also addresses recommendations for the upcoming season.

I have sent out thank you letters for all the participating "private-enterprise" parking managers (mailing list enclosed). Please let me know if I have missed anyone. I feel that they were an important part of our program's success. I have also forwarded a copy of this report to DEP for their records.

cc: All Committee Members
Robert Ganley, City Manager
Nadeen Daniels, Assistant City Manager
Anita LaChance, Assistant City Manager
William Bray, Deputy Director PW
Larry Mead, BB Operations Chairman
Paul Willey, Parking Garage Manager, Parking Division
John Tibbetts, Operations Manager, METRO
Sgt. Tony Germaine, Portland Police Department
Capt. Ed Googins, Portland Police Department
Alex Jaegerman, Planning/DEP Coordination

Maine Center for the Blind & Visually
Impaired
attn: Robert Crouse
189 Park Avenue
Portland, ME. 04102

Maine Center for Cancer Medicine
ATTN: Jayne A. Soles
180 Park Avenue
PO Box 553, DTS
Portland, ME. 04112-0553

Ramada Inn
ATTN: Don Haggett
1230 Congress Street
Portland, ME. 04102

Medical Services Federal Credit Union
ATTN: Ken Aker
272 Park Avenue
PO Box 10659
Portland, ME. 04101

Maine Orthotic Lab
ATTN: Alison Perkins
300 Park Avenue
Portland, ME. 04102

Libby/Morse/Taylor
ATTN: J. Michael Taylor
295 Park Avenue
Portland, ME. 04102

New England Wheel & Rim Co.
ATTN: Maurice Joy
312 St. John Street
Portland, ME. 04102

J. Weston Walsh
ATTN: Suzanne Austin
321 Valley Street
PO Box 658
Portland, ME. 04104-0658

Lock, Stock & Barrell
ATTN: Brian Wallace
369 St. John Street
PO Box 939
Portland, ME. 04104

Dick Cole Tire Co.
ATTN: Dick Cole
333 St. John Street
Portland, ME. 04102

Stadium Pizza & Grille or Current
Resident
325 St. John Street
Portland, ME. 04102

Glidden Paint & Co.
971 Congress Street
Portland, ME. 04102

HP Hood, Inc.
349 Park Avenue
Portland, ME. 04102

Hadlock Field Operations Handbook
Double A Baseball Park

Prepared by:

Mary Ann Theriault
City of Portland, Traffic Principal Engineer

In Cooperation with:

Eaton Traffic Engineering
Hadlock Field Traffic & Parking Operations Committee
Baseball Operations Committee

April 1994

DRAFT
HADLOCK FIELD
OPERATIONS HANDBOOK
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I. EXECUTIVE SUMMARY

PURPOSE

The Hadlock Field Operations Handbook is intended to be a guide to the specific actions to be undertaken to insure efficient and safe pedestrian, bicycle, and vehicular operations. Procedures are set forth for all parties to follow to best manage the coordination, circulation, parking and access to and from ballgames.

This Handbook will be a "working" guide to the Hadlock Field Traffic & Parking Operations Committee, which has been established for continuous fine-tuning of the handbook throughout the season.

The Operations Committee strongly supports "creative" incentives to encourage baseball patrons to utilize alternate modes of transportation. Environmental concerns emphasize our commitment to ridesharing, shuttles, METRO, walking and bicycling. Throughout this handbook, and all joint marketing publications, these alternatives will be given special attention and consideration.

The success of this manual depends on continuous communication between all the parties involved City/EXPO/Sea Dogs personnel, METRO, the USM area, parkside neighbors, and the general public.

AMEND and UPDATE

Methodical evaluations of the actual access and parking habits of ballpark patrons will be conducted with the cooperation of all parties. As behavioral patterns are observed in the field, fine-tuning of this plan will occur immediately. To address the needs expressed by these patterns, a "random" monitoring plan is outlined to amend and update the traffic data for further improvements (see *Section III. D. Monitoring*). Only experience will reveal the preferred modes, routes and parking habits of the ballpark patrons.

COMMENTS

For excellence in customer service, we are encouraging any questions and comments throughout the season to be directed to the Portland Sea Dogs, 874-9300. A Baseball Parking Hotline, 874-0945, has been established for a recorded message on directions to Shuttle Lots, Parking & Hadlock, similar to the Parking Ban phone line. Our joint marketing efforts will promote and display these numbers wherever possible.

A. PARKING SPACE TOTALS

PUBLIC - OFF STREET LOTS

Fitzpatrick Lot	106 (403 Tandem)
King School	74 (215 Tandem)
Hadlock Field, Lot A	31 + 2 HC
Hadlock Field, Lot B	40-45 + 8 HC + 30-60 BIKES (100)
St. James Street Lot	<u>58</u>
SUBTOTAL	309 + 10 HC + 30-60 BIKES (807)

PUBLIC - ON STREET PARKING

Deering Ave (Park to Washburn)	141
State St (Park Exit to Exit- WS only)	45
St. James St (entire)	<u>209</u>
SUBTOTAL	395

PRIVATE - OFF STREET LOTS

USM Lot	500*
Maine Medical Center	500(weekdays)-700(weekends)**
USM - Marginal Way SHUTTLE LOT	<u>369*</u>
SUBTOTAL	<u>1369 (1569)</u>

* Available from May 15 to September 1 weekdays, anytime on Weekends/Holidays

** Available after 5:30 pm weekdays, anytime on Weekends/Holidays

TOTALS 2073 - 2771 Parking Spaces
+ 10 HC + 30-60 BIKES

Note: 1625 SPACES REQUIRED FOR A SELL-OUT GAME
(As defined in Table 2, Hadlock Field Traffic Study, January 1993)

B. PRIVATE ENTERPRISE

The City of Portland Traffic Division and the Portland Exposition Staff hosted an informational meeting for all Parkside Businesses and other interested parties. Our focus was to encourage and assist businesses in utilizing their existing parking lots for Hadlock Field/EXPO events. Generally, the event times for baseball/EXPO are scheduled during non-office hours for most area businesses; therefore, their parking lots would be empty if not utilized for event parking.

Providing parking for Hadlock Field/EXPO events provides an excellent economic opportunity for their business or a non-profit group of their choice. The Maine Special Olympic Organization for Maine has voiced an interest in coordinating efforts in managing the parking attendant/fee process for any area parties.

The interested businesses currently use their lots for commercial purposes and may continue to participate without further City authorization or review.

The following quick checklist was distributed to the sixteen interested parties:

- 1. Check your business location for proper zoning
(The Traffic Division has provided this service for all businesses returning the questionnaire)*
- 2. Check your current liability insurance policies for proper coverage*
- 3. Utilize portable barricades and sign to direct cars where to park*
- 4. Utilize all safety precautions, such as: directional flashlights, adequate lot lighting and fluorescent vests*
- 5. Provide an attendant to collect the money and watch over the lot during event times*

The following represents the potential parking spaces to be provided by private enterprise:

PRIVATE ENTERPRISE - OFF STREET LOTS

Ramada Inn SHUTTLE LOT	100
Maine Center for the Blind, 189 Park Ave.	50-70
Credit Union, 272 Park Ave.	20-25
Maine Orthopedic Lab, 300 Park Ave.	15-20
Medical Office, 295 Park Ave.	35-40
Maine Center for Cancer, 180 Park Ave.	50-70
J. Weston Walsh, Gilman/Park Ave.	<u>50-75</u>
TOTAL	470 (600)

C. PARK 'N RIDE LOTS

The existing METRO Park 'n Ride lots are available for all games; however, the coordinating METRO service hours do not cover the departure times for night games at this time. METRO's regular service is available for day games only, 22 of 71 gamedates. These lots can be utilized for *ridesharing* from the Westbrook, Gorham and Windham areas for all gamedates.

METRO PARK 'N RIDE LOTS

American Legion- Rt. 25 Westbrook/Gorham Townline	50
Bradlees Shopping Center Westbrook, Rt. 25	300
Bowling & Recreation Center Prides Corner/Rt. 302	<u>30</u>
TOTAL	380

DOWNTOWN PARK 'N RIDE LOTS/GARAGES

The Downtown Park 'n Ride lots and garages can be utilized for all game times. The current Downtown Park 'n Shop program provides the opportunity for patrons to receive stamps for one hour free parking if they shop at the participating Downtown businesses. Participating facilities will allow 4-5 stamps per vehicle.

Several Lots/Garages are identified in the current brochure *Portland...A lot of fun for a little city*. This publication was produced by the Portland Downtown District. Whenever applicable, this brochure will be referenced in the Marketing Plans of the Portland SeaDogs, City of Portland, METRO and Downtown/Parkside merchants.

TOTAL 500 (minimum estimated availability)

A. ACCESS

PEDESTRIAN ACCESS

Primarily, pedestrian access to and from the Ballpark is provided by existing sidewalks on one or both side of all the city streets within the area. Along Park Avenue, crosswalks exist at all signalized intersections as well as Park Avenue/Gilman Street and Park Avenue/Weymouth Street. A Portland Police Officer is recommended for pedestrian management at the Gilman Street/Hadlock Field Entrance crosswalk. (See Section VII. F. Control Plans)

Overhead sign along Park Avenue will direct pedestrians and notify vehicular traffic of "Ballpark Entrance Ahead" at Weymouth, westbound and "Ballpark Entrance - STOP for pedestrians in Crosswalk" at the entrance at Gilman Street, both directions.

The Traffic Division will monitor the effectiveness of the overhead sign and upgrade with additional sign or flashing beacons if necessary and recommended by the Parking & Traffic Operations Committee.

For the blind and visually impaired patrons, an audible pedestrian signal is currently operating at the Park Avenue/Deering Avenue and Mellon Street/Park Avenue intersections. The bell sounded is activated by the pedestrian push buttons positioned on the traffic signal posts located on all corners.

BICYCLE ACCESS

Bicycle access is maintained along regular vehicular traffic. *Priority parking* is given to bicyclist in Hadlock Field, Lot B. The designated areas are flexible to accommodate 30-60 bikes utilizing portable bike racks. More bicycle patrons are expected on weekends as opposed to night games.

The Greater Portland area has recently formed a Regional Bike and Pedestrian Advisory Committee. As more detailed pathways are developed and defined, they will be included in our Operations Handbook.

BUS ACCESS

Along Park Avenue, temporary barrels, sign and orange cones will delineate a *priority bus lane* from approximately the Maine School for the Blind to St. John Street. The priority bus lane will be located in the parking lane leaving the four travel lanes free for vehicular use. (See Section VII.F. Control Plans)

A *Priority Drop-off* area is established for METRO Shuttle, Tour and School buses within the priority bus lane in the shared area in front of the Exposition Building and Hadlock Field. Tour and School Buses will also pick-up passengers at this location; however, they will be directed to park on St. James Street during the game (See Section VII. C. Tour Bus Routing and Parking).

METRO Shuttle Buses will load passengers at the existing bus stop at the intersection of St. John Street and Park Avenue. The availability of the shuttle service throughout the game is critical to its success; therefore, they will be allowed to park in the existing bus zone during the game.

Bus/shuttle attendants are recommended for the passenger drop-off areas for efficiency and enforcement. The attendants will monitor the management of these designated areas and amend the operations as needed.

Most of the priority bus lane is currently posted "No Parking Anytime" and neighborhood parking will be minimally affected. A seasonal passenger drop-off will be established in the section of existing "unrestricted" parking located between Hadlock Field Lot A & B. An existing "Handicapped Reserved" parking space in front of the Exposition Building will not be available during gametimes.

The area will be posted in the following manner, (exact wording and times to be fine-tuned, as needed): Hadlock Field Passenger Drop-Off, April 1 to Oct 1-11am to 11pm, 5 minute idling time, tow-away zone. This would allow residential parking for overnight hours.

Careful arrangement of the temporary structures will assure adequate site distance for safe pedestrian crossing at Gilman Street/Hadlock Field Entrance.

METRO and Shuttle Incentives are detailed under *Section IV. E. Transit Incentives.*

VEHICULAR ACCESS

Several Public and Private Lots are available in the area of the ballpark. An overview map depicting the general locations is shown in *Section VII. A. Parking Alternatives*, and an abbreviated version of the directions to parking is detailed in *Section VII. B. Directions to Parking for Hadlock Field*.

All informational sign and literature encourages the use of bordering parking alternatives: **METRO Park 'n Ride Lots** (Rt. 25, Westbrook/Gorham Townline, American Legion, Westbrook Bradlees, and Rt. 302, Prides Corner), **METRO Shuttles** from: (1) USM - Marginal Way Lot; (2) the Congress Street Ramada Inn; and (3) the Downtown area (Middle Street in front of Post Office Park and High Street in front of the Sonesta), as well as the Gilman Street **Maine Medical Center Parking Garage**.

Hadlock Field, Lot B will be reserved for Disabled/Handicapped patrons, bicyclist, and carpools (vehicles with 4 or more ballpark patrons). Signs and Parking Operations personnel will clearly and politely notify baseball patrons of the reserved status and direct all unqualified vehicles to other lots.

EMERGENCY ACCESS

Emergency access to the Ballpark will be accessible through all entrances. A Medical Emergency Vehicle will be parked at each game in a reserved parking area in Hadlock Field, Lot A. Per request of the Portland Fire Chief, the entrance to this lot will be kept open at all times or manned by a parking attendant throughout the game.

Per request of the Portland Fire Chied, the King School Lot will have cones delineating a combined Pedestrian-Emergency pathway during each game.

B. DIRECTIONS

DIRECTIONS TO HADLOCK FIELD

The directions to Hadlock Field will be used for directing patrons to the priority bus drop-off area, (see Section VII. C. Tour Bus Routing and Parking).

From the North

I-95/295 EXIT #6B and/or Forest Avenue, turn right at the first traffic signal onto State Street, and turn right onto Park Avenue to Hadlock Field.

From the Northwest

Forest Avenue to State Street, turn right on State Street, and right on Park Avenue to Hadlock Field

From the South

I-295 EXIT #5 and/or Congress Street to Weymouth Street, and turn left onto Park Avenue to Hadlock Field.

From the East

Take the ferry and then METRO! OR take the Million Dollar Bridge, Route 77, to High Street, and turn left onto Park Avenue to Hadlock Field.

From the West

Brighton Avenue to USM Law School, bear right on Deering Avenue to Park Avenue and turn right onto Park Avenue to Hadlock Field.

DIRECTIONS TO PARKING AT HADLOCK FIELD

The directions to parking at Hadlock Field lead patrons to Shuttle Lots or the peripheral parking areas, USM Bedford Street Lot and the Maine Medical Center Gilman Street Garage, (see Section VII. B. *Directions to Parking for Hadlock Field*).

From the North

I-95 or Washington Avenue (Route 26) to I-295 EXIT #7 Franklin Arterial to Marginal Way, turn right at the end of the ramp and the USM - Marginal Way SHUTTLE LOT is located 500 feet on the right.

From the Northwest

Forest Avenue to Prides Corner Bowling & Recreation Center, METRO Park 'n Ride, OR Forest Avenue (Route 302) to Bedford Street, turn right onto Bedford Street and the USM parking lot is located on the left.

From the South

I-95 to I-295 to Congress Street EXIT #5, straight through two traffic signals, turn right on Gilman Street, the Maine Medical Center Parking Garage is immediately on the left.

From the Southwest

County Road to Congress Street (Route 22) to the Ramada Inn - SHUTTLE LOT.

From the West

Brighton Avenue (Route 25) to Douglas Street, turn right and take first left on St. James Street for on/off-street parking; OR continue east on Brighton Avenue and turn left on Bedford Street to the USM Bedford Street Lot; OR take Route 25 to the METRO Park 'n Ride at the American Legion at the Westbrook/Gorham townline.

From the East

Take the ferry and then METRO or the METRO Shuttle from Post Office Park, OR take the Million Dollar Bridge (Route 77), to Commercial Street, and take Valley Street at the Veteran's Bridge Rotary to "A" Street, and turn right then left onto Gilman Street to the Maine Medical Center Parking Garage.

C. SIGNING

PERMANENT SIGNS

All permanent signs are to read "Municipal Event Parking" or similar wording to coordinate with the overall Marketing Plan detailed in *Section VI. Marketing*. All permanent signs will be installed on new or existing u-posts throughout the ballpark area. The locations are to be coordinated by the Hadlock Field Parking & Traffic Operations Committee.

The City Traffic Division will coordinate, create, and install all permanent signs prior to April 15, 1994. Upon review of any complaints or confusion, additional permanent signs may be added during the baseball season (*see Section VII.D. Preliminary Signing Plan*).

All permanent signs is to remain year round, except sign to USM lots and Maine Medical Center Parking Garage.

The Traffic Division is currently in the process of obtaining authorization for permanent signs on the Interstate and the Maine Turnpike Authority.

TEMPORARY SIGNS

Each parking lot/garage will have a sandwich board sign which will read "Municipal Event Parking" or similar wording, along with additional information, such as "LOT FULL". Other temporary signs will be placed on the portable barrels along Park Avenue to identify the priority bus lane.

The City Traffic Division will coordinate and create all temporary signs for the ballpark. Again, temporary signs may be added as deemed necessary by the Hadlock Field Parking & Traffic Operations Committee.

Parking Attendants for each lot will be responsible for the layout/pick-up of all temporary signs as directed by the Operations Committee and detailed in *Section VII.F. Control Plans*. The temporary signs are to be stored at Hadlock Field at a location to be coordinated with Sea Dogs personnel.

D. MONITORING

IMPROVEMENTS

The following improvements were recommendations listed under the City's Traffic Study, Bill Eaton's Review or DEP Letter of Approval. These improvements are either completed or will be completed prior to April 15, 1994 by the City Traffic Division personnel and other related parties.

City's Traffic Study

1. Park Avenue/Marston Street - (1) review and correct any sight distance restrictions, (2) install an additional stop sign on the right side of the street, and (3) install a painted stop bar on the existing pavement.
2. Park Avenue/St. John Street - Install a painted right turn lane on the southbound leg of St. John Street.
3. Park Avenue - Designate and sign a seasonal bus/passenger drop-off area.
4. Hadlock Field - provide bike racks and an expanded pedestrian walking area (included in new site design).
5. Areawide - create and install permanent sign, as needed.
6. Areawide - create and provide temporary sign, as needed.

Bill Eaton's Peer Review

1. Lighting - upgrade, if needed, the lighting at the entrance of all on-site parking lots and at the main crosswalk at Gilman Street/Hadlock Field entrance.
2. Pedestrian - coordinate and provide a Portland Police Officer for pedestrian control and management.
3. Tandem parking - if used, fixed barriers should be installed to allow vehicles flexibility in departing from their space.

IMPROVEMENTS continued...

DEP's Letter of Approval

1. State Street/Congress Street - install additional overhead or post mounted lane use signs.
2. Marginal Way/State Street/Forest Avenue - (1) increase the clearance interval for the left-turning traffic, (2) replace the current 4-section signal with a 3-section signal and signing for a left-turn on green arrow only, (3) in lieu of creating a painted island to separate lanes, an overhead lane-use sign will be installed on the I-295 overpass and (4) extend the white dashed lane line for improved lane separation.
3. High Street/Park Avenue - (1) increase the "all red" clearance interval, and (2) fine tune the traffic signal system progression on High Street to slow traffic.

MONITORING

The traffic and parking monitoring plan recommendations were listed under the City's Traffic Study, Bill Eaton's Peer Review and DEP's Letter of Approval. The City Traffic Division personnel will be responsible for coordinating the data collection. The information and finding will be reviewed with the Hadlock Field Parking & Traffic Operations Committee prior to any changes made in the field.

This "random" monitoring plan will include, but is not limited to, the following tasks:

1. Accident Data - Randomly review and monitor all High Accident Location's (HAL) in coordination with the City's Police-Traffic Division.
2. Signal Timing Plans - After "real-life" observations, improve the signal timing for ballgame dates, as needed.
3. Pedestrian - Monitor and improve the safety conditions for pedestrians and bicyclist, as needed.

MONITORING continued...

4. Handbook - After "real-life" observations and behavioral patterns are established for parking and traffic operations, update and implement the changes in the field operations, as needed throughout the baseball season.
5. Parking - (1) Randomly repeat the parking counts on City Streets and parking lots (Table 1 - Traffic Study), and (2) compare attendance with parking counts.
6. Residential areas - (1) provide parking enforcement officers and monitor their activity, (2) respond to citizen request and jointly develop solutions that are appropriate to the actual problem that arises.
7. Traffic Operations - Randomly review the overall operations and make improvements, as needed.

A. METRO Bus Service

The METRO operates fixed route bus service in the cities of Portland, Westbrook and the South Portland, Maine Mall area. The METRO presently provides services in the vicinity of Hadlock Field with three regular routes:

1. Route 3 - serves the North Deering area
2. Route 5 - serves Parkside, Westgate, Stroudwater, and the Maine Mall
3. Route 1 - serves the Peninsula from Munjoy Hill to the West End

These services operate seven days a week on a very diverse schedule ranging from 5:40AM to 9:40PM; however, these hours will accommodate ONLY day games (22 of the 71 gamedates). The departure hours for night games are not available at this time.

METRO is currently investigating the possibility and feasibility of expanding its regular hours to better accommodate baseball patrons. Currently, the METRO serves approximately 4500 patrons daily.

B. SOUTH PORTLAND Bus Service

Currently, South Portland Bus Service offers fixed route service to South Portland and Portland which includes a Congress Square and Old Port drop-off. These regular stops will coordinate with the new Shuttle Stops for Downtown.

These services operate six days a week on a very limited schedule ranging from 6:00AM to 5:00PM; however, these hours will accommodate ONLY day games excluding Sundays (7 of the 71 gamedates).

The South Portland Bus Service is currently investigating the possibility and feasibility of expanding its regular hours to better accommodate baseball patrons.

C. SHUTTLEBUS - Biddeford, Saco, Old Orchard Beach

The SHUTTLEBUS currently operates a fixed route bus service from Biddeford, Saco, Old Orchard Beach, Scarborough to Portland. The SHUTTLEBUS presently provides a service stop at the Greyhound Bus Station at the intersection of St. John and Congress Streets, walking distance to Hadlock Field.

These services operate six days a week on a limited schedule from 7:45AM to 5:30PM, excluding Sundays; however, these hours will accommodate ONLY day games (22 of the 71 gamedates). The departure hours for night games are not available at this time.

SHUTTLEBUS is currently investigating the possibility and feasibility of expanding its regular hours to better accommodate baseball patrons.

D. New SEA DOGS SHUTTLE

SHUTTLE LOCATIONS

Currently, METRO is working with the City to coordinate shuttle service for all Sea Dogs games from the following locations:

1. Congress Street - Ramada Inn Parking Lot
2. Marginal Way USM Parking Lot
3. Middle Street (at PO Park) and High Street (at the Sonesta)

BUS ROUTING

METRO is estimating a maximum turn-around time of 10 minutes for each shuttle run. The Shuttles will operate approximately 40 minutes before the game and approximately 30 minutes following the game.

METRO will dispatch as many buses as needed to maintain a 10 minute turn-around time for shuttle passengers. METRO has developed three routes to provide shuttle service to the above locations as shown in *Section VII. E. Shuttle Bus Routing and Parking Plan.*

SHUTTLE FEES

A minimal fee of \$1.00 per passenger will be necessary to support the shuttle costs. Children under 5 ride FREE. An Adult will pay for a maximum of 2 children, all others ride FREE. A payment is made at the pick-up and the ride back is free.

Area businesses are interested in prepurchasing shuttle tickets for their patrons. Tickets will be available through METRO; however, all other passengers will pay as they board the shuttle bus. METRO will develop the feasibility of reduced rates for pre-purchase of tickets in quantity.

INCENTIVES

Priority Front Door drop-off, priority bus lane, shuttle coupon books, prizes from the Sea Dogs are some of the incentives initiated for shuttle passengers. The Marketing Plan will further develop the "fun & excitement" around shuttle lots.

A shuttle booklet containing coupons for Downtown and Parkside Businesses has been created and 10,000 copies will be produced. Other incentives have been developed for shuttle passengers as detailed under *Section IV. E. Transit Incentives*.

All tickets and incentives will be organized and managed by METRO. The initial setup of the incentives has been organized in a partnership of City of Portland - Traffic Division and Economic Development, METRO, Sea Dogs and Portland Downtown District.

DRIVERS/DISPATCHERS/ATTENDANTS

The shuttle bus drivers will be available throughout the game to give patrons a ride back to the shuttle lots/stops.

Shuttle attendants will be coordinated for all shuttle lots/stops approximately 40 minutes before game times. They will be responsible for bus dispatch coordination and crowd control. Hadlock Field will have a dispatcher/attendant on-site before, during and after the game for every Sea Dogs home game.

MANAGEMENT

METRO has reviewed all bus parking and developed the plan for routing. They have also reviewed and approved all the pick-up/drop-off sites. Fine-tuning of these locations will occur throughout the season in cooperation with the Operations Committee.

For fine-tuning of the new shuttle program, METRO will monitor the program and report back to the Operations Committee throughout the baseball season.

METRO will assign a spokesperson to answer any questions concerning the Shuttle Program.

METRO is organizing an informational field trip to Boston College to review their new shuttle program created for their recent stadium expansion. Other members of the Operations Committee will participate in the trip as well.

For the next baseball season, METRO will investigate the feasibility of coordinating the Sea Dogs Shuttle with other METRO Shuttle Programs. METRO is currently establishing a Downtown Shuttle for business employee parking.

E. TRANSIT INCENTIVES

METRO - FREE RIDE HOME Bus & Buy Program

The SeaDogs Staff will coordinate and purchase METRO stickers for ballpark patrons. If a baseball fan arrives at the game by METRO, stickers will be available at the Ticket Office for a FREE RIDE HOME! Several other Portland Businesses currently participate in this successful and growing program.

COSTS:	\$1.00 value - sticker costs shared equally between the sea Dogs and METRO
RESTRICTIONS:	Must arrive at the game by METRO and present your METRO pass at the Ticket Booth. When purchasing a game ticket, the patron must ask for a METRO stamp and they will receive a sticker for a <i>free ride home</i> .
PURCHASE:	Stickers may be obtained at: METRO 114 Valley Street 8:00am - 4:00pm

SEA DOGS SHUTTLE Coupon Books

The City of Portland, Economic Development Department and Traffic Division, METRO, Sea Dogs, and Portland Downtown District have coordinated a FREE coupon booklet to SHUTTLE passengers. Our joint Marketing Plan is to encourage Sea Dog fans to "**Spend the Day in Portland**".

The booklet contains coupons/offers good at participating restaurants and businesses in the Downtown and Parkside areas. The booklet will also have a blank page for a "special" drawing to be coordinated and sponsored by the Sea Dogs (ie: free game tickets, T-Shirts etc).

COSTS:	FREE!
RESTRICTIONS:	Coupons/offers are only good at participating stores/restaurants
PURCHASES:	Personally handed out by Shuttle Bus Drivers

SEA DOGS SHUTTLE PASSES

The OPRLA (Old Port Restaurant and Lounge Association), OPMA (Old Port Merchants Association) and PDD (Portland Downtown District) strongly support a direct shuttle from the Old Port to the Ballgame. Their members have an interest in pre-purchasing shuttle tickets to give to patrons. The Downtown shuttle stops will coordinate with the new Tour Bus Plan at its Middle Street (Post Office Park) location and on High Street (Sonesta Entrance).

COSTS: \$1.00 per person (costs to be paid by the merchants/patron)
RESTRICTIONS: none
PURCHASES: Shuttle passes available at:
METRO
114 Valley Street
8:00am - 4:00pm

DOWNTOWN PARK & SHOP PROGRAM

Park & Shop is currently popular among the Downtown shoppers and merchants. The coordinating Downtown Shuttle will encourage shoppers, businessmen and women to keep their cars downtown and get a ride to the game and back. Merchants and customers are familiar with the program. The Shuttle Marketing plans will include information on the Park & Shop program whenever possible.

The Traffic Division and the Portland Downtown District completed new directional sign to Park & Shop Lots/Garages last summer.

COSTS: \$57.00/booklet for 100 one hour stickers
\$54.00/booklet for four or more purchased at the same time.
Each sticker is redeemable for one free hour of parking at participating facilities.
RESTRICTIONS: Customer use only- not intended for employee use
Place stamp onto customer's parking ticket upon request
PURCHASES: Booklets available at:
Maine Bank & Trust
467 Congress Street
8:00am - 4:00pm

Portland Downtown District
477 Congress Street
8:30am - 5:00pm

A. The ROLE of the Hadlock Field Parking & Traffic Operations Committee

Hadlock Field Parking and Traffic Operations Committee has been organized to continuously fine-tune the operations at Hadlock Field throughout the baseball season.

The team players consist of representatives from the following entities: City of Portland - Traffic Division, Police Department and Parking Division; Portland Sea Dogs; Portland Exposition Staff; and METRO.

The following general tasks are identified for each team player (detailed responsibility will be fine-tuned within the Operations Committee prior to and throughout the baseball season):

PORTLAND SEA DOGS

The Portland Sea Dogs will be responsible for the following tasks although they intend to hire EXPO security to provide the services :

1. Manage and coordinate the necessary Parking Attendants/temporary signs for the following areas around the ballpark:
 - A. Hadlock Field, Lot A
 - B. Hadlock Field, Lot B
 - C. Fitzpatrick Lot
 - D. Ice Arena Lot
 - E The Bus Drop-off Area
 - F. Bus Parking Area, if needed
 - G. Temporary signage/barrels/cones along Park Avenue
2. Randomly document the parking volumes at each of the above lots in coordination with the Traffic Division's Monitoring Program
3. Actively participate in the Operations Committee to keep all parties abreast of any concerns and conflicts
4. Actively participate in the Promotional Plan for Parking Alternatives at Hadlock Field and coordinate any other marketing efforts

PORTLAND EXPOSITION

1. Manage and coordinate the necessary Parking Attendants/temporary signs and parking fees at the King School Lot
2. Randomly document the parking volumes at the above lot in coordination with the Traffic Division's Monitoring Program
3. Actively participate in the Operations Committee to keep all parties abreast of any concerns and conflicts
4. Actively participate in the Promotional Plan for Parking Alternatives at Hadlock Field and coordinate any other marketing efforts

City of Portland - PARKING DIVISION

1. Manage and coordinate the necessary Parking Attendants, temporary signs and the parking fees at the Maine Medical Center Parking Garage
2. Randomly document the parking volumes at the above garage and other parking documentation in coordination with the Traffic Division's Monitoring Program
3. Provide adequate Parking Control Officers to "randomly" enforce the Deering Oaks Park and surrounding neighborhood parking regulations
4. Assist the Portland Police Department with Traffic Control, if needed.
5. Actively participate in the Operations Committee to keep all parties abreast of any concerns and conflicts
6. Actively participate in the Promotional Plan for Parking Alternatives at Hadlock Field and coordinate any other marketing efforts

City of Portland POLICE DEPARTMENT

1. Manage and coordinate the necessary Police Officers for traffic control at the following locations:

- a. Congress Street/Gilman Street*
- b. Congress Street/Maine Med. Garage Exits (2)*
- c. Park Avenue/St. John Street**
- d. Park Avenue/Weymouth Street**

*Traffic Control is suggested for every game, before and after game times

**Traffic Control is suggested for the first game and other promotional games, as needed.

2. Provide an adequate number of Police Officers for pedestrian control and management at the Entrance to Hadlock Field/Gilman Street crosswalk for every game.
3. Actively participate in the Operations Committee to keep all parties abreast of any concerns and conflicts

City of Portland TRAFFIC DIVISION

1. Manage and coordinate the parking and traffic monitoring program
2. Create, coordinate and install all directional, regulatory and misc. signs related to the baseball project area
3. Monitor the effectiveness of all signs and update as needed
4. Complete and/or coordinate all the recommended traffic improvements detailed in *Section II.D. Monitoring*
5. Review and complete any other necessary traffic improvements throughout the season as discussed with the Operations Committee
6. Actively participate in the Operations Committee to keep all parties abreast of any concerns and conflicts
7. Actively participate in the Promotional Plan for Parking Alternatives at Hadlock Field and coordinate any other marketing efforts

B. TYPES of CONFLICTS

Dual Events

In event there are dual events, temporary signs will be expanded to coordinate with the individual circumstances. The plan will be coordinated by the Operations Committee. Tandem parking is not intended to be part of our parking plan; however it is an alternative for extraordinary circumstances.

Ice Arena Schedule

The Ice Arena will share parking with any Sea Dogs game running concurrently with an Ice Arena event. The attendant at the entrance will assure a drop-off area for Ice Arena patrons at the entrance area. The possible conflict times are from July 18 to September 15, where the Ice Arena will be open from 8:00am to Midnight.

School Schedules

Due to the night/weekend baseball schedule, the King Jr. Lot will be available for most of the Sea Dogs games during any season year. However, King Jr. has currently been funded for major renovation work expected to begin this summer. Construction is planned to start with asbestos removal on June 18th. Construction activity will most likely be limited to daytime hours.

This lot will be managed and coordinated for all possible games by a parking attendant from the School Department (EXPO security). Special care will be given to assure that the lot is secured to prevent parking whenever construction vehicles are working on site.

The Public School is in session until June 17th and the USM School is in session until May 15th. Due to the night/weekend baseball schedule, minimal conflicts are expected to effect parking operations/availability. Temporary signs will direct and notify patrons accordingly.

Early Start Weekday Games

The Sea Dogs have (4) games scheduled when the Maine Medical Center Garage is not available. Temporary sign and marketing advertisements will direct and notify patrons accordingly. The Sea Dogs Shuttle program will be operational for all games and shuttle sites will be strongly suggested to patrons attending these games. The Back Bay parking area will be used for overflow parking from the Marginal Way Shuttle Lot, if needed.

Play-off Games

We welcome this type of conflict! In the event the Sea Dogs are involved in any play-off games after the regular season ends, the Operations Committee will coordinate the necessary arrangements. The Sea Dogs will also coordinate any advertisement for the play-off games to include necessary parking information.

Deering Oaks Park

If approved by the City Council, the Deering Oaks Master Plan is currently in the process of eliminating 95% of the parking within Deering Oaks. The Operations Committee will coordinate their parking enforcement efforts to enforce the current regulations or any new restrictions the City Council approves throughout the season.

Residential Parking Issues

The Traffic Division has submitted a proposal to the Portland City Council to increase the regulatory powers of the Traffic Engineer. With successful approval, the Traffic Engineer will be able to determine and erect any time limit parking sign. Therefore, the potential response time for addressing any parking abuse problems during ballgame times can be resolved in a timely manner. The Safety Committee voted unanimously to support the proposal.

RESIDENTIAL PARKING STICKERS

The Parkside and USM area residents are encouraged to obtain the FREE residential parking stickers for their respective districts. Both areas currently are established Residential Districts.

COSTS:	FREE
RESTRICTIONS:	The stickers must be affixed to a motor vehicle windshield with a State of Maine registration. The sticker is only applicable to the district in which one resides (proof of residence address is required) The sticker overrides a one or two hour parking restriction within the respective district.
PURCHASES:	The stickers are available at: City of Portland-Parking Department City Hall, 289 Congress Street 8:00am to 5:00pm

Additional time limit parking zones will be determined on an individual basis as reviewed and deemed necessary by the City of Portland, Traffic Engineer and concerned citizens. Random parking studies in the Parkside and USM area residential districts is part of the Traffic Division's Monitoring Plan detailed under *Section III. D. Monitoring.*

A. PROMOTIONAL PLAN

Parking Alternatives for Portland Sea Dogs' Games

OVERVIEW

With large crowds anticipated for Portland Sea Dogs' home games, it is essential to convince people to find an alternative to "driving down Park Avenue to find a parking space right at Hadlock Field." A significant amount of work has gone into developing a shuttling plan and alternative parking sites.

This plan will go for naught if people do not actually USE these alternatives. It is difficult to get people to change their transportation patterns thus a *professional, sustained promotional plan* must be put in plac to make the plan successful.

OBJECTIVE

The objective of this plan is to extensively publicize the Sea Dogs SHUTTLE and alternate parking sites within walking distance. At the same time the plan must sell the public on the advantages of using these alternatives. This plan must be professionally designed with significant exposure to make an impact on the public.

FOCUS OF PLAN IN ORDER OF PRIORITY

To convince people to use:

1. the Sea Dogs SHUTTLE.
2. the large alternative lots within walking distance that have been secured for use for baseball patrons.
3. make clear some of the restrictions of the plan.
4. make clear City of Portland parking regulations will be enforced throughout adjoining neighborhoods during ballgames.

WAYS TO PROMOTE THE PLAN

Develop advertisements specifically designed to sell the idea of choosing an alternative parking option through:

- * print media
- * television/radio
- * mailings (Sea Dogs)
- * tie-in with other regularly running ad
 - a. regular METRO ads and promotional material
 - b. regular Sea Dogs program, Stadium message center, radio spots during games, etc.

POINTS TO BE EMPHASIZED IN ADVERTISEMENTS

1. Convenience
2. Dependability (regularity of service)
3. Economy

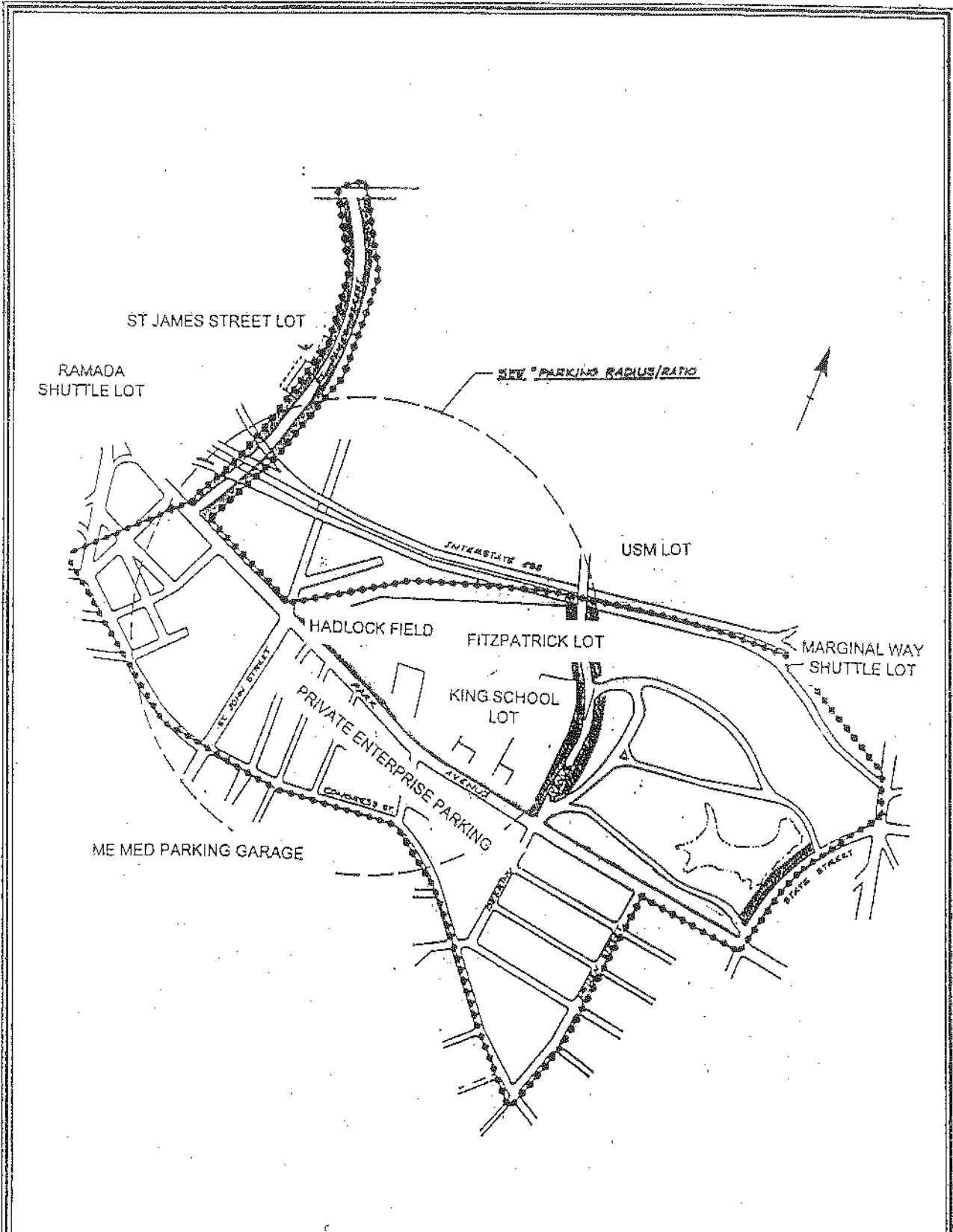
RESPONSIBLE FOR IMPLEMENTING THE CAMPAIGN

A professional public relations firm or advertising firm should be chosen to head up the project as many aspects need to be brought together quickly (radio and video production, printed material design, public service announcement distribution, advertising time purchases, if necessary, etc.). A local firm may be willing to do the work pro bono for community service or at a very reduced rate to gain exposure and to associate with the Sea Dogs.

SUMMARY

There is a need to mount an advertising campaign specifically geared to one purpose...telling people about alternative parking sites and a shuttle program that has been developed for Portland Sea Dogs' games.

It would be a short but intense ad campaign designed to emphasize the advantages to using an alternate site for parking. It would develop new parking habits for this area that will bode well for years to come.



PARKING ALTERNATIVES

FROM NORTH

Take I-95 or Washington Street (Route 26) to I-295 and get off at Exit 7 (Franklin Arterial), take a right turn at the end of the ramp, a right onto Marginal Way. The USM-Marginal Way SHUTTLE LOT is located 500 feet on the right.

FROM NORTHWEST

Take Forest Avenue (Route 302) to the University of Southern Maine Parking Lot on Bedford Street; OR take Forest Avenue to Prides Corner Bowling & Recreational Center, METRO Park 'n Ride.

P PARKING AREAS
S SHUTTLE BUS

FROM WEST

Take Brighton Avenue (Route 25) to Douglas Street, turn right and take first left onto El James Street for on-street parking; or continue east on Brighton and turn left onto Bedford Street to the USM Bedford Street Lot; or take Rt. 25 to the METRO Park 'n Ride at the American Legion Hall at the Westbrook/Gorham Town Line

FROM SOUTHWEST

Take County Road/Congress Street (Route 22) to the Ramada Inn - SHUTTLE LOT

FROM SOUTH

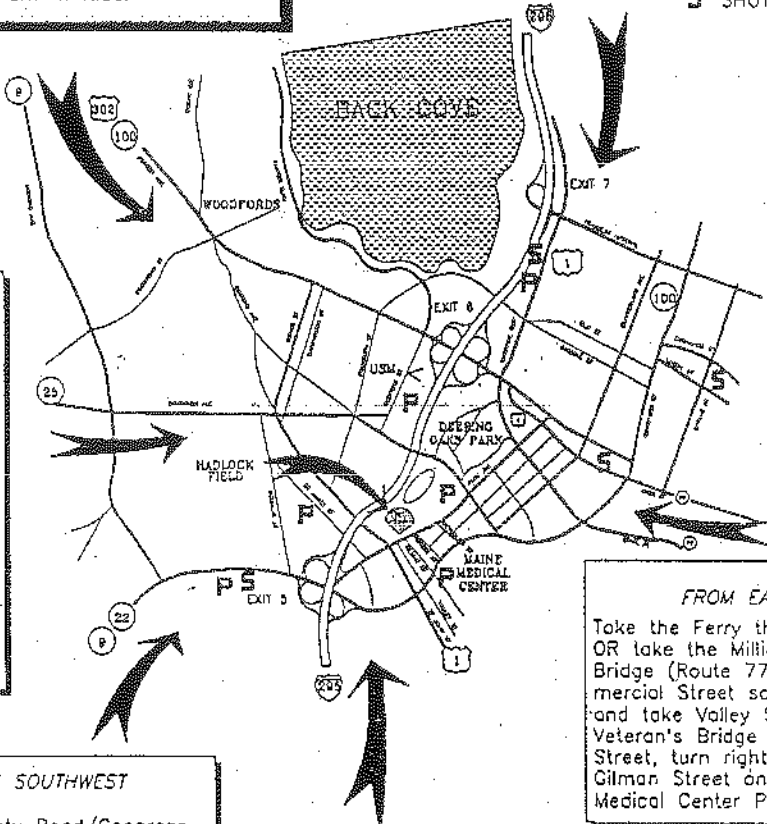
Take I-95 to I-295 to Congress Street (Exit 5 East), straight through two traffic signals, right on Gilman Street to the Maine Medical Center Parking Garage, immediately on the left.

FROM EAST

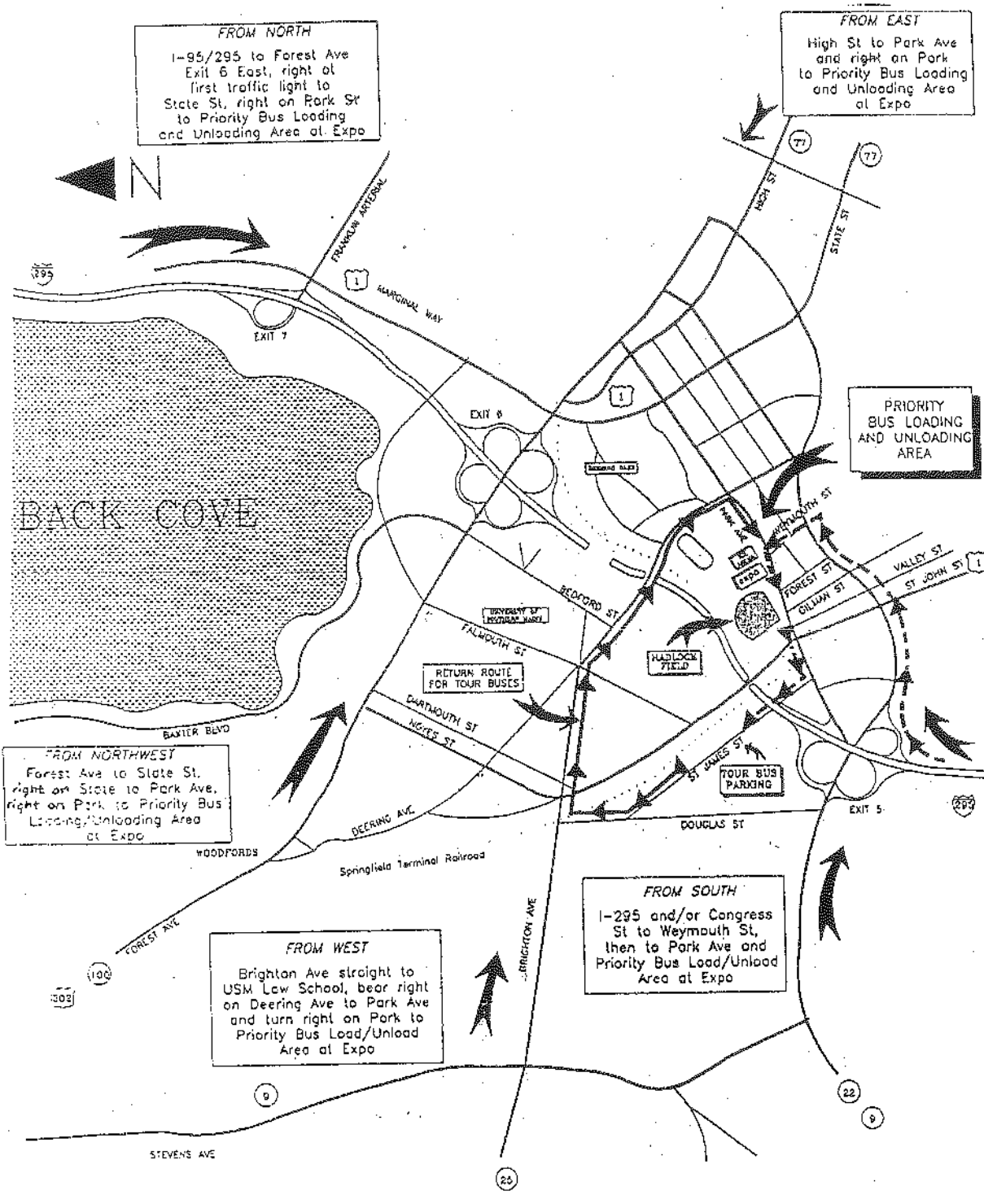
Take the Ferry then METRO! OR take the Millian Dollar Bridge (Route 77) to Commercial Street southbound, and take Valley Street at the Veteran's Bridge Rotary to "A" Street, turn right then left to Gilman Street and the Maine Medical Center Parking Garage

DISABLED PERSONS, BICYCLE AND CARPOOL PARKING

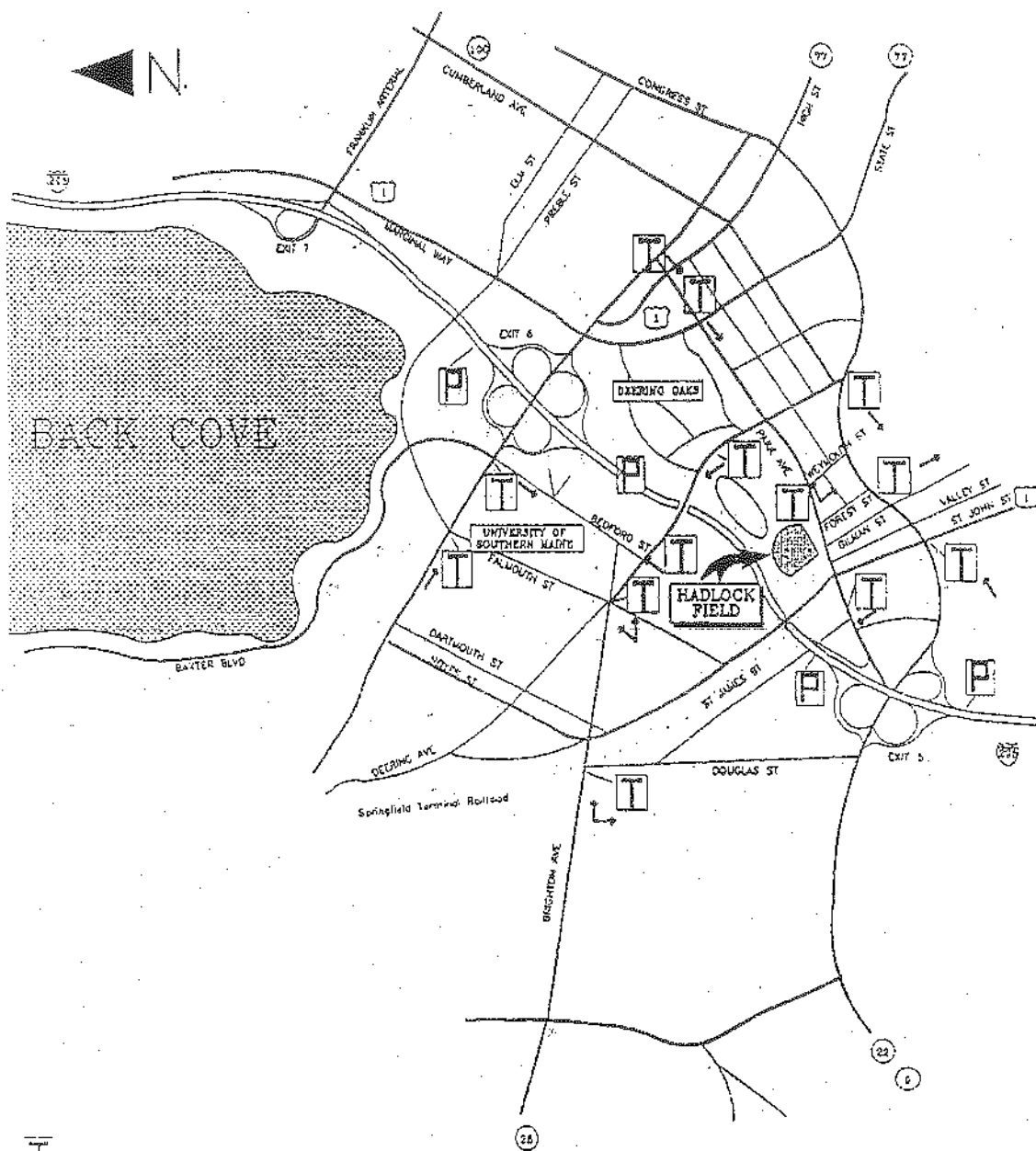
Parking for disabled persons, bicycles and Carpools with 4 or more persons in the vehicle is available in Lot "A" at Hadlock Field next to the Main Entrance on Park Avenue.





DIRECTIONS TO PARKING FOR HADLOCK FIELD

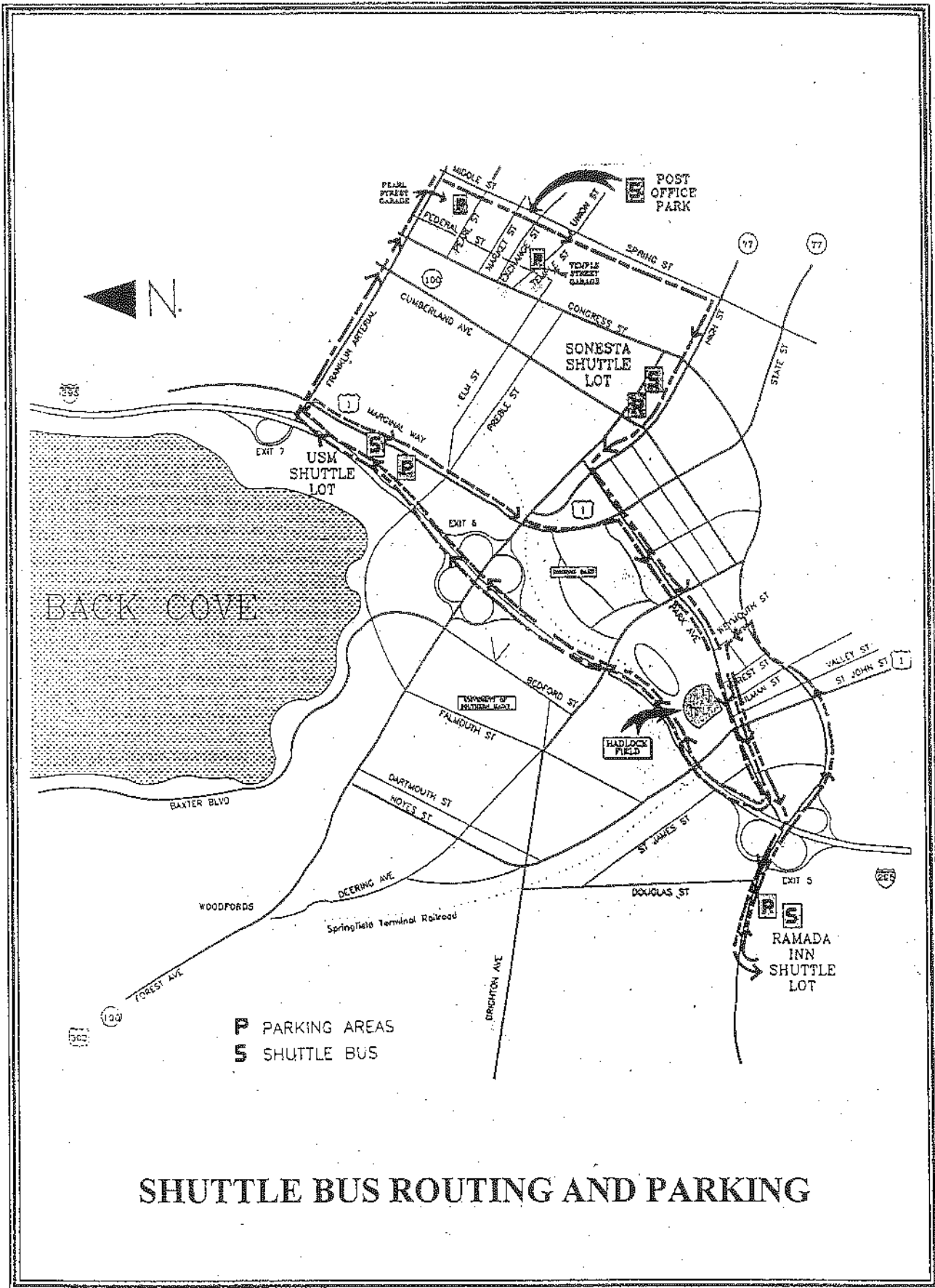


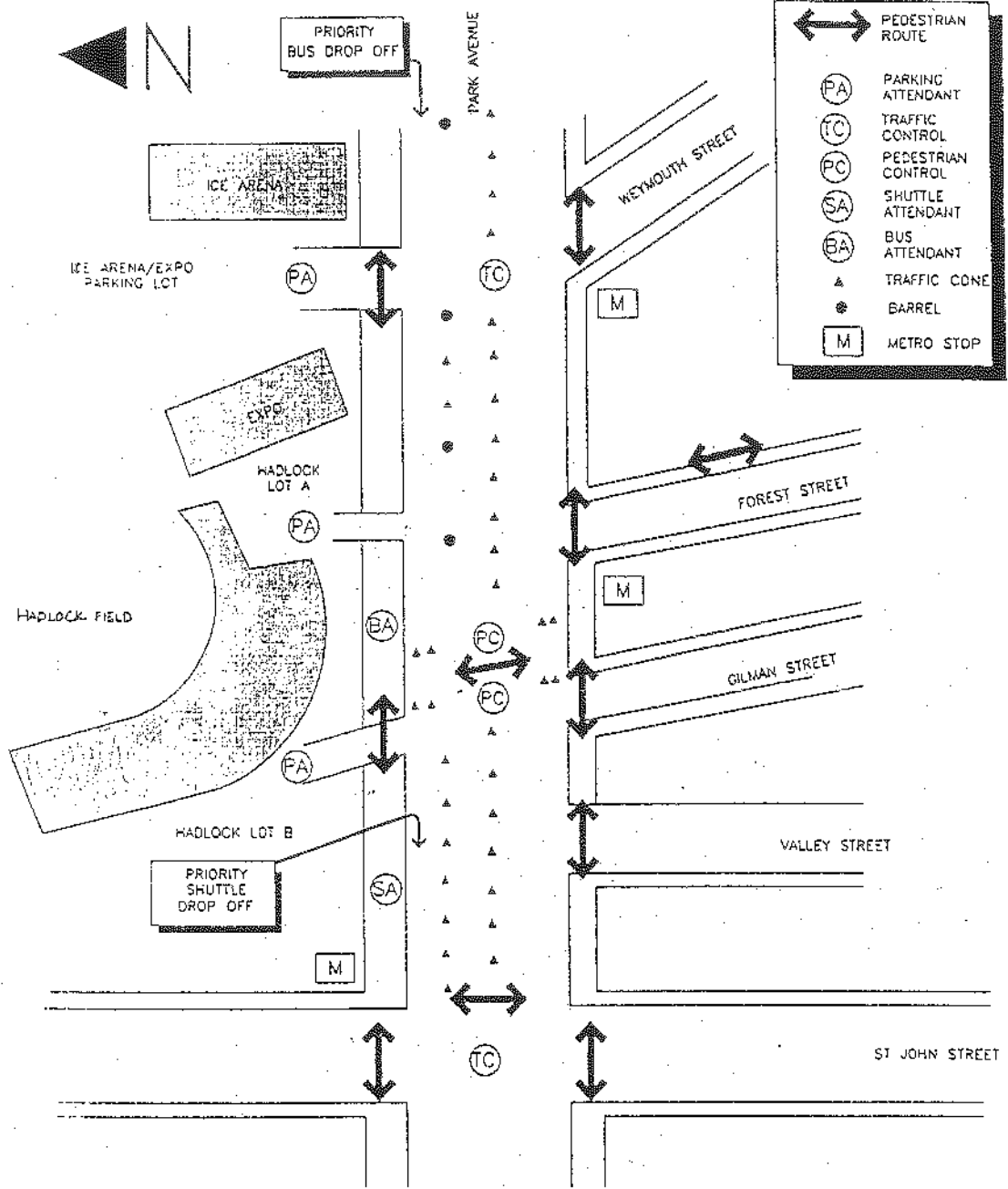
TOUR BUS ROUTING AND PARKING



 TEMPORARY/SEASONAL SIGN
 PERMANENT SIGN

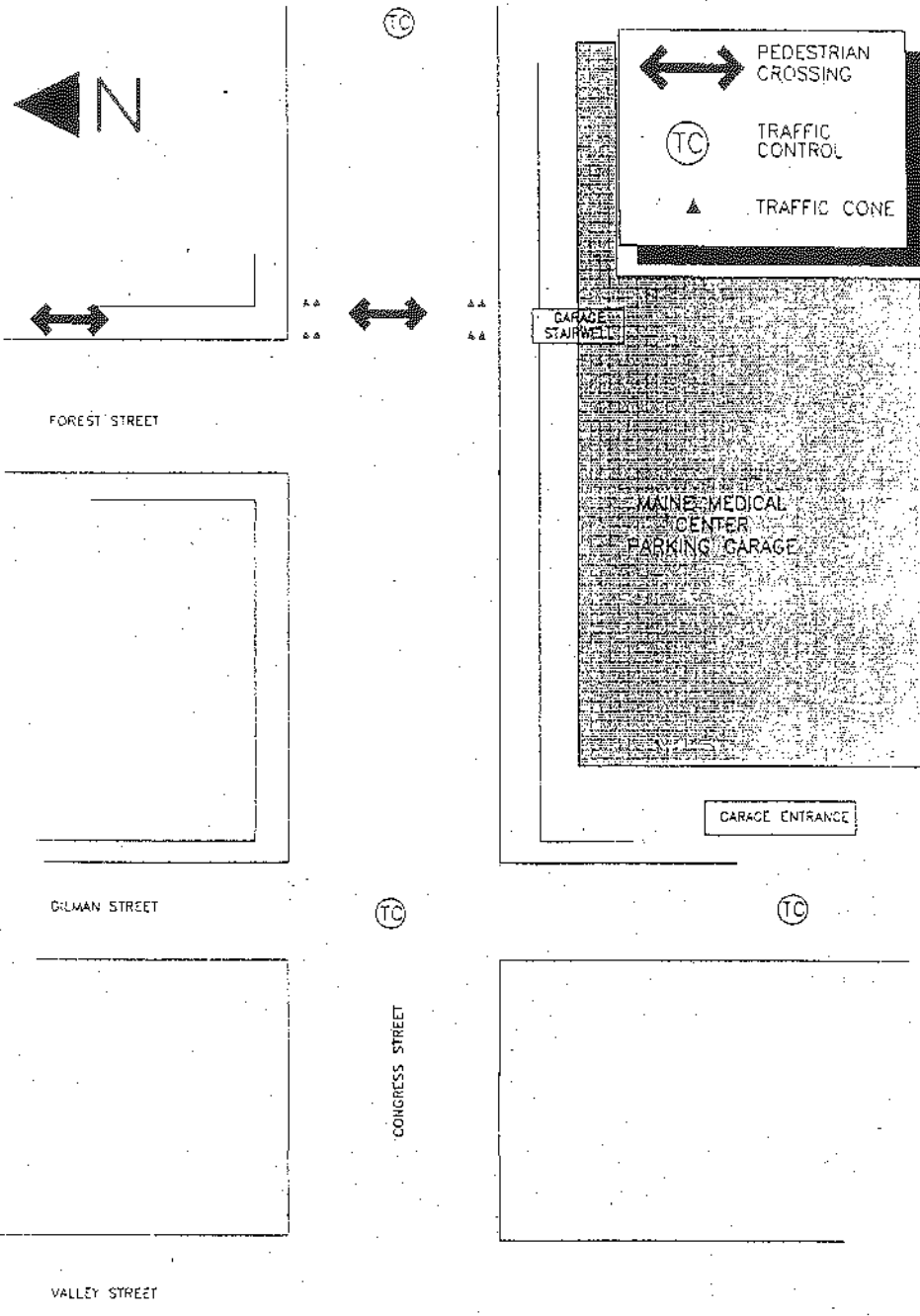
PRELIMINARY SIGNING PLAN





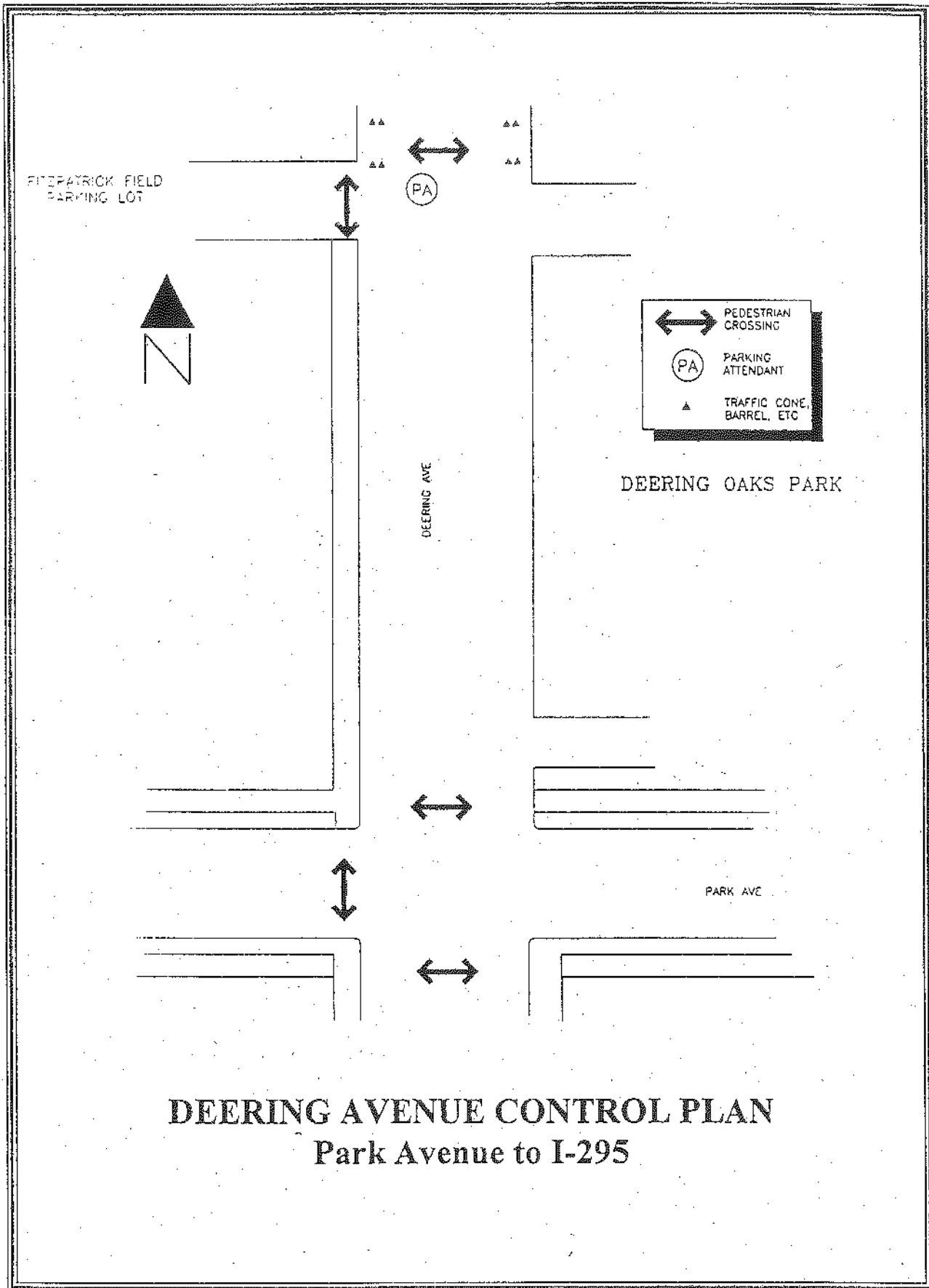
PARK AVENUE CONTROL PLAN
Weymouth Street to St. John Street

(detailed Control Plans are currently being developed)



CONGRESS STREET CONTROL PLAN
St. John Street to Forest Street

(detailed Control Plans are currently being developed)



C O N T E N T S

- RESPONSES TO GENERAL QUESTIONS
- SCHEDULING OF FIELD USE
- PARKING AND TRAFFIC
- EXPO SCHEDULE AND ANSWERS TO QUESTIONS
- PARKING DEPARTMENT MEMORANDUM
- ADDITIONAL FINANCIAL REPORTS
- LEGAL MEMORANDUM
- LOCKER ROOMS AND CLUBHOUSE

CITY OF PORTLAND, MAINE

MEMORANDUM

TO: Robert B. Ganley, City Manager

FROM: John P. Rague, Engineering Division *J.P.R.*

DATE: August 20, 1992

SUBJECT: Hadlock Field - Response to City Council Questions/Concerns

Pursuant to your request via George Flaherty, herewith I have addressed a number of concerns and/or questions by City Council members raised at the workshop of Monday, August 17, 1992.

If I can be of further assistance, please let me know.

JPR/sjf

1. Displacement of Deering High School from Hadlock Field to Harlow Field.

In the 1993 C.I.P. (see attached Exhibit "A"), \$150,000 has been allocated for improvements to Harlow Field. The anticipated work shall include (a) portable fencing, (b) finalization of the upgrading of the drainage which has been ongoing for two (2) years, (c) the upgrading of the playing field, and (d) dugouts.

It is further anticipated that the existing bleachers at Hadlock will be relocated to Harlow Field. With these additions and improvements, Harlow Field will be a class "A" facility.

2. Availability of Hadlock Field for Portland High School Utilization.

As stated by Mr. Burke at the workshop, he fully intends to allow Portland High School use of Hadlock Field.

The ball club will require the field no less than two (2) hours before each game.

As Charlie Eshbach mentioned, it is possible that high school/Double "A" double headers could be scheduled to help alleviate scheduling conflicts and to enhance attendance. Charlie further mentioned that scheduling through his office should eliminate scheduling conflicts.

Given that the high school baseball season ends around May 20 - 25 only about six weeks during the spring would be effected. Given that the pro club will be on the road about one-half of that time, in reality the impact period is only three weeks. Should Portland High School make the playoffs, this period could extend to four weeks.

3. Three non-Eastern League Events.

The concern was raised regarding the ball club's intent to have a maximum of three non-Eastern League Events at the stadium. Charlie Eshbach assured the council such "events" would all be baseball related, and such events do not include activities such as concerts.

4. Concurrent Activities with Exposition Building.

A number of Councilors raised this issue. I met with Frank LaTorre this past Tuesday to discuss same. Frank, under a separate letter within this report, has addressed these concerns. The traffic issues raised by concurrent uses has been addressed by Mary Theriault elsewhere in this report.

5. Ballpark Security.

Security during games is generally provided by the ball club through the engagement of local off-duty police officials. The cost thereof is borne by the club.

6. Bill Whiteds Proposed Bleachers Displacing the Proposed Picnic Area.

Based upon our fact-finding tour of other stadiums, we learned that the home team can greatly enhance income through the integration of "picnic functions" tying into the "family atmosphere" necessary to maintain adequate attendance. I have included such a picnic area down the left field line in my site plan. This is the area Bill Whited proposed additional bleachers. Through discussions with Bill, we decided to maintain the picnic area by adding additional box seats behind the dugout nearest the Expo and additional box seats near the left field bullpen. With these additions we believe we have satisfied seating capacity while saving the picnic area.

7. The "Alms House Sewer"

The Alms House sewer, a 4'-10" x 6'-3" brick oval combined sewer currently traverses the ballpark. The current bleachers straddle this sewer. Drainage from the existing parking lot is disposed of via this sewer.

The proposed grandstand will also straddle the Alms House Sewer. Proposed drainage, both under the grandstand and the expanded parking lot will utilize this sewer as a means of disposal. At the time of final design, care will be taken to line the brick pipe to insure its adequate protection. My site plan shows the location of the Alms House Sewer. It should not be necessary to move or otherwise disturb this pipe.

8. Cost to expand King Middle School Turnaround

Part of the parking study recommended the improvement of a turn around at King Middle School to facilitate tandem onsite parking. Such improvement would include approximately 300 linear feet of 18 foot wide pavement with gravel base. The estimated cost of such improvement is approximately \$60.00/linear foot, or \$18,000 total.

9. Litter Outside the Stadium Confines; Public Address System Noise; and Beer Sales.

In regards to each of the above issues, I telephoned the four (4) contact people at the stadiums visited by the two inspection teams in July resulting in the following:

1) Harrisburg Senators, Harrisburg, Pennsylvania

A) Litter - Early the next morning following each game, Parks and Recreation personnel police the area around the facility. The cost is borne by the City as part of the overall operational cost to maintain the Island Amusement Center of which the stadium is part. Very little litter is collected according to the club's general manager.

B) P.A. Noise - One entire bank of the river adjacent to the park is residential. The club claims not a single complaint has ever been registered.

C) Beer Sales - The sale of beer is closely monitored. Beer sales are necessary to the income flow of the facility. Sale of beer is stopped following the fifth inning.

2) Reading Phillies, Reading, Pennsylvania

A) Litter - Handled by City forces the morning following each game, with the cost borne by the City.

B) P.A. Noise - This stadium is situated within an industrial area. No residential neighborhoods abut the site.

C) Same as Harrisburg.

3) Albany Yankees, Albany, New York

A) Litter - Litter is policed by club personnel and the cost is borne by the club.

B) P.A. Noise - This stadium has a very "rural" setting with no residential abutters.

C) Same as Harrisburg and Reading.

4) Binghamton Mets, Binghamton, New York

A) Litter - Litter is policed by the club and the cost paid for by the club.

B) P.A. Noise - The stadium is situated within a mixed use area including some residential nearby. No complaints have ever been received.

C) Same as other three clubs.

10. Summary of Article #9.

Litter is effectively controlled through signage, availability of trash receptacles and "cute gimmicks." In Harrisburg, they employ "Mr. Tuxedo", a man dressed in a black tux who wanders through the crowds with trash bags collecting trash. He has become a fixture and fans look for him. It's "cute" and it works.

Reading has a similar guy dressed as the "Lone Ranger" whom they call "Rubbish Ranger." He virtually does the same job as "Mr. Tuxedo." All in all, the clubs reported very little litter outside the stadium. They contend the fans are fairly conscientious.

Regarding noise, of the four clubs, two were near residential areas. Neither reported any complaints of noise. I would suspect a "sound engineer" could design a system which would contain itself mostly within the park as Councilor Lord suggested.

As I mentioned earlier, beer sales are important to cash flow affording lower ticket prices. Ticket prices obviously effect attendance, which ultimately determines the success or failure of the team.

In all cases, however, and as Charlie Eshbach stressed, the success of each club is closely tied into providing good, wholesome, family entertainment. Controlling beer sales, therefore, is the key.

Shoreway Access Plan I	50,000
Implementation of Shoreway Access Plan including property and easement acquisition, walkway construction, slope stabilization, signage, lighting , and other improvements. A multi-phase project.	
Deering High School Fields- Phase I	150,000
Harlow field rehabilitation reconstruction of baseball field, including undrains and an irrigation system, reconstruction of infield, resod infield, loam and seed outfield, new bleachers. A multi-phase project.	
FY 93 parks total:	250,000
GOLF	
no allocation	0
EQUIPMENT	
Bucket Truck	70,000
Fireboat rehabilitation	200,000
Landing Craft Mechanized to replace a vessel built in 1958 which the City purchased in 1979.	120,000
pumper	250,000
FY 93 equipment total:	640,000
TOTAL FISCAL YEAR 1993 CIP	9,456,690

CITY OF PORTLAND, MAINE

MEMORANDUM

TO: Robert Ganley, City Manager

FROM: Larry Mead, Superintendent of Recreation

DATE: August 20, 1992

SUBJECT: Scheduling issues related to Double A Baseball

I was asked at the August 17th Council workshop to prepare information regarding the effect of a Double A minor league team in Portland on the schedules of existing school and recreational users. This information is provided herein as requested. I have also provided information related to several other questions raised at the workshop.

LM/sf

SUMMARY

The locating of a Double A baseball franchise in Portland will require the adjustment of existing facility use schedules for both school and non-school teams. The single most significant element in successfully implementing changes will be the proposed renovations to Harlow Field behind Deering High School. The renovations to Harlow will ensure that a quality game facility exists that will be available to the Deering High Varsity, as well to other school and non-school teams. Other game and practice needs can be accommodated within the system. Portland High School will still be able to play home games at Hadlock field and will have access for practices when the minor league team is on the road. The City will work diligently with the Schools and the minor league team to insure that the interests of both organizations are served, and specifically that Portland High School game dates are accommodated at Hadlock.

