

42-A-7

2012-519

54 York St.

Baxter Academy
Rufus Deering

on spreadsheet

Department of Transportation
Traffic Engineering Division
16 State House Station
Augusta, Maine 04333
Telephone: 207-287-3774

FOR MDOT USE 1/2000
ID #
Total Fees:
Date Received:

PERMIT APPLICATION - TRAFFIC TRAFFIC MOVEMENT PERMIT, 23 M.R.S.A. § 704 - A

Please type or print:

This application is for: Traffic 100-200 PCE's Traffic 200+ PCE's

Name of Applicant: Rufus Deering Lumber Company

Address: 384 Commercial Street, Portland, ME 04101 Telephone: (207) 772-6505

Name of local contact or agent: Stephen S. Sawyer, Jr., P.E., Sebago Technics, Inc.

Address: 75 John Roberts Road, Suite 1A, South Portland, ME 04106 Telephone: (207) 200-2082

Name and type of development: Baxter Academy for Technology and Science, Charter High School

Location of development including road, street, or nearest route number: 54 York Street, Portland, ME 04101

City/Town/Plantation: Portland, County: Cumberland, Tax Map # 42 Block A, Lot # 7 & 8

Do you want a consolidated review with DEP pursuant to 23 M.R.S.A. § 704-A (7)? Yes No

Was this development started prior to obtaining a traffic permit? No

Is the project located in an area designated as a growth area (as defined in M.R.S.A. title 30 - A, chapter 187)?

Yes No

Is this project located within a compact area of an urban compact municipality? Yes

Is this development or any portion of the site currently subject to state or municipal enforcement action?
No

Existing DEP or MDOT permit number (if applicable): N/A

Name(s) of DOT staff person(s) contacted concerning this application: _____

Name(s) of DOT staff person(s) present at the scoping meeting for 200+ applications: _____

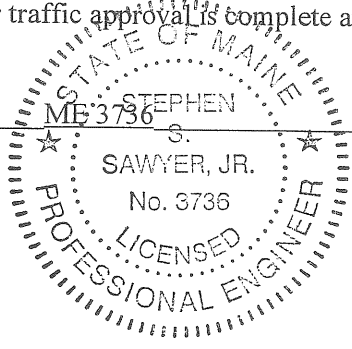
CERTIFICATION

The traffic engineer responsible for preparing this application and/or attaching pertinent site and traffic information hereto, by signing below, certifies that the application for traffic approval is complete and accurate to the best of his/her knowledge.

Signature: Stephen Sawyer, Jr. Re/Cert/Lic No.: ME 3736 S.

Name (print): Stephen S. Sawyer, Jr., P.E.

Date: 9/27/12



If the signature below is not the applicant's signature, attach letter of agent authorization signed by applicant.

"I certify under penalty of law that I have personally examined the information submitted in this document and all attachments thereto and that, based on my inquiry of those individuals immediately responsible for obtaining the information, I believe the information is true, accurate, and complete. I authorize the Department to enter the property that is the subject of this application, at reasonable hours, including buildings, structures or conveyances on the property, to determine the accuracy of any information provided herein. I am aware there are significant penalties for submitting false information, including the possibility of fine and imprisonment."

Dan [Signature]
Signature of applicant

9/27/12
Date

NOTICE OF INTENT TO FILE

Please take notice that Rufus Deering Lumber Company
 384 Commercial Street
 Portland, ME 04101
 (207) 772-6506

is intending to file a Traffic Movement Permit application with the Maine Department of Transportation and City of Portland pursuant to the provisions of 23 M.R.S.A. § 704 - A on or about September 28, 2012

The application is for the development of a 4-year Charter High School focusing in the areas of mathematics, technology, and science. The proposed development will generate 154 AM and 125 PM trips. Construction is anticipated for Fall 2012 at the following location:

54 York Street
Portland, ME 04101

A request for a public hearing must be received by the City, in writing, no later than 20 days after the application is found by the City to be complete and is accepted for processing. Public comment on the application will be accepted throughout the processing of the application.

The application will be filed for public inspection at the Department of Transportation Division office in Scarborough during normal working hours. A copy of the application may also be seen at the municipal offices in Portland, Maine.

Written public comments may be sent to the Department of Transportation, Traffic Engineering Division, 16 State House Station, Augusta, Maine 04333 or the City of Portland Planning Department, 389 Congress Street, Portland, ME 04101-3509.

JN 07383
Baxter Academy Charter School
TMP Abutters List

J. B. Brown and Sons
P.O. Box 207
Portland, ME 04112

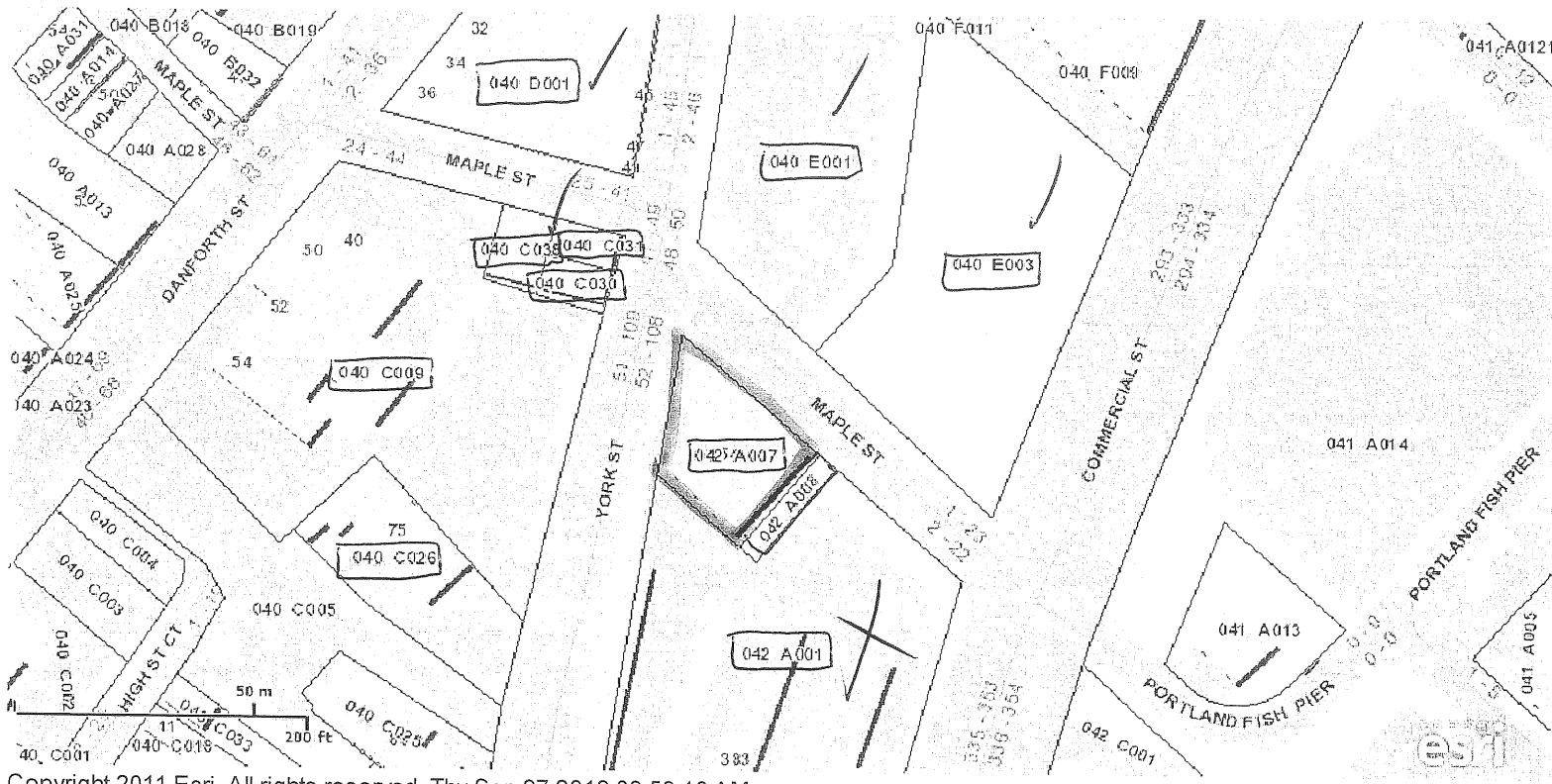
All In Enterprises LLC
5 Lincoln Avenue
Scarborough, ME 04074

75 York Street LLC
15 Franklin Street
Portland, ME 04101

My Map

07383

rd



Copyright 2011 Esri. All rights reserved. Thu Sep 27 2012 09:59:18 AM.

Section 1. Site and Traffic Information

A. Site Plan.

Rufus Deering Lumber Company, 384 Commercial Street, Portland is proposing to remodel their existing building at 54 York Street (the corner of York and Maple Streets) to accommodate the new Baxter Academy for Technology and Science Charter High School. The current property houses the existing Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses of offices, labs, and a dance studio. This 0.22 acre (9,583 s.f.) lot is fully developed with an existing 7,540 s.f., 3-story building in the City's B-5b (Urban Business) Zone. This property is also located within the Old Port Historic District. See the Site Plan in the back of this submission.

B. Existing and Proposed Site Uses.

The existing building is currently developed with the following land uses:

- An exercise studio offering Zumba classes
- A Buddhist Center
- Administrative offices
- Global Biotech
- Deering Lumber's Kitchen and Millwork Showroom

Once the remodeling is complete the building will accommodate the Baxter Academy for Technology and Science Charter School as the primary tenant. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. The school's vision is to convert the entire building into a full 4-year high school over the next 3 years. Total enrollment is planned for 320 students by 2015. In the opening year (2013), 160 - first and second year students are envisioned. In year two, grades 9, 10, and 11 will be provided, and in year three all four years of high school will be offered. It is estimated that 50% of the enrollment will come from Portland, South Portland and Westbrook, with the balance originating from communities outside of the urban core. School operations will run from 8:00 AM to 2:00 PM, resulting in peak traffic associated with the school being between 7-8:00 AM and 2-3:00 PM.

C. Site and Vicinity Boundaries.

A regional map showing the development site and its relationship to the adjacent street network as defined in MaineDOT's Chapter 305 of the General Rules of the Department of Transportation (Section 5B) is presented as part of the Site Plan located at the end of this submission.

D. Other Proposed Uses in the Vicinity of the Proposed Development.

A discussion with the City of Portland's Planning Department revealed that there are no other developments currently under review or have been permitted but not built in the immediate proximity of this Project at this time.

E. Trip generation.

ITE's Trip Generation, 7th Edition, was consulted to determine what industry data suggests for this type of land use. LUC 530 - High School, which includes both public and private institutions was deemed the closest category listed in this reference. Believing that the City's greatest traffic concern would be during the AM peak hour because it will coincide with the morning commute, we have focused our attention on this time period only. Using a base school population of 320 students and 30 employees, we find the following for the AM peak hour using ITE information:

<i>LUC 530</i>	<i>Variable</i>	<i>Total Trips</i>
By Student: $\ln(T) = 0.77\ln(X) + 0.69$	320 (students)	169
By Employee: Average Rate = 4.63	30 (employees)	139
Average of Above		154

The above trip generation assumes that there is no existing traffic associated with the site today, which is not entirely true. Currently there is an exercise studio (Nancy's) that offers among other things, Zumba classes, and a Buddhist Center (Nagaloka), and the administrative office for the Baxter School operating in the building. However, given the operating hours of these three uses, we have not taken a credit for any existing trips during the AM peak hour (7:00 AM to 8:00 AM).

From the data above it is estimated that the peak trip generation from the proposed development will be 154 trips (all primary), which will put the project in the Traffic Movement Permit (TMP) category of 100--200 Passenger Car Equivalent (PCE) trips.

F. Trip Distribution.

The ITE Trip Generation Handbook was also consulted with regard to the directional distribution of site generated trips during the AM peak period. ITE data suggests that during the AM peak period trips at the site will be distributed roughly 70% entering and 30% exiting regardless of which method of generation is used, i.e. "student" or "employee".

G. Trip assignment.

While ITE suggests that traffic generation from the proposed facility could be in the range of new 150 trips in the AM peak hour, the Baxter Academy plans to adopt a series of travel demand management strategies for minimizing this impact on the surrounding street system. Typically, students at four-year public high schools have a variety of transportation choices that must be addressed in the school's planning. These range from parent drop-off and pick-up, to buses, and finally to student drivers with their own cars. The actual mix usually depends on the school's location within the community and the amount of available land surrounding the school. In the case of the Baxter Academy, the setting is very urban with no surplus land for off-street parking. Accordingly, the school's administration has formulated the following Transportation Plan.

First of all, it should be understood that in the initial year of operation, only 9th and 10th graders will be attending the Baxter Academy, so student drivers will not be part of the mix. However, in subsequent years this will need to be a consideration.

Not knowing exactly what to expect from prospective student families, Baxter Academy's administration conducted a Parent Survey to gauge their transportation preferences. A total of 24 families participated in

this survey, which was performed in June of 2012. See the Appendix. The results indicated that approximately 25% of the families preferred to transport their children to and from school either by individual or car pool means. 70% envisioned using bus transportation, either charter service provided by the school or public transportation because they reside within Greater Portland. The remaining 5% indicated they would walk to and from the school. Before running any numbers on this basis, contact was made with a few similar facilities in the region – the Academy for Science and Design in Nashua, NH – a 410 student charter school serving grades 6-12 in the business district of Nashua; Waynflete in Portland, a private K-12 school with 550 students in the west end; and Portland High School on Cumberland Avenue with 920 students. Table 1 presents the results from this research.

Table 1 – Modal Choice by Families of 4-year Urban High Schools

<i>School</i>	<i>% Parent Drop-Off</i>	<i>% Transit</i>	<i>% Student Drivers</i>	<i>% Walkers & Bikers</i>
Academy for Science & Design	65	34	1	0
Portland High School	50	23	16	11
Waynflete School	50	30	14	6
Baxter Parent Survey	25	70	-	5
Percentages Used Adopted	30	50	15	5

From the data in Table 1, there is no clear model that works for all cases – each school’s experience seems to be unique depending on the school’s setting and its market service area. Therefore, the modal split percentages adopted for Baxter are a reasonable blend of parent feedback and other surveyed school’s experiences. Waynflete’s parent drop-offs appeared to be influenced by the younger population K-8. Portland High’s numbers are skewed due to the students all residing within the City where we expect many parents work. The Academy for Science & Design does not provide bus service but relies on the City’s School District for busing, the school’s setting is not conducive to walking or biking, and they only have 5 seniors with licenses at this point – hence the high percentage of parent drop-offs. Finally, the proposed Baxter Academy is planning to provide charter busing to outlying communities within its service area to reduce the need for parent shuttles.

The AM peak hour forecast below for the Baxter Academy is based on a student population of 320 and 30 employees by Year 3 and 160 students and 15 employees in Year 1:

Year 1 (2013) – 160 Students

30% parent drop-off (assumed split of 50% car pooling with 3 students and 50% single occupant): 30% X 160 students = 48 students
24 students in single occupant vehicles = 48 trips
24 students in car pools of 3 = 16 trips
55% of students (88) ride 3 charter buses (1.5 PCE per bus) = 9 trips
10% of students (16) ride existing public transportation = 0 trips
5% of students (8) as walkers = 0 trips
*15 employees @ 67% auto use = 10 trips**
Total estimated AM peak hour trips in Year 1 = 73 trips
Anticipated Distribution = 37 arriving and 36 departing

Year 3 (2015) - 320 Students

30% parent drop-off (assumed split of 50% car pooling with 3 students and 50% single occupant): 30% X 320 students = 96 students

48 students in single occupant vehicles = 96 trips

48 students in car pools of 3 = 32 trips

40% of students (128) ride 3 charter buses (1.5 PCE per bus) = 9 trips

*15% of students (48) drive their own cars = 0 trips**

10% of students (32) ride existing public transportation = 0 trips

5% of students (16) as walkers = 0 trips

*30 employees @ 67% auto use = 20 trips**

Total estimated AM peak hour trips in Year 3 = 137 trips

Anticipated Distribution = 69 arriving and 68 departing

*Because the site will not allow it, the School is not planning to provide any on-site parking for students or employees. A total of 20 off-site parking spaces are initially going to be leased for employees, but since they are off-site their impact will not be felt at the site itself.

The School is proposing a full Traffic Management Plan to organize student arrivals and departures, which is detailed later herein. From a Trip Assignment perspective, though, this Plan will route arriving school buses from York Street onto Maple Street and load/unload curbside on the west side of the street in a designated School Drop-Off Zone. The buses will depart the school using Maple Street to Commercial Street. Arriving parents dropping off their students will be directed to enter Maple Street from Commercial Street, proceed to York Street where they would make a left onto so that they can utilize the designated 8-space School Drop-Off Zone on the north side of the street opposite the front door of the school. Figure 2 illustrates the forecasted school traffic during the AM peak hour for both Years 1 and 3 at the York/Maple intersection under this traffic scenario. Figure 3 illustrates the combined (existing and forecasted) trips at the York/Maple Street intersection.

H. Traffic Analysis

Existing Conditions

Sebago staff performed a traffic turning movement count at the intersection of York Street and Maple Street during the hours of 7:00 AM and 8:00 AM on June 27, 2012 to observe traffic operations in and around the site during the time period when children would be arriving for the school day. Figure 1 presents the results of this peak hour count. It is readily evident that the volumes are quite small during this time period. Synchro/Simtraffic was used to analyze existing traffic operations at the intersection and it was determined that the Level of Service (LOS) was A.

In addition to this data collection and analysis effort, several general observations were made while conducting the count. These were:

1. Only one on-street parking space was used on Maple Street between Commercial Street and York Street during the one hour time period observed. This vehicle was a van that had parked there overnight.
2. The paved parking lot for "The Cannery" that is accessed off Maple Street only had 8 cars enter it between 7:00 AM and 8:00 AM.

3. Maple Street is barely used during this time period. Total traffic during the hour observed was 34 vehicles. Never was a queue experienced waiting to enter onto York Street, and with the exception of one overnight parked vehicle, no on-street parking spaces were occupied.
4. The two on-street parking spaces to the west of the proposed school building on York Street were not occupied during the observation period.
5. While the traffic on York Street was predominantly in-bound, it appeared that the time period 7:00 AM to 8:00 AM might be before the actual peak morning commute hour.

Year 1 - 160 Students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection's LOS in Year 1. The LOS was found to be A and only slightly less than it is today under existing conditions. Furthermore, the resulting "maximum" queues at York Street from the additional Parent Drop-Off traffic, which we assumed would occur within the 30-minute timeframe prior to school commencing at 8:00 AM was found to be 37 feet, or two cars. We modeled the parent drop-offs using appropriate peak hour factors.

Year 3 - 320 Students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection's LOS in Year 3. The LOS was found to still be A. Furthermore, the resulting "maximum" queues at York Street from the additional Parent Drop-Off traffic, which we assumed would occur within the 30-minute timeframe prior to school commencing at 8:00 AM was found to be 57 feet, or three cars. We modeled the parent drop-offs using appropriate peak hour factors.

Based on the above analysis, neither capacity nor queuing appear to be an issue during the AM on Maple Street in either Year 1 or Year 3.

I. Traffic Operations and Management Plan

Arrivals and departures from the Baxter Academy are to be well-orchestrated and staff managed events. Because students will be arriving via various modes of transportation the School's Plan will have a number of elements.

Arrivals

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. Students will depart directly onto the Maple Street sidewalk and proceed to the front door of the School on York Street.

Students using public transit (Metro or South Portland Bus) will depart buses at existing stops on the north side of York Street at either High Street or Fore Street. Students will then proceed to school on existing sidewalks and cross York Street at a repainted formal crosswalk at the intersection of York and Maple Streets. The School will have a staff person supervising this crosswalk for the 45 minutes preceding school's commencement to ensure safe student passage.

Parent drop-offs will occur on the north side of York Street opposite the front door of the school. Parents will be routed onto Commercial Street from their origins so that they can enter Maple Street heading northbound where they would make a left turn onto York Street and pull off into 8 designated drop-off

parking spaces for unloading onto the adjacent northerly sidewalk. The school will have a staff person supervising this operation to keep vehicles orderly, and students will be directed to the intersection of Maple and York, where there will be another staff member acting as a crossing guard for safe passage to the front entrance of the school.

Staff members who elect to drive their own vehicles to school will be parking in the Gulf of Maine parking lot off Commercial Street or the parking garage at the Holiday Inn By-The-Bay and will walk to school via existing sidewalks.

Students who elect to drive their own vehicles to school will likely park on Commercial Street in the unrestricted parking spaces adjacent to the end of Maple Street and will walk to school via existing sidewalks.

Students who elect to ride their bikes to school will be able to store their bikes in bike racks, which will be provided by the school adjacent to the building on Maple Street.

In addition to the staff members on-site managing the student arrival process, the school proposes to:

- use traffic cones to block off the curbside in front of the school to prevent vehicles from unloading on the south side of York Street
- place a School Crossing sign in the middle of the York Street crosswalk in front of the school to keep speeds on York Street under control during peak student usage
- erect School Drop-Off Zone signs on Maple Street for the buses on the west side of the street and on the north side of York Street for parent drop-offs.

See examples below.



"Crosswalk sign used by Waynflete"



“School Drop-Off Zone Sign at Portland High”

Departures

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. At 2:00 PM when regular school is over students who are not participating in after school programs will load the buses from the Maple Street sidewalk as they did during arrivals.

Students using public transit (Metro or South Portland Bus) will depart the school for the bus stops on the north side of York Street at either High Street or Fore Street depending on individual choice and route schedules.

Parent pick-ups will occur on the north side of York Street opposite the front door of the school. The schedule of these pick-ups will not be as concentrated as in the morning because of after school activities. The school is projecting that up to one half of students will be participating in after school programs, which would mean that parent pick-ups in Year 3 would be in the range of 32 at 2:00 PM and the other 32 would be spread over the subsequent 2 hours. Given the 8 on-street spaces available this does not appear to be an issue.

Food Deliveries

Food service to the school will be provided by an off-site vendor. Deliveries will be made curbside on the west side of Maple Street, similar to where the buses will load and unload.

Proposed Signage Changes

Maple Street: The west side of the street is currently posted as 15-minute parking. This would need to be modified to allow for school bus parking between 7:30 and 8:00 AM and between 1:45 and 2:15 PM.

York Street: The north side of York Street opposite the school is currently posted as 30-minute parking. This would need to be modified to allow for 5 minute school drop-off and pick-up between 7:30 and 8:00 AM and between 2:00 PM and 4:00 PM.