SCHEDULING ISSUES: DOUBLE A BASEBALL

Flexible Approach to Scheduling

The principals involved in the Double A team have expressed their intention to work with the City and the School Department to accommodate other uses at the Hadlock facility. Both Daniel Burke, prospective owner, and Charles Eshbach, Eastern League President, have pledged a flexible

approach to scheduling in order to allow other critical uses of the facility, such as Portland High School baseball and occasional tournaments. Examples of this flexible approach could include moving batting practice indoors to the Expo in order to allow a high school game to be played, scheduling a double-header with the High School and sharing a % of proceeds with the school, allowing access to the field for high school practice when the minor league team is not at home, and meeting with appropriate people from the City and School each pre-season to try to avoid scheduling conflicts.

Scheduling Calendar and Current Users of Hadlock Field

The Eastern League Schedule extends from early April until early September, with the potential for playoff games until mid-September (70 scheduled home dates). Generally the teams alternate 6-7 dates at home with 6-7 dates away. Once per season each team enjoys an extended homestand that runs for 9-11 consecutive dates. Rainouts are almost always rescheduled as double-headers, because there are very few off dates (5 or 6).

The high school season begins in mid-April (practices in early April) and ends by the first week of June, including playoffs (8 scheduled home games, in 1992 PHS played 12 game dates at Hadlock). Deering played 12 home games.

Legion baseball begins immediately after high school season ends and concludes in the end of July. Andrews Post has used Hadlock for home games (10 dates, 11 games in 1992).

The Twilight League season begins the last week of May and concludes in mid-August. Two teams, Forest Gardens and South Portland, played home dates at Hadlock. (Forest Gardens: 12 dates, 17 games. South Portland: 10 dates, 13 games).

PROJECTED EFFECT ON OTHER TEAMS FIELD USE

Currently Hadlock Field is used for home games by both Deering and Portland varsity teams. Renovations to Harlow Field will enable DHS to return to this traditional site for home games. Portland High School will experience the most significant change in that it will no longer be able to use Hadlock as its primary practice location.

Other lower level school teams will experience some change in practice location. How much will depend on the practice schedules of individual coaches. However there are fields available for all teams.

Summer recreational users such as Senior League and Babe Ruth do not use Hadlock for regular season games. However, these leagues would experience the relocation of some games to the Payson Park facility from Deering Oaks or Harlow Field.

Portland High School

Varsity: Games: Most games will be played at Hadlock. Efforts will be made to coordinate schedules so that PHS home dates coincide with AA team away dates. Rainouts or occasional schedule conflicts could result in some games being located at Harlow or Deering Oaks. Practices: To be held at both Hadlock and the Oaks. During Double AA homestands practices would be at the Oaks. Practices would be allowed at Hadlock when minor league team is away.

Junior Varsity: No change

Freshmen: Games: No change. Practices: Relocated from Deering Oaks to either Dougherty Field or Payson Park.

Deering High School

Varsity: Games: Relocated to Harlow Field. Practices: No change.

Junior Varsity Games: Relocated from Harlow to Dougherty.
Practices: No change. Freshman: No change.

King Middle School

Games: No change. Practices: Relocated to either Payson or Dougherty.

Lincoln Middle School

Games: No change. Practices: Currently at Dougherty. In future would be located either at Dougherty or Payson.

American Legion

Games: Some games may be allowed at Hadlock but most would be relocated to Harlow or Deering Oaks. Practices: No change.

Twilight League

Games: Relocated to Harlow, Deering Oaks or other community facilities. Practices: No change.

IMPROVEMENT PROPOSED AT OTHER FACILITIES

Harlow Field: \$150,000.00

The major improvements needed to facilitate dislocated users at other

locations would be at Harlow Field. The Harlow Field improvements have been moving up the CIP list for several years, and have already been identified as a priority for FY 93. The projected cost of \$150,000 included improvements to a portion of the all-purpose playing field behind Deering High. Further information regarding Harlow Field improvements is provided provided by John Rague of Parks & Public Works Engineering.

Deering Oaks: \$5,000

The field is in excellent overall condition. Needed improvements include a new backstop, other minor fencing repairs, and new benches in the dugouts. The projected cost assumes purchase of a completely new backstop. It may be possible to repair the existing backstop with new fabric at a lower cost. A new scoreboard could be solicited through private contributions if desired.

Riverton School: \$4,500 (OPTIONAL)

The ballfield behind Riverton School is used only sporadically by Little League North. It could be reconfigured to a full sized baseball diamond, in order to be available for other youth league levels, including Senior Little League, Prep League and Babe Ruth League. This option is not anecessity, but it would provide a greater degree of flexibility in scheduling, and reduce the likelihood of scheduling conflicts. The adjacent playground has already been funded for replacement this year. It could be moved closer to the tennis courts, providing the room needed to expand the field. Little League use could still be accommodated, if necessary, at the Riverside Street youth fields. The projected cost includes additional

fencing, and a new infield.

ANSWERS TO OTHER QUESTIONS FROM THE AUGUST 17TH WORKSHOP

This information was provided by Eastern League President, Charles Eshbach.

1. What are the greater metropolitan populations of the other Eastern league team locations?

New Britain, CT	1,130,000
Albany, NY	860,000
Canton, OH	642,000
Harrisburg, PA	595,000
Reading, PA	333,000
New London, CT	305,000
Binghamton, NY	210,000
Hagerstown, MD	118,000

2. How does the proposed 10 year operating agreement compare to other Eastern League agreements?

Two other teams also have 10 year agreements. The others are for fewer years. A five year agreement is typical.

3. Is it typical in the Eastern League for the compensation to the municipality from the team to be fixed fee, or a percentage of sales?

The majority of agreements are fixed, although there are some that combine fixed with a percentage. In general the total amount of compensation does not vary tremendously from city to city.

LM/sf

8/20/92

CITY OF PORTLAND, MAINE

MEMORANDUM

TO: Robert B. Ganley, City Manager

FROM: Mary A. Theriault, Traffic Project Engineer *Man*

DATE: August 20, 1992

SUBJECT: August 17th Council Workshop

Enclosed please find the response to all Traffic related concerns discussed in the August 17th City Council Workshop on the Hadlock Field Project.

MAT/sjf

cc: Councilor Thomas Harlow

Councilor Peter O'Donnell

Councilor Anne Pringle

Councilor Cheryl Leeman

Councilor Kerry Lord

Councilor John McDonough

Councilor Theodore Rand

Councilor Richard Paulson

George A. Flaherty, Director of Parks/Public Works

William J. Bray, Deputy Director of Parks/Public Works

RESPONSE TO ALL TRAFFIC-RELATED CONCERNS

1. "UNCONTROLLED" PARKING

To define "uncontrolled parking", this simply implies that at a lot such as, St. James Street Parking Area, an attendant is unnecessary. Traffic signage and parking lot striping would be sufficient to direct buses and other large vehicles in that lot. Traffic signage and parking enforcement would be recommended for on-street and/or Deering Oaks parking, if those alternatives become part of a final "Traffic Management Plan."

2. ON-STREET PARKING AVAILABLE

A "Traffic Study" as opposed to a "Parking Study" would include a complete occupancy report which involves actual counting of the cars at several different locations and time frames. Deering, State and St. James Streets will probably have a very low "occupancy", while Park Avenue may have some residential parking demand. Once again, we are looking at several alternatives as "possibilities", a final parking plan will involve a full Traffic Study Analysis.

3. PRIVATE LOTS ON PARK AVENUE

The private, gravel parking lot on the corner of Weymouth and Park Avenue is owned by Richard and Penny Harris. They were unavailable for comment at this time.

The private, paved lot with a building located between Gilman and Valley Streets is owned by J. Walch Publishing Company. After speaking with Suzanne Austin, President, she stated the lot would not be "For Sale"; however, other parking alternatives such as, a lease or private enterprise is a viable possibility (40 SPACES AVAILABLE).

4. MUNICIPAL PARKING LOTS - SHUTTLE SERVICE

The new Bayside Parking Lot, located on the corner of Marginal Way and Preble Street, provides another alternative to overflow parking during the peak baseball season. A shuttle bus system would have to be coordinated; however, the location and spaces are a reasonable alternative. (369 SPACES AVAILABLE FROM MAY 15TH TO SEPTEMBER 1ST).

5. FURTHER RESEARCH OF PRIVATE ENTERPRISE

After speaking with Dave Early, Director of Facilities Management at USM, he has stated and documented, (letter enclosed) that USM would be willing to allow free overflow parking at the Bedford Street Parking Lot following the period of commencement. These areas have been used in prior years for several different events at both the Deering Oaks and the ballfields. (500 SPACES AVAILABLE).

After speaking with Dick Milheron, Parking Supervisor of the Maine Medical Center Parking Garage, located on Gilman Street, he stated overflow parking in their garage was used on football games in the past. The use of the garage for overflow parking during ballgames is indeed feasible, whereas, their demand for spaces is minimized after 5:00 p.m. (APPROX. 638 SPACES AVAILABLE).

After speaking with Robert Crouse, Executive Director of the Maine Center for the Blind, 189 Park Avenue, he also has provided parking for special events in the Deering Oaks and Ballfields. They are strongly in favor of coordinating parking supply during all the scheduled ballgames. (35 SPACES AVAILABLE).

After speaking with Aleta Kilborn, Office Manager of the Maine Center for Cancer Medicine, 180 Park Avenue, she also has provided parking for special events in the past. They have coordinated efforts with Mickey Boutilier of the Special Olympic-Maine Organization. They are very interested in providing parking during ballgames for non-profit donations either through the Special Olympics or the Cancer Society. (50 SPACES AVAILABLE).

After speaking with Don Haggett, Vice President of Operations for Ramada Inn, 1230 Congress Street, he is in favor of coordinating parking efforts with the ballclub. However, their franchise agreement would work more favorably with a promotional swap than an actual "fee for parking" method. (500+ SPACES AVAILABLE).

After speaking with Don Boynton, President of Susse Chalet, they are also interested in coordinating efforts with the ballclub. Like the Ramada, a promotion swap would interest them the most; however other parking alternatives are also possible. (30 SPACES AVAILABLE).

After speaking with Kenneth Acker, President of Medical Services Federal Credit Union, 272 Park Avenue, he is very interested in organizing parking during ballgames. he and his staff have already discussed an interest in doing a project for non-profit donations and this concept fits perfectly. (20 SPACES AVAILABLE).

6. SPECIAL OLYMPIC/MAINE NON-PROFIT ORGANIZATION

After speaking with Mickey Boutilier, President, he has voiced and documented, (letter enclosed), an interest in coordinating efforts in managing the parking attendant/fee process for all ballgames. Their organization has been providing parking attendants for the past eight years during the Deering Oaks Family Festival.

7. "TANDUM PARKING"

The question was raised "Is Tandum Parking needed?". The concept was given as an "alternative" which is currently being used successfully at other special events.

It is my intent to study thoroughly ALL ALTERNATIVES and work with the neighborhoods, council, planning and other City departments to prepare a parking plan that works for everyone. Several alternatives may need to be tried for a "true and practical" solution. We have provided and exceeded many times over the spaces needed for the Hadlock Field Baseball Project.

Also, the AVERAGE ANNUAL "SELLOUTS" is calculated to be 6 ballgames per year. Statistics were received from the four ballclubs visited by City employees.

MAT/sjf

8/20/92



Maine

28 School Street
Gorham, Maine 04038
207-839-6030
1-800-639-2401

Mickey Boutilier
Executive Director

Phil Geelhoed
Program Director

Mark Frank
Training and Area
Development Director

Linda Fredericks
Administrative Assistant

Mary Philbrick
Comptroller

August 20, 1992

Mary Theriault, Traffic Project Engineer
City of Portland Traffic Division
Portland City Hall
Portland, ME 04101

Dear Mary:

Thank you very much for your kind offer to consider Maine Special Olympics as a possible non-profit candidate to operate the public parking concessions at Hadlock Field, should the City of Portland secure a professional baseball franchise.

As you know, operating a parking concession will be nothing new for the hundreds of local and area Special Olympic volunteers. We have operated the parking program at the Deering Oaks Family Festival since it's conception.

Special Olympics also worked the public parking for Union Mutual's Senior Golf Classic, and many other smaller public parking events. All of these projects involved not only the parking of thousands of cars, but also required our volunteers to act as public relations ambassadors for the sponsoring organization.

We are looking forward to having the opportunity to meet with you and your committee to discuss this possibility in greater detail. I also plan to attend the Portland City Council meeting on Monday evening, August 24th.

Once again, thank you for considering Maine Special Olympics and the twenty seven hundred athletes involved!

Sincerely,

Mickey Boutilier
Executive Director

MB/lf



UNIVERSITY OF SOUTHERN MAINE

Department of Facilities Management
Operations—Portland (207) 780-4160
FAX (207) 780-4538
Operations—Gorham (207) 780-5226
FAX (207) 780-5520

96 Falmouth Street
Portland, Maine 04103
Director (207) 780-4656
Engineering Services (207) 780-4660

August 19, 1992

Mr. Robert Ganley
City Manager's Office
389 Congress Street
Portland, Maine 04101

Dear Mr. Ganley,

This letter is written in confirmation of the University of Southern Maine's interest in continuing to collaborate with the City of Portland in addressing our many mutual concerns in order to best serve the citizens of Cumberland County and the State of Maine.

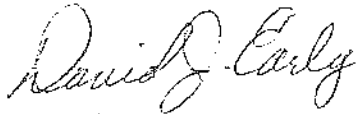
We have learned that our 500 space parking lot between Surrenden, Bedford, and Winslow Streets, commonly referred to as the Bedford Street Parking lot, is of interest in satisfying a potential parking demand generated by Professional Baseball being held at Hadlock Field. USM is indeed willing to work with the city to allow usage of this lot for overflow parking during the summer months when school is out of session. This would typically be the middle of May through the end of August. This is the same lot that has been successfully used to satisfy parking demand for the Deering Oaks Family Festival. Final arrangements will need to be worked out similar to those used for the festival.



Mr. Robert Ganley
Page 2

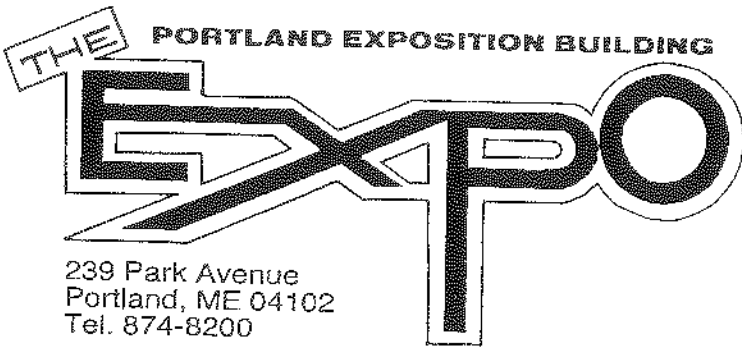
As you know, Hadlock Field has been the site of the NCAA Div. III, New England Regional Tournament for the last three years, during the month of May. The winner of this tournament advances to the College World Series which USM won in 1991, and returned with the States First NCAA National Championship. With the upgrades to Hadlock field currently under consideration, it is possible that Hadlock field could eventually host the College World Series itself, bringing yet even more business and recognition to our city.

Sincerely,

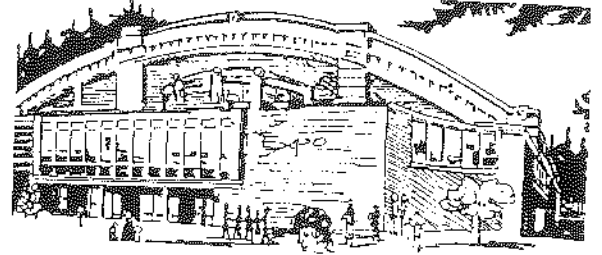
A handwritten signature in cursive script that reads "David J. Early".

David J. Early
Director of Facilities Management

cc: President Pattenaude
Vice-President Andrews



239 Park Avenue
Portland, ME 04102
Tel. 874-8200



August 20, 1992

Portland Exposition Building and Parking at Hadlock Field

Frank LaTorre, C.F.E., Executive Director of the Portland
Exposition Building

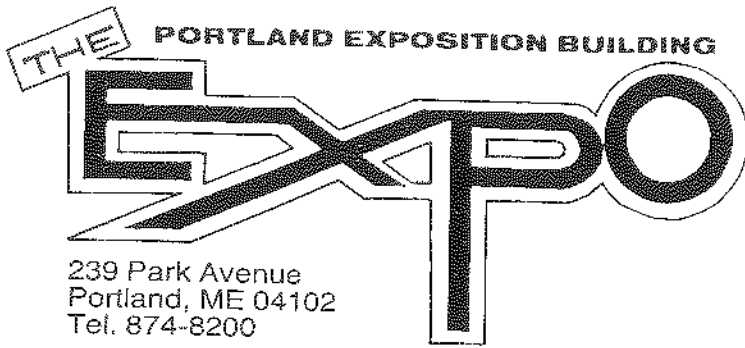
While it is a fact that certain events at the Portland Exposition Building may run concurrently with an Eastern League baseball game at Hadlock Field, the impact of said parking can be absorbed by a cohesive traffic flow/parking plan. The implementation of such a cohesive plan by the City of Portland will assure no adverse financial impact on the new Eastern League team or the Expo due to inconvenience for patrons attending the events. Elements of that plan, as relate to the Expo, presently include:

- * the assurance, we have already secured, from the President of the Eastern League Charles Eshbach that they will do "everything possible to schedule around the major events we now have booked." Though the Expo does have a number of event days booked between April 1 and September 15, the fact is that we have a **limited number** of events that must be avoided in terms of concurrency. Other events have limited attendees or are exclusively in the afternoon. With Mr. Eshbach's firm commitment to "make things work" the avoidance of major conflicts is a very achievable goal.
- * the assurance that the Expo will do everything it can to schedule any future bookings around the Eastern League's home schedule. As the years go by and the patterns of home baseball bookings become clearer, this will become easier and easier.

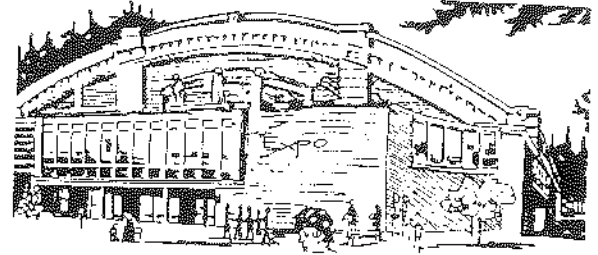


* The fact is while traffic/parking plans developed by the City of Portland are presently working with the figure of 6,000 attendance at each game, reality dictates average figures will be substantially less. Thus if promotional days geared to turning out larger crowds at the Ballpark avoid Expo bookings, there should be no problems. The parking plan to accommodate 6,000 would be more than adequate to absorb the vast majority of concurrent bookings that would occur at the Expo. This neighborhood has shown a resilient ability to handle events of over 3,000 patrons at the Expo. The extra parking spaces added to the mix as shown in the City's Parking Plan provide essential new space that will be needed.

The key to working out potential problems is a commitment to mutual understanding and cooperation. With the assurances given by the President of the Eastern League and the highly cooperative and sensitive attitude the Administration of City of Portland has displayed to the needs of the Portland Exposition Building, I believe we can reach our goal of accommodating events at the Expo and Hadlock Field while minimizing any impacts on surrounding neighborhoods.



239 Park Avenue
Portland, ME 04102
Tel. 874-8200



TO: George Flaherty, Director of Public Works

FROM: Frank LaTorre, Executive Director of the Portland Exposition Building ^{F17}

SUBJECT: Attached Schedule of Expo Events

DATE: August 20, 1992

Here is the schedule of events booked at the Expo for April 1 - September 15, 1993 and April 1 - September 15, 1994. We have listed the specific dates, description of what occurs each day, hours of the day, estimated attendees per day and contract status.

It is important to note that there are only a **limited number** of events where avoidance of concurrent bookings with Hadlock Field is a top priority. As I mention in my narrative dated August 20, 1992 on this issue, the vast majority of dates could be absorbed with the traffic/parking plan laid out by the City of Portland.



ANALYSIS OF SCHEDULED EVENTS AT THE PORTLAND EXPOSITION BUILDING
DURING THE EASTERN LEAGUE BASEBALL SEASON

SUBJECT TO CHANGE

AS OF AUGUST 18, 1992

DAY 1993 (DAY 1994)	EVENT	TENTATIVE EVENT HOURS	ESTIMATED CONTRACT ATTENDEES STATUS PER DAY
SAT MAR 27 * (SAT APR 02)	SMAA MIDDLE SCHOOL INDOOR TRACK EVENT	7:00AM - 7:00PM	4000 CONFIRMED
TUS MAR 30 (TUS APR 05)	PORTLAND HOME AND BETTER LIVING SHOW LOAD IN	6:00AM - 8:00PM	CONFIRMED EXHIBITOR VEHICLES 135
WED MAR 31 (WED APR 06)	PORTLAND HOME AND BETTER LIVING SHOW LOAD IN	6:00AM - 8:00PM	CONFIRMED EXHIBITOR VEHICLES 135
THR APR 01 * (THR APR 07)	PORTLAND HOME AND BETTER LIVING SHOW LOAD IN EVENT	6:00AM - 4:00PM 5:00PM - 10:00PM	CONFIRMED EXHIBITOR VEHICLES 135 1800
FRI APR 02 * (FRI APR 08)	PORTLAND HOME AND BETTER LIVING SHOW EVENT	5:00PM - 10:00PM	CONFIRMED 1800
SAT APR 03 * (SAT APR 09)	PORTLAND HOME AND BETTER LIVING SHOW EVENT	11:00AM - 10:00PM	CONFIRMED 6000
SUN APR 04 * (SUN APR 10)	PORTLAND HOME AND BETTER LIVING SHOW EVENT LOAD OUT	11:00AM - 6:00PM 6:00PM - 11:00PM	CONFIRMED 4000 EXHIBITOR VEHICLES 135
TU APR 05 (MON APR 11)	PORTLAND HOME AND BETTER LIVING SHOW LOAD OUT	6:00AM - 11:00AM	CONFIRMED EXHIBITOR VEHICLES 135
TUS APR 06 * (TUS APR 12)	ADVERTISING AND GRAPHIC ARTS TRADE SHOW LOAD IN EVENT LOAD OUT	7:00AM - 3:00PM 3:00PM - 9:00PM 9:00AM - 11:00PM	CONFIRMED EXHIBITOR VEHICLES 75 1000 EXHIBITOR VEHICLES 75

THR APR 08 (THR APR 14)	MAINE EXPO CAMPGROUND AND RV SHOW LOAD IN	7:00AM - 7:00PM	EXHIBITOR VEHICLES 100	CONFIRMED
FRI APR 09 * (FRI APR 15)	MAINE EXPO CAMPGROUND AND RV SHOW LOAD IN EVENT	7:00AM - 2:00PM 2:00PM - 9:30PM	EXHIBITOR VEHICLES 100 1500	CONFIRMED
SAT APR 10 * (SAT APR 16)	MAINE EXPO CAMPGROUND AND RV SHOW EVENT	11:00AM - 9:30PM	2500	CONFIRMED
SUN APR 11 * (SUN APR 17)	MAINE EXPO CAMPGROUND AND RV SHOW EVENT LOAD OUT	11:00AM - 5:00PM 5:00PM - 11:00PM	1500 EXHIBITOR VEHICLES 100	CONFIRMED
MON APR 12 MON APR 18)	PRIME OF YOUR LIFE EXPO LOAD IN	7:00AM - 7:00PM		TENTATIVE EXHIBITOR VEHICLES 75
TUS APR 13 MON APR 19)	PRIME OF YOUR LIFE EXPO LOAD IN EVENT LOAD OUT	7:00AM - 9:00AM 9:00AM - 5:00PM 5:00PM - 11:00PM	EXHIBITOR VEHICLES 75 500 EXHIBITOR VEHICLES 75	TENTATIVE
FRI APR 16 FRI APR 22)	MAINE'S SPRING ANTIQUES EXPOSITION LOAD IN	7:00AM - 9:00PM	EXHIBITOR VEHICLES 100	CONFIRMED
SAT APR 17 (SAT APR 23)	MAINE'S SPRING ANTIQUES EXPOSITION LOAD IN EVENT	7:00AM - 10:00AM 10:00AM - 5:00PM	EXHIBITOR VEHICLES 100 1000	CONFIRMED
SUN APR 18 (SUN APR 24)	MAINE'S SPRING ANTIQUES EXPOSITION EVENT LOAD OUT	11:00AM - 4:00PM 4:00AM - 11:00PM	1000 EXHIBITOR VEHICLES 100	CONFIRMED
FRI APR 23 FRI APR 29)	PORTLAND HOME SEEKERS WEEKEND LOAD IN EVENT	7:00AM - 6:00PM 6:00PM - 9:00PM	EXHIBITOR VEHICLES 100 500	CONFIRMED
SAT APR 24 (SAT APR 30)	PORTLAND HOME SEEKERS WEEKEND EVENT	10:00AM - 9:00PM	1000	CONFIRMED

SUN APR 25 (SUN MAY 01)	PORTLAND HOME SEEKERS WEEKEND EVENT LOAD OUT	10:00AM - 5:00PM 5:00PM - 11:00PM	500 EXHIBITOR VEHICLES 100	CONFIRMED
TUS APR 27 (TUS MAR 29)	PRVTC 8TH GRADE CAREER FAIR LOAD IN	7:00AM - 7:00PM	EXHIBITOR VEHICLES 50	TENTATIVE
ED APR 28 (WED MAR 30)	PRVTC 8TH GRADE CAREER FAIR LOAD IN EVENT LOAD OUT	7:00AM - 8:00AM 8:00AM - 3:00PM 3:00PM - 7:00PM	EXHIBITOR VEHICLES 50 3000 (BUS) EXHIBITOR VEHICLES 50	TENTATIVE
HR APR 29 (THR MAY 05)	LEADER ELECTRONICS BLOW OUT SALE LOAD IN	7:00AM - 7:00PM	EXHIBITOR VEHICLES 20	TENTATIVE
FRI APR 30 (FRI MAY 06)	LEADER ELECTRONICS BLOW OUT SALE EVENT	TBA	1000	TENTATIVE
SAT MAY 01 (SAT MAY 07)	LEADER ELECTRONICS BLOW OUT SALE EVENT	TBA	2000	TENTATIVE
IN MAY 02 (SUN MAY 08)	LEADER ELECTRONICS BLOW OUT SALE EVENT	TBA	2000	TENTATIVE
ON MAY 03 (MON MAY 09)	LEADER ELECTRONICS BLOW OUT SALE LOAD OUT	7:00AM - 7:00PM	EXHIBITOR VEHICLES 20	TENTATIVE
RI MAY 14 (RI MAY 13)	PET EXPO LOAD IN EVENT	7:00AM - 7:00PM TBA	EXHIBITOR VEHICLES 50 500	TENTATIVE
T MAY 15 (SAT MAY 14)	PET EXPO EVENT	TBA	1000	TENTATIVE
N MAY 16 (SAT MAY 15)	PET EXPO EVENT LOAD OUT	TBA 7:00AM - 7:00PM	500 EXHIBITOR VEHICLES 50	TENTATIVE

FRI MAY 21 (FRI MAY 20)	NEW AGE EXPO LOAD IN	7:00AM - 7:00PM	EXHIBITOR VEHICLES	TENTATIVE 50
	EVENT	TBA		500
SAT MAY 22 (SAT MAY 21)	NEW AGE EXPO EVENT	TBA		TENTATIVE 1000
SUN MAY 23 (SUN MAY 22)	NEW AGE EXPO EVENT LOAD OUT	TBA 7:00AM - 7:00PM	EXHIBITOR VEHICLES	TENTATIVE 500 50
FRI MAY 28 (FRI MAY 27)	PORTLAND PUBLIC SCHOOLS YARD SALE LOAD IN	7:00AM - 7:00PM	EXHIBITOR VEHICLES	TENTATIVE 20
SAT MAY 29 (SAT MAY 28)	PORTLAND PUBLIC SCHOOLS YARD SALE EVENT	9:00AM - 4:00PM		TENTATIVE 500
SUN MAY 30 (SUN MAY 29)	PORTLAND PUBLIC SCHOOLS YARD SALE EVENT LOAD OUT	9:00AM - 4:00PM 7:00AM - 7:00PM	EXHIBITOR VEHICLES	TENTATIVE 500 20
THR JUN 03 * (THR JUN 02)	PORTLAND ADULT COMMUNITY EDUCATION GRADUATION EVENT	6:00AM - 9:00PM		CONFIRMED 2000
SAT JUN 05 * (SAT JUN 04)	ANDOVER COLLEGE GRADUATION EVENT	9:00AM - 5:00PM		CONFIRMED 3000
TUS JUN 08 (TUS JUN 07)	CITY OF PORTLAND POLICE AUCTION LOAD IN	7:00AM - 7:00PM	EXHIBITOR VEHICLES	CONFIRMED 5
WED JUN 09 (WED JUN 08)	CITY OF PORTLAND POLICE AUCTION LOAD IN	7:00AM - 10:00AM	EXHIBITOR VEHICLES	CONFIRMED 5
	EVENT	10:00AM - 10:00PM		2000
FRI JUN 11 * (FRI JUN 10)	CHAMBER OF COMMERCE RECOGNITION NIGHT LOAD IN	7:00AM - 6:00PM	EXHIBITOR VEHICLES	CONFIRMED 5
	EVENT	6:00PM - 11:00PM		1000

FRI JUN 25 (FRI JUN 24)	MAINE ANTIQUE DEALERS SUMMER SHOW LOAD IN	7:00AM - 7:00PM	EXHIBITOR VEHICLES 100	CONFIRMED
SAT JUN 26 (SAT JUN 25)	MAINE ANTIQUE DEALERS SUMMER SHOW EVENT	10:00AM - 5:00PM	1000	CONFIRMED
SUN JUN 27 (SUN JUN 26)	MAINE ANTIQUE DEALERS SUMMER SHOW EVENT LOAD OUT	10:00AM - 5:00PM 5:00PM - 11:00PM	1000 EXHIBITOR VEHICLES 100	CONFIRMED
FRI JUL 16 (FRI JUL 15)	MAINE PAPER AMERICANA SHOW LOAD IN	7:00AM - 7:00PM	EXHIBITOR VEHICLES 100	TENTATIVE
SAT JUL 17 (SAT JUL 16)	MAINE PAPER AMERICANA SHOW EVENT LOAD OUT	10:00AM - 5:00PM 5:00PM - 11:00PM	1000 EXHIBITOR VEHICLES 100	TENTATIVE
THR SEP 09 (THR SEP 10)	KEENAN AUCTION COMPANY SKI AND CLOTHING SALE LOAD IN	7:00AM - 12 NOON	EXHIBITOR VEHICLES 50	CONFIRMED
	EVENT	12 NOON - 8:00PM	1000	
FRI SEP 10 (FRI SEP 11)	KEENAN AUCTION COMPANY SKI AND CLOTHING SALE EVENT	10:00AM - 8:00PM	2000	CONFIRMED
SAT SEP 11 (SAT SEP 12)	KEENAN AUCTION COMPANY SKI AND CLOTHING SALE EVENT	10:00AM - 6:00PM	2000	CONFIRMED
SUN SEP 12 (SUN SEP 13)	KEENAN AUCTION COMPANY SKI AND CLOTHING SALE EVENT LOAD OUT	10:00AM - 3:00PM 3:00PM - 11:00PM	1000 EXHIBITOR VEHICLES 50	CONFIRMED

* ASTERISKED ARE THE MAJOR EXPO EVENT DAYS WHERE AVOIDANCE OF CONCURRENT BOOKINGS WITH HADLOCK FIELD IS A TOP PRIORITY.

CITY OF PORTLAND, MAINE

MEMORANDUM

TO: BOB GANLEY, CITY MANAGER

FROM: MIKE JOSEPHSON, CITY PARKING MANAGER 

RE: HADLOCK FIELD PARKING

DATE: AUGUST 20, 1992

As you requested I contacted several other cities who have minor league baseball teams and stadiums in a like setting.

The cities that I contacted were as follows:

1. Pawtucket, Rhode Island, McCoy Stadium, Pawtucket Red Sox
Contact Person: Ray Houle, Director of Administration and Public Safety. (401) 726-0500.
2. New Britain, Connecticut, The Beehive Stadium at Willowbrook Park, New Britain Red Sox
Contact persons: At the Red Sox organization John Kameishi, (203) 224-8383, and Sgt. Chute at the town hall. (203) 826-3303.
3. Boston, MA, Fenway Park, Boston Red Sox,
Contact Person: Office of the Director of Resident parking, (617) 335-4880.
4. Columbus, OH, Cooper Stadium, Columbus Clippers
Contact Person: Jim Davis (614) 645-7790.
5. Scranton, Pennsylvania, Scranton Red Barrons, The Lackawanna County Stadium
Contact Person: Office of the Parking Director

I spoke with these individual cities about the impact of parking on the surrounding communities adjacent to the stadiums. I also asked them if they had residential parking programs in place and what solutions they used to deal with parking at the stadiums.

Their responses were as follows:

1. In Pawtucket, RI they currently have found no need for a residential only sticker program in the surrounding neighborhoods. They indicated that there is little inconvenience with baseball games and that the benefits far outweigh any problems. They also said that they had no egress problems to the main highways from the ball field.

In Pawtucket the Pawtucket Red Sox have been there for 16 years and the field was used for a AA Team previous to that. According to Ray Houle the ball field is located in a typical New England inner city area not unlike the neighborhood in Portland surrounding Hadlock Field. The general feeling that he gave me is that they have no problems with overflow parking in the neighborhoods.

2. In New Britain, CT, they experience very little impact on the surrounding community from parking at the Beehive Field. They have parking attached to the stadium and some on-street parking. The parking lot holds 750 spaces in conjunction with adjacent lots.

They average about 2,300-2,500 people per game and experience no overflow from their parking area. However, they did mention when there is an exhibition game and they experience upwards of 4,000 in attendance that they then fill up their existing parking, but even then indicated that they do not overflow into the surrounding neighborhoods, but rather local business lots are used for the overflow.

3. In Boston, MA there is a resident permit parking program in effect in the neighborhoods surrounding Fenway Park. They indicated to me that there are parking lots adjacent to the park that take care of all parking needs for the professional baseball games, and that there is very little on-street parking in the vicinity.

4. In Columbus, OH the stadium holds 15,000 and they have 5,000 parking spaces in parking lots adjacent to the stadium.

Interestingly enough they told me that just North of the stadium is a Freeway that is bordered by a residential community which as you know is not unlike the situation that we are facing at Hadlock Field. Jim Davis indicated that rarely do people cross the interstate to park in the residential community, and they have found no reason to put a residential sticker program into effect due to the limited number of incidents that they have experienced.

5. In Scanton, PA the stadium is out away from the city area on the Montage Mountain Road and is surrounded by businesses. Although there are some exclusive residential communities near to the stadium they experience no problems with people from the stadium, as most of them use the stadium parking or the parking lots from the surrounding businesses, and they therefore have no residential parking program in effect for those communities.

I have read the Hadlock Field parking study and it looks to me as though there is more than adequate parking through the combination of on-street spaces, as well as lots at Hadlock and Fitzpatrick, and surrounding parking lots. If however, there were day time games or special exhibition games we could run into some potential shortage since we park quite a few Maine Medical Center employees in the parking lot adjacent to the field, and at Fitzpatrick Stadium during the daytime hours.

Some additional alternatives that may be considered that were not mentioned in the parking study would be to run a shuttle bus to the Ramada parking lot which is just up the street if a usage deal could be worked out with the owner. This lot would be convenient to both the North and South 295 ramps and would avoid the problem of people having to bring their cars intown.

Another possible consideration might be using the open field which is directly adjacent to the stadium for overflow parking.

As I mentioned to you the Spring Street Garage will in all likelihood will be under utilized as regards Civic Center events since the loss of the Maine Mariners. Clearly running a shuttle bus between the Spring Street Garage and Hadlock Field, would make an additional 500 plus spaces available on weekends and nights when there are no concerts or other events going on at the Civic Center.

I hope this provides you with the information that you were looking for. If you need anything additional or you would like me to contact some other cities please let me know.

MAJ:kd/had.1

CITY OF PORTLAND

MEMORANDUM

TO: BOB GANLEY, CITY MANAGER

8/20/92

FROM: DUANE KLINE, FINANCE DIRECTOR

SUBJECT: ADDITIONAL FINANCIAL PROJECTIONS - AA BASEBALL PROPOSALS

The City Council at its Monday, August 17th workshop, requested some additional financial projections. The two original funding scenarios that were presented in the three ring binder were:

Scenario One:

. CIP 1993 (new bonds)	\$ 800,000
. 90-92 CIP Surplus - Anticipated	500,000
. Private Fundraising	<u>200,000</u>
	\$1,500,000

Scenario Two:

. CIP 1993 (new bonds)	\$ 600,000
. 90-92 CIP Surplus - Anticipated	500,000
. Private Fundraising	200,000
. Pre-paid Lease - 4 Years	<u>200,000</u>
	\$1,500,000

At the Council's request I have extended these projections from ten to twenty years on Attachments 1 and 2. I have also prepared Attachments 3 and 4, which add the estimated costs of the reallocated CIP funds to the new bonds proposed. These are as follows:

Attachment 1 - This report uses the same assumptions as in the Scenario One report dated August 14, 1992, except that the information is projected for twenty years, rather than ten. The twenty year estimated net cost is \$1,119,789.

Attachment 2 - This report uses the same assumptions as in the Scenario Two report (\$600,000 new bonds) dated August 14, 1992, except that the information is projected for twenty years, rather than ten. The twenty year estimated net cost is \$962,289.

Attachment 3 - Combines the costs and assumptions of scenario one (\$800,000 new bonds) and adds the estimated debt service from the \$500,000 1990-1992 CIP surplus accounts that would be re-allotted. The twenty year estimated net cost is \$1,922,714.

Attachment 4 - Combines the costs and assumptions of scenario two (\$600,000 new bonds), and adds the estimated debt service from the \$500,000 1990-1992 CIP surplus accounts that would be re-allotted. The twenty year estimated net cost is \$1,765,214.

Previous CIP authorizations for Hadlock Field were:

1985 Hadlock Field Lighting	\$125,000
1987 Hadlock Field Stadium Seating	\$125,000

Since these were not part of the current project, they would not need to be added to the current proposal for referendum determination purposes.

DGK.jlb

Attachments

FINANCIAL PROJECTION - 20 YEAR PROJECTION
RA BASEBALL PROPOSAL
SCENARIO # 1 A

NEW BONDS ISSUED: 800,000
ISSUE DATE: 01-Jan-93

DATE	PRINCIPAL	INTEREST	BALANCE	DEBT SERVICE FISCAL YR	EST MAINT COST	20 YEAR LEASE PAYMENTS	NET COST TO CITY
01-Jan-93			800,000				
01-Jul-93	30,000		800,000				
01-Jan-94	40,000		760,000	100,000	35,000	65,000	70,000
01-Jul-94	28,500		760,000				
01-Jan-95	40,000		720,000	97,000	36,225	65,000	68,225
01-Jul-95	27,000		720,000				
01-Jan-96	40,000		680,000	94,000	37,493	65,000	66,493
01-Jul-96	25,500		680,000				
01-Jan-97	40,000		640,000	91,000	38,805	65,000	64,805
01-Jul-97	24,000		640,000				
01-Jan-98	40,000		600,000	88,000	40,163	65,000	63,163
01-Jul-98	22,500		600,000				
01-Jan-99	40,000		560,000	85,000	41,549	65,000	61,549
01-Jul-99	21,000		560,000				
01-Jan-2000	40,000		520,000	82,000	43,024	65,000	60,024
01-Jul-2000	19,500		520,000				
01-Jan-2001	40,000		480,000	79,000	44,530	65,000	58,530
01-Jul-2001	18,000		480,000				
01-Jan-2002	40,000		440,000	76,000	46,088	65,000	57,088
01-Jul-2002	16,500		440,000				
01-Jan-2003	40,000		400,000	73,000	47,701	65,000	55,701
01-Jul-2003	15,000		400,000				
01-Jan-2004	40,000		360,000	70,000	49,371	65,000	54,371
01-Jul-2004	13,500		360,000				
01-Jan-2005	40,000		320,000	67,000	51,099	65,000	53,099
01-Jul-2005	12,000		320,000				
01-Jan-2006	40,000		280,000	64,000	52,887	65,000	51,887
01-Jul-2006	10,500		280,000				
01-Jan-2007	40,000		240,000	61,000	54,738	65,000	50,738
01-Jul-2007	9,000		240,000				
01-Jan-2008	40,000		200,000	58,000	56,654	65,000	49,654
01-Jul-2008	7,500		200,000				
01-Jan-2009	40,000		160,000	55,000	58,637	65,000	48,637
01-Jul-2009	6,000		160,000				
01-Jan-2010	40,000		120,000	52,000	60,690	65,000	47,690
01-Jul-2010	4,500		120,000				
01-Jan-2011	40,000		80,000	49,000	62,814	65,000	46,814
01-Jul-2011	3,000		80,000				
01-Jan-2012	40,000		40,000	46,000	65,012	65,000	46,012
01-Jul-2012	1,500		40,000				
01-Jan-2013	40,000		0	43,000	67,288	65,000	45,288
TOTALS	800,000	630,000		1,430,000	989,789	1,300,000	1,119,789

SCENARIO ONE ASSUMPTIONS:

SOURCE OF FUNDS:
NEW BONDS ISSUED 800,000
1990-92 CIF REALLOCATIONS 500,000
PRIVATE FUNDRAISING 200,000
TOTAL 1,500,000

10 YEAR LEASE AT \$65,000
RENEWED UNTIL YEAR 20
RD PREPAID LEASE INVOLVED

BONDS WILL BE GENERAL OBLIGATION
BONDS WILL BE FULLY TAX EXEMPT
BOND RATING: A-1
COSTS OF ISSUANCE NOT INCLUDED
STABLE BOND MARKET CONDITIONS

ANNUAL MAINTENANCE COST:
35,000

INFLATION ON MAINT. COSTS, ANNUALLY:
3.50%

PRINCIPAL AMOUNT 800,000
INTEREST RATE, ANNUAL 7.50%
TERM, YEARS 20

FINANCIAL PROJECTION - 20 YEAR PROJECTION
AA BASEBALL PROPOSAL
SCENARIO # 2 A

NEW BONDS ISSUED: 600,000
ISSUE DATE: 01-Jan-93

DATE	PRINCIPAL	INTEREST	BALANCE	DEBT SERVICE FISCAL YR	NEW EST MAINT COST	20 YEAR LEASE PAYMENTS	NET COST TO CITY
01-Jan-93			600,000				
01-Jul-93	22,500	22,500	600,000	75,000	35,000	15,000	95,000
01-Jan-94	30,000	22,500	570,000	75,000	35,000	15,000	95,000
01-Jul-94	21,375	21,375	540,000	72,750	36,225	15,000	93,975
01-Jan-95	30,000	20,250	510,000	70,500	37,493	15,000	92,993
01-Jul-95	19,125	19,125	480,000	68,250	38,805	15,000	92,055
01-Jan-96	30,000	18,000	450,000	66,000	40,163	15,000	91,163
01-Jul-96	16,875	16,875	420,000	63,750	41,569	15,000	90,319
01-Jan-97	30,000	15,750	390,000	61,500	43,024	15,000	89,524
01-Jul-97	14,625	14,625	360,000	59,250	44,530	15,000	88,780
01-Jan-98	30,000	13,500	330,000	57,000	46,088	15,000	88,088
01-Jul-98	12,375	12,375	300,000	54,750	47,701	15,000	87,451
01-Jan-99	30,000	11,250	270,000	52,500	49,371	15,000	86,871
01-Jul-99	10,125	10,125	240,000	50,250	51,099	15,000	86,349
01-Jan-2000	30,000	9,000	210,000	48,000	52,887	15,000	85,887
01-Jul-2000	7,875	7,875	180,000	45,750	54,738	15,000	85,488
01-Jan-2001	30,000	6,750	150,000	43,500	56,654	15,000	85,154
01-Jul-2001	5,625	5,625	120,000	41,250	58,637	15,000	84,887
01-Jan-2002	30,000	4,500	90,000	39,000	60,690	15,000	84,690
01-Jul-2002	3,375	3,375	60,000	36,750	62,814	15,000	84,584
01-Jan-2003	30,000	2,250	30,000	34,500	65,012	15,000	84,512
01-Jul-2003	1,125	1,125	0	32,250	67,288	15,000	84,536
01-Jan-2004	30,000	1,125	0	32,250	67,288	15,000	84,536
TOTALS	600,000	472,500	1,072,500	989,789	1,100,000	962,289	

SCENARIO TWO ASSUMPTIONS:

SOURCE OF FUNDS:
 NEW BONDS ISSUED 600,000
 1990-92 CIP REALLOCATIONS 500,000
 PRIVATE FUNDRAISING 200,000
 PREPAID LEASE 200,000
 TOTAL 1,500,000

10 YEAR LEASE AT \$15,000, YEARS 1-4
 AND AT \$65,000 YEARS 5-10
 RENEWED UNTIL YEAR 20
 PREPAID LEASE OF \$ 200,000 APPLIED TO
 CONSTRUCTION COSTS TO REDUCE BORROWING

BONDS WILL BE GENERAL OBLIGATION
 BONDS WILL BE FULLY TAX EXEMPT
 BOND RATING & Aa1
 COSTS OF INSURANCE NOT INCLUDED
 STABLE BOND MARKET CONDITIONS

ANNUAL MAINTENANCE COST:
35,000

INFLATION OR MAINT. COSTS, ANNUALLY:
3.50%

PRINCIPAL AMOUNT 600,000
 INTEREST RATE, ANNUAL 7.50%

TERM, YEARS 20

NA BASEBALL PROPOSAL
FINANCIAL PROJECTION - 20 YEAR
SHOWING ALL COSTS OF PROJECT DEBT
SCENARIO # 1 B

20-Aug-92
DBK

NEW BONDS ISSUED: 800,000
OLD BONDS REALLOCATED: 500,000
TOTAL PROJECT: 1,300,000

DATE	PRINCIPAL	INTEREST	BALANCE	DEBT SERVICE FISCAL YR	EST MAINT COST	70 YEAR LEASE PAYMENTS	NET COST TO CITY FISCAL YR
01-Jan-83			1,300,000				
01-Jul-83		44,425	1,300,000				
01-Jan-84	65,000	44,425	1,235,000	153,850	35,000	65,000	123,850
01-Jul-84		42,204	1,235,000				
01-Jan-85	65,000	42,204	1,170,000	149,408	36,225	65,000	120,633
01-Jul-85		39,983	1,170,000				
01-Jan-86	65,000	39,983	1,105,000	144,965	37,493	65,000	117,458
01-Jul-86		37,761	1,105,000				
01-Jan-87	65,000	37,761	1,040,000	140,523	38,805	65,000	114,328
01-Jul-87		35,540	1,040,000				
01-Jan-88	65,000	35,540	975,000	136,080	40,163	65,000	111,243
01-Jul-88		33,319	975,000				
01-Jan-89	65,000	33,319	910,000	131,638	41,549	65,000	108,267
01-Jul-89		31,098	910,000				
01-Jan-2000	65,000	31,098	845,000	127,195	43,024	65,000	105,219
01-Jul-2000		28,876	845,000				
01-Jan-2001	65,000	28,876	780,000	122,753	44,530	65,000	102,282
01-Jul-2001		26,655	780,000				
01-Jan-2002	65,000	26,655	715,000	118,310	46,088	65,000	99,398
01-Jul-2002		24,434	715,000				
01-Jan-2003	65,000	24,434	650,000	113,868	47,701	65,000	96,569
01-Jul-2003		22,213	650,000				
01-Jan-2004	65,000	22,213	585,000	109,425	49,371	65,000	93,796
01-Jul-2004		19,991	585,000				
01-Jan-2005	65,000	19,991	520,000	104,983	51,099	65,000	91,081
01-Jul-2005		17,770	520,000				
01-Jan-2006	65,000	17,770	455,000	100,540	52,887	65,000	88,427
01-Jul-2006		15,549	455,000				
01-Jan-2007	65,000	15,549	390,000	96,098	54,738	65,000	85,836
01-Jul-2007		13,328	390,000				
01-Jan-2008	65,000	13,328	325,000	91,655	56,654	65,000	83,309
01-Jul-2008		11,106	325,000				
01-Jan-2009	65,000	11,106	260,000	87,213	58,637	65,000	80,850
01-Jul-2009		8,885	260,000				
01-Jan-2010	65,000	8,885	195,000	82,770	60,690	65,000	78,460
01-Jul-2010		6,664	195,000				
01-Jan-2011	65,000	6,664	130,000	78,328	62,814	65,000	76,141
01-Jul-2011		4,443	130,000				
01-Jan-2012	65,000	4,443	65,000	73,885	65,012	65,000	73,897
01-Jul-2012		2,221	65,000				
01-Jan-2013	65,000	2,221		69,443	67,288	65,000	71,730
TOTALS	1,300,000	932,925		2,232,925	989,789	1,300,000	1,922,714

SCENARIO ONE ASSUMPTIONS:

SOURCE OF FUNDS:
NEW BONDS ISSUED: 800,000
1990-92 CIP REALLOCATION: 500,000
PRIVATE FUNDRAISING: 200,000
TOTAL: 1,500,000

10 YEAR LEASE AT \$65,000
RENEWED UNTIL YEAR 20
NO PREPAID LEASE INVOLVED

UNSPENT 1990-92 CIP BOND PROCEEDS
WOULD BE APPLIED TO THIS PROJECT

BONDS WILL BE GENERAL OBLIGATION
BONDS WILL BE FULLY TAX EXEMPT
BOND RATING: A-1
COSTS OF ISSUANCE NOT INCLUDED
STABLE BOND MARKET CONDITIONS

ANNUAL MAINTENANCE COST:
35,000

INFLATION ON MAINT. COSTS, ANNUALLY:
3.50%

PRINCIPAL AMOUNT: 800,000
INTEREST RATE, ANNUAL: 7.50%
TERM, YEARS: 20

AA BASEBALL PROPOSAL

FINANCIAL PROJECTION - 20 YEAR

SHOWING NET COSTS OF PROJECT-DEBT

SCENARIO # 2 B

NEW BONDS ISSUED: 500,000
 OLD BONDS REALLOCATED: 500,000
 TOTAL PROJECT: 1,000,000

DATE	PRINCIPAL	INTEREST	BALANCE	DEBT SERVICE FISCAL YR	EST MAINT COST	20 YEAR LEASE PAYMENTS	NET COST TO CITY FISCAL YR
01-Jan-93			1,000,000				
01-Jul-93	36,925	36,925	1,100,000			15,000	146,850
01-Jan-94	55,000	36,925	1,045,000	128,850	35,000		
01-Jul-94		35,079	1,045,000				
01-Jan-95	55,000	35,079	990,000	125,158	36,225	15,000	146,383
01-Jul-95		33,233	990,000				
01-Jan-96	55,000	33,233	935,000	121,465	37,493	15,000	143,958
01-Jul-96		31,386	935,000				
01-Jan-97	55,000	31,386	880,000	117,773	38,805	15,000	141,578
01-Jul-97		29,540	880,000				
01-Jan-98	55,000	29,540	825,000	114,080	40,163	65,000	89,243
01-Jul-98		27,694	825,000				
01-Jan-99	55,000	27,694	770,000	110,388	41,569	65,000	86,957
01-Jul-99		25,848	770,000				
01-Jan-2000	55,000	25,848	715,000	106,695	43,024	65,000	84,719
01-Jul-2000		24,001	715,000				
01-Jan-2001	55,000	24,001	660,000	103,003	44,536	65,000	82,532
01-Jul-2001		22,155	660,000				
01-Jan-2002	55,000	22,155	605,000	99,310	46,088	65,000	80,398
01-Jul-2002		20,309	605,000				
01-Jan-2003	55,000	20,309	550,000	95,618	47,701	65,000	79,319
01-Jul-2003		18,463	550,000				
01-Jan-2004	55,000	18,463	495,000	91,925	49,371	65,000	76,296
01-Jul-2004		16,616	495,000				
01-Jan-2005	55,000	16,616	440,000	88,233	51,099	65,000	74,331
01-Jul-2005		14,770	440,000				
01-Jan-2006	55,000	14,770	385,000	84,540	52,887	65,000	72,427
01-Jul-2006		12,924	385,000				
01-Jan-2007	55,000	12,924	330,000	80,848	54,738	65,000	70,588
01-Jul-2007		11,078	330,000				
01-Jan-2008	55,000	11,078	275,000	77,155	56,654	65,000	68,809
01-Jul-2008		9,231	275,000				
01-Jan-2009	55,000	9,231	220,000	73,463	58,637	65,000	67,100
01-Jul-2009		7,385	220,000				
01-Jan-2010	55,000	7,385	165,000	69,770	60,690	65,000	65,460
01-Jul-2010		5,539	165,000				
01-Jan-2011	55,000	5,539	110,000	66,078	62,814	65,000	63,891
01-Jul-2011		3,693	110,000				
01-Jan-2012	55,000	3,693	55,000	62,385	65,012	65,000	62,397
01-Jul-2012		1,848	55,000				
01-Jan-2013	55,000	1,848		58,493	67,288	65,000	60,980
TOTALS	1,000,000	775,425		1,875,425	989,789	1,100,000	1,765,214

SCENARIO 1ND ASSUMPTIONS:

SOURCE OF FUNDS:
 NEW BONDS ISSUED: 500,000
 1990-92 CIP REALLOCATION: 500,000
 PRIVATE FUNDRAISING: 200,000
 PREPAID LEASE: 200,000
 TOTAL: 1,500,000

10 YEAR LEASE AT \$15,000, YEARS 1-4
 AND AT \$ 65,000 YEARS 5-10
 RENEWED UNTIL YEAR 20

PREPAID LEASE OF \$ 200,000 APPLIED TO
 CONSTRUCTION COSTS TO REDUCE BORROWINGS

UNSPENT 1990-92 CIP BOND PROCEEDS
 WOULD BE APPLIED TO THIS PROJECT

BONDS WILL BE GENERAL OBLIGATION
 BONDS WILL BE FULLY TAX EXEMPT

BOND RATINGS: Aa1
 COSTS OF ISSUANCE NOT INCLUDED
 STABLE BOND MARKET CONDITIONS

ANNUAL MAINTENANCE COST:

INFLATION ON MAINT. COSTS, ANNUALLY:
 3.50%

PRINCIPAL AMOUNT: 500,000
 INTEREST RATE, ANNUAL: 7.50%
 TERM, YEARS: 20

CITY OF PORTLAND, MAINE
M E M O R A N D U M

TO: Robert B. Ganley, City Manager
FROM: Elizabeth L. Boynton, Associate Corporation Counsel
DATE: August 18, 1992
RE: "People's Veto" and Baseball Bonds

* * *

The issue was raised at last night's workshop as to whether the baseball bonds were subject to the "People's Veto" procedure under our initiative and referendum ordinance. The answer is no, for the reasons below.

First, our the People's Veto process applies only to an "ordinance dealing with legislative matters on municipal affairs..." Sec. 9-36, Portland City Code.¹ The Bond Order is not such an ordinance. Secondly, there is a specific Charter provision which spells out the procedure for sending Bond Orders to referendum, and I believe this Charter procedure would govern over any ordinance as to when a bond order was subject to referendum.

Thus, I do not believe the Bond Orders scheduled to be acted upon on the 24th are subject to the Initiative and Referendum (People's Veto) procedure of Chapter 9, Article III of the City Code.

A related question is whether earlier expenditures on Hadlock Field must be added to this bond order to determine whether the Charter threshold amount is met. My understanding of the general rule is that it must be considered the "same" project in order to require adding different bond issues together. I do not believe earlier expenditures could be considered part of this project, and I will confirm that with bond counsel as soon as possible.

Please let me know if you have any questions.


Elizabeth L. Boynton
Associate Corporation Counsel

ELB:bb
Duane Kline, Finance Director

¹ With some exceptions, which have been part of the ordinance for many years, i.e. ordinances dealing with appropriations, tax levy or with wages or hours of city employees. §9-36(b) Portland City Code.

CITY OF PORTLAND, MAINE
M E M O R A N D U M

TO: Robert B. Ganley, City Manager

FROM: Elizabeth L. Boynton, Associate Corporation Counsel
Ext. 8480

DATE: August 19, 1992

RE: Baseball Stadium and Public Drinking

* * *

You asked whether the public drinking law would prevent the sale of beer (and/or wine) at the proposed baseball stadium. The answer is no, for the reasons below.

The public drinking law prohibits drinking in certain public places without the permission of the owner of the property. 17 M.R.S.A. §2003-A. The State liquor laws, however, allow the sale of beer and wine in certain outdoor stadiums. An "outdoor stadium" is defined as follows:

...any commercially operated outdoor facility with 5,000 or more seats designed or used for the playing of any sport or event, which is open to the general public, which charges a fee and which has adequate facilities for the sale and consumption of wine and malt liquor. 28-A M.R.S.A. §2(15)(M).

An outdoor facility may obtain either a Class III (wine) or Class IV (malt liquor) license, or both.

Thus, there is no legal conflict with the public drinking law so long as a license is obtained to sell beer and/or wine at the baseball games. There are some restrictions, e.g. the "licensee may not sell any liquor...at any event primarily involving primary or secondary school children" and liquor cannot be sold in the stands and must be in plastic or paper cups. 28-A M.R.S.A. §1074.

You also asked how the Civic Center operates in regard to beer and wine sales. Again, there is a specific license for sales at civic auditoriums, and the Civic Center has such a license. The Civic Center controls all of the concessions and receives the proceeds from the sales. In our case, our tenant could be the licensee and in that case, we would require them to have all of the appropriate insurance protections in regard to alcohol and food sales to protect both them and the City.

Please let me know if you have any questions.


Elizabeth L. Boynton
Associate Corporation Counsel

ELB:bb

CITY OF PORTLAND, MAINE
M E M O R A N D U M

TO: Robert B. Ganley, City Manager

FROM: Elizabeth L. Boynton, Associate Corporation Counsel
Ext. 8480

DATE: August 19, 1992

RE: Outdoor Advertising and Hadlock Field

* * *

You asked whether the Hadlock Field signs would violate the billboard laws if they could be seen from I-295. The answer is yes.

State law, which implements the Federal Highway Act, essentially prohibits any signs which are visible to the traveling public from any public way. "Public way" includes any road capable of carrying motor vehicles, whether a federal, state or local road. 23 M.R.S.A. §§1903(11) and 1908. There are certain exceptions to this prohibition such as "on-premise" signs which advertise the business being carried on (thus a sign advertising the ball team itself might be permitted) and business directional signs. (Even the permitted signs must be located more than 660 feet away from the nearest edge of the interstate system if they can be read from the interstate highway and on-premise signs are prohibited if they contain flashing, intermittent or moving lights, and are visible from the public way).

Thus, it will be necessary to take this limitation into account in the planning and design of Hadlock Field and the location of advertising, if any, on and around the Field.

Please let me know if you have any questions.

Elizabeth
Elizabeth L. Boynton
Associate Corporation Counsel

ELB:bb

CITY OF PORTLAND, MAINE
PARKS & PUBLIC WORKS DEPARTMENT

M E M O R A N D U M

TO: George A. Flaherty, Director

FROM: John P. Rague, Senior Technician *JR*

SUBJECT: Exposition Building Locker Room Facilities

DATE: August 14, 1992

Per the request of the City Manager, Bill Whited, Richard Lauck, Mary Theriault and myself inspected the existing locker rooms in the basement of the Exposition Building. Following the inspection, we met to discuss our findings and derived at the following conclusions.

There are two (2) locker room facilities adjacent to the entrance to the Expo intended to be utilized in conjunction with the new stadium. There are other facilities within the general area of these locker rooms which can be utilized to service the ancillary uses mandated by the league. I have checked the existing square footages of these facilities and compared same with the minimums allowed for "existing structures" under the league rules. We are either in excess of the minimums or close enough to the minimums to satisfy the league.

In keeping with league requirements, a nominal amount of improvements would be necessary. These improvements include:

- 1) Converting an existing storage area into a combination laundry facility and trainer's room. Some plumbing would be needed to complete these improvements.
- 2) The home team field manager's office/locker room and the home team coaches' locker room would need some internal alterations to the space intended for these uses.
- 3) Adjacent to the proposed home team locker room (within an existing open space), a chain link fenced area would be provided for home team storage.
- 4) Carpeting and painting of appropriate areas.
- 5) Some type of rubberized runner down the corridor to protect the floor and provided a safe means of ingress/egress for the players and coaches.

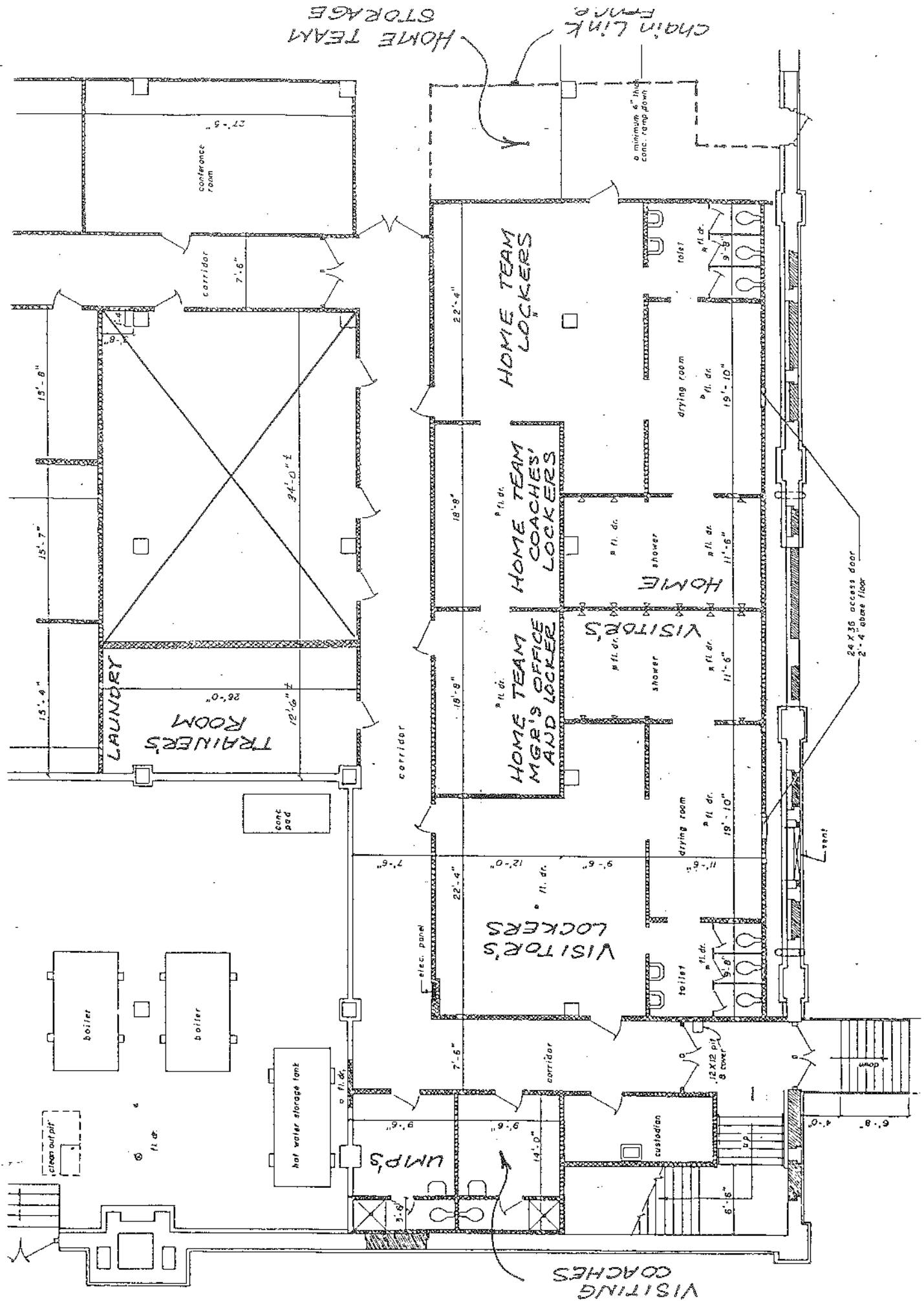
There exists a "conference room" which could be utilized by both teams without any improvement costs.

It is my opinion that the aforementioned improvements/alterations could be completed at a cost not to exceed \$25,000.00.

I have enclosed a plan showing these facilities and highlighted the areas which would need some financial input to bring the entire facility within or very close to league standards.

I hope this satisfies Mr. Ganley's needs at this point. Should there be anything further I can do, please let me know.

cc: Robert B. Ganley, City Manager
Nadeen M. Daniels, Assistant City Manager
William J. Bray, Deputy Director Parks/Public Works
Richard Lauck, Public Buildings Manager
Mary Theriault, Traffic
William E. Whited, Engineer/Architect



PLANNING BOARD REPORT #11-02

RECREATION-OPEN SPACE ZONE

TEXT AMENDMENT

CITY OF PORTLAND, APPLICANT

Submitted to:

Portland Planning Board
Portland, ME

February 12, 2002

I. INTRODUCTION

A public hearing has been scheduled to consider a proposal by the City of Portland to amend the text of the Recreation Open space (R-OS) zone. A copy of the proposed zoning text is shown as Attachment A. The applicant is the City of Portland Public Assembly Facilities Division.

Public notice of the public hearing consisted of newspaper advertisements in the *Portland Press Herald* and 456 notices sent to area property owners in the vicinity of the sports complex. The applicant held a neighborhood meeting concerning this proposal on January 31, 2002.

II. BACKGROUND

The proposed text amendments relate to the R-OS zone. These amendments are proposed to differentiate the City's sports complex on Park Avenue (Hadlock Field, Exposition Building, Portland Ice Arena, Fitzpatrick Stadium) from other parks and open spaces in the city. The amendments would allow more flexibility for impervious surface coverage and floor area ratios for a sports complex use in the R-OS.

The text amendments are needed since the sports complex has reached the maximum 25% impervious standard of the R-OS Zone. Improvements to the facility have occurred in small incremental steps over the years to the point that no further improvements such as building additions or walkways can be built unless the impervious standard is adjusted.

Most public park spaces in the City are zoned R-OS. The amendment has been drafted very narrowly. It will affect only the sports complex. We did not want to introduce the possibility of a concentrated form of development such as the sports complex to other more pastoral green spaces in the city.

Back in January 1997, the Board reviewed virtually the same zoning amendments. The Planning Board held a public hearing on the text amendments and made a positive recommendation to the City Council. It was not forwarded to the Council because a major grandstand renovation for Fitzpatrick Stadium did not go forward. The impetus for the renovation was the opportunity to lure a soccer team that decided to locate elsewhere. Since the Board's R-OS recommendation is now five years old, the amendments should be re-reviewed and a fresh recommendation sent to the City Council.

The proposed amendments were reviewed with the Friends of the Parks Commission. The Commission voted to endorse the amendments at their January 22nd meeting.

III. PROPOSED AMENDMENTS

The proposed amendments include a new term (sports complex), adding a policy statement to the R-OS purpose section, revising the maximum lot coverage and maximum floor area for sports complexes.

Sports Complex Definition

A new term, sports complex, is proposed in the definition section of the ordinance (14-47). The definition is shown below:

Sports Complex: One or more facilities located on the same parcel of land where athletic events are held and with a combined seating capacity of at least six thousand (6,000 seats).

The term sports complex would also be added as a permitted use in the R-OS text (Sec. 14-154). While ball fields and athletic fields are already permitted in this zone, a sports complex definition differentiates it from other park spaces. Aside from the Hadlock Field and Fitzpatrick Stadium, there are no other existing or anticipated park sites in the R-OS zone with a 6,000 seating capacity. The next largest facility, Presumpscot Park, has a seating capacity of half that number.

Recreation Open Space Zone Policy Statement

A fourth policy statement is being added to the R-OS purpose section recognizing the role of the sports complex as a large regional sports and athletic facility.

The policy statement of the R-OS zone is shown below.

- 1) To preserve and protect open space as a limited and valuable open space;
- 2) To permit the reasonable use of open space, while simultaneously preserving and protecting its inherent open space characteristics to assure its continued availability for public use as scenic, recreation, and conservation or natural resource area, and for the containment and structuring of urban development; and
- 3) To coordinate with and carry out federal, state, regional, and city recreation and open space plans.
- 4) To provide a suitable location for large-scale regional sports and athletic facilities.

Maximum Impervious Coverage

Currently the R-OS zone limits lot coverage of buildings, structures and other impervious improvements (parking lots, sidewalks) to no more than 25% of lot area (sec. 14-157(6)). The one current exception is sewage treatment facilities (by the Eastern Prom) which have no limit on coverage. When Hadlock Field was originally constructed, the sports complex had an impervious lot coverage of just under 25%. With various incremental improvements made to the sports complex over the years, the facility has reached the 25% standard.

We have drafted two alternative amendments. The 50% impervious standard was discussed at the workshop. The second alternative amendment would have no limit on maximum coverage. Staff is recommending the no limit on maximum coverage alternative since it provides more flexibility. The amendment is shown below:

Sec 14-157(b) maximum coverage of lot by buildings, structures and other impervious site improvements such as paved sidewalks, drives and parking lots:

- a. Sewage treatment facilities: No limit on maximum coverage
- b. Sports complexes: Fifty (50) percent of lot area or no limit on maximum coverage.
- c. All other uses: Twenty-five percent of lot area.

Maximum Floor Area Ratio

An amendment is proposed to the maximum floor area ratio in the R-OS zone (sec. 14-157(7)). Presently the standard requires that floor area ratios may not exceed 0.2. Floor area ratio is defined as the total floor area of buildings in proportion to the total land area of the site. Hadlock Field Stadium, Expo and Portland Ice Arena have enclosed floor area.

The proposed amendment is shown below:

Sec. 14-157(7) maximum floor area ratio: ~~Two-tenths (0.2)~~ Five-tenths (0.5).

IV. LAND USE POLICY

The purpose of these amendments is to clearly differentiate the sports complex on Park Avenue which consists of Hadlock Field, Exposition Building, Portland Ice Arena and Fitzpatrick Stadium from other park spaces in the city. The scale, size and number of facilities and buildings offered at the sports complex contrasts with the remaining 100 plus spaces of the park system which are primarily open spaces

and smaller athletic facilities in the city. The concentration of such facilities in one location provides for a sharing of resources and conserves existing open spaces so that redundant infrastructure such as utilities, parking and blacktop need not be introduced to other park spaces. Located near the downtown and I-295, the present sports complex site is a logical site for major sports facilities in the city.

While the R-OS zone is appropriate for the city park system, the text did not anticipate the success of the sports complex. With the construction of the Portland Ice Arena (1984), Hadlock Field (1993) and improvements to Fitzpatrick Stadium, text amendments are needed to assure that facility improvements can be accommodated in the future. According to the Portland Public Assembly Facilities Division, over 750,000 people attended events at the sports complex last year.

V. MOTIONS FOR THE BOARD TO CONSIDER

On the basis of plans and materials submitted by the applicant, the policies of the comprehensive plan and the information provided in Planning Board Report #11-02 and/or other findings as follows:

The Board finds that:

1. The proposed zone change (is or is not) consistent with policies of the Comprehensive Plan.

The Planning Board therefore (recommends or does not recommend) to the City Council approval of the proposed zoning amendments.

Sec. 14-157(f) maximum coverage of lot alternative for sports complex (choose a or b)

- a. Fifty (50) percent of lot area
- or
- b. No limit on maximum coverage

Attachments

- A. Zoning Amendments
- B. Sports Complex Map
- C. Zone Change Application

City of Portland, Maine

IN THE CITY COUNCIL

AMENDMENT TO PORTLAND CITY CODE
SS14-47, 14-153, 14-154, 14-157 (ZONING ORDINANCE)
RE: SPORTS COMPLEXES IN THE R-OS ZONE

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF PORTLAND,
MAINE IN CITY COUNCIL ASSEMBLED AS FOLLOWS:

1. That Section 14-47 of the Portland City Code is hereby amended to add a new definition to read as follows:

Sec. 14-47. Definitions.

The following words shall be defined as set forth below for use in this article. Definitions set forth in the building code of the city shall apply to words not herein defined:

Sports complex: One or more facilities located on the same parcel of land where athletic events are held and with a combined seating capacity of at least six thousand (6,000) seats.

2. That Section 14-153(a) is hereby amended to read as follows:

Sec. 14-153. Purpose.

(a) The purpose of this division is:

(1) To preserve and protect open space as a limited and valuable resource;

(2) To permit the reasonable use of open space, while simultaneously preserving and protecting its inherent open space characteristics to assure its continued availability for public use as scenic, recreation, and conservation or natural resource area, and for the containment and structuring of urban development; and

(3) To coordinate with and carry out federal, state, regional, and city recreation and open space plans; and

(4) To provide a suitable location for large-scale regional sports and athletic facilities.

3. That Section 14-154(m) and 14-154(n) is hereby amended to read as follows:

(m) Sports complexes.

(n) Accessory uses, including structures or buildings of less than two thousand five hundred (2,500) square feet of floor area.

4. That Section 14-157(f) and (g) is hereby amended to read as follows:

(f) Maximum coverage of lot by buildings, structures and other impervious site improvements such as paved sidewalks, drives and parking lots: ~~Twenty-five (25) percent of lot area, except that sewage treatment facilities are not required to meet this standard.~~

1. Sewage treatment facilities: No limit on maximum coverage.

2. Sports complexes: Fifty (50) percent of lot area. OR No limit on maximum coverage.

3. All other uses: Twenty-five (25) percent of lot area.

(g) Maximum floor area ratio: ~~Two tenths (0.2)~~ Five-tenths (0.5).



APPLICATION FOR ZONING AMENDMENT
City of Portland, Maine
Department of Planning and Urban Development
Portland Planning Board

1. Applicant Information:
PUBLIC ASSEMBLY FACILITIES DIVISION
Name FRANK LATORNO, DIVISION DIRECTOR
239 PARK AVE
Address
PORTLAND ME 04107
874 8000 874 8130
Phone Fax

2. Subject Property:
SPORTS COMPLEX AT
Address
239 PARK AVE
PORTLAND ME
(INCLUDES ICE AREA, EXPO,
Assessor's Reference (Chart-Block-Lot)
HADLOCK FIELD, FITZPATRICK
STADIUM AND ADJOINING
CITY OWNED PARKING LOTS,

3. Property Owner: Applicant Other
CITY OF PORTLAND, MAINE
Name

Address

Phone Fax

4. Right, Title, or Interest: Please identify the status of the applicant's right, title, or interest in the subject property:
THIS IS CITY OF PORTLAND PROPERTY

Provide documentary evidence, attached to this application, of applicant's right, title, or interest in the subject property. (For example, a deed, option or contract to purchase or lease the subject property.)

5. Vicinity Map: Attach a map showing the subject parcel and abutting parcels, labeled as to ownership and/or current use. (Applicant may utilize the City Zoning Map or Parcel Map as a source.)

6. Existing Use:

Describe the existing use of the subject property: PUBLIC ASSEMBLY FACILITIES USED
FOR A VARIETY OF PURPOSES

7. Current Zoning Designation(s): ROS

8. Proposed Use of Property: Please describe the proposed use of the subject property. If construction or development is proposed, please describe any changes to the physical condition of the property.

(See above)

9. Sketch Plan: On a separate sheet please provide a sketch plan of the property, showing existing and proposed improvements, including such features as buildings, parking, driveways, walkways, landscape and property boundaries. This may be a professionally drawn plan, or a carefully drawn plan, to scale, by the applicant. (Scale to suit, range from 1"=10' to 1"=100')

10. Proposed Zoning: Please check all that apply:

A. Zoning Map Amendment, from _____ to _____

B. Zoning Text Amendment to Section 14- 157(6)

For Zoning Text Amendment, attach on a separate sheet the exact language being proposed, including existing relevant text, in which language to be deleted is depicted as crossed out (example), and language to be added is depicted with underline (example).

C. Conditional or Contract Zone

A conditional or contract rezoning may be requested by an applicant in cases where limitations, conditions, or special assurances related to the physical development and operation of the property are needed to ensure that the rezoning and subsequent development are consistent with the comprehensive plan and compatible with the surrounding neighborhood. (Please refer to Division 1.5, Sections 14-60 to 62)

11. Application Fee: A fee for this application for a zoning amendment must be submitted, by check payable to the City of Portland in accordance with Section 14-54 of the Municipal Code (see below). The applicant also agrees to pay all costs of publication (or advertising) of the Public Hearing Notice as required for this application. Such amount will be billed to the applicant following the appearance of the advertisement.

	<u>1-25 Units</u>	<u>26-50 Units</u>	<u>51-75 Units</u>	<u>75 & Over</u>
Residential Zones	\$350.00	\$400.00	\$450.00	\$500.00
Nonresidential Zones	\$350.00	\$400.00	\$450.00	\$500.00
	0-15,000 sq. ft. or 0-5 acres (whichever is less)	15,000-30,000 sq. ft. or 6-10 acres (whichever is less)	30,000-45,000 sq. ft. or 10-15 acres (whichever is less)	45,000-60,000 sq. ft. or 15-20 acres (whichever is less)

- Legal Advertisements (one for workshop and one for public hearing) percent of total bill
- Notices (one for workshop and one for public hearing) 40 cents each
- Text Amendments \$300.00

_____ Amount of Fee

12. Signature: The above information is true and accurate to the best of my knowledge.

12/21/01
Date of Filing

Frank LaTone FOR CITY OF PORTLAND'S
Signature of Applicant PUBLIC ASSEMBLY FACILITIES
Division

Further Information:

Please contact the Planning Office for further information regarding the rezoning process. Applicants are encouraged to make an appointment to discuss their rezoning requests before filing the application.

Applicants are encouraged to include a letter or narrative to accompany the rezoning application which can provide additional background or context information, and describe the proposed rezoning and reasons for the request in a manner that best suits the situation.

In the event of withdrawal of the zoning amendment application by the applicant in writing prior to the submission of the advertisement copy to the newspaper to announce the public hearing, a refund of two-thirds of the amount of the zone change fee will be made to the applicant by the City of Portland.

Portland Planning Board
Portland, Maine

Effective: July 6, 1998

Frank P. LaToire, C.F.E.
Division Director

Arthur H. Stephenson III
Assistant Director



Joseph E. Gray, Jr.
City Manager

Anita R. Lachance
Assistant City Manager

City of Portland
Public Assembly Facilities Division

Merrill Auditorium at City Hall Portland Exposition Building Hadlock Field Fitzpatrick Stadium

Need for Zoning Amendments to Section 14-157(6)

The Portland Sports Complex combines Hadlock Field, the Portland Exposition Building, Fitzpatrick Stadium, the Portland Ice Arena, the "Horse Barn" building and adjoining City owned property and parking lots.

We are presently at the 25% area that is all that can be covered with impervious surface according to City of Portland ordinance 14-157. We need to be exempt from this ordinance because it has been the will of the Portland City Council to improve these facilities to make them safer and more user-friendly to the public. This includes more paving of pathways to eliminate dirt/mud walkways that can be a danger during inclement weather. We are making ADA improvements to Fitzpatrick Stadium to increase handicap accessibility to prime viewing areas. At some point, we are planning to add a locker room and rest rooms to Fitzpatrick to handle the increased community usage that has resulted from the installation of sports turf on the playing field.

Presently over 750,000 people enjoy a variety of events at the Portland Sports Complex. All the plans to improve the facility are in line with the City's mission to make this a thriving "community center." Thus we should be exempt from ordinance 14-157 so that we can carefully and judiciously improve usage, safety and accessibility.

Frank LaToire

From: Frank LaTorre
To: Rick Knowland
Date: Mon, Feb 4, 2002 11:12 AM
Subject: Summary of the Neighborhood Meeting

The City of Portland held a Neighborhood Meeting at the Expo at 7PM Thursday January 31, 2002. Proper notification of the meeting and its purpose, to get input on proposed zoning amendments for the Sports Complex on Park Avenue, were detailed in the mailing sent to over 425 households. No one attended the meeting.

CC: ANITA R. LACHANCE

**CITY OF PORTLAND, MAINE
MEMORANDUM**

TO: Joseph E. Gray, Jr., City Manager
FROM: Richard Knowland, Senior Planner
DATE: April 16, 2002
RE: Recreation-Open Space (R-OS) Zoning Text Amendments Related to the Sports Complex

This memo is intended to give an update on the Recreation-Open Space (R-OS) Zone Text Amendments related to the sports complex. This item was tabled at the April 1st Council meeting.

Neighborhood Meeting

The Parkside Neighborhood Association has been contacted whether they would like a presentation of the sports complex zoning amendments for their neighborhood meeting.

Tony Holt (Parkside Neighborhood Association representative) indicated in a conversation with Lee Urban that the Parkside Neighborhood Association has no objections to the amendments and a meeting with them is not necessary. A neighborhood meeting had been previously scheduled (on January 31, 2003) at the Portland Expo but no one from the neighborhood attended.

Actual Impervious Surface Percentage

At a previous City Council meeting, there was a comment that perhaps the R-OS sports complex impervious surface percentage should be increased from 25% to 50% rather than the 75% recommended by the Planning Board. Impervious surfaces areas include buildings, parking, walkways and other "hard" surfaces.

With the assistance of the Public Works GIS Workgroup, the existing impervious surface for the sports complex site has been calculated. The impervious surface for the sports complex is 54.27% according to their calculations. See attached map. Staff therefore continues to recommend the 75% coverage standard. This would give the City some flexibility to initiate future improvements to the site.

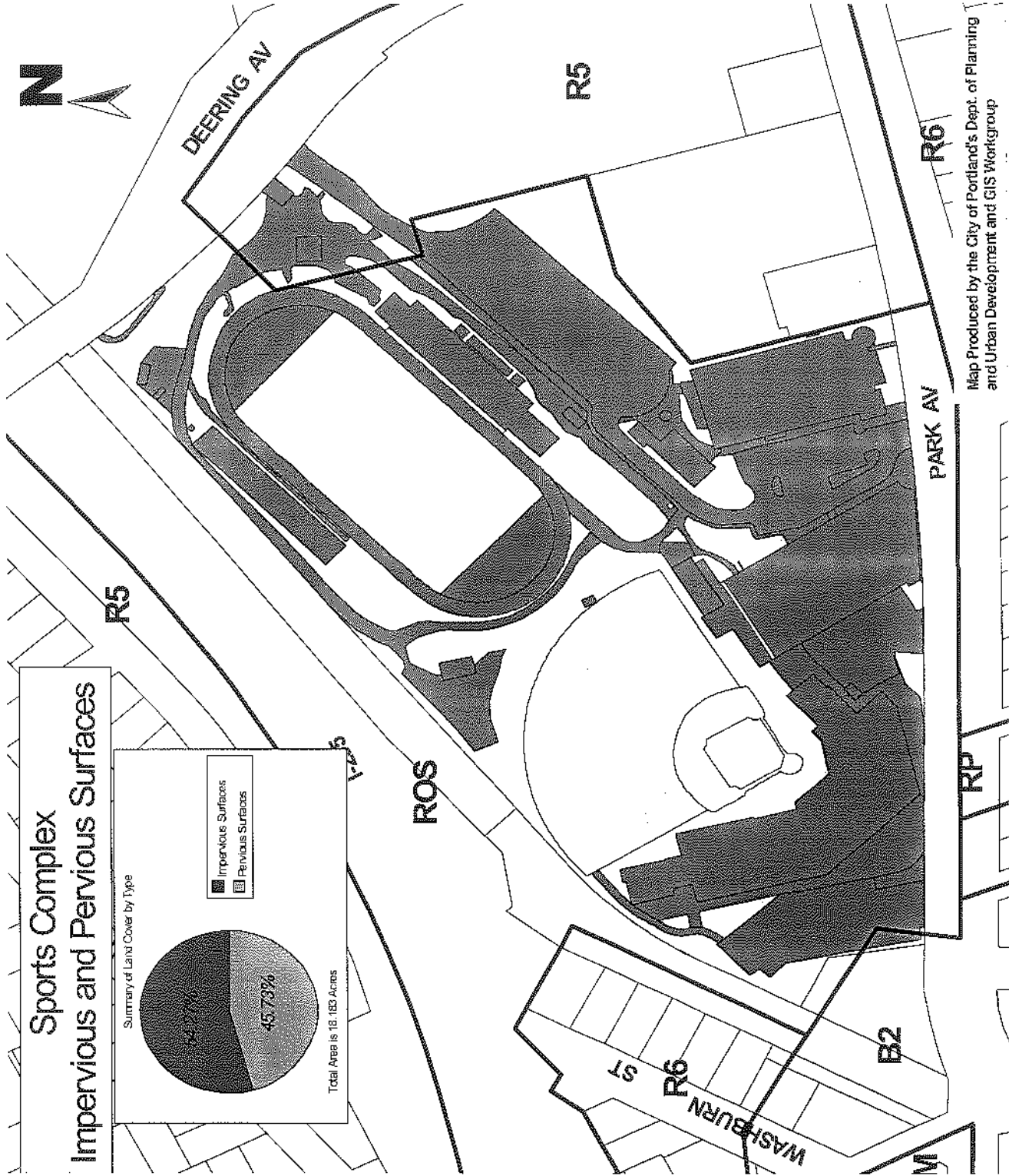
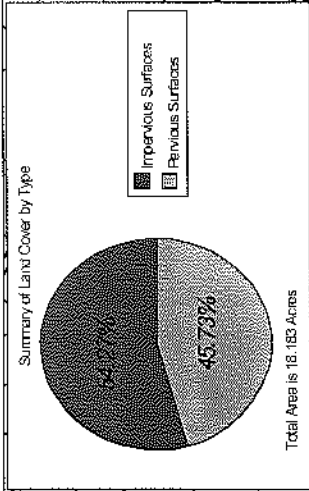
Impervious Surface Increase

The original Hadlock Field Development Plan (to accommodate the Sea Dogs submitted to the Planning Office referenced a 24.87% impervious surface coverage on the site. The impervious calculation by the GIS Group is much higher. There are several reasons for this increase.

1. I suspect the original calculation did not include the bleacher seating of Fitzpatrick Stadium nor the many gravel walkways and driveways that run on and through the stadium complex. Since 1993 some of these have been paved and the number of walkways has increased. The bleachers may not be physically covered but they should be considered impervious for zoning purposes.
2. There is a storage building behind the outfield fence of Hadlock Field (built in 1999) but the accompanying gravel staging area is many times larger than the building. A trailer has also been placed in this area.
3. Over the years there have been a series of small-scale improvements made to Hadlock Field. The actual amount of impervious surfaces within Hadlock Field has been very limited.
4. The Fitzpatrick Stadium surfaces have changed since 1993. The new track surfaces surrounding the football field should be considered impervious. Of all the surface changes discussed above, this appears to represent the largest impervious surface increase among the post-Hadlock Field improvements.



Sports Complex Impervious and Pervious Surfaces



CITY OF PORTLAND, MAINE
CITY COUNCIL AGENDA REQUEST FORM

TO: Linda Cohen, City Clerk
FROM: Alexander Jaegerman
DATE: February 19, 2002
SUBJECT: Agenda Request

- 1) Council Meeting at which action is requested (Date): March 18, 2002
2) Can action be taken at a later date? YES NO

I. SUMMARY OF ISSUE

The Planning Board is forwarding a recommendation to the City Council to approve a series of zoning text amendments to the Recreation Open Space (R-OS) Zone. These amendments are proposed to differentiate the City's sports complex on Park Avenue (Hadlock Field, Exposition Building, Portland Ice Arena, Fitzpatrick Stadium) from other parks and open spaces in the City. The amendments would allow more flexibility for impervious surface coverage and floor area ratios for a sports complex in the R-OS zone.

II. REASON FOR SUBMISSION (What issue/problem will this address?)

The text amendments are needed since the sports complex has reached the maximum 25% impervious standard of the R-OS zone. Improvements to the facility have occurred in small incremental steps over the years to the point that no further improvements such as walkways or building additions can be constructed unless the impervious standard is adjusted.

III. INTENDED RESULT (How does it resolve the issue/problem?)

The proposed R-OS text amendments allow more flexibility for future improvements to the sports complex.

IV. FINANCIAL IMPACT

There are no known negative financial impacts associated with these amendments. The amendments will allow the City to continue to make improvements to the sports complex as necessary.

V. STAFF ANALYSIS & RECOMMENDATION

See attached Planning Board report.

PLANNING BOARD REPORT #15-02

RECREATION-OPEN SPACE ZONE

TEXT AMENDMENT

CITY OF PORTLAND, APPLICANT

Submitted to:

Portland City Council
Portland, ME

March 18, 2002

I. INTRODUCTION

The Planning Board is forwarding a recommendation to the City Council to approve several text amendments to the Recreation Open Space (R-OS) zone. The applicant is the City of Portland Public Assembly Facilities Division.

II. BACKGROUND

The proposed text amendments relate to the R-OS zone. These amendments are proposed to differentiate the City's sports complex on Park Avenue (Hadlock Field, Exposition Building, Portland Ice Arena, Fitzpatrick Stadium) from other parks and open spaces in the city. The amendments would allow more flexibility for impervious surface coverage and floor area ratios for a sports complex use in the R-OS.

The text amendments are needed since the sports complex has reached the maximum 25% impervious standard of the R-OS Zone. Improvements to the facility have occurred in small incremental steps over the years to the point that no further improvements such as walkways or building additions may be built unless the impervious standard is adjusted.

~~Most public park spaces in the City are zoned R-OS. The amendment has been~~ drafted very narrowly. It will affect only the sports complex. We did not want to introduce the possibility of a concentrated form of development such as the sports complex to other more pastoral green spaces in the city.

Back in January 1997, the Planning Board reviewed virtually the same zoning amendments. The Board held a public hearing on the text amendments and made a positive recommendation to the City Council. It was not forwarded to the Council because a major grandstand renovation for Fitzpatrick Stadium did not go forward. The impetus for the renovation was the opportunity to lure a soccer team that decided to locate elsewhere. Since the Board's R-OS recommendation was five years old, the amendments were re-reviewed and a fresh recommendation sent to the City Council.

III. PROPOSED AMENDMENTS

The proposed amendments include a new term (sports complex), adding a policy statement to the R-OS purpose section, revising the maximum lot coverage and maximum floor area for sports complexes.

Sports Complex Definition

A new term, sports complex, is proposed in the definition section of the ordinance (14-47). The definition is shown below:

Sports Complex: One or more facilities located on the same parcel of land where athletic events are held and with a combined seating capacity of at least six thousand (6,000 seats).

The term sports complex would also be added as a permitted use in the R-OS text (Sec. 14-154). While ball fields and athletic fields are already permitted in this zone, a sports complex definition differentiates it from other park spaces. Aside from the Hadlock Field and Fitzpatrick Stadium, there are no other existing or anticipated park sites in the R-OS zone with a 6,000 seating capacity. The next largest facility, Presumpscot Park, has a seating capacity of half that number.

Recreation Open Space Zone Policy Statement

A fourth policy statement is being added to the R-OS purpose section recognizing the role of the sports complex as a large regional sports and athletic facility.

The policy statement of the R-OS zone is shown below.

- 1) To preserve and protect open space as a limited and valuable open space;
- 2) ~~To permit the reasonable use of open space, while simultaneously~~ preserving and protecting its inherent open space characteristics to assure its continued availability for public use as scenic, recreation, and conservation or natural resource area, and for the containment and structuring of urban development; and
- 3) To coordinate with and carry out federal, state, regional, and city recreation and open space plans.
- 4) To provide a suitable location for large-scale regional sports and athletic facilities.

Maximum Impervious Coverage

Currently the R-OS zone limits lot coverage of buildings, structures and other impervious improvements (parking lots, sidewalks) to no more than 25% of lot area (sec. 14-157(6)). The one current exception is sewage treatment facilities (by the Eastern Prom) which have no limit on coverage. When Hadlock Field was originally constructed, the sports complex had an impervious lot coverage of just under 25%. With various incremental improvements made to the sports complex over the years, the facility has reached the 25% standard.

The Planning Board is recommending that the impervious surface standard for the sports complex be increased to 75%. The Board viewed a 75% standard as

appropriate to address any future improvements while being a compromise between the existing 25% standard and the 100% surface coverage for sewage treatment facilities:

The amendment is shown below:

Sec 14-157(b) maximum coverage of lot by buildings, structures and other impervious site improvements such as paved sidewalks, drives and parking lots:

- a. Sewage treatment facilities: No limit on maximum coverage.
- b. Sports complexes: Seventy-five (75) percent of lot area.
- c. All other uses: Twenty-five percent of lot area.

Maximum Floor Area Ratio

An amendment is proposed to the maximum floor area ratio in the R-OS zone (sec. 14-157(7)). Presently the standard requires that floor area ratios may not exceed 0.2. Floor area ratio is defined as the total floor area of buildings in proportion to the total land area of the site. Hadlock Field Stadium, Expo and Portland Ice Arena have enclosed floor area.

The proposed amendment is shown below:

Sec. 14-157(7) maximum floor area ratio: ~~Two-tenths (0.2)~~ Five-tenths (0.5).

IV. LAND USE POLICY

The purpose of these amendments is to clearly differentiate the sports complex on Park Avenue which consists of Hadlock Field, Exposition Building, Portland Ice Arena and Fitzpatrick Stadium from other park spaces in the city. The scale, size and number of facilities and buildings offered at the sports complex contrasts with the remaining 100 plus spaces of the park system which are primarily open spaces and smaller athletic facilities in the city. The concentration of such facilities in one location provides for a sharing of resources and conserves existing open spaces so that redundant infrastructure such as utilities, parking and blacktop need not be introduced to other park spaces. Located near the downtown and I-295, the present sports complex site is a logical site for major sports facilities in the city.

While the R-OS zone is appropriate for the city park system, the text did not anticipate the success of the sports complex. With the construction of the Portland Ice Arena (1984), Hadlock Field (1993) and improvements to Fitzpatrick Stadium, text amendments are needed to assure that facility improvements can be accommodated in the future. According to the Portland Public Assembly

Facilities Division, over 750,000 people attended events at the sports complex last year.

V. PUBLIC PROCESS

The Planning Board held a workshop on this item on January 8, 2002 with a public hearing on February 12, 2002. Four hundred fifty-six notices were sent to area property owners (in the vicinity of the sports complex) for the workshop as well as the public hearing. A newspaper advertisement appeared in the January 28, 2002 edition of the Portland Press Herald for the public hearing. The applicant held a neighborhood meeting concerning this proposal on January 31, 2002.

The proposed amendments were reviewed with the Friends of the Parks Commission. The Commission voted to endorse the amendments at their January 22, 2002 meeting.

VI. PLANNING BOARD RECOMMENDATION

On February 12, 2002, the Planning Board held a public hearing and voted 3-0 (Caron, Cole, Krichels absent) on the following motion:

The proposed R-OS zone text amendments are consistent with the policies of the Comprehensive Plan of the City of Portland. The Planning Board therefore recommends to the City Council approval of the proposed zoning amendments.

The advertisement for the public hearing offered two alternatives for maximum impervious coverage, 50% or no limit on maximum coverage. The Board decided to strike a compromise between the two figures and is recommending 75%. The Board felt there should be an appropriate upper limit for maximum impervious coverage but that 50% wasn't enough and 100% may be too high. The Board therefore is recommending a 75% standard.

Attachments

- A. Zoning Amendments
- B. Sports Complex Map
- C. Zone Change Application

KAREN A. GERAGHTY (MAYOR) (2)
PETER E. O'DONNELL (1)
NATHAN H. SMITH (3)
CHERYL A. LEEMAN (4)
JAY M. HIBBARD (5)

CITY OF PORTLAND
IN THE CITY COUNCIL

ATTACHMENT A-1
JAMES F. CLOUTIER (A/L)
PHILIP J. DAWSON (A/L)
JILL C. DUSON (A/L)
NICHOLAS M. MAVODONES (A/L)

AMENDMENT TO PORTLAND CITY CODE
§§14-47, 14-153, 14-154, 14-157 (ZONING ORDINANCE)
RE: SPORTS COMPLEXES IN THE R-OS ZONE

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF PORTLAND,
MAINE IN CITY COUNCIL ASSEMBLED AS FOLLOWS:

1. That Section 14-47 of the Portland City Code is hereby
amended to add a new definition to read as follows:

Sec. 14-47. Definitions.

The following words shall be defined as set forth
below for use in this article. Definitions set forth in the
building code of the city shall apply to words not herein
defined:

Sports complex: One or more facilities located on the
same parcel of land where athletic events are held and with
a combined seating capacity of at least six thousand
(6,000) seats.

2. That Section 14-153(a) is hereby amended to read as
follows:

Sec. 14-153. Purpose.

(a) The purpose of this division is:

- (1) To preserve and protect open space as a limited
and valuable resource;
- (2) To permit the reasonable use of open space, while
simultaneously preserving and protecting its
inherent open space characteristics to assure its
continued availability for public use as scenic,
recreation, and conservation or natural resource
area, and for the containment and structuring of
urban development; and
- (3) To coordinate with and carry out federal, state,
regional, and city recreation and open space
plans; and

(4) To provide a suitable location for large-scale regional sports and athletic facilities.

3. That Section 14-154(m) and 14-154(n) is hereby amended to read as follows:

(m) Sports complexes.

(n) Accessory uses, including structures or buildings of less than two thousand five hundred (2,500) square feet of floor area.

4. That Section 14-157(f) and (g) is hereby amended to read as follows:

(f) Maximum coverage of lot by buildings, structures and other impervious site improvements such as paved sidewalks, drives and parking lots: ~~Twenty-five (25) percent of lot area, except that sewage treatment facilities are not required to meet this standard.~~

1. Sewage treatment facilities: No limit on maximum coverage.

2. Sports complexes: Seventy-five (75) percent of lot area.

3. All other uses: Twenty-five (25) percent of lot area.

(g) Maximum floor area ratio: ~~Two-tenths (0.2)~~ Five-tenths (0.5).



APPLICATION FOR ZONING AMENDMENT
City of Portland, Maine
Department of Planning and Urban Development
Portland Planning Board

Applicant Information:

PUBLIC ASSEMBLY FACILITIES DIVISION
Name FRANK LATONAG, DIVISION DIRECTOR
239 PARK AVE
Address

PORTLAND ME 04102

874 8000 Phone 874 8130 Fax

2. Subject Property:

SPORTS COMPLEX AT
Address

239 PARK AVE

PORTLAND ME
(INCLUDES ICE AREA, EXPO,
Assessor's Reference (Chart-Block-Lot)

HADLOCK FIELD, FITZPATRICK
STADIUM AND ADJOINING
CITY OWNED PARKING LOTS,

3. Property Owner: Applicant Other

CITY OF PORTLAND, MAINE
Name

Address

Phone

Fax

4. Right, Title, or Interest: Please identify the status of the applicant's right, title, or interest in the subject property:

THIS IS CITY OF PORTLAND PROPERTY

Provide documentary evidence, attached to this application, of applicant's right, title, or interest in the subject property. (For example, a deed, option or contract to purchase or lease the subject property.)

5. Vicinity Map: Attach a map showing the subject parcel and abutting parcels, labeled as to ownership and/or current use. (Applicant may utilize the City Zoning Map or Parcel Map as a source.)

Existing Use:

C-2

Describe the existing use of the subject property: PUBLIC ASSEMBLY FACILITIES USED
FOR A VARIETY OF PURPOSES

Current Zoning Designation(s): RDS

Proposed Use of Property: Please describe the proposed use of the subject property. If construction or development is proposed, please describe any changes to the physical condition of the property:

(See above)

9. Sketch Plan: On a separate sheet please provide a sketch plan of the property, showing existing and proposed improvements, including such features as buildings, parking, driveways, walkways, landscape and property boundaries. This may be a professionally drawn plan, or a carefully drawn plan, to scale, by the applicant. (Scale to suit, range from 1"=10' to 1"=100')

10. Proposed Zoning: Please check all that apply:

A. Zoning Map Amendment, from _____ to _____

B. Zoning Text Amendment to Section 14- 157 (6)

For Zoning Text Amendment, attach on a separate sheet the exact language being proposed, including existing relevant text, in which language to be deleted is depicted as crossed out (example), and language to be added is depicted with underline (example).

C. Conditional or Contract Zone

A conditional or contract rezoning may be requested by an applicant in cases where limitations, conditions, or special assurances related to the physical development and operation of the property are needed to ensure that the rezoning and subsequent development are consistent with the comprehensive plan and compatible with the surrounding neighborhood. (Please refer to Division 1.5, Sections 14-60 to 62)

Application Fee: A fee for this application for a zoning amendment must be submitted, by check payable to the City of Portland in accordance with Section 14-54 of the Municipal Code (see below). The applicant also agrees to pay all costs of publication (or advertising) of the Public Hearing Notice as required for this application. Such amount will be billed to the applicant following the appearance of the advertisement.

C-3

	<u>1-25 Units</u>	<u>26-50 Units</u>	<u>51-75 Units</u>	<u>75 & Over</u>
Residential Zones	\$350.00	\$400.00	\$450.00	\$500.00
Nonresidential Zones	\$350.00	\$400.00	\$450.00	\$500.00
	0-15,000 sq. ft. or 0-5 acres (whichever is less)	15,000-30,000 sq. ft. or 6-10 acres (whichever is less)	30,000-45,000 sq. ft. or 10-15 acres (whichever is less)	45,000-60,000 sq. ft. or 15-20 acres (whichever is less)

- Legal Advertisements (one for workshop and one for public hearing) percent of total bill
- Notices (one for workshop and one for public hearing) 40 cents each
- Text Amendments \$300.00

Amount of Fee

12. Signature: The above information is true and accurate to the best of my knowledge.

12/21/01
Date of Filing

Frank LaTone
Signature of Applicant

FOR CITY OF PORTLAND'S
PUBLIC ASSEMBLY FACILITIES
DIVISION

Further Information:

Please contact the Planning Office for further information regarding the rezoning process. Applicants are encouraged to make an appointment to discuss their rezoning requests before filing the application.

Applicants are encouraged to include a letter or narrative to accompany the rezoning application which can provide additional background or context information, and describe the proposed rezoning and reasons for the request in a manner that best suits the situation.

In the event of withdrawal of the zoning amendment application by the applicant in writing prior to the submission of the advertisement copy to the newspaper to announce the public hearing, a refund of two-thirds of the amount of the zone change fee will be made to the applicant by the City of Portland.

Portland Planning Board
Portland, Maine

Effective: July 6, 1998

Frank P. LaTorre, C.F.E.
Division Director

Arthur H. Stephenson III
Assistant Director



Joseph E. Gray, Jr.
City Manager

Anita R. Lachance
Assistant City Manager

City of Portland
Public Assembly Facilities Division

Merrill Auditorium at City Hall Portland Exposition Building Hadlock Field Fitzpatrick Stadium

Need for Zoning Amendments to Section 14-157(6)

The Portland Sports Complex combines Hadlock Field, the Portland Exposition Building, Fitzpatrick Stadium, the Portland Ice Arena, the "Horse Barn" building and adjoining City owned property and parking lots.

We are presently at the 25% area that is all that can be covered with impervious surface according to City of Portland ordinance 14-157. We need to be exempt from this ordinance because it has been the will of the Portland City Council to improve these facilities to make them safer and more user-friendly to the public. This includes more paying of pathways to eliminate dirt/mud walkways that can be a danger during inclement weather. We are making ADA improvements to Fitzpatrick Stadium to increase handicap accessibility to prime viewing areas. At some point, we are planning to add a locker room and rest rooms to Fitzpatrick to handle the increased community usage that has resulted from the installation of sports turf on the playing field.

Presently over 750,000 people enjoy a variety of events at the Portland Sports Complex. All the plans to improve the facility are in line with the City's mission to make this a thriving "community center." Thus we should be exempt from ordinance 14-157 so that we can carefully and judiciously improve usage, safety and accessibility.

Frank LaTorre

From: Frank LaTorre
To: Rick Knowland
Date: Mon, Feb 4, 2002 11:12 AM
Subject: Summary of the Neighborhood Meeting

The City of Portland held a Neighborhood Meeting at the Expo at 7PM Thursday January 31, 2002. Proper notification of the meeting and its purpose, to get input on proposed zoning amendments for the Sports Complex on Park Avenue, were detailed in the mailing sent to over 425 households. No one attended the meeting.

CC: ANITA R. LACHANCE

PLANNING BOARD REPORT #11-02

RECREATION-OPEN SPACE ZONE

TEXT AMENDMENT

CITY OF PORTLAND, APPLICANT

Submitted to:

Portland Planning Board
Portland, ME

February 12, 2002

I. INTRODUCTION

A public hearing has been scheduled to consider a proposal by the City of Portland to amend the text of the Recreation Open space (R-OS) zone. A copy of the proposed zoning text is shown as Attachment A. The applicant is the City of Portland Public Assembly Facilities Division.

Public notice of the public hearing consisted of newspaper advertisements in the *Portland Press Herald* and 456 notices sent to area property owners in the vicinity of the sports complex. The applicant held a neighborhood meeting concerning this proposal on January 31, 2002.

II. BACKGROUND

The proposed text amendments relate to the R-OS zone. These amendments are proposed to differentiate the City's sports complex on Park Avenue (Hadlock Field, Exposition Building, Portland Ice Arena, Fitzpatrick Stadium) from other parks and open spaces in the city. The amendments would allow more flexibility for impervious surface coverage and floor area ratios for a sports complex use in the R-OS.

The text amendments are needed since the sports complex has reached the maximum 25% impervious standard of the R-OS Zone. Improvements to the facility have occurred in small incremental steps over the years to the point that no further improvements such as building additions or walkways can be built unless the impervious standard is adjusted.

Most public park spaces in the City are zoned R-OS. The amendment has been drafted very narrowly. It will affect only the sports complex. We did not want to introduce the possibility of a concentrated form of development such as the sports complex to other more pastoral green spaces in the city.

Back in January 1997, the Board reviewed virtually the same zoning amendments. The Planning Board held a public hearing on the text amendments and made a positive recommendation to the City Council. It was not forwarded to the Council because a major grandstand renovation for Fitzpatrick Stadium did not go forward. The impetus for the renovation was the opportunity to lure a soccer team that decided to locate elsewhere. Since the Board's R-OS recommendation is now five years old, the amendments should be re-reviewed and a fresh recommendation sent to the City Council.

The proposed amendments were reviewed with the Friends of the Parks Commission. The Commission voted to endorse the amendments at their January 22nd meeting.

III. PROPOSED AMENDMENTS

The proposed amendments include a new term (sports complex), adding a policy statement to the R-OS purpose section, revising the maximum lot coverage and maximum floor area for sports complexes.

Sports Complex Definition

A new term, sports complex, is proposed in the definition section of the ordinance (14-47). The definition is shown below:

Sports Complex: One or more facilities located on the same parcel of land where athletic events are held and with a combined seating capacity of at least six thousand (6,000 seats).

The term sports complex would also be added as a permitted use in the R-OS text (Sec. 14-154). While ball fields and athletic fields are already permitted in this zone, a sports complex definition differentiates it from other park spaces. Aside from the Hadlock Field and Fitzpatrick Stadium, there are no other existing or anticipated park sites in the R-OS zone with a 6,000 seating capacity. The next largest facility, Presumpscot Park, has a seating capacity of half that number.

Recreation Open Space Zone Policy Statement

A fourth policy statement is being added to the R-OS purpose section recognizing the role of the sports complex as a large regional sports and athletic facility.

The policy statement of the R-OS zone is shown below.

- 1) To preserve and protect open space as a limited and valuable open space;
- 2) To permit the reasonable use of open space, while simultaneously preserving and protecting its inherent open space characteristics to assure its continued availability for public use as scenic, recreation, and conservation or natural resource area, and for the containment and structuring of urban development; and
- 3) To coordinate with and carry out federal, state, regional, and city recreation and open space plans.
- 4) To provide a suitable location for large-scale regional sports and athletic facilities.

Maximum Impervious Coverage

Currently the R-OS zone limits lot coverage of buildings, structures and other impervious improvements (parking lots, sidewalks) to no more than 25% of lot area (sec. 14-157(6)). The one current exception is sewage treatment facilities (by the Eastern Prom) which have no limit on coverage. When Hadlock Field was originally constructed, the sports complex had an impervious lot coverage of just under 25%. With various incremental improvements made to the sports complex over the years, the facility has reached the 25% standard.

We have drafted two alternative amendments. The 50% impervious standard was discussed at the workshop. The second alternative amendment would have no limit on maximum coverage. Staff is recommending the no limit on maximum coverage alternative since it provides more flexibility. The amendment is shown below:

Sec 14-157(b) maximum coverage of lot by buildings, structures and other impervious site improvements such as paved sidewalks, drives and parking lots:

- a. Sewage treatment facilities: No limit on maximum coverage
- b. Sports complexes: Fifty (50) percent of lot area or no limit on maximum coverage.
- c. All other uses: Twenty-five percent of lot area.

Maximum Floor Area Ratio

An amendment is proposed to the maximum floor area ratio in the R-OS zone (sec. 14-157(7)). Presently the standard requires that floor area ratios may not exceed 0.2. Floor area ratio is defined as the total floor area of buildings in proportion to the total land area of the site. Hadlock Field Stadium, Expo and Portland Ice Arena have enclosed floor area.

The proposed amendment is shown below:

Sec. 14-157(7) maximum floor area ratio: ~~Two-tenths (0.2)~~ Five-tenths (0.5).

IV. LAND USE POLICY

The purpose of these amendments is to clearly differentiate the sports complex on Park Avenue which consists of Hadlock Field, Exposition Building, Portland Ice Arena and Fitzpatrick Stadium from other park spaces in the city. The scale, size and number of facilities and buildings offered at the sports complex contrasts with the remaining 100 plus spaces of the park system which are primarily open spaces

and smaller athletic facilities in the city. The concentration of such facilities in one location provides for a sharing of resources and conserves existing open spaces so that redundant infrastructure such as utilities, parking and blacktop need not be introduced to other park spaces. Located near the downtown and I-295, the present sports complex site is a logical site for major sports facilities in the city.

While the R-OS zone is appropriate for the city park system, the text did not anticipate the success of the sports complex. With the construction of the Portland Ice Arena (1984), Hadlock Field (1993) and improvements to Fitzpatrick Stadium, text amendments are needed to assure that facility improvements can be accommodated in the future. According to the Portland Public Assembly Facilities Division, over 750,000 people attended events at the sports complex last year.

V. MOTIONS FOR THE BOARD TO CONSIDER

On the basis of plans and materials submitted by the applicant, the policies of the comprehensive plan and the information provided in Planning Board Report #11-02 and/or other findings as follows:

The Board finds that:

1. The proposed zone change (is or is not) consistent with policies of the Comprehensive Plan.

The Planning Board therefore (recommends or does not recommend) to the City Council approval of the proposed zoning amendments.

Sec. 14-157(f) maximum coverage of lot alternative for sports complex (choose a or b)

- a. Fifty (50) percent of lot area
- or
- b. No limit on maximum coverage

Attachments

- A. Zoning Amendments
- B. Sports Complex Map
- C. Zone Change Application

City of Portland, Maine

IN THE CITY COUNCIL

AMENDMENT TO PORTLAND CITY CODE
 §§14-47, 14-153, 14-154, 14-157 (ZONING ORDINANCE)
 RE: SPORTS COMPLEXES IN THE R-OS ZONE

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF PORTLAND,
 MAINE IN CITY COUNCIL ASSEMBLED AS FOLLOWS:

1. That Section 14-47 of the Portland City Code is hereby amended to add a new definition to read as follows:

Sec. 14-47. Definitions.

The following words shall be defined as set forth below for use in this article. Definitions set forth in the building code of the city shall apply to words not herein defined:

Sports complex: One or more facilities located on the same parcel of land where athletic events are held and with a combined seating capacity of at least six thousand (6,000) seats.

2. That Section 14-153(a) is hereby amended to read as follows:

Sec. 14-153. Purpose.

(a) The purpose of this division is:

- (1) To preserve and protect open space as a limited and valuable resource;
- (2) To permit the reasonable use of open space, while simultaneously preserving and protecting its inherent open space characteristics to assure its continued availability for public use as scenic, recreation, and conservation or natural resource area, and for the containment and structuring of urban development; and
- (3) To coordinate with and carry out federal, state, regional, and city recreation and open space plans; and
- (4) To provide a suitable location for large-scale regional sports and athletic facilities.

3. That Section 14-154(m) and 14-154(n) is hereby amended to read as follows:

(m) Sports complexes.

(n) Accessory uses, including structures or buildings of less than two thousand five hundred (2,500) square feet of floor area.

4. That Section 14-157(f) and (g) is hereby amended to read as follows:

(f) Maximum coverage of lot by buildings, structures and other impervious site improvements such as paved sidewalks, drives and parking lots: ~~Twenty-five (25) percent of lot area, except that sewage treatment facilities are not required to meet this standard.~~

1. Sewage treatment facilities: No limit on maximum coverage.

2. Sports complexes: Fifty (50) percent of lot area. OR No limit on maximum coverage.

3. All other uses: Twenty-five (25) percent of lot area.

(g) Maximum floor area ratio: ~~Two tenths (0.2)~~ Five-tenths (0.5).



APPLICATION FOR ZONING AMENDMENT
City of Portland, Maine
Department of Planning and Urban Development
Portland Planning Board

1. Applicant Information:

PUBLIC ASSAULT FACILITIES DIVISION
Name FRANK LATORNE, DIVISION DIRECTOR
239 PARK AVE
Address
PORTLAND ME 04107
874 8800 874 8130
Phone Fax

2. Subject Property:

SPORTS COMPLEX AT
Address
239 PARK AVE
PORTLAND ME
(INCLUDES ICE AREA, EXPO,
Assessor's Reference (Chart-Block-Lot)
HADLOCK FIELD, FITZPATRICK
STADIUM AND ADJOINING
CITY OWNED PARKING LOTS,

3. Property Owner: Applicant Other

CITY OF PORTLAND, MAINE
Name

Address

Phone Fax

4. Right, Title, or Interest: Please identify the status of the applicant's right, title, or interest in the subject property:

THIS IS CITY OF PORTLAND PROPERTY

Provide documentary evidence, attached to this application, of applicant's right, title, or interest in the subject property. (For example, a deed, option or contract to purchase or lease the subject property.)

5. Vicinity Map: Attach a map showing the subject parcel and abutting parcels, labeled as to ownership and/or current use. (Applicant may utilize the City Zoning Map or Parcel Map as a source.)

6. Existing Use:

Describe the existing use of the subject property: PUBLIC ASSEMBLY FACILITIES USED
FOR A VARIETY OF PURPOSES

7. Current Zoning Designation(s): RDS

8. Proposed Use of Property: Please describe the proposed use of the subject property. If construction or development is proposed, please describe any changes to the physical condition of the property.

(See above)

9. Sketch Plan: On a separate sheet please provide a sketch plan of the property, showing existing and proposed improvements, including such features as buildings, parking, driveways, walkways, landscape and property boundaries. This may be a professionally drawn plan, or a carefully drawn plan, to scale, by the applicant. (Scale to suit, range from 1"=10' to 1"=100'.)

10. Proposed Zoning: Please check all that apply:

- A. Zoning Map Amendment, from _____ to _____
- B. Zoning Text Amendment to Section 14- 157 (6)

For Zoning Text Amendment, attach on a separate sheet the exact language being proposed, including existing relevant text, in which language to be deleted is depicted as crossed out (example), and language to be added is depicted with underline (example).

C. Conditional or Contract Zone

A conditional or contract rezoning may be requested by an applicant in cases where limitations, conditions, or special assurances related to the physical development and operation of the property are needed to ensure that the rezoning and subsequent development are consistent with the comprehensive plan and compatible with the surrounding neighborhood. (Please refer to Division 1.5, Sections 14-60 to 62)

11. Application Fee: A fee for this application for a zoning amendment must be submitted, by check payable to the City of Portland in accordance with Section 14-54 of the Municipal Code (see below). The applicant also agrees to pay all costs of publication (or advertising) of the Public Hearing Notice as required for this application. Such amount will be billed to the applicant following the appearance of the advertisement.

	<u>1-25 Units</u>	<u>26-50 Units</u>	<u>51-75 Units</u>	<u>75 & Over</u>
Residential Zones	\$350.00	\$400.00	\$450.00	\$500.00
Nonresidential Zones	\$350.00	\$400.00	\$450.00	\$500.00
	0-15,000 sq. ft. or 0-5 acres (whichever is less)	15,000-30,000 sq. ft. or 6-10 acres (whichever is less)	30,000-45,000 sq. ft. or 10-15 acres (whichever is less)	45,000-60,000 sq. ft. or 15-20 acres (whichever is less)

- Legal Advertisements (one for workshop and one for public hearing) percent of total bill
- Notices (one for workshop and one for public hearing) 40 cents each
- Text Amendments \$300.00

_____ Amount of Fee

12. Signature: The above information is true and accurate to the best of my knowledge.

12/21/01
Date of Filing

Paul LaTone FOR CITY OF PORTLAND'S
Signature of Applicant PUBLIC ASSEMBLY FACILITIES
DIVISION

Further Information:

Please contact the Planning Office for further information regarding the rezoning process. Applicants are encouraged to make an appointment to discuss their rezoning requests before filing the application.

Applicants are encouraged to include a letter or narrative to accompany the rezoning application which can provide additional background or context information, and describe the proposed rezoning and reasons for the request in a manner that best suits the situation.

In the event of withdrawal of the zoning amendment application by the applicant in writing prior to the submission of the advertisement copy to the newspaper to announce the public hearing, a refund of two-thirds of the amount of the zone change fee will be made to the applicant by the City of Portland.

Portland Planning Board
Portland, Maine

Effective: July 6, 1998

Frank P. LaTorre, C.F.E.
Division Director

Arthur H. Stephenson III
Assistant Director



C-4
Joseph E. Gray, Jr.
City Manager

Anita R. Lachance
Assistant City Manager

**City of Portland
Public Assembly Facilities Division**

Merrill Auditorium at City Hall Portland Exposition Building Hadlock Field Fitzpatrick Stadium

Need for Zoning Amendments to Section 14-157(6)

The Portland Sports Complex combines Hadlock Field, the Portland Exposition Building, Fitzpatrick Stadium, the Portland Ice Arena, the "Horse Barn" building and adjoining City owned property and parking lots.

We are presently at the 25% area that is all that can be covered with impervious surface according to City of Portland ordinance 14-157. We need to be exempt from this ordinance because it has been the will of the Portland City Council to improve these facilities to make them safer and more user-friendly to the public. This includes more paving of pathways to eliminate dirt/mud walkways that can be a danger during inclement weather. We are making ADA improvements to Fitzpatrick Stadium to increase handicap accessibility to prime viewing areas. At some point, we are planning to add a locker room and rest rooms to Fitzpatrick to handle the increased community usage that has resulted from the installation of sports turf on the playing field.

Presently over 750,000 people enjoy a variety of events at the Portland Sports Complex. All the plans to improve the facility are in line with the City's mission to make this a thriving "community center." Thus we should be exempt from ordinance 14-157 so that we can carefully and judiciously improve usage, safety and accessibility.

Frank LaTorre

From: Frank LaTorre
To: Rick Knowland
Date: Mon, Feb 4, 2002 11:12 AM
Subject: Summary of the Neighborhood Meeting

The City of Portland held a Neighborhood Meeting at the Expo at 7PM Thursday January 31, 2002. Proper notification of the meeting and its purpose, to get input on proposed zoning amendments for the Sports Complex on Park Avenue, were detailed in the mailing sent to over 425 households. No one attended the meeting.

CC: ANITA R. LACHANCE

Department of Planning & Development
Lee D. Urban, Director



CITY OF PORTLAND

Division Directors
Mark B. Adelson
Housing & Neighborhood Services

Alexander Q. Jaegerman, AICP
Planning

John N. Lufkin
Economic Development

January 23, 2003

Charles Eshbach
Portland Sea Dogs
P.O. Box 636
Portland ME 04104

RE: Proposed Signage at Hadlock Field

Dear Charlie:

We have reviewed your sign application submitted for various signs at Hadlock Field. Based on our review, the following additional information will need to be submitted. Please note that these items are referenced on the second page of the signage application previously distributed to you.

1. A letter of permission from the property owner. I will contact the City Manager's office concerning drafting such a letter.
2. Submit a sketch plan of the property showing the location of all the proposed signs, the Citgo sign, the Coke sign and the 17 sign panels proposed along the right field wall. The sketch plan could be a modified version of a previously submitted Hadlock Field site plan appropriately modified showing the location of all the proposed signs. The plan should reference the Sign Solutions folder depicting existing and proposed signage along the outfield wall.

Once we receive the above information, Marge Schmuckal, Zoning Administrator, can then process your application accordingly. Please note that a separate permit will be required for signage involving electricity.

Should you have any questions concerning this letter, please call me.

Sincerely,

Richard Knowland
Senior Planner

cc: Joseph E. Gray, Jr., City Manager
Lee Urban, Planning and Development Department Director
Alexander Jaegerman, Planning Division Director
Frank LaTorre, Public Assembly Director
Sarah Hopkins, Development Review Program Manager
Marge Schmuckal, Zoning Administrator



RELOCATED BLEACHERS

KING HULL SCHOOL

MODULAR CLASSROOM

PRESS BOX

ICE ARENA

ICE ARENA PARKING LOT

Innes Lane 293

Park Avenue

Deerfoot Avenue

RIZZO STADIUM

HADLOCK FIELD

EXPO

NEW GRANDSTANDS

NEW PARKING LOT

St. John Street

MITCHELL & ASSOCIATES
LANDSCAPE ARCHITECTS

April 25, 1995

Mr. Richard Knowland, Senior Planner
City of Portland
Planning Department
389 Congress Street
Portland, Maine 04101

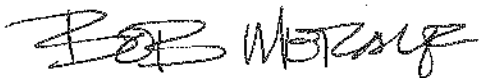
RE: HADLOCK STADIUM

Dear Rick:

On behalf of the Portland Sea Dogs, we are requesting a de minimis change to the plan recently approved for the new bleachers at Hadlock Stadium. In order to facilitate movement of equipment on and off the field the Sea Dogs propose to pave a twelve foot wide bituminous drive from the paved walkway around the bleacher to the existing gates in left field. The additional impervious area is equal to 750 square feet which still keeps the total impervious cover ratio for the stadium below 25 percent. In addition we are requesting a change in pavement configuration between the existing stadium and the new bleachers. This change is based upon input from the city engineering department responsible for the remaining site improvements around the new bleachers.

The proposed changes are reflected on the revised site plan along with as-built conditions to date. Should you have any questions or comments, please do not hesitate to contact our office.

Sincerely,
Mitchell & Associates



Robert B. Metcalf

cc: Mike Fagerson

Enclosure

MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

February 6, 1995

Mr. Richard Knowland, Senior Planner
Planning & Urban Development
City of Portland
389 Congress Street
Portland, Maine 04101

RE: HADLOCK FIELD BLEACHER EXPANSION

Dear Rick:

This submission is in regards to documentation requested as part of the approval process for the proposed bleacher expansion at Hadlock Field. Attached for your review are the following exhibits:

Sheet 1 - Existing Conditions (including original approval for paved picnic area)

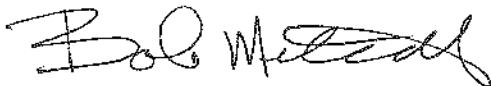
Sheet 2 - Proposed Grading Plan

Sheet 3 - Proposed Layout Plan

Per your request we have calculated the impervious surface ratio for this proposal. The original approval allowed for 24.87% coverage that included the paved picnic area that was to be constructed where the bleachers are now proposed. The total impervious cover for the picnic area was 9,888 square feet. The new bleachers and paved circulation total 4,177 square feet, leaving a net reduction in paving of 5,711 square feet. The reduction in pavement keeps the total impervious area under 25 percent.

Should you have any questions concerning this documentation, please do not hesitate to contact our office.

Sincerely,
Mitchell & Associates



Robert B. Metcalf

Enclosure

cc: Sam Hofsus
Mike Fagerson

HADLOCK FIELD STADIUM
 AMENDED SITE PLAN

EXISTING CONDITIONS

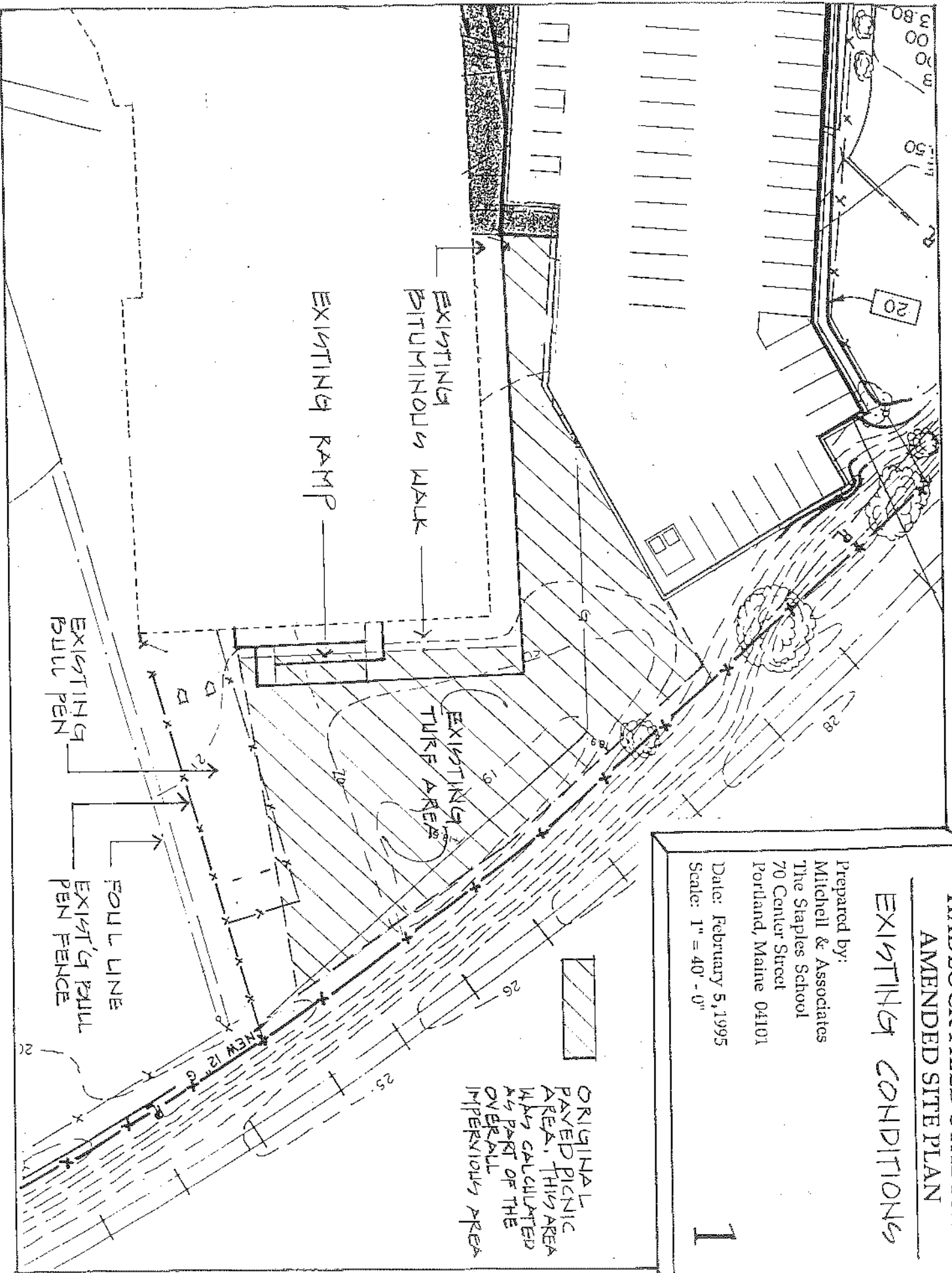
Prepared by:

Mitchell & Associates
 The Staples School
 70 Center Street
 Portland, Maine 04101

Date: February 5, 1995
 Scale: 1" = 40' - 0"

1

ORIGINAL
 PAVED PICNIC
 AREA, THIS AREA
 WAS CALCULATED
 AS PART OF THE
 OVERALL
 IMPROVEMENTS AREA



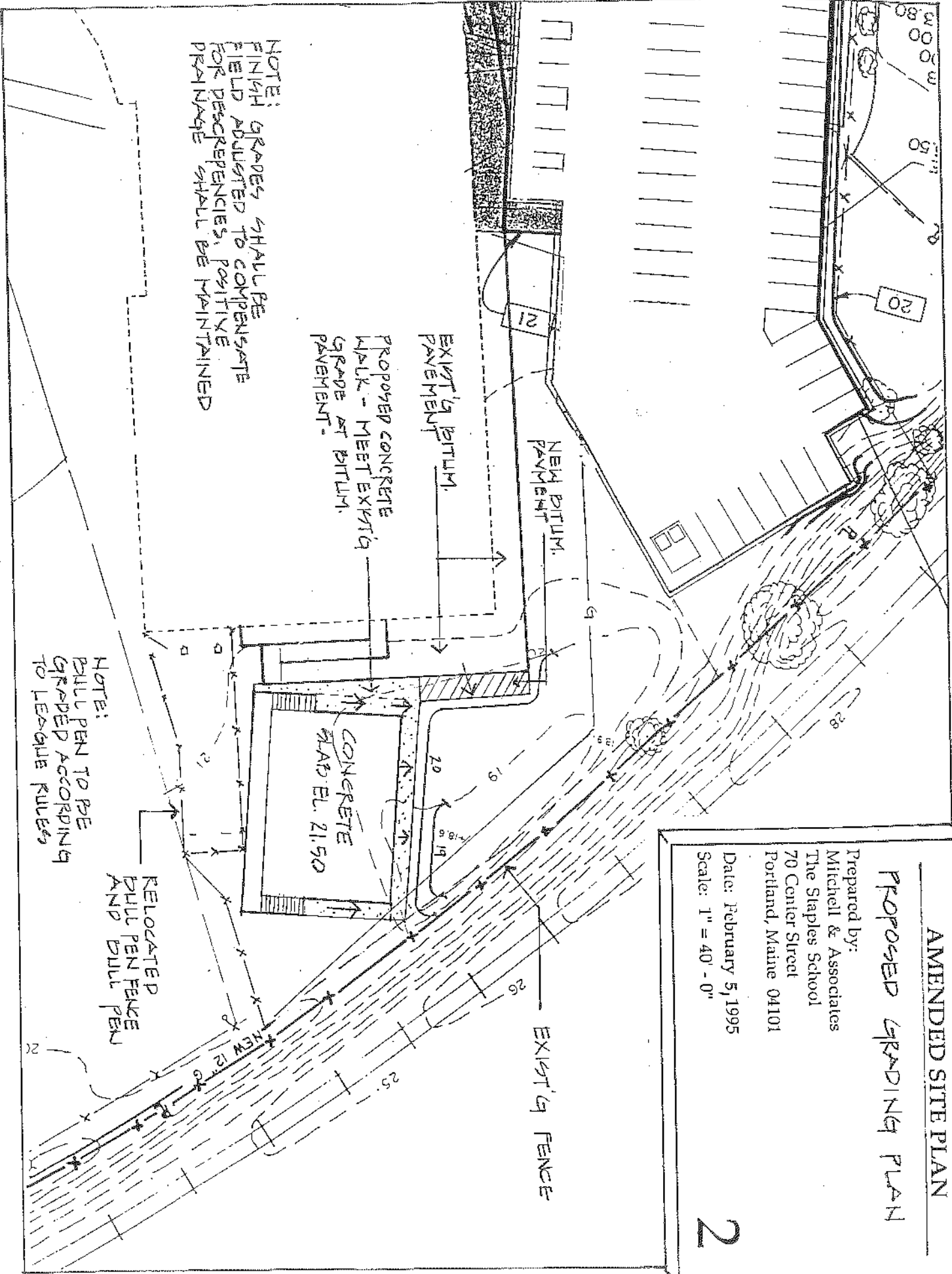
HADLOCK FIELD STADIUM
 AMENDED SITE PLAN

PROPOSED GRADING PLAN

Prepared by:
 Mitchell & Associates
 The Staples School
 70 Center Street
 Portland, Maine 04101

Date: February 5, 1995
 Scale: 1" = 40' - 0"

2



PROPOSED CONCRETE
 WALK - MEET EXIST'G
 GRADE AT BITUM.
 PAVEMENT.

NOTE: GRADES SHALL BE
 FINISH GRADES ADJUSTED TO COMPENSATE
 FOR DISCREPANCIES. POSITIVE
 DRAINAGE SHALL BE MAINTAINED

NOTE:
 BULL PEN TO BE
 GRADED ACCORDING
 TO LEASING RULES

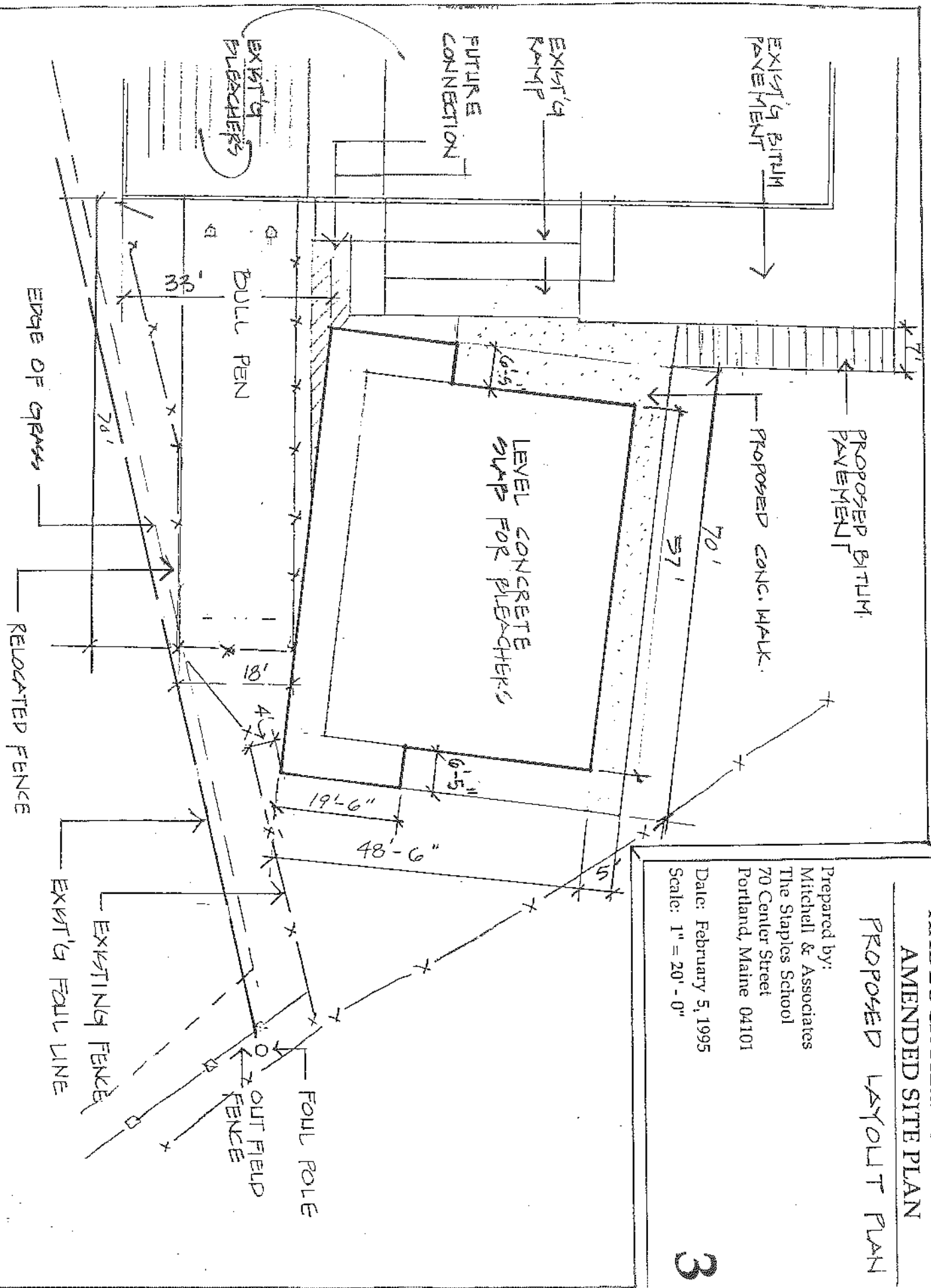
RELOCATED
 BULL PEN FENCE
 AND BULL PEN

HADLOCK FIELD STADIUM
 AMENDED SITE PLAN

PROPOSED LAYOUT PLAN

Prepared by:
 Mitchell & Associates
 The Staples School
 70 Center Street
 Portland, Maine 04101

Date: February 5, 1995
 Scale: 1" = 20' - 0"



MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

March 3, 1995

Mr. Richard Knowland, Senior Planner
Planning & Urban Development
City of Portland
389 Congress Street
Portland, Maine 04101

RE: HADLOCK FIELD BLEACHER EXPANSION

Dear Rick:

This correspondence is in regards to our conversation today concerning your Review Comments and Conditions of Approval for the proposed bleacher expansion at Hadlock Field. The enclosed five copies of the Amended Site Plan and Elevations have been revised to reflect the following:

1. The bituminous sidewalk along the rear of the proposed bleacher has been increased from 5 feet to 8 feet wide.
2. The 13 foot setback dimensions from the corner of the bleacher closets to the property line has been added.
3. A note has been added to the elevations indicating that the wire mesh on the proposed bleacher shall be black to match the existing grandstand fencing. A note has also been added stating that the proposed doors under the bleachers shall be the same green color as the metal panel siding.
4. The drain detail for the proposed dry well has been revised to add geotextile fabric in the bottom of the structure.

Should you have any questions concerning these revisions please do not hesitate to contact our office.

Sincerely,
Mitchell & Associates



Robert B. Metcalf

Enclosure

cc: Mike Fagerson

HADLOCK FIELD STADIUM
AMENDED SITE PLAN

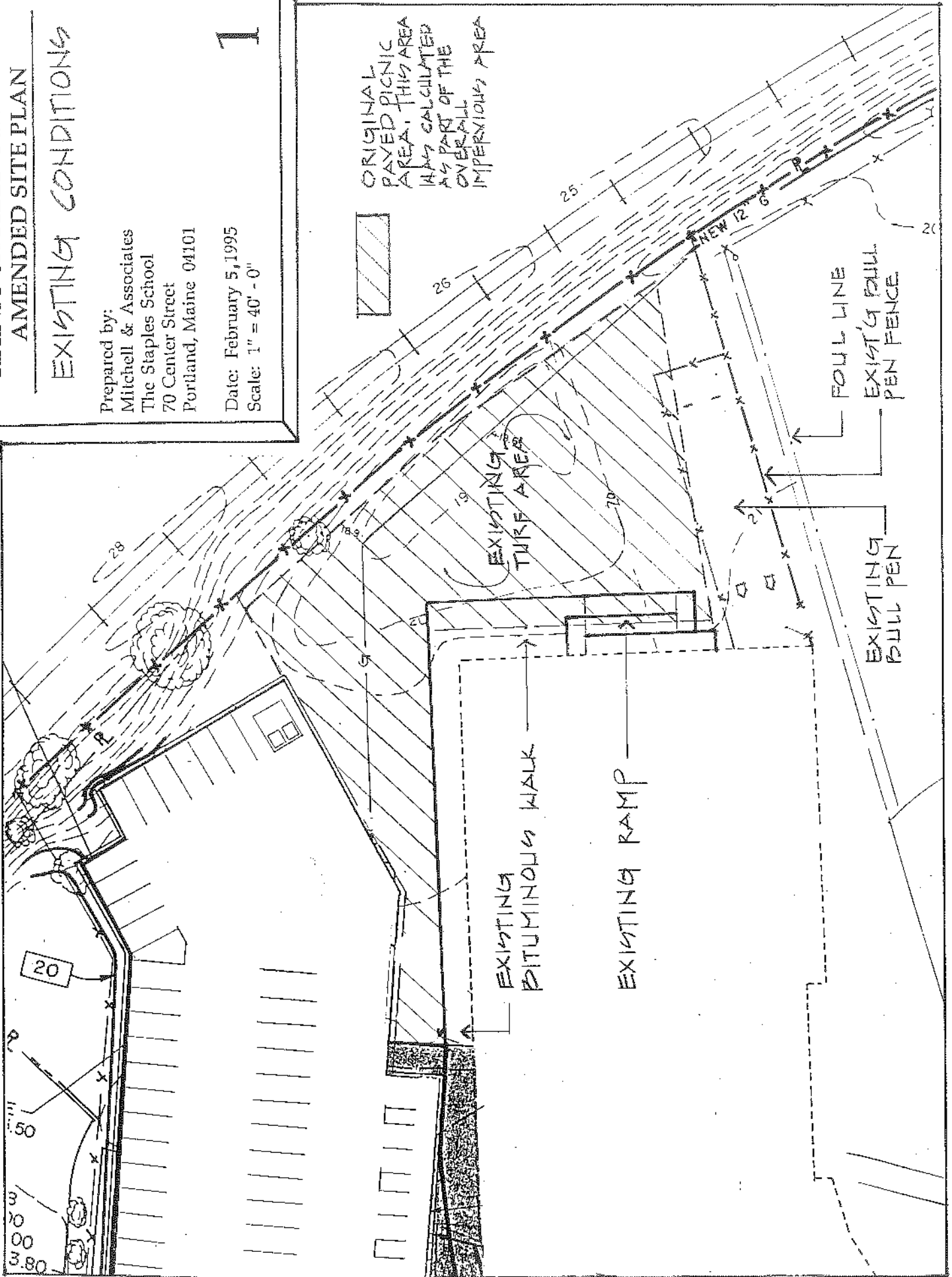
EXISTING CONDITIONS

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

1

ORIGINAL
PAVED PICNIC
AREA. THIS AREA
WAS CALCULATED
AS PART OF THE
OVERALL
IMPERVIOUS AREA



HADLOCK FIELD STADIUM
AMENDED SITE PLAN

PROPOSED GRADING PLAN

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

2

REVISIONS:

- A ADD GETBACK DIMENSION FEB. 7, 1995
- A PUNT REVISION FEB 13, 1995

EXIST'G FENCE

13' GETBACK

25

NEW 12' 6"

RELOCATED
BILL PEN FENCE
AND BULL PEN

NOTE:
BILL PEN TO BE
GRADED ACCORDING
TO LEAGUE RULES

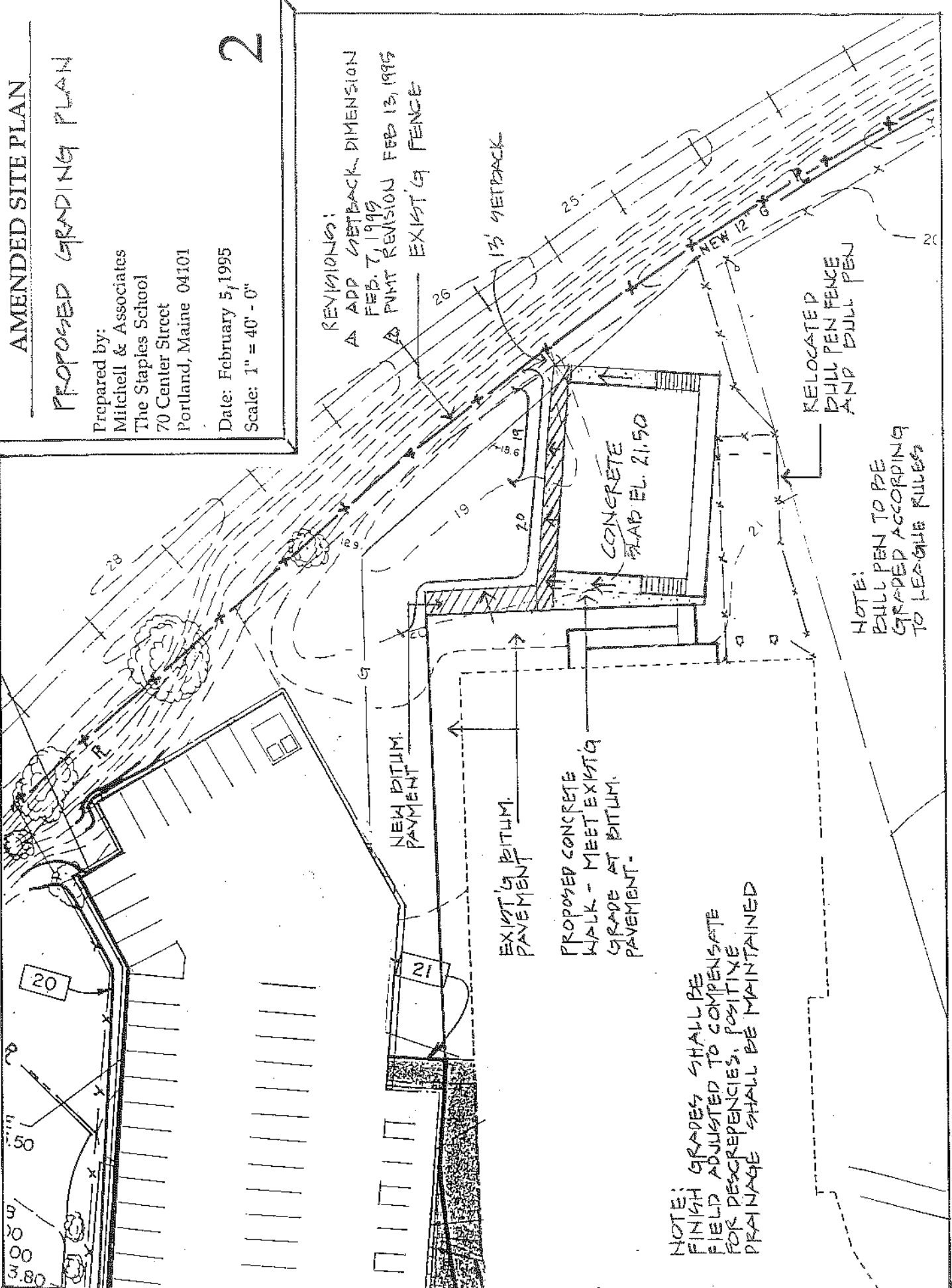
CONCRETE
SLAB EL. 21.50

NEW BITUM.
PAVEMENT

EXIST'G BITUM.
PAVEMENT

PROPOSED CONCRETE
WALK - MEET EXIST'G
GRADE AT BITUM.
PAVEMENT.

NOTE:
FINISH GRADES SHALL BE
FIELD ADJUSTED TO COMPENSATE
FOR DISCREPANCIES. POSITIVE
DRAINAGE SHALL BE MAINTAINED



HADLOCK FIELD STADIUM
AMENDED SITE PLAN

PROPOSED LAYOUT PLAN

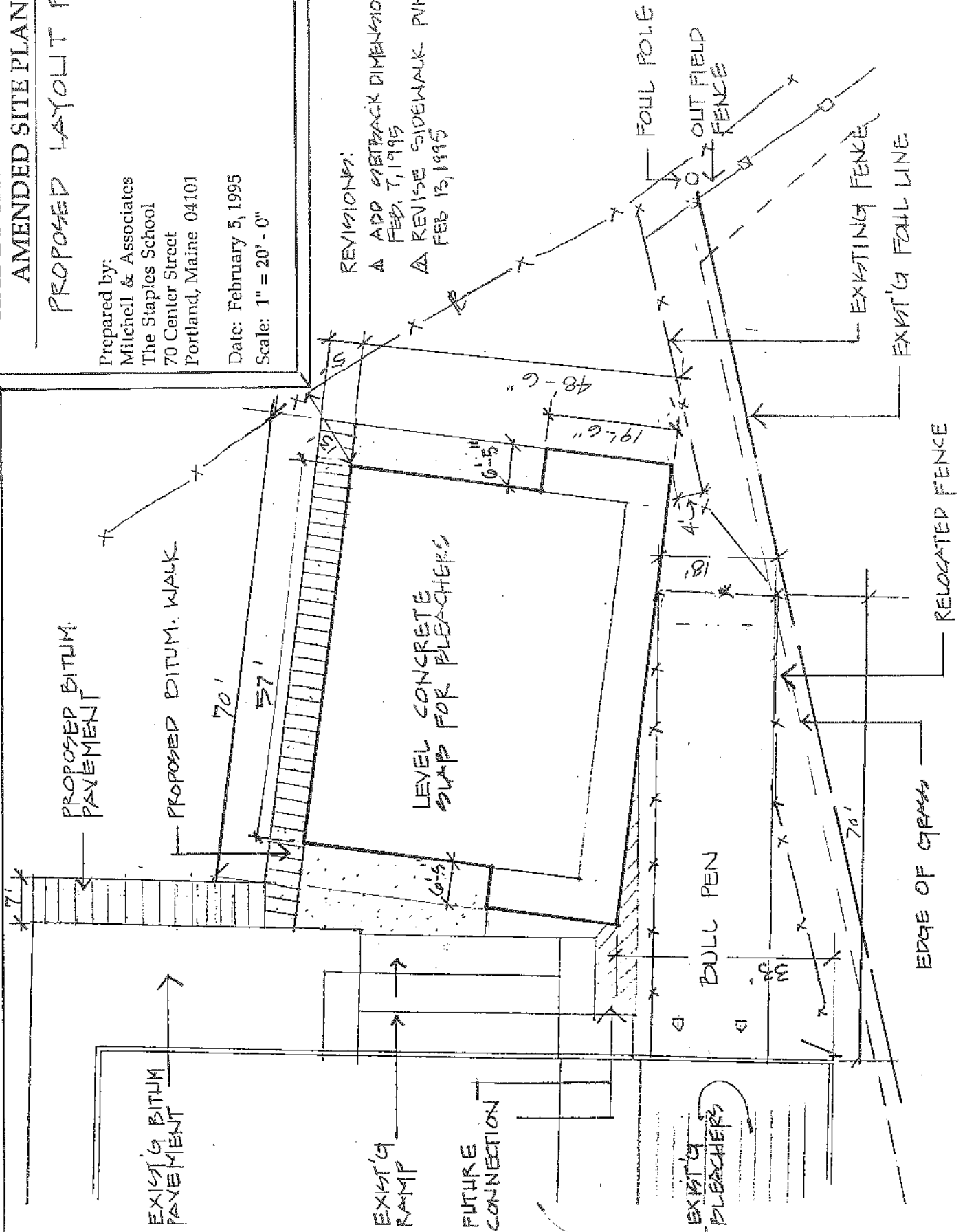
Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 20' - 0"

3

REVISIONS:

- ▲ ADD OBTACK DIMENSION FEB. 7, 1995
- ▲ REVISE SIDEWALK PVMT. FEB 13, 1995



MITCHELL & ASSOCIATES

LANDSCAPE ARCHITECTS

February 6, 1995

Mr. Richard Knowland, Senior Planner
Planning & Urban Development
City of Portland
389 Congress Street
Portland, Maine 04101

RE: HADLOCK FIELD BLEACHER EXPANSION

Dear Rick:

This submission is in regards to documentation requested as part of the approval process for the proposed bleacher expansion at Hadlock Field. Attached for your review are the following exhibits:

Sheet 1 - Existing Conditions (including original approval for paved picnic area)

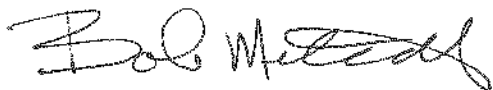
Sheet 2 - Proposed Grading Plan

Sheet 3 - Proposed Layout Plan

Per your request we have calculated the impervious surface ratio for this proposal. The original approval allowed for 24.87% coverage that included the paved picnic area that was to be constructed where the bleachers are now proposed. The total impervious cover for the picnic area was 9,888 square feet. The new bleachers and paved circulation total 4,177 square feet, leaving a net reduction in paving of 5,711 square feet. The reduction in pavement keeps the total impervious area under 25 percent.

Should you have any questions concerning this documentation, please do not hesitate to contact our office.

Sincerely,
Mitchell & Associates



Robert B. Metcalf

Enclosure

cc: Sam Hofsus
Mike Fagerson

- DEC 14 MEETING WITH CHARLIE, MIKE, FRANK
- DEC 27 ALGX MET WITH THEM AT NADLOCK FLD
- JAN 5 MEETING WITH MIKE FAGERSON ... WENT OVER LOTTON TO HIM
CALLED TO SET UP MEETING
- JAN 25 MET WITH C.E, J.M, M.F AND JAM DISCUSSED LOTTON AGAIN
REQUIREMENTS AND TIMING

OCT 27

MET WITH DALE ACKLEY ABOUT THE STADIUM, SHOWED ME A
ROUGH CONCEPT PLAN, INDICATED ^{LOCATION} DSP SITE LOCATION REVIEW
ISSUES, REVIEWED AT STAFF MFC, BILL C. SAYS SETBACK
IS OK
TELEPHONED DALE WITH COMMENTS

NOV 14

TELEPHONED DALE ACKLEY ON STATUS OF PROJECT, EMPHASIZED
TIGHT TIMEFRAME, ^{LOCAL REVIEW} DSP AND TRAFFIC ISSUES

TO: Bob Ganley, City Manager
FROM: Frank LaTorre, Facility Director 418
SUBJECT: Newspaper Racks in front of Hadlock Field Stadium
DATE: March 20, 1995

Based on your phone call about the red "Real Estate by Owner" newspaper distribution box in front of Hadlock Field, I have gathered some information to let you know where things stand.