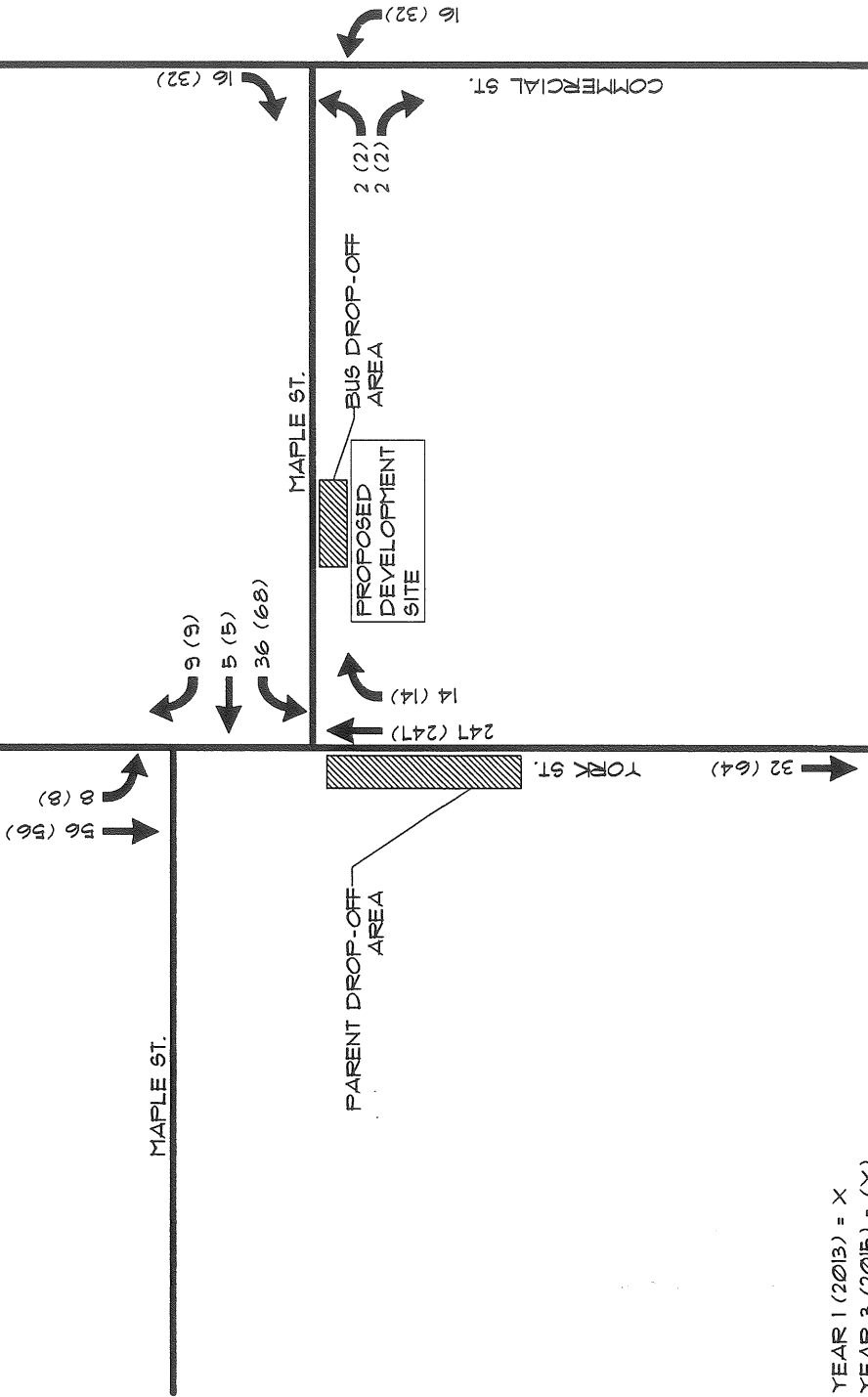
On-Going Plan Refinement

The Baxter Academy realizes that their Traffic Management Plan is subject to individual student family preferences from year to year. As such, the school administration is committed to refining their operation each year to reflect the modal choices made by their enrollees. Given that Years 1 and 2 are ramping up years in terms of the student population, the school will have adequate time to fine-tune operations prior to reaching the ultimate goal of 320 students in Year 3.

J. Parking.

From the information presented above, the number of students and employees that may require parking in the vicinity of the school is 10 staff members in Year 1 and by Year 3 this may grow to 20 staff members and 48 students. The School has made leasing arrangements for between 20-40 off-street parking spaces in the Gulf of Maine lot off Commercial Street. Other options in surrounding parking garages also exist if this is not possible. The Baxter Academy is committed to reducing the number of students driving their cars to school. In this regard the School's informational packets will encourage families to use car pooling, public transportation, school provided charter buses, walking and biking to access the facility. To further support this policy, the school is prepared to offer an incentive to students in the form of a 50-cent subsidy on public transit fares for those students within the Metro/South Portland Bus network.

For those students in Years 2 and beyond that do not choose to avail themselves of public transportation, and want to bring their own vehicles to school, they will be required to register their vehicles with the school and park legally within the surrounding area. This is similar to Waynflete's policy. The school will monitor these situations, and work with the City to address any violators should they be brought to the school's attention.



SCALE:	NTS
DATE:	9/26/12
FIGURE:	3

FOR: RUFUS DEERING LUMBER CO.
383 COMMERCIAL ST.
PORTLAND, ME

LOCATION:
YORK ST. @ MAPLE ST.
PORTLAND, ME

SEBAGO
TECHNICS

WWW.SEAGOTECHNICS.COM
75 John Roberts Rd. - Suite 1A
South Portland, ME 04240
Tel. 207-200-2100

250 Goddard Rd. - Suite B
Lewiston, ME 04240
Tel. 207-783-6656

Section 2. Traffic Accidents

MaineDOT's Accident Records Unit was contacted to obtain the most recent 3-year crash information for the area in the vicinity of the proposed project. See attached materials from the Department in the Appendix. In summary, of the 12 intersections surveyed, only one is defined as a High Crash Location, i.e. a total of 8 crashes in three years and a Critical Rate Factor (CRF) greater than 1.0. This intersection is Commercial Street at High Street with a CRF of 1.50.

Road segments were also examined as evidenced by the enclosed Department data, and no segment immediately abutting the proposed project is identified as a High Crash Location.

Section 3. Development Entrances and Exits

The proposed development is to occur at 54 York Street – the corner of York and Maple Streets. The main walk-in entrance to the school will be located on York Street with Maple Street used as the curbside location for school bus drop-offs and pick-ups, as well as food service deliveries. See the proposed Site Plan in the Appendix.

Section 4. Title, Right or Interest

Please see the following Deed and supporting documents.

SURVEYOR'S REPORT

Fill out and sign with all Surveys in accordance with the Instructions on the reverse side.

To: **LAWYERS TITLE INSURANCE CORPORATION**

Richmond, Virginia.

THIS IS TO CERTIFY, that on December 3, 1987, I made an accurate survey of the premises standing in the name of William J. Dowd

situated at Portland Cumberland Maine
City County State

briefly described as: Commercial Structure
 and shown on the accompanying survey entitled: Standard Boundary Survey for William J. Dowd

I made a careful inspection of said premises and of the buildings located thereon at the time of making such survey, and again on December 17, 1987, and at the time of such latter inspection I found William J. Dowd to be in possession of said premises as owner
(tenant) or (owner)

I further certify as to the existence or non-existence of the following at the time of my last inspection:

1. Rights of way, old highways, or abandoned roads, lanes or driveways, drains, sewer, water, gas or oil pipe lines across said premises (Include any such matters shown on the recorded plat of subdivision):
15 foot Right of Way to land now or formerly of Rufus Deering
Location of sewer connection uncertain

2. Springs, streams, rivers, ponds, or lakes located, bordering on or running through said premises: None observed

3. Cemeteries or family burying grounds located on said premises. (Show location on plat): None observed

4. Telephone, telegraph or electric power poles, wires or lines located on, under, overhanging or crossing said premises and serving other property or properties: None observed above ground below grade lines uncertain

5. Joint driveways or walkways; party walls or rights of support; porches, steps or roofs used in common or joint garages: 15 foot strip above mentioned apparently owned by Dowd subject to use by abutter.

6. Disputed boundaries, encroachments or overhanging projections. (If the buildings, projections or cornices thereof, or signs affixed thereto, fences or other indications of occupancy encroach upon or overhang adjoining properties or easement areas, or the like encroach upon or overhang surveyed premises, specify all such):
Encroachment of building onto 15 foot Right of Way, Sign overhangs York Street, Fire Escapes overhang Maple Street, Fence of Abutter encroaches on said property.

7. Physical evidence of boundary lines on all sides. (Be specific): Iron pins found at two corners (see plan)

8. Is the property improved? Yes
 (a) Building is: Brick (X); Clapboard (); other ()
 (b) Building is: One story (); Two story (X); split-level (); other ()
Specify Specify

9. Indications of building construction, alterations or repairs within recent months: Exterior brick cleaned, repainted, new doors
 (a) If new improvements under construction, how far have they progressed? 50 per cent completed more or less

10. Changes in street lines either completed or officially proposed: City in progress of determining York Streetline
 (a) Are there indications of recent street or sidewalk construction or repairs? None

11. Are all abutting streets or roads maintained by public authorities? Yes
 (a) Is access to such streets or roads limited? NO

12. If the surveyed premises are subject to restrictive covenants, do the improvements, use and occupancy comply with such? (If the premises are subject to restrictive covenants, have the examining attorney furnish you verbatim copy of them). Note: Origination Deed 1955/24 requires Grantee to build and maintain fence. Existing fence appears to have been constructed by abutter.

William A. Crossfield Jr.
 Civil Engineer of Surveyor

NOTE: In all cases where there are encroachments, easements, party walls, etc., they should also be denoted upon the map of your survey. Also, be certain map complies with Instructions on reverse side. Particular attention is directed to Paragraph 3 of these instructions.

(Be sure to answer each item. If the property is not subject to any such of items 1 to 8 inclusive, insert the word "none" following the item.)

BEAGLE, PEARCE & RIDGE, LLC
ATTORNEYS AT LAW

C. ALAN BEAGLE
MICHAEL J. PEARCE
MARTIN J. RIDGE
JACQUELYN M. KURZ
PAULA M. CRAIGHEAD

24 CITY CENTER
P.O. BOX 7044
PORTLAND, MAINE 04112
(207) 773-1751
FAX (207) 775-3382

November 30, 1998

Mr. William M. Moody, Jr., President
Rufus Deering Company
383 Commercial Street
P.O. Box 880
Portland, ME 04112-0880

Sally Brown
302

Re: Rufus Deering Company/Peoples Heritage Bank/York
Street, Inc.

Dear Bill:

Enclosed please find the following original documents:

1. Warranty Deed from York Street, Inc. to Rufus Deering Company. This Warranty Deed has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 13.
2. Assignment of Leases and Security Deposits from York Street, Inc. to Rufus Deering Company. This Assignment has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 36.

If you have any questions, please feel free to contact me.

Very truly yours,

Robyn A. Pearce

Robyn A. Pearce
Assistant to Michael J. Pearce

enclosure

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.
By: Werner Schnetzer
Werner Schnetzer
Its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,
Paul S. Bunker
Notary Public/Attorney-at-Law
Print Name: Paul S. Bunker

122895/PSB/SKLF
91401.4926

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4: 48

CUMBERLAND COUNTY

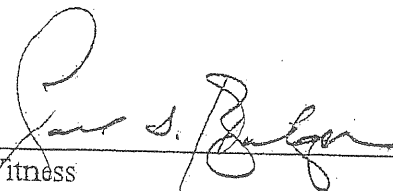
John B O'Brien

61274

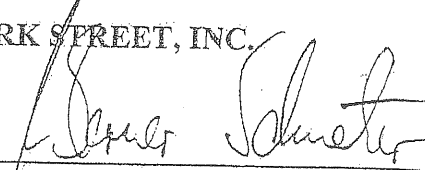
ASSIGNMENT OF LEASES AND SECURITY DEPOSITS

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04106, for consideration, does hereby ASSIGN to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, its successors and assigns, all its right, title and interest in and to any and all leases, subleases and/or tenancies of the premises located on or about 54-62 York Street in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto, including, without limitation, any and all tenant security deposits relating thereto.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by Werner Schnetzer, its duly authorized President, on this 1st day of September, 1998.



Witness

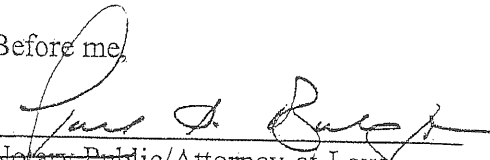
YORK STREET, INC.
By: 

Werner Schnetzer
Its: President

STATE OF MAINE
CUMBERLAND, SS.

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in said capacity and the free act and deed of said YORK STREET, INC.

Before me,


Notary Public/Attorney-at-Law
Print Name: Paul S. Bussler

122937/PSB/SKLF
91401.4926

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4:52

CUMBERLAND COUNTY

John B. O'Brien

Section 5. Public or private Rights-of-Way

There is a 15' wide Right-of-Way across the southerly boundary of this parcel for a passageway in common with Portland Terminal Company. See the Existing Conditions Survey Plan in the Appendix.

Section 6. Schedule

Building remodeling is planned to begin in the fall of 2012.

Maple Street @ York Street, Portland ME
Existing Conditions

9/24/2012

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Delay / Veh (s)	0.5	0.3	2.7	0.2	6.2	6.5	2.7	6.1	0.8
Total Stops	0	0	2	0	3	6	7	12	30
Stop/Veh	0.00	0.00	0.40	0.00	1.00	1.00	1.00	1.00	0.08
Vehicles Entered	245	11	5	64	3	6	7	12	353

Total Network Performance

Total Delay (hr)	0.1
Delay / Veh (s)	1.1
Total Stops	30
Stop/Veh	0.08
Vehicles Entered	353

Maple Street @ York Street, Portland ME
Existing Conditions

9/24/2012

Intersection: 1: York Street & Maple Street

Movement	WB	NB	SB
Directions Served	LT	LTR	LTR
Maximum Queue (ft)	18	24	34
Average Queue (ft)	2	6	9
95th Queue (ft)	13	19	32
Link Distance (ft)	422	345	186
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Network Summary

Network wide Queuing Penalty: 0

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.0	0.0	0.1
Delay / Veh (s)	0.6	0.4	3.0	0.2	5.9	8.6	3.3	6.1	1.4
Total Stops	0	0	2	0	35	3	10	13	63
Stop/Veh	0.00	0.00	0.33	0.00	1.00	1.00	1.00	1.00	0.16
Vehicles Entered	242	17	6	59	36	3	10	13	386

Total Network Performance

Total Delay (hr)	0.2
Delay / Veh (s)	1.7
Total Stops	63
Stop/Veh	0.16
Vehicles Entered	386

Intersection: 1: York Street & Maple Street

Movement	WB	NB	SB
Directions Served	LT	LTR	LTR
Maximum Queue (ft)	27	37	34
Average Queue (ft)	2	13	9
95th Queue (ft)	16	30	32
Link Distance (ft)	422	345	186
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Network Summary

Network wide Queuing Penalty: 0

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.0	0.0	0.2
Delay / Veh (s)	0.5	0.3	2.9	0.2	6.6	8.4	4.2	6.6	1.9
Total Stops	0	0	2	0	70	6	11	13	102
Stop/Veh	0.00	0.00	0.33	0.00	1.00	1.00	1.00	1.00	0.24
Vehicles Entered	241	16	6	56	70	6	11	13	419

Total Network Performance

Total Delay (hr)	0.3
Delay / Veh (s)	2.4
Total Stops	102
Stop/Veh	0.24
Vehicles Entered	419

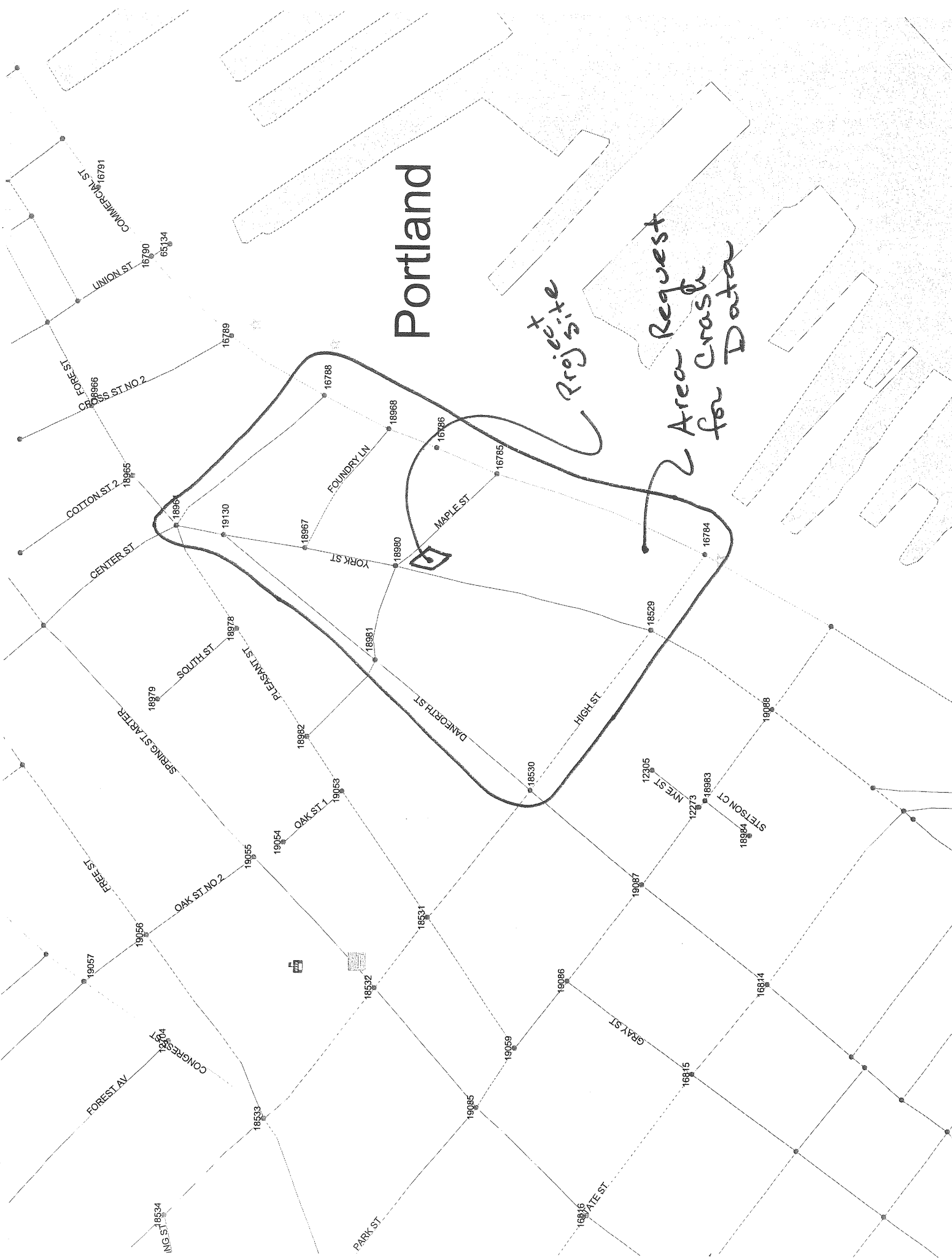
Intersection: 1: York Street & Maple Street

Movement	WB	NB	SB
Directions Served	LT	LTR	LTR
Maximum Queue (ft)	18	57	39
Average Queue (ft)	2	20	10
95th Queue (ft)	12	41	34
Link Distance (ft)	422	345	186
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Network Summary

Network wide Queuing Penalty: 0

Portland



Project Site

Area Request for Crash Data

Crash Summary Report

Report Selections and Input Parameters

REPORT SELECTIONS

Crash Summary I Section Detail Crash Summary II 1320 Included 1320 & Driver Report Included

REPORT DESCRIPTION

York St area

REPORT PARAMETERS

Year 2009, Start Month 1 through Year 2011 End Month: 12

Route: 0560122 Start Node: 16788 End Node: 18964 Start Offset: 0 End Offset: 0 Exclude First Node Exclude Last Node

Route: 3200508 Start Node: 18964 End Node: 18529 Start Offset: 0 End Offset: 0 Exclude First Node Exclude Last Node

Route: 0001A Start Node: 16784 End Node: 16788 Start Offset: 0 End Offset: 0 Exclude First Node Exclude Last Node

Route: 0560365 Start Node: 16784 End Node: 18529 Start Offset: 0 End Offset: 0 Exclude First Node Exclude Last Node

Route: 0077W Start Node: 18529 End Node: 18530 Start Offset: 0 End Offset: 0 Exclude First Node Exclude Last Node

Route: 0560191 Start Node: 19130 End Node: 18530 Start Offset: 0 End Offset: 0 Exclude First Node Exclude Last Node

Route: 0560475 Start Node: 18980 End Node: 18981 Start Offset: 0 End Offset: 0 Exclude First Node Exclude Last Node

Route: 0560475 Start Node: 16785 End Node: 18980 Start Offset: 0 End Offset: 0 Exclude First Node Exclude Last Node

Route: 0560291 Start Node: 18967 End Node: 18968 Start Offset: 0 End Offset: 0 Exclude First Node Exclude Last Node

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary I

Node	Route - MP	Node Description	U/R	Nodes							Percent Annual M Ent-Veh	Crash Rate	Critical Rate	CRF
				Total Crashes	K	A	B	C	PD	Injury				
16788	0560122 - 0	Int of CENTER ST, COMMERCIAL ST	9	8	0	0	1	1	6	25.0	5.645	0.47	1.08	0.00
											Statewide Crash Rate: 0.62			
18964	0560122 - 0.10	Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST	9	3	0	0	0	1	2	33.3	3.777	0.26	1.18	0.00
											Statewide Crash Rate: 0.62			
19130	3200508 - 0.02	Int of DANFORTH ST YORK ST	9	1	0	0	0	0	1	0.0	2.346	0.14	1.31	0.00
											Statewide Crash Rate: 0.62			
18967	3200508 - 0.06	0509387 POR,YORK,FOUNDRY LA.	2	0	0	0	0	0	0	0.0	0.930	0.00	0.51	0.00
											Statewide Crash Rate: 0.13			
18980	3200508 - 0.10	0509400 POR,MAPLE,YORK ST.	2	0	0	0	0	0	0	0.0	1.849	0.00	0.43	0.00
											Statewide Crash Rate: 0.13			
16784	0001A - 11.15	Int of COMMERCIAL ST HIGH ST	2	8	0	0	1	2	5	37.5	5.816	0.46	0.30	1.50
											Statewide Crash Rate: 0.12			
16785	0001A - 11.27	Int of COMMERCIAL ST MAPLE ST	2	2	0	0	0	1	1	50.0	5.507	0.12	0.31	0.00
											Statewide Crash Rate: 0.12			
16786	0001A - 11.30	Non Int COMMERCIAL ST	2	0	0	0	0	0	0	0.0	5.382	0.00	0.31	0.00
											Statewide Crash Rate: 0.12			
18968	0001A - 11.33	Int of COMMERCIAL ST FOUNDRY LN	2	0	0	0	0	0	0	0.0	5.466	0.00	0.31	0.00
											Statewide Crash Rate: 0.12			
18529	0077W - 1.39	Int of HIGH ST YORK ST	9	10	0	0	0	3	6	33.3	3.377	0.99	1.21	0.00
											Statewide Crash Rate: 0.62			
18981	0560191 - 0.10	Int of DANFORTH ST MAPLE ST	2	0	0	0	0	0	0	0.0	0.877	0.00	0.45	0.00
											Statewide Crash Rate: 0.11			
18530	0560191 - 0.20	Int of DANFORTH ST HIGH ST	9	18	0	0	1	5	12	33.3	6.348	0.95	1.06	0.00
											Statewide Crash Rate: 0.62			
Study Years: 3.00				50	0	0	3	13	33	32.0	47.320	0.35	0.47	0.75
NODE TOTALS:				50	0	0	3	13	33	32.0	47.320	0.35	0.47	0.75