A call to Portland News shows they will be having a total of 5 newspaper sales machines in front of Hadlock again this year - Press Herald, Boston Globe, Boston Herald, USA Today, USA Sports Weekly. They were all "economically viable and will return" according to Mark Storms of Portland News because "they are on a right of way and have a right to be there."

The owner of the red boxes distributing the free "Real Estate by Owner" is Bob Kennett of Citiracks (799-1985). He said he has "a legal right to be there." He removed the box temporarily as a personal favor but "will return the box if the other newspaper boxes are going to be allowed to stay. He said "it is not right to single out free newspaper boxes from paid ones and so if the paid ones stay I will be returning my red box." Mr. Kennett said he checked with the City Planning Department and the City Public Works Department and "could find no ordinance prohibiting him from placing his box in front of Hadlock Field on a public way as it does not interfere with pedestrian flow." As I said he will return his box should the others be allowed to stay.

Mr. Kennett mentioned that in Freeport there are restricted areas where those distribution boxes can be placed. A lottery is held to determine who will be placed in each location.

I wanted to update you on where this presently stands

Inspection Services
P. Samuel Hoffses
Chief



Planning and Urban Development
Joseph E. Gray Jr.
Director

CITY OF PORTLAND

March 9, 1995

RE 271 Park Avenue

Portland Sea Dogs
P. O. Box 636
Portland, ME 04101

Dear Sir:

Your application to make exterior renovations-expand bleacher seats, has been reviewed and a permit is herewith issued subject to the following requirements. This permit does not excuse the applicant from meeting applicable State and Federal laws.

Site Plan Review Requirements

Inspection Services Approved Marge Schmuckal
Fire Department Approved LT. McDougall
Planning Division Landscaping shall be planted adjacent to the bleaches
addition as required by the City Arborist Jeff Tarling, Public Works
Public Works Approved Rick Knowland for Jim Seymour

Building and Fire Code Requirements

1. If combustible materials are stored below bleaches, the sprinkler system will have to be extended into new space.
2. The proposed bleaches shall meet the requirements of Chapter 10, section 13 of the City's building code. (The BOCA National Building Code/1993)
3. The builder of a facility to which Section 4594-C of the Maine State Human Rights Act, Title 5 MRSA refers, shall obtain a certification from a design professional that the plans of the facility meet the standards of construction required by this section. Prior to commencing construction of the facility, the builder shall submit the certification to the Division of Inspection Services.
4. Guardrail & Handrails - A guardrail system is a system of building components located near the open sides of elevated walking surfaces for the purpose of minimizing the possibility of an accidental fall from the walking surface to the lower level. Minimum height all Use Groups 42", except Use Group R which is 36". In occupancies in Use Group A, B, H-4, I-1, I-2, M and R and public garages and open parking structures, open guards shall have balusters or be of solid material such that a sphere with a diameter of 4" cannot pass through any opening. Guards shall not have an ornamental pattern that would provide a ladder effect.
5. Every building and structure shall be designed and constructed to resist the effects of earthquake motions determined in accordance with this Chapter 16, section 12 of the City's building code.
6. All construction documents required for a building permit application shall be prepared by a registered design professional. The documents shall include the name, and address of the registered design professional and shall be signed, sealed and dated by the registered design professional.
7. It is strongly recommended that a fire protective signaling system be installed in the proposed area under the bleaches.


Portland Sea Dogs

2

3/9/95

If you have any questions regarding these requirements, please do not hesitate to contact this office.

Sincerely,



P. Samuel Hoffses
Chief of Inspection Services

/el

cc: LT. Gaylen McDougall, Fire Prevention Officer
Marge Schmuckal, Asst. Chief of Inspection Services
Rick Knowland, Senior Planner



CITY OF PORTLAND

January 5, 1995

Mr. Michael Fagerson, Operations Manager
Portland Sea Dogs
271 Park Avenue
Portland, ME 04102

Dear Mr. Fagerson:

This letter is intended to outline the submission requirements and issues related to the proposed seating expansion to Hadlock Field. This application will require site plan review (see Attachment A) as well as DEP Site Location review that will be conducted by the City of Portland. The submission requirements are shown starting on page 1355. The review standards are shown on page 1361. The DEP however does have the ability to assert jurisdiction so that we will want to have as complete an application as possible.

I have attached a copy of the Site Plan Ordinance as well as the Recreation Open Space Zone (see Attachment B). The proposed project will need to meet the standards of these ordinances. As your plans and information come together, I would be happy to review it so we make sure the project stays on track. As this appears to be a fast track project, the sooner we get a complete application, the sooner we can complete our review. As I mentioned at our meeting, the DEP has the ability to assert jurisdiction of the review within 30 days of our forwarding a complete application to them.

A narrative should be submitted outlining the proposed project. This represents an introduction to the project so that we can get an understanding of the proposal in the context of the overall plan of the site.

Traffic and parking are undoubtedly the biggest review issue. While the original traffic and parking report provides an excellent base of information, this will need to be updated to reflect the additional traffic and parking generated by the seating expansion. Mary Theriault will be a key resource since she did the original report. Bill Eaton, a traffic engineering consultant, conducted the peer review of Mary's report which expedited the final site plan approval. Since Bill has prior experience with this project it may be advantageous to have him involved again to expedite the required traffic and parking analysis/review.

A revised site plan should be submitted showing the new footprint of the addition. If regrading will occur this should be shown on the plan as well as any other changes to the site, such as pavement surfaces, walkways, utilities/exterior lighting or other physical changes to the site.

The Recreation Open Space (R-OS) zone requires that no more than 25% of the lot may be an impervious surface (see Sec. 14-157 of the R-OS zone.) An impervious surface includes buildings, parking areas and other "hard surfaces." We will need a revised calculation reflecting the new addition. I believe John Rague may be a good resource person to talk to, since an impervious surface calculation was generated as part of the original Madlock Field application.

A drawing to scale should be submitted showing what the addition will look like including the type of materials and color. There should be elevations (front, rear and sides) showing a section of the adjacent stadium with the new addition such that we can understand how it relates to the existing facade.

A short narrative should be submitted as part of the application addressing the impact of the addition on the following elements:

- solid waste
- water
- sewer

The submission should include a construction schedule of the proposed addition.

We have some preliminary comments regarding site engineering issues. A drainage analysis or narrative needs to be prepared and stamped by a professional engineer. This analysis would include a description of existing site conditions, the proposed stormwater management plan and an evaluation of the project's impact on site drainage. This narrative should also include a description of erosion and sedimentation measures as well as plans to revegetate disturbed areas.

There is a gas line in the vicinity of the site. Has the gas company been contacted? What construction measures will be undertaken to deal with this line.

Should you have any questions on this letter, please call me.

Sincerely,



Richard Knowland
Senior Planner

cc: Joseph E. Gray, Jr., Director of Planning and Urban Development
Richard Knowland, Senior Planner
Owens McCullough, Acting Development Review Coordinator



The Sheridan Corporation

January 30, 1995

Mr. Samuel Hoffses
Code Enforcement Department
City of Portland
389 Congress Street
Portland, ME 04101

Re: Soffit Additions to Hadlock Field

Dear Mr. Hoffses:

The Sheridan Corporation has been hired by the City Public Works Department, Portland, Maine to add 20 feet of steel soffit material to the underside of the upper deck of Hadlock Field (see referenced photograph for location). This addition will be made to each side of the main entry; only the left side is shown in the photograph.

Our intention is to put sleepers between the currently existing structural members. Those will be screw attached with brackets to the steel members. After that a soffit panel liner will be added to the sleepers and the existing structural to enclose those structurals.

The intention of this modification is strictly to keep the birds from nesting on those areas and causing the problems shown in the photograph.

The cost of this project is around \$4,500.00 and therefore we are submitting a request for a building permit.

We wish to start this work on Wednesday, February 21st. if at all possible.

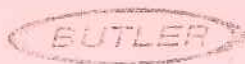
Sincerely,

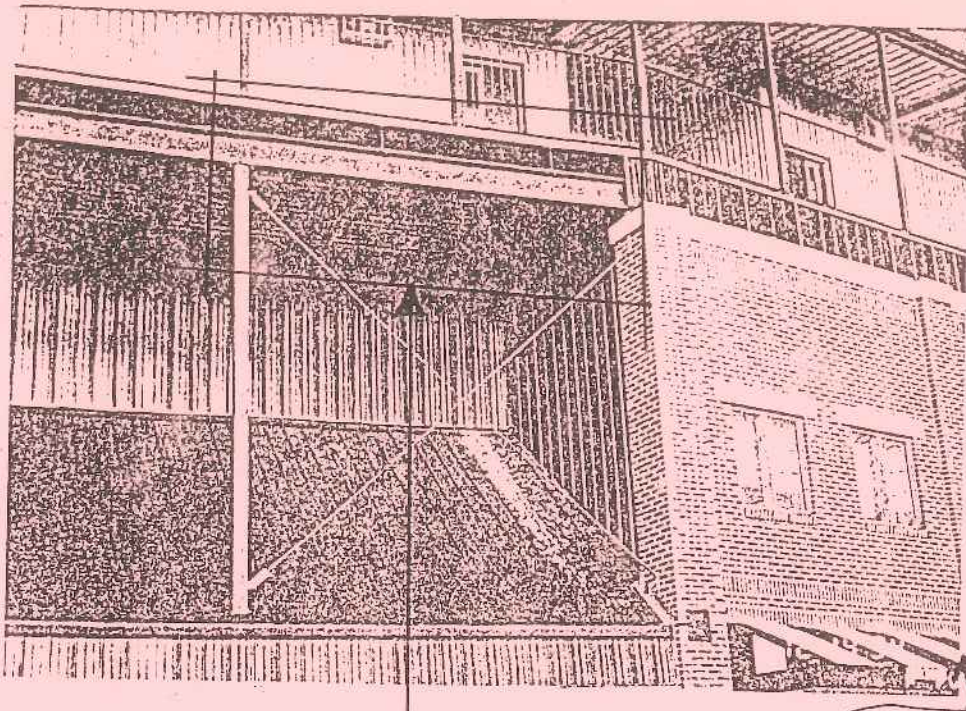
Kenneth S. Lamoreaux
Director of Engineering

KSL/cat

Enclosure

cc: D. Akeley, B. Nelson, S. Hoak
ref id: portland-hr





SOFFIT WORK WILL BE
DONE IN BOXED AREA
OR BOTH RIGHT AND LEFT
SIDES OF ENTRY.

MAIN ENTRY
HADLOCK FIELD
PORTLAND, ME

Maine Center for the Blind & Visually
Impaired
attn: Robert Crouse
189 Park Avenue
Portland, ME. 04102

Maine Center for Cancer Medicine
ATTN: Jayne A. Soles
180 Park Avenue
PO Box 553, DTS
Portland, ME. 04112-0553

Ramada Inn
ATTN: Don Haggett
1230 Congress Street
Portland, ME. 04102

Medical Services Federal Credit Union
ATTN: Ken Aker
272 Park Avenue
PO Box 10659
Portland, ME. 04101

Maine Orthotic Lab
ATTN: Alison Perkins
300 Park Avenue
Portland, ME. 04102

Libby/Morse/Taylor
ATTN: J. Michael Taylor
295 Park Avenue
Portland, ME. 04102

New England Wheel & Rim Co.
ATTN: Maurice Joy
312 St. John Street
Portland, ME. 04102

J. Weston Walsh
ATTN: Suzanne Austin
321 Valley Street
PO Box 658
Portland, ME. 04104-0658

Lock, Stock & Barrell
ATTN: Brian Wallace
369 St. John Street
PO Box 939
Portland, ME. 04104

Dick Cole Tire Co.
ATTN: Dick Cole
333 St. John Street
Portland, ME. 04102

Stadium Pizza & Grille or Current
Resident
325 St. John Street
Portland, ME. 04102

Glidden Paint & Co.
971 Congress Street
Portland, ME. 04102

HP Hood, Inc.
349 Park Avenue
Portland, ME. 04102

CITY OF PORTLAND, MAINE

Dept. of Planning & Urban Development

City Hall

389 Congress Street
Portland, Maine 04101

FACSIMILE MESSAGE COVER SHEET

Return Fax #
(207) 874-8716

Date: 2/20/95 Time: 12:35

To the Attention of: BOB METCALFE

Company/Entity: MITCHELL ASSOCIATES

Message From: RICK KNOWLAND

Department: PLANNING

Phone #: 874-8300 ext 8725

Receiving FAX #: 874-2460

Total # of Pages including cover sheet: 3

Message: BOB

ATTACHED ARE COMMENTS REGARDING THE DRAINAGE PLAN

2/17/95

MITCHELL & ASSOCIATES
LANDSCAPE ARCHITECTS

February 16, 1995

JIM
UPDATES ON HADLOCK
FIELD, IS THIS
OK?
RK

Mr. Richard Knowland, Senior Planner
Planning & Urban Development
City of Portland
389 Congress Street
Portland, Maine 04101

RE: HADLOCK FIELD BLEACHER EXPANSION

Dear Rick:

This submission is in response to our meeting at the site on Wednesday February 15th where we discussed the issues concerning stormwater management. At that meeting it was agreed to provide a dry well structure at the low point near the existing access gate to the proposed bleacher area. A six foot diameter by six foot deep dry well will be excavated and filled with one inch crushed rock, topped with a geotextile fabric and four inches of loam. The finished grade will be pitched toward this low point. In addition a four inch diameter perforated PVC underdrain will be installed along the front of the new bleachers and connect to the proposed dry well.

In response to the question concerning existing soils conditions, we have reviewed R.W. Gillespie's geotechnical report for Hadlock Field. Boring B-3 was taken at the corner of the stadium close to the proposed dry well location. Indications are the soils are a fill material consisting of silty sands, silty gravels, rock fragments, charcoal fragments and miscellaneous material. Groundwater was incurred at approximately thirteen feet.

In addition, we have revised the plans to show a bituminous walkway along the rear of the bleachers rather than concrete as originally indicated.

Should you have any questions or concerns regarding this documentation, please do not hesitate to contact our office.

Sincerely,
Mitchell & Associates

BOB METCALF

Robert B. Metcalf

Enclosure

cc: Mike Fagerson



RIK,
SEE SHEET 2 FOR MY
COMMENTS AND SUGGESTED GRADING
WE MAY NEED MORE STONE TO CREATE
MORE VOIDS AND IN TURN VOLUME FOR
STORAGE. PLEASE HAVE THEM SUBMIT
A DRY WELL DETAIL!
Jim

HADLOCK FIELD STADIUM AMENDED SITE PLAN

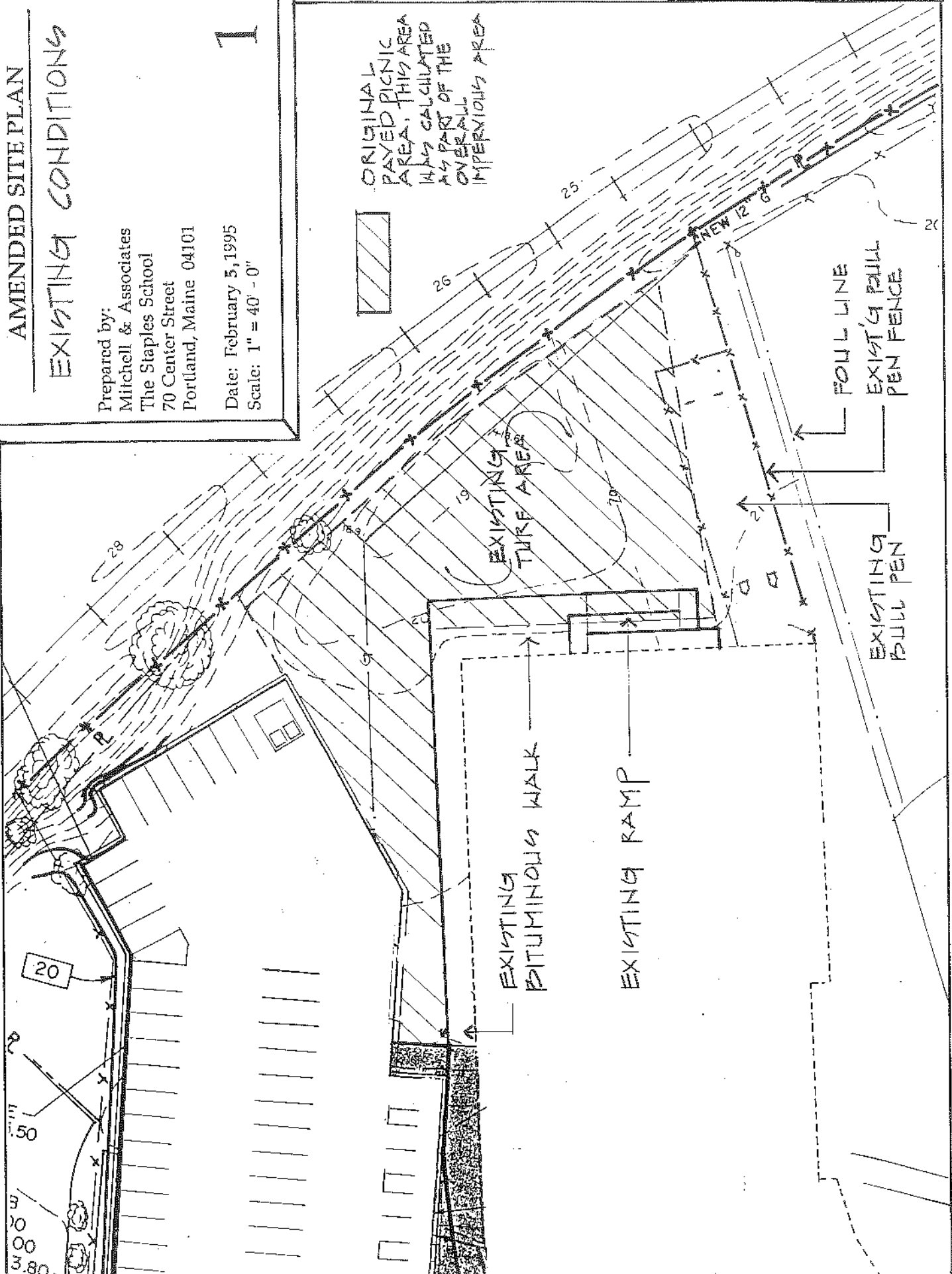
EXISTING CONDITIONS

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

1

ORIGINAL
PAVED PICNIC
AREA. THIS AREA
WAS CALCULATED
AS PART OF THE
OVERALL
IMPERVIOUS AREA



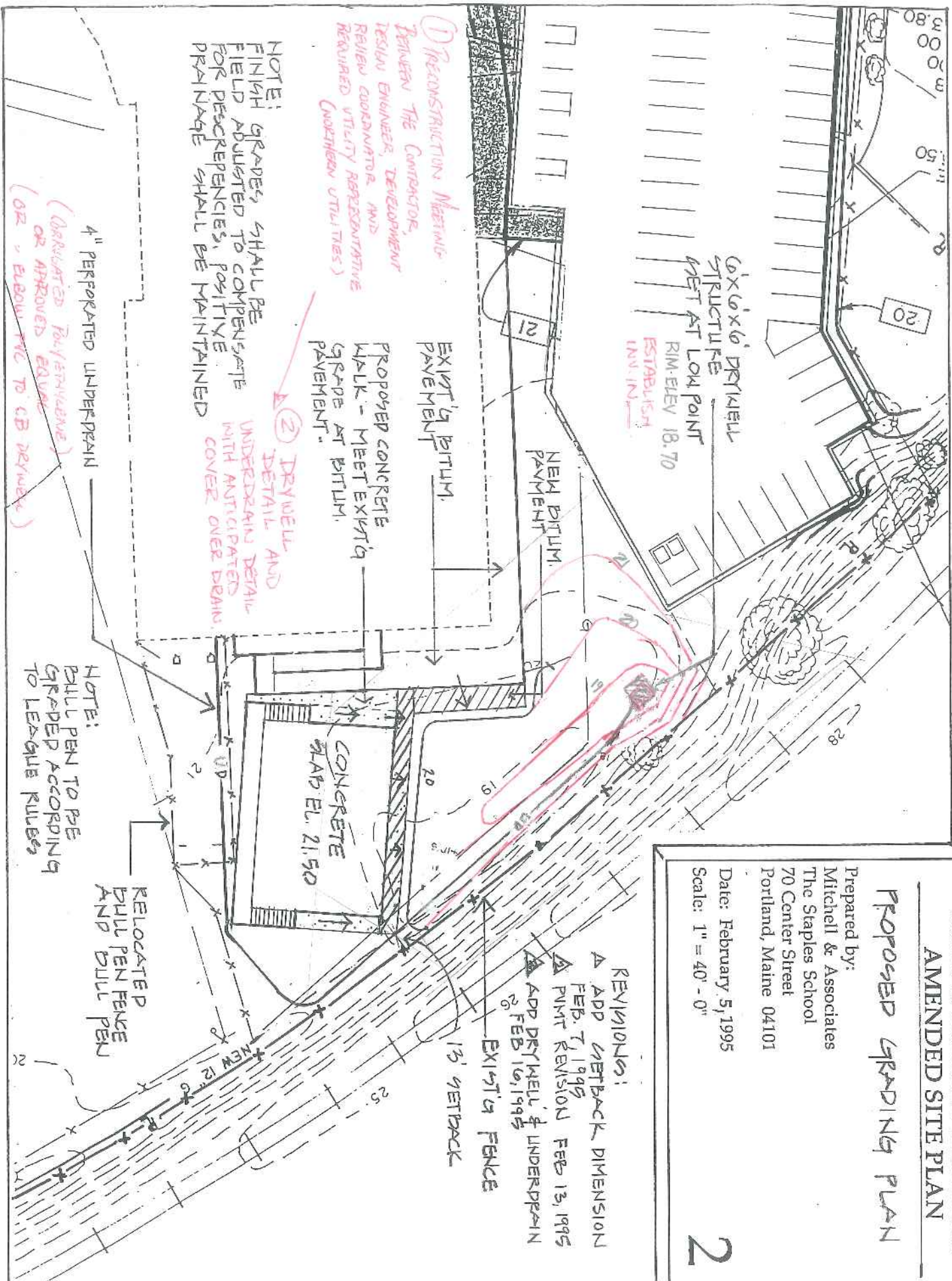
HADLOCK FIELD STADIUM
AMENDED SITE PLAN

PROPOSED GRADING PLAN

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

2



① **RECONSTRUCTION MEETINGS:**
Between The Contractor,
Resin Engineer, Development
Resin Coordinator, AND
Review Coordinator. (UTILITY REPRESENTATIVE
REQUIRED (UNDERGROUND UTILITIES))

NOTE: GRADES SHALL BE
FIELD ADJUSTED TO COMPENSATE
FOR DISCREPANCIES, POSITIVE
DRAINAGE SHALL BE MAINTAINED

② **DRYWELL AND
DETAIL AND
UNDERDRAIN DETAIL
WITH ANTICIPATED
COVER OVER DRAIN**

4" PERFORATED UNDERDRAIN
(OR RATED POLYETHYLENE)
OR APPROVED EQUAL
(OR - ELBOW TIC TO CB DRYWELL)

NOTE:
BULL PEN TO BE
GRADED ACCORDING
TO LEAGUE RULES

- REVISIONS:
- ▲ ADD SETBACK DIMENSION
FEB. 7, 1995
 - ▲ PINT REVISION FEB. 13, 1995
 - ▲ ADD DRYWELL & UNDERDRAIN
FEB. 16, 1995

RELOCATED
BULL PEN FENCE
AND BULL PEN

HADLOCK FIELD STADIUM AMENDED SITE PLAN

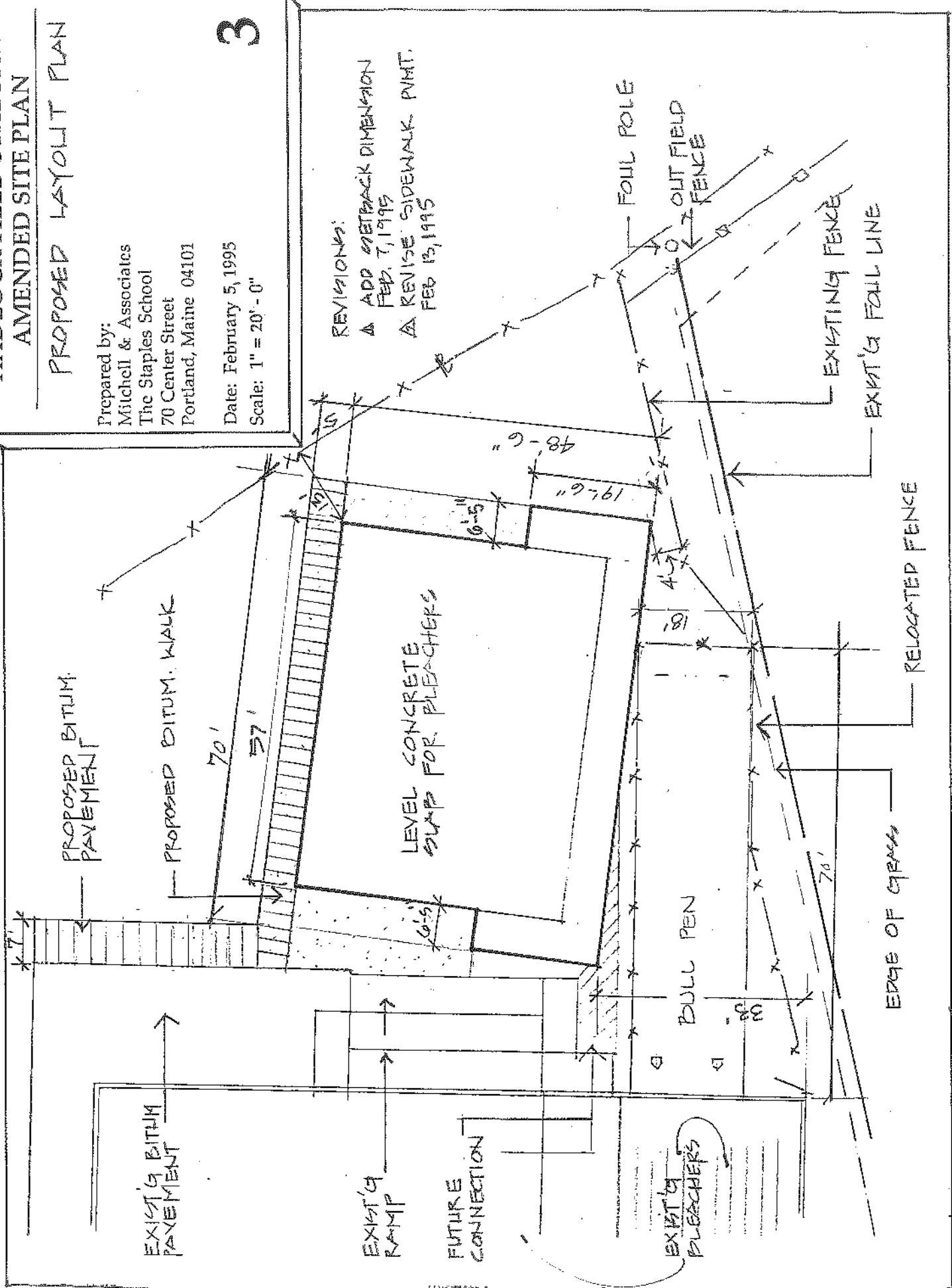
PROPOSED LAYOUT PLAN

3

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 20' - 0"

REVISIONS:
▲ ADD METRACK DIMENSION
FEB. 7, 1995
▲ REVISE SIDEWALK PMNT.
FEB 13, 1995



FEB 15, 1995

SAM

ATTACHED ARE THE UPDATED SUBMISSIONS FOR THE HADLOCK
FIELD CONCRETE PAD. A PERMIT MAY BE ISSUED FOR
THE PAD ONLY SUBJECT TO THE FOLLOWING CONDITION.

1. THAT A REVISED DRAINAGE PLAN BE SUBMITTED
[FOR THE PAD AREA] FOR REVIEW AND APPROVAL
BY THE ACTING DEVELOPMENT REVIEW COORDINATOR.

THE CONCRETE PAD WAS SHOWN ON THE ORIGINAL PLAN
AS A HAND-DRAWN PICNIC AREA. THE SEEDS ARE IN
THE PROCESS OF SUBMITTING A REVISED SITE PLAN
APPLICATION TO COVER THE BLEACHON EXPANSION WHICH
WE WILL REVIEW LATER

RK

HADLOCK FIELD STADIUM
AMENDED SITE PLAN

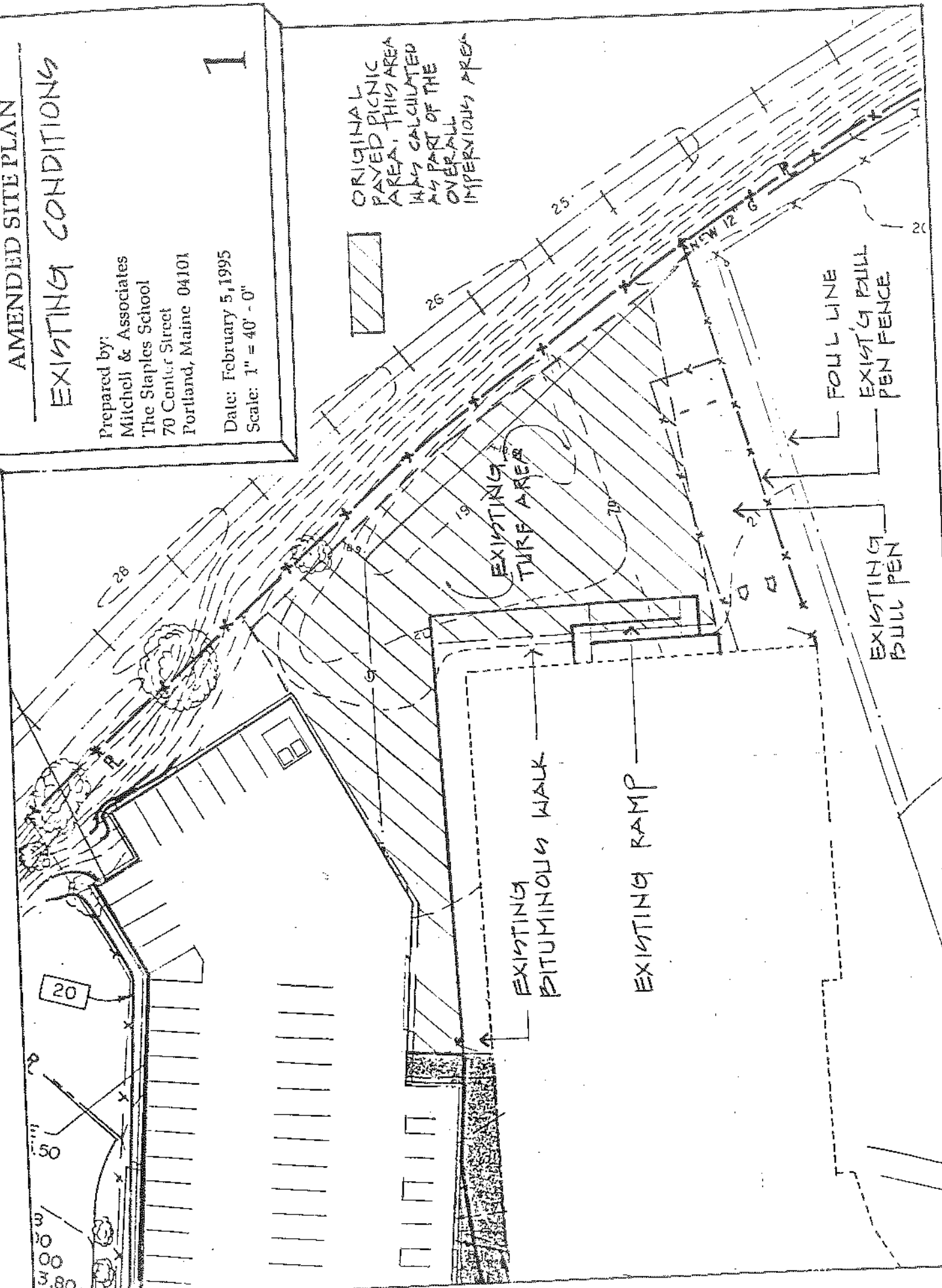
EXISTING CONDITIONS

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

1

ORIGINAL
PAVED PICNIC
AREA. THIS AREA
HAS CALCULATED
AS PART OF THE
OVERALL
IMPERVIOUS AREA



HADLOCK FIELD STADIUM
 AMENDED SITE PLAN

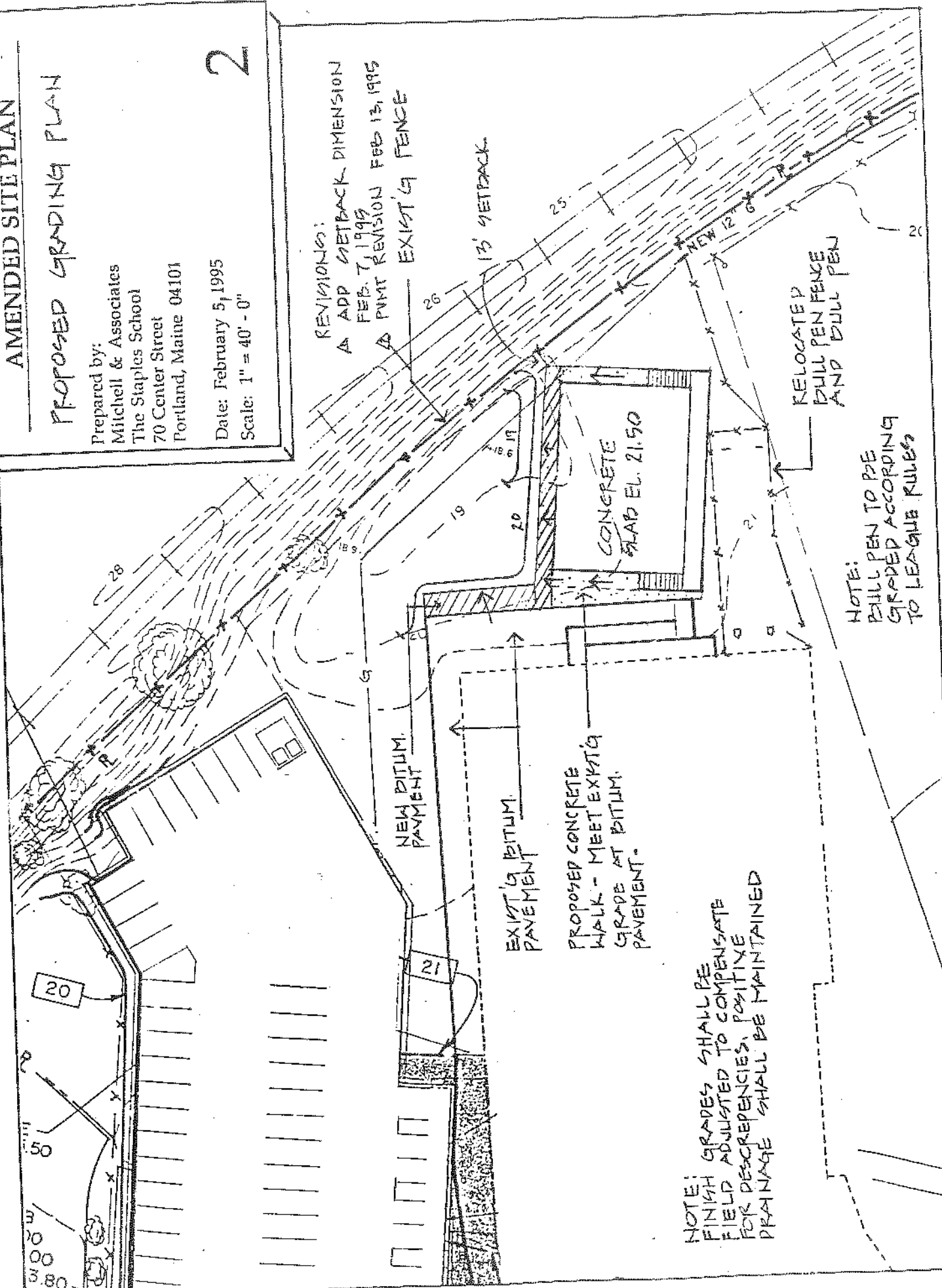
PROPOSED GRADING PLAN

2

Prepared by:
 Mitchell & Associates
 The Staples School
 70 Center Street
 Portland, Maine 04101

Date: February 5, 1995
 Scale: 1" = 40' - 0"

REVISIONS:
 A ADD SETBACK DIMENSION
 FEB. 7, 1995
 B PUMP REVISION FEB 13, 1995
 C EXIST'G FENCE



NOTE:
 BULL PEN TO BE
 GRADED ACCORDING
 TO LEAGUE RULES

NOTE:
 FINISH GRADES SHALL BE
 FIELD ADJUSTED TO COMPENSATE
 FOR DISCREPANCIES. POSITIVE
 DRAINAGE SHALL BE MAINTAINED

HADLOCK FIELD STADIUM
AMENDED SITE PLAN

PROPOSED LAYOUT PLAN

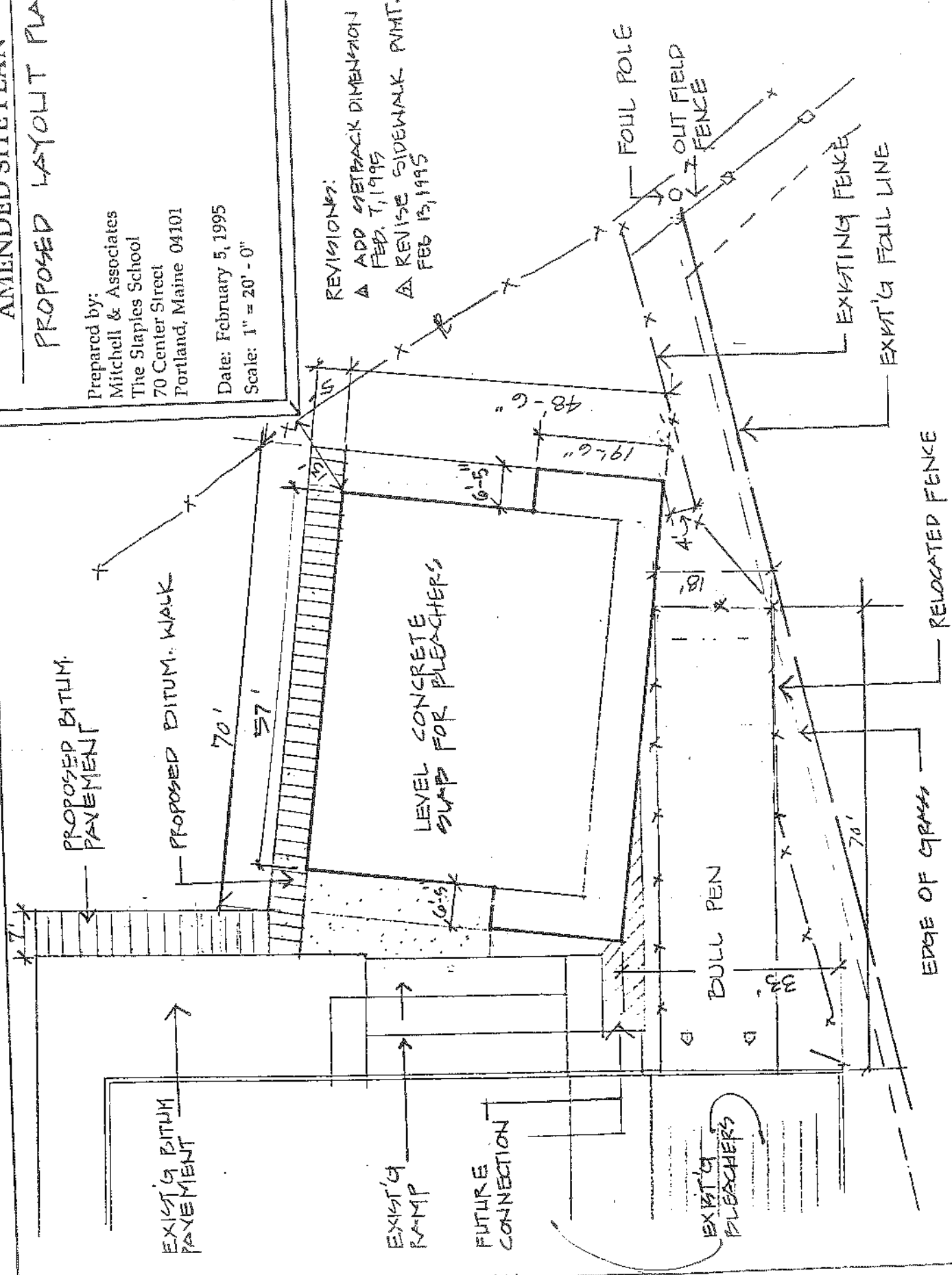
3

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 20' - 0"

REVISIONS:

- ▲ ADD METRACK DIMENSION FEB. 7, 1995
- ▲ REVISE SIDEWALK PMNT. FEB 13, 1995



BOB

WE ARE AWAITING A FORMAL SUBMISSION BY THE SEADOG FOR THE EXTRA SEATING AT HADLOCK FIELD. ATTACHED ARE SOME PHOTOS OF WHAT THE NEW SEATING WILL LOOK LIKE. THE ACTUAL SEATS WILL BE THE SAME BLUE SEATS THAT ARE CURRENTLY AT HADLOCK FIELD. THE LOWER AREA OF THE BLEACHERS WILL BE COVERED WITH THE SAME GREEN METAL MATERIAL THAT IS ON HADLOCK FIELD. AGAIN, WE ARE AWAITING THEIR SUBMISSION PACKET THAT WILL DOCUMENT THEIR PLANS.

ATTACHED IS A CROSS SECTION OF THE BLEACHER PLANS. I HAVE ALSO ATTACHED A LETTER TO THE SEADOG DATED JAN 5TH WHICH WAS A FOLLOW-UP WITH A MEETING I HAD WITH THEM ON DEC 14TH TO EXPLAIN THE REVIEW PROCESS FOR THE BLEACHER EXPANSION.

RICK K

WE ARE MAKING SURE THE SEADOG USE THE MOST UP TO DATE SITE PLAN.

1995 TICKETS
ON SALE
SATURDAY
NOV. 26TH.



ART WORK
1995 TICKETS

NOV 26 1995
NOV 26 1995

NOV 26 1995

777 677

NOV 26 1995
NOV 26 1995

NOV 26 1995
NOV 26 1995
NOV 26 1995

LETTER OF TRANSMITTAL

MITCHELL & ASSOCIATES
THE STAPLES SCHOOL
70 CENTER STREET
PORTLAND, MAINE 04101

DATE: February 14, 1995

TO: Rick Knowland
City of Portland
389 Congress Street
Portland, Maine 04101

TEL (207) 774-4427
FAX (207) 874-2460

We are sending you the following items:

- Prints
- Plans
- Copy of Letter
- Other

Re: Hadlock Stadium

Copies	Date	Description
3 sets		Revised paving sheets 1 through 3

These are transmitted as checked below:

- For approval
- For your use
- For review and comment
- As requested by: _____

Remarks:

Copy to: _____ Signature: Robert Metcalf

City of Portland

Planning Department, 4th Floor, 389 Congress Street
Portland, ME 04101
(207)874-8721
Fax: (207)756-8258

FAX TRANSMISSION COVER SHEET

Date: 8-21-96
To: BOB METCALF
Fax: 874-2460
Re: HADLOCK FIELD
Sender: RICK KNOWLAND

YOU SHOULD RECEIVE 3 PAGE(S), INCLUDING THIS COVER SHEET.
IF YOU DO NOT RECEIVE ALL THE PAGES, PLEASE CALL (207)874-8721.

BOB,

ATTACHED IS A MEMO FROM THE FILE THAT WAS SENT TO
THE DEP ON HADLOCK FIELD. OFF THE TOP OF MY HEAD
I CAN'T REMEMBER IF THERE WERE OTHER REPIRIS DING.
MANY (THEIRSELF) CONROY IS THE MOST KNOWLEDGEABLE PERSON
ON THIS SUBJECT

RIL

DEC - 5 10



Portland Exposition Building



Hadlock Field Stadium

239 Park Avenue Portland, Maine 04102-2911 (207) 874 - 8200 FAX (207) 874 - 8130

TO: Bob Ganley, City Manager

FROM: Frank LaTorre, Facility Director *FLT*

SUBJECT: Sea Dogs and Stadium Expansion

DATE: December 1, 1994

I met with Charlie to go over a number of issues regarding winterization of the stadium and procedure for the 1995 season.

During the course of the discussion, the subject of adding seats to Hadlock Field arose. He said he was still "talking with contractors." I asked if he had made any overture to the Planning Department regarding approvals. He said it was his understanding that the addition would not require Planning Board approval and that no such contact with the Planning Department had been made.

I suggested he get a clarification of what approvals he might need from the Planning Department. He said he would seek such a clarification.

I just wanted to make you aware of this.

Joe Gray - You may want to write to Sea Dogs re: requirements for receiving approval for additional seats

HADLOCK FIELD STADIUM AMENDED SITE PLAN

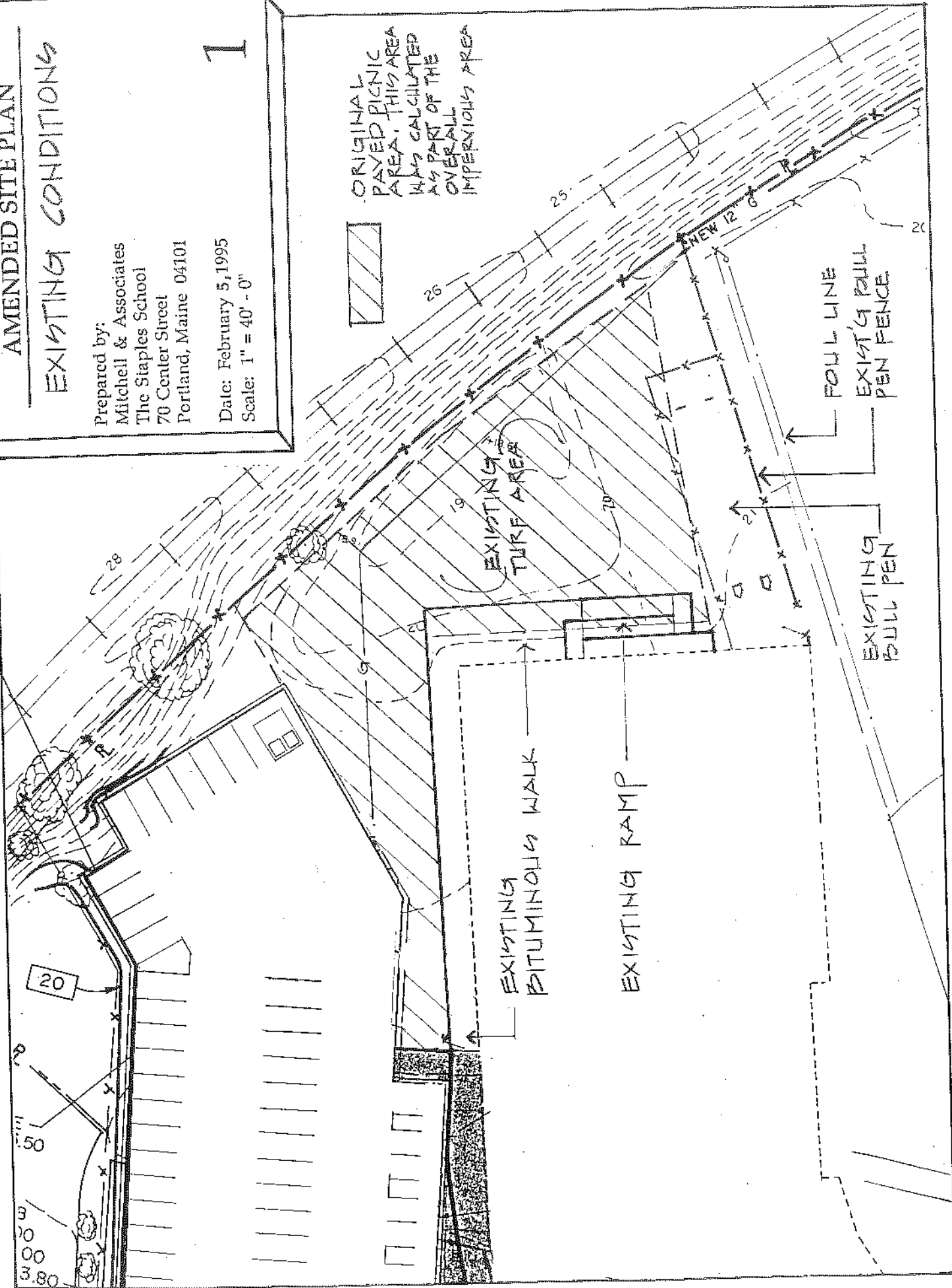
EXISTING CONDITIONS

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

1

ORIGINAL
PAVED PICNIC
AREA. THIS AREA
HAS CALCULATED
AS PART OF THE
OVERALL
IMPERVIOUS AREA



EXISTING
TURF AREA

EXISTING
BITUMINOUS WALK

EXISTING RAMP

NEW 12' G

FULL LINE
EXIST'G PULL
PEN FENCE

EXISTING
PULL PEN

20

1.50

3.00
3.80

28

19

20

26

25

20

HADLOCK FIELD STADIUM
AMENDED SITE PLAN

PROPOSED GRADING PLAN

Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 40' - 0"

2

- REVISIONS:
- A ADD SETBACK DIMENSION FEB. 7, 1995
 - A PUNT REVISION FEB 13, 1995
 - A ADD DRYWELL & UNDERDRAIN FEB 16, 1995

EXISTING FENCE

13' SETBACK

NOTE: FINISH GRADES SHALL BE FIELD ADJUSTED TO COMPENSATE FOR DISCREPANCIES. POSITIVE DRAINAGE SHALL BE MAINTAINED

RELOCATED FULL PEN FENCE AND DULL PEN

NOTE: DULL PEN TO BE GRADED ACCORDING TO LEAGUE RULES

4" PERFORATED UNDERDRAIN

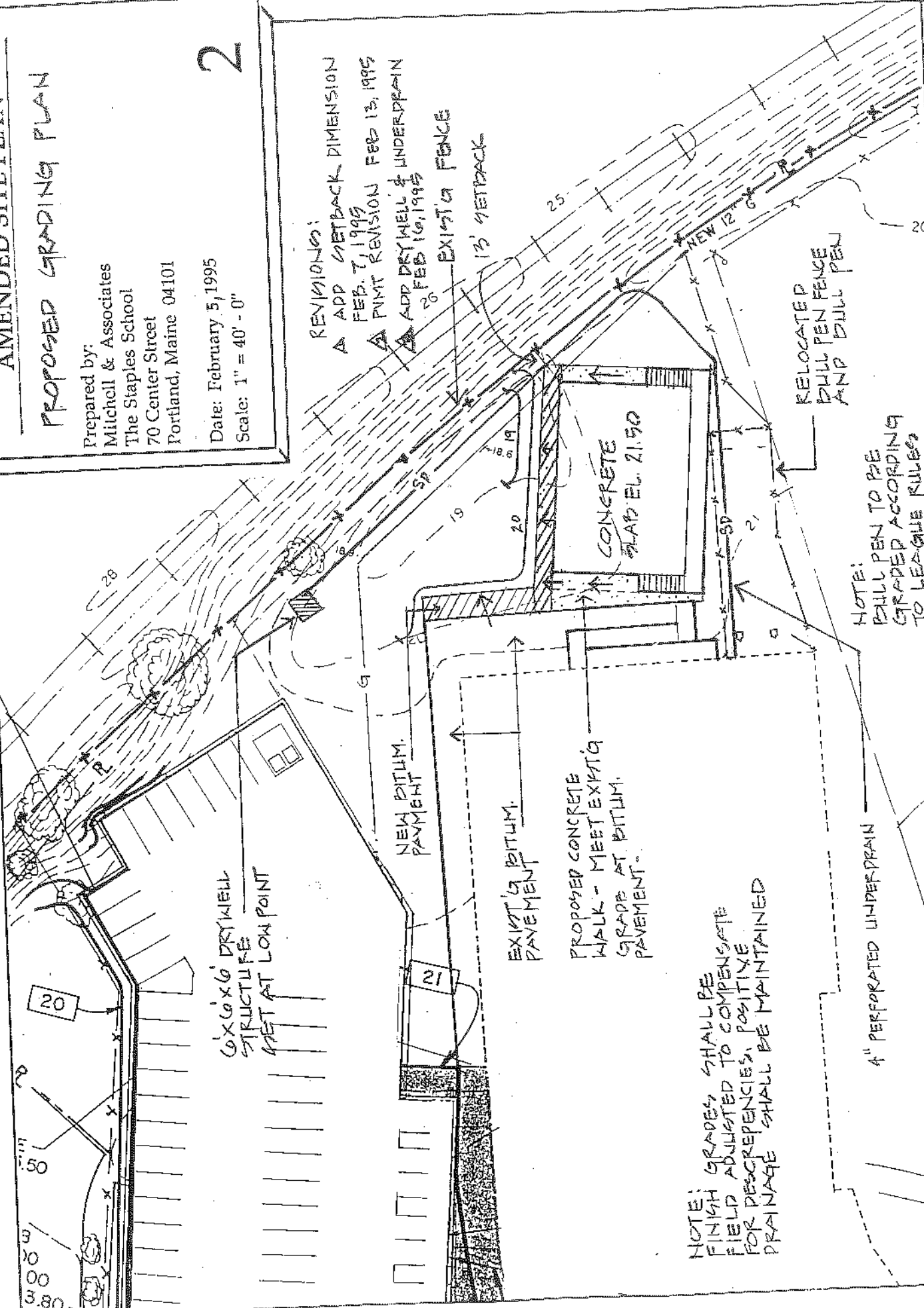
CONCRETE SLAB EL. 21.50

6'x6'x6' DRYWELL STRUCTURE SET AT LOW POINT

NEW BITUM. PAVEMENT

EXIST'G BITUM. PAVEMENT

PROPOSED CONCRETE WALK - MEET EXIST'G GRADE AT BITUM. PAVEMENT.



HADLOCK FIELD STADIUM AMENDED SITE PLAN

PROPOSED LAYOUT PLAN

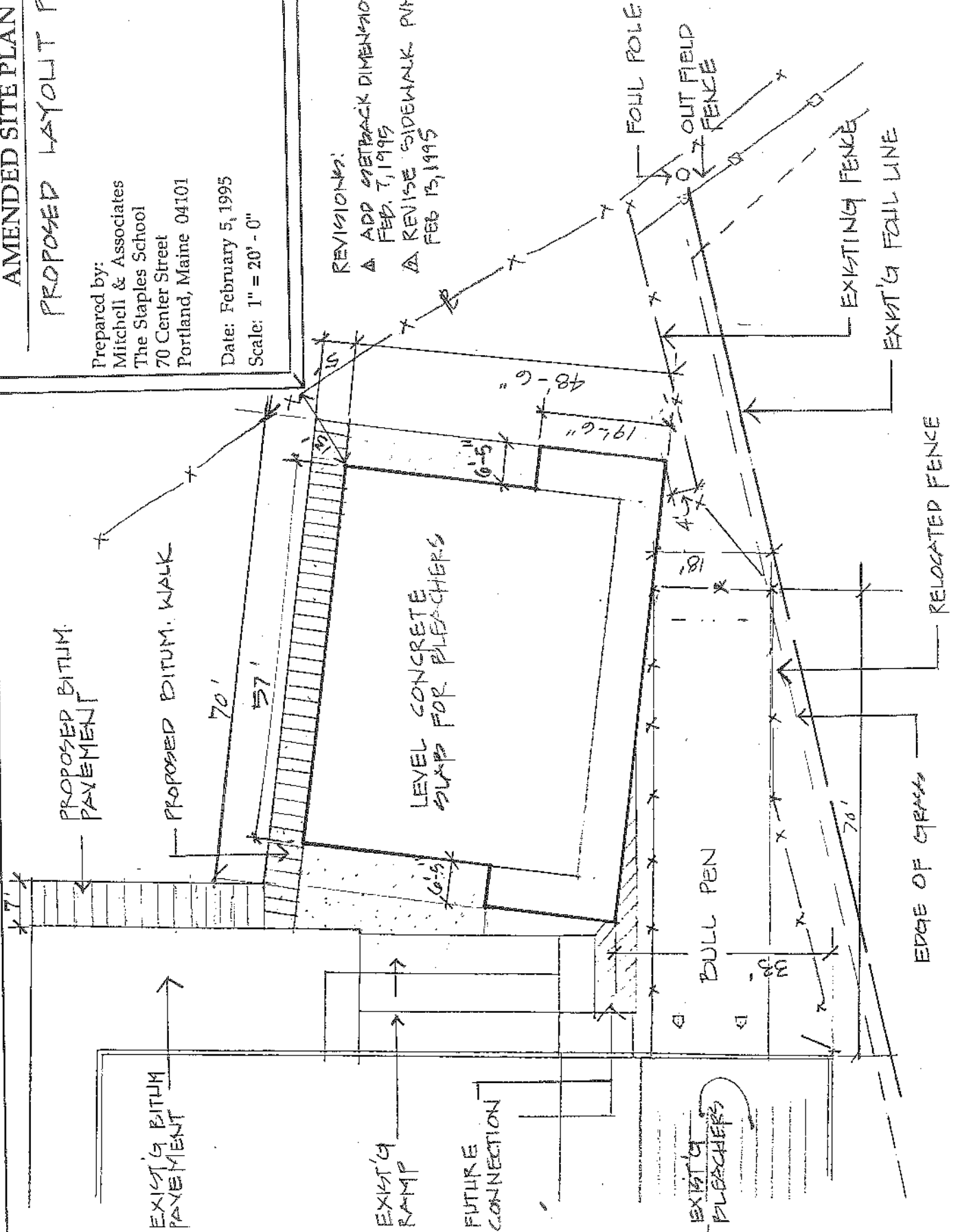
Prepared by:
Mitchell & Associates
The Staples School
70 Center Street
Portland, Maine 04101

Date: February 5, 1995
Scale: 1" = 20' - 0"

3

REVISIONS:

- ▲ ADD SETBACK DIMENSION
FEB. 7, 1995
- ▲ REVISE SIDEWALK PAVT.
FEB 13, 1995



RELOCATED FENCE

EDGE OF GRASS

EXIST'G FENCE

EXIST'G FOUL LINE

FOUL POLE

OUT FIELD FENCE

LEVEL CONCRETE
SLAB FOR PLEACHERS

BULL PEN

PROPOSED BITUM.
PAVEMENT

PROPOSED BITUM. WALK

EXIST'G BITUM
PAVEMENT

EXIST'G
RAMP

FUTURE
CONNECTION

EXIST'G
PLEACHERS



CITY OF PORTLAND
Planning and Urban Development Department

MEMORANDUM

TO: Larry Mead, Director of Parks and Recreation
William Bray, Deputy Director of Public Works

FROM: ✓ Richard Knowland, Senior Planner

DATE: June 1, 1995

SUBJECT: Hadlock Field, 275 Park Avenue

On May 2nd there was a meeting at Bill Bray's office to discuss certain changes to the Hadlock Field site plan. The primary changes involved resurfacing the picnic area with concrete and installing a catchbasin.

I will need a revised site plan so that I can finally sign off on this change. Bill Clark did an earlier version of a plan for this area, perhaps it could be modified reflecting these changes. It will be important that someone prepare a revised site plan since this was a DEP site location review and we must account for any changes to the site.

Please let me know what the timetable would be for submitting a revised plan.

Should you have any questions on this please call me.

cc: Joseph E. Gray, Jr., Director of Planning and Urban Development
Alexander Jaegerman, Chief Planner
Michael O'Sullivan, Development Review Coordinator
Kathi Staples, City Engineer

Worksheet 2: Runoff curve number and runoff

Project Hadlock Stadium Site By Bat Date 2/19/93
 Location Portland, ME Checked _____ Date _____
 Circle one: Present Developed Predeveloped condition

1. Runoff curve number (CN)

Soil name and hydrologic group (appendix A)	Cover description (cover type, treatment, and hydrologic condition; percent impervious; unconnected/connected impervious area ratio)	CN ^{1/}			Area <input type="checkbox"/> acres <input type="checkbox"/> mi ² <input checked="" type="checkbox"/> %	Product of CN x area
		Table 2-2	Fig. 2-3	Fig. 2-4		
DeB-Dorfield Soil Group B	Urban District Commercial & Business	94			194,000 19.8%	18.6
Cu-Filled Area	Impervious <small>1 9 10 24 25 33 Area #3</small>	98			82,990 8.5%	8.3
Cu-Filled Area Soil Group C*	Vegetated	79			414,000 42.4%	33.5
H1B-Hinckley Soil Group B	Impervious <small>23 4 5 6 7 8 11 12 13 14 15 16 17 18 19 20 21 22 23 Area #3</small>	98			193,000 19.7%	19.3
H1B-Hinckley Soil Group C*	Vegetated	79			93,400 9.6%	7.6
Totals =						87.3

^{1/} Use only one CN source per line.

$$CN \text{ (weighted)} = \frac{\text{total product}}{\text{total area}} = \underline{\underline{87.3}}$$
 Use CN = 87.3

2. Runoff

Frequency yr
 Rainfall, P (24-hour) in
 Runoff, Q in
 (Use P and CN with table 2-1, fig. 2-1, or eqs. 2-3 and 2-4.)

Storm #1	Storm #2	Storm #3
2	10	25
2.0	4.5	5.4
.9	3.1	4.0

*Per soils analysis for Hadlock stadium.

Worksheet 3: Time of concentration (T_c) or travel time (T_t)

Project Hadlock Stadium Site By BCE Date 2/19/93
 Location Portland ME Checked _____ Date _____
 Circle one: Present Developed _____ Predeveloped condition
 Circle one: T_c T_t through subarea _____

NOTES: Space for as many as two segments per flow type can be used for each worksheet.
 Include a map, schematic, or description of flow segments.

Sheet flow (Applicable to T_c only)

- Segment ID
- Surface description (table 3-1)
 - Manning's roughness coeff., n (table 3-1) ..
 - Flow length, L (total L \leq 300 ft) ft
 - Two-yr 24-hr rainfall, P_2 in
 - Land slope, s ft/ft
 - $T_t = \frac{0.007 (nL)^{0.8}}{P_2^{0.5} s^{0.4}}$ Compute T_t hr

AB	1
Base Grass	
.24	
300	
2.0	
.005	
1.26	+ [] = 1.26

Shallow concentrated flow

- Segment ID
- Surface description (paved or unpaved)
 - Flow length, L ft
 - Watercourse slope, s ft/ft
 - Average velocity, V (figure 3-1) ft/s
 - $T_t = \frac{L}{3600 V}$ Compute T_t hr

BC	
unpaved	
100	
.005	
1	
.03	+ [] = .03

Channel flow

- Segment ID
- Cross sectional flow area, a ft²
 - Wetted perimeter, p_w ft
 - Hydraulic radius, $r = \frac{a}{p_w}$ Compute r ft
 - Channel slope, s ft/ft
 - Manning's roughness coeff., n
 - $V = \frac{1.49 r^{2/3} s^{1/2}}{n}$ Compute V ft/s
 - Flow length, L ft
 - $T_t = \frac{L}{3600 V}$ Compute T_t hr
 - Watershed or subarea T_c or T_t (add T_t in steps 6, 11, and 19) hr

	+ [] = 1.29

Worksheet 4: Graphical Peak Discharge method

Project Hadlock Stadium Jife By Bart Date 2/19/83
 Location Portland, ME Checked _____ Date _____
 Circle one: Present Developed Predeveloped condition

1. Data:

Drainage area $A_m = \underline{.035}$ mi² (acres/640)
 Runoff curve number CN = 87.3 (From worksheet 2)
 Time of concentration .. $T_c = \underline{129}$ hr (From worksheet 3)
 Rainfall distribution type = III (I, IA, II, III)
 Pond and swamp areas spread throughout watershed = 1.0 percent of A_m (_____ acres or mi² covered)

2. Frequency

	Storm #1	Storm #2	Storm #3
yr	2	10	25
in	2.0	4.5	5.4

3. Rainfall, P (24-hour)

in	.291	.291	.291
----	------	------	------

4. Initial abstraction, I_a
 (Use CN with table 4-1.)

	.146	.065	.054
--	------	------	------

5. Compute I_a/P

in	250	260	265
----	-----	-----	-----

6. Unit peak discharge, q_u
 (Use T_c and I_a/P with exhibit 4-III)

in	.9	3.1	4.0
----	----	-----	-----

7. Runoff, Q
 (From worksheet 2).

	1	1	1
--	---	---	---

8. Pond and swamp adjustment factor, F_p
 (Use percent pond and swamp area with table 4-2. Factor is 1.0 for zero percent pond and swamp area.)

9. Peak discharge, q_p
 (Where $q_p = q_u A_m Q F_p$)

cfs	7.9	28.2	37.1
-----	-----	------	------

Worksheet 3: Time of concentration (T_c) or travel time (T_t)

Project Hadlock Stadium Site By BAF Date 2/19/93
 Location Portland ME Checked _____ Date _____
 Circle one: Present Developed Post-developed condition
 Circle one: T_c T_t through subarea _____

NOTES: Space for as many as two segments per flow type can be used for each worksheet.

Include a map, schematic, or description of flow segments.

Sheet flow (Applicable to T_c only)

Segment ID		
1. Surface description (table 3-1)	AB	
2. Manning's roughness coeff., n (table 3-1) ..	Grass	
3. Flow length, L (total L \leq 300 ft)24	
4. Two-yr 24-hr rainfall, P_2	300	ft
5. Land slope, s	2.0	in
6. $T_t = \frac{0.007 \cdot (nL)^{0.8}}{P_2^{0.5} s^{0.4}}$ Compute T_t005	ft/ft
	1.26 +	1.26

Shallow concentrated flow

Segment ID		
7. Surface description (paved or unpaved)	BC	
8. Flow length, L	unpaved	
9. Watercourse slope, s	100	ft
10. Average velocity, V (figure 3-1)005	ft/ft
11. $T_t = \frac{L}{3600 V}$ Compute T_t	1	ft/s
	.03 +	.03

Channel flow

Segment ID		
12. Cross sectional flow area, a		ft ²
13. Wetted perimeter, p_w		ft
14. Hydraulic radius, $r = \frac{a}{p_w}$ Compute r		ft
15. Channel slope, s		ft/ft
16. Manning's roughness coeff., n		
17. $V = \frac{1.49 r^{2/3} s^{1/2}}{n}$ Compute V		ft/s
18. Flow length, L		ft
19. $T_t = \frac{L}{3600 V}$ Compute T_t		hr
20. Watershed or subarea T_c or T_t (add T_c in steps 6, 11, and 19)		1.29

Worksheet 4: Graphical Peak Discharge method

Project Hadlock Stadium Site By BAJ Date 2/19/93
 Location Portland ME Checked _____ Date _____
 Circle one: Present Developed best developed condition

1. Data:

Drainage area $A_m =$.035 mi^2 (acres/640)
 Runoff curve number $CN =$ 88.5 (From worksheet 2)
 Time of concentration .. $T_c =$ 1.29 hr (From worksheet 3)
 Rainfall distribution type = III (I, IA, II, III)
 Pond and swamp areas spread throughout watershed = 1.0 percent of A_m (_____ acres or mi^2 covered)

2. Frequency yr

Storm #1	Storm #2	Storm #3
2	10	25
2.0	4.5	5.4

3. Rainfall, P (24-hour) in

.260	.260	.260
------	------	------

4. Initial abstraction, I_a in
(Use CN with table 4-1.)

.13	.058	.048
-----	------	------

5. Compute I_a/P

250	260	265
-----	-----	-----

6. Unit peak discharge, q_u csm/in
(Use T_c and I_a/P with exhibit 4-III)

.9	3.2	4.1
----	-----	-----

7. Runoff, Q in
(From worksheet 2).

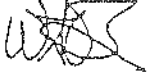
1	1	1
---	---	---

8. Pond and swamp adjustment factor, F_p
(Use percent pond and swamp area with table 4-2. Factor is 1.0 for zero percent pond and swamp area.)

7.9	29.1	38.0
-----	------	------

9. Peak discharge, q_p cfs
(Where $q_p = q_u A_m Q F_p$)

CITY OF PORTLAND, MAINE
MEMORANDUM

TO: William J. Bray, Deputy Director of Parks & Public Works
 FROM: William S. Boothby, Director of Engineering 
 DATE: December 23, 1992
 SUBJECT: Hadlock Field Site Review - Area Computations

Per your request, my staff computed areas of existing construction and pavements and proposed construction and pavements as they relate to DEP Site Location requirements pertaining to Hadlock Field and surrounding contiguous land owned by the City. The findings are as follows:

Existing Buildings Constructed Since 1970:

1985	Ice Arena	29,872 SF
1987	Fitzpatrick Stadium, Press Box	1,272 SF
1987	King Middle School Modular Classroom	1,325 SF
Total Area Constructed Since 1970		<u>32,469 SF</u>

Proposed Building Construction Under New Grandstands:

Restrooms, Concession Areas and Various Uses (Area contained solely within "footprint" of grandstands structure)	29,967 SF
--	-----------

TOTAL EXISTING AND PROPOSED BUILDING AREA: 62,436 Square Feet¹

Note¹ - Total floor area to be added since August 3, 1988 will be 29,192 Square Feet, attributable solely to building construction under and above new proposed grandstands as follows:

Under Grandstands (Ground Level):	22,483 SF
Administrative Offices (Mezzanine):	2,461 SF
Sky Boxes, Media Booth, Restrooms on Upper Deck:	4,983 SF
Total	<u>29,927 Square Feet</u>

Non-Revegetated Areas Constructed Since September 30, 1975:

1985	Ice Arena	29,872 SF
1985	Ice Arena Parking Lot	7,500 SF
1987	King Middle School Modular Classroom	1,325 SF
1987	Fitzpatrick Stadium Press Box	1,272 SF

Total Existing Non-Revegetated Areas Since September 30, 1975	39,969 Square Feet
--	--------------------

Proposed Non-Revegetated Areas:

Proposed: New Hadlock Field Grandstands (including Dugouts)	41,948 SF
Proposed: New Hadlock Field Parking Lot	22,281 SF
Proposed: Existing Hadlock Bleachers to be Relocated at Fitzpatrick Stadium	4,288 SF

Total Proposed Non-Vegetated Areas	68,517 Square Feet
------------------------------------	--------------------

TOTAL PROPOSED AND EXISTING SINCE (SEPTEMBER
30, 1975) NON-REVEGETATED AREAS

108,486 Square Feet

(2.49 Acres)



Baseball Building and
Operation Committee

Robert Ganley
Nadeen Daniels
Larry Mead - Chair
William Bray
Mary Theriault
John Rague
Richard Lauck
Frank LaTorre
P. Samuel Hoffses
Ellen Sanborn
Richard Anderson, Jr.
Richard Knowland
Paul Pendelton
Michael Claus

CITY OF PORTLAND

January 25, 1993

To: Chair Cole and Members of the Portland Planning Board

From: Larry Mead, Chair of Baseball Buildings and Operation Committee

Subject: Hadlock Stadium Project

The City of Portland proposes to develop a new 6,000 seat baseball facility at the Hadlock Stadium site on Park Avenue, adjacent to the Exposition Building. This facility will be the home to a new Double AA minor league baseball team beginning in April, 1994. The Portland City Council authorized this project in August, 1992, and the Eastern Baseball League awarded a franchise to be located in Portland shortly thereafter.

The facility consists of the following major components:

1. A 6,000 seat grandstand structure primarily consisting of structural steel, approximately 345 feet long and 100 feet wide.
2. A support building underneath the grandstand. The facility under the grandstand will include a concourse separating and connecting restrooms and concessions, as well as providing access to the seating areas. The administrative offices for the team will be located here.
3. A redesigned parking configuration separating public parking for the event from employee parking.
4. A public plaza in front of the stadium along Park Avenue.

This project is of tremendous benefit to the City of Portland. It brings to Portland's downtown a major entertainment attraction that will bring thousands of visitors to the downtown area. It provides to the City's and the region's residents an alternative entertainment attraction that is reasonably priced and appropriate for the entire family. Professional baseball complements the vitality and excitement that is present in the urban center.

This is the first opportunity the Planning Board has had to give consideration to the site plan for the new baseball facility at Hadlock Stadium. The project has already undergone a highly visible scrutiny by the City Council during public hearings this past summer. The proposal received an extraordinary level of publicity through the communications media. Ample opportunity for public comment was provided. During this process City staff endeavored to provide information to the Council and the public concerning the facility, including its effect on the neighborhood, and its parking and traffic impacts. Throughout this process, the public

Site Plan Elements

A site plan has been provided to the Planning Board. The plan reflects the limitations imposed on the site by the location of the EXPO building. Since Professional Baseball Association rules require 6,000 seats in this facility, the positioning of the grandstand must conform to the requirements of PBA and the existing location of the EXPO. This, of necessity, results in an unbalanced configuration, turning the main entrance to the facility slightly away from the EXPO and towards Falmouth Street. The plaza situated in front of the stadium serves to connect the EXPO, as well as Park Avenue, to the facility.

Parking and Traffic

A comprehensive parking and traffic study has been completed and provided to the Planning Board. The **parking study** identifies a combination of 2309 parking spaces within a one-half mile radius of the facility exclusive of any parking in Deering Oaks Park, more than adequate to serve the 1693 spaces needed for a *capacity crowd* at the stadium. Of this total, 1701 are within a one-quarter mile radius. The parking plan utilizes tandem parking in the municipal lots closest to the stadium, an approach that is routinely used in Portland for Civic Center events. Since the average attendance in the Eastern League is less than 3000, and the highest average attendance is 4000, the proposed parking plan will readily handle a "typical" game crowd. Past experience with events at the EXPO demonstrate that the area can accommodate parking for similar numbers of vehicles. The steps outlined in the parking plan, such as tandem parking and access to Maine Medical Center garage, provide additional parking capacity than has been previously available.

The **traffic study** incorporates a recent field report completed at a November, 1992 UMO football game at Fitzpatrick Stadium. The study concludes that the affected area is able to readily accommodate traffic levels and will not be adversely affected by projected baseball traffic.

Coordination of events within the Sports Complex

The proposed facility is located within a sports complex that also includes the Portland Ice Arena, the Exposition Building, and Fitzpatrick Stadium. The seasonal nature of events at the Ice Arena and Fitzpatrick Stadium insure that there will be almost no conflicts with the baseball season (April - August). In addition, the schedules of both of these facilities are managed by the Recreation Division. The Recreation Division will also be responsible for coordinating the schedule of Hadlock Stadium and will work closely with the baseball team and the Eastern League to minimize potential conflicts. With respect to the EXPO, the team will work closely with the EXPO Director to prevent scheduling conflicts in advance whenever possible, through joint planning. If conflicts are likely, the two entities will minimize negative conditions by making modifications to the timing of events, such as changing the starting time of a baseball game. EXPO Director Frank LaTorre is serving on the Operations Committee in order to address this and other potential issues of coordination.

In summary, the proposed stadium development represents a positive contribution to the City and to the neighborhood. The stadium will physically complement the existing elements of the Sports Complex. Functionally, it will be located exactly where it belongs, in proximity to other quality sports facilities, and at the site which has been home to Portland baseball from the sport's earliest days in the City. Traffic and parking requirements have been accommodated up to the stadium's maximum capacity, with a plan that reflects the urban center location of the facility. This project will bring tremendous vitality and excitement into Portland. It is right for downtown, the neighborhood, and the City.

response has been overwhelmingly supportive. Subsequently, staff has further refined the information pertaining to parking and traffic for the benefit of the Planning Board. D-26

This proposed stadium project is particularly appropriate to the Hadlock Field site, which is rich with a tradition of baseball in Portland. Baseball has been played in this immediate vicinity since early in the first decade of this century. During the 1920's, there were over 70 amateur baseball teams playing in Portland. Many of the games were played in the Hadlock field area. The baseball field was then called Richardson's Field. During the 1930's and 40's, Portland's minor league team, the Pilots, played at this location, consistently attracting the largest attendance in the league. Baseball has continued to be played at this site up to the present day. The field was renamed after long-time Portland High School coach, Edson Hadlock in the early 1980's. Hadlock Field was renovated in 1987, including seating and field lighting.

The proposed development does not introduce a new type of use to the neighborhood. This Sports Complex area is often the location for events with large numbers of patrons, including events at the EXPO, regional tournaments at any of the facilities, major football contests at Fitzpatrick Stadium (Thanksgiving, University of Maine), and statewide track and field events. The new Hadlock Stadium will appropriately complement the other major sports facilities located within the larger site area. The entire sports complex provides the finest multi-sport facilities in the state, including the EXPO building, the Portland Ice Arena, and Fitzpatrick stadium.

Project Participants

The City Manager appointed a Baseball Operations and Building Committee to oversee the development of the proposed facility. This committee is composed of people representing key functions within the City organization, in addition to other resource people from outside of the organization. Committee members are identified on the front page of this memorandum.

The Building Committee will utilize professional and technical assistance from several sources:

William Whited: Mr. Whited provides the design, engineering, and architectural support to the stadium building. In addition, Mr. Whited is providing technical assistance to the design of the entire facility.

Ellerbe Becket, Inc.: Ellerbe Becket is an architectural/engineering firm specializing in the development of sports facilities, with extensive experience in the development of major and minor league baseball facilities. Ellerbe Becket is providing comprehensive review and analysis of the entire facility to the City.

Grandstand Supplier: The City is currently negotiating with a national supplier of baseball stadium grandstands for the design and construction of the grandstands and seating. The negotiations involve final modifications of facility design and final contract costs, and should be completed within two weeks.

SITE PLAN REVIEW FOR
AN ICE ARENA
ADJACENT TO THE EXPOSITION BUILDING

Submitted to:

City Planning Board
Portland, Maine
June 26, 1984

I. Introduction

The City of Portland and Community Constructors, Inc., have requested site plan review for development for an ice arena adjacent to the Exposition Building in the vicinity of Park Avenue and Deering Avenue.

Seventy eight notices were sent to area property owners of this meeting.

II. Findings of Fact

- Proposed Use: ice skating arena, 900 seat capacity
- Existing Zoning: Recreation Open Space (ROS)
- LDP Zoning: Open Space
- Building Ground Floor Area: 32,000 sq.ft.
- Building Height: 2 stories plus a basement
- Parking: 180 new spaces
- Project Cost: 1,590,000.

At the Planning Board meeting of April 12, 1983, the Board recommended to the City Council a zone change from R-5 to ROS for the Expo area. The City Council approved the zone change on April 20, 1983. The fire house and King Middle School were excluded from the zone change as well as a small triangular shaped portion of the Expo site adjacent to the Maine Center for the Blind.

On May 31, 1984, the ice skating facility was approved by the Board of Appeals as a conditional use (section 14-155-156,474(c)).

A. Existing Uses

Existing uses on the site include the Expo building, athletic fields, various Parks and Public Works buildings, including the greenhouse and the Parks Director's residence (removed in 1983). Under this proposal, all the buildings will be demolished except for the brick "almshouse" which has recently been converted to a City horse barn and storage uses.

B. Vicinity Uses

Abutting uses to the site along Deering Avenue include a gas station, a residence, Maine Center for the Blind and King Middle School. The southerly side of Park Avenue is primarily multi-family residential with a convenience store and a medical office building located near by. There are also several vacant lots along Park Avenue. The corner of St. John and Park Avenue has a more commercial character. Other uses in the area include Deering Oaks Park (ROS) to the east and I-295 to the north.

III. Proposed Development

The proposed ice arena will be located adjacent to the gas station which abuts the property on Park Avenue. This site has been modified from that site approved by the Planning Board last year as adjacent to the Expo building. In the new scheme, an entryway, drop-off and 46 space parking lot is proposed between the proposed ice arena and Expo building.

The new location was selected to take advantage of better soils conditions as well as provide increased parking near the buildings.

A large new parking area will be developed on the site of existing City buildings that will be removed from the site to the rear and parallel to the football stadium. The rear parking lot will have a capacity of 135 parking spaces and will be paved and striped.

Entrance ways to the parking lot will be from Park Avenue and Deering Avenue.

Elevations of the proposed ice arena are brick along the front with metal-clad siding along the other three sides of the pre-manufactured structure. The front facade also contains a variety of building forms.

IV. Staff Review

The site plan has been reviewed for compliance with the review criteria set forth in section 14-527 of the Site Plan Ordinance. The Fire Department and Building Inspections Services have reviewed and approved the site plan. Public Works has reviewed and approved the plan. (see attachment).

In addition to the site plan review criteria, the Planning Board is also required to review this project for compliance with the special development standards for Recreation and Open Space uses (section 14-158). These standards are outlined later in this report.

1. Traffic

William Bray, Traffic Engineer has reviewed and approved traffic related concerns for the site. The main entrance to the ice arena will be from Park Avenue both ingress and egress. An area for loading at the Expo building has been reserved at the existing loading location. A drop-off area immediately in front of the ice arena building entrance and a 46 space parking lot will accommodate most of the parking needs of the facility except during larger spectator games.

A two-way driveway connects this front area with the rear parking lot with an existing passageway behind the almshouse/horse barn.

The rear parking lot would also contain ingress and egress from Deering Avenue, with two egress lanes.

Other traffic - related development includes a minor curb realignment of Park Avenue at the entranceway on Park Avenue to improve the street line configuration at this point as requested by the City Traffic Engineer.

The Zoning Ordinance (14-332) requires one parking space per 5 fixed seats in auditorium, theaters and assembly halls and this requirement has been used for this facility, which contains 900 fixed bleacher seats. A parking requirement of 180 spaces is met.

Pedestrian walkways are proposed along the westerly side of the ice arena from Park Avenue, along the easterly side of the Expo, between the ice arena and almshouse/horse barn, along the northerly side of the rear parking lot parallelling the football stadium and behind the almshouse to the football stadium area.

New fencing is proposed along the rear parking lot to separate the Maine Center for the Blind and the King Middle School from the facility and along the top of the football stadium from the area behind the almshouse to Deering Avenue.

A small horse corral is proposed adjacent to and westerly of the almshouse/horse barn.

These improvements will adequately separate the various conflicting activities in the Sports Complex area.

The Traffic Engineer has approved the proposal as per the attached memo. The four changes he requests have been included on the submitted final site plan.

2. Bulk, location, sewers, sanitary, storm drains.

The proposed structure meets all space and bulk Zoning Ordinance requirements. The ice arena will be 2 stories high which is less than the height of the Expo.

Sanitary sewer needs of the site will be provided by pumping to an existing manhole on Park Avenue sewer line. Public Works has reviewed and approved the drainage plan and stormwater runoff calculations for the project.

The proposed buildings mass, scale and height fits approximately into the neighborhood and as viewed from I-295.

3. Landscaping

A landscaping plan has been approved by staff. The existing large shade trees will be preserved on Park Avenue and near the almshouse/horse barn.

A 6 foot high chainlink fence is indicated along the parking lot and abutting properties. Landscaping will be planted along portions of that fence line. A wood fence will surround the horse corral. The City Arborist has approved the landscaping plan.

Utilities will be underground.

4 Soil and Drainage

The entire driveway and parking area will be paved. Other ground areas not occupied by buildings will be seeded with grass to prevent soil erosion.

5. Lighting

Lighting for the parking lots is addressed by 5-30 foot high utility poles with non-glare luminaire, 250 watt high pressure sodium lamps. Decorative lighting will also be mounted on the westerly side of the ice arena and southerly side of the horse barn for pedestrian walkway lighting.

6. Financial Capability

The City Council has authorized the funding for this facility.

7. Fire

The Fire Department has reviewed and approved fire related concerns for the site plan.

8. Section 14-158 Development Standards for Recreation and Open Space Zone

All development in the Recreation and Open Space Zone shall comply with the following development standards, which shall be reviewed by the Planning Board in conjunction with the Site Plan Review:

- a. All ground areas not used for parking, loading, vehicular or pedestrian areas and not left in their natural state, shall be suitably landscaped.
- b. Natural features, such as mature trees and natural surface drainage ways shall be preserved to the greatest possible extent consistent with the uses of the property.
- c. Loading areas shall be screened and parking areas shall be screened and landscaped so as to avoid a large continuous expanse of paved area.
- d. Buildings and structures shall be sited to avoid obstructing significant scenic views presently enjoyed by nearby residents, passerby, and users of the site.
- e. Storage of commodities and equipment shall be completely enclosed within buildings or provided with screening by a fence, wall, or landscaping.
- f. The outer perimeter or playfields, playlots, and other active recreational areas shall be screened, or shall be located a reasonable distance from any residential use.

- g. Off-street parking shall conform to the requirements of Division 20 where applicable. . . Otherwise, off-street parking adequate to serve projected employee and visitor needs shall be provided. . . Parking need projections provided by the applicant or the Planning Department should be considered in the review.

V. Staff Summary

The Planning staff finds that the proposal meets the requirements set forth above as presented.

Attachments

PLANNING REPORT #46-87

PLANNING DEPARTMENT REPORT
PORTLAND SCHOOL DEPARTMENT MODULAR CLASSROOMS
CONDITIONAL USE

Submitted to:

Portland Planning Board
Portland, Maine

May 26, 1987

I. INTRODUCTION

The Portland School Department is requesting conditional use approvals for modular classrooms at five Portland elementary and middle schools. The classrooms were deemed necessary by the school board to alleviate crowding at these schools. They are proposed to be in place for two years.

The schools are in the R-2, R-3 and R-5 zones. They are Lyseth Elementary, Moore Middle, Baxter Elementary, Jack Elementary and King Middle. A location plan is attached as #1. Each unit will be 24 x 56 feet, made of grooved wood siding with ten windows each. Each unit will hold two classrooms and each classroom will have two exits.

This hearing was advertised in the Guy Gannett newspapers of May 18, 1987 and notices were mailed to landowners in the area. The plans were reviewed under conditional use standards for each zone and evaluated under the pertinent site plan standards. The plans are attached to this report.

II. SITE LOCATION

The five sites were chosen during site visits made by representatives of the School Department, School Board, the applicant's engineer, the City's Building Inspection Division, Fire Department and Planning Department. Because of the fire wall rating of each unit, the Inspection Services Division requires that the units be at least 30' from the principal structure or school. The Fire Department requires access to the units and proximity to a hydrant. Each location proposed has met the requirements of those departments.

At Lyseth-Moore the Fire Department required a new hydrant between the schools. This will be installed as part of the access drive reconstruction being planned by the City. In addition, the Fire Department requested certification from a registered engineer that the asphalt surface behind the Baxter Elementary School could support emergency vehicles. Pinkham and Greer, consultants from Falmouth, have provided a letter of certification, which is attached as #2.

III. SITE PLAN REVIEW

The plans have been reviewed by City staff using pertinent site plan standards. The Public Works Department and City's Traffic Engineer have reviewed and do not oppose the plans. Each building will have electrical and fire alarm service from the existing school buildings.

Lyseth Elementary School R-2 zone

The classroom at Lyseth is proposed to be put between two wings of the school building on an asphalt area. The site will be accessible on a new 20' wide fire land to be constructed beside the school. This proposal is acceptable to the Fire Department.

The City has scheduled for this summer a reconstruction of the school's parking lot and drive, including changes in the site layout. The proposed fire hydrant between Lyseth and Moore schools will be paid for by the school department and installed by the Portland Water District. A possible condition of approval is that if the city construction work is not complete by the time the schools open in September, the school and Water District install the hydrant prior to completion of the paving project.

Moore Middle School R-2

This unit is located between the school and the parking lot. It is concealed from public view from the front by the school building. Again, no landscaping is proposed. The unit is 320 feet from the nearest residential area.

Baxter Elementary School R-5

An asphalt play area was chosen as the site at this school. The applicant has provided a letter to the Fire Department certifying that the substrate will hold emergency vehicles. This area is not used by vehicles so the location is not expected to impede vehicular circulation. The unit is approximately 140' from the nearest residential area. No landscaping is proposed.

Jack Elementary School R-3

This unit will be located in a fenced asphalt play area encircled by the school building and abutting North Street. There is no landscaping in this area and none is proposed. Carmela Barton, City Arborist, has suggested landscaping at this school (see attachment #3). In the past the pavement has been used as a snow emergency parking area in the winter. The school would continue to use part of the area in that capacity.

King Middle School R-5

This unit is proposed for the south corner of the parking area, 60' from the school building. The location leaves adequate room for traffic circulation. Nine spaces will be lost with the addition of this unit, leaving 61 spaces. The school requires 32 spaces.

The King site is directly adjacent to a residential area. While there is a sparse vegetative buffer on-site now, the Board may wish to condition a more substantial buffer.

IV. CONDITIONAL USE STANDARDS

The following conditional use standards apply:

Upon a showing that a proposed use is a conditional use under this article, a conditional use permit shall be granted unless the board determines that:

- a. There are unique or distinctive characteristics or effects associated with the proposed conditional use;
- b. There will be an adverse impact upon the health, safety or welfare of the public or the surrounding area; and
- c. Such impact differs substantially from the impact which would normally occur from such a use in that zone.

V. TIME LIMIT

The School Department proposes to use these units for no longer than two years. At the workshop session the board asked whether a conditional use permit could be subject to a time constraint. Jim Katsiaficas, Associate Corporation Counsel, has determined that it may. His memo is attached as #4.

VI. MOTIONS FOR THE BOARD TO CONSIDER

On the basis of plans and materials submitted by the applicant, and the findings of this board reflected in the information provided in Planning Report #46-87 relevant to standards for conditional use permit, and/or other findings as follow:

The Board finds that:

- 1. The plan is in conformance with the applicable portions of the site plan ordinance.

A. Conditions of Approval

- i. That landscaping be provided as a visual buffer around the modular units, per specifications of the City Arborist.

- B. The plan is in conformance with the conditional use standards.

List of Attachments

- 1. Location plan and site plans
- 2. Surface strength study
- 3. Memo from Carmela Barton
- 4. Memo from Jim Katsiaficas

HADLOCK FIELD PARKING STUDY*Parking Space Totals**March 1993*

<u>Lot Description</u>	<u>Total Spaces with "Tandem"</u>	<u>Total Spaces without "Tandem"</u>
1. Fitzpatrick Lot	403	106
2. King School	215	74
3. Hadlock Field, Lot B	186 (108)*	106 (58)*
4. Hadlock Field, Lot A	50	50 (29)*
5. St. James St. Lot	58	58
6. Deering Ave.(Park to Washburn)	141	141
7. State St. (Park Entr. to Exit-WS)	45	45
8. St. James St. (Entire)	209	209
9. UMO Bedford St. Lot	500	500
10. Me. Med Gilman St. Lot	500	500
<i>TOTALS</i>	<u><u>2307 (2229)*</u></u>	<u><u>1789 (1720)*</u></u>

Total spaces required for a "capacity" ballgame total 1693. Both totals above exceed the capacity demand according to Professional Baseball Standards. We have included "tandem" parking as an alternative for the "Operations Handbook". We recommend having the first (3) lots used for "tandem" parking for the initial season. Although this type of parking may not be desirable to all patrons, it is very successful for other events in our area and at Fenway Ballpark.

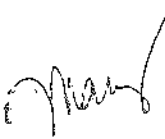
Actual parking habits will be observed and closely monitored by the City. The possibility of converting to "normal" parking in these lots remains an viable option (while maintaining the PBA parking requirements).

* Includes corrections for the latest Site Plan as of March 19, 1993

CITY OF PORTLAND

Parks & Public Works Department - Traffic Division

M E M O R A N D U M

To: Portland Planning Board Members
From: William J. Bray and Mary Ann Theriault 
Date: February 5, 1993
Subject: Response to the Hadlock Field Workshop

Several traffic and parking issues were identified at the January 26th Workshop relative to the proposed Hadlock Field Baseball Stadium. The following responses appropriately address each of the identified concerns:

1. PEDESTRIAN ACCESS TO/FROM THE BALLPARK

Primarily, pedestrian access to and from the Ballpark is provided by existing sidewalks on one or both sides of all the city streets within the study area. Along Park Avenue, crosswalks exist at all signalized intersections as well as Park Avenue/Gilman Street and Park Avenue/Weymouth Street.

Within the proposed "tandem" parking areas, pedestrian pathways are provided as follows:

(1) Fitzpatrick Lot has an existing sidewalk along the northside of the parking area. The Fitzpatrick Lot will need a short, 50 foot section of sidewalk installed to connect the existing EXPO sidewalk with the Fitzpatrick Lot sidewalk. We are recommending this project for completion in the summer of '93 by City forces.

(2) Hadlock Field's proposed site plan includes an expanded pedestrian walkway area connecting both Park Avenue and Hadlock Field, Lots A & B.

(3) King School Lot will use cones and portable signage to temporarily delineate pedestrian walkways. A detailed layout will be included in the Operations Handbook.

All other proposed off-street parking sites are currently bordering one or more city sidewalks. A thorough inspection of all sidewalks will be conducted in the spring. Improvement recommendations will be forwarded to the Parks and Public Works Director for future Capital Improvement consideration.

Response to the Planning Workshop...

In the Traffic Division's FY94 budget, we have proposed pedestrian signal upgrades along all of Park Avenue. This would include new informational signage for the better understanding of walk, flashing don't walk, and steady don't walk indications.

2. BICYCLE ACCESS TO/FROM THE BALLPARK

By state law, bicyclist must obey the same rules as vehicular traffic. We have already demonstrated and documented adequate mobility of traffic; therefore, bicyclist mobility is also considered satisfactory. We will monitor and introduce any new strategies adopted by the City or State Planning Departments. The proposed site plan for Hadlock Field includes a designated area for bike racks.

3. DIRECTION OF OUT-OF-TOWN VISITORS

Generally, traffic destined for the stadium from out-of-town communities will approach the site from three specific directions: NORTH, SOUTH and WEST. The following directions are a draft to be further coordinated with the Ballteam's personnel:

FROM THE NORTH

Traffic approaching from the north take I-295 to the Franklin Arterial or Forest Avenue-Eastbound ramps. Take a right at the first traffic signal for either ramp and follow the signs to the designated parking areas for the ballpark.

FROM THE SOUTH

Traffic approaching from the south take I-295 to the Congress Street ramp. Take a right at the second traffic signal for the Gilman Street parking garage or take Route 1 - Veterans Bridge straight through to Valley Street. Follow the signs to the designated parking areas for the ballpark.

FROM THE WEST

Traffic approaching from the west take any of the following arterial highways: Congress Street, Forest Avenue, Washington Avenue or Brighton Avenue. Follow the signs to the designated parking areas for the ballpark.

Our primary goal when providing directional information will be to divert the site traffic onto as many arterial roadways as possible to lessen or dilute the measured traffic impact.

The proposed Traffic and Parking Operations Handbook will contain a detailed signage plan for arrival and departure ballpark traffic. Appropriately located informational signs will be used to direct traffic to the Stadium, designated parking areas, major highway arterials, METRO facilities, etc.

Response to the Planning Workshop continued...

Several additional concerns were raised by Herb Adams, House Representative, District 27, in a letter to Jadine O'Brien. Each of his issues, that were not detailed previously in this memo, are addressed as follows:

7. NO PARKING IN DEERING OAKS

Joe Gray, Director of Planning and Urban Development, addressed this issue at the January 26th Planning Workshop. In summary, the City Manager has requested funding through the upcoming HCD budget to retain a Consultant to develop a long-range Master Plan for Deering Oaks. The Consultant will be required to examine access control, parking, and general circulation. This comprehensive plan will be completed prior to the opening of the baseball facility.

Again, Our Hadlock Field Parking Study conclusively demonstrates sufficient off-street and on-street parking supply within the half-mile study area without the use of Deering Oaks roadways for parking.

8. PARKING IN ASSIGNED AREAS/ENFORCEMENT IN UNASSIGNED AREAS

The Operations Handbook will identify the designated parking areas. Permanent and portable "Baseball Parking" signage will be designed to encourage all Stadium patrons to the "preferred" designated parking areas.

Parking control personnel will be assigned to monitor and enforce on-street parking regulations during stadium events. As in the Notification Approach, we will coordinate efforts with the City Parking Control Manager.

9. MAINE SCHOOL FOR THE BLIND

The City has already contacted Mr. Robert Crouse, Executive Director of the Maine Center for the Blind, regarding gametime parking. They are strongly in favor of coordinating parking supply during all scheduled ballgames as they have for many special events throughout the year. Therefore, they will be aware of all home game schedules. We will contact Mr. Crouse again for review of the Operations Handbook.

They do not feel that providing ballgame parking in their own lot adversely affects the safety of their patrons/students. The additional traffic utilizing King School parking "attendant-controlled" lot will be for short "pre-game" time periods. The "post-game" traffic will depart through the proposed Deering Avenue throughway, to be constructed prior to opening day in April '94.

Response to the Planning Workshop...

The following information was added to further support the approach and analysis as performed by the City Traffic Division:

10. THE INTERSECTION OF CONGRESS STREET and ST. JOHN STREET

Traffic turning movement counts were conducted at the intersection of Congress Street and St. John Street from 6:00pm to 9:00pm on a Friday night, and from 12:00pm to 4:00pm on a Saturday afternoon.

The previously submitted Traffic Study under Mobility Analysis, Page 7, described Level of Service and the analysis process of ballpark traffic flow. A capacity analysis of Congress Street and St. John Street was performed for both the "weekday" evening and "weekend" afternoon condition. Again, separate analyses were calculated for the "pre-game" and "post-game" traffic conditions.

The results of the analyses reflected Level of Service "B" for all time periods. Enclosed please find the "revised" Table 4 depicted the detailed results of our calculations.

11. TRAFFIC and PARKING STUDY PEER REVIEW

Eaton Traffic Engineering has been retained to conduct a peer review of the Traffic and Parking Study as prepared by the City Traffic Division. A final report summarizing the findings of the review will be available prior to the public hearing. To date, there have been no outstanding omissions or discrepancies in the reports.

Hopefully, this memo and the previously submitted Traffic and Parking Study Reports provide the adequate documentation for review of the Hadlock Field Baseball Stadium. Once again, "specific details" will be provided in the upcoming Operations Handbook to be coordinated with City and Ballpark personnel.

SIGNALIZED INTERSECTION
CAPACITY SUMMARY

LOCATION	<u>1994 BASE CONDITION</u>				<u>1994 BASEBALL GAME CONDITION</u>			
	"WEEKDAY"		"WEEKEND"		"WEEKDAY"		"WEEKEND"	
	ARRIVAL PEAK	DEPARTURE PEAK	ARRIVAL PEAK	DEPARTURE PEAK	ARRIVAL PEAK	DEPARTURE PEAK	ARRIVAL PEAK	DEPARTURE PEAK
PARK AVE/ DEERING AVE	<u>17.8</u> C	<u>17.3</u> C	<u>16.5</u> C	<u>15.9</u> C	<u>16.8</u> C	<u>16.3</u> C	<u>19.2</u> C	<u>18.2</u> C
PARK AVE/ ST. JOHN ST.	<u>18.6</u> C	<u>17.4</u> C	<u>22.9</u> C	<u>20.3</u> C	<u>18.9</u> C	<u>21.2</u> C	<u>23.6</u> C	<u>25.9</u> D
CONGRESS/ ST. JOHN ST	<u>11.3</u> B	<u>10.5</u> B	<u>12.2</u> B	<u>11.7</u> B	<u>11.5</u> B	<u>10.9</u> B	<u>12.8</u> B	<u>12.1</u> B

(1) (1)

(1) Assumes minor changes in both signal cycle and split timings

TABLE 4

EROSION and SEDIMENT CONTROL

A. GENERAL CONSTRUCTION DETAILS

The equipment anticipated to be used for the construction includes the following: backhoe, bulldozer, loader, and trucks. The following measures will be undertaken to provide maximum protection to the soil, water, and abutting lands:

1. Siltation fence will be installed across the slope on the contour at the downhill limit of the work as protection against construction related erosion.
2. All siltation fence will be inspected by the contractor on a weekly basis or following any significant rainfall (1/2 inch or more) or snowmelt. All damaged siltation fence will be repaired and/or replaced immediately. Trapped sediment will be removed before it has accumulated to one half of the installed siltation fence height. Siltation fence no longer serviceable due to sediment accumulation will also be repaired and/or replaced as necessary.
3. If final seeding of the disturbed areas is not completed by September 15 of the year of construction, then these areas will be graded, smoothed, and seeded to a winter cover crop of rye at a rate of 3 lbs. per 1,000 sq. ft. The following will be incorporated into the soil prior to rye seeding: ground limestone at a rate of 3 tons per acre, followed by a 10-10-10 fertilizer at a rate of 600 lbs. per acre. Hay mulch will be applied at a rate of 100 lbs. per 1,000 sq. ft. following seeding. If the rye seeding cannot be completed by October 1, then on that date hay mulch shall be applied at the rate of 2 tons per acre to provide winter protection. If rye does not make adequate growth by December 1st, then on that date, hay mulch shall be applied at the rate of 100 lbs. per 1,000 sq. ft. A suitable binder such as Curasol or RMB Plus shall be used on hay mulch for wind control. Biodegradable netting will be installed on steep slopes (3:1 and steeper) and on areas of concentrated flow.
4. Intercepted sediment will be returned to the site and incorporated into landscaped areas.
5. All areas designated as grass areas per site plan will be loamed and sodded.

B. MONITORING PROGRAM

Sediment and erosion control structures will be inspected continually by the contractor, and all structures damaged by construction equipment, vandals, or the elements will be repaired immediately. Following rainstorms, the site and all structures will be inspected for erosion and damage. All damaged structures will be repaired and/or additional erosion control structures will be installed prior to continuing the construction.

Following the final seeding the site will be inspected to ensure that the vegetation has been established. Reseeding will be carried out, with follow-up inspections, in the event of any unsatisfactory growth.

After the project area has stabilized, the contractor shall remove all siltation fence and any other temporary erosion control measures.



R. W. Gillespie & Associates

CONSULTING GEOTECHNICAL & ENVIRONMENTAL SPECIALISTS

December 30, 1992

Mr. Robert T. Ganley, Manager
City of Portland
389 Congress Street
Portland, Me 04101

Subject: Geotechnical Investigation
Proposed Hadlock Field Stadium
Portland, Maine
Project No. 557-01

Dear Mr. Ganley:

In accordance with our agreement of 03 December 1992 we have conducted a geotechnical investigation at the above referenced site. We discussed our findings with your engineering department (Michael Claus, P.E.) and provided preliminary recommendations.

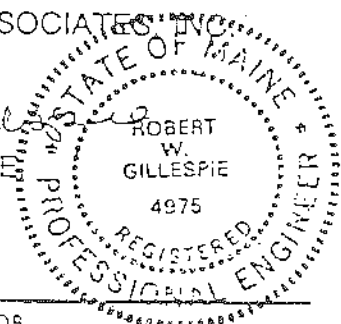
Soils at the site consist of fill underlain by an intercalated clay-peat deposit. These strata extend to depths of 10 to more than 20 feet and are followed by loose to dense sands. Refusal with augers and/or standard penetration testing equipment was met at depths of 28 to more than 93 feet below existing grade.

Driven treated timber piles, founded in the sand stratum, are recommended for support of the structure. Lengths can be varied to suit the loading condition at structural points up to a maximum of 25 tons per pile.

We have enjoyed serving you on this project and look forward to a continuing relationship. If you have any questions please contact us.

Very truly yours,
R.W. GILLESPIE & ASSOCIATES, INC.


Robert W. Gillespie, P.E.



RWG:mah
In Fifteen Copies

REPORT
OF
GEOTECHNICAL INVESTIGATION
PROPOSED HADLOCK FIELD STADIUM

TO
CITY OF PORTLAND
PORTLAND, MAINE

PREPARED
BY
R.W. GILLESPIE & ASSOCIATES, INC.
SANFORD, MAINE

DECEMBER 1992

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- Plates 2-5. Grain Size Distribution

R. W. Gillespie & Associates

INTRODUCTION

In this report we present the results of our geotechnical investigation for the proposed Hadlock Field Stadium in Portland, Maine. The purpose of the investigation was to obtain information regarding subsurface conditions and soil properties on which to base recommendations for design and construction of foundations and slab on grade floors.

The proposed stadium will replace the existing grandstands and create an arc of approximately 350 feet behind the backstop and along the first and third base lines. Total area is on the order of 33,000 square feet, and construction will be steel frame and concrete. Based on data provided to us by the structural engineer, column loads are a maximum of 45 kips with most less than 37 kips. Several small buildings or function areas will be incorporated into the stadium at ground level. Some interior masonry walls or partitions may be included in these interior areas.

SITE INVESTIGATION

A truck mounted hollow stem auger drill was used to investigate and sample subsurface soils. Seven test borings were made at the approximate locations shown on Figure 1, attached. The eighth boring could not be drilled at the desired location because of a very heavy concentration of utilities including electrical, water, sewer, and gas lines. Standard penetration resistance tests were made at discrete intervals on the engineers judgement and recovered samples were preserved in airtight jars for transport to our laboratory. Ground water levels were measured in each boring after removal of the augers and recorded on the boring log. Locations of borings were determined by tape measurement from existing landmarks.

LABORATORY INVESTIGATION

All samples were visually examined and when necessary reclassified using the procedures of the Unified Soil Classification System. To aid in classification and to obtain an estimate of physical and engineering properties water content, grain size distribution, and direct shear tests were performed on selected samples. Water content and grain size tests provide a benchmark for comparison of field classifications, and are an indicator of certain properties such as dilatancy or liquefaction potential. Direct shear is a measure of internal strength for granular soils such as sands.

The results of field and laboratory tests are presented on the borings logs, Appendix A, and Plates 1 through 5. Sampling procedures are described in Appendix B.

R. W. Gillespie & Associates

SITE AND SUBSURFACE CONDITIONS

Site

The project is located on Park Avenue at the existing Hadlock Field in Portland, Maine. The stadium portion of the site is bounded by the Portland Expo Building on the east, the stadium parking lot on the south and west, and the playing field on the north. The general surrounding area except the playing field is paved or otherwise covered by structures. Topography is flat but drainage appears to be to the west in the parking lot and the north and west in the playing field. Numerous utilities cross the site including electrical, telephone, natural gas, water, and sewer. There is a concentration or confluence of them in the vicinity of the east end of the third base dugout.

Subsurface

Soils at the site are composed of fill underlain by peat-clay mixtures which are, in turn, underlain by sands and silty sands. Bedrock underlies the sands at variable depths. The fill is a heterogeneous mixture of ash, sand, rubble, organics, and unidentifiable decomposing matter. Based on our borings and those which were performed for the light standards in 1984, ash and sand are the major components of the fill. Thickness of the fill ranges from approximately 9 to 20 feet at the boring locations. The fill was placed in the area over a period of time beginning with organization of Portland in order to provide additional land space.

The fill is underlain by peat and silty clay which occur as separate stratum and as intercalated deposits. That is, the peat and clay were deposited as intermixed layers without the discretization observed in other locations. The clay is silty, has low to moderate plasticity, and is soft to very soft with standard penetration resistance (N) values of less than 5 blows per foot. By experience, compressibility is high and made somewhat unpredictable by the inclusion of peat. The peat is a non-fibrous variety with slight sand content. It is soft, compressible, and has low shear strength. The stratum occurs as a distinct unit in the vicinity of B-1,4, and 5 but thickness pinches to the northwest while intercalation occurs to the east and southeast. Combined thicknesses of the clay and peat range from less than one foot to about 23 feet at the boring locations.

The sand stratum is thought to be ice contact and possibly alluvial in origin. The sand is composed of silty to clean, fine to medium grained fractions. Grains are subrounded to rounded indicating lengthy transport time and/or distances during the depositional processes. N values range from single digit values to more than 40 blows per foot with most in the 15 to 20 range. In a general sense N values increase with depth. The direct shear test yielded a friction angle of 39° for the peak and 31° for the residual shear which are consistent with the soil type and relative density as indicated by standard penetration test values. The sand is a good load bearing stratum.

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Six of the seven borings were drilled to refusal by either augers or standard penetration equipment. Only B-6 which was taken to a depth of 93 feet did not reach refusal. In the other borings refusal was met at depths of 28.5 to 73 feet. Based on other work in the area refusal is thought to be bedrock.

Ground Water

Ground water was encountered in all borings at the time of drilling. Depths ranged from 10 to 16 feet at boring locations but other data available to us suggest that the levels of 6 to 7 feet below grade are possible. Irrigation of the playing field also exerts an influence on the levels but the impact is not well defined.

Fluctuations of the ground water level may occur due to variations in rainfall, temperature, tides, and other factors not evident at the time measurements were made and reported herein. An evaluation of these factors is beyond the scope of this investigation.

EVALUATION OF GEOTECHNICAL DATA

During the proposal phase of the project we were advised that shallow foundations were desirable if subsurface conditions were amenable to that system. Therefore, we have evaluated both shallow and deep systems to provide the rationale for the recommended foundation type. Figure 2 presents a projected curvilinear subsurface profile along the centerline of the stadium which provides insight for the analyses which follow.

The fill and clay/peat strata are variable in composition and properties which make settlement prediction difficult. Spread footings would bear in the fill and could settle as much as 2 to 4 inches from compression of the fill, peat, and clay with contact pressures of 1.5 to 2.5 kips per square foot. This estimate assumes that the peat contributes little to the movement. If the structure loads were distributed over the area of the stadium with a mat foundation then settlements could approach values similar to those of spread footings because the zone of influence is much larger. That is, the width of the mat is large in relation to the depth and thickness of the compressible zones. Settlements of these magnitudes are considered excessive for structures of this type and alternative systems are needed to reduce the movement.

Deep foundations utilizing the sand as a load carrying stratum could include steel H-piles, cast in place concrete, precast concrete piles, and treated timber piles. Since loads are relatively light the need for high capacity piles bearing on bedrock is obviated. Timber piles are common in southern and central Maine, and local contractors are equipped to install them. Therefore, they tend to be competitive in cost. Nominal 12 inch diameter (14 inch butt, 10 inch tip) treated timber piles driven into the sand would have

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capacities of 16 to 25 tons depending on length. Since the sand is relatively dense the pile would function as both a frictional and end bearing unit. Settlement of the piles is expected to be less than one quarter inch.

Lateral load resistance was evaluated using the methods of Broms (1964) for a single fixed-head pile. A ground line movement of the pile and cap of 0.25 inches was assumed as the limiting factor. Lateral capacities range from 0.6 to 1.0 kips depending on pile length.

Floor slab loads are expected to be light which is beneficial since a pile supported floor is expensive. If the floor load is 150 pounds per square foot or less, exclusive of the weight of concrete, then settlements are expected to be less than one inch. Storage or other heavily loaded floor spaces will require pile support to minimize settlement problems.

RECOMMENDATIONS

Foundations

1. The proposed stadium should be supported on driven timber piles designed for the capacities presented in Table 1 below.

TABLE I

SUMMARY OF PILE CAPACITIES

Pile Length, Ft.	Axial Capacity, tons	Lateral Capacity, kips
50	25	1.0
45	20	0.8
40	16	0.6

Piles should then be spaced no closer than 4 feet center-to-center.

2. Timber piles should have a butt diameter of 14 inches and a tip diameter of 10 inches. All piles should meet the requirements of ASTM D25. Piles should be southern pine or Douglas Fir. Other wood will be considered on a case-by-case basis.

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3. Pressure treatment should be in accordance with AWPI Standard C3. Cut or otherwise exposed surfaces should be treated with a preservative solution such as Penta.
4. The pile hammer should be a single or double acting air or diesel hammer in good working condition. It should be capable of producing 10,000 to 15,000 foot-pounds of energy at rated operating conditions.
5. Piles should be driven to the lengths shown in Table 1 but in no case should they be driven to a resistance of more than 5 blows per inch of penetration with the recommended hammer.
6. Steel banding should be used to protect the tip and butt during driving.
7. Any fill needed to achieve grade should be placed prior to pile installation.

Floor Slabs

8. Slab on grade floors which meet the load criteria presented in the "Evaluation" section should be supported on 12 inches of structural fill meeting the gradational requirements presented below.

Screen or Sieve Size	Percent Passing
6 inches	100
3 inches	90 - 100
No. 4	35 - 70
No. 40	5 - 35
No. 200	0 - 5

9. Floors which are heavily loaded should be supported on piles and grade beams.

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Seismic Loading

10. The on-site soils classify as S_3 according to the latest edition of the BOCA National Building Code. A seismic "S" factor value of 1.5 is appropriate for design.

Temporary Excavations

11. The Owner and the Contractor should make themselves aware of and become familiar with applicable local, state, and federal safety regulations, including the current OSHA Excavation and Trench Safety Standards. Construction site safety generally is the sole responsibility of the Contractor, who shall also be solely responsible for the means, methods, and sequencing of construction operations. We are providing this information solely as a service to our client. Under no circumstances should the information provided below be interpreted to mean that R. W. Gillespie & Associates is assuming responsibility for construction site safety or the Contractor's activities; such responsibility is not being implied and should not be inferred.
12. The Contractor should be aware that slope height, slope inclination, or excavation depth (including utility trench excavations) should in no case exceed those specified in local, state, or federal safety regulations, e.g., OSHA Health and Safety Standards for Excavations, 29 CFR Part 1926, or successor regulations, such as regulations are strictly enforced and, if they are not followed, the Owner, Contractor, and/or earthwork and utility subcontractor could be liable for substantial penalties.

Fill depths of 13 feet may also be present in certain areas of the site. The following slope inclinations are presented for planning and construction purposes only.

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Soil Type	Depth of Excavation feet	Slope Horizontal to Vertical
Fill	0-4	1:1
	4-8	1½:1
	deeper than 8 or below ground water	shored
Peat/Clay	0-4	1:1
	4-6	1½:1
	deeper than 6 below ground water	2:1 shored
Sand	0-4	¾:1
	4-8	1½:1
	deeper than 8 or below ground water	shored

If any excavation, including a utility trench, is extended to a depth of more than twenty (20) feet, it will be necessary to have the side slopes designed by a professional engineer registered in the state where construction is occurring.

As a safety measure, it is recommended that all vehicles and spoil piles be kept a minimum lateral distance from the crest of the slope equal to no less than 100% the slope height.

The exposed slope face should be protected against the elements.

We should be retained to monitor the soils exposed in all excavations, and provide engineering services for such slopes. This will provide an opportunity to monitor for such types encountered and to modify the excavation slopes as necessary. It also offers an opportunity to verify the stability of the excavation slopes during construction.

13. Site grading should provide positive drainage away from constructed facilities during and after construction. We cannot overemphasize the importance of this during construction.

M



STATE OF MAINE

DEPARTMENT OF ENVIRONMENTAL PROTECTION

JOHN R. McKERNAN, JR.
GOVERNOR

RECEIVED

DEAN C. MARRIOTT
COMMISSIONER

DEBRAH RICHARD
DEPUTY COMMISSIONER

May 6, 1993

MAY 10 1993

PORTLAND PLANNING OFFICE

Richard Knowland
City of Portland
55 Portland Street
Portland, Maine 04101

Re: Hadlock Field

Dear Rick:

This is to inform you that the Department of Environmental Protection has reviewed the application of The City of Portland for the expansion of Hadlock Field in Portland. This Department is satisfied that the conditions of Title 38, Section 489-A for Municipal review of development has been met by The City of Portland.

This finding is dependent on the understanding that the City has incorporated into their plans for the expansion the traffic mitigation measures outlined in the memo dated May 6, 1993, from Paul J. Minor, M.D.O.T. - Planning to Linda Kokemuller, D.E.P. - Land Bureau. Some of these measures require follow-up monitoring and it is our understanding that this monitoring will be done and that all necessary corrective actions indicated by the results of that monitoring will be implemented. Furthermore if the City intends to allow activities to occur at the adjacent facilities, Fitzpatrick Stadium, Portland Expo, Ice Arena, and King School, concurrently with activities at the ballpark additional traffic studies would need to be undertaken. I also note that M.D.O.T. still does not agree with the methodology of the City's high accident location (HAL) analysis and commented that all HAL's should continue to be monitored.

Sincerely,

Mark T. Margerum
Licensing Coordinator
Division of Site Location Review

cc: Paul Minor, MDOT
Linda Kokemuller, DEP

SO. MAINE PROPERTY MANAGEMENT
Suite 136
295 Forest Avenue
Portland, Maine 04101-2000
(207) 773-9305

March 7, 1994

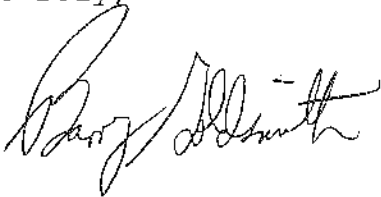
Mr. Joseph Gray
Director of Planning and Urban Development
City Hall, Room 211
389 Congress Street
Portland, Maine 04101

Dear Mr. Gray,

As Property Managers of 113 Gilman Street, we have some concerns about parking during Sea Dog games. We are thrilled to have professional baseball in our backyard, however, we feel that parking during games on Gilman Street will create a major problem for our tenants. With limited parking already in existence and our tenants relying on street parking, we would like to see resident permit parking on Gilman to protect our tenants parking privileges.

Our major concern is that if tenants become unhappy, they will leave, which will put a major burden on the property owner.

Sincerely,

A handwritten signature in cursive script, appearing to read "Barry Goldsmith". The signature is written in dark ink and is positioned above the typed name.

Barry Goldsmith
Property Manager

402 Amherst Street - Suite 300
Nashua, NH 03063-1287
603 595 1614
603 595 2414 FAX

 GUILFORD TRANSPORTATION INDUSTRIES, INC.

March 3, 1994

Dear Sir:

Either the Boston and Maine Corporation or the Maine Central Railroad has recently received a notice concerning a property owners intent to seek zoning relief pursuant to your local zoning ordinance (copy enclosed).

Please be advised that although the abutting railroad has not had an opportunity to review the proponent's plans, to the extent said plans involve construction within 25 feet from the centerline of an abutting railroad track or to the extent such plans indicate any type of crossing of railroad property that the abutting railroad hereby objects to said plans.

This letter should not be considered a waiver of any other objections which the appropriate railroad may have to the proponent's plans.

Sincerely,



Leonard A. Lucas
Vice President

LAL:img
Enclosure

TO RESIDENTS AND PROPERTY OWNERS IN THE VICINITY OF
265 PARK AVENUE - HADLOCK FIELD

On Tuesday, March 8, 1994, the Portland Planning Board will review the Field Operations Handbook (Parking Management Plan) for the Hadlock Field Baseball Stadium in the vicinity of 265 Park Avenue. A condition of Planning Board Site Plan approval for the Hadlock Field Baseball Stadium states that the Field Operations Handbook (Parking Management Plan) must be submitted to the Planning Board for review and comment.

The meeting is a workshop session and is scheduled to begin at 3:30 p.m. in Room 209, City Hall, 389 Congress Street, Portland, Maine. The workshop is an opportunity for the applicant to present a plan to the Planning Board in an informal session, which is open to the public. Public comments are not generally received at the workshop meeting. If you wish to submit written comments on the proposal, please address your comments to Joseph E. Gray, Jr., Director of Planning and Urban Development, City Hall, Room 211, 389 Congress Street, Portland, Maine 04101.

Alexander Jaegerman
Chief Planner
/RK

July 13, 1993

Portland Planning Board
Portland, Maine

submitted to:

CITY OF PORTLAND, APPLICANT
REVIEW OF CONDITIONS OF APPROVAL
BADLOCK FIELD BASEBALL STADIUM

I. Introduction

On March 23, 1993, the Planning Board approved a site plan for the new Hadlock Field subject to a series of conditions. These conditions included submission to the Planning Board of a final facade elevation of the ballpark, a final landscaping plan and an exterior lighting plan. The City of Portland is submitting revised plans to address these conditions.

310 notices were sent to area residents.

II. Landscaping

Since the Board's last workshop on this item, the applicant has had Portland Design Team review the previously submitted landscaping plan and has offered recommendations. These recommendations have been incorporated in the revised site plan. A description of the landscaping, plaza and lighting design elements are shown as Attachment A. The revised landscaping plan and planting list is shown as Attachment B. The previous landscaping plan and plant list is shown as Attachment C.

A double row of trees is proposed between the sidewalk and the ballpark along Park Avenue. Like the original plan, the large existing Maple tree adjacent to the small parking area and the Expo will remain. There have been some refinements to landscaping on the westerly side of the stadium, including enlargement of one of the landscaped islands, adding understory plantings in all of the islands and increasing the screening along the westerly property line (former firehouse now medical offices.) Three planters are shown on the plan, one near the main entrance and two near the small parking lot. These will be designed as a seating wall.

The plaza area in front of the stadium will have a concrete surface. The concrete will be divided into eight foot squares. The configuration of the concrete plaza adjacent to the Park Avenue sidewalk has been rounded near the entrance to reflect the shape of the stadium. Granite accents will radiate from the stadium support columns along the plaza surface to the brick sidewalk along Park Avenue.

III. Exterior Lighting

The exterior lighting of the plaza and parking lot area has now been refined. The lighting plan for the plaza has been developed by Portland Design Team to reinforce the design elements and character of the plaza and the stadium.

An architectural light fixture (Candela series) for the plaza has been selected (see Attachment A-7). Eight of the fixtures will be mounted on poles within the plaza area. Five of the light fixtures will be spaced between the street trees along Park Avenue which enhances the street edge character of the site. The fixtures will be mounted on 15 foot high poles with banner arms to allow the placement of banners. Eleven of the luminaires will be mounted along the facade of the stadium, 2 of them at the entrance, the remainder at intervals lining up with the granite accent strips within the paved surface.

OUTLINE OF PROPOSED LEASE AND CONTRACT
BETWEEN THE CITY OF PORTLAND, MAINE AND
MR. DANIEL BURKE FOR PREMISES OWNED BY
THE CITY AND KNOWN AS HADLOCK FIELD AND
THE EXPOSITION BUILDING LOCATED AT
271 PARK AVENUE, PORTLAND, MAINE

1. LEASED PREMISES.

The City shall provide a facility that meets the mandatory requirements established for minor league baseball as shown in Exhibit A attached to this outline. The facility shall include but not be limited to a stadium, baseball field, clubhouse facilities, office space, and any other space or structures needed to meet the requirements. The leased facility shall include concession stands which shall be equipped at the expense of the ball club. Optional facilities may be provided at a later date by mutual agreement between the City and the ball club.

2. BALL FIELD MAINTENANCE.

The City shall be responsible for the maintenance of the ball field. The ball field shall be maintained in accordance with the standards established for minor league baseball. Ball field maintenance shall be performed in accordance with the agreement established between the City and the ball club as contained in Exhibit B which will be provided at a later date.

3. FACILITY AVAILABILITY.

- a. Home games. The facility shall be available to the ball club for home games from April 1 to September 15 of each year. On game days, the ball club shall have exclusive control of the field from six hours before game time until the end of each game.

Use of the facility by Portland High School. The City will be able to use the facility for the regular home schedule of the Portland High School baseball team as long as such use does not conflict with the requirement that the field be available at least six hours before game time to the professional ball club.

- b. Use of facility when ball club not at home. The use of the facility when the ball club is not at home will be at the discretion of the City. For the purposes of this agreement, other possible uses would include but not be limited to Deering High School baseball games, State or collegiate tournament games and State or regional youth

championships.

- c. The double AA team will have the option of using the facility for three (3) non Eastern League baseball events during the regular season.
- d. Clubhouse Facilities. The clubhouse facilities, including the training and storage facilities, shall be exclusively available to the ball club between April 1 and September 15. The visiting team club house shall be exclusively available to the visiting team while the ball club is at home.
- e. Umpire Facilities. The facilities provided for umpires shall be exclusively available between April 1 and September 15 while the ball club is at home.
- f. Additional Office Space. In addition to facilities for visiting teams and umpires, the City will provide office space of approximately 1500 square feet that will be exclusively available to the ball club throughout the year.

4. TERM OF LEASE.

The term of the lease will be ten years.

5. RENT.

Rent shall be Sixty-five Thousand Dollars (\$65,000) per year which shall include the rental for the entire facility and for the maintenance of the ball field.

6. FACILITY REVENUES.

All revenues generated from the operation of the facility during professional baseball games will belong to the ball club.

7. SUCCESSORS TO BE BOUND.

All of the provisions of this Lease and Agreement will be binding on Mr. Burke or his wife, or their successors. If both Mr. Burke and his wife die during the period of the Lease and the ball club is not willed to one of the members of Mr. Burke's immediately family, then the City of Portland will have the option for a period of one (1) year to provide local ownership for the baseball team. In such a case, the price of the franchise shall be determined by an appraisal process that is mutually agreed to by the parties. Regardless of ownership, the ball club is bound to the Lease for the 10 year

terms.

8. OPERATION OF THE BASEBALL TEAM.

The ball club shall be responsible for hiring and employing all personnel associated with the baseball operation and its home games. This responsibility shall include providing the personnel necessary for selling tickets, providing security, providing clean-up, operating the concession stands, selling souvenirs and any other operations that are related to the team and its functions.

9. OPTIONAL PROVISIONS.

- a. Construction of corporate boxes and revenues therefrom. The owner may construct corporate boxes at the expense of the owner. Any revenue generated from such boxes shall belong to the owner. The City and the owner may agree to mutually acceptable terms that require the City construct corporate boxes with financial reimbursement from the owner.
- b. Scoreboard and sound system. The owner or the corporate community may arrange for a more expensive scoreboard and sound system than required by League Rules and have them installed at the facility, so long as such system or installation is done at no cost to the City.
- c. Light bills. In relation to the lights necessary to light the ball field, the City will make a good faith effort to have separate meters installed on the light poles so that any electricity required for other events will be billed separately and will not be the financial responsibility of the ball club.
- d. The ball club will be responsible for payment of all utilities connected with their use of the facility.

This outline provides a general overview of how the City and the ball club intend to address the major issues that will ultimately be included in a binding lease and agreement between the parties.

Prepared for: Robert B. Ganley
City Manager

Prepared by: Office of Corporation Counsel
City of Portland, Maine



Bring AA Baseball to Hadlock Field"

© 1992

August 7, 1992

Mr. Robert Ganley
City Manager
City Hall
389 Congress Street
Portland, ME 04101

Re: Contributions to Hadlock Field Improvements

Dear Mr. Ganley:

I am writing to let you know of our plans for contributing to the improvements to Hadlock Field.

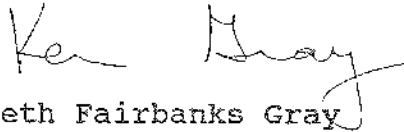
As you know, Citizens for Portland Baseball, Inc. is soliciting funds for Hadlock Field improvements by encouraging citizens to give \$100 to "sponsor" stadium seats. Our literature (our flyer, which is included for your information) states that "proceeds for sponsored seats will help defray the cost of improving Hadlock Field for professional baseball standards." We hope to raise \$200,000.

The funds raised through our effort will be turned over to the city to reduce the city's costs of improving Hadlock Field to meet the minimum standards required by the AA baseball league. We intend the contributions to directly defray the costs that the Portland or its taxpayers would otherwise be called on to pay. The understanding we have with contributors is that the funds will not be used for improvements that are beyond those required by the league. Further, we have suggested that for \$100 a name may be inscribed on a seat, but we have tried to make it clear that the contributions may or may not be used to pay directly for seats, and that the funds may pay for any expenses necessary to improve Hadlock Field to professional baseball standards, as determined by the city.

Mr. Robert Ganley
Page 2
August 7, 1992

Please share this letter with others as you see fit. Also, please call if you have any questions or concerns (773-6411). Of course, feel free to call if we can help in any other way.

Very truly yours,

A handwritten signature in cursive script that reads "Ken Gray". The signature is written in dark ink and is positioned above the printed name.

Kenneth Fairbanks Gray
Vice President

CITY OF PORTLAND, MAINE
DEPARTMENT OF PARKS/PUBLIC WORKS

MEMORANDUM

TO: Robert B. Ganley, City Manager

FROM: George A. Flaherty, Director Parks/Public Works


SUBJECT: Hadlock Field - Maintenance
Double "A" Baseball Standards

DATE: August 13, 1992

The FY'93 budget for ballfield maintenance has a current appropriation of \$133,949. The cost to maintain Hadlock Field to Double "A" professional standards is estimated to be an additional \$50,000 per year based on 1992-1993 costs.

We estimate that an additional \$50,000 for capital equipment is needed for the first year start up.

Two exhibits are attached outlining the additional maintenance costs and another listing the capital equipment needs.


GAF/bjm

cc: William J. Bray, Deputy Director Parks/Public Works
Donn Mathews, Supervisor Parks
Rick Anderson, Foreman, Parks
John Rague, Engineering
Desk File
Equipment

HADLOCK FIELD
DOUBLE "A" BASEBALL FIELD MAINTENANCE

Budget

August 13, 1992

Exhibit A		
<u>130.0 Temporary Help</u> Two seasonal temporaries for games Field maintenance assistance @ \$6.00 an hour each 70 games x 8 hours per game = 560 hrs ea. @ \$6.00 per hour x 1120 hours = \$6,720	\$ 6,720	
<u>140.0 Overtime</u> Grounds superintendent (foreman) @ \$18.03 per hour, 560 hours = \$10,096.80 (\$10,097)	\$10,097	
<u>Total Personnel Services</u>		<u>\$16,817</u>

HADLOCK FIELD
DOUBLE "A" BASEBALL FIELD MAINTENANCE

Budget

August 13, 1992

Exhibit A		
<u>232.0 Electricity</u> Field lighting	\$15,000	
<u>233.0 Water</u>	\$ 1,000	
<u>242.0 Plant Equipment Repair</u> Electrical contractor Field lighting repair/maintenance Electrical system Scoreboard repairs	\$ 5,000	
<u>Total Contract Services</u>		\$21,000

HADLOCK FIELD
DOUBLE "A" BASEBALL FIELD MAINTENANCE

Budget

August 13, 1992

Exhibit A		
<u>302.0 Agricultural Supplies</u>	\$ 1,212	
Marking lime		
150 - #50 bags @ \$2.16 per bag	\$324	
Grass seed		
800 pounds @ \$1.11 per pound	\$888	
<u>306.0 Concrete Supplies</u>	\$ 847	
Ballfield mix		
3 tons @ \$14.00 per ton	\$42	
Stone dust		
3 tons @ \$12.50 per ton = \$37.50	\$38	
Field Dry		
6 tons @ \$127.75 per ton = \$766.50	\$767	
<u>316.0 Minor Equipment</u>	\$ 5,024	
Mower, trimmers, edgers, spreaders		
Batter cage, pitcher cage, field drags		
Netting		

HADLOCK FIELD
DOUBLE "A" BASEBALL FIELD MAINTENANCE

Budget

August 13, 1992

Exhibit A		
<u>316.1 Electrical Supplies</u>	\$ 3,100	
Field Lighting		
Fixtures, bulbs, cable, etc.		
Scoreboard		
Circuit boards		
Sound System		
Parts and equipment		
<u>320.0 Plumbing Supplies</u>	\$ 2,000	
Sprinkler head		
Valves		
Pipe		
Fountains		
<u>Total Commodities</u>		<u>\$12,183</u>
<u>Total</u>		<u>\$50,000</u>

HADLOCK FIELD
DOUBLE "A" BASEBALL - CAPITAL EQUIPMENT

Budget

August 13, 1992

Exhibit B		
<u>Emergency Lighting System</u>	\$ 9,200	
Generator 7500 watt \$6,200 Electrical Panel \$3,000		
Small field tractor Four wheel drive with rear 3 point hitch and front bucket	\$17,000	
Infield tarp spring	\$ 8,200	
Batting Cage	\$ 1,200	
Infield tarp with tube roller	\$ 4,300	
	<hr/> <hr/>	<hr/> <hr/> \$39,900

Meeting with George Flaherty regarding Double A Baseball
July 29, 1992

Agenda

- I Baseball needs and Expo needs
 - A Locker rooms
 - B Offices
 - C Conference rooms
 - D Storage
- II Plans for reconstruction
- III Moving teams that use Expo lockers
 - A Bringing in the new Athletic Director of PHS
- IV Parking
 - A Expo needs
 - B Coordinate with Ice Arena, Fitzpatrick Stadium
 - C Maine Medical Center parking rental spaces
- V Expo custodial needs of baseball team
- VI Decision making process between City and School Department regarding this project
- VII Miscellaneous

July 29, 1992

BASIC INFORMATION SHEET ABOUT USAGE OF PORTLAND EXPOSITION BUILDING
COMPILED BY FRANK LATORRE, EXECUTIVE DIRECTOR

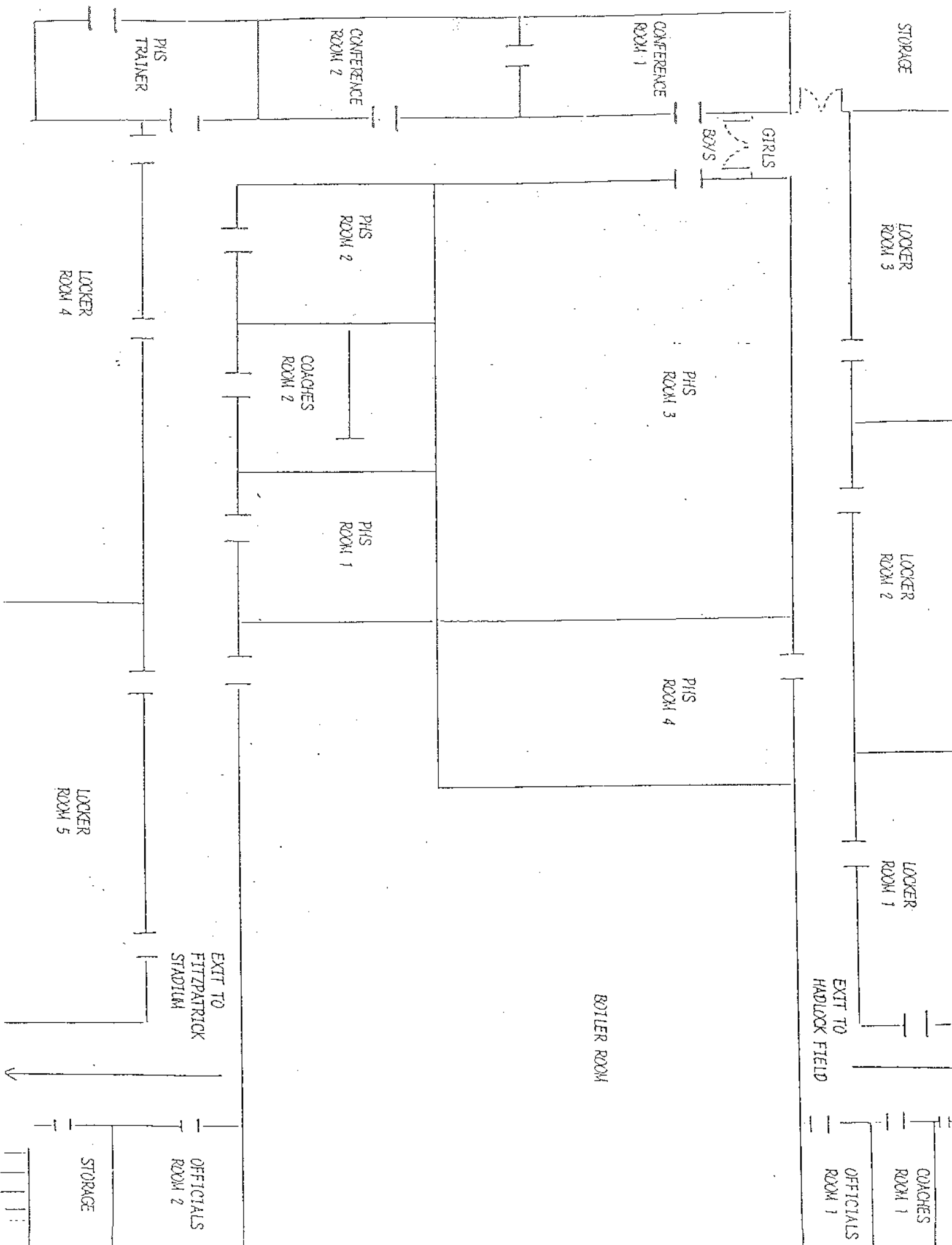
The Portland Exposition Building is presently used over three-hundred and twenty days per year. The Expo is used an average of 90 hours per week, fifty-two weeks of the year. This usage is composed of sporting events and practices, consumer shows (home shows, antique shows, camping shows, etc.), trade shows (real estate industry, plumbing industry, etc.), business meetings, banquets, concerts, flea markets and special events. Usage of the building has increased substantially over the past decade as a result of marketing efforts, the support services provided, and a quality reputation within the industry.

The physical layout of the Expo presently consists of five basic areas: main arena, basement locker rooms, basement storage, lobby, and the offices above the lobby. In addition to the main arena being well utilized, the basement is also very heavily utilized. The basement locker rooms house nine Portland School Department teams: eight for Portland High School and one for Deering High School. All locker rooms are also in use for concert dressing rooms, college and high school basketball tournaments, and high school track, when the building is rented for these events.

The basement storage area is very cramped as it is also utilized as an indoor training area for some Portland High School teams in inclement weather.

The upstairs offices are presently used by the Administrative staff of the Portland Exposition Building (seven full-time employees), the staff of the Portland School Department Multilingual Program, and for conference rooms utilized by promoters when they contract to rent the Expo. These conference rooms are essential to the marketability of the Expo, as they provide the promoter an "office" within the building to conduct work essential to their respective events.

Most Expo events draw from 1,000 to 5,000 people per day, thus parking spaces around the building are quickly utilized. Coupled with frequent events at the ice arena and fall events at Fitzpatrick Stadium, parking becomes a major consideration with regards to providing a service to those that rent the facilities, as well as a service to those citizens that patronize the events.



STORAGE

LOCKER ROOM 3

LOCKER ROOM 2

LOCKER ROOM 1

COACHES ROOM 1

EXIT TO HADLOCK FIELD

OFFICIALS ROOM 1

GIRLS

BOYS

PHS ROOM 3

PHS ROOM 4

BOILER ROOM

CONFERENCE ROOM 1

CONFERENCE ROOM 2

PHS ROOM 2

COACHES ROOM 2

PHS ROOM 1

PHS TRAINER

LOCKER ROOM 4

LOCKER ROOM 5

EXIT TO FITZPATRICK STADIUM

OFFICIALS ROOM 2

STORAGE

Portland Expo
Sports use log
FY 1991

Team	Locker Room Use	Arena Use
Soccer, girls	11.0	0.0
Soccer, boys	16.5	0.0
Football	676.5	6.25
Weights	496.0	0.0
Cheerleading	58.5	0.0
Basketball, boys	120.0	364.5
Basketball, girls	106.25	212.25
PHS Hockey	208.0	0.0
DHS Hockey	Started in fiscal year 92	
Track, boys	26.5	2.0
Track, girls	29.75	2.5
Softball	70.0	17.0
Baseball	325.5	25.5
<hr/>		
Totals	2,144.5	630.0

TO: Robert Ganley

DATE: August 13, 1992

FROM: Bethany Holt *BH*

SUBJECT: The business community and baseball.

The following is a synopsis of my phone conversations with the various cities which serve as host to an Eastern League Double A baseball team. In most cases, the Chambers of Commerce were not that helpful and ended up referring me to either the city's Department of Economic Development or to the team's General Manager. Eight teams comprise the Eastern League. I have contacted the seven teams located in the United States (the eighth team is located in Ontario).

HARRISBURG SENATORS

Harrisburg Chamber of Commerce (717) 232-4121
Barbara Groce

The business community has been very supportive of the Harrisburg Senators, and in turn, the Senators have helped the business climate in the Harrisburg area. The stadium is located on City Island, in the center of the Susquehanna River. There are several eateries and small restaurants located near the stadium on City Island which benefit the most although there is some spillover into the downtown area. Local businesses also support the Senators through advertisements in the programs and on the billboards located in the stadium. Barbara suspected that a good number of businesses purchase season tickets to use for entertainment purposes. Overall, the team helps to bring a positive moral into the community. There have not been any formal studies done to determine how much money is brought into the business community by the Senators. The Chamber uses the Senators as a promotional tool in attracting tourists to the city.

Barbara referred me to the Parks & Recreation Dept. (717) 255-3020. They did not have any information on the benefits to the business community.

I phoned Todd Vander Woude, General Manager of the Senators (717) 231-4444. He reaffirmed that the business community was very supportive. Approximately 75% of season tickets are purchased by local businesses to use for entertainment and employee incentive give-aways. The Senators sell various advertising packages which are very popular amongst the business community e.g. program advertisement; outfield billboard advertisement. It is difficult for him to put a dollar amount on how much money trickles back to the business community, however, the stadium promotes clean, wholesome, family entertainment that local businesses want to be affiliated with. There is plenty of parking available at the stadium and it is free. Season box tickets cost \$300, reserve seating \$250; general admission \$4 adults, \$2 children under 12.

ALBANY YANKEES

Chamber of Commerce (518) 465-8975

No information available. Referred me to the State Data Center

State Data Center (518) 474-6005

No information available.

I phoned Ralph Acampora, General Manager of the Yankees (518) 869-9236. He felt that overall the business community was receptive to the Yankees however they don't derive many benefits from the team. The stadium is located approximately 15 miles outside of Albany in a smaller town called Colonie and is situated out near the airport. Because of the location, it does not bring much business into the downtown area. Most people eat at the concession stands located within the stadium. Parking is provided free of charge for approximately 1,000 cars.

BINGHAMTON METS

Chamber of Commerce (607) 772-8860

No information available. Referred me to the Dept. of Economic Development.

Dept. of Economic Development (607) 772-7001. Spoke to Bob Sweet. He did not have any tangible information on the benefits that the business community has experienced since the Mets established themselves in Binghamton.

I phoned R.C. Reuteman, General Manager of the Mets (607) 723-METS. Although the Mets have only been in Binghamton for one year, he felt that the business community was extremely supportive of the team. He sold out immediately on the outfield advertising and had a good deal of support on the program advertising. Three games this year were televised so the businesses that advertised in the stadium received additional coverage. There were approximately 1,400 season tickets sold this year but he did not have a breakdown of how many were to local businesses. R.C. did not have any quantitative information but he felt that there might have been some increase to businesses located near the stadium (especially taverns). The stadium is located in the eastern portion of the central business district. Parking is available, but it is limited (aprox. 520 spaces - seating capacity 6,000) and costs \$3 per car. There are some lots and garages located within 1/2 mile of the stadium which many attendees have to utilize.

CANTON-AKRON INDIANS

Chamber of Commerce (216) 456-7253

No information available.

I phoned Glen Strong, General Manager of The Indians (216) 456-5100. He suggested that I contact the team in Old Orchard Beach to get a better feel on how the business community supports baseball...

Anyway, the business community is supportive of the team. The Indians have approximately 60 billboards in the outfield (\$2,300 each) and approximately 40 companies advertising in the program. Out of the 960 season tickets sold this year, almost all went to local businesses. It's hard for him to get a feel for what benefits the business community gets by having a team located there. According to Glen, there aren't any good restaurants or taverns nearby that might see an increase in business attributed to the baseball team. There is plenty of parking at the stadium and costs \$1 per car. Season box seats cost \$275, season reserve seats cost \$210; general admission \$5 box, \$4 general admission and \$3 seniors and kids under 14.

HAGERSTOWN SUNS

Chamber of Commerce (301) 739-3025
No information available.

I phoned Bob Miller, General Manager of the Hagerstown Suns (301) 791-6266. He was not available for comment.

NEW BRITAIN RED SOX

Chamber of Commerce (203) 229-7665
information available. Referred me to the Municipal Economic Dev. Agency.

Municipal Economic Dev. Agency (203) 225-5507
No information available.

I Phoned Gerry Berthiaume, General Manager of the Red Sox (203) 224-8383. The stadium is located about 2 mins. from the downtown area and is very accessible from both Interstates 91 and 84. They draw alot of people from MA, NY and NJ. The stadium is located within Willowbrook Park, a large sports complex which houses the baseball stadium, football stadium, softball field and practice field. The business community is very enthusiastic about the Red Sox and the City, overall, is very sports-oriented. Parking at the complex is more than ample and is free. Businesses advertise heavily in both the stadium outfield billboards and the programs. They have the support of the entire business community from the banks, to the larger manufacturers to the mom and pop stores. He felt that the businesses don't necessarily benefit from people eating out before or after the games as the complex has a number of concession stands which are marketed well. Gerry did mention that because the team draws a number of out of state people, the local hotels and restaurants benefit from that, in addition to the visiting team and their fans that also frequent the hotels. According to Gerry, although it is difficult for a business to say "OK, we've increased our business 10% because we advertise in the program and are located near the park", he feels the local businesses want to be a part of it because baseball, and the Sox, make people feel good!

ADINS PHILLIES

Chamber of Commerce (215) 376-7677

Tony Grim, President. Tony also sits on the Advisory Committee for the Phillies.

Tony stated that there is a very harmonious relationship between the business community, the team owners, and the City. The team was purchased by new owners approx. four years ago and has since turned around to become a real "old fashioned ball park". The business community does alot of advertising through the team because the Phillies promote "family" and businesses want to be associated with that. Reading has a diverse industry base, including steel, textiles and chemicals. It is also the third largest agricultural district in Pennsylvania. He didn't know if the local restaurants benefited from the team because the stadium sells low-cost food at the concession stands (everything costs between \$1 and \$1.50, including beer). The stadium also promotes affordable entertainment (tickets, parking, food and beverages) to families that probably do not have the money to go out on the town. Apparently, Reading is the Outlet Capital of the World so the ball team does well because it provides something for visitors to do in the evening. Many of the hotels buy season tickets and give them away to their guests as a promotional to get them to stay in their hotels. A good majority of the box tickets are sold to local companies for entertainment and employee incentive purposes. The stadium also has a picnic area that companies can rent for the evening which includes tickets to the game so alot of businesses have their company outings at the stadium. They also have give-away nights where business sponsor prizes to be given away to the kids (balls, bats, pictures of the team, watches, jackets). The prizes have both the company and Phillies logo on them.

Bob, overall, it seems difficult to obtain, in dollar terms, a firm grasp on just how much the business communities benefit by having a ball park located in their city. However, each person that I spoke with indicated that the local businesses were all receptive and supportive of the team and in some way wanted to be associated with it. Each stated that the teams enjoyed a good relationship with the local business community.

cc: Virginia Hildreth

TO: Bob Ganley

DATE: August 13, 1992

FROM: Bethany Holt *Bmh*

SUBJECT: The business community and baseball.

I have contacted the following mayor's offices to discuss the economic benefits to the business community by having a baseball team located there. Only one city has done a formal study on the economic impacts and can give quantitative information:

Reading, PA

Steve Whitney, Assistant to the Mayor
(215) 320-6234

They recently completed a study on the impact of having the Phillies located in Reading which contains some information on the benefits to the business community. He has faxed that portion of the study to me and is attached to this memo.

Shamton, NY

Janita Crabb
(607) 772-7001

Out of town - I left a message. She will return on Monday. I also spoke with the Department of Economic Development yesterday. They did not have any information.

Anton, OH

Richard Watkins, Mayor - referred me to Bill McGeorge, Director of Eco. Dev.
(216) 489-3000

No formal studies have been done. They have never looked at the benefits to the local business community, they look at it in terms of "doing something nice for the community as a whole".

Harrisburg, PA

Andy King, Secretary to the Mayor (the Mayor is on vacation)
(717) 255-3040

Andy is out of the office and has not yet returned my phone call.

To: Steve Whitney
From: Chuck D.

This is very accurate

The National Association and the Community

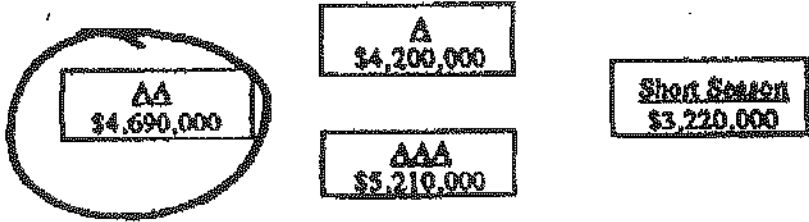
Minor League Baseball has a very positive economic impact on a community. Studies show that every new dollar brought to a community through the presence of Minor League Baseball turns over 4 times. This economic impact ranges from more than \$2 million annually for short season communities, to several million in larger market cities. Player salaries, visiting teams and umpires expenditures as well as parent club expenditures make up the new money coming into the community. The local club itself will spend anywhere from \$300,000-\$1 million in its community annually.

In the last four years, 18 new stadiums have been built through public funding. In addition, many communities have pledged substantial funds (\$2-5 million range) for major renovations to existing stadiums in the corresponding period. Approximately 12 new ballparks are planned for construction during the next few years.

Minor League Baseball is important to the fabric of the communities it serves. Member clubs are commonly quite active in the communities they represent, assisting in various charity causes among other worthwhile endeavors.

ECONOMIC IMPACT FACTORS

- * Expenditures by Players, Manager, Trainer & Families
Food, Housing, Entertainment, Transportation
- * Expenditures by Visiting Teams & Staff
Hotels, Food, Incidentals
- * Expenditures by Umpires, Scouts & Other Baseball Personnel
Hotels, Food, Automobiles, & Incidentals
- * Expenditures by Major League Affiliate
Staff Hotels, Food, Automobiles & Incidentals
Equipment, Food & Uniform Reimbursements
- * Expenditures by Fans
Souvenirs, Concessions, Programs
- * Expenditures by Club Within The Community
Labor, Concessions, Park Maintenance, Advertising



JUL 29 1992

CITY OF PORTLAND, MAINE
MEMORANDUM

TO: Joseph E. Gray, Jr., Director PUD
FROM: P. Samuel Hoffses, Chief Inspection Services
SUBJECT: Binghamton - Mets Stadium, Binghamton, NY

DATE: July 29, 1992

Construction Costs \$4.1 million: Construction type steel, concrete slab & concrete block.

The Binghamton Mets Stadium is a state of the arts AA baseball stadium. The stadium is built on a old railroad yard in a neighborhood setting of mixed uses, single family, retail, multi-family light industrial. (shipping and receiving area). The stadium is a plus to the area. I spoke to an elderly lady who lives across the street from the stadium and asked her about her concerns. Her concerns were parking and once in a while a ball might break a window. The other person I spoke to was a local retailer who thought it was great.

The Mets Stadium sits on a corner lot with a railroad yard in the outfield. Left field across the street is light industrial and across the street in front is retail and residential, with parking lot for 520 cars outside the fence on the right side of field. Within 1200' of home plate there is parking for 2500 parking spaces, both public and private lots. (street parking)

The stadium has 4 concession stands all run by the Mets organization but sub lease with full control under the Mets. All concession equipment owned or leased by Mets.

The facility has a beer garden outside outfield third base line. The restaurant serves the sky box seats and supplies food to the concession stands.

The restrooms are ample with baby care in both male and female restrooms. There are 4 public telephones, 5 ticket windows and 8 turnstiles. The price of tickets \$6.50 and \$4.00 with discount for senior citizens and children under 14.

The security is off duty City Police officers paid for by the Mets. The number is 3 to 6 per game. There are no special security personnel. The structure has a very up to date security system.

During games the Mets employ 2 EMTS with ambulance service. (first aid room on site).

The Administrative office has 4 staff members; but the organization employes

up to 100 part time during games. 8 ticket takers, 4 sellers, 20-30 ushers & usherettes, and 2 to 3 parking attendants.

The stadium seats 6200 total, 48 sky lux box seats, 2222 reserved with the rest general admission. The seats are all plastic with arm and backing material on concrete slabs. Slab three tier per section.

All stadium facilities are located under seating except for beer garden. All locker rooms, training rooms and team restroom areas off dugouts. Dugout construction concrete block & slab.

The facility gives to the city 5 events per year. Also high school baseball can be played on field. Facilities not designed as multi purpose area. (NO FOOTBALL).

The stadium media facilities has 2 radio boxes, TV space and ten (10) press boxes. There are private restrooms for the press boxes. The elevator serves the sky boxes, also, the facility has 2 photo boxes.

There are 31 fence signs 12'x16' costing \$3,000 per year. No seats for advertisement as in Albany. It's cost \$5000 to have your message on tri-vision screen (scoreboard).