Crash Summary I

Sections

Start Node	End Node	Element	Offset Begin - End	Route - MP	Section U/R Length	Total Crashes	K	Injury Crashes			PD	Percent Injury	Annual HMVM	Crash Rate	Critical Rate	CRF
								A	B	C						
16788	18964	192464	0 - 0.10	0560122 - 0 RD INV 05 60122	0.10	2	1	0	0	1	0	100.0	0.00121	275.07	982.08	0.00
		Int of CENTER ST, COMMERCIAL ST												Statewide Crash Rate: 336.22		
18529	18980	194048	0 - 0.14	3200508 - 0.04 RD INV 3200508	0.14	2	2	0	0	2	0	100.0	0.00243	274.26	820.78	0.00
		Int of HIGH ST, YORK ST												Statewide Crash Rate: 336.22		
18964	19130	3106830	0 - 0.02	3200508 - 0 RD INV 3200508	0.02	2	1	0	0	0	1	0.0	0.00046	721.13	607.32	1.19
		Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST												Statewide Crash Rate: 142.40		
18967	19130	194651	0 - 0.04	3200508 - 0.02 RD INV 3200508	0.04	2	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00
		0509387 POR, YORK, FOUNDRY LA.												Statewide Crash Rate: 336.22		
18967	18980	194650	0 - 0.04	3200508 - 0.06 RD INV 3200508	0.04	2	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00
		0509387 POR, YORK, FOUNDRY LA.												Statewide Crash Rate: 336.22		
16784	16785	3116402	0 - 0.12	0001A - 11.15 US 1A	0.12	2	6	0	0	1	5	16.7	0.00645	309.87	388.11	0.00
		Int of COMMERCIAL ST, HIGH ST												Statewide Crash Rate: 171.50		
16785	16786	3130488	0 - 0.03	0001A - 11.27 US 1A	0.03	2	3	0	0	1	2	33.3	0.00163	614.36	551.86	1.11
		Int of COMMERCIAL ST, MAPLE ST												Statewide Crash Rate: 171.50		
16788	18968	3119256	0 - 0.04	0001A - 11.29 US 1A	0.04	2	1	0	0	0	1	0.0	0.00216	154.31	513.40	0.00
		Int of CENTER ST, COMMERCIAL ST												Statewide Crash Rate: 171.50		
16786	18968	3118839	0 - 0.03	0001A - 11.30 US 1A	0.03	2	1	0	0	0	1	0.0	0.00160	208.12	554.11	0.00
		Non Int COMMERCIAL ST												Statewide Crash Rate: 171.50		
16784	18529	3115419	0 - 0.05	0560365 - 0 RD INV 05 60365	0.05	2	0	0	0	0	0	0.0	0.00063	0.00	683.39	0.00
		Int of COMMERCIAL ST, HIGH ST												Statewide Crash Rate: 171.50		
18529	18530	3106683	0 - 0.10	0077W - 1.39 ST RTE 77W	0.10	2	10	0	0	0	10	0.0	0.00383	870.25	446.50	1.95
		Int of HIGH ST, YORK ST												Statewide Crash Rate: 173.49		
18981	19130	3106832	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	2	0	0	0	2	0.0	0.00071	935.70	573.37	1.63
		Int of DANFORTH ST, MAPLE ST												Statewide Crash Rate: 142.40		
18530	18981	3106686	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	4	0	0	0	4	0.0	0.00083	1609.24	557.81	2.88
		Int of DANFORTH ST, HIGH ST												Statewide Crash Rate: 142.40		
18980	18981	194666	0 - 0.05	0560475 - 0.07 RD INV 05 60475	0.05	2	0	0	0	0	0	0.0	0.00004	0.00	558.41	0.00
		0509400 POR, MAPLE, YORK ST.												Statewide Crash Rate: 336.22		
16785	18980	192461	0 - 0.07	0560475 - 0 RD INV 05 60475	0.07	2	2	0	0	0	2	0.0	0.00015	4537.85	1451.68	3.13
		Int of COMMERCIAL ST, MAPLE ST												Statewide Crash Rate: 336.22		
18967	18968	194649	0 - 0.07	0560291 - 0 RD INV 05 60291	0.07	2	0	0	0	0	0	0.0	0.00013	0.00	1448.34	0.00
		0509387 POR, YORK, FOUNDRY LA.												Statewide Crash Rate: 336.22		
Study Years:	3.00				1.10	33	0	0	0	1	4	28	0.02360	466.01	338.32	1.38
Grand Totals:					1.10	83	0	0	0	4	17	61	0.02360	1172.09	461.67	2.54

Crash Summary

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	Injury Crashes				Crash Report	Crash Date	Crash Mile Point	Injury Degree	
						K	A	B	C					PD
16788	18964	192464	0 - 0.10	0560122 - 0	1	0	0	0	1	0	2010-5651C	03/14/2010	0.01	C
18964	19130	3106830	0 - 0.02	3200508 - 0	1	0	0	0	0	1	2011-7755C	04/15/2011	0.01	PD
18967	19130	194651	0 - 0.04	3200508 - 0.02	0	0	0	0	0	0				
18967	18980	194650	0 - 0.04	3200508 - 0.06	0	0	0	0	0	0				
18529	18980	194048	0 - 0.14	3200508 - 0.10	2	0	0	0	2	0	2010-3489C	02/06/2010	0.11	C
											2011-9159	08/31/2011	0.20	C
16784	16785	3116402	0 - 0.12	0001A - 11.15	6	0	0	0	1	5	2011-8884	08/28/2011	11.16	C
											2011-6597	08/03/2011	11.16	PD
											2009-32880C	12/29/2009	11.16	PD
											2009-11594C	05/22/2009	11.17	PD
											2010-14757C	07/20/2010	11.17	PD
											2010-12965C	05/31/2010	11.18	PD
16785	16786	3130488	0 - 0.03	0001A - 11.27	3	0	0	1	0	2	2010-30463C	12/26/2010	11.28	B
											2009-10518C	05/13/2009	11.28	PD
											2010-15225C	06/21/2010	11.28	PD
16786	18968	3118839	0 - 0.03	0001A - 11.30	1	0	0	0	0	1	2009-19621C	08/31/2009	11.31	PD
16788	18968	3119256	0 - 0.04	0001A - 11.33	1	0	0	0	0	1	2010-14622C	07/13/2010	11.36	PD
16784	18529	3115419	0 - 0.05	0560365 - 0	0	0	0	0	0	0				
18529	18530	3106683	0 - 0.10	0077W - 1.39	10	0	0	0	0	10	2009-6432C	03/04/2009	1.40	PD
											2011-1699C	01/30/2011	1.40	PD
											2011-3653	06/29/2011	1.41	PD
											2010-9558C	05/05/2010	1.43	PD
											2011-16796	11/28/2011	1.45	PD
											2009-1917C	01/09/2009	1.45	PD
											2011-5217	07/19/2011	1.46	PD
											2009-18444C	08/16/2009	1.48	PD
											2011-710C	01/07/2011	1.48	PD
											2010-14209C	07/10/2010	1.48	PD
18981	19130	3106832	0 - 0.10	0560191 - 0	2	0	0	0	0	2	2009-6337C	02/20/2009	0.01	PD
											2010-16740C	08/11/2010	0.02	PD
18530	18981	3106686	0 - 0.10	0560191 - 0.10	4	0	0	0	0	4	2011-5063	07/15/2011	0.11	PD
											2010-24444C	11/05/2010	0.15	PD
											2010-1886C	01/26/2010	0.15	PD
											2010-26329C	11/30/2010	0.19	PD

Crash Summary

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	Injury Crashes			Crash Report	Crash Date	Crash Mile Point	Injury Degree	
						K	A	B					C
18980	18981	194666	0 - 0.05	0560475 - 0.07	0	0	0	0	0				
16785	18980	192461	0 - 0.07	0560475 - 0	2	0	0	0	2	2010-9693C	05/14/2010	0.04	PD
18967	18968	194649	0 - 0.07	0560291 - 0	0	0	0	0	0	2011-2927C	02/04/2011	0.06	PD
Totals:					33	0	0	1	4	28			

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Day and Hour

Day Of Week	Hour of Day												Un	Tot												
	AM						PM																			
	12	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6	7	8	9	10	11		
SUNDAY	1	3	0	1	0	0	0	1	0	1	2	0	0	0	0	2	1	0	0	0	0	2	0	0	0	14
MONDAY	0	0	0	0	0	0	0	0	1	0	2	2	0	1	2	1	2	2	0	0	0	1	0	0	0	14
TUESDAY	0	1	0	0	0	0	0	1	1	1	0	1	1	0	0	2	0	1	0	0	1	0	0	0	0	10
WEDNESDAY	0	0	0	0	0	0	0	0	1	1	1	0	2	1	0	0	3	1	2	0	0	0	0	0	0	12
THURSDAY	0	0	0	0	0	0	0	1	1	0	1	0	2	0	0	0	1	1	0	1	0	0	0	0	0	8
FRIDAY	1	0	0	0	0	0	0	2	0	1	1	2	2	2	1	2	2	2	0	2	0	0	0	0	0	20
SATURDAY	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1	1	0	0	1	0	0	0	0	5
Totals	2	4	0	1	0	0	0	5	4	4	7	5	7	4	3	9	10	8	2	3	3	2	0	0	83	

Vehicle Counts by Type

Unit Type	Total	Unit Type	Total
1-Passenger Car	103	23-Bicyclist	2
2-(Sport) Utility Vehicle	30	24-Witness	13
3-Passenger Van	13	25-Other	3
4-Cargo Van (10K lbs or Less)	0	Total	193
5-Pickup	20		
6-Motor Home	0		
7-School Bus	0		
8-Transit Bus	0		
9-Motor Coach	0		
10-Other Bus	0		
11-Motorcycle	1		
12-Moped	0		
13-Low Speed Vehicle	0		
14-Autocycle	0		
15-Experimental	0		
16-Other Light Trucks (10,000 lbs or Less)	0		
17-Medium/Heavy Trucks (More than 10,000 lbs)	6		
18-ATV - (4 wheel)	0		
20-ATV - (2 wheel)	0		
21-Snowmobile	0		
22-Pedestrian	2		

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Driver Action at Time of Crash

Driver Action at Time of Crash	Dr 1	Dr 2	Dr 3	Dr 4	Dr 5	Other	Total
No Contributing Action	9	5	1	0	0	0	15
Ran Off Roadway	1	0	0	0	0	0	1
Failed to Yield Right-of-Way	8	9	0	0	0	0	17
Ran Red Light	1	1	0	0	0	0	2
Ran Stop Sign	0	0	0	0	0	0	0
Disregarded Other Traffic Sign	0	0	0	0	0	0	0
Disregarded Other Road Markings	0	0	0	0	0	0	0
Exceeded Posted Speed Limit	1	2	1	0	0	0	4
Drove Too Fast For Conditions	0	0	0	0	0	0	0
Improper Turn	3	2	0	0	0	0	5
Improper Backing	2	1	0	0	0	0	3
Improper Passing	1	2	0	0	0	0	3
Wrong Way	0	0	0	0	0	0	0
Followed Too Closely	4	7	1	0	0	0	12
Failed to Keep in Proper Lane	1	0	0	0	0	0	1
Operated Motor Vehicle in Erratic, Reckless, Careless, Negligent or Aggressive Manner	0	0	0	0	0	0	0
Swerved or Avoided Due to Wind, Slippery Surface, Motor Vehicle, Object, Non-Motorist in Roadway	0	0	0	0	0	0	0
Over-Correcting/Over-Steering	0	0	0	0	0	0	0
Other Contributing Action	0	1	0	0	0	0	1
Unknown	0	1	0	0	0	0	1
Total	31	31	3	0	0	0	65

Crashes by Apparent Physical Condition And Driver

Apparent Physical Condition	Dr 1	Dr 2	Dr 3	Dr 4	Dr 5	Other	Total
Apparently Normal	72	72	10	1	0	2	157
Physically Impaired or Handicapped	0	0	0	0	0	0	0
Emotional(Depressed, Angry, Disturbed, etc.)	0	0	0	0	0	0	0
Ill (Sick)	0	0	0	0	0	0	0
Asleep or Fatigued	0	0	0	1	0	0	1
Under the Influence of Medications/Drugs/Alcohol	3	3	0	0	0	0	6
Other	1	2	0	0	0	0	3
Total	76	77	10	2	0	2	167

Driver Age by Unit Type

Age	Driver	Bicycle	SnowMobile	Pedestrian	ATV	Total
09-Under	0	0	0	0	0	0
10-14	0	0	0	0	0	0
15-19	14	0	0	0	0	14
20-24	39	0	0	0	0	39
25-29	22	0	0	0	0	22
30-39	26	0	0	0	0	26
40-49	18	0	0	0	0	18
50-59	28	0	0	0	0	28
60-69	14	0	0	0	0	14
70-79	5	0	0	0	0	5
80-Over	3	0	0	0	0	3
Unknown	7	2	0	2	0	11
Total	176	2	0	2	0	180

Crash Summary II - Characteristics

Most Harmful Event		
Most Harmful Event	Total	Total
1-Overturn / Rollover	0	0
2-Fire / Explosion	0	3
3-Immersion	0	0
4-Jackknife	0	0
5-Cargo / Equipment Loss Or Shift	0	31
6-Fell / Jumped from Motor Vehicle	0	
7-Thrown or Falling Object	0	
8-Other Non-Collision	0	
9-Pedestrian	1	
10-Pedalcycle	0	
11-Railway Vehicle - Train, Engine	0	
12-Animal	0	
13-Motor Vehicle in Transport	24	
14-Parked Motor Vehicle	2	
15-Struck by Falling, Shifting Cargo or Anything Set in Motion by Motor Vehicle	0	
16-Work Zone / Maintenance Equipment	0	
17-Other Non-Fixed Object	0	41
18-Impact Attenuator / Crash Cushion	0	2
19-Bridge Overhead Structure	0	0
20-Bridge Pier or Support	0	0
21-Bridge Rail	0	8
22-Cable Barrier	0	0
23-Culvert	0	0
24-Curb	0	0
25-Ditch	0	0
26-Embankment	0	0
27-Guardrail Face	0	0
28-Guardrail End	0	0
29-Concrete Traffic Barrier	0	28
30-Other Traffic Barrier	0	3
31-Tree (Standing)	0	
32-Utility Pole / Light Support	0	
33-Traffic Sign Support	0	
34-Traffic Signal Support	1	
35-Fence	0	
36-Mailbox	0	
37-Other Post Pole or Support	0	
Total		82

Injury Data		
Severity Code	Injury Crashes	Number Of Injuries
K	0	0
A	0	0
B	4	6
C	17	23
PD	61	0
Total	82	29

Road Character		Total
Road Grade		
1-Level		39
2-On Grade		41
3-Top of Hill		2
4-Bottom of Hill		1
5-Other		0
Total		83

Traffic Control Devices		Total
Traffic Control Device		
1-Traffic Signals (Stop & Go)		41
2-Traffic Signals (Flashing)		2
3-Advisory/Warning Sign		0
4-Stop Signs - All Approaches		0
5-Stop Signs - Other		8
6-Yield Sign		0
7-Curve Warning Sign		0
8-Officer, Flagman, School Patrol		0
9-School Bus Stop Arm		0
10-School Zone Sign		0
11-R.R. Crossing Device		0
12-No Passing Zone		0
13-None		28
14-Other		3
Total		82

Light		Total
Light Condition		
1-Daylight		57
2-Dawn		1
3-Dusk		4
4-Dark - Lighted		21
5-Dark - Not Lighted		0
6-Dark - Unknown Lighting		0
7-Unknown		0
Total		83

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Year and Month

Month	2009	2010	2011	Total
JANUARY	3	1	2	6
FEBRUARY	3	2	4	9
MARCH	1	4	0	5
APRIL	2	3	3	8
MAY	3	5	2	10
JUNE	1	2	2	5
JULY	2	4	6	12
AUGUST	4	1	5	10
SEPTEMBER	1	0	0	1
OCTOBER	2	4	0	6
NOVEMBER	1	3	4	8
DECEMBER	2	1	0	3
Total	25	30	28	83

Report is limited to the last 10 years of data.

Crash Summary II - Characteristics

Crashes by Crash Type and Type of Location

Crash Type	Straight Road	Curved Road	Three Leg Intersection	Four Leg Intersection	Five or More Leg Intersection	Driveways	Bridges	Interchanges	Other	Parking Lot	Private Way	Cross Over	Railroad Crossing	Total
Object in Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rear End / Sideswipe	26	0	12	15	0	2	0	0	0	0	0	0	0	55
Head-on / Sideswipe	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Intersection Movement	0	0	2	13	1	5	0	0	0	0	0	0	0	21
Pedestrians	0	0	2	0	0	0	0	0	0	0	0	0	0	2
Train	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Went Off Road	0	0	0	2	1	0	0	0	0	0	0	0	0	3
All Other Animal	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bicycle	0	0	1	1	0	0	0	0	0	0	0	0	0	2
Other	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Jackknife	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rollover	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Fire	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Submersion	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Thrown or Falling Object	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bear	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Deer	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Moose	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Turkey	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	26	0	17	31	2	7	0	0	0	0	0	0	0	83

Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Blowing Sand, Soil, Dirt												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Blowing Snow												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Clear												
Dark - Lighted	13	0	0	0	0	0	0	2	0	0	0	15
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	36	1	0	0	0	0	0	0	0	0	1	38
Dusk	2	0	0	0	0	0	0	0	0	0	0	2
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Cloudy												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	8	0	0	0	0	0	0	0	0	0	3	11
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Fog, Smog, Smoke												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	1	0	0	0	0	0	0	0	0	0	0	1
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Other												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Rain												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	4	4
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	1	1
Daylight	0	0	0	0	0	0	0	0	0	0	5	5
Dusk	0	0	0	0	0	0	0	0	0	0	1	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Severe Crosswinds												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Sleet, Hail (Freezing Rain or Drizzle)												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Snow												
Dark - Lighted	0	0	0	0	0	0	0	1	0	0	1	2
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	2	0	0	0	2
Dusk	0	0	0	0	0	0	0	1	0	0	0	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL	60	1	0	0	0	0	0	6	0	0	0	83

You have a BASIC account | To remove the limits of a BASIC account and get unlimited questions, upgrade now!

Parent Survey June 22

Education

Design Survey Collect Responses **Analyze Results**

- [View Summary](#)
- [Browse Responses](#)
- [Filter Responses](#)
- [Crosstab Responses](#)
- [Download Responses](#)
- [Share Responses](#)

Default Report [Add Report](#)

Response Summary

Total Started Survey: 26
Total Finished Survey: 26 (100%)

PAGE: 1

1. What is your preferred method of transportation to school? [Create Chart](#) [Download](#)

	Response Percent	Response Count
walking	3.8%	1
bicycle	0.0%	0
public transportation	11.5%	3
parent ride	19.2%	5
parent carpool	11.5%	3
school bus	53.8%	14
answered question		26
skipped question		0

2. Would you prefer a parent carpool if Baxter Academy were to help facilitate? [Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	52.0%	13
no	48.0%	12
answered question		25
skipped question		1

3. Would you be willing to drive as part of a carpool? [Create Chart](#) [Download](#)

	Response Percent	Response Count
--	------------------	----------------

SurveyMonkey – Survey Results

yes	70.8%	17
no	29.2%	7
answered question		24
skipped question		2

4. Would you be willing to pay a monthly bus fee? [Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	80.8%	21
no	19.2%	5
answered question		26
skipped question		0

5. Would you be picking your child up from school at dismissal at 2pm? [Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	36.0%	9
no	64.0%	16
answered question		25
skipped question		1

6. Would you want your child to ride a school bus back to your hometown? (outside of Portland only) [Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	65.0%	13
no	35.0%	7
answered question		20
skipped question		6

7. Would you be picking up your child later in the day from the Portland Public Library, or from some other downtown location? [Create Chart](#) [Download](#)

	Response Percent	Response Count
--	------------------	----------------

SurveyMonkey – Survey Results

yes	36.0%	9
no	64.0%	16

answered question 25

skipped question 1

Follow Us: [Facebook](#) • [Twitter](#) • [LinkedIn](#) • [Our Blog](#)

Help: [Tutorials](#) • [Answers & FAQs](#) • [Contact Support](#)

About Us: [Management Team](#) • [Board of Directors](#) • [Partners](#) • [Newsroom](#) • [Contact Us](#) • [We're Hiring](#) • [Sitemap](#)

Policies: [Terms of Use](#) • [Privacy Policy](#) • [Anti-Spam Policy](#) • [Security Statement](#) • [Email Opt-Out](#)

[Dansk](#) • [Deutsch](#) • [English](#) • [Español](#) • [Français](#) • [한국어](#) • [Italiano](#) • [Nederlands](#) • [日本語](#) • [Norsk](#) • [Português](#) • [Русский](#) • [Suomi](#) • [Svenska](#) • [中文\(繁體\)](#)

Copyright © 1999-2012 SurveyMonkey

Memorandum
Department of Planning and Urban Development
Planning Division



To: Phil DiPierro- Development Review Coordinator
Assessor's Office
Marge Schmuckal- Zoning Administrator
Rhonda Zazzara, Public Services
Bill Clark- Public Services

From: Shukria Wiar, Planner *-Gave 3 copies to Phil*

Date: August 20, 2013
RE: Level III Site Plan Review and Traffic Movement Permit
CBL: 042 A007001
App #: 2012-519
Project Address: 54 York Street

The attached are the final approved plans for the Baxter Academy at 54 York Street, approved January 22, 2013.

If you have any questions, please contact me.

Thanks.

SEBAGO TECHNICS, INC.
 75 John Roberts Road
 Suite 1A
 South Portland, ME 04106-6963

LETTER OF TRANSMITTAL

Phone (207) 200-2100 **FAX** (207) 856-2206

DATE: 08-20-13	STI Project 07383
ATTENTION: Shukria Wiar	
RE: Baxter Academy	

TO City of Portland
 ATTN: Planning Division
 389 Congress Street
 Portland, ME 04101

WE ARE SENDING YOU Attached Under separate cover via _____ the following items:

- Shop drawings Prints Plans Specifications Samples
 Copy of letter Change order _____

COPIES	DATE	NO.	DESCRIPTION
8			Baxter Academy Plan Set
1			CD of AutoCAD Files

THESE ARE TRANSMITTED as checked below:

- For approval Approved as submitted Resubmit _____ copies for approval
 For your use Approved as noted Submit _____ copies for distribution
 As requested Returned for corrections Return _____ corrected prints
 For review and comment _____
 FOR BIDS DUE _____ PRINTS RETURNED AFTER LOAN TO US

REMARKS:

Shukria,

Please find the attached approved plan sets and CD with electronic files for the Baxter Academy. If you require any more information do not hesitate to contact us.

Jayson Haskell

COPY TO: 07383 _____

SIGNED:  _____

If enclosures are not as noted, kindly notify us at once.

Att. 2

CITY OF PORTLAND, MAINE
HISTORIC PRESERVATION BOARD

Rick Romano, Chair
Martha Burke Vice-Chair
Scott Benson
Rebecca Ermlich
Ted Oldham
Susan Wroth

June 21, 2013

Dan LaBrie
Rufus Deering Company
383 Commercial Street
Portland, Maine 04101

Re: Request to Remove Chimney; 54 York Street

Dear Mr. LaBrie:

On June 19, 2013, the City of Portland's Historic Preservation Board voted 5-0 (Benson, Burke absent) to deny your request to remove altogether the existing chimney at 54 York Street. The Board's decision was based on findings that the chimney is an important character-defining feature of the historic industrial building which clearly communicates its historic function. Based on this determination, the Board found that the proposal to eliminate the chimney failed to meet Standards 1, 2, and 6 of the historic preservation ordinance, which read as follows:

- (1) *Every reasonable effort shall be made to provide a compatible use for the property which requires minimal alteration to the character-defining features of the structure, object or site and its environment or to use a property for its originally intended purpose.*
- (2) *The distinguishing original qualities or character of a structure, object or site and its environment shall not be destroyed. The removal or alteration of any historic material or distinctive architectural features should be avoided when possible.*
- (6) *Deteriorated historic features shall be repaired rather than replaced wherever feasible. Where the severity of deterioration requires replacement of a distinctive feature, the new feature should match the feature being replaced in composition, design, texture and other visual qualities and, where possible, materials. Repair or replacement of missing historic features should be based on accurate duplications of features, substantiated by documentary, physical or pictorial evidence rather than on conjectural designs or the availability of different architectural elements from other structures or objects.*

Please Note: Although the Historic Preservation Board did not support eliminating the chimney as an architectural feature, this does not preclude removing the existing chimney, if it is confirmed to be structurally unsound. If removed, it should be reconstructed in kind to match the existing. Should you wish to pursue demolition/reconstruction of the chimney, please contact Historic Preservation staff.

Sincerely,

A handwritten signature in black ink, appearing to read "Deborah Andrews". The signature is written in a cursive style with a large, stylized initial "D".

Deborah Andrews
Historic Preservation Program Manager

Cc: Rob Wiener

HISTORIC PRESERVATION BOARD
CITY OF PORTLAND, MAINE

PUBLIC HEARING
54 YORK STREET

TO: Chair Romano and Members of the Historic Preservation Board

FROM: Deborah Andrews, Historic Preservation Program Manager

DATE: July 19, 2013

RE: July 24, 2013 Public Hearing

Request: Request for Reconsideration of 6/19/13 decision regarding removal of obsolete chimney

Address: 54 York Street, C.J. McDonald Building

Applicant: Dan Labrie, Rufus Deering Company

Introduction

Applicant Dan LaBrie, representing Rufus Deering Company, has submitted a request for reconsideration of the Board's recent decision regarding his application to remove the chimney at 54 York Street. On June 19th, the Board voted unanimously to deny the applicant's request to remove the chimney, based on findings that the proposal failed to meet Standards # 1, 2, and 6 of the historic preservation ordinance. (Attached is a copy of the decision letter which summarizes the Board's deliberations and findings.) Mr. LaBrie has received additional information which he believes is relevant to his original request.

Note: At this time, the Board is being asked only to approve the applicant's request for reconsideration. If the Board agrees to reconsider, the item will be put on the Board's next agenda. The Board need not consider the substance of the application at this time.

Attachments

1. Request for reconsideration
2. 6/21/13 decision letter regarding original application

A.H.1

Attn: Deb Andrews

Portland Historic Preservation Board

Dan LaBrie, Senior Vice President of Rufus Deering is requesting your reconsideration in the chimney issue located at 54 York Street for the soon to be location of the Baxter Academy of Science and Technology.

CITY OF PORTLAND, MAINE
HISTORIC PRESERVATION BOARD

Rick Romano, Chair
Rebecca Ermlich, Vice-Chair
Scott Benson
Martha Burke
Ted Oldham
Bruce Wood
Susan Wroth

August 9, 2013

Dan LaBrie
Rufus Deering Company
383 Commercial Street
Portland, Maine 04101

Re: Chimney Consolidation/Coating; 54 York Street

Dear Dan:

On August 7, 2013, the City of Portland's Historic Preservation Board voted 3-1 (Wood abstaining; Benson, Burke and Wroth absent) to approve your request to apply a Conproco pigmented Elastometric membrane coating to the existing chimney at 54 York Street. The coating is intended to consolidate and preserve the existing masonry of the chimney.

Board approval was made subject to the following condition:

- Chimney cap to be flush with the edge of the chimney. Cap to have minimal thickness; applicant is encouraged to consider a bluestone or granite cap which would have limited visibility

Installation to be carried out as described in the plans and specifications submitted for the 8/9/2013 public hearing and/or as described above. Changes to the approved plans and specifications and any additional work that may be undertaken must be reviewed and approved by this office prior to construction, alteration, or demolition. If, during the course of completing the approved work, conditions are encountered which prevent completing the approved work, or which require additional or alternative work, you must apply for and receive a Certificate of Appropriateness or Non-Applicability PRIOR to undertaking additional or alternative work.

This Certificate is granted upon condition that the work authorized herein is commenced within twelve (12) months after the date of issuance. If the work authorized by this Certificate is not commenced within twelve (12) months after the date of issuance or if such work is suspended in significant part for a period of one year after the time the work is commenced, such Certificate shall expire and be of no further effect; provided that, for cause, one or more extensions of time for periods not exceeding ninety (90) days each may be allowed in writing by the Department.

Sincerely,



Deborah Andrews
Historic Preservation Program Manager

CITY OF PORTLAND, MAINE
PLANNING BOARD

Carol Morrisette, Chair
Stuart O'Brien, Vice Chair
Timothy Dean
Bill Hall
David Silk
Elizabeth Boepple
Sean Dundon

February 12, 2013

Rufus Deering Lumber Company
Attention: Dan Labrie
383 Commercial Street
Portland, ME 04101

Sebago Technics, Inc.
Attention: James Seymour, P.E.
75 John Roberts Road, Suite 1A
South Portland, ME 04106

Project Name: Baxter Academy for Technology and Science Project ID: 2012-519
Address: 54 York Street CBL: 042 A 007
Applicant: Rufus Deering Lumber Company
Planner: Shukria Wiar

Dear Mr. Labrie:

On January 22, 2013, the Planning Board considered a Level II and Traffic Movement Permit application for Baxter Academy for Technology and Science to convert the building at 54 York Street to a charter high school, which is an institutional use. The Planning Board reviewed the proposal for conformance with the standards of the Site Plan and Traffic Movement Permit. The Planning Board voted 6-0 (Silk absent) to approve the application with the following waivers and conditions as presented below:

WAIVERS

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report # 05-2013 for application # 2012-519 relevant to the Portland Technical and Design Standards and Guidelines and other regulations and the testimony presented at the Planning Board hearing:

- i. The Planning Board voted 5-1 (Morrisette opposed, Silk absent) to waive the requirement of Section 14-526 (a) (2) (c) of the Site Plan Ordinance that all development shall provide sidewalk and curbing along all frontages, to allow no sidewalk and curbing on the east side of the York Street (Attachment Plan 3 to Planning Board Report# 05.2013).
- ii. The Planning Board voted 0-6 (Silk absent) on the motion to waive the requirement of Section 14-526 (a) (2) (c) of the Site Plan Ordinance that all development shall provide sidewalk and curbing along all frontages, to allow no sidewalk and curbing on the lower east side on Maple Street (Attachment Plan 3 to Planning Board Report # 05.2013). Thus, the waiver **failed** to pass and the applicant shall install a sidewalk and reuse the existing cobblestones per the requirement of Historic Preservation.

TRAFFIC MOVEMENT PERMIT

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #05-2013 for application # 2012-519 relevant to the Traffic Movement Permit and the testimony presented at the Planning Board hearing, the Planning Board voted 6-0 (Silk absent)

2. **Separate Building Permits Are Required** This approval does not constitute approval of building plans, which must be reviewed and approved by the City of Portland's Inspection Division.
3. **Site Plan Expiration** The site plan approval will be deemed to have expired unless work has commenced within one (1) year of the approval or within a time period up to three (3) years from the approval date as agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the one (1) year expiration date.
4. **Performance Guarantee and Inspection Fees** A performance guarantee covering the site improvements, inspection fee payment of 2.0% of the guarantee amount and seven (7) final sets of plans must be submitted to and approved by the Planning Division and Public Services Department prior to the release of a building permit, street opening permit or certificate of occupancy for site plans. If you need to make any modifications to the approved plans, you must submit a revised site plan application for staff review and approval.
5. **Defect Guarantee** A defect guarantee, consisting of 10% of the performance guarantee, must be posted before the performance guarantee will be released.
6. **Preconstruction Meeting** Prior to the release of a building permit or site construction, a pre-construction meeting shall be held at the project site. This meeting will be held with the contractor, Development Review Coordinator, Public Service's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the Development Review Coordinator will confirm that the contractor is working from the approved site plan. The site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the pre-construction meeting.
7. **Department of Public Services Permits** If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)
8. **As-Built Final Plans** Final sets of as-built plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (*.dwg), release AutoCAD 2005 or greater.

The Development Review Coordinator must be notified five (5) working days prior to the date required for final site inspection. The Development Review Coordinator can be reached at the Planning Division at 874-8632. All site plan requirements must be completed and approved by the Development Review Coordinator prior to issuance of a Certificate of Occupancy. Please schedule any property closing with these requirements in mind.

If there are any questions, please contact Shukria Wiar at (207) 756-8083 or via shukriaw@portlandmaine.gov

Sincerely,



Carol Morrissette, Chair
Portland Planning Board

CITY OF PORTLAND, MAINE
HISTORIC PRESERVATION BOARD

Rick Romano, Chair
Martha Burke Vice-Chair
Scott Benson
Rebecca Ernlich
Ted Oldham
Susan Wroth

June 21, 2013

Dan LaBrie
Rufus Deering Company
383 Commercial Street
Portland, Maine 04101

Re: Request to Remove Chimney; 54 York Street

Dear Mr. LaBrie:

On June 19, 2013, the City of Portland's Historic Preservation Board voted 5-0 (Benson, Burke absent) to deny your request to remove altogether the existing chimney at 54 York Street. The Board's decision was based on findings that the chimney is an important character-defining feature of the historic industrial building which clearly communicates its historic function. Based on this determination, the Board found that the proposal to eliminate the chimney failed to meet Standards 1, 2, and 6 of the historic preservation ordinance, which read as follows:

- (1) *Every reasonable effort shall be made to provide a compatible use for the property which requires minimal alteration to the character-defining features of the structure, object or site and its environment or to use a property for its originally intended purpose.*
- (2) *The distinguishing original qualities or character of a structure, object or site and its environment shall not be destroyed. The removal or alteration of any historic material or distinctive architectural features should be avoided when possible.*
- (6) *Deteriorated historic features shall be repaired rather than replaced wherever feasible. Where the severity of deterioration requires replacement of a distinctive feature, the new feature should match the feature being replaced in composition, design, texture and other visual qualities and, where possible, materials. Repair or replacement of missing historic features should be based on accurate duplications of features, substantiated by documentary, physical or pictorial evidence rather than on conjectural designs or the availability of different architectural elements from other structures or objects.*

Please Note: Although the Historic Preservation Board did not support eliminating the chimney as an architectural feature, this does not preclude removing the existing chimney, if it is confirmed to be structurally unsound. If removed, it should be reconstructed in kind to match the existing. Should you wish to pursue demolition/reconstruction of the chimney, please contact Historic Preservation staff.

Sincerely,

A handwritten signature in black ink that reads "Deborah Andrews". The signature is written in a cursive style with a large initial "D" and a stylized "A".

Deborah Andrews
Historic Preservation Program Manager

Cc: Rob Wiener

From: Bruce Hyman
To: Shukria Wiar
CC: Barbara Barhydt; David Margolis-Pineo; Katherine Earley; Tom Errico
Date: 11/7/2012 9:09 AM
Subject: York Street Sidewalk Quality Community Program Grant App to MaineDOT: August 2012
Attachments: TE2012_YorkStSidewalk_Aug03-12Final.pdf

Good morning, Shukria,

As we discussed, attached is the QC Program Grant application the city made to the MaineDOT this past August. Decisions are not expected until Spring 2013, if funded, the federal portion of the funding becomes available in Oct 2013 at the soonest. Also as we discussed, the application includes costs above and beyond a sidewalk to improve ADA accessibility at the intersection of York and High Streets, including curb ramps and pedestrian signals.

Please do not hesitate to contact me if you require more information.

Bruce



MaineDOT

MaineDOT Quality Community Program Transportation Enhancement & Safe Routes

Date Application
Received

(For MaineDOT Use
Only)

For potential inclusion in fiscal years 2014-2015 Biennial Capital Work Plan

2012 Application

Section 1: General Information

Applicant Name(s): City of Portland		
Contact Person: Katherine Earley, P.E.		
Mailing Address: 55 Portland Street		
City: Portland	State: Maine	Zip: 04101
Daytime Phone No.: 207-874-8801	Email: kas@portlandmaine.gov	

Please place an (x) next to all the programs your project is eligible for:

- Federal Transportation Enhancement (TE) Program (If you have committed to minimum 20% local match)
- Federal Safe Routes to School Program (if within 2 miles of a K-8 school and will improve safety for children)

Please Note: If Safe Routes to School is checked, the community agrees to conduct a before and after project count of pedestrians and bicyclists traveling to and from school, as well as to engage with the Maine Safe Routes to School Program on safety education and encouragement initiatives.

NOTE: The following sections of this application request specific project-related information. If warranted, pictures, maps, exhibits, diagrams, survey summaries, etc., should be included with the application. Please be concise. If additional space is required, please attach supplemental sheets.

Section 2: Project Information

Note: Separate complete application(s) are required for each different project proposal

Location of Project: Provide street name(s), beginning and ending location(s), and additional relevant project location information. *Attach designs/ diagrams, maps, etc. that will help provide a clear description of the proposed scope and location. If possible, divide proposed project into logical sections if project can potentially proceed in steps:*

York Street Sidewalk, West End Neighborhood/Downtown Portland, Portland, Maine – see attached.
City of Portland; York Street between High St and Maple St in Downtown Portland commercial district/neighborhood. See Attached map.

Project Summary (*Outline proposed improvements in 30 words or less*): Project will fill strategic gap in sidewalk/pedestrian signal network that provides regional connectivity between Casco Bay Bridge/South Portland and Downtown Portland. Striping and signing will also improve the bikeway network.

Describe the proposed scope of the improvements:

- Installation of granite curb and 5' wide brick sidewalk within existing street
- Establish crosswalks with signals for ADA crossing alignment compliance
- Adjust drainage to new curb line
- Establish shoulder for bicycle traffic
- Installation of sidewalk ramps with truncated domes
- Repaint crosswalks

See attached graphic.

Describe in detail the ownership of the affected properties, whether all or part of the proposed project is in the public right of way, and how wide the estimated right of way is if on a public road system. If the land of the proposed project is not along a public right of way or on public land, please describe any contact with landowner(s).

The improvements are located entirely within the public street right-of-way. York Street has a variable right-of-way width with a minimum width of 50 feet.

Describe the project(s) transportation value(s) and purpose(s): *Each project should serve primarily transportation purposes, as opposed to recreation purposes. A project serves valid transportation purposes if it serves as a connection between origins and destinations, increases safety, and/or relates directly to the transportation system.*

By filling a significant gap in the sidewalk network, this project will provide a needed safe, convenient and attractive pedestrian connection between Downtown Portland and the Casco Bay Bridge. Many pedestrians rely on this connection on a daily basis for job commuting and shopping between Portland and South Portland. Associated bike lane striping also supports bicycle travel.

This linkage is clearly part of a larger pedestrian network that links these two communities together including the East Coast Greenway which is routed within one block of the site.

Describe why this project is important to your community and region (if applicable):

This project represents a strategic linkage between Downtown Portland, the Casco Bay Bridge and South Portland. Many pedestrians and bicyclists use this link on a daily basis to commute to work, for shopping or procuring services. Presently the York Street sidewalk ends and pedestrians are "dumped" into a busy intersection with nowhere to go except into the street, an unsafe and unacceptable condition given the importance of this linkage.

The Portland peninsula/downtown area and Mill Creek/Knightville area of South Portland have an urban commercial and residential density that is supportive of and sustained by pedestrian activity. Pedestrian activity along with requisite pedestrian linkages provides an alternative to the single occupant motor vehicle which reduces traffic congestion, air pollution and supports a denser form of development that would otherwise be encumbered by automobiles and parking spaces.

The linkage is located in an area with the largest concentration of residents and workers in the region and state so the benefits of the trail are heightened.

While this project provides a quality linkage between two communities' trail network it is part of a larger regional pedestrian/bicycle system including the East Coast Greenway which runs within a block of the site.

Describe the potential positive impacts on the community, including at a minimum (if applicable) improving safety, mobility, or transportation in general, and the local/regional economy:

Pedestrian safety will be enhanced by providing a sidewalk along this busy commuter route. Bicycle safety will also be improved by the painting of a bike lane.

The project improves mobility by correcting an existing deficiency in the pedestrian network along York St. as well as the crosswalk at the corner of High St. and York St. Since Portland and South Portland both have bus systems (the South Portland bus runs along this linkage), the project boosts the synergy among modes in the overall transportation network by enhancing non-automobile alternatives for people in both communities.

The project has economic implications since it's a key route for people walking to work, going shopping or using services in the Downtown Portland or the Knightville/Millcreek area of South Portland. The safety and convenience of the improved linkage improves the likelihood people will walk and shop in this area rather than getting into a car and traveling to an outlying area.

Lack of regular exercise is scientifically proven as one of the major contributors to epidemic rise in obesity rates. Such health issues have been identified as part of the Quality Community Program. We need to make the healthy choice the easy choice for our community, and the proposed project will make it easier for more residents to commute on all modes of transportation, including foot and bike.

Describe how the project brings new opportunities for public benefit, including clear descriptions of the people, neighborhoods, etc., and potential businesses that will benefit:

Filling this gap in the transportation network will attract new users to alternative transportation, increase physical activity and help spur economic development.

Pedestrians including commuters, residents going shopping or procuring services will benefit directly from this project. Specifically those people that reside or work on the Portland peninsula including the West End, Downtown, Old Port and Munjoy Hill neighborhoods. Similarly commuters and residents from South Portland will also benefit as well as people on the Portland side wishing to commute to South Portland.

Businesses within the Portland Peninsula as well as in the Mill Creek/Knightville will also benefit from these improvements. With an enhanced ability to walk or ride a bike, people will presumably be more likely to patronize such businesses rather than hopping in a car and traveling to an outlying business district. These businesses will also benefit from having a reduced demand on parking facilities for employees and shoppers.

The health benefits of improving opportunities for walking and biking are discussed in the prior section.

Describe any environmental permitting and/or design needed, completed or under way and any potential issues, and timeline(s):

There are no known environmental issues or permits associated with this project. A detailed design of the improvements will be developed as described in the paragraph below.

Describe the specific timeline for design and/or construction of proposed project:

Design will commence once the city-state agreement is reached. A Preliminary Design Report will be completed prior to June 30, 2014, with the project out to bid by that point as well if funded for construction, for completion by Fall 2014.

Describe the public processes completed and/or planned, including the date(s) of the public meeting(s), planning studies completed, groups that have been involved, public support, and date(s) of select board or council approval of moving ahead with the project (please attach agenda(s) and/or meeting summaries from relevant meetings):

The need for this project was highlighted during the development review process for the Rufus-Deering Lumber on Commercial Street.

It was submitted for funding in 2010 but not funded. At that time, it was endorsed for submittal by the City Council.

In 2010 and 2012 the project received endorsement from Portland's city council for submittal of Quality Community applications. At its June 18, 2012 meeting, the Portland City Council unanimously endorsed (9-0) the package of Portland SRTS and TE applications. [See attached meeting agenda and Council Order.](#)



Pedestrian Facility Gaps are Evident on York Street at High Street (Source: Google Street View).

Section 3: Project Budget (funding request including cash match)

Section 3.1 - Estimated cost of infrastructure project (funding request including cash match)

NOTE: In most instances, if the project is approved, (1.) Design/Engineering/Permitting and (2.) State and Federal Reviews will be the only stages funded in the 2014-2015 Biennial Capital Work Plan. The project will become eligible for construction funding once design is complete, contingent upon when federal funding becomes available.

Design/Engineering/Permitting (12% of Construction or \$10,000, whichever is greater):	1. \$ 10,000
Estimated Right of Way Costs including Appraisal	2. \$ 100
State and Federal Review(s) (5% of Design/Engineering, or \$3,000, whichever is greater)	3. \$ 3,000
Construction (including 10% Contingency):	4. \$ 87,148
Construction Oversight/Engineering (10% of Construction or \$10,000, whichever is greater):	5. \$ 10,000
Total Value of Project as it relates to funding request (add lines 1 through 5):	6. \$ 110,248

Local Match/Voluntary Contributions (Note: All match/voluntary contributions must be detailed. Bonus consideration may be given to applications that offer additional match/voluntary contributions beyond any applicable required local match.)

- Total Local Cash Match (20% minimum for TE projects: % of project 20%) 7. \$ 22,050
- Additional labor and/or materials contributions (will not count towards local match requirement, and should not be listed in above budget items) 8. \$ 0
- Please describe local cash match and/or voluntary contributions in **DETAIL**:
Upon award, local match will be provided by the FY 2015 Capital Improvement Program

Funds Requested from MaineDOT: 10. \$ 88,198

Note: The sum of Lines 7 and 10 should equal Line 6 above.

Note: Section 3.2 of this form also requires further details on costs. The amount shown on line 5 on the above estimate should match the total outline of costs in Section 3.2, or an explanation must be given for the discrepancy.

Section 3.2 - Detailed Description of Infrastructure Project and Cost

Local Force Account: Does the municipality plan on potentially using local forces to build the project? (YES OR NO)

NO

If yes, please include separate detailed construction estimates including design, materials, labor, (labor cost per hour including the overhead amount for town forces), mobilization, construction oversight, etc, for both town led project, and constructor awarded project.

If yes, please initial to indicate that the municipality is willing to move forward with the project even if force account is not approved. _____

If yes, please indicate the cost difference (savings) between a potential local force account led project and contractor led project.

Note: The intent of the Federal Aid program is that most projects will be put out to bid and built by contractors. If the municipality believes that the construction portion of the project will save significant federal transportation dollars if built with local forces and potentially using local materials, than a process is in place to determine if a local project can get approved to do the work with local forces.

For all applicants: Please provide a detailed line item budget for all items from Section 3.1 Feel free to provide an attachment.

Section 4: Authorized Signatures

Please initial that you have read and agree to the proposed schedule: BAH

- Spring of 2013: Municipalities receive notice of award
- Before October 31, 2013: Signed agreement with MaineDOT to proceed
- Before June 30, 2014: Preliminary Design Report completed, or Construction out to bid if funded for construction

This signature(s) indicate the willingness/ability to provide the designated level of matching funds and a willingness to enter into a municipal/State agreement with the Department requiring the municipality/applicant/sponsor to administer the development, design, and construction of the project abiding to federal, State, and local requirements. Signature(s) indicate the understanding and agreement that if the municipality decides to cancel the project after funds have been expended, the municipality may be required to reimburse the federal and/or state funds already expended. The applicant will also be responsible for future maintenance of the completed project. Note that design should meet all applicable federal and state standards and ADA Guidelines, and as always, funding award is contingent upon the availability of federal funding.

A municipal/state agreement with the Maine Department of Transportation is required for the development, design, and construction of the project in accordance with federal, state, and local requirements.