The score board is supplied by Budweiser beer it is operated by two computers located in the media box area. Also, in the media box area is a sound room which controls all field voice messages and music.

cc: Robert Ganley, City Manager
George Flaherty, Public Works Director

and wash down with hose. The team supplies the cleaning crew, the dugouts are single concrete block with pre-cast concrete slab roof.

The locker training for the home team and restrooms are located under the right field seating made of concrete block. The restrooms are very small and get a great number of complaints. Also under the same seating area are eating concessions, with a special eating area to the right of the seating on grade level. Also a money maker is an area next to this eating area is an enclosed (netting) area where you can throw a baseball and have the speed measured by radar.

Under the covered seating behind home plate is the main office, and cooking area for pizza, burgers, dogs, fries etc. Beer is also sold on site but all sales for beer stop at 8th inning. This section is also concrete block with pre-cast concrete slab roof. (All cooking equipment is owned or leased by the team).

The visiting team has facilities (clubhouse) located outside the left field fence. The structure is wood frame 2x4 studs, T-111 siding looked like no foundation, (on slab) also in this area is the wight room and storage of the towns equipment same type of construction.

The stadium has 64 ad signs 8'x16' @\$2,000 per year. This also gives the signs owner 2 season tickets and advertising in programs. The stadium is also leased to anybody at \$200 per day and if lights are used it is \$400. This price should be increased to minimum of \$1,000 to cover all costs.

cc: Robert Ganley, City Manager
George Flaherty, Public Works Director

INSPECTION TEAM'S
GENERAL COMMENTS

The members of the Inspection Team found the trip quite enlightening. The local government officials and team officials accepted us warmly and were very willing to share information and spend hours giving us tours and answering questions.

The Inspection Team came home with very definite impressions as to how Portland should proceed at this point.

1) Long range planning is imperative to the future success of our endeavor. Expandability and future growth need be address today in order to be properly implemented in the future.

2) Parking, we feel, is the major issue to be addressed in the overall scheme. Inundating neighboring residential streets with parked cars creates dissension among the residents. Instead of the stadium becoming an amenity, it becomes a nuisance. We further discovered, although no one yet had a written copy, that the Major Leagues is mandating parking at a 1-space per every 3-attendee ratio. With a proposed 6,000 capacity stadium, a minimum of 2,000 spaces would be required under such mandate.

3) The Harrisburg Senators are successful, at least in part, due to the incorporation of its stadium within an amusement park area. Onsite parking and pedestrian accessibility are additional factors. The Reading Phillies are the most successful team in the league (from an attendance standpoint) due to (1) their longevity in the community, (2) their affiliation with the Phillies, (3) the team's marketing and the way it integrates itself with the community, and (4) the team's understanding of who their market is.

In both instances, the patron is being offered clean facilities, and wholesome, family-oriented fun which is affordable! Neither local government has attempted to "milk" proceeds from the team or the stadium. In both instances, the team has the exclusive use of the stadium with very minor exceptions (which are generally pre-approved by the team).

It seemed local government's approach was:

- A) They are providing a recreational service to the taxpayers of their respective cities, and
- B) They allow the team to make a sufficient profit to insure longevity, and
- C) They realize the economic benefit to the entire community by the team's existence.

In both cities, there exists a strong "working understanding" between team ownership and local officials.

INSPECTION TEAM:

Team Leader.....William Bray

Engineering.....John Rague

Facility Maintenance...Richard Anderson

SITE VISITED:

Harrisburg, Pa. July 27, 1992

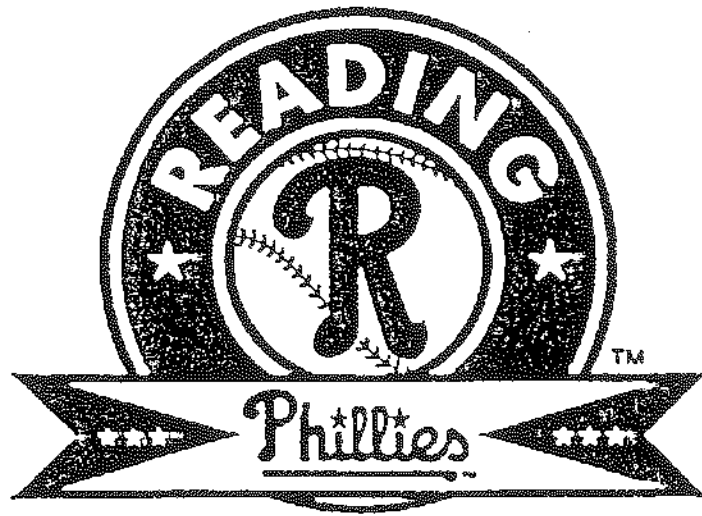
Harrisburg Senators

Montreal Expo's AA Farm Team

Reading, Pa. July 28, 1992

Reading Phillies

Philadelphia Phillies AA Farm Team



The following report reflects the findings of an "Inspection Team's" onsite review of two Minor League baseball parks and facilities, one each in Harrisburg, Pa. and Reading, Pa. in July 1992.

At both sites, the Inspection Team visited with local government officials and officials of the respective ball clubs as well as grounds-keeping personnel and public relations personnel.

AREA DEMOGRAPHICS

Unlike Harrisburg, Reading is a very "blue-collar" area. The City itself is a "tired city"...financially depressed, however, the Reading Phillies annually draw the largest yearly attendance in the Eastern League.

The City of Reading has 78,000 people. The metropolitan area has 300,000 people. The Phillies are drawing between 5,000 and 6,000 people per game.

The City is mostly industrial. Its moniker is "The Outlet Capital of the World".

The Reading Municipal Memorial Stadium sits in the heart of the industrial area. It is accessible off two major State Routes.

CONSTRUCTION FINANCING

The stadium was built in 1950. No information was readily available regarding costs or types of financing.

Over the last two years, however, the owner of the Reading Phillies has pumped approximately \$1,000,000 into stadium improvements, upgrading the field, new seats, a roof over the grandstand, a new picnic area. The City of Reading supplemented this investment with \$80,000 of municipal funds in 1992.

LEASES & COSTS OF OPERATIONS

The ballpark is owned by the City of Reading. There is a "Stadium Commission" which oversees all stadium business. Back in 1979, the annual rent to the Phillies was \$32,500 which remained stable for a number of years. In 1989, in consideration of the stadium improvement costs, the yearly rent was lowered to \$6,500 plus 10% of annual ticket sales over \$100,000.

The City also enjoys income from concessions and beer sales. They receive 30% of gross receipts after taxes on concessions. The club receives all receipts up to \$40,000 on beer sales, then split 50/50 with the City thereafter.

Some maintenance and operational costs remain the responsibility of the City. The annual Stadium Commission budget ranged from \$55,495 in 1988 to \$41,455 in 1992 (projected).

The Phillies are currently under a five year lease.

FACT SHEET

STADIUM

Reading Municipal Memorial

CITY

Reading, Pa.

CONTACT PERSON

Mark Wallace
Public Relations Director

SEATING CAPACITY

Box Seats	400	\$5.00
Reserved Seating	500	\$4.50
General Admission	6,300	\$3.50 Adults \$2.00 Children/Seniors Free Under 5 years

Total 7,200 Seats

AVERAGE ATTENDANCE

5,500 per Game

PARKING

2,000 spaces onsite and adjacent sites

STADIUM FACILITIES

Locker Rooms	Separate for both teams. No direct access to dugouts
Trainer's Room	Separate room with two whirlpools in home team locker room only. A trainer's area off visitor's locker room. No whirlpool
Other	Umpire's room with private shower and lockers Laundry room with commercial washer and dryer Manager and coaches' room with private shower and lockers
Dugouts	Poured concrete, three steps deep in the ground. No direct access to locker rooms
Locker Room Attendant	Part-time student hired by club

STADIUM FACILITIES

Rest Rooms

Locked, did not view

First Aid Sta.

No onsite first aid station. An ambulance service with paramedic personnel attend each game.

GROUNDSKEEPING

One full-time, yearround person and two part-time, seasonal assistants.

Hired by ballclub with "some" funding assistance by City

PARK SECURITY

Private Security Company, 4-5 per game hired by ballclub

No onsite Security Command Post

CONCESSIONS/NOVELTIES

Four contiguous concession stands under the grandstand in the concourse area plus two outside the stands.

Storage also under grandstand

One novelty stand in concourse.

The ballclub operates all novelty and concession stands and retains all profits.

There are two separate beer stands. There is also a semi-private beer hall for players, umpires, club officials and guest under the grandstand.

There is a "Vendor's" station under the grandstand where the "in-the-stands" vendors are stocked.

TICKET WINDOWS

There are 4 ticket windows and 3 turnstiles leading into the concourse area.

AMENITIES

Two public telephones

A fenced in picnic area where the ballclub accommodates large group outings.

MEDIA FACILITIES

Media facilities are provided atop the grandstands.

Radio: 2 separate booths each 10x12
Press: 1 large booth, 12x30
P.A. : 1 booth, 10x12
P.R. : Scoreboard computer, key-
board and sound system, 10x12
(Equipment in this booth
costs \$12,000)

SUMMARY

Unlike Harrisburg, this stadium is nestled in an industrial area of the City. Other than the interior functions of the stadium, it has no other amenities or surrounding activities to help draw people.

As mentioned earlier, Reading leads the Eastern League annually in attendance. The blue-collar ethic is prominent throughout the area which creates an affinity with the ballclub. It is their ballclub and they support it in large numbers. Being a Philadelphia Phillies affiliate is a definite positive.

The stadium is a throwback to yesteryear incorporating brick and concrete construction. It has gone through a major modernization over the last couple of years yet it has maintained its charm and appeal.

Much like Harrisburg, there is a strong sense of community between the ballclub and the City of Reading, both from a governmental and resident support aspect. Ballgames at the stadium represent good, clean, wholesome fun for the whole family. As reflected in the ticket prices, the ballclub realizes the spending power of its market. Hot dogs are only a dollar, a 14-ounce beer is a dollar and a half. Little profit is made on ticket sales. A high volume in concessions and novelty produces the bulk of the club's income.

Government officials in Reading also felt that having this team produced in excess of 1.5 million dollars each year to the local economy. team produced 1-2 million dollars of value to the local economy.



READING PHILLIES

Year	W	L	PCT	Overall Finish	Play-off Results	Attendance	Manager	Affiliate
1933	60	56	.518	3	vs Albany 4-1	97,757	Kirby Farrell	Indians
1934	72	66	.522	3	vs Binghamton 1-4	106,368	Kirby Farrell	Indians
1952	75	63	.543	3	vs Schenectady 4-2	76,076	Merrill May	Indians
1953	101	47	.682	1	vs Elmira 4-0	99,142	Jo Jo White	Indians
1954	71	69	.507	4	vs Binghamton 3-0	102,418	Don Helfner	Indians
1955	84	53	.613	1	vs Schenectady 0-3	95,528	Jo Jo White	Indians
1956	80	59	.576	3	vs Schenectady 3-0	70,722	Clyde McCullough	Indians
1957	74	66	.529	3	vs Albany 3-1	81,311	Al Hollingsworth	Indians
1958	75	58	.564	2	vs Williamsport 0-2	75,777	Ray Mueller	Indians
1959	71	69	.507	5		53,283	Ray Mueller	Indians
1960	69	71	.493	4		46,541	Eddie Popowski	Red Sox
1961	59	81	.421	6		51,200	Wesley Kurovski	Indians
1962	61	79	.436	6		46,594	Frank Lucchesi	Phillies
1963	60	60	.500	6		69,350	Bob Wellman	Phillies
1964	60	60	.500	6		85,559	Bob Wellman	Phillies
1965	53	66	.441	6		107,168	Bob Wellman	Phillies
1966	53	66	.441	6		69,350	Bob Wellman	Phillies
1967	70	69	.504	3		96,684	Andy Sgimnick	Phillies
1968	81	59	.579	2		78,737	Nolan Campbell	Phillies
1969	81	59	.579	2		65,874	Jim Buning	Phillies
1970	78	67	.538	2		78,651	Cal Emery	Phillies
1971	72	67	.518	2		73,509	Bob Wellman	Phillies
1972	70	69	.504	3		85,357	Bob Wellman	Phillies
1973	75	62	.551	1		71,152	Bob Wellman	Phillies
1974	75	62	.551	1		57,540	Granny Hamner	Phillies
1975	69	66	.511	3		66,968	Leo Eita	Phillies
1976	84	53	.613	1		84,200	Jim Snyder	Phillies
1977	84	53	.613	1		97,235	Ron Clark	Phillies
1978	84	53	.613	1		117,050	Ron Clark	Phillies
1979	84	53	.613	1		81,975	John Felske	Phillies
1980	84	53	.613	1		86,484	Bill Dancy	Phillies
1981	84	53	.613	1		85,293	Bill Dancy	Phillies
1982	84	53	.613	1		75,818	Tony Taylor	Phillies
1983	84	53	.613	1		83,506	George Culver	Phillies
1984	84	53	.613	1		100,895	George Culver	Phillies
1985	84	53	.613	1		144,107	Bill Dancy	Phillies
1986	84	53	.613	1		178,734	Mike Hart	Phillies
1987	84	53	.613	1		204,240	Don McCormack	Phillies
1988	84	53	.613	1		250,610	Don McCormack	Phillies
1989	84	53	.613	1		3,496,795*	Don McCormack	Phillies
1990	84	53	.613	1				
1991	84	53	.613	1				
1992	84	53	.613	1				
1993	84	53	.613	1				
1994	84	53	.613	1				
1995	84	53	.613	1				
1996	84	53	.613	1				
1997	84	53	.613	1				
1998	84	53	.613	1				
1999	84	53	.613	1				
2000	84	53	.613	1				

*Attendance Records Incomplete for 1933 and 1934 seasons.

LEAGUE CHAMPION: 1957, 1968, 1973

Double A
All-Star Game
Monday, July 13
Charlotte, North Carolina



GENERAL MANAGER CHUCK DOMINO

Chuck Domino is in his fifth year as General Manager of the Reading Phillies and his eighth year as a minor league G.M. Over the last seven years Domino has been the G.M. for clubs that have set seven straight attendance records.

In 1988, Domino captured Eastern League Executive of the Year honors in his first season with the Phillies. In 1989, Domino was a repeat winner of the E.L. Executive of the Year award while receiving the Sporting News Double-A Executive of the Year honor.

Domino came to Reading after serving as General Manager of the Idaho Falls Braves where he was Pioneer League Executive of the Year in 1986.

Major League Affiliate: Philadelphia Phillies
 Coaches: Carlos Arroyo, Kelly Heath
 Trainer: Mark Ruffner
 General Manager: Chuck Domino
 Name of Park: Municipal Memorial Stadium
 Capacity: 7,000
 Outfield Distances: LF 330', CF 400', RF 330'
 Director Public Relations: Mark Wallace
 Business Manager: Crystal Domino
 Director Sales/Marketing: Todd Parnell
 Address: P.O. Box 15050, Centre Avenue
 Reading, PA 19612
 Telephone: (215) 375-8469
 Fax: (215) 373-5868

READING RECORDS

INDIVIDUAL SEASON BATTING RECORDS

Games:	141	Greg Luzinski	1970
At Bats:	559	Ray Flood	1933
Runs:	111	Bob Damler	1980
Hits:	189	Ray Flood	1933
Total Bases:	287	Greg Luzinski	1970
Doubles:	38	Dom Dallasandro	1934
Triples:	20	Ray Flood	1933
Home Runs:	34	Harold Jones	1960
RBI:	121	Rocky Colavito	1953@
Walks:	132	Bob Baell	1970
Strikeouts:	148	Greg Luzinski	1970
Sacrifices:	14	Ray Flood	1933
Sacrifice Flies:	14	George Yankovich	1979
Stolen Bases:	90	Jeff Stone	1983
Average:	.338	Ray Flood	1933

TEAM SEASON BATTING RECORDS*

Most:	Least:
149	1953@
142	1954
4855	1953@
801	1983
111	1980
189	1933
1925	1953@
1875	1983
215	1978
78	1953@
113	1955
726	1953@
701	1983
699	1984
1117	1963
110	1954
110	1953@
50	1980
272	1983
.288	1990
	.232
	1985

INDIVIDUAL SEASON PITCHING RECORDS

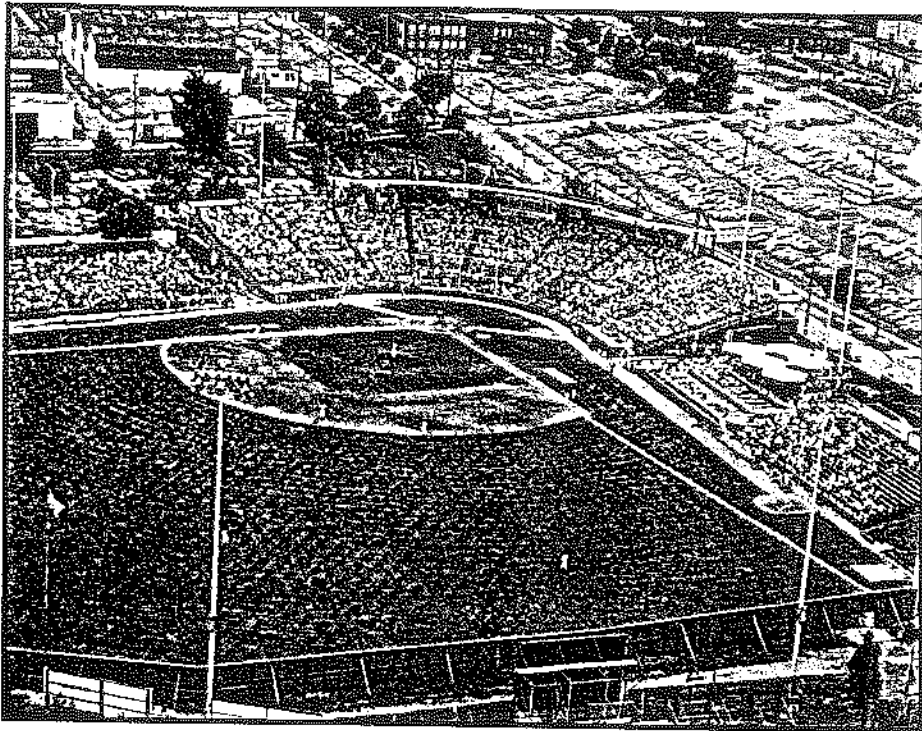
Games:	82	Rocky Childress	1984
Complete Games:	62 <th>Rick Surhoff</th> <th>1984</th>	Rick Surhoff	1984
Shutouts:	13 <th>Warren Bruslar</th> <th>1978</th>	Warren Bruslar	1978
Hits:	19 <th>Gene Robinson</th> <th>1960</th>	Gene Robinson	1960
Losses:	17 <th>Mark Davis</th> <th>1983@</th>	Mark Davis	1983@
Saves:	24 <th>Warren Bruslar</th> <th>1976</th>	Warren Bruslar	1976
Innings:	228 <th>Toby Borland</th> <th>1991</th>	Toby Borland	1991
Walks:	128 <th>Larry Locke</th> <th>1956</th>	Larry Locke	1956
Strikeouts:	192 <th>Julius Grant</th> <th>1960</th>	Julius Grant	1960
ERA:	1.83 <th>Gary Bell</th> <th>1956</th>	Gary Bell	1956
		Brooks Lawrence	1953

TEAM SEASON PITCHING RECORDS**

Most:	Least:
149	1953@
142	1954
64	1975
21	1998
101	1953@
84	1955, 1975
44	1887
9	1974
1153	1987
653	1982
192	1963
2.17	1969
	4.42
	1977

*Team batting records for Reading do not include 1933-1934 seasons.
 **Team pitching records are incomplete for seasons prior to 1963.
 @ 1953 season had 154 scheduled games.

MUNICIPAL STADIUM INFORMATION GUIDE



GAME TIMES*

Monday-Saturday:	
Single Game.....	7:05
Doubleheader.....	6:05
Sunday:	
Single Game.....	1:05
Doubleheader.....	1:05

* All game times subject to change

TICKET PRICES

Box Seats:	\$5.00
Reserved Seats:	\$4.50
General Admission:	
Adult	\$3.50
Child (5-14)	\$2.00
Senior Citizen (62-over)	\$2.00
Group Rate: (20 or more)	
Adult	\$2.50
Child/Senior	\$1.00
Children under 5 free except on giveaway days	

ADVANCED TICKET SALES

BY MAIL: Send your request, including date(s) and number of tickets desired to:

TICKETS

Reading Phillies Baseball Club
PO Box 15050
Reading, PA 19612

IN PERSON: Tickets for all R-Phils home games are available at the Phillies' office during normal business hours (Monday-Friday 9-4) and at the Advanced Ticket Window following the start of every home game.

WE SELL TICKETS BY PHONE USING VISA OR MASTERCARD.

ADVANCED TICKET & INFORMATION WINDOW:

The Phillies' Advanced Ticket and Information Window is located in the main concourse behind section 3. This window is open from the first inning on, and is open for advanced ticket sales, lucky logo winners, and any other information.

BIRTHDAY PARTIES: The Phillies offer special birthday packages that will make your child's birthday one to remember. Stop by the Advanced Ticket Window or call the Phillies' office for more information.

BOX SEAT EXCHANGES: Unused box seat tickets may be exchanged for general admission tickets to any regular season R-Phils game. The exchange can take place at the Phillies' office during regular business hours, at the Advanced Ticket Window or at the main ticket booth at game time.

FOOD & DRINKS: NO CANS, BOTTLES, COOLERS, FOOD, OR ALCOHOLIC BEVERAGES MAY BE BROUGHT INTO READING MUNICIPAL STADIUM!! Food and coolers collected at the main gate may be claimed at the Phillies' office following the game.

FOUL BALLS: Fans are welcome to keep balls hit into the stands. However, fans are not permitted on the field to retrieve a ball or to interfere with a ball in play. This action will result in immediate ejection from the ballpark premises.

HOMERUN BALLS: Fans are not permitted behind the fence to retrieve homerun balls. This action will also result in immediate ejection.

GATES: Gates normally open one hour before game time. This may vary for certain special events. Call the Phillies' office for updated information.

LOST & FOUND: If someone has lost or found a personal item, or if a person has become lost

from their group, please check in at the Advanced Ticket Window.

PARKING: Ample free parking for Phillies games is available in the Municipal Stadium parking lot or in the lot across from the stadium on Front Street. The Phillies' management, the Stadium Commission, and the City of Reading ARE NOT liable for damage to parked cars from foul balls or vandalism. Parking is free and done at your own risk.

PHILLIES' OFFICE: The Reading Phillies' Executive Offices are located in the main concourse next to the Advanced Ticket Window and across from the main concession stand.

PHONE NUMBERS: The Reading Phillies' office phone number is (215) 375-8469. The number for the Municipal Stadium pressbox is (215) 372-4836.

RAINCHECKS: If a game is postponed due to rain before it is officially completed, the Phillies will exchange the raincheck portion of your ticket for a ticket to any regular season game. Box seats will be exchanged for the same seat if available. The exchange can be made at the Phillies' office during regular business hours or at the main ticket booth at game time. No refunds will be given.

REST ROOMS: Men's and Women's rest rooms are located in the main concourse under sections 2 & 3 and behind the RF & LF bleachers.

SCHEDULES: Schedules are available at the main gate or Advanced Ticket Window during games and at the Phillies' office during regular business hours. Schedules can be obtained through the mail by writing the Reading Phillies, PO Box 15050, Reading, PA 19612 and including a self-addressed, stamped envelope.

SOUVENIRS: A wide variety of R-Phils, Philadelphia, and major-league souvenirs are available at the souvenir stand to the right of the main gate. Souvenirs are also available in the Phillies' office during regular business hours and year-round.

SPEEDBALL: How fast can you throw a baseball? Try our Speedball game and find out. Throw two pitches and then guess how hard you can throw your third. If you guess correctly, you'll win two tickets to a future Reading Phillies game. Speedball is located behind the RF bleachers.

TELEPHONES: Pay telephones are located in the main concourse next to the home clubhouse.

WHEELCHAIRS: A limited number of wheelchairs can be accommodated in the main grandstand in Box 22. Wheelchairs can also be accommodated in the right-field bleachers.

ENJOY YOUR VISIT TO MUNICIPAL STADIUM!!! WE HOPE TO SEE YOU AGAIN!!!



Society for American Baseball Research
March '92

The new house rules

Here are some highlights of the minor league stadium standards, set forth in the new PBA:

MIN. SEATING CAPACITY:

Class AAA — 10,000.
Class AA — 6,000.
Class A — 4,000.
Short season — 2,500.

TYPES OF SEATING:

General admission — Bench seating (minimum).

Reserved seating — a bench with a back.

Box seats — armchair seats with backs.

Recommended distribution: GA 50%, reserved 25%, box 25%.

COMFORT FACILITIES:

1 toilet per 450 men.

1 toilet per 125 women.

1 urinal per 125 men.

All women's restrooms shall provide mirrors and purse shelves; a table for changing diapers is recommended.

TICKETS:

1 ticket window per 1,500 seats.

1 turnstile per 1,500 seats.

CONCESSIONS:

5 lineal feet of counter space per 350 seats.

1 vendor per 350 seats.

Stadium clubs/beer gardens optional.

PARKING:

1 space per 3 seats (within half-mile).

HOME CLUBHOUSES:

Minimum 1,000 square feet (new facility).

Minimum 800 square feet (existing facility).

At least 5 more lockers than active player limit.

At least 8 showerheads (new facility); 6 showerheads (existing).

VISITING CLUBHOUSES:

Minimum 750 square feet (new facility).

Minimum 500 square feet (existing facility).

At least 3 more lockers than player limit.

At least 8 showerheads (new facility); 4 showerheads (existing).

PLAYING FIELD:

Outfield wall 8 feet high (minimum).

Warning track within 15 feet of walls.

Minimum grade 6 inches from base of mound to foul territory; 20 inches from second base to outfield warning track.

Solid, one-color batter's eye in center field, 40-by-16 feet (minimum).

Foul poles 30 feet high (minimum), with screen on fair side.

HARRISBURG
SENATORS

The logo for the Harrisburg Senators is presented in a two-tiered format. The top tier consists of the word "HARRISBURG" in a bold, black, sans-serif font. Below it, the word "SENATORS" is rendered in a highly stylized, gothic-style font with a stippled or textured fill. The letters of "SENATORS" are intricately designed: the 'S' is large and sweeping; the 'E' and 'A' are connected; the 'N' features a baseball bat graphic; the 'T' has two stars on its vertical stem; the 'O' is a baseball; and the 'R' is also large and sweeping. The entire logo is centered on the page.

AREA DEMOGRAPHICS:

Harrisburg is an older city which has enjoyed extensive urban renewal. The City has a population base of only 52,000, however, the metropolitan popular is around 500,000 people.

The city base is composed mostly of light industrial, banking, insurance companies and other such business oriented enterprises.

The ballpark was constructed on an "Island" in the middle of the Susquehanna River and is but one facet of an "Amusement Park" oriented theme encompassing the entire island. The island is accessible by both vehicular and foot traffic via bridges. Improvements were completed in 1987.

CONSTRUCTION FINANCING:

The construction cost for the ballpark with adjacent ancillary uses was \$5,000,000. The entire island improvements, including onsite parking, was \$9,000,000. All island improvements were financed by interest earned off Municipal Bonds floated to finance a \$392,000,000 hydroelectric plant. NO TAX DOLLARS WERE UTILIZED. Tax credits were issued to contractors as an incentive for work and other donations. Approximately \$250,000 was raised through private donations.

LEASES & COSTS OF OPERATIONS:

The ballpark is owned by the City of Harrisburg. It is leased to the Harrisburg Senators under a current annual fee of \$40,000 per year.

All operating costs are borne by the Senators. Capital improvement items are the responsibility of the City.

The Inspection Team was denied access to the Senator's operational budgets and income figures.

Income derived from ticket sales, concessions, novelties, etc., remain the property of the Senators.

The City constructed an administration/ticket office building adjacent to the stadium which is also leased to the Senators. Team officials and staff maintenance yearround occupancy in this facility.

The Senators are currently under a 10 year lease to the City

FACT SHEET

STADIUM

Riverside Stadium

CITY

Harrisburg, Pa.

CONTACT PERSON

Todd Vander Woude, GM

SEATING CAPACITY

Box Seats	727	\$6.00
Reserved Seating	1,626	\$5.00
General Admission	2,847	\$4.00 Adults \$2.00 Children

Total 5,200 Seats

AVERAGE ATTENDANCE

3,000 per Game

PARKING

1,000 spaces onsite
1,500 within walking distance
25 handicapped adjacent to stadium
50 players, staff, press, etc., adjacent to stadium

STADIUM FACILITIES

Locker Rooms	Separate for both teams with direct access to respective dugouts. Adjoining door.
Trainer's Room	Separate room with whirlpool in home locker room area. A trainer's area off visitor's locker room. No whirlpool
Other	Umpire's Room with private shower and lockers Laundry area with commercial washer & dryer Manager's and Coaches' room with private shower and lockers
Dugouts	Poured concrete, three steps deep into ground. Direct access to locker room
Locker Room Attendant	Part-time student hired by club

STADIUM FACILITIES

Rest Rooms

Ladies 1 large with 20 stalls
Men 1 large with trough
urinals and 5 stalls

Located adjacently under stands

(In lieu of advertising fee, a local florist supplies ladies' room with fresh flowers before each game, nice touch!)

First Aid Station

Wooden structure manned by nurse during games. Ambulance service available

GROUNDSKEEPING

One full-time, yearround person
Hires part-timers on as-needed basis. Has one full-time assistant throughout baseball season.

Groundskeeper, assistant, etc., employees for ballclub.

Own and maintain their own equipment.

Everyone from the General Mgr. on down assists in groundskeeping from time to time, i.e., placing and removing tarps.

PARK SECURITY

Two local policemen during games only. The entire island is policed by a Ranger hired by the City. Policemen are paid by Senators for this service.

There is a Security Command Post onsite

CONCESSIONS/NOVELTIES

There are 2 Concession Stands under the grandstand. There are also Mobil Units throughout the grounds.

There is one Novelty stand near the main entrance to the park.

Concessions and Novelties are run by "Sports Services" out of Buffalo, NY. The Senators receive a percentage of the proceeds.

CONCESSIONS/NOVELTIES

There are two separate stands which serve only beer.

There is a Vendor's station where the "in-the-stands" vendors are stocked.

Concession storage area is provided under the administration building which has a front to rear elevation change of about 10 feet.

TICKET WINDOWS

There are two ticket windows and two major points of ingress into the stadium. The ticket windows are incorporated into the administration building described above. This they did for security reasons.

AMENITIES

Three public telephones

Picnic area (fenced) outside rightfield foul line for concession patrons.

MEDIA FACILITIES

Media facilities are provided atop the grandstand (10x40 total) Radio, PA Announcer, Scoreboard Keeper and the Press occupy the booth.

The comment was made that the media facilities should have been subdivided for each function's privacy. Also, restrooms should have been provided in the media booth

SUMMARY

The setting of the ballpark, that is, on an island with pedestrian walkways and onsite parking, is an attraction within itself. The amusement park theme for the entire island affords the ballpark the ability to play off from the other activities and amenities of the entire island.

There is a strong "family oriented" theme throughout. It is relatively inexpensive entertainment for the whole family. The surrounding activities on the island include a marina with riverboat rides, a miniature railroad station with a kiddy-ride train, a miniature golf course, picnic areas, gazebos along the river bank, etc.. It is an attractive place to be, a place to unwind and relax. It is a clean and wholesome atmosphere providing clean and wholesome entertainment.

There is a strong sense of community between the team and the City residents. The inspection team got the feeling that the City was not looking to make profits, per se, from the park, but rather it is providing its residents with a recreational facility. There exists a strong level of commitment by the City to the ballclub to insure its continued success. The ballclub, conversely, provides public speaking by its players who, thereby, provide a role model influence on the area children. City officials felt the real benefit comes in the form of dollars pumped into the local economy. It was estimated that approximately \$1,500,000 per year change hands due to the ballpark facility alone.

This particular facility was planned out through "long-range" thought. The initial construction was such that expandability was accomplished with minimal expense and without major disruption to the existing facility.

Although there exists approximately 2,500 onsite and offsite parking spaces, team officials felt more parking is needed. They also felt that additional onsite storage for equipment, food goods, and the like was something they wished they had addressed more closely during the planning stages.

HARRISBURG SENATORS

Year	W	L	PCT	Overall Finish	Playoff Results	Attendance	Manager	Affiliate
1924	70	60	.538	4			A. Rankin Johnson	
1925	61	69	.469	5			A. Rankin Johnson	
1926	47	84	.359	8			Winn Clark	
1927	87	51	.630	1			Glenn Killinger	
1928	82	54	.603	1			John Tillman	
1929	75	62	.547	3		61,242	John Tillman	
1930	70	69	.504	4			Joe Cobbs	
1931	63	56	.527	1			Edward Onslow	Braves
1932	74	66	.528	2			Edward Onslow	Braves
1933	60	78	.441	7			Edward Onslow	Braves
1934	60	75	.444	8			Leslie Mann	Braves
	(31)	(39)	(.443)	8	First Half			
	(29)	(36)	(.446)	7	Second Half			
1935	59	77	.434	7	First Half		Art Shires	Senators
	(33)	(34)	(.493)	6	Second Half			
	(26)	(43)	(.377)	8	vs Reading 3-2	212,141	Dave Trembley	Pirates
1987	77	63	.550		vs Vermont 3-1	215,940	Dave Trembley	Pirates
1988	65	73	.471	7		200,196	Dave Trembley	Pirates
1989	71	65	.522	2	vs Canton-Akron 3-2	223,033	Mac Bombard	Pirates
1990	69	69	.500	5	vs Albany 1-3	233,423	Mike Quade	Expos
1991	87	53	.621	1	vs. Canton-Akron 3-1			
					vs. Albany 0-3			
17	1,197	1,122	.516		13-12 .520	1,416,975*		

League Champion: 1927, 1928, 1931, 1987

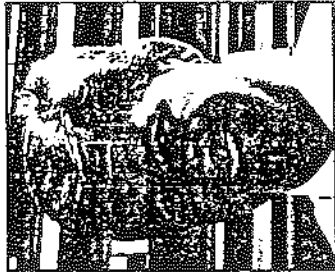
INDIVIDUAL SEASON BATTING RECORDS

Games	Ed Burke	1927	Most	1927, 1932, 1987	130	1924
At Bats:	140	1927	4759	1931	4292	1988
Runs:	560	1927	797	1927	519	1988
Hits:	134	1932	1393	1930	1129	1986
Total Bases:	385	1925	2056	1930	1486	1933
Doubles:	40	1930	227	1927	150	1933
Triples:	21	1930	109	1930	28	1988
Home Runs:	33	1925	98	1925	13	1933
RBI:	129	1925	707	1927	438	1926
Walks:	88	1925	552	1927	331	1925
Strikeouts:	112	1988	854	1991	338	1930
Sacrifices:	30	1928	63	1931	19	1989
Sacrifice Flies:	11	1988	185	1987	44	1934
Stolen Bases:	52	1987	185	1987	44	1934
Average:	.400	1925	.301	1927	.248	1988

INDIVIDUAL SEASON PITCHING RECORDS

Games:	54	Pete Young	1991	140	1927, 1932, 1987	130	1924
Complete Games:	22	Louis Poffi	1927	29	1969	13	1991
Shutouts:	5	Louis Poffi	1927	6	1991	6	1987
Wins:	23	Clint Brown	1928	87	1987, 1991	55	1935
Losses:	18	Chant Parks	1929	77	1988	51	1927
Saves:	15	Joe Ausanio	1990	44	1991	28	1988, 1989
Innings:	202	Chant Parks	1929	1257.1	1991	1116.1	1989
Strikeouts:	130	Bill Dietrich	1951	328	1987	369	1989
ERA:	160	Rich Savour	1987	916	1991	704	1990
	2.15	Clint Brown	1928	2.97	1991	4.24	1987

*Record incomplete

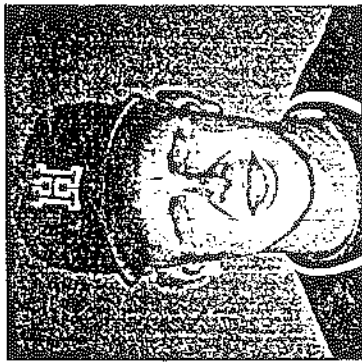


GENERAL MANAGER RICK REDD

The 1992 baseball season is Rick Redd's 11th in professional baseball management and his 6th as General Manager of the Harrisburg Senators. He began his baseball career in 1982 with the Columbus Clippers, then moved on to the Richmond Braves organization from 1983-1986. In 1987 the franchise was moved to Harrisburg (from Nashua, NH) and Redd became its first General Manager. The Senators captured the Eastern League title that inaugural season and Rick was named the League's Executive of the Year. He holds a Masters Degree in Sports Administration from the Ohio State University.

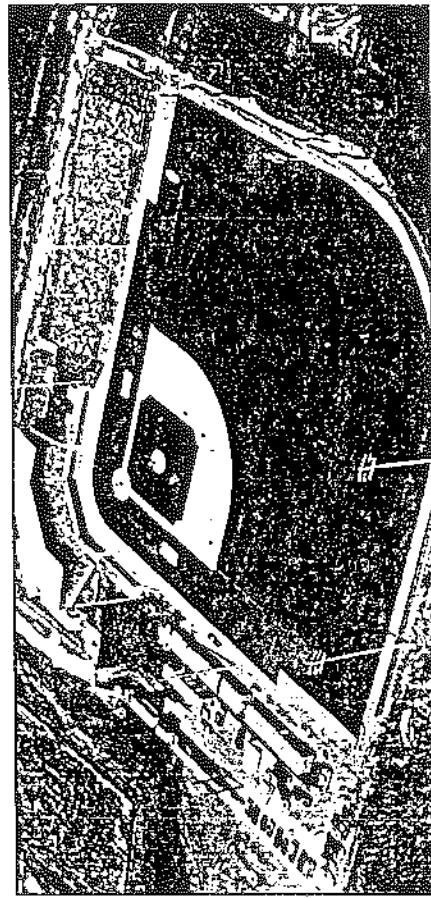
MANAGER MIKE QUADE

The 1991 Eastern League Manager of the Year, Mike Quade, returns for his second season with the Harrisburg Senators. Quade led the Senators to their best regular season record in 1991, as Harrisburg took the regular season crown. This season marks Quade's sixth in the Montreal Expos organization and his sixth overall, as a manager. Mike has also managed at Macon (1985-1986 for Pittsburgh) and at Rockford (1989-1990 for Montreal). Quade was drafted by the Pittsburgh Pirates where he played in the minors from 1979 to 1983.



Major League Affiliate: Montreal Expos
 Owner: Jerome Milleur
 President: Scott Carter
 General Manager: Rick Redd
 Assistant General Manager: Todd Vander Woude
 Director of Public Relations: Mark Matern
 Group Sales: Mark Clarke
 Ticket Operations: Karen Sambo
 Hutter
 Receptionist: Marion Geab
 Grounds Maintenance: Will Schnell
 Field Manager: Mike Quade
 Coach: Mike Parrott

Trainer: Jay Williams
 Name of Park: City Island Park
 Capacity: 5,600
 Outfield Distances: LF 335'
 CF 400' RF 335'
 Outfield Fence Height: 10'
 Address: P. O. Box 15757
 Harrisburg, PA 17105
 Telephone: (717) 231-4444
 Fax: (717) 231-4445



HARRISBURG SENATORS
RIVERSIDE STADIUM

1992 SENATORS INFORMATION



APRIL						
S	M	T	W	T	F	S
			1	2	3	4
5	6	7	8	9	10	11
12	13	14	15	16	17	18
19	20	21	22	23	24	25
26	27	28	29	30		

MAY						
S	M	T	W	T	F	S
					1	2
3	4	5	6	7	8	9
10	11	12	13	14	15	16
17	18	19	20	21	22	23
24	25	26	27	28	29	30

JUNE						
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7	8	9	10	11	12	13
14	15	16	17	18	19	20
21	22	23	24	25	26	27
28	29	30				

JULY						
S	M	T	W	T	F	S
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5	6	7	8	9	10	11
12	13	14	15	16	17	18
19	20	21	22	23	24	25
26	27	28	29	30	31	

AUGUST/SEPTEMBER						
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2	3	4	5	6	7	8
9	10	11	12	13	14	15
16	17	18	19	20	21	22
23	24	25	26	27	28	29
30	31	SEPT 1				

VISITING CLUBS & AFFILIATES
 AL - ALBANY YANKEES (N.Y. Yankees)
 BI - BINGHAMTON METS (N.Y. Mets)
 CA - CANTON AKRON INDIANS (Cleveland Indians)
 HT - HAGERSTOWN SUNS (Baltimore Orioles)
 LO - LONDON TIGERS (Detroit Tigers)
 NB - NEW BRITAIN RED SOX (Boston Red Sox)
 RE - READING PHILLIES (Philadelphia Phillies)

CATCH THE SENATORS HOME AND AWAY ON



* Played in Scranton ** AA All-Star at Charlotte, NC

■ HOME GAME □ AWAY GAME

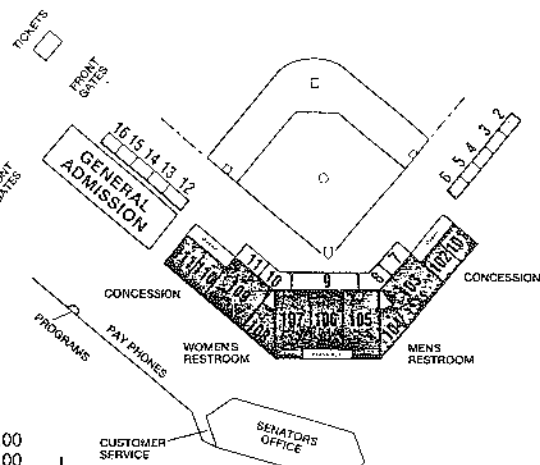


ADMISSION PRICES:
 Box Seats \$6.00
 Reserved Seats \$5.00
 General Admission
 Adults \$4.00
 Youth (12 & under) \$2.00
 Seniors (over 59) \$2.00

General Admission Bonus Books
 10 Tickets \$35.00
 25 Tickets \$80.00

HOW TO ORDER TICKETS:
 Senators tickets can be ordered by phone and charged to MasterCard or Visa; or purchased by mail with a personal check. Call the Senators office at (717) 231-4444. Ticket orders called in at least one week prior to the game will be mailed; orders placed less than one week prior to the game will be left in your name at the will call window. You can insure yourself of having the best seats available to all Senators home games by purchasing tickets in advance at the Senators Office - at RiverSide Stadium, between 9 AM and 5 PM, Monday through Friday; and during any Senators Home Games at the Customer Service Window.

GROUP PACKAGES:
 Special discounts are available for groups of 25 or more. Groups coming to the game will receive recognition over the Senators public address system. All group tickets must be purchased at least 48 hours prior to the game that your organization would like to attend. Please contact the Senators office at 231-4444 for further details.



PUBLIC PHONES:
 Pay phones are located in the main concourse behind the 3rd base reserved grandstand.

PROGRAMS:
 The program stand is located on the main concourse behind the 3rd base reserved grandstand.

FOUL BALLS:
 Foul balls can be very dangerous. Please keep your attention focused on the field while the game is in progress. Any foul ball into the stands may be kept. **Absolutely no one is permitted on the playing field to retrieve a ball.**

FANS NOT ALLOWED ON FIELD:
 Any fan entering the playing field will be subject to fine, imprisonment or both.

CONCESSION STANDS:
 Full service concession stands are located behind each dugout underneath the reserved seating section. Senators and major league souvenirs may be purchased at the trailer located behind Box Section 6 by first base.

BEER SALES:
 Beer is available at RiverSide Stadium. Beer sales will begin one hour prior to game time and conclude at the end of the 7th inning on a regular basis. Pennsylvania law prohibits the selling to or purchasing of beer for minors. The Senators management reserves the right to discontinue beer sales without notice and/or refuse to sell beer to any individual.

RAIN CHECK POLICY:
 Tickets for any game rained out may be exchanged for tickets of equal value for any remaining regular season home game - subject to availability. No cash refunds will be given.

FOOD AND DRINK:
 In compliance with Eastern League Rules, no cans, coolers, bottles, alcoholic beverages or food are permitted to be brought into the stadium.

FIRST AID:
 First aid is available in the trailer next to the souvenir stand located behind first base.

LOST AND FOUND:
 Items found should be turned in at the Senators Customer Service Window, located at the Senators Office.

RESTROOMS:
 The womens restroom is located underneath the grandstand on the 3rd base side of the main concourse; the mens restroom on the 1st base side.

REPORT

Inspection of "AA" baseball facilities

at

Albany and Binghamton, New York

July 27 & 28, 1992

Team: George A. Flaherty
Richard Lauck
Sam Hoffses
Donn Mathews

Sites:

Albany-Colonie, N. Y. July 27
Albany Yankees
New York Yankee Farm Team

Binghamton, New York July 28
Binghamton Mets
New York Mets Farm team

ALBANY -- COLONIE YANKEES

The Albany-Colonie Yankee facility is located in a rural area of the town of Colonie, a suburb of Albany. The facility is called Heritage Park.

Capacity: 5,500

Constructed: 1983

Outfield distances: Left, 325'
Right, 335'
Center, 400'

Outfield fence height: 8' to 16'

The area around Heritage Park is open fields; there is no residential housing. Access to the ballpark is over a two lane rural type paved roadway. Access to the Interstate System is nearby. There were no traffic signal systems.

Three parking lots serve the field. One is at the main entrance and this is paved. Another is along the third base side and is unpaved. The third is a large, paved lot beyond the outfield. Total capacity: approximately 3000 cars. The fee is 1.25. The parking is on town property and the town collects the fees and supplies security. There are no sidewalks from the parking lots or in the area of the field.

The stadium has an open seating area and a covered area in the center which includes the press box. The stadium is owned jointly by the town of Colonie and the County, but the County does not take an active role.

All standards for the stadium and its operation are set by the PBA. This stadium has one concession stand for food, two stands (trailers) for souvenirs. All concessions are inside the fence and run by the ball team. During games the team employs 45 people to take tickets, usher and run concessions.

There are 4 ticket windows. Prices are: Box, 6.50; reserved, 5.00 and general admission 4.00.

TEAM MANAGEMENT

The inspection team met with the general manager, Ralph Acampora. The inspection team also gathered excellent information from John Liburdi, head groundskeeper. The latter also works for the town of Colonie. There are three other persons who work in the administrative office which is located under the stands behind home plate.

During games the team employs 45 people to take care of ticketing, ushering and concessions.

There are 4 ticket windows. Prices are: Box, 6.50; Reserved, 5.00 and general admission, 4.00.

SECURITY/FIRST AID

During games the team hires a private security force of 2 to 4 depending on the event (When an old-timer like Phil Rizzuto visits they use at least 4). First aid is covered by the local EMS team, which simply shows up for games. The facility needs improvement of handicapped access.

GROUNDSKEEPING

This is done by town employees who also work on City parks. The town has its own equipment which is stored in a maintenance building behind the left field fence. A crew of three cleans the stands by blowing off the loose paper and then hosing everything down.

The field is used every day. When the Albany Yankees are not there the local school uses the field as well as other baseball leagues. Cost to rent the facility is \$200 per day plus \$200 for lighting. This does not reflect true cost to operate which is about \$1,000.

The groundskeeper pointed out that there are major concerns with some aspects of the facility: you need many more women's toilets than are required by the league so as to avoid long lines. The concession stands must serve very quickly so that people do not miss parts of the game. Adequate storage for all operations is a must. The concessions must have walk-in freezers and coolers. Team equipment and supplies need adequate storage.

Another major item: the backstop behind home plate should not be of chain link fence, rather, a suspended net is better for viewing.

STADIUM CONSTRUCTION

This facility was constructed without a complete set of league rules and is regarded as one of the least desirable in the league. The right field bleachers are steel fram with aluminum, shaped benches and backs. The covered section of the stadium is steel and pre-cast concrete. Plastic seats with backs are used in this section. Left field seating is steel and aluminum benches. The seats are easy to clean and wash down. The team supplies the cleaning crew. The dugouts are concrete block with pre-cast concrete slab roof.

The locker training room for the home team and the restrooms for same are located under the right field seating and are made of concrete block. The restrooms are small and generate many complaints.

Also under the right field stands are eating concessions with a special "Cafe" just outdoors on grade level. Near this Cafe is an enclosed (net) area where patrons can throw a baseball and have the speed measured; this is a money-maker for the team.

Under the covered seating behind home plate is the main office as well as the cooking area for food sold. Beer is sold on site but all sales for beer stop at the 8th inning. This section is also concrete block with a slab roof. All cooking equipment is owned or leased by the team.

The visiting team has facilities located outside the left field fence. The structure is wood frame with t-111 siding on a slab. Also in this area is the weight room and storage for the town equipment in a building similar to the visitors' clubhouse. There is good access for the visiting team bus.

Some problems with this facility: Administrative offices are small and cluttered (poor storage), home team facilities are too small as is the training area. The visitors' clubhouse is distant from the center of activity. The team areas need custodial support. As noted before, restrooms are inadequate.

PRESS BOX/MEDIA FACILITY

The press box is small. Radio broadcasting takes place. The scorekeeper is seated in the press box. The announcer and controller of the scoreboard work here also. T. V. cameras and film are set up on wings just outside the press box. The scoreboard is capable of sending messages.

BINGHAMTON METS

The Binghamton Mets facility is located in downtown Binghamton on the site of an old railroad yard. The town population is 55,000 and the area is about 210,000 in population. Average attendance at the games is 4,000, or 65% of capacity.

Financing of the facility: this was done privately; the facility is owned by the Mets, but is leased to the town. The attached lease agreement will explain this. The cost of this new facility was 4.5 million dollars. The current season is the first played in the stadium. The design of the stadium was monitored closely by the general manager, who called in a specialist in stadium design when they ran into problems.

The facility is rented to local teams, but the general manager retains complete control during such events, that is, he uses all of his people to run the operation. The team retains all parking and concession revenues.

The operating budget for the team was not available.

TEAM MANAGEMENT

The team is owned by the New York Mets (it is currently "for sale"). The general manager is R. C. Reuteman. There are five other persons in the administrative offices.

The head groundskeeper is Shaun Lynch, who is in training. He is assisted by two part-time persons. During games and events the team employs up to 100 persons for ushering, concessions and security.

FACILITY

This new facility seats 6,000. It is construction of concrete and steel. All seating is of the same type (plastic fold up seats with a back). The facility is completely handicapped accessible. It is all on grade except for a batting cage underground.

The facility has quite adequate locker rooms, trainer's room, umpire's room as well as laundry and supply rooms. There is a private lounge for the team. The restrooms are large and very adequate. They include a pull-down changing table for infants. There is a special first-aid facility. All of the above is located under the stands in concrete block and steel structures. An important feature of the stadium is a contiguous concourse which allows for good crowd movement. There are two concession stands and an additional cafeteria with an outdoor eating area. The souvenir shop is open year-round.

There are four ticket windows and plenty of space for crowds to enter. Persons can come to the stadium during the day to purchase tickets.

The seating includes "boxes" (2,400 seats). There is plenty of space for persons in wheelchairs.

PRESS BOX/MEDIA

The press box is large and the working areas are off a hallway which also serves the four "skyboxes". One of the latter is rented for \$15,000 per year. The press box is 200 s. f. and has restrooms. It is served by an elevator. There are two photography pits on the field. The stadium has a \$175,000 scoreboard provided by Budweiser. It is capable of all kinds of graphics.

In summary, this new facility was designed by persons with intimate knowledge of the needs of a ballpark. It addresses the needs of administration and making money. It also addresses the needs of a family-oriented entertainment facility. The general manager, R. C. Reuteman, retains a tight control of all aspects and events at the stadium. From the press box to the restrooms the details are carefully worked out and the result is a pleasant facility.

The photographs show this better than words.

ATTACHMENT D

FACILITY STANDARDS AND COMPLIANCE INSPECTION PROCEDURES (Rule 40)

Standards

Unless expressed as recommendations, these facility standards are minimum requirements for all new Minor League facilities. Notwithstanding its facility's designation as a "new facility," a Minor League Club which can demonstrate that its new facility construction planning and approval process was at such a stage as of November 17, 1990 that requiring compliance with a minimum new facilities standard (other than those outlined in Sections 11, 12 and 13) will cause it to suffer a material hardship, may apply to the President of the Minor League Association for a variance from such standard. The standards outlined in Sections 11, 12 and 13 are applicable to both new and existing facilities.

New Facilities

Any facility which is scheduled for a construction starting date of January 1, 1991 or later shall be considered a "new facility." All plans for new facilities, including construction time schedules, must be submitted to field inspection personnel designated by the Commissioner's office and to the President of the Minor League Association, for review and approval by the field inspection personnel prior to the start of construction. Such review must be completed within thirty (30) days after submission or the plans shall be deemed approved. If such plans meet the standards they shall be approved.

Existing Facilities

Any facility other than a "new facility" as defined above shall be considered an "existing facility." All existing facilities must meet the standards outlined in Sections 11, 12 and 13 (playing field and other team facilities) by no later than April 1, 1994. All plans for additions, alterations or renovations of such facilities, including new turf installations, must be submitted to field inspection personnel designated by the Commissioner's office and to the President of the National Association, for review and approval by the field inspection personnel (including construction time schedules) prior to the start of construction. Such review must be completed within thirty (30) days after submission or the plans shall be deemed approved. If such plans meet the standards they shall be approved.

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SECTION 1.0 SEATING

This section establishes standards for the number, type and arrangement of seating in all facilities.

1.1 SEATING CAPACITY

Seating capacities shall be established to be appropriate for the size of the minor league club's market. Recommended minimum capacities are as listed below. All facilities shall conform with the seating grade, seating distribution and spacing requirements described in sections 1.2, 1.3 and 1.4.

1.1.1	Class AAA Capacity	10,000 seats
1.1.2	Class AA Capacity	6,000 seats
1.1.3	Class A Capacity	4,000 seats
1.1.4	Short Season Class A/Rookie	2,500 seats

1.2 GRADES OF SEATING

In order to enhance the professional atmosphere of the facility, each facility shall provide a minimum of two separate and distinct grades of seating (three separate and distinct grades are recommended). This provision is intended to designate and define general types of seating and not to define pricing or ticketing structures.

1.2.1 TYPES OF SEATING

Seating types shall be defined as in sections 1.2.2, 1.2.3, and 1.2.4.

1.2.2 BOX SEATING

Defined as Arm Chair Seats with Backs. Additional seat width and leg room is recommended, with an additional three (3) inches of tread width to be provided as compared to the tread width in the other seating areas. Following the traditional definition of box seating, it is recommended that additional access to smaller groupings of box seats be provided.

1.2.3 RESERVED SEATING

Defined as a bench with back as a minimum requirement.

1.2.4 GENERAL ADMISSION SEATING

Defined as a bench as a minimum requirement.

1.3 SEATING DISTRIBUTION

In no event shall more than 90% of the total seating capacity be General Admission seating. Recommended seating distributions are as follows.

For two grades of seating:

Box or Reserved: 25% of total capacity
General Admission: 75% of total capacity

For three grades of seating:

Box: 25% of total capacity
Reserved: 25% of total capacity
General Admission: 50% of total capacity

1.4 SEAT SPACING

The spacing and layout of all seating, aisles, vomitories, cross-aisles and concourses comprising the established exiting system shall conform to all applicable local, state and federal codes and regulations. (NFPA.101 for Assembly Occupancies shall be considered the minimum requirement if the facility does not fall under jurisdiction of other regulations.)

1.5 HANDICAPPED ACCESSIBILITY

All facilities shall comply with all applicable local, state and federal codes and regulations regarding access of Handicapped patrons and employees. (ANSI.A117-1 shall be considered the minimum requirements.)

SECTION 2.0. PUBLIC COMFORT STATIONS

This section determines and defines the number of plumbing fixtures and their arrangement at the facilities.

2.1 COMFORT STATION DISTRIBUTION

The distribution of the fixtures should be in accordance with the distribution of the seating locations and exiting system to allow minimal walking distances from all parts of the facility to public toilet facilities.

2.2 PLUMBING FIXTURES

The minimum plumbing fixture ratios shall be as follows:

Water closets	1:125 Women
	1:450 Men

Lavatories (sinks) 1:150 Women
1:150 Men

Urinals 1:125 men

2.2.1 COMFORT STATION ACCESSORIES

All public restroom facilities shall provide mirrors, purse shelves (in women's), hand drying facilities and trash cans. It is recommended that a table/platform for diaper changing be located in each restroom.

2.3 HANDICAPPED ACCESSIBILITIES

All facilities shall comply with all applicable local, state and federal codes and regulations (ANSI. A117-1) It is recommended that all facilities provide a minimum of one, unisex h.c. toilet facility per level. This facility shall be similar to a residential bathroom, and allow a h.c. patron to use the facility with the assistance of his/her companion of the opposite sex.

2.4 DRINKING FOUNTAINS

All facilities shall provide drinking fountains per local, state and federal codes and regulations.

2.5 PUBLIC TELEPHONES

All facilities shall provide telephones per local, state and federal codes and regulations.

SECTION 3.0 CONCESSION AND VENDING

The following standards for Concessions and Vending are recommended for all facilities. Many of the conditions may be affected by an existing operational agreement between the facility and concessionaire. It is recommended that these standards be incorporated into any new operational agreement negotiated after the effective date of this PBA.

3.1 CONCESSION AREAS

It is recommended all facilities provide 5 lineal feet of counter space (with corresponding support space) per 350 seats in the total facility capacity. The distribution of the concession areas shall be commensurate with the distribution of the patrons to minimize walking distances. [Example: 12,000 seats/350 = 34.28 X 5' = 171 lineal feet of counter. Each stand averages 25' per stand. Therefore, a minimum of 7 stands, distributed throughout the facility are recommended.]

3.2 CONCESSION VENDORS

If concession vendors are provided at the facility, the following ratios are recommended: one vendor per 350 seats, with 15 sq.ft. of vending commissary space for each vendor separate from the concession areas.

3.3 CONCESSION COMPLIANCE/CODES AND REGULATIONS

Concessionaires are responsible for compliance with all local, state and federal regulations in regard to Health Standards, Fire Department regulations, power, exhaust and ventilation requirements. The agreement between the facility and concessionaire shall define which party is responsible for required modifications.

3.4 CONCESSION STORAGE AND NOVELTY STANDS

The following standards shall be minimum requirements.

3.4.1 CONCESSION STORAGE

All facilities shall provide adequate storage for concession inventory. It is recommended that the storage area be of such size to store the inventory necessary to stage the number of games in an average home stand. In the Agreement between the facility and the concessionaire, the concessionaire shall provide empirical data to determine the required amount of storage space.

3.4.2 NOVELTY STANDS

Any provided novelty stand(s) acting as a sales point for retail sales shall present products in a professional manner commensurate with a standard retail sales areas.

SECTION 4.0 MISCELLANEOUS PUBLIC AREAS

4.1 STADIUM CLUB/RESTAURANT/BANQUET FACILITY

This type of facility shall be optional.

4.2 PICNIC/BEER GARDEN FACILITY

This type of facility shall be optional.

4.3 FAMILY RECREATION AREA

This type of facility shall be optional.

SECTION 5.0 TICKET WINDOWS AND ENTRY TURNSTILES

The following Sections 5.1.1, 5.2.1, and 5.3.1 shall be minimum requirements.

5.1 TICKET WINDOWS

All facilities shall provide one (1) ticket window for each 1500 seats of total capacity.

5.2 TURNSTILES/ENTRY POSITIONS

All facilities shall provide one (1) turnstile or equivalent entry position (minimum of 30" wide) for each 1500 seats of total capacity.

5.3 HANDICAPPED ACCESSIBILITY

All facilities shall provide access per all applicable local, state and federal codes and regulations to all public and private areas of the facility. (ANSI A117.1)

SECTION 6.0 SECURITY AND FIRST AID

6.1 SECURITY COMMAND POST

All facilities shall provide a "command post" for event security forces, centrally located with provisions for removing unruly patrons from the facility.

6.2 FIRST AID STATION

All facilities shall provide a first aid station during all events. It is recommended that certified medical personnel staff the station at all events.

SECTION 7.0 PARKING AND FACILITY ACCESS

The following Sections 7.1.1, 7.2.1 and 7.3.1 shall be applicable to all facilities.

7.1 PARKING SPACES

It is recommended all facilities shall provide public parking spaces at a ratio of 1 space per 3 seats of total capacity. Such parking spaces shall be on-site or within a 10 minute (1/2 mile) walking distance of the stadium.

7.2 ACCESS AND CONTROL

All facilities shall coordinate with local law enforcement officials to provide controlled on-site traffic access, so as to promote a safe and trouble-free access environment.

7.3 HANDICAPPED PARKING

All facilities shall conform with all applicable local, state and federal regulations.

SECTION 8.0 SOUND SYSTEM AND SCOREBOARD

8.1 SOUND SYSTEM

All facilities shall provide an acoustically balanced sound system integrated with the capacity to deliver clear audio messages to the press box, concourses and all public areas within the facility.

8.2 SCOREBOARD

All facilities shall provide a scoreboard that provides the following as minimum requirements. All scoreboard characters are to be large enough to be seen throughout the facility.

Line Score
Ball-Strike-Out
Player at Bat

8.3 SCOREBOARD LOCATION

No part of any scoreboard and/or associated lighted advertising panels may be located within 50' of the center line of the playing field.

8.4 CLOCK

All facilities shall provide a time of day clock which will be in full view of all field personnel from the beginning of batting practice through the close of each game.

SECTION 9.0 MEDIA FACILITIES

9.1 PRESS PARKING AND ACCESS

It is recommended all facilities provide a parking area for all members of the media with direct access to the facility. It is also recommended parking be provided for television vans and broadcast trucks.

9.2 PUBLIC ADDRESS/SCOREBOARD PERSONNEL

All facilities shall provide space in the press box for the public address announcer and scoreboard operator(s). It is recommended the PA/scoreboard area have a minimum of 50 sq. ft. of floor space in addition to the floor space required for the scoreboard equipment.

9.3 RADIO BROADCAST BOOTHS

It is recommended all facilities provide two (2) radio broadcast booths (home and visitor) that provide a direct view of the entire field and facilitate the broadcast of the game. Each shall provide counters, chairs, power, lighting and telephone jack.

9.4 TELEVISION BROADCAST AND CAMERA BOOTH

It is recommended all facilities provide a spare broadcast/camera booth available for local television broadcasts and local television media. The booth should have a direct view of the entire field with operable windows or closures.

9.5 PRINT MEDIA AREA

It is recommended all facilities provide a separate area for 6 to 10 members of the print media with a direct view of the entire field. Counter, chairs, power, lighting and telephone jack shall be provided.

9.6 MEDIA TOILET FACILITIES

It is recommended all facilities provide media restroom facilities separate from public restrooms, located with direct access to the press box.

9.7 MEDIA WORKROOM/LOUNGE

This type of facility shall be optional.

9.8 HANDICAPPED ACCESSIBILITY TO PRESS BOX

Facilities shall conform to all applicable local, state and federal codes and regulations for accessibility to the press box. (ANSI-A117.1)

SECTION 10.0 ADMINISTRATION AREA

10.1 FACILITY ADMINISTRATION AREA

It is recommended all facilities provide administrative space of 250-300 sq. ft. per person for facility and maintenance operations with separate toilet facilities directly adjacent.

10.2 STADIUM PERSONNEL DRESSING/LOCKER FACILITIES

It is recommended all facilities provide separate dressing/locker facilities (separate for each sex) for all maintenance and event employees (including concession personnel) separate from the public.

10.2.1 STADIUM PERSONNEL TOILET FACILITIES

It is recommended all facilities provide toilet facilities for stadium personnel separate from the public. Direct access to personnel locker rooms is desirable.

10.3 TEAM ADMINISTRATION AREA

If the tenant team has a permanent administration area away from the facility, an on-site game day team administration area must be provided. If the team's permanent administration area is at the facility, it is recommended the area provide 250-300 sq. ft. per person for team operations with adjacent toilet facilities.

SECTION 11.0 TEAM FACILITIES

The following shall be minimum requirements.

11.1 HOME CLUBHOUSE/DRESSING AREA

The number of lockers provided shall be at least five more than the club's active player limit for their classification of play. The minimum of size of each locker shall be 24" w x 72" h (36" w x 72" h is recommended). A lockable storage compartment is recommended for each locker.

Minimum floorspace requirements for the team dressing area shall be as follows:

New facility: 1,000 sq. ft.

Existing facility: 800 sq. ft. (1,000 sq. ft. is recommended)

11.2 SHOWER AND TOILET FACILITIES

All facilities shall provide separate shower, drying and toilet areas with the following minimum fixture counts:

New facility:	shower heads: 8 (10 recommended)
	water closets: 2
	urinals: 2
	laboratories: 4 (8 recommended)

Existing facilities: shower heads: 6 (8 recommended)
water closets: 2
urinals: 2
lavatories: 2 (4 recommended)

11.3 TRAINING ROOM

All new facilities shall provide a separate training room of not less than 300 sq. ft. divided into three areas: treatment, whirlpool and rehabilitation. The training room shall have space for 1 or 2 treatment tables, a minimum of 2 whirlpools, hydroculator, scale, stationary bicycle, ice machine and an area for 2 or 3 pieces of rehabilitation/weight equipment. The training room shall contain a lockable storage area for training supplies. It is recommended that additional space be provided for a separate office/dressing area for the trainer and team physician. It is also recommended that a valuable storage box be installed in the training room.

All existing facilities shall comply with the above paragraph, with the exception that the minimum square footage requirement shall be 175 sq. ft. (300 sq. ft. is recommended).

11.4 TEAM LAUNDRY FACILITY

All facilities shall provide commercial quality laundry facilities (washer and dryer) for the home team to provide daily washing capability. This room may be combined with the Team Equipment Room.

11.5 TEAM EQUIPMENT ROOM

All facilities shall provide adequate lockable equipment storage space (minimum of 300 sq. ft. in a new facility) contiguous with the clubhouse.

11.6 COACHES LOCKERS

All new facilities shall provide a minimum of 4 coaches lockers (6 are recommended) in addition to the players lockers. It is recommended these lockers shall be in a separate area from the players lockers. Locker size and floor space requirements (per capita) shall be the same as in the players dressing area.

Existing facilities shall comply with the above paragraph, with the exception that a minimum of 3 coaches lockers are to be provided.

11.7 FIELD MANAGER'S OFFICE

All facilities shall provide a field manager's office with direct access to the home clubhouse. It shall include a separate toilet, shower and dressing area, along with a desk and

adequate meeting space for 6-8 persons. At existing facilities the separate toilet, shower and dressing area is recommended and not required.

11.8 VISITORS CLUBHOUSE/DRESSING AREA

The number of lockers provided shall be at least three more than the club's active player limit for their classification of play. Minimum floor space requirements shall be as follows:

New facility:	750 sq. ft.
Existing facility:	500 sq. ft (750 sq. ft. is recommended)

11.9 VISITORS SHOWER AND TOILET FACILITIES

All facilities shall provide separate shower, drying and toilet facilities with minimum fixture counts as follows:

New facility:	showers heads:	6 (8 recommended)
	water closets:	2
	urinals:	2
	lavatories:	4

Existing facility:	shower heads:	4 (8 recommended)
	water closets:	2
	urinals:	2
	lavatories:	2 (4 recommended)

11.10 VISITORS TRAINING ROOM

All new facilities shall provide a separate training room (minimum of 150 sq. ft.) with space for one training table and one whirlpool. In existing facilities, this area may be integrated into the players' dressing area, provided that the dressing area is at least 650 sq. ft.

11.11 VISITING FIELD MANAGER'S OFFICE

All facilities shall provide a separate office for the visiting field manager. It shall include a separate toilet, shower and dressing area, along with a desk and adequate meeting space for 2-4 people. At existing facilities, the separate toilet, shower and dressing area is recommended and not required.

11.12 TEAM STORAGE (MAJOR LEAGUE PARENT TEAM)

It is recommended that all facilities provide a minimum of 300 sq. ft. of lockable team storage, separate from other team storage, with year round access only to the major league team.

11.13 UMPIRE FACILITIES

All facilities shall provide a private dressing, shower, and toilet facility for umpires. This area shall provide enough lockers (each a minimum of 36" w x 72" h) to accommodate the number of umpires typically assigned to work in the applicable classification of play. In new facilities, this area shall be a minimum of 200 sq. ft.

11.14 FIELD/DUGOUT ACCESS

It is required that all new facilities and recommended that all existing facilities provide a direct access route to the dugout/playing field. Similar access is to be provided for the umpires.

11.15 PLAYER PARKING

It is recommended that all facilities designate a parking area with clubhouse access for players and other uniformed team personnel.

11.16 HITTING/PITCHING TUNNELS

It is recommended that all facilities provide 2 covered tunnels for players to practice hitting and pitching in an enclosed environment. If provided, these tunnels should be reasonably close to the home clubhouse with minimal public access.

11.17 PRE- AND POST-GAME WAITING AREA

It is recommended that all facilities provide a pre-game and post-game waiting area for families of players and other uniformed personnel.

SECTION 12.0 PLAYING FIELD

12.1 FIELD DIMENSIONS

Layouts of all new fields (and modifications to existing fields) shall be submitted for approval by the parent Major League club and the National Association club. All field dimensions shall comply with the minimum dimensions specified in Section 1.04 of the Official Baseball Rules.

12.2 PLAYING SURFACE

All facilities shall provide a field surface (natural or synthetic) without major defects and/or "trip-hazards" that could affect the normal play of the game. Warning track material shall identify all zones within 15' of all walls and fences. This warning track must be of a material to provide

visual and tactile notice of a significant change in surface type.

12.3 FIELD GRADE

The maximum allowable grade from the base of the pitcher's mound to the warning track in foul territory shall be 6". The maximum allowable grade from second base to the outfield warning track shall be 20".

12.4 FIELD WALL

The permanent outfield wall or fence in all new facilities shall be a minimum of 8' high.

12.5 BULLPENS

All facilities must provide a bullpen area for each team. These areas may be located in foul territory down the baselines or just immediately outside the field wall. Each must be visible to both dugouts and to the press box. Each shall have two regulation pitching mounds and home plates, adequate distance and clearance for each pitcher and catcher, and a bench for 10 players. If the bullpens are in foul ball areas, care shall be taken to integrate the slope of the pitcher's mound into the field so as not to create a trip hazard for fielders as they approach the bullpen. It is recommended that all facilities have phones connecting the bullpens to the dugouts.

12.6 DUGOUTS

All facilities must provide two enclosed dugouts (home and visitor). Each dugout in a new facility must accommodate 25-30 uniformed personnel on a bench with seatback. Each dugout in an existing facility shall accommodate 20-25 uniformed personnel. Each dugout must have a helmet rack for a minimum of 15 helmets and a bat rack for a minimum of 30 bats. It is recommended that a bat swing/storage area be directly accessible to each dugout. It is recommended that each dugout include a refrigerated water cooler (drinking fountain) and provide direct access to a restroom. It is recommended that all facilities have telephones connecting the dugouts to the bullpens and to the press box. All dugouts shall provide as feasible an anti-skid surface as possible on steps and walkways.

12.7 FIELD EQUIPMENT

All facilities shall provide the following field equipment. Examples given shall serve as guidelines for equipment quality, and the equipment provided shall meet or exceed the examples specified.

12.7.1 BATTING CAGE

All facilities shall provide a full cover batting cage. New batting cages shall have minimum dimensions of 18' wide, 14' deep and 9' high. It is recommended that the cage be portable and made of an aluminum frame to provide maximum maintainability. Existing batting cages not meeting the above standards may be approved by the parent Major League club.

12.7.2 FIELD SCREENS

All facilities shall provide a pitching screen, first base screen, 2nd base/double play screen, and a shag protector screen. New screens shall have the following minimum dimensions:

Pitching Screen - 7' h x 8' w with 4' x 4' notch in upper corner.

Double play screen: 7' h x 14' w with hinged wings.

First base and shag protector screens - 7' h x 8' w.

All existing screens not meeting the above standards may be approved by the parent Major League club.

Periodic checks of the batting cage and all screens shall be performed to verify frame and net integrity.

12.7.3 BATTER'S EYE

All facilities shall provide a solid monochromatic batter's eye with minimum dimensions of 16' high and 40' wide centered in the outfield. If a centerfield camera is integrated into the batter's eye, it must be the same color as the batter's eye. It is recommended that all new facilities provide a batter's eye with minimum dimensions of 40' high and 80' wide.

12.7.4 FOUL POLES

All facilities shall provide two foul poles of a bright color which are a minimum of 30' high (45' is recommended) with a screen to the fair side of the pole. No white signs shall be allowed immediately adjacent to each side of the foul pole.

12.7.5 FLAG POLE

All facilities shall provide a flag pole for the United States Flag or Canadian Flag in clear view of the entire seating bowl.

12.8 FIELD LIGHTING

All new lighting systems shall maintain the following minimum brightness requirements after 100 hours of burning:

Class AAA and Class AA: 100 fc average in infield/70 fc average in outfield.

Class A and Rookie: 70 fc average in infield/50 fc average in outfield.

The height and location of poles in all new lighting systems shall following IES standards. [See Exhibit A].

All existing lighting systems shall maintain the following brightness requirements:

Class AAA and Class AA: 70 fc average in infield/50 fc average in outfield.

Class A and Rookie: 60 fc average in infield/40 fc average in outfield.

All lighting systems shall operate with a maximum variance ratio of 1.2/1 in the infield and 2/1 in the outfield. The variance ratios shall be computed by comparing the highest and lowest footcandle readings in the infield and the outfield.

12.9 BATTING CAGE GATE

All new facilities shall provide a gate large enough to allow the batting cage to be freely taken to and from the playing field.

12.10 BACKSTOP

All facilities shall provide a backstop behind home plate. The configuration and dimensions shall vary due to sight-lines for the press box and insurance requirements for the facility. Periodic inspections shall be performed to insure the integrity of the backstop.

12.11 PLAYING FIELD TARPS

All Class AAA, Class AA and full season Class A facilities shall provide a full infield tarp and pitcher's mound, home plate, base pit, and bullpen tarps, except that this requirement may be waived by the President of the Minor League Association in the event that the facility is located in an area that does not experience sufficient rainfall to justify the expense of tarps. The tarps shall be oversized to prevent water from running under the edge to a dirt area. The tarps shall be stored in an easily accessible location but in a way not to create a safety hazard on the playing field. Each facility is required to provide adequate manpower to operate the placement and/or removal of the tarps.

SECTION 13.0 MAINTENANCE

This section outlines requirements and recommendations for overall maintenance of the facility and playing field in a professional manner.

13.1 FACILITY MAINTENANCE AND CLEANLINESS

Each facility shall develop a maintenance program (both short-term and long-term) for use by its maintenance personnel. All public areas shall be completely free of trash and rubbish at the opening of each event, and stadium personnel shall be responsible for cleanliness during the event.

Each facility shall follow its maintenance program for interior repairs and touch-ups to maintain the professional atmosphere of the facility. Long-term maintenance shall be ongoing in order to deter major facility problems and to minimize potential disruptions to the public.

13.2 FIELD MAINTENANCE

The playing field shall be maintained at the highest possible professional level. Every reasonable effort shall be made to insure the safety of the players and the smooth play of the game. The facility shall follow professional grounds-keeping practices and shall utilize proper maintenance equipment. Nail-drags, screens, tampers and rakes are recommended to maintain all dirt areas. Proper turf care equipment (mowers, tractors, etc.) shall be used, and an appropriate maintenance plan shall be developed and followed to care for the playing field.

13.2.1 PLAYING FIELD RECONDITIONING

The pitcher's mound and base pit areas shall be reconditioned prior to each game through the use of clay materials and tampers.

13.2.2 FIELD MAINTENANCE MATERIALS

All facilities are required to have a sufficient amount of drying material on hand at all times for reconditioning the infield. A chemical drying agent and/or calsonite clay may be used in combination with sand to stabilize areas affected by excessive moisture. Sand may not be the sole drying agent.

13.2.3 LAYOUT OF PLAYING FIELD

The entire playing field shall be laid out to coincide with the provisions of Sections 1.04 through 1.08 of the Official Baseball Rules.

13.2.4 IRRIGATION SYSTEM

All new facilities shall provide a full field irrigation system as well as water lines 1 1/2" or larger behind both home plate and second base for watering the infield grass and base pit areas. It is recommended that a series of water outlets 1" or larger be distributed around the playing field in order to water the field if the irrigation system should become inoperable. It is recommended that a full-field irrigation system be provided at all existing facilities.