Note: Information on Locally Administered Project (LAP) requirements can be found at:

<http://www.maine.gov/mdot/lap/lpa.php>

An authorized representative of the city/town

Municipal Official:

Name(s): Michael Bobinsky

Title: Director, Department of Public Services

Phone#: 874-8801

Email: mbobinsky@portlandmaine.gov



Signature(s)

August 2, 2012
Date

Local Project Municipal Contact (likely to be the Local Project Administrator)

Name: Bruce Hyman

Title: Bicycle-Pedestrian Program Coordinator

Phone#: 874-8833

Email: bhyman@portlandmaine.gov

Send all application information as follows: Applications must be postmarked by August 3rd, 2012. Seven (7) complete hardcopy application packages (including all attachments and all required documents) must be submitted to:

MaineDOT
Attn: Daniel Stewart
Quality Community Program Manager
Bureau of Transportation Systems Planning
16 State House Station
Child Street
Augusta, ME 04333-0016

City of Portland
 Transportation Enhancement
 York Street Sidewalk TE Project

<u>Item</u>	<u>Units</u>	<u>Qty</u>	<u>Unit Cost</u>	<u>Cost</u>	<u>Notes</u>
Sidewalk - Brick	SF	2675	15 \$	40,125	535 LF of sidewalk
Granite Curb - Straight	LF	620	40 \$	24,800	
Granite Curb - Radius	LF	0	45 \$	-	
Curb ramp	EA	4	2500 \$	10,000	
Detectable WP	EA	0	1000 \$	-	
Flasher	per 2	0	\$	-	
Crosswalk	EA	0	100 \$	-	
Pedestrian signals	LS	1	2800 \$	2,800	
Pavement markings	LS	1	1500 \$	1,500	
			\$	-	
Subtotal			\$	79,225	
Design/Engineering/Permitting			12% \$	10,000	12% of Construction (or \$10k)
State and Federal Review			5% \$	3,000	5% of Design/Engineering (or \$3k)
Construction Oversight/Engin.			10% \$	10,000	10% of Construction (or \$10k)
Contingency			10% \$	7,923	10% of Construction (drainage, etc)
ROW			\$ 100 \$	100	
Total			\$	110,248	
Estimated Local Match			\$	22,050	
Estimated Request			\$	88,198	

IN COUNCIL REGULAR MEETING JUNE 18, 2012 VOL. 127 PAGE 236

result of construction on the Property including, but not limited to, construction of the residential and office uses described in this agreement.” #11. Under no circumstances shall the Developer be permitted to convert “any residential property in the R-4 zone to an institutional use.” Passage 7-2, (Duson, Suslovic.)

Motion was made by Councilor Anton and seconded by Councilor Duson to refer this item back to the planning board. Motion failed 3-6 (Duson, Mavodones, Donoghue, Marshall, Suslovic, Brennan).

Motion was made by Councilor Marshall and seconded by Councilor Suslovic for passage as amended. Passage 6-3 (Coyne, Anton, Leeman.)

Order 215-11/12

Amending Chapter 10, Fire Prevention and Protection; Revisions to Articles I, III, V, VII & VIII – Sponsored by Public Safety and Health & Human Services Committee – Councilor Edward J. Suslovic, Chair.

Motion was made by Councilor Suslovic and seconded by Councilor Donoghue to postpone Order 215 to the July 2, 2012 Regular City Council meeting. Passage 9-0.

Order 216-11/12

Amending Chapter 13 (General Assistance) RE: Levels of Assistance – Sponsored by Mayor Michael F. Brennan. This item was given first reading on June 4, 2012.

Motion was made by Councilor Suslovic and seconded by Councilor Duson for passage with an emergency enactor. Passage 9-0.

ORDERS:

Order 222-11/12

Order Approving Project Applications Re: Maine DOT Quality Community Program – Sponsored by the Transportation, Sustainability, and Energy Committee, David A. Marshall, Chair.

Motion was made by Councilor Marshall and seconded by Councilor Donoghue for passage. Passage 9-0.

Order 223-11/12

Instructing the Public Safety, Health and Human Services Committee to Consider an Ordinance or Recommendation Banning the Sale of Non-Recyclable Polystyrene Foam (PSF) Containers – Sponsored by Councilor Edward J. Suslovic.

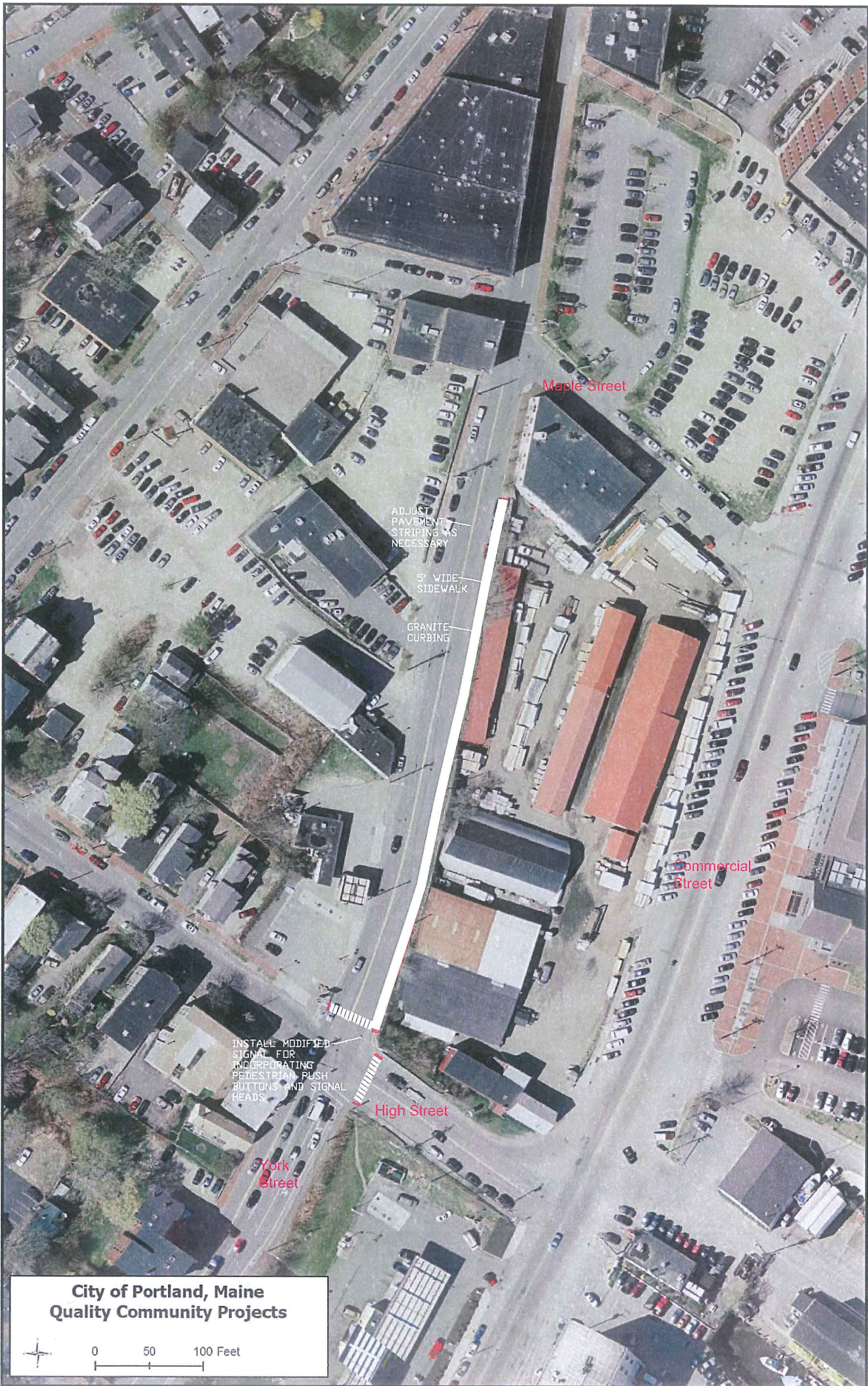
Order 222-11/12
Passage: 9-0, 6/18/12
MICHAEL F. BRENNAN (MAYOR)
KEVIN J. DONOGHUE (1)
DAVID A. MARSHALL (2)
EDWARD J. SUSLOVIC (3)
CHERYL A. LEEHAN (4)

CITY OF PORTLAND
IN THE CITY COUNCIL

JOHN R. COYNE (5)
JOHN M. ANTON (A/L)
JILL C. DUSON (A/L)
NICHOLAS M. MAVODONES (A/L)

**ORDER APPROVING PROJECT APPLICATIONS
RE: MAINE DOT QUALITY COMMUNITY PROGRAM**

ORDERED, that the thirteen project applications for grants from the Maine DOT Quality Community Program as described in the attached memorandum from DFS Director Michael Bobinsky are hereby approved for submission to the Maine Department of Transportation.



City of Portland, Maine
Quality Community Projects



YORK STREET SIDEWALK TE PROJECT

12/10/2012

Maple St Sidewalk (Commercial St and connect to existing brick sidewalk)

Item No	Qty.	UOM	Description	Unit Cost	Ext.
608.15	45	SY	Brick Sidewalk with Bituminous Base	\$150.00	\$6,750.00
609.3811	55	LF	Reset Granite Boarder	\$50.00	\$2,750.00
615.08	80	SY	Loam, Seed and Mulch	\$20.00	\$1,600.00
Subtotal					\$11,100.00
20% Contingency					\$2,220.00
Total Estimate					\$13,320.00

5' x 80'

Note: No granite curb being installed

Leave granite cobbles at edge of road

Install 5' brick walk- reset large granite blocks as boarder to brick walk

Loam and seed sides as necessary

DAG

MaineDOT Quality Community Programs 2012-2013 Application
SECTION 3.1 - Estimated Cost of Infrastructure Project

PROJECT: York Street - High Street to Maple Street

ITEM NO.	ITEM	QUANTITY	UNIT	UNIT PRICE	SUBTOTAL
1	Environmental Evaluation (N/A for this project)	0	LS		\$0
2	Preliminary/Final Design Engineering (assume 15% of Item No 5)	1	LS		\$16,275
3	MaineDOT Proj Design Admin (5% of Item No 2, per application instructions)	1	LS		\$814
4	Right of Way/Land Acquisition (N/A for this project)	0	LS		\$0
5	Construction				
	5.1 Mobilization-Traffic Control Etc	1	LS	\$12,000.00	\$12,000
	5.2 Brick Sidewalk	345	SY	\$120.00	\$41,400
	5.3 Vertical Curb	620	EA	\$35.00	\$21,700
	5.4 Drainage Adjustments	1	LS	\$3,500.00	\$3,500
	5.5 Pipe Replacement	20	LF	\$45.00	\$900
	5.6 ADA Ramp w/Truncated Domes	1	EA	\$3,000.00	\$3,000
	5.7 Pavement Markings	1	LS	\$1,500.00	\$1,500
	5.8 Pedestrian Signals	1	LS	\$2,800.00	\$2,800
	Subtotal:				\$86,800
	Contingency (assume 25%):				\$21,700
	SUBTOTAL, Construction:				\$108,500
6	Construction Engineering (assume 10% of Item No 5)				\$10,850
7	MaineDOT Constr Admin (10% of Item No 6, per application instructions)				\$1,085
8	Total Value of Project (Lines 1 thru 6)				\$136,439
	Note: Estimate does not include utility relocations.				
CLIENT:	City of Portland				
DATE:	June 30, 2010				
BY:	GV-City of Portland				

York Street Sidewalk

Item No	Qty.	UOM	Description	Unit Cost	Ext.
202.15	1	EA	Remove Manhole or Catch Basin	\$1,500.00	\$1,500.00
604.24	1	EA	Catch Basin	\$2,500.00	\$2,500.00
608.08		SY	Reinforced Concrete Sidewalk	\$100.00	\$0.00
608.15	298	SY	Brick Sidewalk with Bituminous Base	\$134.00	\$39,932.00
608.25		SY	Hot Bituminous Sidewalk	\$42.00	\$0.00
608.60	8	SF	Detectable Warning Panel	\$32.00	\$256.00
609.11	535	LF	Vertical Curb, Type 1	\$36.50	\$19,527.50
609.12		LF	Vertical Curb, Type 1 Circular	\$46.00	\$0.00
609.234		EA	Tipdown Curb Type 1, 4 Foot	\$158.25	\$0.00
609.2341		EA	Tipdown Curb Type 1, 4 Foot (Circular)	\$198.00	\$0.00
609.237		EA	Tipdown Curb Type 1, 7 Foot (Straight)	\$268.00	\$0.00
609.2371		EA	Tipdown Curb Type 1, 7 Foot (Circular)	\$337.40	\$0.00
609.38		LF	Remove and Reset Existing Curb	\$21.00	\$0.00
609.41	113	LF	Remove and Stack existing Curb	\$4.00	\$452.00
615.08	60	SY	Loam, Seed and Mulch	\$10.00	\$600.00
629.05	5	HR	Hand Labor, Straight Time	\$14.25	\$71.25
631.10	5	HR	Air Compressor (including operator)	\$20.00	\$100.00
631.12	5	HR	All Purpose Excavator (including operator)	\$75.00	\$375.00
631.171	5	HR	Truck - Small (including operator)	\$50.00	\$250.00
652.39	1	LS	Work Zone Traffic Control	\$3,000.00	\$3,000.00
TOTAL					\$68,563.75

+ 2350

20%

70910

\$ 85,000

LaBRECQUE CONSTRUCTION, Inc.

**1350 RIVERSIDE STREET
PORTLAND, MAINE 04103**

Telephone 797-6305

Rufus Deering
383 Commercial St
Portland, Maine
04101

July 11, 2013

Att: Dan LaBrie

We are pleased to quote on the sidewalk work at the front entrance to Baxter Academy on York Street as per our meeting.

ITEMS:

1. Street Opening permit
2. Supply, place and finish 11sy concrete
3. Remove and reset 25sy brick sidewalk
4. Traffic control

Price: \$3,000.00

Terms: Payment due within 30 days of completion

Authorized Signature.....Sincerely,


Scott LaBrecque

LaBRECQUE CONSTRUCTION, Inc.

**1380 RIVERSIDE STREET
PORTLAND, MAINE 04103**

Telephone 797-6305

June 25, 2013

Rufus Deering
383 Commercial St
Portland, Maine
04101

Att: Dan LaBrie

We are pleased to quote on the work for Baxter Academy as per plans and our meetings.

Maple Street Work:
ITEMS:

1. Dig Safe and Street Opening Permits
2. Supply, install and reset approximately 50LF type I granite curb
3. Construct +/- 116sy brick sidewalk per City of Portland Specs.
4. Supply and install 8SF ADA panel.

Price: \$22,570.00

York Street ADA Ramps:

ITEMS:

1. Dig Safe and Street Opening Permits
2. Supply, install and reset +/- 40LF type I granite curb
3. Construct 45sy brick sidewalk per City of Portland Specs
4. Supply and install 32SF ADA panels

Price: \$12,800.00

\$35,000

Not Included:

1. Tree Work/Stump Removal

approx \$3500.00

Walkways + signage - ???

*(Front door) 3000.00
Walkway see attached.*

Terms: Payment due within 30 days of completion

Authorized Signature.....Sincerely,

MacLeod Construction
Building, Remodeling & Masonry
Fully Insured

17 Kirkland Ave
South Portland, Maine, 04106
(207) 650-5892

July 19 2013
Proposal

Homeowner/ Client Information:

Baxter Academy
54 York St.
Portland, Me .

MacLeod Construction will provide material and labor for the following work .

Take existing 6' x 6' x 20' +- chimney and flues down to roof line and re, build with new Material and copper and rubber at chimney and roof transition for flashing .

Approximate new material list. +-

- 1) 3000 college blend water struck brick
- 2) 1000 backer brick
- 3) 15 24x24 flue liners
- 4) 5 bags cement finish for wash
- 5) 30 bags type n mortar
- 6) Mortar sand
- 7) Copper belt flashing and rubber
- 8) Re, factory cement
- 9) Detergent and sealers

Other expenses

- 1) Dump fees
- 2) permits and possibly closing down sidewalks
- 3) hiring seacoast scaffolding

Once chimney is completed we will re, install existing stainless steel cap and detergent and seal at completion .

Owner of building is responsible for having furnace cleaned and re,tuned

Material and labor \$ 48,500.00

Respectfully Submitted,

CITY OF PORTLAND, MAINE
PLANNING BOARD

Carol Morrisette, Chair
Stuart O'Brien, Vice Chair
Timothy Dean
Bill Hall
David Silk
Elizabeth Boepple
Sean Dundon

February 12, 2013

Rufus Deering Lumber Company
Attention: Dan Labrie
383 Commercial Street
Portland, ME 04101

Sebago Technics, Inc.
Attention: James Seymour, P.E.
75 John Roberts Road, Suite 1A
South Portland, ME 04106

Project Name: Baxter Academy for Technology and Science Project ID: 2012-519
Address: 54 York Street CBL: 042 A 007
Applicant: Rufus Deering Lumber Company
Planner: Shukria Wiar

Dear Mr. Labrie:

On January 22, 2013, the Planning Board considered a Level II and Traffic Movement Permit application for Baxter Academy for Technology and Science to convert the building at 54 York Street to a charter high school, which is an institutional use. The Planning Board reviewed the proposal for conformance with the standards of the Site Plan and Traffic Movement Permit. The Planning Board voted 6-0 (Silk absent) to approve the application with the following waivers and conditions as presented below:

WAIVERS

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report # 05-2013 for application # 2012-519 relevant to the Portland Technical and Design Standards and Guidelines and other regulations and the testimony presented at the Planning Board hearing:

- i. The Planning Board voted 5-1 (Morrisette opposed, Silk absent) to waive the requirement of Section 14-526 (a) (2) (c) of the Site Plan Ordinance that all development shall provide sidewalk and curbing along all frontages, to allow no sidewalk and curbing on the east side of the York Street (Attachment Plan 3 to Planning Board Report# 05.2013).
- ii. The Planning Board voted 0-6 (Silk absent) on the motion to waive the requirement of Section 14-526 (a) (2) (c) of the Site Plan Ordinance that all development shall provide sidewalk and curbing along all frontages, to allow no sidewalk and curbing on the lower east side on Maple Street (Attachment Plan 3 to Planning Board Report # 05.2013). Thus, the waiver **failed** to pass and the applicant shall install a sidewalk and reuse the existing cobblestones per the requirement of Historic Preservation.

TRAFFIC MOVEMENT PERMIT

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #05-2013 for application # 2012-519 relevant to the Traffic Movement Permit and the testimony presented at the Planning Board hearing, the Planning Board voted 6-0 (Silk absent)

the plan is in conformance with the standards for Traffic Movement Permit application for Baxter Academy for Technology and Science and grants a permit subject to the following condition:

- i. The applicant shall contribute \$2,200.00 towards future improvements at the Commercial Street/High Street intersection;

SITE PLAN REVIEW

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #05-2013 for application # 2012-519 relevant to the Site Plan and other regulations, and the testimony presented at the Planning Board hearing, the Planning Board Planning Board voted 6-0 (Silk absent) that the plan is in conformance with the site plan standards of the Land Use, subject to the following conditions to be met prior to the issuance of a building permit unless otherwise stated:

- i. The applicant shall submit a financial capacity letter for review and approval by the Planning Authority;
- ii. The bus and parent loading/unloading conditions shall be monitored within three months following the opening of the school and a report shall be provided to the City summarizing existing conditions. Adjustments to the Plan may be required following this monitoring exercise. Additional annual monitoring shall be required in conjunction with the TDM Plan;
- iii. As presented in the application, the applicant proposes to lease fifteen (15) off-site parking spaces at the Spring Street garage. As part of the TDM plan, the parking demand evaluation shall be conducted and in consultation with the City and that any revisions, if any, to the number of off-site parking spaces needed, shall be identified. The parking evaluation shall be conducted one year after the school is fully occupied and then annually;
- iv. It shall be the responsibility of the applicant for all snow removal on site, so to have pedestrian pathways clear for the use of the students and staff;
- v. A final site plan must be submitted for review that incorporates Jeff Tarling, City Arborist review comments from his memo dated July 12, 2012;
- vi. A final lighting plan, including a photometric plan and catalogue cuts for the proposed exterior lighting fixture meeting the City's technical standards, shall be submitted for review and approval by the Planning Authority prior to the issuance of a building permit;
- vii. Specific details on implementation of traffic signage shall be coordinated and approved by the Planning Authority before installation.
- viii. The applicant shall submit the plans for the Maple Streets sidewalk for review and approval prior to the release of a building permit.

The approval is based on the submitted plans and the findings related to site plan review standards as contained in the Planning Report for application #2012-519 which is attached.

STANDARD CONDITIONS OF APPROVAL

Please note the following standard conditions of approval and requirements for all approved site plans:

1. **Develop Site According to Plan** The site shall be developed and maintained as depicted on the site plan and in the written submission of the applicant. Modification of any approved site plan or alteration of a parcel which was the subject of site plan approval after May 20, 1974, shall require the prior approval of a revised site plan by the Planning Board or Planning Authority pursuant to the terms of Chapter 14, Land Use, of the Portland City Code.

2. **Separate Building Permits Are Required** This approval does not constitute approval of building plans, which must be reviewed and approved by the City of Portland's Inspection Division.
3. **Site Plan Expiration** The site plan approval will be deemed to have expired unless work has commenced within one (1) year of the approval or within a time period up to three (3) years from the approval date as agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the one (1) year expiration date.
4. **Performance Guarantee and Inspection Fees** A performance guarantee covering the site improvements, inspection fee payment of 2.0% of the guarantee amount and seven (7) final sets of plans must be submitted to and approved by the Planning Division and Public Services Department prior to the release of a building permit, street opening permit or certificate of occupancy for site plans. If you need to make any modifications to the approved plans, you must submit a revised site plan application for staff review and approval.
5. **Defect Guarantee** A defect guarantee, consisting of 10% of the performance guarantee, must be posted before the performance guarantee will be released.
6. **Preconstruction Meeting** Prior to the release of a building permit or site construction, a pre-construction meeting shall be held at the project site. This meeting will be held with the contractor, Development Review Coordinator, Public Service's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the Development Review Coordinator will confirm that the contractor is working from the approved site plan. The site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the pre-construction meeting.
7. **Department of Public Services Permits** If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)
8. **As-Built Final Plans** Final sets of as-built plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (*.dwg), release AutoCAD 2005 or greater.

The Development Review Coordinator must be notified five (5) working days prior to the date required for final site inspection. The Development Review Coordinator can be reached at the Planning Division at 874-8632. All site plan requirements must be completed and approved by the Development Review Coordinator prior to issuance of a Certificate of Occupancy. Please schedule any property closing with these requirements in mind.

If there are any questions, please contact Shukria Wiar at (207) 756-8083 or via shukriaw@portlandmaine.gov

Sincerely,



Carol Morrissette, Chair
Portland Planning Board

Attachments:

1. Jeff Tarling memorandum, dated 07.12.2013
2. Planning Board Report
3. Portland City Code: Chapter 32
4. Performance Guarantee Packet

Electronic Distribution:

cc: Jeff Levine, AICP, Director of Planning and Urban Development
Alexander Jaegerman, FAICP, Planning Division Director
Barbara Barhydt, Development Review Services Manager
Shukria Wiar, Planner
Philip DiPierro, Development Review Coordinator, Planning
Marge Schmuckal, Zoning Administrator, Inspections Division
Tammy Munson, Inspection Division Director
Lannie Dobson, Administration, Inspections Division
Gayle Guertin, Administration, Inspections Division
Michael Bobinsky, Public Services Director
Katherine Earley, Engineering Services Manager, Public Services
Bill Clark, Project Engineer, Public Services
David Margolis-Pineo, Deputy City Engineer, Public Services
Doug Roncarati, Stormwater Coordinator, Public Services
Greg Vining, Associate Engineer, Public Services
Michelle Swcney, Associate Engineer
John Low, Associate Engineer, Public Services
Rhonda Zazzara, Field Inspection Coordinator, Public Services
Mike Farmer, Project Engineer, Public Services
Jane Ward, Administration, Public Services
Jeff Tarling, City Arborist, Public Services
Jeremiah Bartlett, Public Services
Captain Chris Pirone, Fire Department
Thomas Errico, P.E., TY Lin Associates
David Sensus, P.E., Woodard and Curran
Rick Blackburn, Assessor's Department
Approval Letter File



PLANNING BOARD REPORT PORTLAND, MAINE

Baxter Academy for Technology and Science
54 York Street
Level II and Traffic Movement Permit
2012-519
Rufus Deering Lumber Company, Applicant

Submitted to: Portland Planning Board: Public Hearing Date: January 22, 2013	Prepared by: Shukria Wiar Date: January 18, 2013 Planning Board Report Number: 05-2013
--	--

I. INTRODUCTION

Dan Labrie, on behalf of Rufus Deering Lumber Company, has requested a public hearing with the Planning Board for a change of use at 54 York Street and a traffic movement permit. The proposal is to convert most of the building to the Baxter Academy for Technology and Science (a charter high school), which is an institutional use. The building has three floors and the current uses include Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses for offices, labs, and dance studio. The site for the proposed Charter School consists of 0.22 acre (9,583sq.ft.) that is fully developed and the existing three-story building is located in the Urban Commercial Business B-5b zone and the Old Port Historic District.

The Level II site plan application was submitted in July of 2012. At that time, the applicant sought to phase the project over three years: 9th grade in the first year, 10th and 11th grade in the second year, 12th in the third year. The building space was to be converted in phases as well to accommodate the incoming grades with the anticipated start date of September of 2012. On July 17, 2012, Baxter Academy was given conditional approval by the Maine Charter School Commission to open a charter school in Fall of 2013. The applicant is seeking a Traffic Movement Permit for the full enrollment, which generates more than peak 100 trips. On November 16, 2012, Baxter Academy received final approval from the Commission. The School is the first public charter school focusing on science, technology, engineering and mathematics.

The project had a workshop meeting with the Planning Board on November 21, 2012. At the meeting, the concerns that were discussed are the following:

1. Sidewalk on York Street
2. The drop-off zones on York and Maple Streets
3. Midblock Crosswalk on York Street

The City has the delegated review for the Traffic Movement Permit and the Board will review the submitted Level II site plan for the change of use that is less than 20,000 square feet. The applicant is also requesting a waiver of sidewalks on York and Maple Streets along the frontage of the proposed site. The requirement for the sidewalks is from the Site Plan Ordinance, Section 14-526 (a) (2) c.

One Hundred and eighty-eight (188) notices were sent to area residents and interested citizen's list. A notice also appeared in the January 14th and January 15th editions of the *Portland Press Herald*.

Applicant Name Dan Labrie, Rufus Deering Lumber Company
Consultants Steve Sawyer and Jim Seymour, PE Sebago Technics

II. PROJECT DATA

Existing Zoning:	Urban Commercial Business B-5b
Existing Use:	Rufus Lumber Milling Display Store with Offices, Labs, and Dance Studio
Proposed Use:	Institutional (Charter School)
Existing number of lots:	One
Proposed number of lots:	One
Parcel Size:	0.22 acre (9,583 s.f.)
Impervious Surface Area:	
Existing:	9,715 sq. ft.
Proposed:	Zero
Net Change:	Zero
Building Area:	7,540 sq. ft.
Parking Spaces:	
Existing:	12
Proposed:	Applicant is looking into leasing parking spaces; properties in the B-5b zone and historic districts are exempt from providing off-street parking requirements
Bicycle Parking Spaces:	
Existing:	Zero
Proposed:	5
Estimated Cost of Project:	\$185,000
Uses in Vicinity:	Commercial; restaurants; school/research; lumber yard

III. EXISTING CONDITIONS

The 7,540 square foot office building at the northern corner of York and Maple Streets is owned by Rufus Deering, but is not part of the commercial lumber company and lumber yard operation. Surface parking and Portland Pie Company are

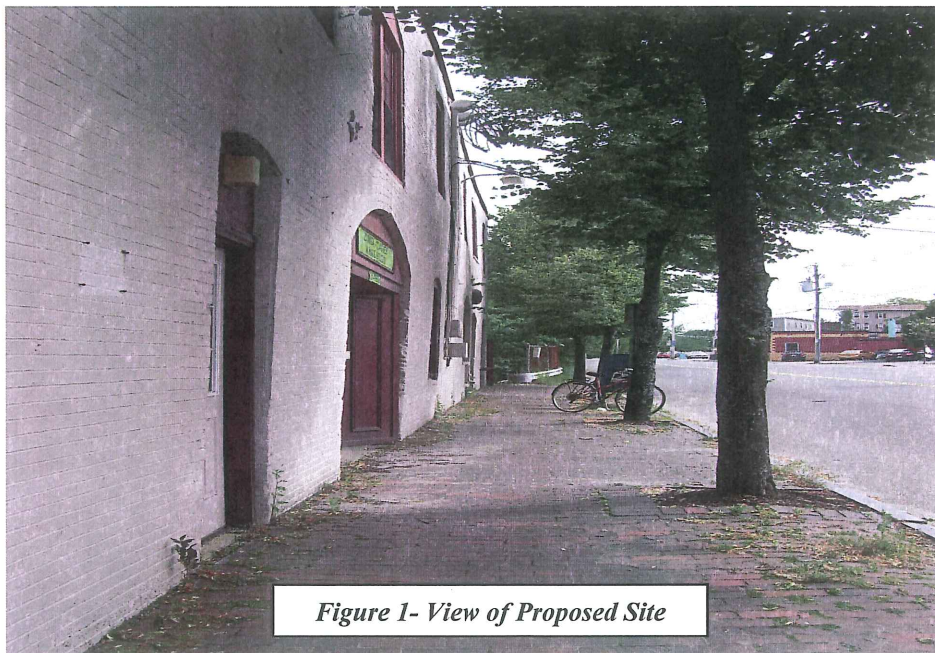


Figure 1- View of Proposed Site

located across York Street and surface parking is across Maple Street. The JB Brown proposal for a hotel and other uses, which is currently under review, is proposed for the corner of Maple and Commercial Streets. The lumber yard surrounds the building to the east and south.

There is an existing curb cut and three street trees along the building frontage on Maple Street. There are no sidewalks on the lower portion of Maple Street or across the street on Maple Street. There are two street trees and no curb cuts on the York Street frontage. The building is currently developed with the following land uses:

- An exercise studio;
- A Buddhist Center;
- Administrative offices;
- Global Biotech;
- Deering Lumber’s Kitchen and Millwork Showroom

Once the conversion is complete, the building will house the Baxter Academy for Technology and Science Charter School as the primary tenant. According to the applicant, either the Global Biotech or the Deering Lumber’s Kitchen will remain in the basement.



Figure 2- View of York Street

IV. PROPOSED DEVELOPMENT

Rufus Deering Lumber Company is proposing to renovate and convert their existing building at 54 York Street (the corner of York and Maple Streets) to house the new Baxter Academy for Technology and Science Charter High School. The lot is fully developed with an existing 7,540 square foot, three-story building. The current uses consist of the Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses of offices, labs, and a dance studio.

The Baxter Academy is proposed as a public charter high school focusing in the fields of mathematics, technology, and science. The school’s proposal is to convert much the building into a full four-year high school over the next two years. The total enrollment is planned for approximately 320 students by 2014. In the opening year which is September of 2013, an estimated 160 freshmen and sophomore year students are projected to enroll. In the second year, grade 9 through 11 will be provided and all four grades in the third year. It is projected that half of the total enrollment will come from Portland, South Portland and Westbrook, with the remaining coming from communities outside of the urban core. School operations will be from 8:00 AM to 2:00 PM, resulting in peak traffic generation associated with the school being between 7-8:00 AM and 2-3:00 PM.



Figure 3- View of Maple Street

V. PUBLIC COMMENT

Since the application is a Level II review, a neighborhood meeting is not required. As of the date of writing this report, no public comments have been submitted. As part of the Traffic Movement Permit, the direct

abutters were notified and notices for the workshop and public hearing were sent to property owners within 500 feet and to the interested citizen's list. The legal ad appeared in the Portland Press Herald on January 14th and 15th editions.

VI. RIGHT, TITLE AND INTEREST AND FINANCIAL/TECHNICAL CAPACITY

- a. The owner of the property is Rufus Deering Company. The applicant has provided a copy of a warranty deed, recorded at the Cumberland County Registry of Deeds (Book 14115 Page 13), which demonstrates their right, title and interest in the property.

- b. The estimated cost of the development is \$185,000. The applicant has not submitted a financial letter as demonstration of their financial and technical capacity to complete the proposed development.

A potential condition of approval:

Prior to the issuance of a building permit, the applicant shall submit a financial capacity letter for review and approval by the Planning Authority.

VII. ZONING ASSESSMENT

Following are the review comments submitted by Marge Schmuckal, Zoning Administrator:

I have reviewed the most current submission. The applicant wants to change the use from offices to a new school (a public charter high school). The property is located in a B-5b zone which allows the proposed use under, "Public or private schools of any type". There is no proposed addition to the building with the exception of an ADA ramp into the City's ROW along York Street. All changes would be interior to the existing building. There are two floors and a basement.

The B-5b zone has no minimum setbacks required. The 10' maximum setback along street lines is currently being met. There are no changes to change this existing condition. The maximum building height of 65' is more than being met with the two story and daylight basement in the existing building. Section 14-332.1(g) exempts the B-5 & B-5b zones from providing off-street parking requirements in the zone.

All HVAC equipment shall meet the sound level requirements listed under 14-230.5(b). Separate permits are required through Inspection Services for HVAC systems. Such application(s) shall provide HVAC dBA equipment levels. Separate permits are required for any new exterior signs. Signs are also subject to Historic review before issuance.

VIII. SITE PLAN SUBMISSION REQUIREMENTS (Section 14-527)

The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and meets the submission requirements.

A. SITE PLAN STANDARDS (Section 14-526)

The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and applicable regulations. Staff comments are listed below.

1. Transportation Standards

The proposed charter school will generate approximately 154 trips and therefore requires a Traffic Movement Permit (TMP), which the City may issue under delegated review. The applicant had a scoping meeting on October 10, 2012 with Tom Errico, Consultant Traffic Engineer. Mr. Errico finds the TMP acceptable, please see below comment:

- i. I generally agree with the trip generation estimate. I would note that the estimate assumes that students will not be driving their own vehicles. It is likely that some students will drive, but in my professional opinion this added traffic would not be expected to alter the conclusions of the traffic analysis. I would note that the school will not be providing parking for students and therefore it is expected that students will seek on-street parking spaces where time limit parking regulation do not exist.

- a. **Status: I have no further comment.**

a. *Impact on Surrounding Street Systems/ Access and Circulation*

At the workshop meeting, the applicant was proposing that the route for school buses will be arriving from York Street onto Maple Street and load/unload curbside on the west side of the street in a designated School Drop-Off Zone. The route for arriving parents to drop off their students will be directed to enter Maple Street from Commercial Street. The parents would then continue to York Street where they would make a left onto York Street in order to utilize the parking spaces for a school drop-off zone proposed on the north side of York Street (opposite the front door of the school). Due to traffic concerns, the Planning Board members suggested that the bus and parent drop off zones be reconfigured. The bus drop-off is now proposed on York Street in front of the building and the proposed parents drop-off area will be on Maple Street.

Mr. Errico has reviewed the School Drop-Off Zones and offers the following comments:

- i. The project is proposing parent drop-off activity to occur on the north side of York Street, just west of Maple Street. I'm concerned about whether parents will drop off students in this area (particularly those originating from Casco Bay Bridge and points from the west). Additionally, the York Street crosswalk is not likely to be used by students being dropped off, given the indirectness of walking to the crosswalk. It is unclear how much curb space will be designated as school drop-off.

- a. **Status: The applicant in coordination with the City is proposing that bus drop-off activity occur on York Street in front of the school and parent drop-off activity occur on Maple Street. I find this plan to be acceptable.**

- ii. While Maple Street is a relatively low volume street, plans for a nearby hotel/restaurant will increase traffic volume and activity. Details on specific bus loading/unloading activity shall be provided including curb space requirements, flashing light restrictions and how all street activities will be accommodated within the curb-to-curb width of Maple Street.

- a. **Status: The applicant in coordination with the City is proposing that bus drop-off activity occur on York Street in front of the school and parent drop-off activity occur on Maple Street. I find this plan to be acceptable. I would note that conditions shall be monitored within three months following opening of the school and a report shall be provided to the City summarizing existing conditions. Adjustments to the Plan may be required following this monitoring exercise. I would note that additional annual monitoring will be required in conjunction with the TDM Plan.**

A potential condition of approval:

- b. *The bus and parent loading/unloading conditions shall be monitored within three months following the opening of the school and a report shall be provided to the City summarizing existing conditions. Adjustments to the Plan may be required following this monitoring exercise. Additional annual monitoring shall be required in conjunction with the TDM Plan.*

The school intends to have a staff person overseeing the parent drop off operation to keep vehicles orderly on Maple Street. Another staff person will be acting as a crossing guard at the intersection of Maple and York Street where the students will be directed to the front entrance of the school.

The applicant states that some of the students, as well as teachers, will be walking and biking to the site. There is a sidewalk only along the York Street frontage of the building, but it does not extend between this site to the High Street intersection. There is a sidewalk on Maple Street only along the building frontage. All sidewalks are in brick. Please refer to Paragraph 3a for further discussion on the sidewalks.

According to the traffic study, the intersection of Commercial and High Streets is a high crash location. Tom Errico states that the “City has plans for implementing improvements at this location and has been requesting monetary contributions for the implementation of improvements. Based upon standard cost methods, the applicant shall contribute \$2,200.00 towards future improvements at the Commercial Street/High Street intersection”.

A potential condition of approval:

The applicant shall contribute \$2,200.00 towards future improvements at the Commercial Street/High Street intersection prior to the issuance of a building permit.

b. *Parking*

According to the applicant, staff members who elect to drive their own vehicles to school will be parking in the City of Portland parking garage on Spring Street and will walk to school via existing sidewalks. The applicant has submitted a Letter of Intent to Lease for the parking spaces at the garage (Attachment H). Initially, the applicant proposed securing twenty (20) parking spaces, but the recent submission cites fifteen (15) spaces.

The Consultant Traffic Engineer recommends:

- i. The applicant shall provide information on the location and the number of off-site parking spaces to be allocated to the school. The traffic study notes that 20 parking spaces will be initially leased for the project, but I would suggest that full-build parking plans be a consideration, for planning purposes.
 - a. **Status: The applicant is leasing 15 parking spaces at the Spring Street garage. It is recommended that in conjunction with their TDM Plan, a parking demand evaluation be conducted and in consultation with the City revisions to the number of off-site parking spaces be identified. The parking evaluation should be conducted annually until one year after the school is fully occupied.**

A potential condition of approval:

As presented in the application, the applicant proposes to lease fifteen (15) off-site parking spaces at the Spring Street garage. As part of the TDM plan, the parking demand evaluation shall be conducted and in consultation with the City and that any revisions to the number of off-site parking spaces needed, shall be identified. The parking evaluation shall be conducted one year after the school is fully occupied and then annually.

c. *Bicycle Parking*

Currently there is no bicycle parking on site. The applicant is proposing to install five bicycle parking spaces at the back of the building in the alleyway and eighteen (18) on Maple Street along the building. The applicant is required to install three bicycle parking spaces for the twenty (20) parking spaces being proposed. Mr. Errico has stated that “a parking demand evaluation be conducted and in consultation with the City revisions to the number of off-site parking spaces be identified. The parking evaluation should be conducted annually until one year after the school is fully occupied”.

d. *Transportation Demand Management (TDM)*
 A transportation demand management plan has been submitted. According to the plan (Attachment K), the School is encouraging variety of transportation options for students and staff. These options include charter buses, providing incentives for commuters on the METRO or South Portland Bus services, and encouraging carpooling and walking and biking. The School believes if they do not provide parking for students, the students will be discouraged from driving their own vehicles. Tom Errico has reviewed the plan and recommends an annual monitoring plan, please see his comments below:

- i. A TDM study shall be prepared for the project. I will review the details of the study when provided. I would note that it will be my recommendation that an annual TDM/Traffic Management Plan be provided to the City for review and comment.
 - a. **Status: I have reviewed the TDM Plan and generally find it to be acceptable. It is important to note that a detailed annual monitoring plan (with required action items) is required to access pick-up and drop-off activities, off-site parking needs, and bicycle storage needs.**



YORK STREET SIDEWALK TE PROJECT

A potential condition of approval:

Annually (from the date of receiving a certificate of occupancy) applicant shall submit a detailed monitoring plan that addresses access for the bus and parent pick-up and drop-off activities, off-site parking needs, and bicycle storage needs.

2. Environmental Quality Standards

a. *Landscaping*

The final site plan must be submitted for review that incorporates Jeff Tarling, City Arborist review comments (Attachment 5). Mr. Tarling recommends the replanting of one street tree on Maple Street to replace the dead tree and recommends that the tree type be either 'Armstrong' Red Maple or other upright species such as upright Pin Oak, or English Oak.

A potential condition of approval:

A final site plan must be submitted for review that incorporates Jeff Tarling, City Arborist review comments from his memo dated July 12, 2012.

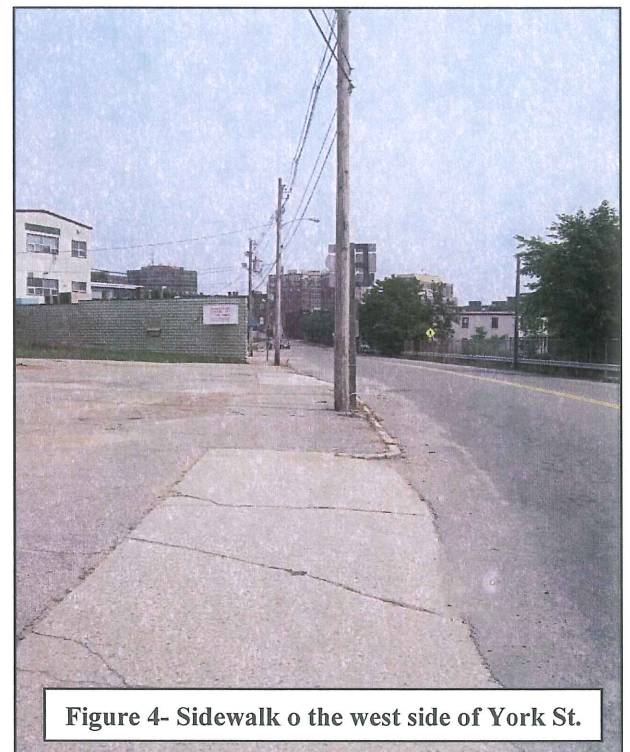


Figure 4- Sidewalk on the west side of York St.

b. *Water Quality, Storm Water Management and Erosion Control*

The project consists of converting the existing Rufus Deering Lumber Milling Display Store into the Baxter Academy for Technology and Science; no building additions are anticipated. The existing site is completely impervious; therefore, the project will not create any new impervious areas.

3. Public Infrastructure and Community Safety Standards

a. *Sidewalk*

This lot is a separate lot, but is under the same ownership as the Rufus Lumber Yard and thus is considered as one tract of land. Therefore this site encompasses an entire city block. The site plan ordinance (Section 14-526 (a) (2) c. i) states

“all developments shall provide sidewalks along all frontages in accordance with Section 14-498 and 14-499 of the City Code, installed to City specifications as described in Section 1 of the Technical Manual. An applicant may request a waiver from curb and sidewalk installation requirements if they meet applicable waiver criteria listed in Section 14-506 (b) of the code.”

Thus the above provision applies to all four frontages of the site. Currently, there are existing brick sidewalks along the Maple Street building frontage, but there is no sidewalk between the proposed site and Commercial Street. There are two driveway cuts on the side of Rufus Deering Lumber Yard. The lower portion of the right-of way does have historic cobble stones, please see photos.

There is a brick sidewalk along the York Street building frontage, but again, there is no sidewalk or curbing between the proposed site and the High Street intersection (approximately 620 feet of frontage). Along this portion of York Street there is existing granite sea wall, approximately 25 feet tall, with York Street running along the top. There is a sidewalk along the opposite side of York Street of various materials and conditions. High Street has bituminous sidewalk and curbing along both sides of the street. The applicant has requested waivers from the sidewalk and curb requirements to install brick sidewalk along York and Maple Streets frontage (see Attachment I).

As part of the application and the Transportation Demand Management Plan, the applicant has indicated that they expect some of the students and staff to walk to the site.

The Department of Public Services has made a Quality Community Program Grant application to the MaineDOT this past August to install a sidewalk at this location. The decision of the application is not expected until Spring 2013 and if it is funded, the



Figure 5- Sidewalk on Lower Maple



Figure 6- Cobblestones on Maple St.

federal portion of the funding becomes available in October 2013 at the earliest. The total local cash match for the project is \$22,000. The application includes costs of the sidewalk, improvement to ADA accessibility at the intersection of York and High Streets, including curb ramps and pedestrian signals.

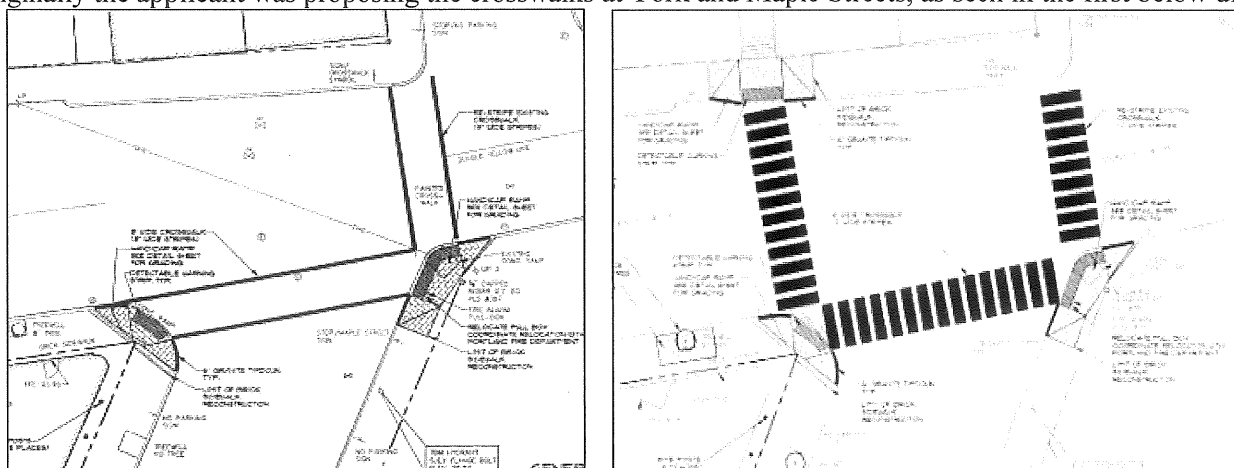
Based upon the anticipated pedestrian activity of the high school students, staff and parents, the City staff and the consulting traffic engineer do not support the requested sidewalk waivers. The staff recommend that both the Maple and York Street sidewalk segments should be constructed. Department of Public Services and Planning Staff recommend that applicant make the contribution of the local match of \$22,000. If the City does not receive the grant, it will be the responsibility of the applicant to construct the sidewalk. The City Staff has recommended that the cobble stones along Maple Street remain and be integrated into the sidewalk. With the retention of the historic cobble stones, curbing will not be needed and could be waived along Maple Street.