13.2.5 FIELD DRAINAGE SYSTEM

All new facilities shall provide an underfield drainage system integrated into the subbase of the turf (natural or synthetic) surface. This system shall be a system of a drain tile fields in a porous collection bed (or similar system) below the turf base.

It is recommended an optimal slope of .5% be maintained from the base of the pitcher's mound to the baselines and from second base to the outfield warning track.

City of Portland, Maine
IN THE CITY COUNCIL

WHEREAS, the City of Portland has a unique opportunity to bring professional baseball to Portland by becoming the home of a Double A Minor League baseball franchise; and

WHEREAS, the creator of that opportunity, Mr. Daniel Burke, has asked the City Council, as the elected representatives of the citizens of Portland, for an expression of support in behalf of this effort; and

WHEREAS, it is the sense of the Council that bringing minor league baseball to Portland will improve and enhance the social, cultural and entertainment fabric and quality of the community; and

WHEREAS, these improvements will increase what the City offers to its citizens and visitors at the Hadlock Municipal Facility; and

WHEREAS, the City's athletic facilities are suitable for this type of endeavor with some improvements; and

WHEREAS, baseball has many lifelong fans within Portland and throughout the region and the state; and

WHEREAS, Portland has consistently proven its ability to support minor league professional sports; and

WHEREAS, it is the sense of the Council that the citizens of Portland strongly support baseball and this effort and will strongly support the team located in Portland; and

WHEREAS, the presence of a minor league team in Portland will benefit not only Portland and its individual and business citizens but the entire State of Maine;

BE IT THEREFORE RESOLVED, that the City Council strongly endorses the location of a Double A Minor League team in Portland and pledges itself to take any reasonable steps necessary to bring such a team to the City.



OFFICE OF THE MAYOR

REVEREND DR. MARTIN LUTHER KING, JR.
CITY GOVERNMENT CENTER
HARRISBURG, PENNSYLVANIA 17101-1678

STEPHEN R. REED
MAYOR

June 19, 1992

Mr. Robert B. Ganley, City Manager
City of Portland
389 Congress Street
Portland, Maine 04101

Dear Mr. Ganley:

It is my understanding that with the planned expansion of the Eastern Professional Baseball League by two clubs that the City of Portland, Maine is considering the prospect of being a new host community for Class AA Minor League Professional Baseball. I take this opportunity to let you know of our experience with having done the same.

The City of Harrisburg, Pennsylvania was considered, under the HUD Federal distress criteria, as the second most distressed city in the Nation eleven years ago. When we came into office in 1982, we began an aggressive economic development initiative intended to reverse nearly three decades of very serious decline. Economic development is generally regarded as commercial, industrial and residential development and it would be accurate to state that on such development we have focused a considerable amount of attention and expenditure. Bricks and mortar, however, do not entirely make-up a community, as we both know. We have always known that our ability to attract and retain jobs, businesses and residents would be directly related to the arts, cultural activities, recreation, parks and sports.

One of the most alluring and sustainable spectator sports is baseball. There exists, in most communities, a strong market for baseball, including professional baseball at the minor league level. Indeed, for the amount of effort involved, there is a far greater return to a city from minor league baseball than from the considerably more expensive major league teams in the larger cities.

Harrisburg built a facility which opened in April of 1987. Prior to its opening and at the time of the construction start, we completed final arrangements for the move of one of the Eastern Professional Baseball League's member teams to Harrisburg. Their first season, in 1987, resulted in their winning the Eastern League post-season championship. Regional historians continue to list the 1987 "Cinderella season" of our Harrisburg Senators as one of the most significant events in Central Pennsylvania sports history.

Mr. Robert B. Ganley
June 19, 1992
Page Two

Since then, attendance has steadily grown such that, last year, we achieved our highest paid attendance at more than 220,000 fans.

For the use of our Riverside Stadium, which we have expanded twice since its initial opening in 1987, we have in place an agreement between the baseball club and the City of Harrisburg which sets forth reasonable terms and conditions for the use of the facility. Essentially, these terms and conditions set forth that city government will not subsidize the operating costs of the team and the City will not set rental or other demands on the team that would make their operation unprofitable. Minor league professional baseball clubs do not operate on any significant profit margin and thus things, such as rental fees, can make a major difference between economic viability or loss.

The benefits to the City of Harrisburg from having an Eastern Professional Baseball League team at a city-owned recreational facility is considerable. It would be inappropriate for a city to look upon a baseball franchise as a major profit-making opportunity for the city since the economics of minor league baseball simply do not allow for such. You can, however, if you have a local amusement tax, for example, assure that the municipality receives a continuing source of tax revenue based upon the actual gross dollar volume from gate receipts. Additionally, a baseball club is a business enterprise which hires both year-round, full-time staff and many more numbers of staff for seasonal work. The direct economic impact of these jobs and the baseball club, as a business enterprise, runs well into the seven figures in Harrisburg's case.

Further, since baseball's operation involves the consumption of potentially substantial volumes of food, beverage, memorabilia, printing and other supplies and materials, there is significant spin-off value from such an operation. The bottom line is that to the local economy, Class AA Minor League Professional Baseball means a multi-million dollar economic benefit. The precise amount will be directly related to the level of attendance.

Further, the City of Harrisburg knowingly utilized the establishment of minor league baseball as an anchor tenant to major recreational facilities of which our baseball stadium is only a part. Immediately contiguous and near to our baseball stadium, we have built marinas, villages, beach house, miniature golf, miniature steam-driven train, horse stables, corrals, riverboat, water taxis and a separate, smaller sports complex. We knew that the placement of minor league baseball as the first major tenant in these facilities would create an instant and continuing market for other community recreational operations. As a direct result of baseball's arrival and the public attendance they have always engendered, we have successfully attracted other vendors to operate both seasonal and year-round facilities and attractions in the immediate area of the baseball stadium. Whether or not the City of Portland has a similar opportunity to place recreational and sports facilities near to a baseball stadium, the fact remains that the presence of baseball will clearly allow the city to engender interest and private dollar infusion in the construction and operation of other

Mr. Robert B. Ganley
June 19, 1992
Page Three

sports and recreational facilities and activities. Minor league baseball has a significant synergistic value for community recreation.

Finally, I would be remiss if I did not underscore the intangible benefit of minor league baseball to a city. As we both know, it is popular these days across America to cast cities in a negative image. Nothing can refocus civic pride and community spirit better or more meaningfully than having minor league professional baseball based in your town. In the past eleven years, with most of the investment having occurred in the past five and one-half years, this City has realized over \$900 million in economic development activity. I have no doubt, whatsoever, that when the history of this era of resurgence is written, that minor league baseball will be viewed as symbolic of the renewed spirit and vitality of this City which is why the presence of an Eastern Professional Baseball League team can be extraordinarily exciting and useful component to a city's image, attractiveness and quality of life. This becomes even more so in the case of the Eastern League because of the stewardship and sensitivity which have been accorded the member cities by Charles Eshbach, the League President, and the League board. They want their member cities to do well and you would find them to be an ally and a source of continuing support.

We wish the City of Portland well in its consideration of the prospect of minor league baseball. I am hopeful that this letter may be of some value in your decision-making process.

With warmest personal regards, I am

Yours sincerely,



Stephen R. Reed
Mayor

SRR:kb

Attachment


As City Traffic Engineer, I am fully confident that the traffic impacts generated by the proposed "Double A" baseball park can be adequately addressed to permit safe ingress and egress of baseball patrons as well as the motoring public.

WJB/bjm

cc Nadeen Daniels, Assistant City Manager
George A. Flaherty, Director Parks/Public Works
Mary Theriault, Project Engineer

**CITY OF PORTLAND, MAINE
MEMORANDUM**

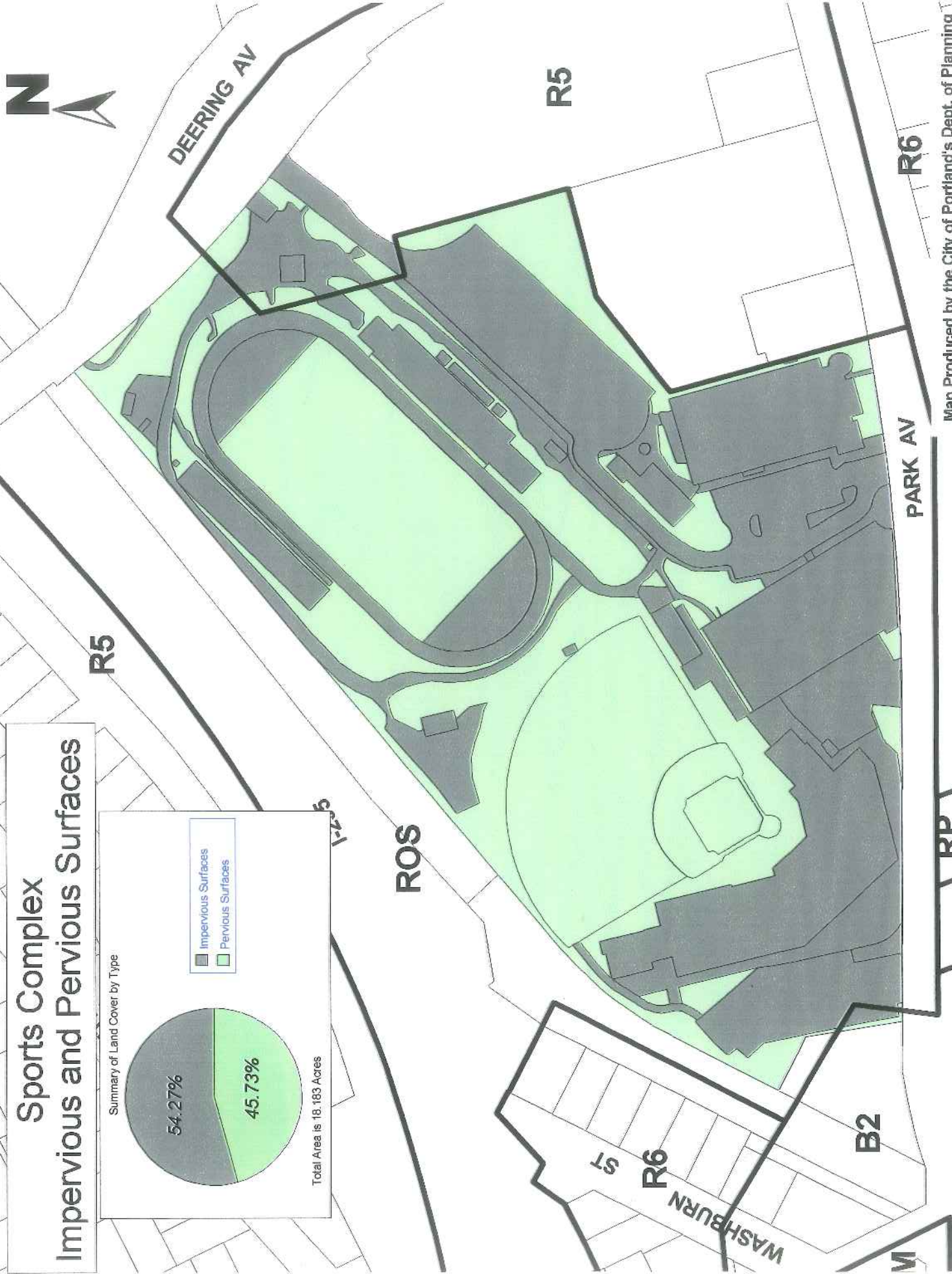
TO: Marge Schmuckal, Zoning Administrator

FROM: Joseph E. Gray, Jr., City Manager 

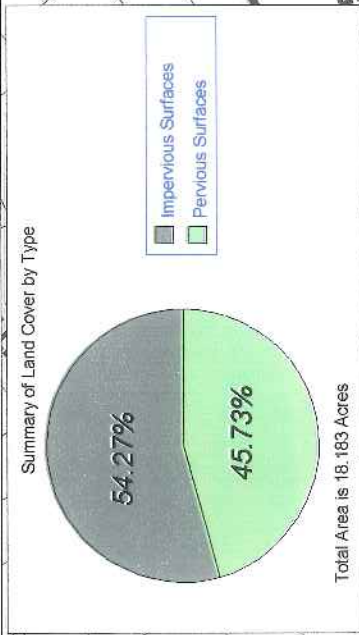
DATE: January 23, 2003

SUBJECT: Hadlock Field Signs

As property owner of Hadlock Field, this memorandum is to confirm that the City of Portland has given permission to the Portland Sea Dogs (as tenant of Hadlock Field) to apply for any sign related permits for Hadlock Field.

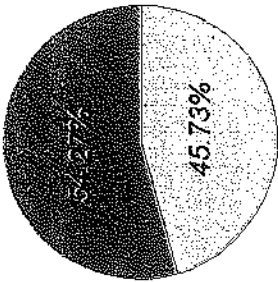


Sports Complex Impervious and Pervious Surfaces

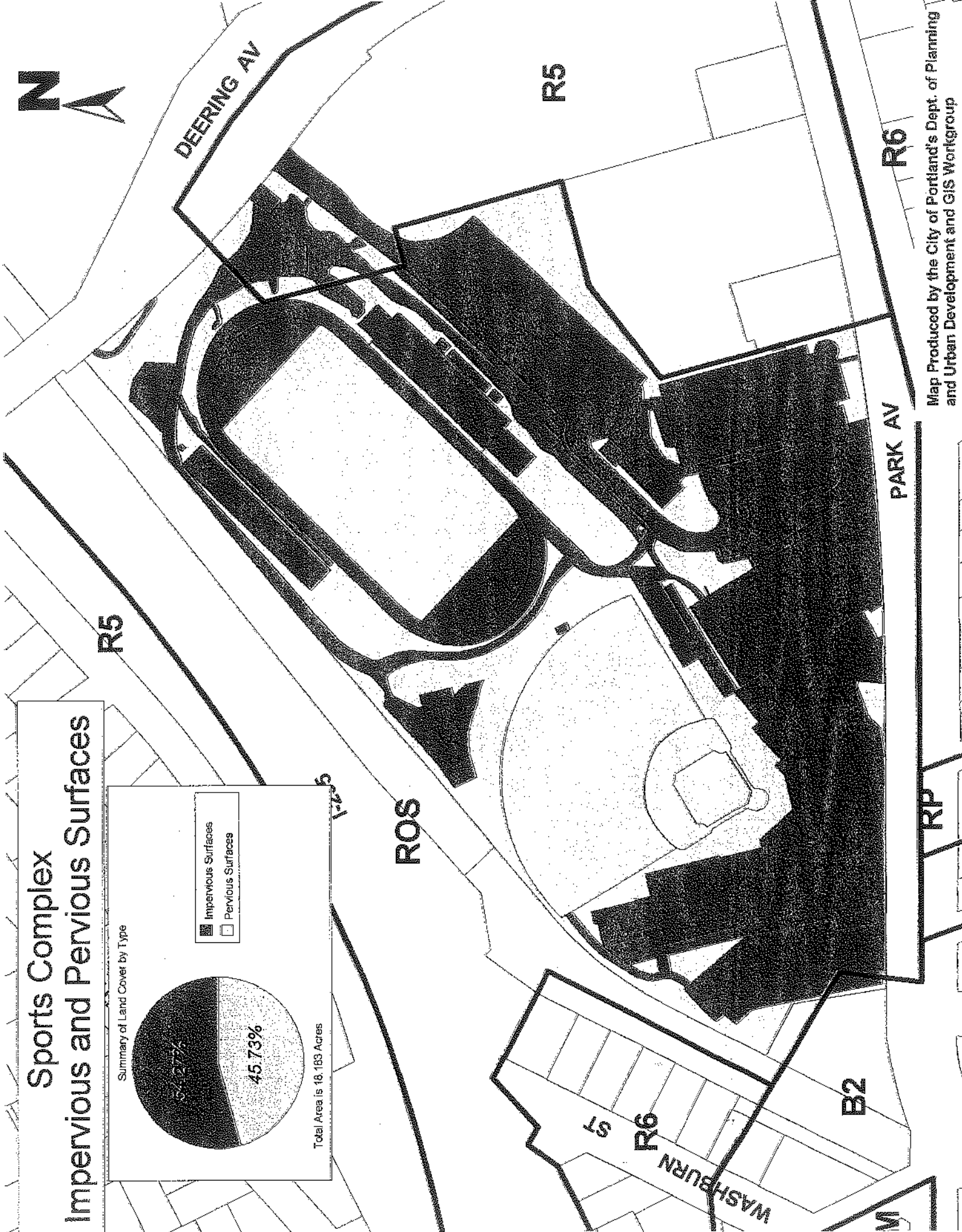
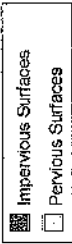


Sports Complex Impervious and Pervious Surfaces

Summary of Land Cover by Type

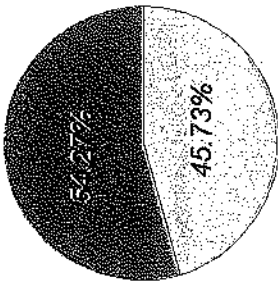


Total Area is 18,183 Acres





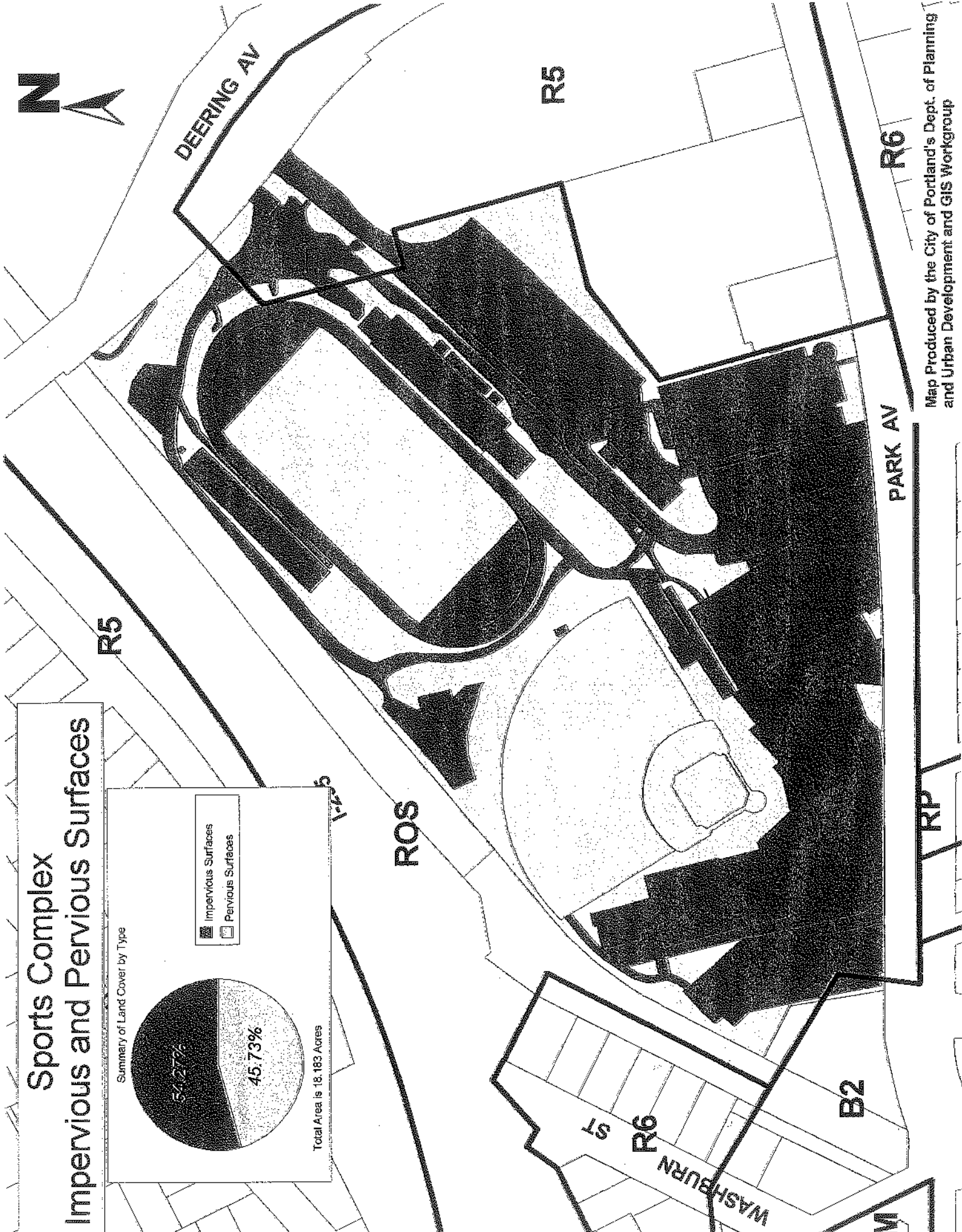
Sports Complex Impervious and Pervious Surfaces

Summary of Land Cover by Type



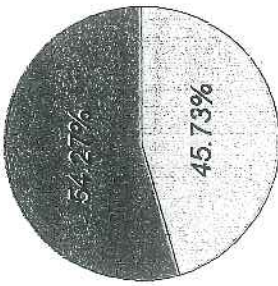
Total Area is 18,183 Acres

-  Impervious Surfaces
-  Pervious Surfaces



Sports Complex Impervious and Pervious Surfaces

Summary of Land Cover by Type



- Impervious Surfaces
- Pervious Surfaces

Total Area is 18,183 Acres



DEERING AV

R5

R6

PARK AV

R5

ROS

RP

B2

ST

R6

WASHBURN

M

Table 2.4

ROS & RPZ Open Space Zones and Signs in all Municipal Parks

These regulations shall not apply to municipal stadiums with a seating capacity of greater than 6000 seats.

See also Sec. 14-370.7 "Special Sign Types", for regulation of signs associated with seasonal sales/markets, special events, fairs/festivals, etc. Signs in designated historic landscape districts shall also be subject to the provisions of article IX of this chapter.

Freestanding Signs (a)	General Park Identification Sign	Concession/Facility Signs (b)
Area	20 sq. ft.	16 sq. ft.
Height	5 ft.	8 ft.
Setback	5 ft.	5 ft.
# Freestanding signs per lot	1 per major vehicular entry	1 per concession or facility (c)

- (a) All signs shall be integrated with existing landscape features or shall be visually related to existing architecture in terms of materials, color, scale, etc., as determined by the planning authority.
- (b) Product trademarks shall be limited to not more than 5% of the total sign area.
- (c) Concession stands or other facilities located within an RPZ or ROS zone shall be allowed one freestanding sign or building sign, not both.

Building Signs (a)

	Concession/Facility Signs
Maximum permitted sign area	20 sq. ft.
Sq. ft. per linear ft. of bldg. facade on which sign will be placed	1 sq. ft.
# bldg. signs permitted per lot	1 per concession or facility (b)

- (a) Building signs shall be visually related to the building on which they are located in terms of materials, color, scale, etc., as determined by the planning authority. Product trademarks shall be limited to not more than 5% of the total sign area.
- (b) Concession stands or other facilities located within an RPZ or ROS zone shall be allowed one freestanding sign or building sign, not both.

continued →

ROS/RPZ Zones, Signs in all Municipal Parks Continued

Scoreboards

Scoreboard size shall correspond to the size and type of facility in which it is to be located, as determined by the Recreation Director. Commercial sign content shall not exceed 15% of surface area of scoreboard.

Most Schools Are in Residential Zones & would be subject
Also to These requirements

Ballfield Booster or Sponsor Signs

Such signs shall be exempt from regulation, provided they remain in place only for the applicable season and do not exceed 32 sq. ft. in area each.

SIGNAGE PRE-APPLICATION

PLEASE ANSWER ALL QUESTIONS

ADDRESS: HADLOCK FIELD, 271 PARK AVENUE ZONE: ROS

OWNER: PORTLAND SEA DOGS / CITY OF PORTLAND

APPLICANT: PORTLAND SEA DOGS

ASSESSOR NO. _____

PLEASE CIRCLE APPROPRIATE ANSWER

SINGLE TENANT LOT? YES NO MULTI-TENANT LOT? YES NO

FREESTANDING SIGN? (ex. Pole Sign) YES NO DIMENSIONS 8 HEIGHT _____

MORE THAN ONE SIGN? YES NO DIMENSIONS _____ HEIGHT _____

SIGN ATTACHED TO BLDG.? YES NO DIMENSIONS see bottom

MORE THAN ONE SIGN? YES NO DIMENSIONS _____

AWNING: YES NO IS AWNING BACKLIT? YES NO HEIGHT OFF SIDEWALK _____

IS THERE ANY MESSAGE, TRADEMARK OR SYMBOL ON IT? _____

LIST ALL EXISTING SIGNAGE AND THEIR DIMENSIONS:

*** TENANT BLDG. FRONTAGE (IN FEET): n/a

*** REQUIRED INFORMATION

AREA FOR COMPUTATION

relocating 18 - 8'x16' Ad boards
 adding 2 ad boards 1 12'x12'
 1 15' high soda bottle sign

YOU SHALL PROVIDE:

A SITE SKETCH AND BUILDING SKETCH SHOWING EXACTLY WHERE EXISTING AND NEW SIGNAGE IS LOCATED MUST BE PROVIDED. SKETCHES AND/OR PICTURES OF PROPOSED ARE ALSO REQUIRED.

SIGNATURE OF APPLICANT: _____ DATE: _____

THIS IS NOT A PERMIT/CONSTRUCTION MAY NOT COMMENCE UNTIL THE PERMIT IS ISSUED

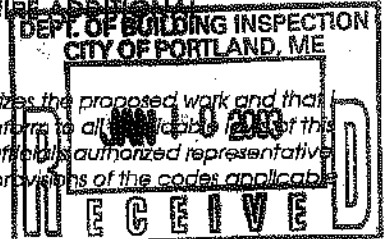
Signage Application

If you or the property owner owes real estate or personal property taxes or user charges on any property within the City, payment arrangements must be made before permits of any kind are accepted.

Location/Address of Construction: WADLOCK FIELD, 271 PARK AVENUE		
Total Square Footage of Proposed Structure	Square Footage of Lot	
Tax Assessor's Chart, Block & Lot Chart# <u>049</u> Block# <u>A</u> Lot# <u>001</u>	Owner: PORTLAND SEA DOGS / CITY OF PORTLAND	Telephone: 874-9300
Lessee/Buyer's Name (if Applicable)	Applicant name, address & telephone: PORTLAND SEA DOGS 271 PARK AVE.	Total s.f. of signage _____ x 1.00 per s.f. \$ _____, plus \$30.00 base fee Fee: \$ _____
Current use: <u>BASEBALL STADIUM</u> waived fee		
If the location is currently vacant, what was prior use: _____		
Approximately how long has it been vacant: _____		
Proposed use: _____		
Project description: <u>relocation 18 8x16 Advertising boards / adding advertising boards</u> 112x12 115' high zeta bottle		
Contractor's name, address & telephone: CHARLES ESHBACH OR		
Who should we contact when the permit is ready: PETER DRIVAS, c/o PORTLAND SEA DOGS 874-9300		
Mailing address: P.O. Box 636 Portland, ME 04104		
We will contact you by phone when the permit is ready. You must come in and pick up the permit and review the requirements before starting any work, with a Plan Reviewer. A stop work order will be issued and a \$50.00 fee if any work starts before the permit is picked up. Phone: 874-9300		

IF THE REQUIRED INFORMATION IS NOT INCLUDED IN THE SUBMISSIONS THE PERMIT WILL BE AUTOMATICALLY DENIED AT THE DISCRETION OF THE BUILDING/PLANNING DEPARTMENT, WE MAY REQUIRE ADDITIONAL INFORMATION IN ORDER TO APPROVE THIS PERMIT.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Code Official's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.



Signature of applicant:	Date: <u>1/9/03</u>
-------------------------	---------------------

This is NOT a permit, you may not commence ANY work until the permit is issued. If you are in a Historic District you may be subject to additional permitting and fees with the Planning Department on the 4th floor of City Hall



January 9, 2003

To: Alex Jaegeman

From: Charles Eshbach

Re: #1-Signage on top of the new leftfield fence
#2-Relocation of signage from both levels of the existing leftfield fence

Attached is our application to facilitate the changes we need to make per the installation of the new leftfield wall. Details on the Citgo and Coke signs should be arriving within the next day or so. I will deliver them to you as soon as I receive them. The information pertaining to the relocation of the signs from both levels of the existing leftfield wall is attached. Due to the ever-dwindling time before Opening Day, I ask that you move forward on that portion of our request immediately. Any consideration you can give in regards to the acceleration of this portion of the application would be greatly appreciated.

*Received
1/21/03*



*Notices to
the world*

1/21/03

January 17, 2003

To: Alex

From: Charlie

Re: CITGO & Coke Signs

Enclosed are specs for the two signs. They will both be mounted on our existing fence supports.

I met with representatives from MDOT this morning. They are okay on the CITGO sign, and indicated that we could put our own logo on the back of the CITGO sign. They think that the Coke bottle is okay as long as it doesn't say Coke on the highway side.

Please let me know if you have any questions.

*Separate permit permit for electrical
legal - ^{permissions} letter from owner - liability
site plan - with location*

12'x12' HI-RISE SIGN - 35' TAPERED POLE

SIGN SPECIFICATIONS

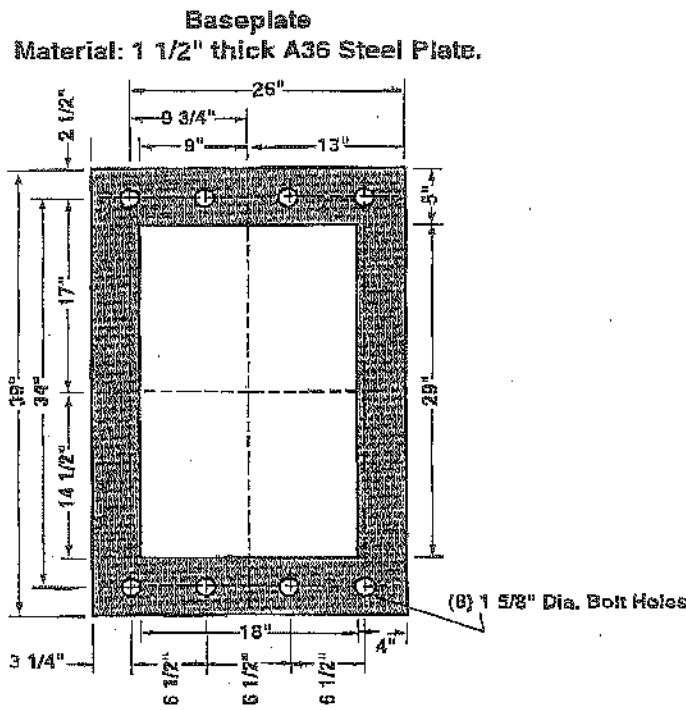
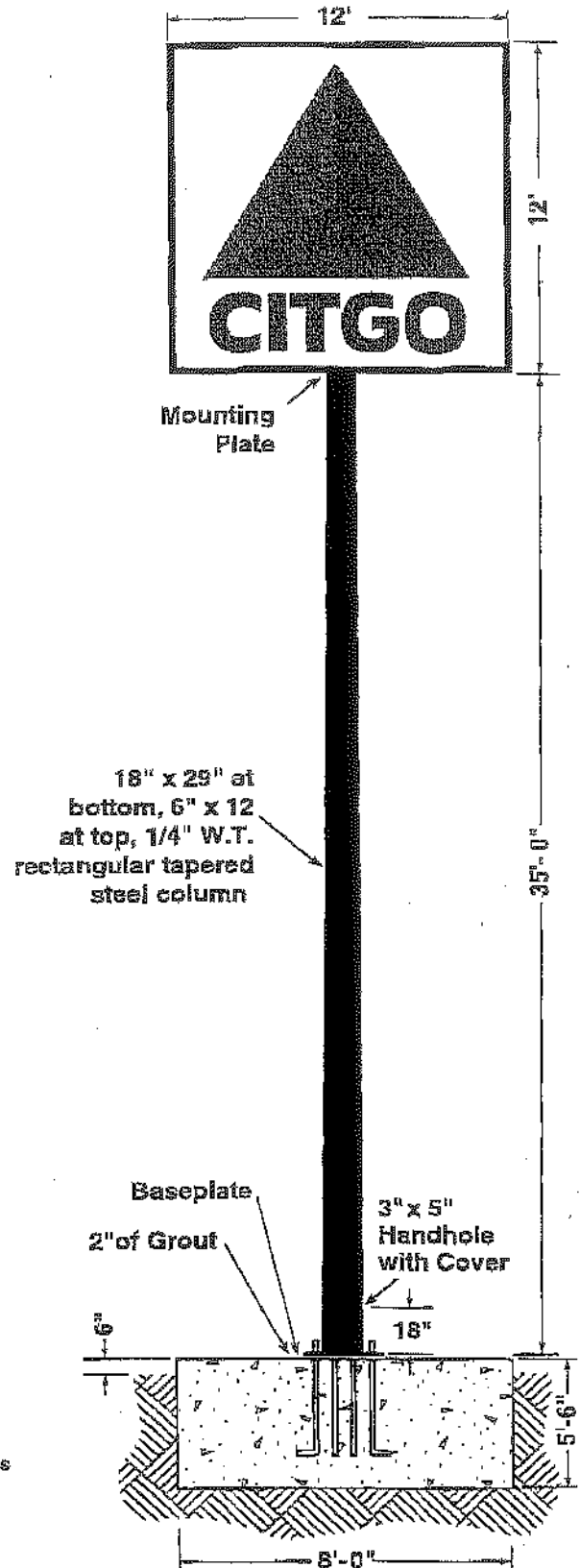
Amps: 35.1
 Net Weight: 1,296 lbs.
 Area: 144 square feet

POLE SPECIFICATIONS

Design Windload: 30 PSF/100MPH
 Pole Supports: 1500 lbs.
 Rectangular Tapered Corten Steel Pole,
 18" x 29" at the bottom, 6" x 12" at the top,
 1/4" wall thickness
 Baseplate:
 26" x 39" x 1 1/2" thick A36 Steel Plate.
 Sign Mounting Plate:
 18" x 26" x 1 3/8" thick A36 Steel Plate.
 Anchor Bolts:
 Eight (8) 1 3/8" Dia. x 56" long Steel Anchor
 Bolts with 6" long hooks.

FOUNDATION SPECIFICATIONS

Size:
 15'-0" x 8'-0" x 5'-6" deep (6" above grade).
 Amount of Concrete: 24.45 Cubic Yards.
 Design Windload: 30 PSF/100MPH



35
 12

 47
 Fall

DISC. 12'x 12' D/F C/P HIGH RISE

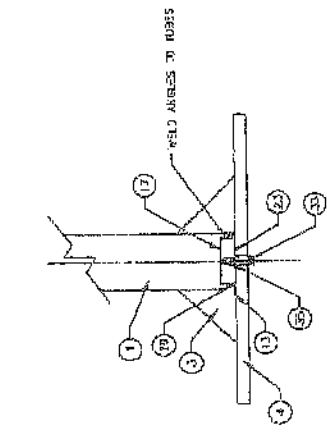
NO. CIT-0-B031

SHT. 1 OF 2

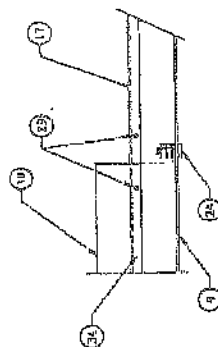
BILL-OF-MATERIALS

ISSUE DATE: 9-14-93
REVISIONS

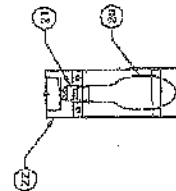
ITEM	SPEC.	DESCRIPTION	ZIM. NO.	QTY.
1	A-36	.187 x 5" x 5" STL. TUBE @ 11'-5 7/8" LG.		2
2	A-36	1" x 30 1/2" x 23 3/4" STL. MOUNTING PLATE	ZIM-1	1
3	A-36	1/2" x 5" x 5" GUSSET	ZIM-1	4
4	A-36	C4 x 5.4 STL. CHANNEL (6) 4'-8 1/2" (3) 1'-2"		9
5	A-36	3/16" x 3" x 3" STL. ANGLE x 2'-6" LG.		4
6	A-36	3/16" x 2 1/2" x 2 1/2" STL. ANGLE @ 11'-6" LG.		4
7	A-36	3/16" x 2 1/2" x 2 1/2" STL. ANGLE @ 11'-0 7/8" LG.		4
8	A-36	3/16" x 2 1/2" x 2 1/2" STL. ANGLE @ 2'-6" LG.		14
9	A-36	3/16" x 2 1/2" x 2 1/2" STL. ANGLE @ 3'-0" LG.		4
10				
11				
12	3003-H14	.050 ALUM. RETAINER (8) 8'-0" (8) 3'-6 1/4" LG.	ZIM-1	16
13	A-36	1/8" x 1" x 1" STL. ANGLE (8) 10'-11" LG. (4) 5" LG.		8
14		FLEX FACE CLAMP ASSY W/ NUT		136
15	3003-H14	.050 x 2'-6" x 11'-6" ALUM. SHEET METAL (4) 8'-0" (4) 3'-6"		8
16	3003-H14	.050 x 1'-8" x 2'-0" ALUM. SHEET METAL DOOR		2
17	6053-T5	M2076 RWC. 9(A) @4" 3(B) @10 7/8" 4(C) @1'-4" 3(D) @10 15/16"		19
18		ø1/2" x 1 1/2" RIGID NIPPLE		9
19		#1110-247SC-TC LAMP BALLAST @3.9 AMPS		9
20		400 WT METAL HALIDE UNIVERSAL MOUNT		9
21		HID LAMP SOCKET		9
22		LAMP DIFFUSER \ ANTI ROTATION BRACKET		9
23		M2056 EXTR. ALUM RWY @ 1'-4" LG.		1
24		ø1/4" x 1" H.H. BOLTS W/LOCKWASHERS & NUTS		36
25		1/4" x 4" x 4" LIFTING ANGLE @ 16" w/(ø3/4" BOLTS & WELD NUTS)		1set
26		SOUTHCO FASTENERS		4
27		PIANO HINGE		2
28		#8x 3/4" THREAD CUTTING SCREWS		-



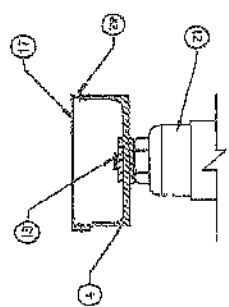
SECTION A-A
SCALE: R.I.S.



TYP. BALLAST MOUNTING DETAIL
N.T.S.



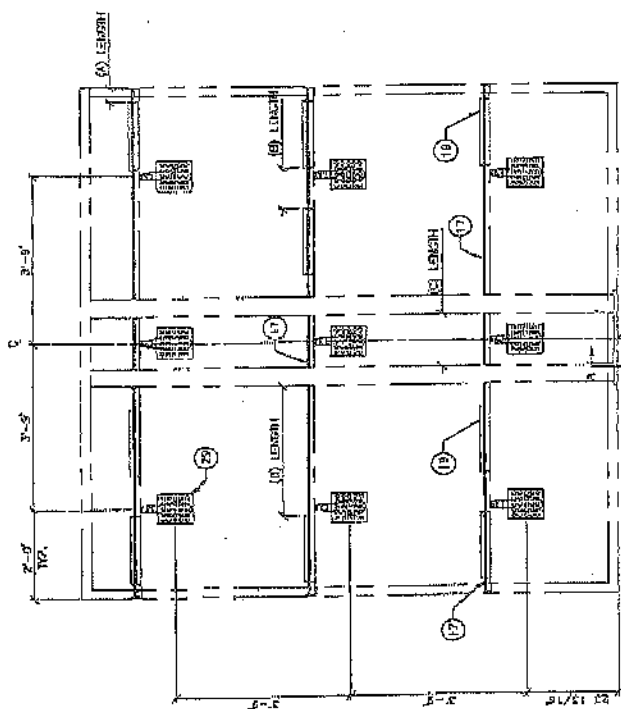
LAMP/DIFFUSER DETAIL
N.T.S.



LAMP MOUNTING DETAIL
N.T.S.



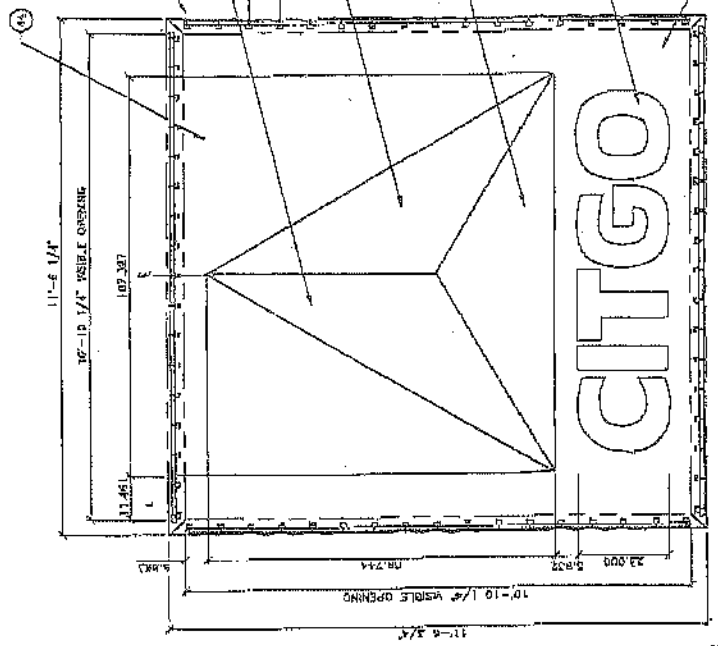
END VIEW
SCALE: 3/4\"/>



ELECTRICAL ELEVATION
SCALE: 3/4\"/>

REVISIONS		ZIMMERMAN SIGN CO. DALLAS, TEXAS	
NO.	DATE	BY	CHKD.
1	01/13/03
2
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Rain Lip



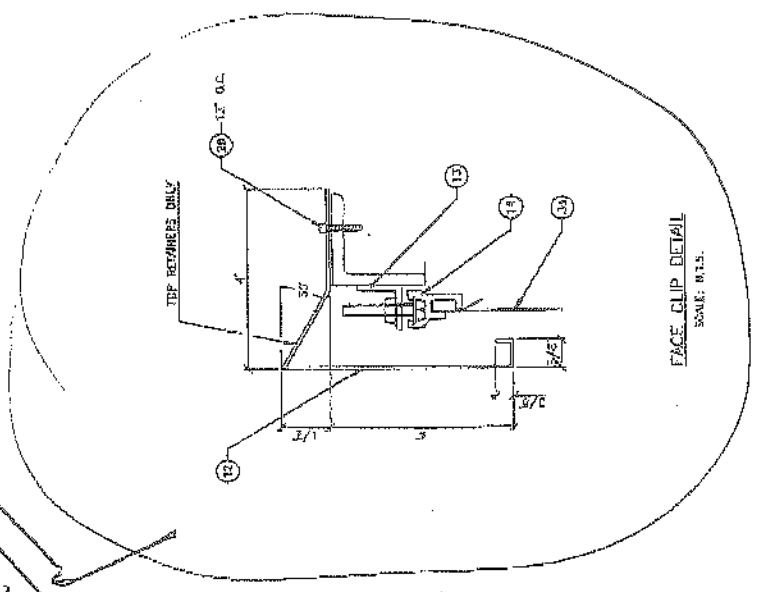
SIGN SPECIFICATIONS:

STEEL SIZE ... 132.46 SBLT.
 ACTUAL SIZE ... 11'-8" x 11'-6"
 APPROXIMATE WEIGHT ... 2134.0 LBS.
 ELECTRICAL ...
 WAYS ... 35.1 TOTAL
 COUPLERS ... (5) TO
 COLLECTORS ...
 UPPER LT. HAND TROUGH ... L3850 FR-1-3141A-B-DXW (PLUS 179) TRANSLUCENT
 UPPER RT. HAND TROUGH ... M3850 FR-1-3355A-B-DXW (PLUS 181) TRANSLUCENT
 LOWER MESSAGE ... M3850 FR-1-3354 (PLUS 180) TRANSLUCENT
 CTRP ... BILE OF-1-410-4A-B-3P1 TRANSLUCENT
 BACKGROUND ... M3850 FR-1-3353
 C-BOARD & REPAIRS ... 150 BUCKING

FACE ELEVATION
 SCALE 3/8" = 1'-0"

NOTE:

ALSCHE WIND FACE SIZE 12'-0" x 17'-0" (WIND LOAD PERMITS 10-13 1/4" x 10'-10 1/4")



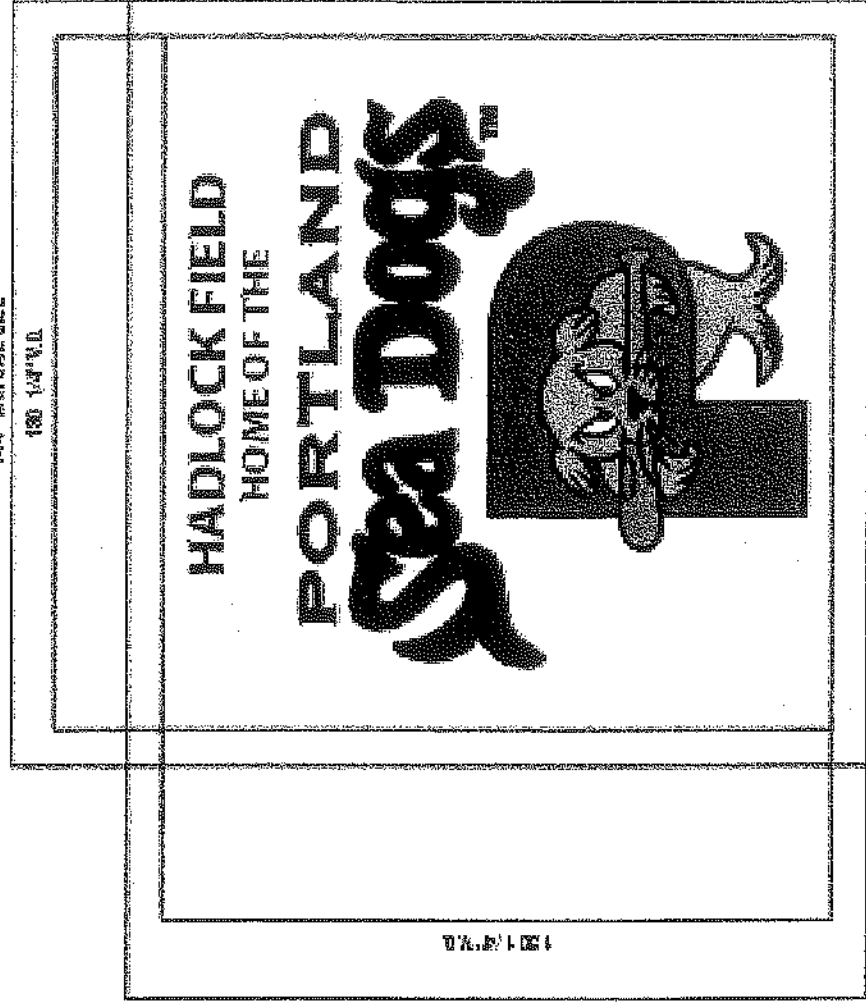
FACE CLIP DETAIL
 SCALE: 3/16" = 1"

ZIMMERMAN SIGN CO. DALLAS, TEXAS	
PROJECT NO.	12' x 12' FLEX HIGH RISE
DATE	11-15-53
BY	J.S.
CHECKED BY	J.S.
APPROVED BY	J.S.
SCALE	3/8" = 1'-0"
PROJECT NO.	12' x 12' FLEX HIGH RISE
DATE	11-15-53
BY	J.S.
CHECKED BY	J.S.
APPROVED BY	J.S.
SCALE	3/8" = 1'-0"



12X12 FLEX FACE

144" ACTUAL SIZE
180 14" W.D.



144" ACTUAL SIZE
180 14" W.D.

Approved Approved with changes

DATE: _____

BY: _____

PART NUMBER		DESCRIPTION	COLORS
	3M 3630-22		BLACK
	3M 3630-96		BLUE
	3M 3630-99		RED
	3M 3630-75		YELLOW
	3M 3630-51		GRAY

Designed by
 [Logo]
 [Text]
 [Text]

Sales Rep: Dan Fuller
 Acct Mgr: Dianne Day
 Artist: [Signature]

Job #: C102659
 Date: 01.15.03
 Released



RELEASE A

Charlie Eshbach

From: <Artproinc@cs.com>
To: <ceshbach@portlandseadogs.com>
Cc: <lmelanson@ccne.com>
Sent: Tuesday, January 14, 2003 3:24 PM
Attach: 15'-Portland.jpg
Subject: CC Bottle

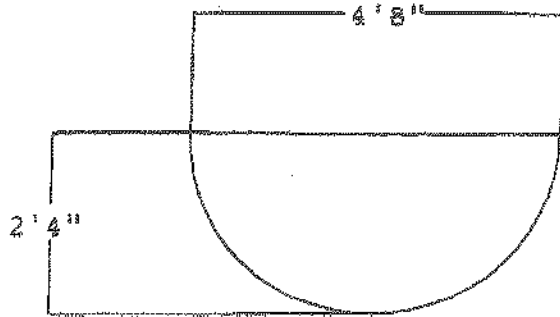
Charlie -

This is a photo of a 15' bottle, similar design as the Fenway bottles. The next email will have a drawing like the previous ones, but updated with information specifically for this bottle, and a stat sheet. Thanks, Dianna

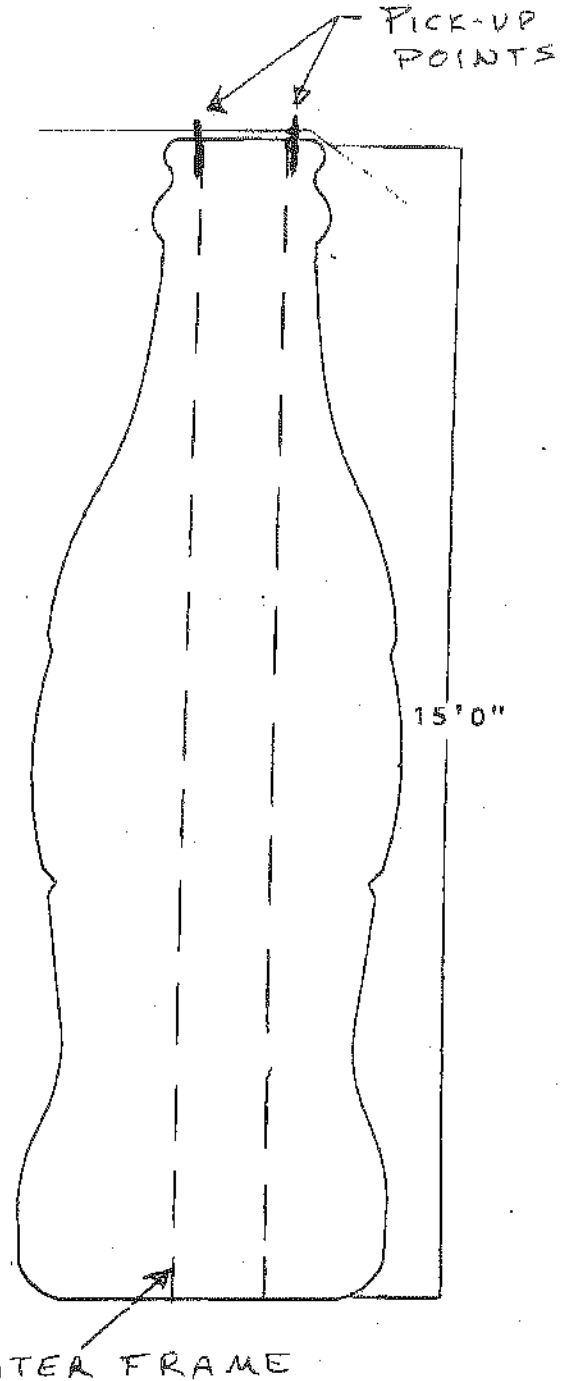
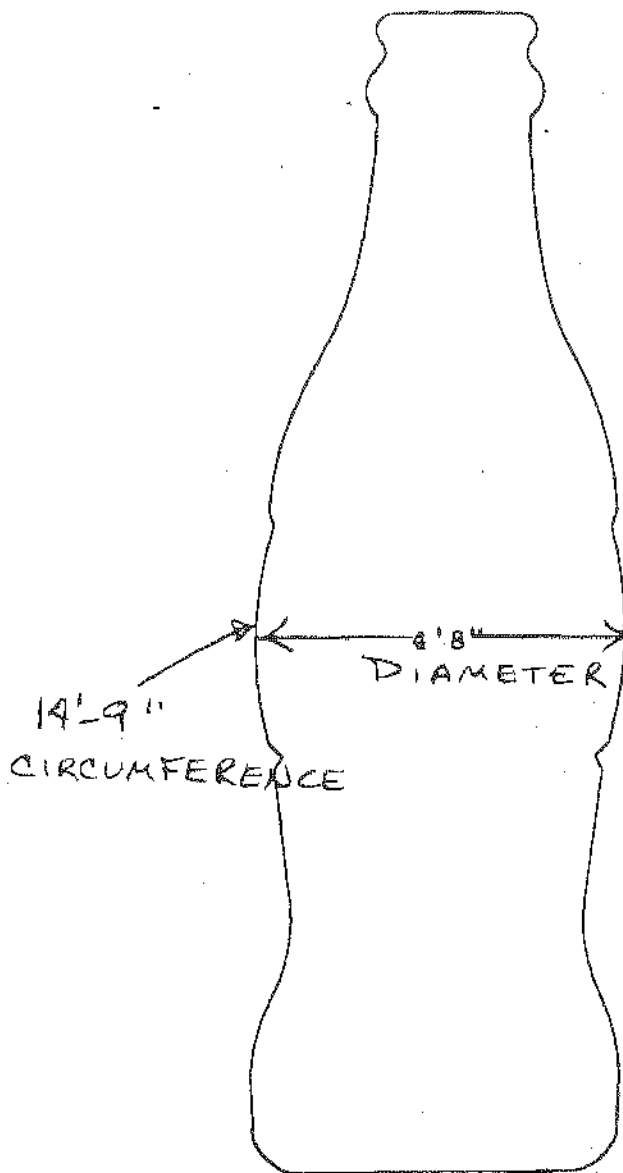


ART PRODUCTIONS, INC.

211 NEWPORT DR. • PEACHTREE CITY, GEORGIA 30269
PH 770-632-5525 • FAX 770-631-9789
(Call for shipping address.)



15' 3-D FIBERGLASS COCA-COLA BOTTLE



ART PRODUCTIONS, INC.

artproductionsinc.com

January 14, 2003

TO: Charlie Eshbach
Portland Sea Dogs
FAX 207-780-0317
FROM: Dianna Snell
RE: Coca-Cola bottles

Charlie –

Larry asked me to forward some additional information to you.

Attached is a photo of a 15' bottle used during the Olympics in Atlanta. This design is the same as what is in Fenway Park with the exception of the label.

The fiberglass will be a standard 3/16" (approx) thickness. The bottle will be full round with an interior 'sleeve' corresponding to the pipe or I-beam that it will slide over. The dimensions are as designated on the attached drawing. Included is an example of how the interior will be fabricated. There will be 'pick-up' points for lifting the bottle into place. Based on adding the interior structure the estimated weight will be around 600 lbs.

I suggest setting the center of the bottle back at least 36" to make sure the bottle will not 'over-hang' the field.

Please let me know if you need anything else.

Thanks, Dianna Snell



STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
16 STATE HOUSE STATION
AUGUSTA, MAINE
04333-0016

R. Croce

JOHN ELIAS BALDACCI
GOVERNOR

CARL A. CROCE
ACTING COMMISSIONER

January 30, 2003

Joseph E. Gray, Jr.
City Manager
389 Congress Street
Portland, ME 04101

Dear Mr. Gray:

Thank you for your letter of January 6, 2003 concerning certain renovations that are being considered for Hadlock Field in anticipation of the Boston Red Sox affiliation with the Portland Sea Dogs.

I think the entire State of Maine and its Red Sox supporters are anxious to welcome the new team. In that light, I had Bruce Ibarguen, State Traffic Engineer and Bob Sinclair, Supervisor of Right of Way Control, visit with Charlie Eshback to discuss the issues relative to the proposed signing to be incorporated in the new left field façade. Specifically, they wanted to make a visual inspection of the site from the streets near the ballpark, as well as from I-295, to assure that no violation of the Maine Traveler Information Services Act would occur.

I believe that Mr. Eshback assured them that measures would be taken to keep the structure within the intent of the law. To that end, it is now our understanding that the Citgo Sign will be one-sided, facing the playing field, and it will have "Hadlock Field - Home of the Portland Sea Dogs" as a message on the back side facing I-295. The Coke bottle will be similarly one-sided, albeit the backside toward I-295 will still be in the shape of the soda bottle. However, no corporate name will be visible to traffic on the interstate. Mr. Eshback will be sending a letter to the Department to verify our understanding in this regard. Following receipt of this conformational letter, we are prepared to give this proposed "Fenway Park Replica" our approval.

Once again, the State of Maine, and the Department of Transportation, welcomes this exciting new franchise to the State. Let's hope that this solid "Maine influence" can break the "Curse of the Bambino"!

Sincerely,

Carl A. Croce
Acting Commissioner

CAC/BAI/kh

CC: Representative Boyd P. Marley
Charlie Eshback
Bruce Ibarguen
Bruce Van Note
Jim Smith



PRINTED ON RECYCLED PAPER

Department of Planning & Development
Lee D. Urban, Director



CITY OF PORTLAND

Division Directors
Mark B. Adelson
Housing & Neighborhood Services

Alexander Q. Jaegerman, AICP
Planning

John N. Lutkin
Economic Development

January 23, 2003

Charles Eshbach
Portland Sea Dogs
P.O. Box 636
Portland ME 04104

RE: Proposed Signage at Hadlock Field

Dear Charlie:

We have reviewed your sign application submitted for various signs at Hadlock Field. Based on our review, the following additional information will need to be submitted. Please note that these items are referenced on the second page of the signage application previously distributed to you.

1. A letter of permission from the property owner. I will contact the City Manager's office concerning drafting such a letter.
2. Submit a sketch plan of the property showing the location of all the proposed signs, the Citgo sign, the Coke sign and the 17 sign panels proposed along the right field wall. The sketch plan could be a modified version of a previously submitted Hadlock Field site plan appropriately modified showing the location of all the proposed signs. The plan should reference the Sign Solutions folder depicting existing and proposed signage along the outfield wall.

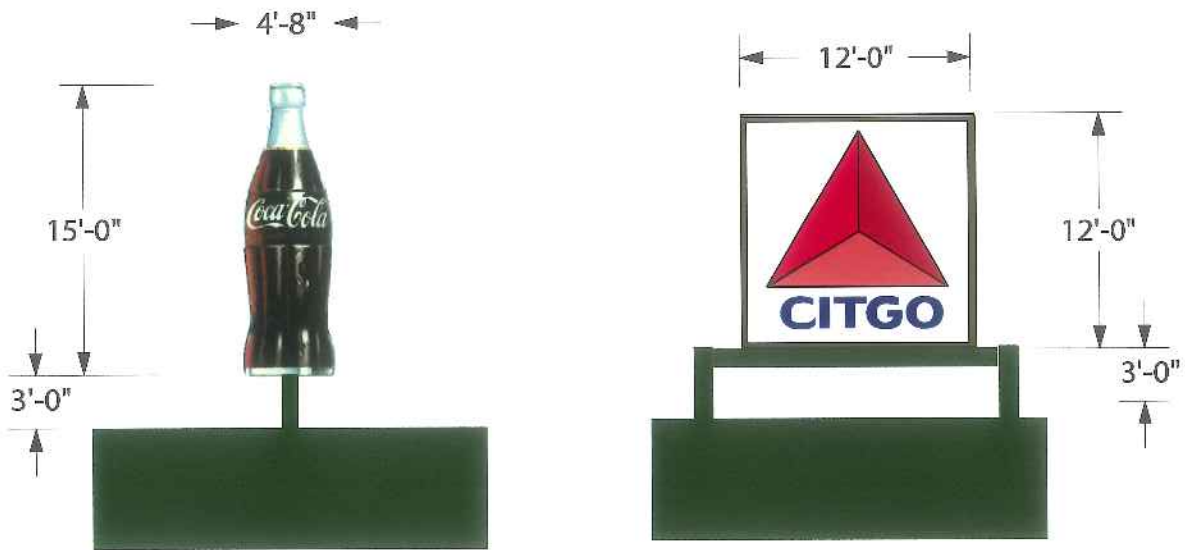
Once we receive the above information, Marge Schmuckal, Zoning Administrator, can then process your application accordingly. Please note that a separate permit will be required for signage involving electricity.

Should you have any questions concerning this letter, please call me

Sincerely,


Richard Knowland
Senior Planner

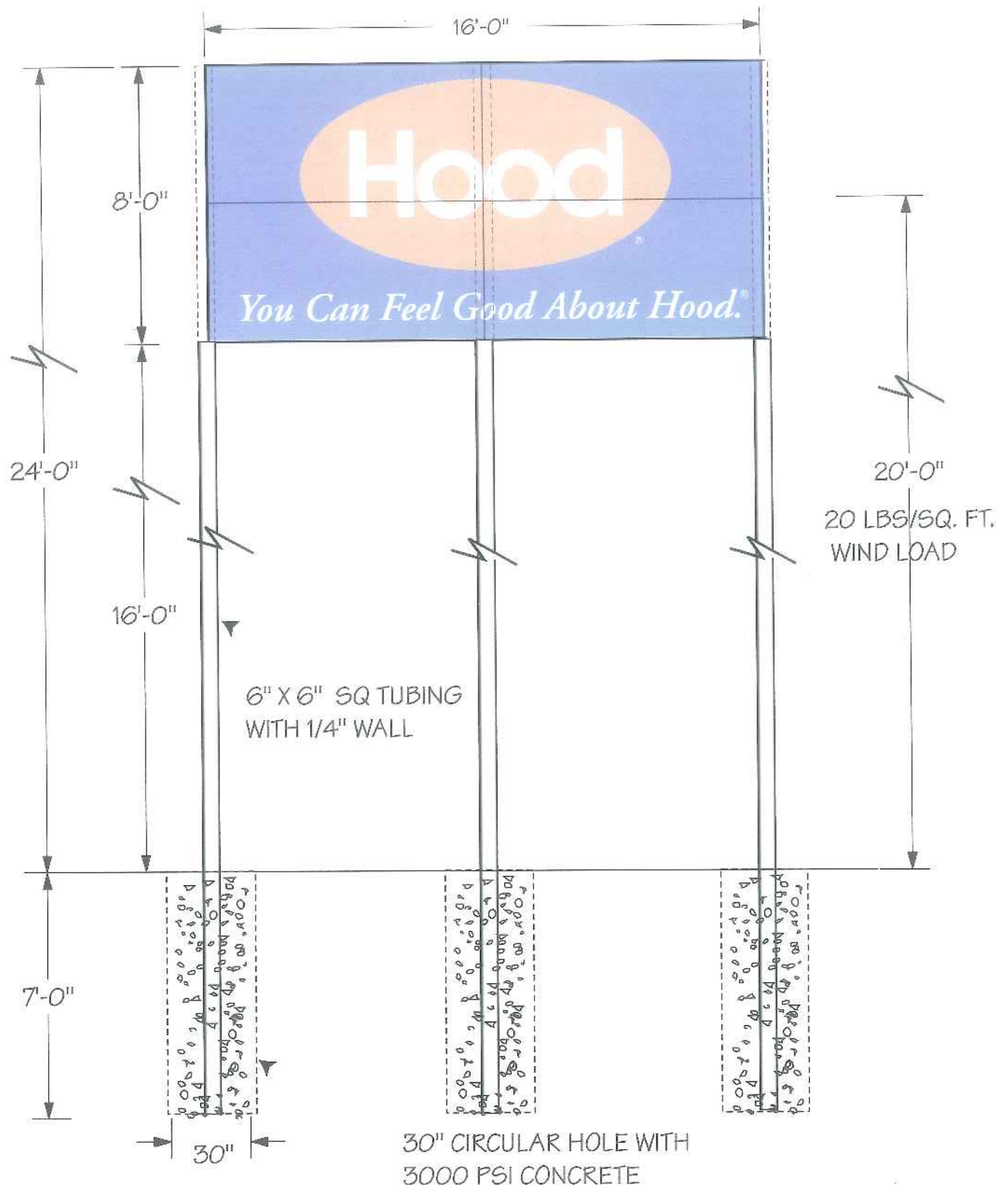
cc. Joseph E. Gray, Jr., City Manager
Lee Urban, Planning and Development Department Director
Alexander Jaegerman, Planning Division Director
Frank LaTorre, Public Assembly Director
Sarah Hopkins, Development Review Program Manager
Marge Schmuckal, Zoning Administrator



MANUFACTURE AND INSTALL:
 ONE COKE COLA SIGN AND ONE CITGO SIGN MOUNTED ON EXSITING
 STEEL IN LEFT FIELD. INSTALLATION AND ENGINEERING BKY OTHERS.


*This design created in conjunction with this project being planned for you is the exclusive property of SIGN SOLUTIONS,
 and can not be copied, exhibited or shown to anyone outside of your organization with consent of SIGN SOLUTIONS*

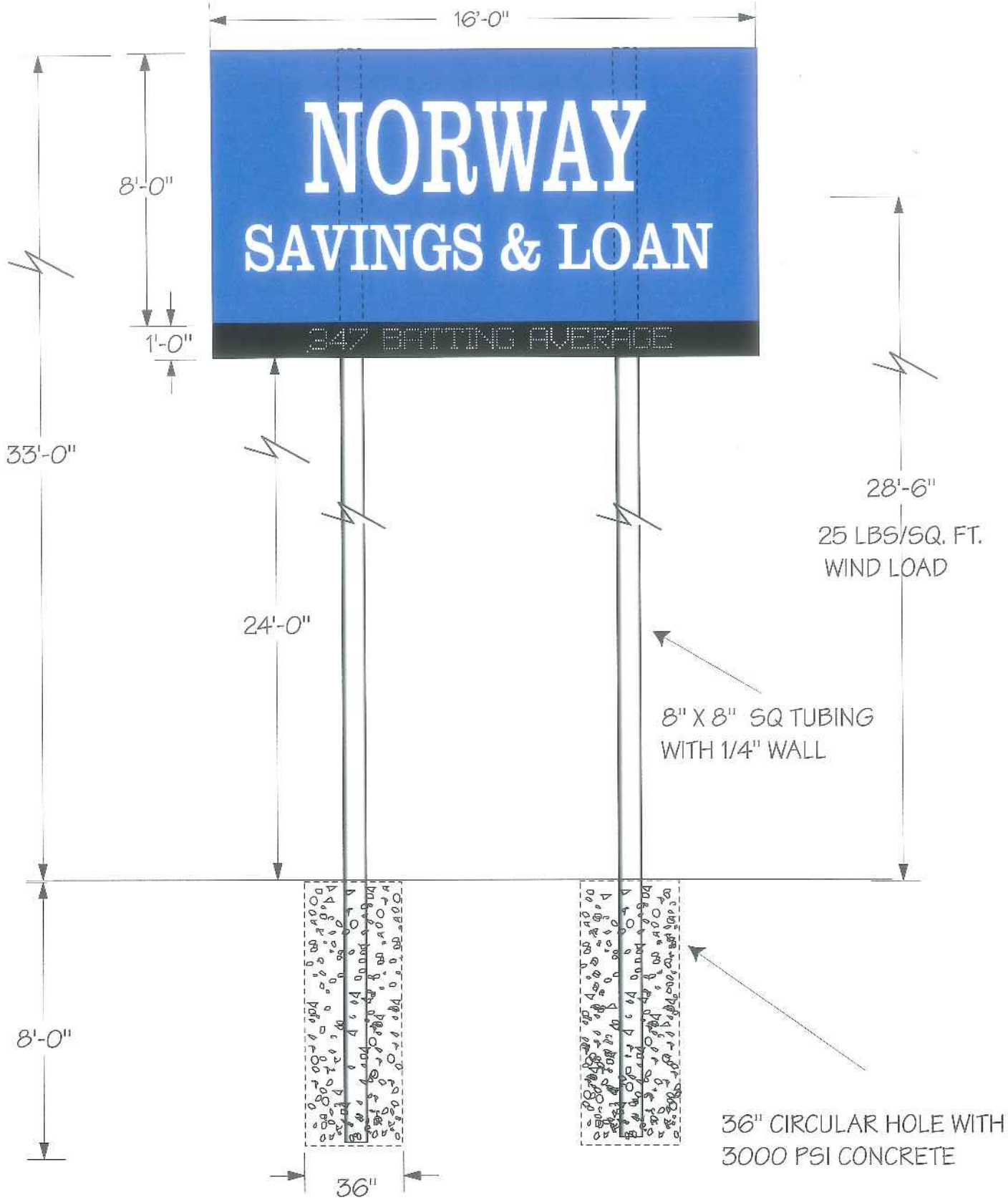
	DATE: 1/24/03	SCALE: NTS
	WORK ORDER NO.:	DESIGNER: BOB PHILLIPS
DWG NO.:	JOB NAME: NEW BILLBOARDS.	
REVISION:	LOCATION: HADLOCK FIELD PORTLAND, ME.	
APPROVED:	© Copyright 2002	



MANUFACTURE AND INSTALL:
SIXTEEN NEW BILLBOARDS FOR THIRD ROW OF SIGNS AT BALLPARK.


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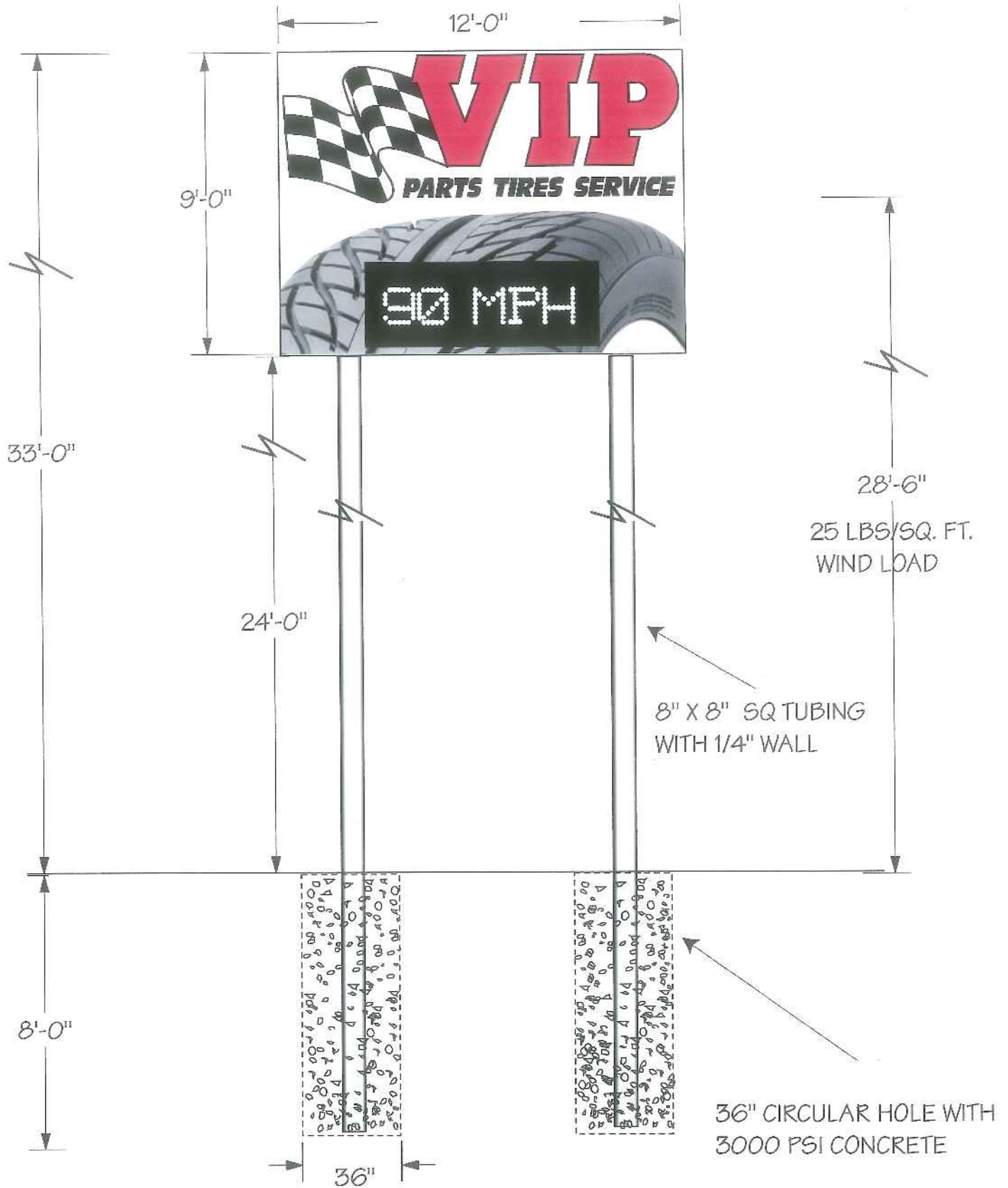
	DATE: 1/20/03	SCALE: NTS
	WORK ORDER NO: BP3908	DESIGNER: BOB PHILLIPS
DWG NO:	JOB NAME: NEW BILLBOARDS.	
REVISION:	LOCATION: HADLOCK FIELD PORTLAND, ME.	
APPROVED:	© Copyright 2002	



MANUFACTURE AND INSTALL:
 NEW BILLBOARDS FOR FOURTH ROW OF SIGNS AT BALLPARK.
 NORWAY SAVINGS & LOAN

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	DATE: 1/20/03	SCALE: NTS
	WORK ORDER NO: BP3908	DESIGNER: BOB PHILLIPS
DWG NO:	JOB NAME: NORWAY SAVINGA NEW BILLBOARDS.	
REVISION:	LOCATION: HADLOCK FIELD PORTLAND, ME.	
APPROVED:	© Copyright 2002	



MANUFACTURE AND INSTALL:
 NEW BILLBOARDS FOR FOURTH ROW OF SIGNS AT BALLPARK.
 VIP SPEED SPITCH SIGN

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 SOLUTIONS	DATE: 1/20/03	SCALE: NTS
	WORK ORDER NO: BP3908	DESIGNER: BOB PHILLIPS
	JOB NAME: VIP SPEED PITCH SIGN NEW BILLBOARD	
	LOCATION: HADLOCK FIELD PORTLAND, ME.	
DWG NO:		
REVISION:		
APPROVED:		
© Copyright 2002		



NORWAY
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COMMERCIAL MORTGAGES
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Portland Press Herald
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KID'S CLUB

MACOM
Running is everything

got milk?
#CBSUS DairyWishes

Pet Quarters
Pet Care & Supplies

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Cumberland Farms
Fresh Produce

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Northern New England
AAA

Pet Quarters
Pet Care & Supplies

Handyman Rental
Handyman Services

RUFUS DEAN LUMBER
Lumber & Building Materials

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Fresh Produce

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#CBSUS DairyWishes

UNICEL
Cellular Services

KeyBank
Financial Services

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#CBSUS DairyWishes

Peoples
Financial Services

Pet Quarters
Pet Care & Supplies

Handyman Rental
Handyman Services

RUFUS DEAN LUMBER
Lumber & Building Materials

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Center for Toyota
www.smertsontoyota.com

Cumberland Farms
Fresh Produce

get milk?
#CBSUS DairyWishes

UNICEL
Cellular Services

KeyBank
Financial Services

get milk?
#CBSUS DairyWishes

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BUD
Beer & Wine

Brightman's
Sunglass Emporium

Dodge
Automotive

MEGA BUCKS
3.6 million

UNICEL
Cellular Services


KeyBank
Financial Services

UNICEL
Cellular Services

KeyBank
Financial Services

get milk?
#CBSUS DairyWishes



	DATE:	11/22/02	SCALE:	NTS
	WORK ORDER NO.:		DESIGNER:	BOB PHILLIPS
	JOB NAME:	NEW MDO BOARDS	LOCATION:	HADLOCK FIELD PORTLAND, ME 04103
	DWG NO.:		APPROVED:	
REVISION:				

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**MANUFACTURE AND INSTALL:
SEVENTEEN 8' X 16' MDO PLYWOOD SIGNS TO FORM A THIRD ROW OF SIGNAGE FOR ADVERTISING.
ONE 8' X 16' ILLUMINATED SIGN TO REPLACE THE 3' X 16' ILLUMINATED DOWNEASTER SIGN.**

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Table 2.4

ROS & RPZ Open Space Zones and Signs in all Municipal Parks

These regulations shall not apply to municipal stadiums with a seating capacity of greater than 6000 seats.