Mr. Errico has reviewed the final plans and does not recommend waiving the sidewalk waivers for both York and Maple Streets based on the following comments:

- i. In my professional opinion, the provision of a sidewalk along the south side of York Street between High Street and the school is critically important for this project as students are expected to walk to school. The City has submitted a grant application for the noted sidewalk, but it is unknown if funding will be secured. A similar grant application was submitted previously and the City was not successful. If funding is obtained, it is unlikely a sidewalk will be available by the time school opens in September 2013. I strongly suggest that a sidewalk be installed before the school opens, given anticipated pedestrian activity.
 - a. **Status: The City does not believe this application meets the sidewalk waiver policy and therefore a sidewalk shall be constructed. Fundamental to not meeting the waiver criteria is the expectation of pedestrian usage along the south side of York Street. Pedestrians currently walk in the road and in my opinion there is expectation that some Baxter Academy students will walk along the south side of York Street. I would note that if the City is successful in obtaining a grant application the applicant would only be responsible for providing funds for the City's required local match.**
- ii. The applicant has submitted a waiver for construction of a short section of sidewalk on Maple Street near Commercial Street. The City does not believe this application meets the sidewalk waiver policy and therefore a sidewalk shall be constructed. Pedestrian activity is currently present and pedestrian demand along this sidewalk is expected.

b. *Crosswalks*

Originally the applicant was proposing the crosswalks at York and Maple Streets, as seen in the first below diagram:



During the workshop meeting, Board members suggested that there be a crosswalk a midblock crossing near the entrance of the building. The applicant went to the Crosswalk Committee on January 3, 2013 and were approved. The applicant is providing an improvement plan that upgrades the existing crosswalk and sidewalk ramps and installs a new crosswalk. The second diagram aboveshowes the revised approved crosswalk design.

c. *Public Safety and Fire Prevention*

Captain Chris Pirone of Fire Department has reviewed the submitted site plans and finds the proposed site plan to be acceptable.

d. *Availability and Adequate Capacity of Public Utilities*

The current building has public utilities that are available at the site. The school does not intend on having a kitchen and lunch will be outsourced to a local vender.

4. Site Design Standards

a. *Snow and Ice Loading*

Snow and Ice Loading area has not been shown on the site plan. It has been discussed with the School that the applicant will be responsible for snow removal on site.

A potential condition of approval:

It shall be the responsibility of the applicant for all snow removal on site so to have pedestrian pathway clear for the use of the students and staff.

b. *Historic Resources*

The project site is located in the Old Port Historic District. The project was reviewed and approved by the Historic Preservation Board on January 16, 2013.

c. *Exterior Lighting*

A lighting plan has not been submitted for review. Staff will need to review and approve the photometric plan and catalogue cuts of proposed lighting fixtures.

A potential condition of approval:

A final lighting plan, including a photometric plan and catalogue cuts for the proposed exterior lighting fixture meeting the City's technical standards, shall be submitted for review and approval by the Planning Authority prior to the issuance of a building permit.

d. *Signage and Wayfinding*

A traffic signage and wayfinding plan has been submitted for review. Mr. Errico finds the plan acceptable with "Specific details on implementation will need to be coordinated and approved by the City before installation".

A condition of approval:

Specific details on implementation of traffic signage shall be coordinated and approved by the Planning Authority before installation.

e. *Zoning Related Design Standards*

The proposal is a reuse of an existing building. There are no additions or alterations being proposed to the building.

IX. PLANNING BOARD PROPOSED MOTIONS

1. WAIVERS

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report # 05-2013 for application # 2012-519 relevant to the Portland Technical and Design Standards and Guidelines and other regulations and the testimony presented at the Planning Board hearing:

meets conditions and 3

(5-1 silk absent)

- i. The Planning Board waives **does not waive** the requirement of Section 14-526 (a) (2) (c) of the Site Plan Ordinance that all development shall provide sidewalk and curbing along all frontages, to allow no sidewalk and curbing on the east side of the York Street (Attachment Plan 3 to Planning Board Report # 05-2013).
- ii. The Planning Board waives **does not waive** the requirement of Section 14-526 (a) (2) (c) of the Site Plan Ordinance that all development shall provide sidewalk and curbing along all frontages, to allow no sidewalk and curbing on the lower east side on Maple Street (Attachment Plan 3 to Planning Board Report # 05-2013 05-2013).

6-0-6 opposed

Substitute iii for item 1

2. TRAFFIC MOVEMENT PERMIT

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #05-2013 for application # 2012-519 relevant to the Traffic Movement Permit and the testimony presented at the Planning Board hearing, the Planning Board finds the plan is **is not** in conformance with the standards for Traffic Movement Permit application for Baxter Academy for Technology and Science and grants **does not grant** a permit subject to the following condition:

- i. As part of the TDM plan, the parking demand evaluation shall be conducted and in consultation with the City ~~that~~ revisions to the number of off-site parking spaces shall be identified. The parking evaluation shall be conducted annually until one year after the school is fully occupied.

(6-0)

3. SITE PLAN REVIEW

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #05-2013 for application # 2012-519 relevant to the Site Plan and other regulations, and the testimony presented at the Planning Board hearing, the Planning Board finds that the plan is **is not** in conformance with the site plan standards of the land use code, subject to the following conditions to be met prior to the issuance of a building permit unless otherwise stated:

(6-0)

1-10

- i. The applicant shall submit a financial capacity letter for review and approval by the Planning Authority;
- ii. The bus and parent loading/unloading conditions shall be monitored within three months following the opening of the school and a report shall be provided to the City summarizing existing conditions. Adjustments to the Plan may be required following this monitoring exercise. Additional annual monitoring shall be required in conjunction with the TDM Plan;
- iii. The applicant shall contribute \$2,200.00 towards future improvements at the Commercial Street/High Street intersection;
- iv. As presented in the application, the applicant proposes to lease fifteen (15) off-site parking spaces at the Spring Street garage. As part of the TDM plan, the parking demand evaluation shall be conducted and in consultation with the City and that any revisions to the number of off-site parking spaces needed, shall be identified. The parking evaluation shall be conducted one year after the school is fully occupied and then annually;

(6-0)

delete

v. On an annual basis, that applicant shall submit a detailed monitoring plan that addresses access the bus/ Annually (from the date of receiving a certificate of occupancy) applicant shall submit a detailed monitoring plan that addresses access for the bus and parent pick-up and drop-off activities, off-site parking needs, and bicycle storage needs;

- vii. It shall be the responsibility of the applicant for all snow removal on site, so to have pedestrian pathways clear for the use of the students and staff;
- viii. A final site plan must be submitted for review that incorporates Jeff Tarling, City Arborist review comments from his memo dated July 12, 2012;
- viii. A final lighting plan, including a photometric plan and catalogue cuts for the proposed exterior lighting fixture meeting the City's technical standards, shall be submitted for review and approval by the Planning Authority prior to the issuance of a building permit;
- viii. Specific details on implementation of traffic signage shall be coordinated and approved by the Planning Authority before installation.

If the waivers are not be granted:

include

- x. The applicant shall submit the plans for the ~~York and Maple Streets~~ sidewalk for review and approval prior to the release of a building permit. ~~The monetary contribution for York Street, if applicable, or the sidewalks shall be installed prior to the issuance of a certificate of occupancy.~~

ATTACHMENTS:

PLANNING BOARD MEMO ATTACHMENTS

1. Marge Schmuckal Review 06.18.2012 and 01.06.2013
2. Tom Errico Review 01.17.2013
3. David Senus Review 01/15/2013
4. Jeff Tarling Review 07.12.2012
5. Bruce Hyman, Crosswalk Committee 01.03.2013

APPLICANT'S SUBMITTAL

- A. Written Description of Project
- B. Application
- C. Description of Charter School
- D. Photos of Existing Site
- E. Development Description
- F. Surveyor's Report
- G. Warranty Deed
- H. Intent to Lease Agreement
- I. Traffic Signage Plan
- J. Sidewalk Waiver Request
- K. Summary Package for Traffic Movement Permit (TMP)
- L. Baxter Academy for Technology and Science (TMP)
- M. Traffic Movement Permit Application

PLANS

- Plan 1 Cover Sheet
- Plan 2 Existing Conditions Plan
- Plan 3 Site Plan
- Plan 4 Details
- Plan 5 Code Summary
- Plan 6 Existing Lower Level Plan
- Plan 7 Existing First Floor Plan with Proposed Changes
- Plan 8 Existing Second Floor Plan with Proposed Changes
- Plan 9 Door and Hardware Schedules Wall Details and Stair Railings
- Plan10 York Street Elevation

Shukria Wiar - 54 York Street

From: Marge Schmuckal
To: Shukria Wiar
Date: 1/16/2013 3:57 PM
Subject: 54 York Street

Hi Shukria,

I do not have any zoning concerns for this project. My comments and requirements for HVAC and signs are still in force. Inspection Services will require a change of use permit prior to use.

Marge

Shukria Wiar - 54 York St - Baxter Academy

From: Marge Schmuckal
To: Shukria Wiar
Date: 6/18/2012 3:20 PM
Subject: 54 York St - Baxter Academy

54 York Street - 042-A-7 & 8 - B-5b Zone & Historic Overlay
2012-519 Level II site plan review
June 18, 2012

I have reviewed the most current submission. The applicant wants to change the use from offices to a new school (a public charter high school). The property is located in a B-5b zone which allows the proposed use under, "Public or private schools of any type". There is no proposed addition to the building with the exception of an ADA ramp into the City's ROW along York Street. All changes would be interior to the existing building. There are two floors and a basement.

The B-5b zone has no minimum setbacks required. The 10' maximum setback along street lines is currently being met. There are no changes to change this existing condition.

The maximum building height of 65' is more than being met with the two story and daylight basement in the existing building.

Section 14-332.1(g) exempts the B-5 & B-5b zones from providing off-street parking requirements in the zone.

All HVAC equipment shall meet the sound level requirements listed under 14-230.5(b). Separate permits are required through Inspection Services for HVAC systems. Such application(s) shall provide HVAC dBA equipment levels.

Separate permits are required for any new exterior signs. Signs are also subject to Historic review before issuance.

Marge Schmuckal
Zoning Administrator

Shukria Wiar - 54 York Street - Baxter Academy

From: Tom Errico <thomas.errico@tylin.com>
To: Shukria Wiar <SHUKRIA.W@portlandmaine.gov>
Date: 1/17/2013 11:18 AM
Subject: 54 York Street - Baxter Academy
CC: David Margolis-Pineo <DMP@portlandmaine.gov>, Katherine Earley <KAS@port...

Shukria – The following represents my final comments.

- The project is proposing parent drop-off activity to occur on the north side of York Street, just west of Maple Street. I'm concerned about whether parents will drop off students in this area (particularly those originating from Casco Bay Bridge and points from the west). Additionally, the York Street crosswalk is not likely to be used by students being dropped off, given the indirectness of walking to the crosswalk. It is unclear how much curb space will be designated as school drop-off.

Status: The applicant in coordination with the City is proposing that bus drop-off activity occur on York Street in front of the school and parent drop-off activity occur on Maple Street. I find this plan to be acceptable.
- I generally agree with the trip generation estimate. I would note that the estimate assumes that students will not be driving their own vehicles. It is likely that some students will drive, but in my professional opinion this added traffic would not be expected to alter the conclusions of the traffic analysis. I would note that the school will not be providing parking for students and therefore it is expected that students will seek on-street parking spaces where time limit parking regulation do not exist.

Status: I have no further comment.
- A TDM study shall be prepared for the project. I will review the details of the study when provided. I would note that it will be my recommendation that an annual TDM/Traffic Management Plan be provided to the City for review and comment.

Status: I have reviewed the TDM Plan and generally find it to be acceptable. It is important to note that a detailed annual monitoring plan (with required action items) is required to access pick-up and drop-off activities, off-site parking needs, and bicycle storage needs.
- The applicant shall provide information on the location and the number of off-site parking spaces to be allocated to the school. The traffic study notes that 20 parking spaces will be initially leased for the project, but I would suggest that full-build parking plans be a consideration, for planning purposes.

Status: The applicant is leasing 15 parking spaces at the Spring Street garage. It is recommended that in conjunction with their TDM Plan, a parking demand evaluation be conducted and in consultation with the City revisions to the number of off-site parking spaces be identified. The parking evaluation should be conducted annually until one year after the school is fully occupied.
- In my professional opinion, the provision of a sidewalk along the south side of York Street between High Street and the school is critically important for this project as students are expected to walk to school. The City has submitted a grant application for the noted sidewalk, but it is unknown if funding will be secured. A similar grant application was submitted previously and the City was not successful. If funding is obtained, it is unlikely a sidewalk will be available by the time school opens in September 2013. I strongly suggest that a sidewalk be installed before the school opens, given anticipated pedestrian activity.

Status: The City does not believe this application meets the sidewalk waiver policy and therefore a sidewalk shall be constructed. Fundamental to not meeting the waiver criteria is the expectation of pedestrian usage along the south side of York Street. Pedestrians currently walk in the road and in my opinion there is expectation that some Baxter Academy students will walk along the south side of York Street. I would note that if the City is successful in obtaining a grant application the applicant would only be responsible for

providing funds for the City's required local match.

- While Maple Street is a relatively low volume street, plans for a nearby hotel/restaurant will increase traffic volume and activity. Details on specific bus loading/unloading activity shall be provided including curb space requirements, flashing light restrictions and how all street activities will be accommodated within the curb-to-curb width of Maple Street.

Status: The applicant in coordination with the City is proposing that bus drop-off activity occur on York Street in front of the school and parent drop-off activity occur on Maple Street. I find this plan to be acceptable. I would note that conditions shall be monitored within three months following opening of the school and a report shall be provided to the City summarizing existing conditions. Adjustments to the Plan may be required following this monitoring exercise. I would note that additional annual monitoring will be required in conjunction with the TDM Plan.
- The crosswalk on York Street shall be upgraded to "block" style painted markings. The sidewalk ramps may need to be upgraded to comply with City/ADA standards. Detailed recommendations on the sidewalk will be provided by DPS.

Status: The applicant has provided an improvement plan that upgrades the existing crosswalk and sidewalk ramps and installs a new crosswalk. I find this plan to be acceptable.
- The specific location of bicycle racks shall be identified (the study notes they will be located on Maple Street). The original application indicated 5 bicycle parking locations to be provided. Given the type of activity expected, I would suggest additional bicycle spaces be allocated.

Status: The applicant shall be required to provide additional bicycle racks as determined through the conclusions of the annual monitoring program of the TDM Plan.
- The proposed "5 MIN. SCHOOL DROP-OFF ZONE" parking regulations will need to be coordinated with John Peverada. My initial conversation with John is that this is workable, although specific regulations and time limits need to be evaluated. It should be noted that some parking regulation changes may require approval by the City Council for a parking schedule revision. If required, the applicant will be responsible for providing materials in support of the parking schedule change.

Status: The applicant has submitted a signage plan that I find acceptable. Specific details on implementation will need to be coordinated and approved by the City before installation.
- It is unclear what is being proposed along York Street in front of the subject property. Prior site plans have indicated that handicapped parking spaces will be provided with a sidewalk ramp connection to the street. Previous comments by the City have been in opposition to the provision of handicapped parking spaces on the street.

Status: This comment is no longer relevant. York Street in the subject area is proposed for bus drop-off activity.
- As noted in the traffic study, the intersection of Commercial Street and High Street is a High Crash Location. The City has plans for implementing improvements at this location and has been requesting monetary contributions for the implementation of improvements. I will provide information on the contribution amount in the future.

Status: Based upon standard cost methods the applicant shall contribute \$2,200.00 towards future improvements at the Commercial Street/High Street intersection.
- The applicant has submitted a waiver for construction of a short section of sidewalk on Maple Street near Commercial Street. The City does not believe this application meets the sidewalk waiver policy and therefore a sidewalk shall be constructed. Pedestrian activity is currently present and pedestrian demand along this sidewalk is expected.

If you have any questions, please contact me.

From: "David Senus" <dsenus@woodardcurran.com>
To: "Shukria Wiar" <SHUKRIAW@portlandmaine.gov>
Date: 1/15/2013 12:40 PM
Subject: RE: Baxter Charter School Final Application

We will not have any comments.

Thanks,
Dave

-----Original Message-----

From: Shukria Wiar [mailto:SHUKRIAW@portlandmaine.gov]
Sent: Tuesday, January 15, 2013 9:34 AM
To: Chris Pirone; David Margolis-Pineo; John Peverada; Jeff Tarling;
Marge Schmuckal; Thomas Errico; David Senus
Subject: Baxter Charter School Final Application

Hello:

Baxter School has submitted their final application and is uploaded into eplan. This project is on for a public hearing on Jan. 22 so the report is going out on Friday. Please get me your final comments by Thursday. If you do not have any comments, please state that.

Thanks you.

Shukria

Notice: Under Maine law, documents - including e-mails - in the possession of public officials or city employees about government business may be classified as public records. There are very few exceptions. As a result, please be advised that what is written in an e-mail could be released to the public and/or the media if requested.

--

Shukria Wiar - 54 York Street Project

From: Jeff Tarling
To: Barbara Barhydt ; Shukria Wiar
Date: 7/12/2012 12:16 PM
Subject: 54 York Street Project
CC: David Margolis-Pineo

Hi Shukria -

In review of the landscape portion of the proposed 54 York Street project I would recommend the replanting of one street tree on Maple Street to replace the dead tree.

Recommended tree type would be 'Armstrong' Red Maple or other upright species such as upright Pin Oak, or English Oak.

Jeff Tarling

↑
Species
need to
noted on
Site Plan

Shukria Wiar - Baxter Academy: Crosswalk Committee Action

From: Bruce Hyman
To: Barbara Barhydt; Shukria Wiar; Tom Errico
Date: 1/3/2013 10:14 AM
Subject: Baxter Academy: Crosswalk Committee Action
CC: David Margolis-Pineo; Jeremiah Bartlett; Katherine Earley

Good morning, Shukria,

This morning, the city's Crosswalk Committee deliberated on the proposed curb ramp and crosswalk configurations as shown in the Baxter Academy site plan dated 12/14/12 per Sebago Technics (sheet 3 of 4). The Crosswalk Committee approves the new crosswalk across York Street on the west side of Maple Street, as shown. The Committee understood that there is to be new exterior lighting on the Baxter Academy building as well the existing exterior lighting on the Portland Pie building to adequately light the new crossing during the nighttime.

Please do not hesitate to contact me if you require any additional information.

Best regards,

Bruce

Bruce Hyman
Bicycle & Pedestrian Program Coordinator

City of Portland, Maine
Dept. of Public Services
bhyman@portlandmaine.gov
207-874-8833



January 10, 2013
07383

Shukria Wiar, Planner
City of Portland Planning Division
City Hall 4th Floor
389 Congress St.
Portland, ME 04101

Site Plan Application Submittal – Change of Use Permit-Final Plans
Proposed Baxter Academy for Technology and Science –Charter School
54 York Street, Portland, ME

Dear Ms. Wiar:

Please find a copy of the Final Site Plan packet and associated reports that we have prepared for the Baxter Academy for Technology and Science on property owned by Rufus Deering Lumber Company at 54 York Street, located on the corner of York Street and Maple Street in Portland Maine. The parcel proposed for development is the existing Rufus Deering Lumber Milling display store with other mixed uses for offices, labs, and dance studios. The property consists of a 0.22-acre lot fully developed with an existing 7,540 square foot (SF) footprint for a 3-story building zoned in the B-5b (Urban Commercial Business Zone). The proposed use will entail a floor area conversion of over 10,000 SF change of use and will require a review by the Planning Staff. It is also within the Old Port Historical District.

The overall improvement of the site will not add any additional impervious areas, as the site now is completely impervious. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. Their vision is to convert the entire building into a full 4 year high school over the next 3 to 4 years. Due to the Baxter Academy's plan to convert the existing structure into a school, all the retrofitting will occur with the interior spacing, and no building additions are anticipated to the exterior. Therefore, items such as stormwater and existing utilities services will not be altered as a result of the change of use. No stormwater management is proposed due to the lack of site work required on site; therefore, we request a waiver of the standard.

Rufus Deering Lumber Company will provide the lease for the Baxter Academy and will be incorporating all necessary interior building improvements for the Charter School. We anticipate that the Baxter Academy will be a long-term lease as the Charter School envisions an enrollment of 320 students by 2014. However, in the first year, the expectation is an enrollment near 160 students. Our primary issue on the site has to deal with the transportation of students and parking arrangements for faculty and administrators.

Based on the City's parking requirements, school parking requirements are based on age. One regulation is based on parking for schools with students 15 years of age and under, and one deals with students 16 years and older. Baxter Academy has a letter of intent to lease parking spaces in the City's Spring Street Parking Garage and would expand that as the school expands in the future. Options did exist with the Gulf of Maine Research Institute parking off Commercial Street but due to zoning issues, that parking was not fully approved. If that parking becomes acceptable, the School may acquire leased spaces from there in the future.

In addition, the Director, John Jacques, has indicated that the Charter School intends to contract for bus transportation for three full sized buses for student transportation in year one, which is the equivalent of upwards of 160 students. Other students could take other modes of public transportation or be dropped

off by parents. Student parking will not be encouraged or provide by the school. York Street has been designated as a location for bus delivery and pick up of students directly in front of the school's entrance. Buses will be loaded/unloaded from the existing sidewalk adjacent to the proposed school building. A full Traffic Management Demand Plan and Traffic Report have been submitted under separate cover. We have been coordinating with staff on this plan along with the Sidewalk Committee to resolve final suggested crosswalks to accommodate student pedestrian foot traffic across Maple and York Streets. The Sidewalk Committee agreed to the proposed location of a sidewalk from the upper end of Maple Street to a midpoint location in York Street which appears a natural extension and will provide convenient crossings for the school students. The increase in trips potentially impacting the site due to the change of use will require a Traffic Movement Permit (TMP), which the City has delegated authority to approve from the State of Maine Department of Transportation with Planning Board review and approval.

We are requesting a waiver of the sidewalk and curbing construction requirement along York Street. A separate request is included in the TMP package explaining our position for the waiver under the City's Sidewalk and Curbing Construction requirements. In addition, the State has determined that the site does not need to provide a dedicated handicap parking space. Americans with Disabilities Act accessibility will be provided through the main entrance off York Street. Baxter Academy and/or Rufus Deering will clear all sidewalks on the lots frontage of York and Maple Streets and will contract to have snow removed as there is no appreciable location for storage. It is the Academy's interest to maintain a clean and safe sidewalk for its students and staff as well as other public pedestrians.

Rufus Deering Lumber Company is very anxious to start construction immediately as the Charter School wishes to begin this 2013 school year. Rufus Deering has provided the Historical Preservation Committee with information as to their planned exterior revisions for lighting, signs, and general improvements. We have provided architectural elevation views and plans, and site improvements plans also for your review. Our understanding is a hearing on that will be held next week. We intend to provide the Planning Department results or conditions of the committee's findings prior to our proposed hearing.

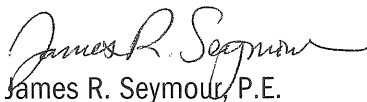
The applicant is still coordinating and reviewing HVAC systems, and will prefer to provide these as a condition of approval. Lighting is being provided by the owner and is to mimic the lighting similar to Portland Pie Co. directly across the street where the lighting is reflected back into the building and sidewalk from a curled pipe extension and shield light pan. We will provide those cut sheets once we know they have been accepted by the Historical Committee.

We look forward to meeting with the Planning Board on January 22nd to further discuss the TMP and TMD as well as the general improvements planned for the Baxter Academy especially with parking and transportation needs.

If you have any questions, please feel free to contact me at our office (207-200 2083). We look to working with the Planning Division staff and City engineers to facilitate the opening of this new opportunity for a Charter School in the Downtown Portland area.

Sincerely,

SEBAGO TECHNICS, INC.



James R. Seymour, P.E.

Project Manager

JRS:jrs/kn

Enc.

cc: Dan Labrie, President, Rufus Deering Lumber Company
John Jacques, Executive Director, Baxter Academy for Technology & Science

Effective August 18, 2010



Level II – Preliminary and Final Site Plans Development Review Application Portland, Maine

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Level II: Preliminary or Final Site Plan. Please note that Portland has delegated review from the State of Maine for reviews under the Site Location of Development Act, Chapter 500 Stormwater Permits, and Traffic Movement Permits. General information pertaining to the thresholds of review, public noticing procedures and the fee structure is contained in the Notice to Developer's Packet.

Level II: Site Plan Development includes:

- New structures with a total floor area of less than 10,000 sf except in Industrial Zones.
- New structures with a total floor area of less than 20,000 sf in Industrial Zones.
- Any new temporary or permanent parking area, paving of an unpaved surface parking area, or creation of other impervious surface area greater than 7,500 sf.
- Building addition(s) with a total floor area of less than 10,000 sf (cumulatively within a 3 year period) except in Industrial Zones.
- Building addition(s) with a total floor area of less than 20,000 sf in Industrial Zones.
- Park improvements: New structures or buildings with a total floor area of less than 10,000 sf, facilities encompassing an area of greater than 7,500 sf and less than 20,000 sf (excludes rehabilitation or replacement of existing facilities).
- New piers, docks, wharves, bridges, retaining walls, and other structures within the Shoreland Zone.
- Land disturbance between 1 and 3 acres (includes stripping, grading, grubbing, filling or excavation).
- A change in the use of a total floor area between 10,000 and 20,000 sf in any existing building (cumulatively within a 3 year period).
- Construction of a lodging house, bed and breakfast facility, emergency shelter or special needs independent living unit.
- Signage subject to approval pursuant to Section 14-526 (d) 8.a. (IV) of the Land Use Code.
- Any new major or minor auto service station with less than 10,000 sf of building area that is outside the B-2 or B-5 zones.
- The creation of day care or home babysitting facilities to serve more than 12 children in a residential zone (not permitted as a home occupation under section 14-410) in any principal structure that has not been used as a residence within the 5 years preceding the application.

The Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division
Fourth Floor, City Hall
389 Congress Street
(207) 874-8721 or 874-8719

Office Hours
Monday thru Friday
8:00 a.m. – 4:30 p.m.

B.1

PROJECT NAME: BAXTER ACADEMY for TECHNOLOGY & SCIENCE

PROPOSED DEVELOPMENT ADDRESS:
54 YORK ST.

PROJECT DESCRIPTION:
INTERIOR CHANGE OF USE FROM OFFICES TO
CHARTER (PUBLIC) SCHOOL > 10,000 SF FLOOR SPACE.

CHART/BLOCK/LOT: MAP 42 - BLOCK A PRELIMINARY PLAN 6/5/12 (date)
LOTS 7 & 8 FINAL PLAN _____ (date)

CONTACT INFORMATION:

Applicant's Contact for electronic plans
Name: JAMES SEYMOUR P.E.
e-mail: JSEYMOUR@SEBAQOTECHNICS.COM
work #: 200-2083

<p>Applicant – must be owner, Lessee or Buyer</p> <p>Name: <u>DAN LABRIE</u></p> <p>Business Name, if applicable: <u>RUFUS DEERING LUMBER Co.</u></p> <p>Address: <u>383 COMMERCIAL ST.</u></p> <p>City/State : <u>PORTLAND, ME</u> Zip Code: <u>04101</u></p>	<p>Applicant Contact Information</p> <p>Work # <u>772-6505</u></p> <p>Home# <u>767-1055</u></p> <p>Cell # <u>838-0444</u> Fax# <u>772-6981</u></p> <p>e-mail: <u>Dlabrie@rufusdeering.com</u></p>
<p>Owner – (if different from Applicant)</p> <p>Name:</p> <p>Address: <u>SAME</u></p> <p>City/State : Zip Code:</p>	<p>Owner Contact Information</p> <p>Work #</p> <p>Home#</p> <p>Cell # Fax#</p> <p>e-mail:</p>
<p>Agent/ Representative</p> <p>Name: <u>N/A</u></p> <p>Address:</p> <p>City/State : Zip Code:</p>	<p>Agent/Representative Contact information</p> <p>Work #</p> <p>Cell #</p> <p>e-mail:</p>
<p>Billing Information</p> <p>Name: <u>APPLICANT.</u></p> <p>Address:</p> <p>City/State : Zip Code:</p>	<p>Billing Information</p> <p>Work #</p> <p>Cell # Fax#</p> <p>e-mail:</p>

Engineer SEBAGO TECHNICS INC Name: JAMES SEYMOUR, P.E. Address: 75 JOHN ROBERTS ROAD, SUITE 1A City/State: SO. PORTLAND, ME Zip Code: 04106	Engineer Contact Information Work # 200-2083 Cell # 632-1199 Fax# 856-2256 e-mail: jseymour@sebagotechnics.com
Surveyor SEBAGO TECHNICS INC Name: WILLIAM SHIPPEN PLS Address: 75 JOHN ROBERTS RD, SUITE 1A City/State: SO. PORTLAND, ME Zip Code: 04106	Surveyor Contact Information Work # 200-2084 Cell # Fax# 856-2256 e-mail: wshippen@sebagotechnics.com
Architect RICK RENNER ARCHITECTURE Name: CHARLES YOUNG Address: 35 Pleasant St. City/State: PORTLAND, ME Zip Code: 04101	Architect Contact Information Work # 773-9699 x203 Cell # Fax# e-mail: cyoung@rrennerarchitects
Attorney PEARCE & DOW Name: MIKE PEARCE Address: 50 MONUMENT SQ. City/State: PORTLAND, ME Zip Code: 04101	Attorney Contact Information Work # 822-9900 Cell # Fax# 822-9901 e-mail:

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

Level II Development (check applicable reviews) <input checked="" type="checkbox"/> Less than 10,000 sq. ft. (\$400.00) <input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)	Fees Paid (office use) _____ _____	Other Reviews (check applicable reviews) <input type="checkbox"/> Traffic Movement (\$1,000) <input type="checkbox"/> Stormwater Quality (\$250) <input type="checkbox"/> Section 14-403 Review (\$400 + \$25/lot) # of Lots _____ x \$25/lot = _____ <input type="checkbox"/> Other _____ <input checked="" type="checkbox"/> Change of Use <input type="checkbox"/> Flood Plain <input type="checkbox"/> Shoreland <input type="checkbox"/> Design Review <input type="checkbox"/> Housing Replacement <input type="checkbox"/> Historic Preservation	Fees Paid (office use) _____ _____
The City invoices separately for the following: <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) Third party review is assessed separately.			
Plan Amendments (check applicable reviews) <input type="checkbox"/> Planning Staff Review (\$250) <input type="checkbox"/> Planning Board Review (\$500)	Fees Paid (office use) _____ _____		

APPLICATION SUBMISSION

All site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Application submissions shall include one (1) paper packet with folded plans containing the following materials:

Numbers below changed- can't seem to highlight

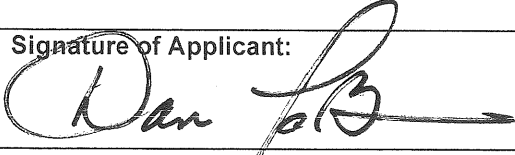

1. One (1) full size site plans that must be folded.
 One (1) set of all written materials or as follows, unless otherwise noted:
 - a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
2. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet.
3. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
4. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
5. One (1) set of plans reduced to 11 x 17.

Refer to the application checklist for a detailed list of submittal requirements.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level II Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: 	Date: 
--	---

PROJECT DATA

The following information is required where applicable, in order complete the application

Total Site Area	9715	sq. ft.
Proposed Total Disturbed Area of the Site	N/A	sq. ft.
(If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)		
IMPERVIOUS SURFACE AREA		
• Proposed Total Paved Area	N/A	sq. ft.
• Existing Total Impervious Area	9715	sq. ft.
• Proposed Total Impervious Area	N/A	sq. ft.
• Proposed Impervious Net Change	0	sq. ft.
BUILDING AREA		
• Proposed Building Footprint	-NA-	sq. ft.
• Proposed Building Footprint Net change	0	sq. ft.
• Existing Total Building Floor Area		sq. ft.
• Proposed Total Building Floor Area	0	sq. ft.
• Proposed Building Floor Area Net Change	0	sq. ft.
• New Building		(yes or <input checked="" type="radio"/> no)
ZONING		
• Existing	B5b	
• Proposed, if applicable	SAME	
LAND USE		
• Existing	MIXED USE -	OFFICES · RETAIL · LABS · ZUMBA
• Proposed		CHARTER SCHOOL
RESIDENTIAL, IF APPLICABLE		
• Proposed Number of Affordable Housing Units		N/A
• Proposed Number of Residential Units to be Demolished		
• Existing Number of Residential Units		
• Proposed Number of Residential Units		
• Subdivision, Proposed Number of Lots		
PARKING SPACES		
• Existing Number of Parking Spaces	12	
• Proposed Number of Parking Spaces	10	(LEASED)
• Number of Handicapped Parking Spaces	2	(YORK ST.)
• Proposed Total Parking Spaces	1	
BICYCLE PARKING SPACES		
• Existing Number of Bicycle Parking Spaces	0	
• Existing Number of Bicycle Parking Spaces	0	
• Proposed Number of Bicycle Parking Spaces	5	
• Total Bicycle Parking Spaces	5	
ESTIMATED COST OF PROJECT		
	\$185,000.00	

**General Submittal Requirements – Preliminary Plan (Optional)
Level II Site Plan
Preliminary Plan Phase Check list (if elected by applicant)**

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Completed application form
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Application fees
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written description of project
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Evidence of right, title and interest.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Copies of required State and/or Federal permits.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of proposed project's compliance with applicable zoning requirements.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written description of existing and proposed easements or other burdens.
<input checked="" type="checkbox"/> NA	<input type="checkbox"/>	1	Written requests for waivers from individual site plan and/or technical standards, where applicable.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Traffic analysis (may be preliminary, in nature, during the preliminary plan phase).
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written summary of significant natural features located on the site.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written summary of project's consistency with related city master plans.
Applicant Checklist	Planner Checklist	Number of Copies	Site Plan Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>		Boundary Survey meeting the requirements of Section 13 of the City of Portland Technical Manual.
<input type="checkbox"/>	<input type="checkbox"/>	1	Preliminary Site Plan Including the following: (*information provided may be preliminary in nature during preliminary plan phase):
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed structures with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Location of adjacent streets and intersections and approximate location of structures on abutting properties.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Proposed site access and circulation.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Proposed grading and contours.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Preliminary landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Existing and proposed utilities (preliminary layout).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Preliminary infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, transit infrastructure, roadway improvements).
<input checked="" type="checkbox"/> NA	<input type="checkbox"/>		▪ Preliminary stormwater management and erosion control plan.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b) 1. of the Land Use Code).
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Proposed alterations to and protection measures for significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important

NA

<i>natural features listed in Section 14-526 (b)1. of the Land Use Code).</i>
▪ <i>Existing and proposed easements or public or private rights of way.</i>

**General Submittal Requirements – Final Plan (Required)
Level II Site Plan**

Final Plan Phase Check list (including items listed above in General Requirements for Preliminary Plan, if applicant did not elect to submit for a preliminary plan review)

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirement
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of financial and technical capacity.
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of utilities' capacity to serve the development.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of fire safety (referencing NFPA fire code and Section 3 of the City of Portland Technical Manual).
<input type="checkbox"/>	<input type="checkbox"/>	1	Construction management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Traffic Plan (if development will (1) generate 100 or more PCE or (2) generate 25 or more PCE and is located on an arterial, within 1/2 mile of a high crash location, and/or within ¼ mile of an intersection identified in a previous traffic study as a failing intersection).
<input checked="" type="checkbox"/> NA	<input type="checkbox"/>	1	Stormwater management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of solid waste generation and proposed management of solid waste.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of conformity with applicable design standards.
<input type="checkbox"/>	<input type="checkbox"/>	1	Manufacturer's verification that HVAC and manufacturing equipment meets applicable state and federal emissions requirements.

Final Plan Phase			
<input type="checkbox"/>	<input type="checkbox"/>	1	Final Site Plan Including the following
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Existing and proposed structures on the site with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Location of adjacent streets and intersections and approximate location of structures on abutting properties.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed site access and circulation.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed grading and contours.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways. Proposed curb lines must be shown.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed loading and servicing areas, including applicable turning templates for delivery vehicles</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed snow storage areas or snow removal plan.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed trash and recycling facilities.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Existing and proposed utilities.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Location and details of proposed infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, public transit infrastructure, roadway improvements).</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed septic system, if not connecting to municipal sewer. (Portland Waste Water Application included in this application)</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed finish floor elevation (FFE).</i>

<input type="checkbox"/>	<input type="checkbox"/>	▪ Exterior building elevation(s) (showing all 4 sides).
<input type="checkbox"/>	<input type="checkbox"/>	▪ Proposed stormwater management and erosion controls.
<input type="checkbox"/>	<input type="checkbox"/>	▪ Exterior lighting plan, including street lighting improvements..
<input type="checkbox"/>	<input type="checkbox"/>	▪ Proposed signage.
<input type="checkbox"/>	<input type="checkbox"/>	▪ Identification of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code). Wetlands must be delineated.
<input type="checkbox"/>	<input type="checkbox"/>	▪ Proposed alterations to and protection measures for of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code).
<input type="checkbox"/>	<input type="checkbox"/>	▪ Total area and limits of proposed land disturbance.
<input type="checkbox"/>	<input type="checkbox"/>	▪ Soil type and location of test pits and borings.
<input type="checkbox"/>	<input type="checkbox"/>	▪ Details of proposed pier rehabilitation (Shoreland areas only).
<input type="checkbox"/>	<input type="checkbox"/>	▪ Existing and proposed easements or public or private rights of way.

Review Staff
City of Portland
Portland, Me 04101

Dear Staff Members:

Baxter Academy for Technology and Science
54 York Street

HISTORY

The building is located on the corner of York and Maple (see attached map) in Portland. Fifty-four York Street has had numerous uses over the years. Most recently approximately 20,000 sq. ft. was leased to Media Power. In April of 2010 Media Power needed more space and moved to a building on Foden Road in South Portland.

In mid-2011 John Jaques, future Executive Director of Baxter Academy, and Daniel LaBrie, Senior Vice President of Rufus Deering Lumber Company, began discussing a possible partnership by leasing 54 York Street to them for a charter school.

OBJECTIVE

The school aims to link students with local businesses, scientists, researchers and other professionals working in fields of science, technology, engineering and mathematics. The Portland location offers easy access to the Gulf of Maine Research Institute, Maine Medical Center, the University of Southern Maine and Southern Maine Community College.

The school hopes to get a charter from the state by July 1st and open in September. It hopes to be the first of its kind in Portland.

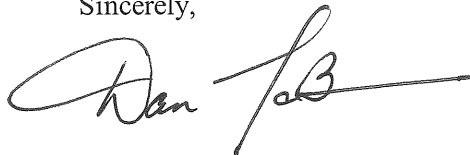
PROPOSAL

The fit up to prepare the building for a school consists of upgrading HVAC, Security system, Handicap Accessibility, new carpets and paint. The building as it stands today does not have an elevator. An exterior elevator will be installed on the southwest side of the building on a 15' easement.

CLOSING

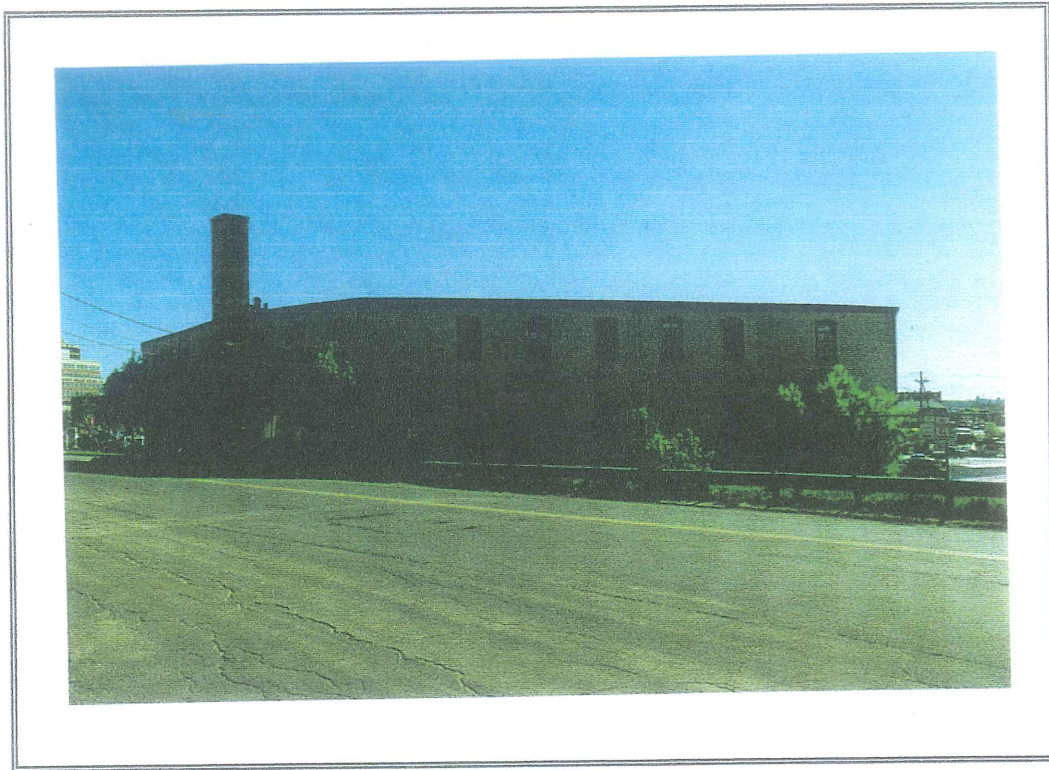
I believe this is a perfect location for a charter school. It is wonderful to be able to offer our youth an alternative education to the usual public school system. These charter schools have been very successful all over the country. Time is of the essence, I would ask that you move this process along and grant the permit in a timely manner.

Sincerely,

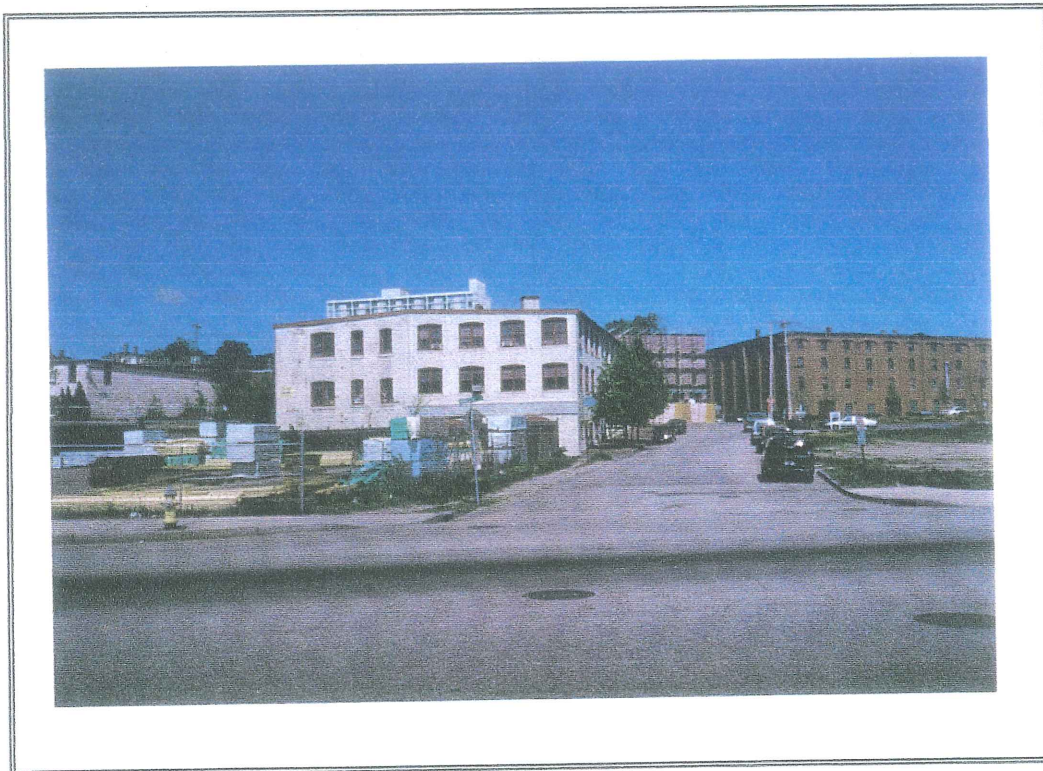


Daniel LaBrie
Senior Vice President

SUBJECT PHOTOGRAPHS

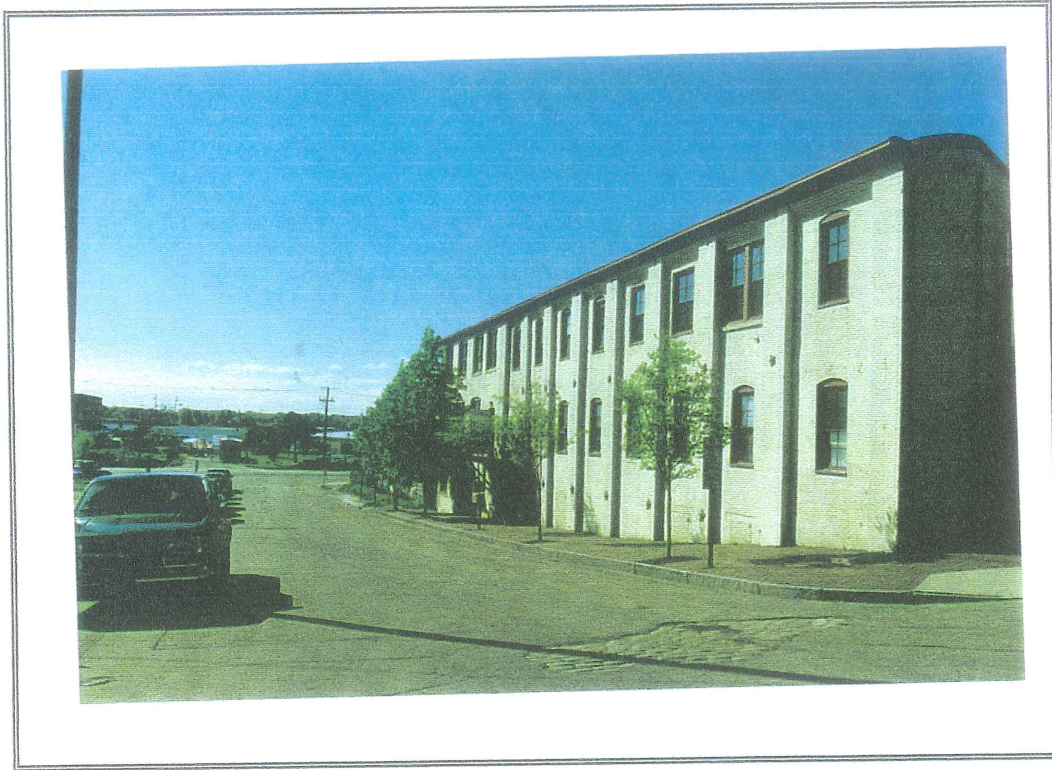


1) Easterly view of subject from York Street



2) Westerly view of subject from Commercial Street, looking up Maple Street