See also Sec. 14-370.7 "Special Sign Types", for regulation of signs associated with seasonal sales/markets, special events, fairs/festivals, etc. Signs in designated historic landscape districts shall also be subject to the provisions of article IX of this chapter.

Freestanding Signs (a)

	General Park Identification Sign	Concession/Facility Signs (b)
Area	20 sq. ft.	16 sq. ft.
Height	5 ft.	8 ft.
Setback	5 ft.	5 ft.
# Freestanding signs per lot	1 per major vehicular entry	1 per concession or facility (c)

- (a) All signs shall be integrated with existing landscape features or shall be visually related to existing architecture in terms of materials, color, scale, etc., as determined by the planning authority.
- (b) Product trademarks shall be limited to not more than 5% of the total sign area.
- (c) Concession stands or other facilities located within an RPZ or ROS zone shall be allowed one freestanding sign or building sign, not both.

Building Signs (a)

	Concession/Facility Signs
Maximum permitted sign area	20 sq. ft.
Sq. ft. per linear ft. of bldg. facade on which sign will be placed	1 sq. ft.
# bldg. signs permitted per lot	1 per concession or facility (b)

- (a) Building signs shall be visually related to the building on which they are located in terms of materials, color, scale, etc., as determined by the planning authority. Product trademarks shall be limited to not more than 5% of the total sign area.
- (b) Concession stands or other facilities located within an RPZ or ROS zone shall be allowed one freestanding sign or building sign, not both.

continued →

**IMPORTANT NOTICE FROM CITY OF PORTLAND
PLANNING DIVISION**

**TO RESIDENTS AND PROPERTY OWNERS
IN THE VICINITY OF 211 PARK AVENUE (HADLOCK FIELD)**

WHAT The City of Portland has received a signage application from the Portland Sea Dogs for a series of signs at Hadlock Field. Two new signs will be mounted above the left field wall of Hadlock Field. These include a 12 ft. by 12 ft. Cingo sign and a 4 ft. 8 in. by 15 ft. Coca Cola sign. Seventeen (17) existing advertising signs (8 ft. by 16 ft.) along the lower left field wall will be relocated to the right field wall forming a third level of signage. In addition one 8 ft. by 11 ft. illuminated sign will replace an existing 3 ft. by 16 ft. illuminated sign.

FOR MORE INFORMATION

Plans are available in the Portland Planning Division, 4th. floor, City Hall. If you wish to submit written comments, address them to Richard Knowland, Senior Planner, Planning Division, City Hall, 389 Congress Street, Portland, Maine 04101, contact by phone at 874-8725 or e-mail at rwk@ci.portland.me.us.

Public comment will be accepted on this application through Friday, February 7, 2003.

**IMPORTANT NOTICE FROM CITY OF PORTLAND
PLANNING DIVISION**

**TO RESIDENTS AND PROPERTY OWNERS
IN THE VICINITY OF 211 PARK AVENUE (HADLOCK FIELD)**

WHAT The City of Portland has received a signage application from the Portland Sea Dogs for a series of signs at Hadlock Field. Two new signs will be mounted above the left field wall of Hadlock Field. These include a 12 ft. by 12 ft. Cingo sign and a 4 ft. 8 in. by 15 ft. Coca Cola sign. Seventeen (17) existing advertising signs (8 ft. by 16 ft.) along the lower left field wall will be relocated to the right field wall forming a third level of signage. In addition one 8 ft. by 11 ft. illuminated sign will replace an existing 3 ft. by 16 ft. illuminated sign.

FOR MORE INFORMATION

Plans are available in the Portland Planning Division, 4th. floor, City Hall. If you wish to submit written comments, address them to Richard Knowland, Senior Planner, Planning Division, City Hall, 389 Congress Street, Portland, Maine 04101, contact by phone at 874-8725 or e-mail at rwk@ci.portland.me.us.

Public comment will be accepted on this application through Friday, February 7, 2003.

**IMPORTANT NOTICE FROM CITY OF PORTLAND
PLANNING DIVISION**

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CITY OF PORTLAND, MAINE
MEMORANDUM

TO: Marge Schmuckal, Zoning Administrator

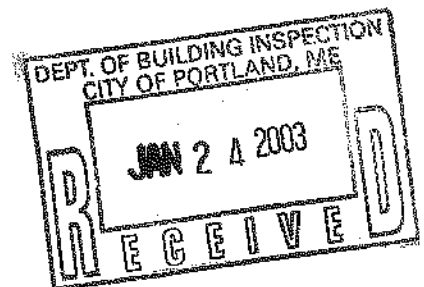
FROM: Joseph E. Gray, Jr., City Manager



DATE: January 23, 2003

SUBJECT: Hadlock Field Signs

As property owner of Hadlock Field, this memorandum is to confirm that the City of Portland has given permission to the Portland Sea Dogs (as tenant of Hadlock Field) to apply for any sign related permits for Hadlock Field.



From: "sandy leeman" <slleeman@sacoriver.net>
To: Portland.CityHall(RWK)
Date: 2/6/03 7:14AM
Subject: billboards at hadlock

Richard Knowland,

I own the property at 146 Park Ave Portland, Maine and I understand that my response is a few days late to your February 7th deadline.

In regards to the sign update at Hadlock field. As I understand it, Billboards are not permissible on the side of highways. It appears that the Seadogs are breaking this rule in the interests of money. If these signs are visible from the outside of Hadlock, they are breaking the rules. I do not know the layout & plan of the sign updates that the Seadogs are requesting but as long as they are required to follow the Dept of Transportation and City of Portland Ordinances regarding signs there should be no issue.

Special interests and/or special treatment for the Seadogs opens the door for other business. These guidelines protect the taxpayers & tourism industries so our state does not become littered with advertising.

No special treatment please.

Is the Dept. of Transportation involved?

Sandra Leeman
727-5276

January 31, 2003

City of Portland, Maine
Planning Division
City Hall, 4th Floor
389 Congress Street
Portland, Maine 04104-3553

Dear Sir/Madam:

Enclosed please find a copy of the notice I recently received pertaining to proposed land use in the City of Portland, ME. Please be advised that Guilford Transportation Industries, Inc. manages real estate matters for several corporations. This response is sent on behalf of the following corporations in reply to your notice received on January 30, 2003:

- Boston and Maine Corporation;
- Maine Central Railroad Company;
- Portland Terminal Company;
- Springfield Terminal Railway Company;
- Hudson River Estates, Inc.;
- Delaware River Estates, Inc.; or
- DH Estates, Inc. (formerly Delaware and Hudson Railroad).

As you may well be aware, the railroads listed above receive numerous notices on a daily basis. Based upon the limited time frame existing between the date on which the notice is received and the scheduled hearing date, I respectfully request your cooperation regarding this matter.


Although the railroads are concerned with each and every notice, there are certain instances which are of particular concern to the railroads. You may be able to advise me if the petition involves land plans showing a crossing of railroad property. Specifically the railroads' concern exists even in the event that your petitioner indicates that it is the holder of a valid railroad crossing. Also, the railroads must be advised if the petition proposes any development within 25' from the centerline of an existing railroad track, even if such development occurs on non-railroad property. As such, petitions for variances from set back requirements can be particularly troublesome.

City of Portland
Planning Division
January 31, 2003
Page Two

Please be advised that this letter should not be construed as a waiver of any other objections which the railroads may have to a proponent's plan. However, the railroads respectfully request your assistance in bringing their attention to any such matters.

Thank you for your anticipated cooperation in this regard.

Sincerely,


Roland L. Theriault, Vice President - Real Estate

RLT/rjs
Enclosure

D:\DATA\WP\ZONING\MASTERS\MAINE\2003\PORTLAND.206

**IMPORTANT NOTICE FROM CITY OF PORTLAND
PLANNING DIVISION
TO RESIDENTS AND PROPERTY OWNERS
IN THE VICINITY OF 211 PARK AVENUE (HADLOCK FIELD)**

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Public comment will be accepted on this application through Friday, February 7, 2003.

CONFIDENTIAL

CITY OF PORTLAND, MAINE
M E M O R A N D U M

TO: Joseph E. Gray, Jr., City Manager

FROM: Gary C. Wood, Corporation Counsel

DATE: December 4, 2002

SUBJECT: Erection of Citgo Sign and Large Coke Bottles at Hadlock Field

It is my understanding that the Seadog's are interested in trying to duplicate Fenway Park as much as possible at Hadlock Field. Two possible components of that duplication are the erection of a Citgo Sign on or just outside the outfield fence and the erection of large Coke bottles on the light towers, both of which can be seen at Fenway Park.

You asked me to review the legalities of these proposals and advise you. This opinion reflects my reading of the controlling State law in 23 M.R.S.A. § 901-1925 (Chapter 21 Maine Traveler Information Services Act/a.k.a. the Motor Board law). I will leave it to Penny and Marge to guide you regarding any local ordinances that would control the erection of these signs.

To begin with, I believe that this decision needs to be made by the Commissioner of the Department of Transportation. 23 M.R.S.A. §1914(9) confers jurisdiction on municipalities over the administration of the law except in the important area that I believe is applicable to this situation: the last sentence of paragraph 9 states: "in compact areas of an urban compact municipality adjacent (emphasis added) to the interstate, the Department of Transportation is responsible for the administration of this section."

We could try to finesse this issue by sending MDOT a letter outlining the proposal and state that we plan to exercise local jurisdiction under paragraph 9 unless we hear back from MDOT within 10-15 days. That approach should avoid after-construction claim of jurisdiction by MDOT and the concurrent exposure to the penalties for violating the State law (removal of the illegal signs at the owner's expense plus fines of \$100 per day per sign). The basic argument would be that Hadlock Field is separated from the interstate by a rail corridor and the statute does not define the word "adjacent" so that any ambiguity should be construed in favor of local jurisdiction.

My second major concern is that even if we obtain local jurisdiction (if I were advising the Commissioner of Transportation) I see the State law as

prohibiting the proposed signs to the extent that they are visible from the Interstate.

23 M.R.S.A. §1908 (Regulation of Outdoor Advertising) is the controlling section in the billboard law. It states: "no person may erect or maintain signs visible to the traveling public from a public way except as provided in this Chapter." The applicable exception that allows signs visible from a public way is in §1914 which allows on-premise signs. However, the definition of "on-premise" signs states as follows:

"On-premise sign" means a sign which is erected and maintained according to the standards set forth in §1914 upon the same real property that the business, facility or point of interest is located (emphasis added) or an approach sign as permitted by §1914 (10). The signs shall only (emphasis added) advertise the business, facility or point of interest conducted thereon (emphasis added) or the sale, rent or lease of the property upon which it is located.

To me that definition means that an "on-premise" sign must advertise the very business that exists on the premises where the sign is erected. Since there will clearly be no Citgo Gas Station next to the Citgo sign nor will there be any Coke manufactured or bottled next to the light towers those signs do not qualify as "on-premise" signs as defined by State law.

NB: If the Citgo sign could be erected in such a way the word "Citgo" is not visible to the traveling public on the interstate ("visible" means capable of being seen without visual aid by a person of normal visual acuity) then I think the only limitations on the Citgo sign would be imposed by our local ordinances. Having passed Hadlock Field several times during the day and at night on the interstate heading both north and south, it is hard for me to see how the Coke bottles could be erected on the light towers in a similar fashion so as to be non-visible from the highway.

Third, even if we get by the first two hurdles (jurisdiction by MDOT Commissioner and the requirement that the advertised business be on-site) 23 M.R.S.A. §1914 (8) places an additional impediment in the way of the proposed signs. That section applies to on-premise signs. It states as follows:

8. **Height.** The maximum height of on-premise signs shall be 25 feet above the ground level of land upon which it is located or if the sign is affixed to or is part of a building, the maximum height is 10 feet above the roof of the building.

That prohibition would clearly eliminate the Coke bottles and it would be a stretch at best to conclude that the fence is part of a building and that the Citgo sign could be as much as 10 feet above the top of the fence.

Here is a list of possible courses of action running from safest to riskiest for you to consider:

- (1) craft legislation and get our delegation on board to carve out an exemption for sports facilities being renovated to reflect historic baseball parks;
- (2) erect the Citgo sign so that it is as invisible as possible from the interstate and forego the Coke bottles which are quite frankly a much more recent addition to Fenway Park than the Citgo sign;
- (3) send the "we have jurisdiction" letter to MDOT and go with both the Citgo sign and the Coke bottles after informing the Council that there is some risk in relation to the State law, but the Seadogs have agreed to remove the structures at their expense and pay any applicable fines if a successful enforcement action ensues.

I am willing to revisit my legal opinion if Charlie wants to go over it with the Seadog's attorney and he or she reaches a different conclusion. As I am sure you will both realize, this is a difficult opinion for me to render given the fact that I love baseball, Hadlock Field, the Seadogs and the Red Sox and I think that the proposed signs would add a lot of fun to the experience by duplicating Fenway Park - the best ball park in the United States.

CC: Penny Littell, Associate Corporation Counsel
Lee Urban, Director of Planning and Urban Development
Charles Eshbach, President of the Portland Sea Dogs

GCW:ses
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Address https://www.citgo.com/MarketNetWebOther/LightOils/Branding/SignsPoles.pdf


SIGN SYSTEM: N *

Standard 7' x 7' Illuminated Sign - 12' Pole

SIGN SPECIFICATIONS
 Amps: 7.0
 Net Weight: 390 lbs.
 Area: 49 Square feet

POLE & FOUNDATION SPECIFICATIONS
 Design Windload: 30 PSF/100MPH
 Pole Supports:
 8" x 6" x 1/4" x 12' tube
 Baseplates:
 1" x 22" x 21"
 Anchor Bolts:
 1 1/4" x 39" + 6" hook
 Foundation:
 36" Dia. x 7'-6" Deep

Insert corresponding code for Mounting Option.
See list of options below.




*Mounting Option Codes:
 C - Center Pole
 B - Between Poles

SIGN SYSTEM: P *

Standard 6' x 6' Illuminated Sign - 12' Pole

SIGN SPECIFICATIONS

Insert corresponding code for Mounting Option.
See list of options below.



Done

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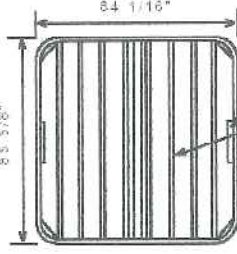
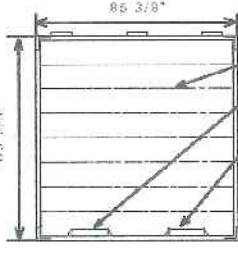
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SIGN SYSTEM: N

Standard 7' x 7' Illuminated Sign - Specifications

Center Pole Mount	Between Pole Mount
 <p>84 1/16"</p> <p>85 5/8"</p> <ul style="list-style-type: none"> (2) 256-484 JEFFERSON BALLASTS (8) F84 T12 H.O.C.W. LAMPS <p>Amps - 7.0 Watts - 840 Net Weight - 390 lbs. Area - 49 S.F. Lumens - 1273 P.S.F.</p>	 <p>86 3/8"</p> <p>85 5/8"</p> <ul style="list-style-type: none"> (7) F84 T12 H.O.C.W. LAMPS (1) 256-484 JEFFERSON BALLAST (1) 256-464 JEFFERSON BALLAST <p>Amps - 5.9 Watts - 705 Net Weight - 285 lbs. Area - 49 S.F. Lumens - 1114 P.S.F.</p>

SIGN SYSTEM: P

Standard 6' x 6' Illuminated Sign - Specifications

Center Pole Mount	Between Pole Mount

Done

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150%

12' UNIVERSAL POLE SPECIFICATIONS

The drawing includes the following specifications and details:

- Side View:** Shows a vertical pole with a total height of 12'. The top section has a diameter of 8" and a wall thickness of 1/4". The bottom section has a diameter of 6" and a wall thickness of 1/4".
- Tube Specification:** TUBE 8" x 6" x 1/4" WALL
- CITGO CAISSON FOUNDATION:** A cross-section showing the pole's connection to a foundation. It features 6#-8 bars and 6#-3 HOOPS. Dimensions include a 36" DIA. top section, a 10" diameter hole, a 7' 6" height, and a 42" diameter section.
- Anchor Bolt:** ANCHOR BOLT 1-1/4" x 39" - 6" HOOK
- Foundation Details:** (4) 1 1/16" DIA. HOLE and (4) SLOT 7/8" DIA. x 1-1/8" DIA. with a 1 1/16" spacing and 1 1/8" TYP. offset.

Done

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8" x 6" x 1/4" WALL

12'

POLE ELEVATION

1/4" x 1 1/4" PLATE FORMED TO FIT

CITGO CAISSON FOUNDATION

ANCHOR BOLT 1-1/4" x 3/8" + 6" HOOK

(4) 1 1/16" DIA. HOLE

(4) SLOT 7/8" DIA. x 1-1/8" DIA.

1 1/16" TYP.

1 1/2"

1 1/2"

8 1/2"

16 1/2"

7"

2 1/4"

6 1/8" x 6 1/8" CUT-OUT

MOUNTING PLATE

8 1/2"

8 1/2"

4 1/4"

4 1/4"

1"

8 1/2"

21"

Done

Start Inbox - Micros... SAP Logon #60 Display Purch... CITGO Market... https://www... Sign 7 sign.do... Internet

11:30 AM

The image shows a technical drawing of a sign pole assembly. On the left is a 'POLE ELEVATION' showing a vertical pole with a height of 12 feet and a diameter of 8 inches. A section of the pole is labeled '8" x 6" x 1/4" WALL'. Below the pole is a '1/4" x 1 1/4" PLATE FORMED TO FIT'. To the right are detailed views of the 'CITGO CAISSON FOUNDATION' and 'MOUNTING PLATE'. The caisson foundation is a rectangular base with four anchor bolts. The mounting plate is a rectangular plate with four holes for the anchor bolts. Dimensions for the mounting plate include a total width of 16 1/2 inches, a total height of 21 inches, and a central cut-out of 6 1/8 inches by 6 1/8 inches. Callouts specify '(4) 1 1/16" DIA. HOLE' and '(4) SLOT 7/8" DIA. x 1-1/8" DIA.' for the anchor bolts. The drawing is displayed in a Microsoft Internet Explorer browser window at 150% zoom.

<https://www.citgo.com/MarketNetWebOther/LightOils/Branding/SignsPoles.pdf> - Microsoft Internet Explorer provided

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158%

POLE ELEVATION

1/4" x 1 1/4" PLATE FORMED TO FIT OPENING

3"
 5"
 1/4" x 3/4" BAR
 DRILL & TAP FOR 1/4" - 20
 1/4" - 20 MACH. SCREW
 STEEL TUBE
 HANDHOLE & COVER

MOUNTING PLATE

8 1/2"
 8 1/2"
 4 1/4"
 4 1/4"
 8 1/2"
 11"
 22"
 21"
 10 1/2"
 6 1/8" x 6 1/8" CUT-OUT
BASE PLATE

-25-

Done

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January 9, 2003

To: Alex Jaegeman

From: Charles Eshbach

Re: #1-Signage on top of the new leftfield fence

#2-Relocation of signage from both levels of the existing leftfield fence

Attached is our application to facilitate the changes we need to make per the installation of the new leftfield wall. Details on the Citgo and Coke signs should be arriving within the next day or so. I will deliver them to you as soon as I receive them. The information pertaining to the relocation of the signs from both levels of the existing leftfield wall is attached. Due to the ever-dwindling time before Opening Day, I ask that you move forward on that portion of our request immediately. Any consideration you can give in regards to the acceleration of this portion of the application would be greatly appreciated.

SIGNAGE PRE-APPLICATION

PLEASE ANSWER ALL QUESTIONS

ADDRESS: HADLOCK FIELD, 271 PARK AVENUE ZONE: ROS

OWNER: PORTLAND SEA DOGS / CITY OF PORTLAND

APPLICANT: PORTLAND SEA DOGS

ASSESSOR NO. _____

PLEASE CIRCLE APPROPRIATE ANSWER

SINGLE TENANT LOT? YES NO MULTI-TENANT LOT? YES NO

FREESTANDING SIGN? (ex. Pole Sign) YES NO --- DIMENSIONS _____ HEIGHT _____

MORE THAN ONE SIGN? YES NO DIMENSIONS _____ HEIGHT _____

SIGN ATTACHED TO BLDG.? YES NO DIMENSIONS _____

MORE THAN ONE SIGN? YES NO DIMENSIONS _____

AWNING: YES NO IS AWNING BACKLIT? YES NO HEIGHT OFF SIDEWALK _____

IS THERE ANY MESSAGE, TRADEMARK OR SYMBOL ON IT? _____

LIST ALL EXISTING SIGNAGE AND THEIR DIMENSIONS:

*** TENANT BLDG. FRONTAGE (IN FEET): _____

*** REQUIRED INFORMATION

AREA FOR COMPUTATION

YOU SHALL PROVIDE:

A SITE SKETCH AND BUILDING SKETCH SHOWING EXACTLY WHERE EXISTING AND NEW SIGNAGE IS LOCATED MUST BE PROVIDED. SKETCHES AND/OR PICTURES OF PROPOSED ARE ALSO REQUIRED.

SIGNATURE OF APPLICANT: _____ DATE: _____

THIS IS NOT A PERMIT/CONSTRUCTION MAY NOT COMMENCE UNTIL THE PERMIT IS ISSUED

Signage Application

If you or the property owner owes real estate or personal property taxes or user charges on any property within the City, payment arrangements must be made before permits of any kind are accepted.

Location/Address of Construction: HADLOCK FIELD, 271 PARK AVENUE		
Total Square Footage of Proposed Structure	Square Footage of Lot	
Tax Assessor's Chart, Block & Lot Chart# Block# Lot#	Owner: PORTLAND SEA DOGS / CITY OF PORTLAND	Telephone: 874-9300
Lessee/Buyer's Name (If Applicable)	Applicant name, address & telephone: PORTLAND SEA DOGS 271 PARK AVE.	Total s.f. of signage _____ x 1.00 per s.f. \$ _____, plus \$30.00 base fee Fee: \$
Current use: <u>BASEBALL STADIUM</u>		
If the location is currently vacant, what was prior use: _____		
Approximately how long has it been vacant: _____		
Proposed use: _____		
Project description:		
Contractor's name, address & telephone: CHARLES ESHBACH OR		
Who should we contact when the permit is ready: PETER DRIVAS, c/o PORTLAND SEA DOGS		
Mailing address: P.O. BOX 636 874-9300		
PORTLAND, ME 04104		
We will contact you by phone when the permit is ready. You must come in and pick up the permit and review the requirements before starting any work, with a Plan Reviewer. A stop work order will be issued and a \$50.00 fee if any work starts before the permit is picked up. Phone: 874-9300		

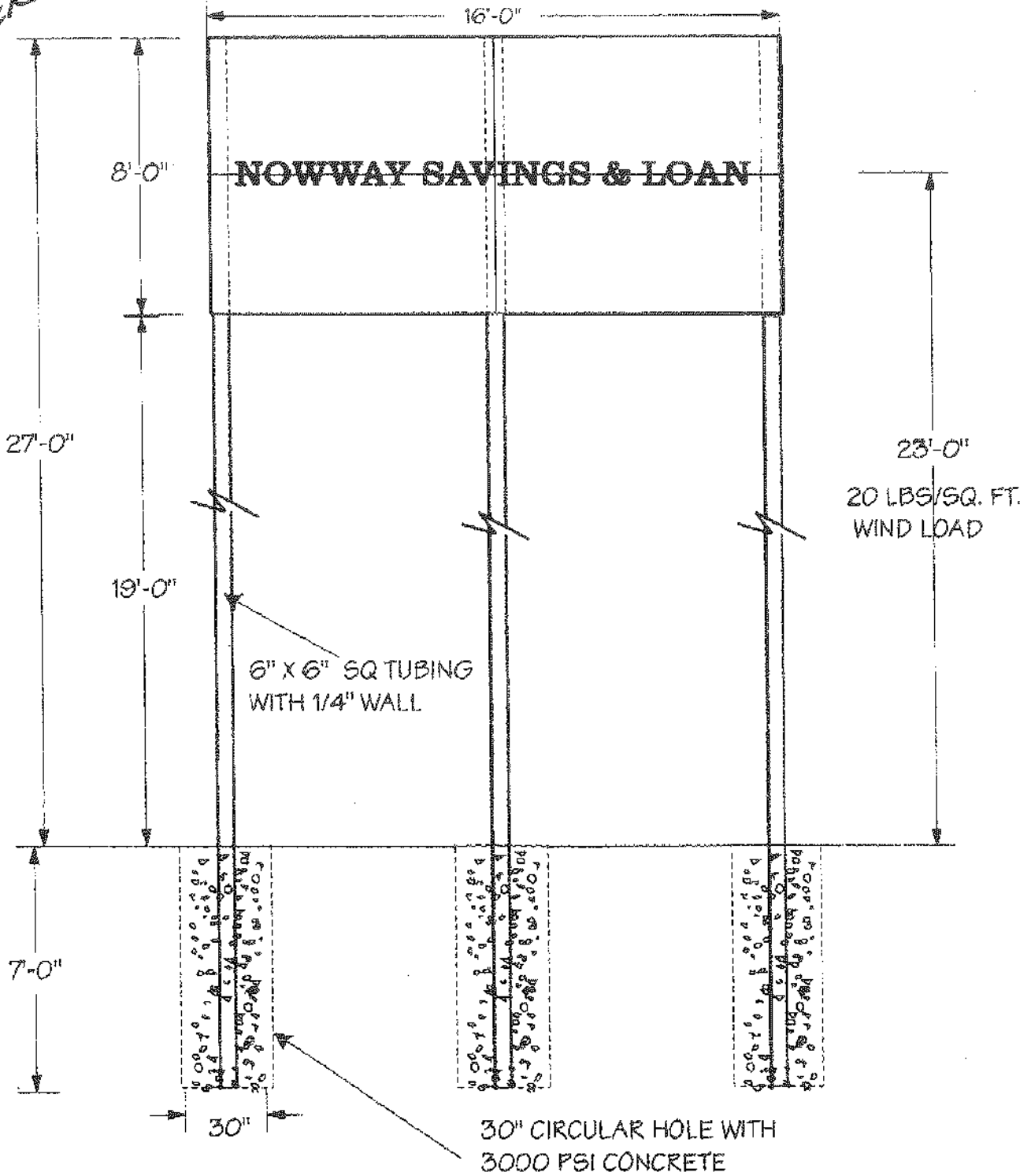
IF THE REQUIRED INFORMATION IS NOT INCLUDED IN THE SUBMISSIONS THE PERMIT WILL BE AUTOMATICALLY DENIED AT THE DISCRETION OF THE BUILDING/PLANNING DEPARTMENT, WE MAY REQUIRE ADDITIONAL INFORMATION IN ORDER TO APPROVE THIS PERMIT.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Code Official's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

Signature of applicant: 	Date: 1/9/03
---	---------------------


This is NOT a permit, you may not commence ANY work until the permit is issued. If you are in a Historic District you may be subject to additional permitting and fees with the Planning Department on the 4th floor of City Hall

PETER



MANUFACTURE AND INSTALL:
 NEW BILLBOARDS FOR THEIR ROW OF SIGNS AT BALLPARK.

This design created in conjunction with this project being planned for you is the exclusive property of SIGN SOLUTIONS, and can not be copied, exhibited or shown to anyone outside of your organization with consent of SIGN SOLUTIONS

 DATE NO: REVISION: APPROVAL:	DATE: 12/28/02	SCALE: NTS
	WORK ORDER NO: 8P3908	DESIGNED BY: BOB PHILLIPS
JOB NAME: NEW BILLBOARDS.		
LOCATION: HADLOCK FIELD PORTLAND, ME.		
© Copyright 2002		

**CITY OF PORTLAND, MAINE
MEMORANDUM**

TO: Marge Schmuckal, Zoning Administrator
FROM: Joseph E. Gray, Jr., City Manager
DATE: January 23, 2003
SUBJECT: Hadlock Field Signs

As property owner of Hadlock Field, this memorandum is to confirm that the City of Portland has given permission to the Portland Sea Dogs (as tenant of Hadlock Field) to apply for any sign related permits for Hadlock Field.

THIS IS NOT A PERMIT/CONSTRUCTION MAY NOT COMMENCE UNTIL THE PERMIT IS ISSUED

Signage Application

If you or the property owner owes real estate or personal property taxes or user charges on any property within the City, payment arrangements must be made before permits of any kind are accepted.

Location/Address of Construction:		
Total Square Footage of Proposed Structure		Square Footage of Lot
Tax Assessor's Chart, Block & Lot Chart# Block# Lot#	Owner:	Telephone:
Lessee/Buyer's Name (if Applicable)	Applicant name, address & telephone:	Total s.f. of signage _____ x 1.00 per s.f. \$ _____, plus \$30.00 base fee Fee: \$
Current use: _____		
If the location is currently vacant, what was prior use: _____		
Approximately how long has it been vacant: _____		
Proposed use: _____		
Project description:		
Contractor's name, address & telephone:		
Who should we contact when the permit is ready: _____		
Mailing address:		
We will contact you by phone when the permit is ready. You must come in and pick up the permit and review the requirements before starting any work, with a Plan Reviewer. A stop work order will be issued and a \$50.00 fee if any work starts before the permit is picked up. Phone:		

IF THE REQUIRED INFORMATION IS NOT INCLUDED IN THE SUBMISSIONS THE PERMIT WILL BE AUTOMATICALLY DENIED AT THE DISCRETION OF THE BUILDING/PLANNING DEPARTMENT, WE MAY REQUIRE ADDITIONAL INFORMATION IN ORDER TO APPROVE THIS PERMIT.

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Signature of applicant:	Date:
-------------------------	-------

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THIS IS NOT A PERMIT/CONSTRUCTION MAY NOT COMMENCE UNTIL THE PERMIT IS ISSUED

Please check off the following indicating that you have included the below items to expedite the process of this sign application:

LEGAL • Certification of Liability listing the City additionally insured if any portion of the sign abuts or encroaches on the public right of way, or can fall into the public right of way, you must be in the amount of \$400,000.00

LETTER FROM CITY • Letter of permission from the owner

A sketch plan indicating the following:

- NEED TO SHOW*
- Drawing of the property showing all dimensions of the lot
 - Location of all buildings and property setbacks from all buildings
 - Driveways and abutting streets showing street frontage and any right of ways
 - Indicate on drawing the dimensions of all buildings on the lot
 - Define in footage the frontage of your business front
 - Indicate on drawing of existing signage and dimensions of each sign
 - Indicate on drawing all proposed signage and dimension of each sign

SIGN SOLUTIONS
LOCATION & APPROX. #
COLOR PHOTO

The Provisions of Section 3102.0 of the City's Building Code "The BOCA National Building Code 1999 edition" shall govern the construction, alteration, repair and maintenance of outdoor signs together with the associated appurtenant and auxiliary devices in respect to structural and fire safety. In accordance to section 3102.4.4 of BOCA construction documents and owners consent is all follows; "Before any permit is issued for the erection of a sign, Construction Documents shall be filed with the code official showing the dimensions, materials and required details of construction, including loads, stresses and anchorage. The applications shall be accompanied by the written contract of the owner or lessees of the premises upon which the sign is to erected. NO PERMIT CAN OR WILL BE ISSUED UNLESS THIS INFORMATION IS SBUMITTED AND APPROVED BY THE INSPECTIONS OFFICE.

separate permit **ELECTRICAL SIGNAGE PERMITS/RESPONSIBILITIES**

All sign companies or any persons engaged in the installation, interchange or maintenance of signage in the City of Portland must have the Electrician or Electrical Contractor who provided power to the sign(s) or associated equipment apply for an electrical permit in the Inspections Office.

It is the responsibility of your company to contact your sub-contractor or he owner to inform them of this policy. Whether your company does the final connections, which requires a valid Maine Electricians License or your sub contractor provides this service; permits and inspections shall be required.

Failure to comply with this procedure may result in the denial of sign permits by this office for your company or its representation to install or interchange any future signage in the City of Portland.

BY FILLING OUT THIS APPLICATION IS DOES NOT MEAN THAT YOU WILL BE APPROVED FOR THE AMOUNT OF SIGNAGE YOU ARE APPLYING FOR

IT IS SUGGESTED THAT YOU DO NOT ORDER ANY SIGNAGE UNTIL YOU HAVE RECEIVED YOUR SIGN PERMIT THAT HAS BEEN SIGNED BY THE BUILDING, ZONING AND POSSIBLE HISTORICAL OFFICIALS OF THE CITY OF PORTLAND

SIGNAGE PRE-APPLICATION

PLEASE ANSWER ALL QUESTIONS

ADDRESS: _____ ZONE: _____

OWNER: _____

APPLICANT: _____

ASSESSOR NO. _____

PLEASE CIRCLE APPROPRIATE ANSWER

SINGLE TENANT LOT? YES NO MULTI-TENANT LOT? YES NO
FREESTANDING SIGN? (ex. Pole Sign) YES NO --- DIMENSIONS _____ HEIGHT _____
 MORE THAN ONE SIGN? YES NO DIMENSIONS _____ HEIGHT _____
SIGN ATTACHED TO BLDG.? YES NO DIMENSIONS _____
 MORE THAN ONE SIGN? YES NO DIMENSIONS _____
AWNING: YES NO IS AWNING BACKLIT? YES NO HEIGHT OFF SIDEWALK
IS THERE ANY MESSAGE, TRADEMARK OR SYMBOL ON IT?

LIST ALL EXISTING SIGNAGE AND THEIR DIMENSIONS:

*** TENANT BLDG. FRONTAGE (IN FEET): _____
*** REQUIRED INFORMATION

AREA FOR COMPUTATION

YOU SHALL PROVIDE:

**A SITE SKETCH AND BUILDING SKETCH SHOWING EXACTLY WHERE
EXISTING AND NEW SIGNAGE IS LOCATED MUST BE PROVIDED. SKETCHES
AND/OR PICTURES OF PROPOSED ARE ALSO REQUIRED.**

SIGNATURE OF APPLICANT: _____ DATE: _____

Side Walk Signs Design/Location/Construction Standards

Quantity

One sign per establishment for each street frontage having a public entrance provided that all dimension and location standards are met. When standards would not otherwise permit a sign, a sign may consist of multiple listings.

Sign Dimensions

Single listing: Maximum width - 24 inches or such lesser width sufficient to retain 4 ½ feet of unobstructed sidewalk width perpendicular to major flow. Maximum height - 40 inches to top of sign in place. Minimum height 30 inches to top of sign in place.

Multiple: Maximum width - 30 inches or such lesser width sufficient to retain 4 ½ feet of unobstructed sidewalk width perpendicular to major flow. Maximum height - 4- inches to top of sign in place. Minimum height 30 inches to top of sign in place.

Location

Minimum distance between signs - 20 feet. Maximum distance of sign from public entrance of advertiser 20 feet. The City may vary these distances for exceptional physical circumstances where public safety and streetscape aesthetics will be maintained. However, under no circumstances shall signs obstruct vehicular stops, benches, fire hydrants, or other street visual amenities. Signs shall be located near the curb rather than the building face.

Materials and Graphics

All signs shall be of an A-Frame type design, shall be constructed of durable, weather-resistant materials and finish, shall have no moving parts, and shall be non-electrified. All signs shall be maintained in a clean and original appearance. Sign materials, graphics, and finish shall be of a unified design and shall be compatible with the local streetscape. All signs shall have horizontal braces spanning each side of the sign to assure rigid support. Lettering shall be legible and consistent.

Sign Removal

All signs shall be removed when the business is closed or while any snow or ice exists on the walk within eight feet of the sign in any direction.

Insurance

No permit shall be issued unless the applicant has posted in advance with the City a suitable public liability insurance certificate in an amount adequate to protect the City.

Enforcement

A sign may be removed after notice to the owner and the permit, if issued, may be revoked if the sign does not conform to the standards herein.

For permit come to City Hall 389 Congress Street room 315 with:

1. Certificate of liability insurance
2. Drawing of sign showing dimensions
3. Payment of ~~1.00~~ per sq. ft. plus \$30.00

389 Congress Street, 4th Floor
Portland, ME 04101
(207)874-8721 or (207)874-8719
Fax: (207)756-8258

**City of Portland
Planning and Development Department
Planning Division**

Fax

To: CHARLIE ESHBACH Company: PORTLAND SEA DOGS
Fax: 780-0317 Date: 1-23-02
From: RICK KNOWLAND

Comments:

CHARLIE,
ATTACHED IS A LETTER REGARDING SIGNS AT HADLOCK
FIELD. MAKE SURE THAT THE LOCATION OF THE CITGO SIGN
IS ACCURATELY DEPICTED. IT OBVIOUSLY NEEDS TO BE ON
CITY PROPERTY BUT THERE ISN'T MUCH ROOM BETWEEN THE
OUTFIELD FENCE AND THE RAILROAD PROPERTY LINE.
SHOULD YOU HAVE ANY QUESTIONS GIVE ME A CALL.

RICK KNOWLAND

You should receive 2 page(s), including this cover sheet. If you do not receive all of the pages, please call (207)874-8721.



January 17, 2003

To: Alex

From: Charlie

Re: CITGO & Coke Signs

Enclosed are specs for the two signs. They will both be mounted on our existing fence supports.

I met with representatives from MDOT this morning. They are okay on the CITGO sign, and indicated that we could put our own logo on the back of the CITGO sign. They think that the Coke bottle is okay as long as it doesn't say Coke on the highway side.

Please let me know if you have any questions.

12'x12' HI-RISE SIGN - 35' TAPERED POLE

SIGN SPECIFICATIONS

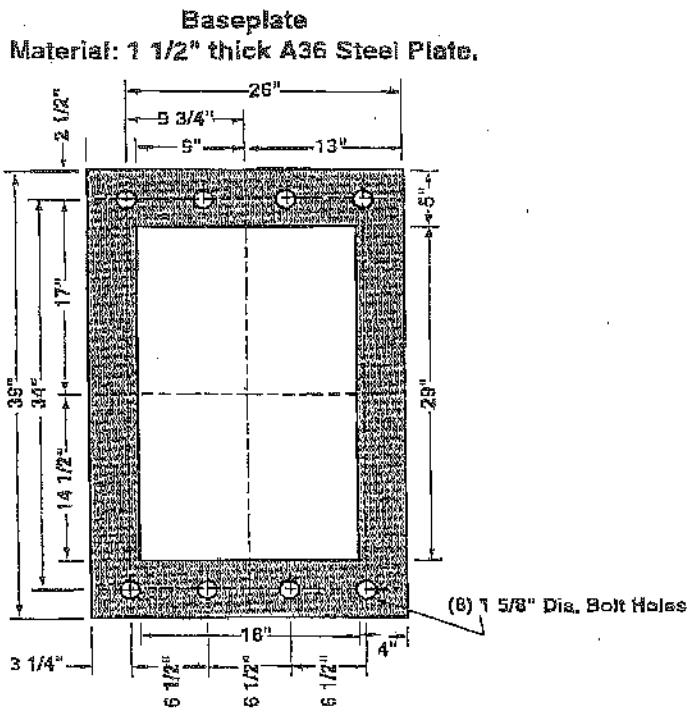
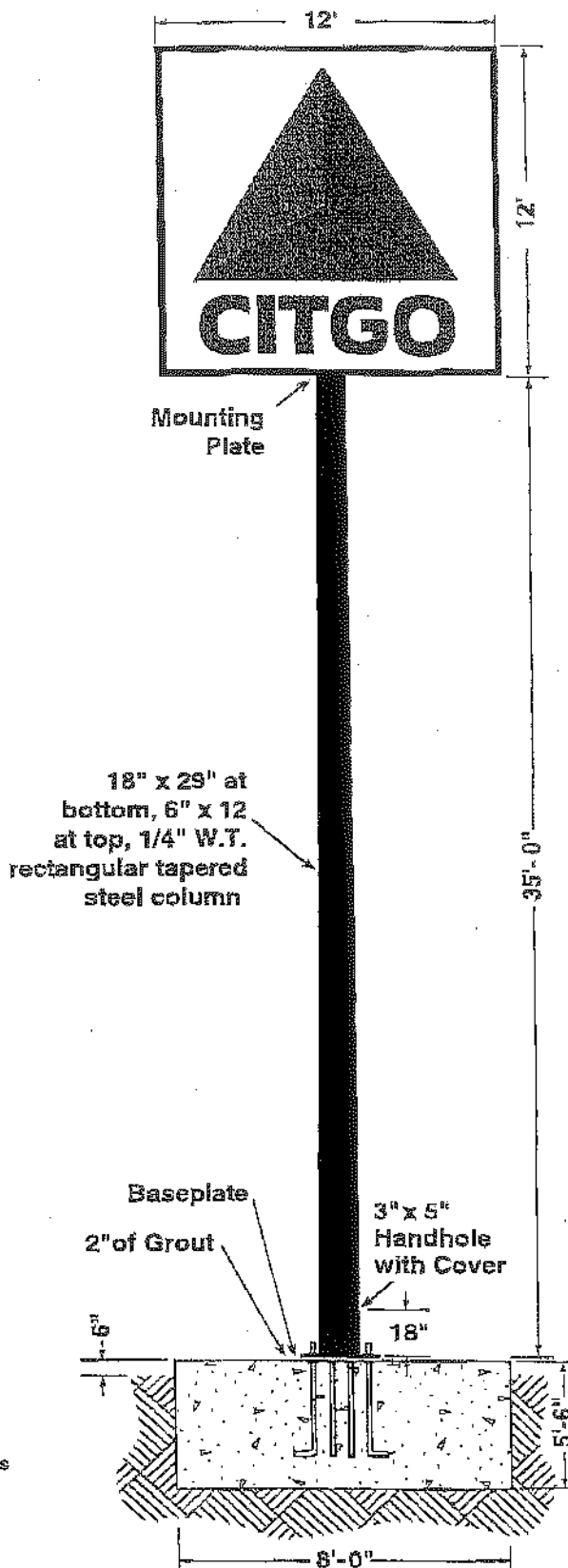
Amps: 35.1
 Net Weight: 1,296 lbs.
 Area: 144 square feet

POLE SPECIFICATIONS

Design Windload: 30 PSF/100MPH
 Pole Supports: 1500 lbs.
 Rectangular Tapered Corten Steel Pole,
 18" x 29" at the bottom, 6" x 12" at the top,
 1/4" wall thickness
 Baseplate:
 26" x 39" x 1 1/2" thick A36 Steel Plate.
 Sign Mounting Plate:
 18" x 26" x 1 3/8" thick A36 Steel Plate.
 Anchor Bolts:
 Eight (8) 1 3/8" Dia. x 56" long Steel Anchor
 Bolts with 6" long hooks.

FOUNDATION SPECIFICATIONS

Size:
 15'-0" x 8'-0" x 5'-6" deep (6" above grade).
 Amount of Concrete: 24.45 Cubic Yards.
 Design Windload: 30 PSF/100MPH

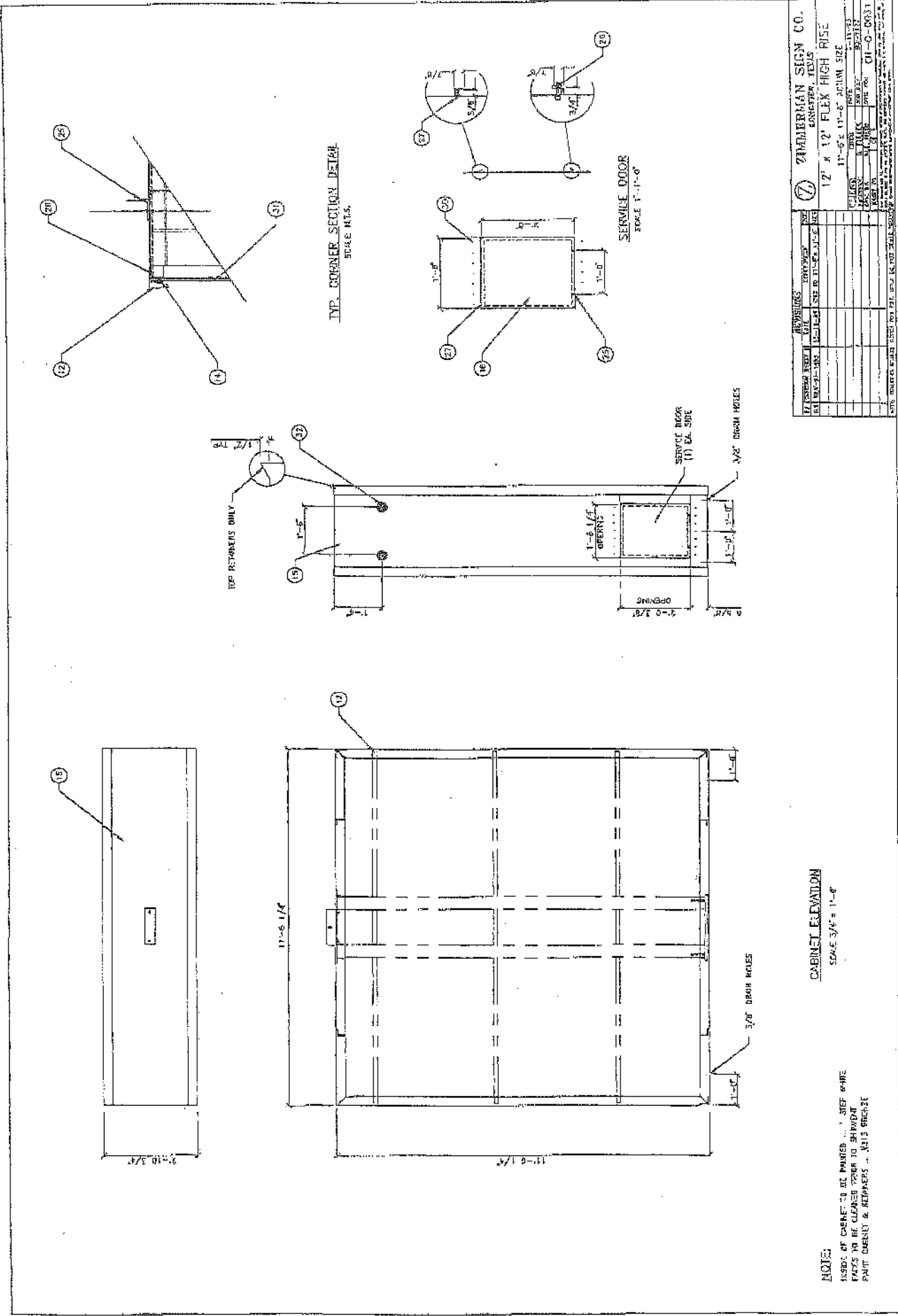


DISC. 12'x 12' D/F C/P HIGH RISE
 NO. CIT-0-B031
 SHT. 1 OF 2

BILL-OF-MATERIALS

ISSUE DATE: 9-14-93
 REVISIONS

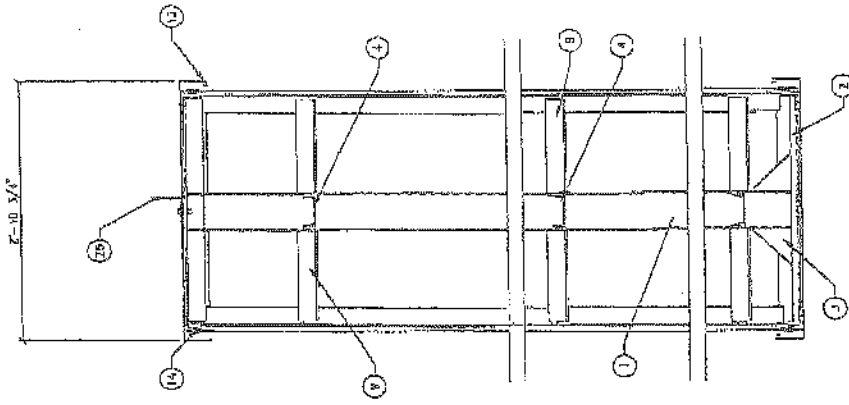
ITEM	SPEC.	DESCRIPTION	ZIM. NO.	QTY.
1	A-36	.187 x 5" x 5" STL. TUBE @ 11'-5 7/8" LG.		2
2	A-36	1" x 30 1/2" x 23 3/4" STL. MOUNTING PLATE	ZIM-1	1
3	A-36	1/2" x 5" x 5" GUSSET	ZIM-1	4
4	A-36	C4 x 5.4 STL. CHANNEL (6) 4'-8 1/2" (3) 1'-2"		9
5	A-36	3/16" x 3" x 3" STL. ANGLE x 2'-6" LG.		4
6	A-36	3/16" x 2 1/2" x 2 1/2" STL. ANGLE @ 11'-6" LG.		4
7	A-36	3/16" x 2 1/2" x 2 1/2" STL. ANGLE @ 11'-0 7/8" LG.		4
8	A-36	3/16" x 2 1/2" x 2 1/2" STL. ANGLE @ 2'-6" LG.		14
9	A-36	3/16" x 2 1/2" x 2 1/2" STL. ANGLE @ 3'-0" LG.		4
10				
11				
12	3003-H14	.050 ALUM. RETAINER (8) 8'-0" (8) 3'-6 1/4" LG.	ZIM-1	16
13	A-36	1/8" x 1" x 1" STL. ANGLE (8) 10'-11" LG. (4) 5" LG.		8
14		FLEX FACE CLAMP ASS'Y W/ NUT		136
15	3003-H14	.050 x 2'-6" x 11'-6" ALUM. SHEET METAL (4) 8'-0" (4) 3'-6"		8
16	3003-H14	.050 x 1'-8" x 2'-0" ALUM. SHEET METAL DOOR		2
17	6063-T5	M2076 RWC. 9(A) @4" 3(B) @10 7/8" 4(C) @1'-4" 3(D) @10 15/16"		19
18		Ø1/2" x 1/2" RIGID NIPPLE		9
19		#1110-247SC-TC LAMP BALLAST @3.9 AMPS		9
20		400 WT METAL HALIDE UNIVERSAL MOUNT		9
21		HID LAMP SOCKET		9
22		LAMP DIFFUSER \ ANTI ROTATION BRACKET		9
23		M2056 EXTR. ALUM RMY @ 1'-4" LG.		1
24		Ø1/4" x 1" H.H. BOLTS W/LOCKWASHERS & NUTS		36
25		1/4" x 4" x 4" LIFTING ANGLE @ 16" w/(Ø3/4" BOLTS & WELD NUTS)		1set
26		SOUTHCO FASTENERS		4
27		PIANO HINGE		2
28		#8x 3/4" THREAD CUTTING SCREWS		-



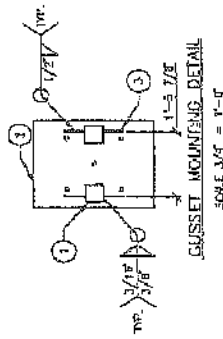
ZIMMERMAN SIGN CO.	
EXHIBIT, JETS	
12' x 12' FLEX HIGH RISE	
11'-6\"/>	
REV	DATE
1	11-11-03
2	11-11-03
3	11-11-03
4	11-11-03
5	11-11-03
6	11-11-03
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98	11-11-03
99	11-11-03
100	11-11-03

CABINET ELEVATION
SCALE 3/4" = 1'-0"

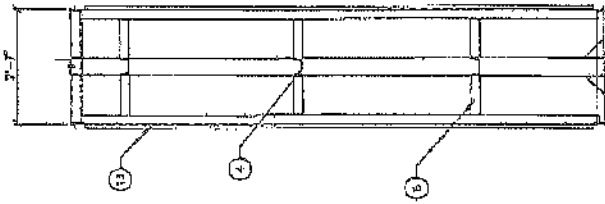
NOTE:
INSIDE OF CABINET TO BE PAINTED... SEE 6-WFE
FACES TO BE CLEANED PER TO SHIP/DIE
PAINT CABINET & REPAIRS - R115 BRN3T



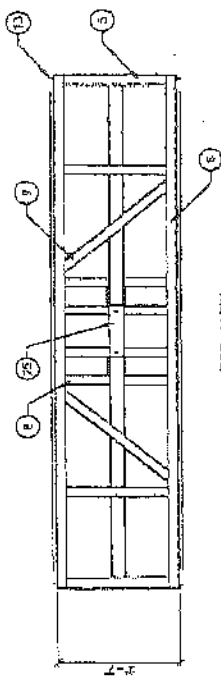
TYPICAL SECTION
SCALE 1 1/2" = 1'-0"



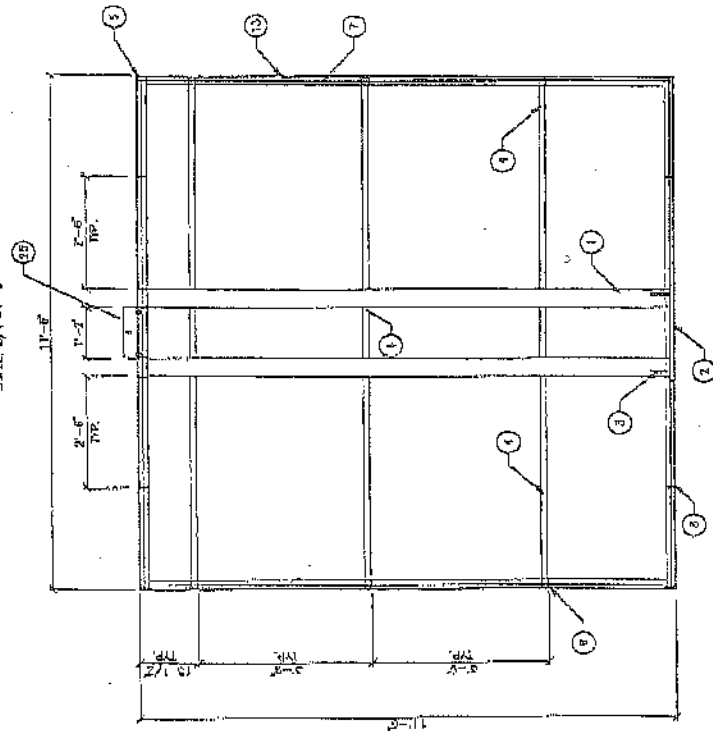
GUSSET MOUNTING DETAIL
SCALE 3/4" = 1'-0"



END VIEW
SCALE 3/4" = 1'-0"



TOP VIEW
SCALE 3/4" = 1'-0"



STRUCTURAL DETAIL
SCALE 3/4" = 1'-0"

REV	DATE	DESCRIPTION
1	01-13-03	ISSUE FOR CONSTRUCTION
2	01-13-03	ISSUE FOR CONSTRUCTION
3	01-13-03	ISSUE FOR CONSTRUCTION
4	01-13-03	ISSUE FOR CONSTRUCTION
5	01-13-03	ISSUE FOR CONSTRUCTION
6	01-13-03	ISSUE FOR CONSTRUCTION
7	01-13-03	ISSUE FOR CONSTRUCTION
8	01-13-03	ISSUE FOR CONSTRUCTION
9	01-13-03	ISSUE FOR CONSTRUCTION
10	01-13-03	ISSUE FOR CONSTRUCTION

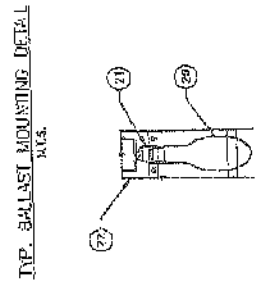
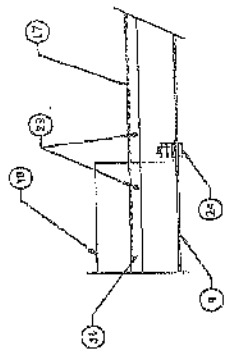
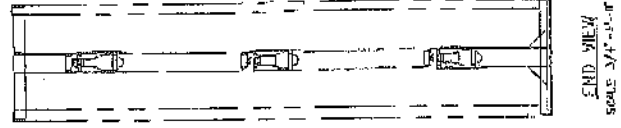
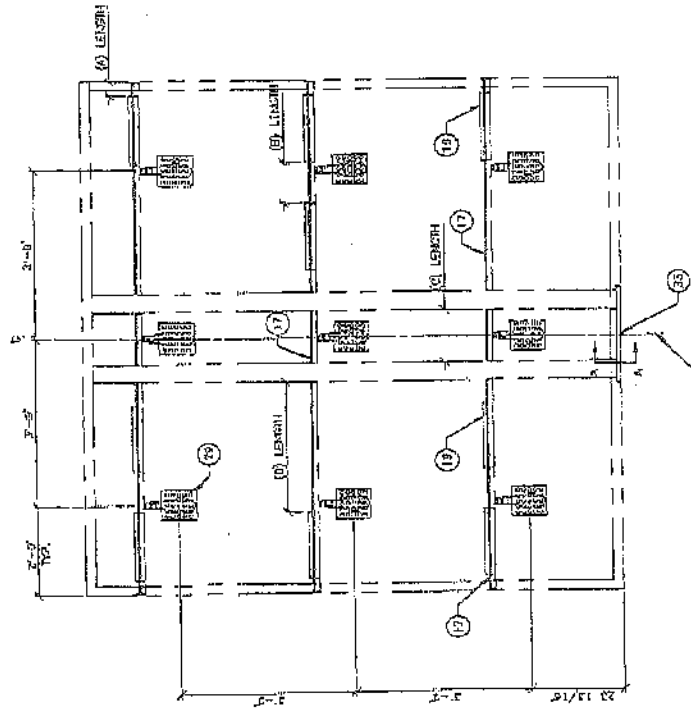
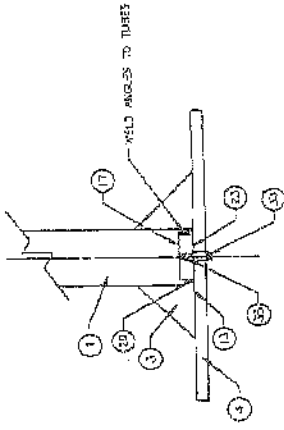
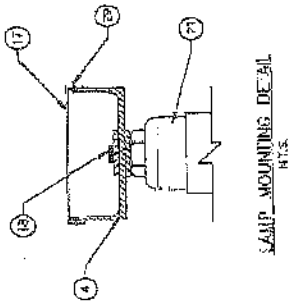
ZIMMERMAN SIGN CO.
LONGMONT, COLORADO

12' x 12' FLEX HIGH RISE

17'-8" x 17'-6" ACTUAL SIZE

OWNER: [REDACTED]
ARCHITECT: [REDACTED]
DATE: [REDACTED]
PROJECT NO.: [REDACTED]

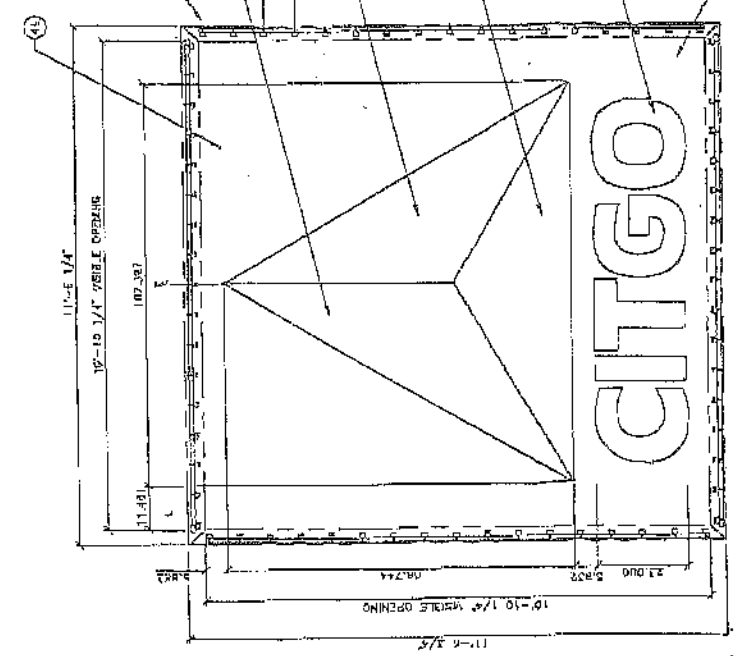
PRIME & PAINT STRUCTURE ... 1 STEP WHITE



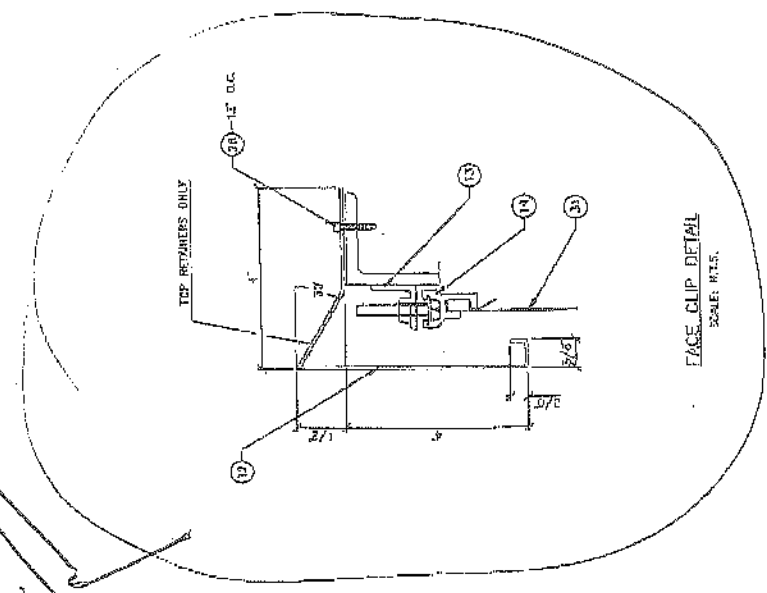
ZIMMERMAN SIGN CO.	
CONSTRUCTION DEPARTMENT	
12' x 12' FLEX HIGH RISE	
PROJECT NO.	11-0000
DATE	11-00-00
SCALE	1/4" = 1'-0"
BY	J. J. J.
CHECKED BY	J. J. J.
DATE	11-00-00
SCALE	1/4" = 1'-0"
THIS DRAWING IS THE PROPERTY OF ZIMMERMAN SIGN CO. IT IS TO BE USED ONLY FOR THE PROJECT AND AT THE LOCATION SPECIFIED THEREON. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF ZIMMERMAN SIGN CO.	

ELECTRICAL ENTRANCE (AS 1 AMP 107/A)
151 20 AMP CIRCUITS (RED.)

RAIN LI



- TOP CLIP DETAIL
- UPPER LI. (R. TRANSL. LI. RED FR-1-351/AD-6334 PMS 329 (TRANSLUCENT))
- UPPER FR. (R. TRANSL. FR. RED GT-1-3858/1B-6299 PMS 301 (TRANSLUCENT))
- LOWER TRANSL. (R. RED UB-6481 (PMS 183) (TRANSLUCENT))
- COPY BLUE (P-1-410-4/B-2331 (TRANSLUCENT))
- UPPER WHITE (P-1-176/A/S-6334 (TRANSLUCENT))



SIGN SPECIFICATIONS:
 SIGN SIZE ...
 AREA ...
 ACTUAL SIZE ...
 APPROXIMATE WEIGHT ...
 MATERIAL ...
 COLORS ...
 FINISH ...
 MOUNTING ...
 LOWER LI. (R. TRANSL. LI. RED FR-1-351/AD-6334 PMS 329 (TRANSLUCENT))
 LOWER FR. (R. TRANSL. FR. RED GT-1-3858/1B-6299 PMS 301 (TRANSLUCENT))
 UPPER TRANSL. (R. RED UB-6481 (PMS 183) (TRANSLUCENT))
 COPY BLUE (P-1-410-4/B-2331 (TRANSLUCENT))
 UPPER WHITE (P-1-176/A/S-6334 (TRANSLUCENT))
 CABINET & RETURNERS ...

FACE ELEVATION
 SCALE 3/4" = 1'-0"

NOTE:
 MOBILE FACE OPENING: 10'-10 1/4" x 10'-10 1/4"

ZIMMERMAN SIGN CO. LONGVIEW, TEXAS	
PROJECT NO. 11-0-44	DATE 11-23-02
CLIENT: MOBILE	SCALE: 3/4" = 1'-0"
DATE: 11-23-02	BY: [Signature]
CHECKED: [Signature]	DATE: 11-23-02
APPROVED: [Signature]	DATE: 11-23-02



12X12 FLEX FACE

144" ACTUAL SEE

180" 14" V.D.

HADLOCK FIELD
HOME OF THE
Sea Dogs™



144" ACTUAL SEE

180" 14" V.D.

Approved Approved via email

OWNER _____ CAP _____

REVISION _____ JUSTIFIED _____

RELEASE A

PART NUMBER	DESCRIPTION	COLORS
3M 3630-22		BLACK
3M 3630-06		BLUE
3M 3630-33		RED
3M 3630-75		YELLOW
3M 3630-51		GRAY

DESIGN BY
 Copyright © 2003 CICO
 All Rights Reserved

Sales Rep: Dan Fuller
 Acct Mgr: Dianne Day
 Artist: [Signature]

co #: C102559
 date: 01.15.03
 elessert



Charlie Eshbach

From: <Artproinc@cs.com>
To: <ceshbach@portlandseadogs.com>
Cc: <lmelanson@cconne.com>
Sent: Tuesday, January 14, 2003 3:24 PM
Attach: 15'-Portland.jpg
Subject: CC Bottle

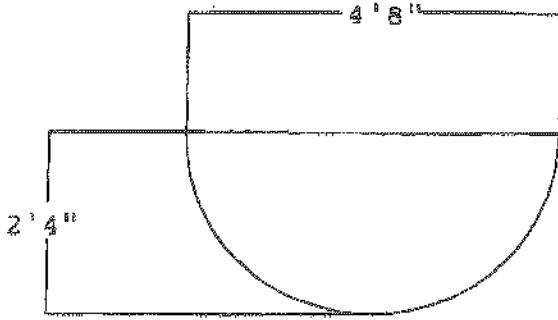
Charlie -

This is a photo of a 15' bottle, similar design as the Fenway bottles. The next email will have a drawing like the previous ones, but updated with information specifically for this bottle, and a stat sheet. Thanks, Dianna

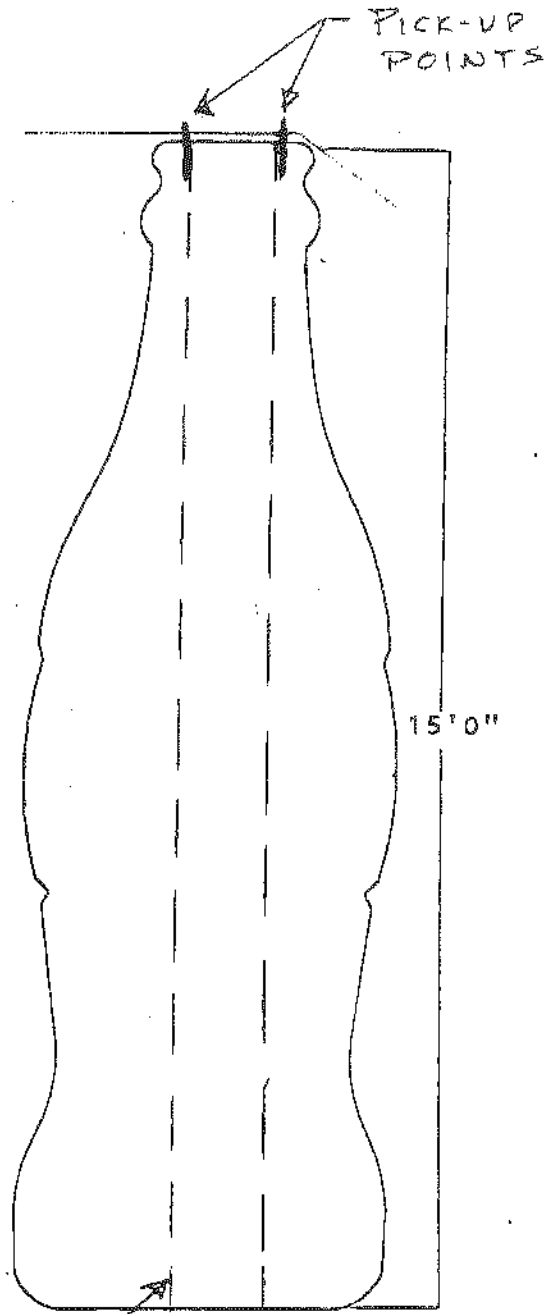
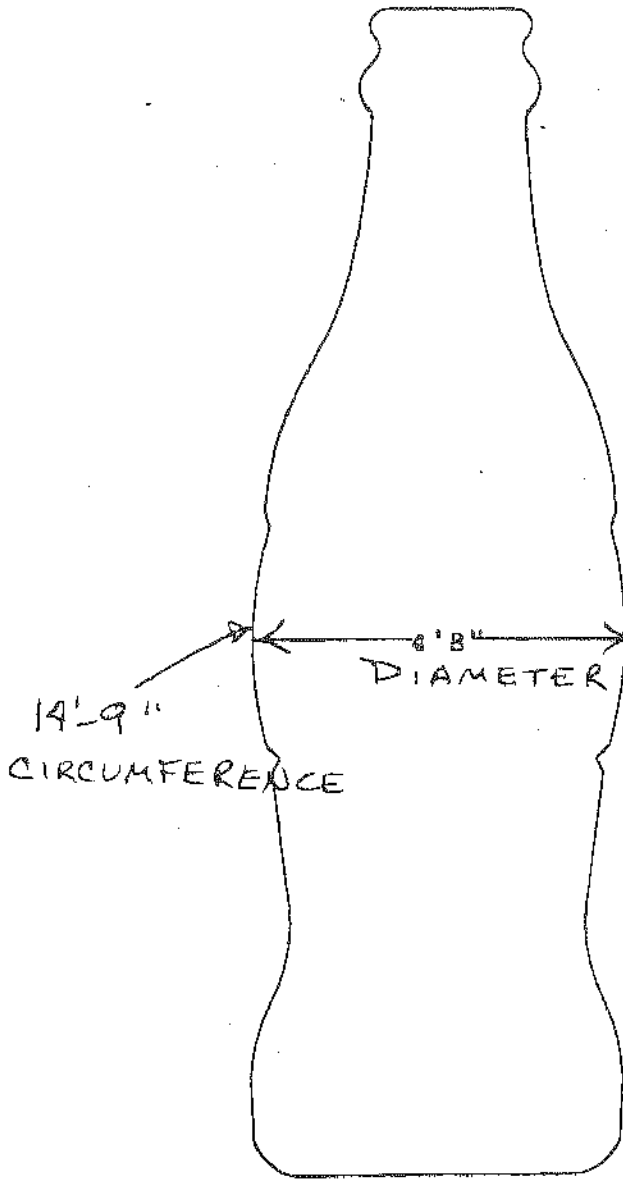


ART PRODUCTIONS, INC.

211 NEWPORT DR. * PEACHTREE CITY, GEORGIA 30269
PH 770-632-5525 * FAX 770-631-9789
(Call for shipping address.)



15' 3-D FIBERGLASS COCA-COLA BOTTLE



CENTER FRAME

ART PRODUCTIONS, INC.

artproductionsinc.com

January 14, 2003

TO: Charlie Eshbach
Portland Sea Dogs
FAX 207-780-0317
FROM: Dianna Snell
RE: Coca-Cola bottles

Charlie –

Larry asked me to forward some additional information to you.

Attached is a photo of a 15' bottle used during the Olympics in Atlanta. This design is the same as what is in Fenway Park with the exception of the label.

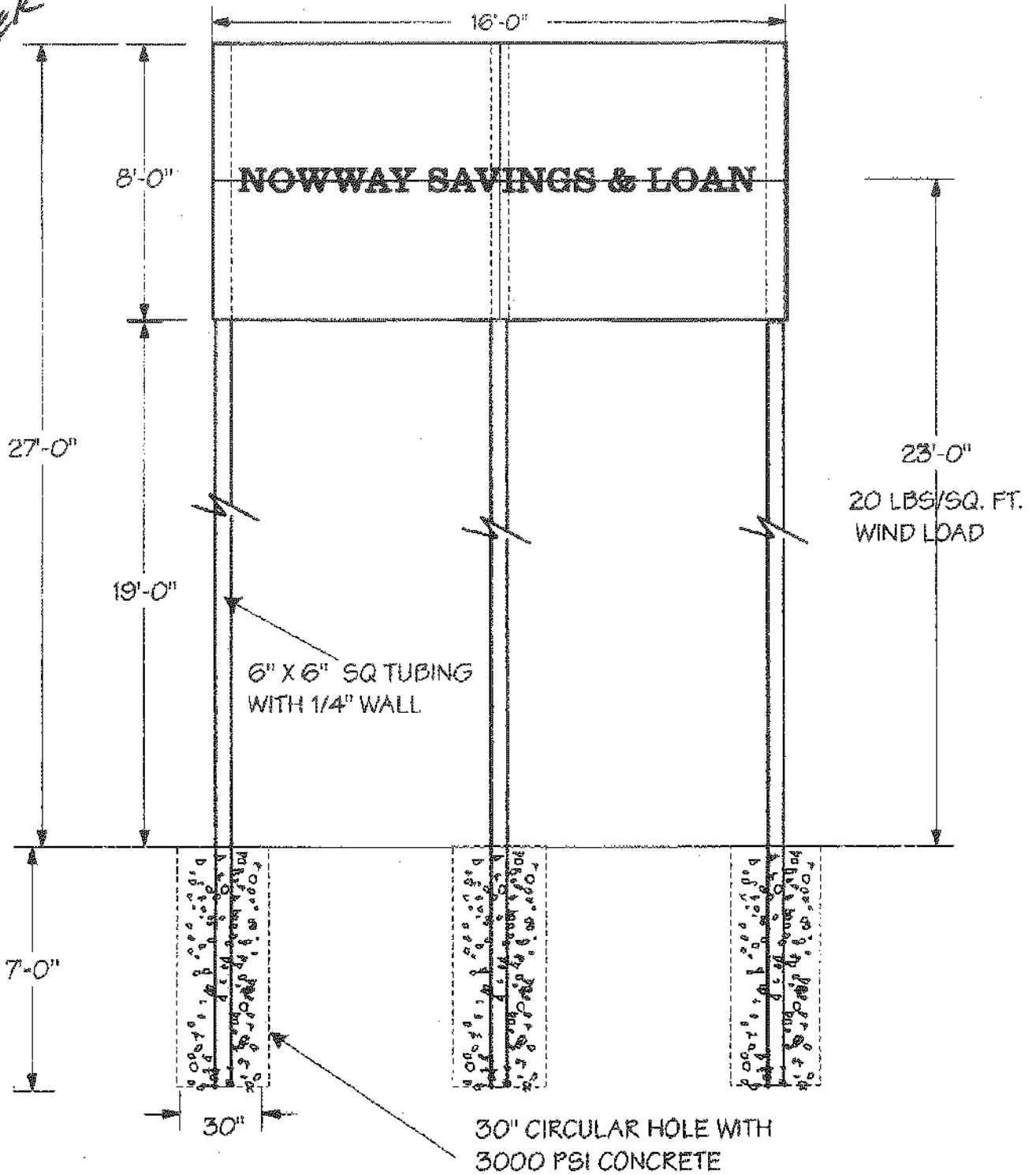
The fiberglass will be a standard 3/16" (approx) thickness. The bottle will be full round with an interior 'sleeve' corresponding to the pipe or I-beam that it will slide over. The dimensions are as designated on the attached drawing. Included is an example of how the interior will be fabricated. There will be 'pick-up' points for lifting the bottle into place. Based on adding the interior structure the estimated weight will be around 600 lbs.

I suggest setting the center of the bottle back at least 36" to make sure the bottle will not 'over-hang' the field.

Please let me know if you need anything else.


Thanks, Dianna Snell

PETER



MANUFACTURE AND INSTALL:
 NEW BILLBOARDS FOR THEIR ROW OF SIGNS AT BALLPARK.

This design created in conjunction with this project being planned for you is the exclusive property of SIGN SOLUTIONS, and can not be copied, exhibited or shown to anyone outside of your organization with consent of SIGN SOLUTIONS

 DATE: 12/28/02 WORK ORDER NO: 073905 JOB NAME: NEW BILLBOARDS. LOCATION: HADLOCK FIELD FORTLAND, ME. © Copyright 2002	SCALE: NTS
	DESIGNER: BOB PHILLIPS
	CHECKED:
	APPROVED: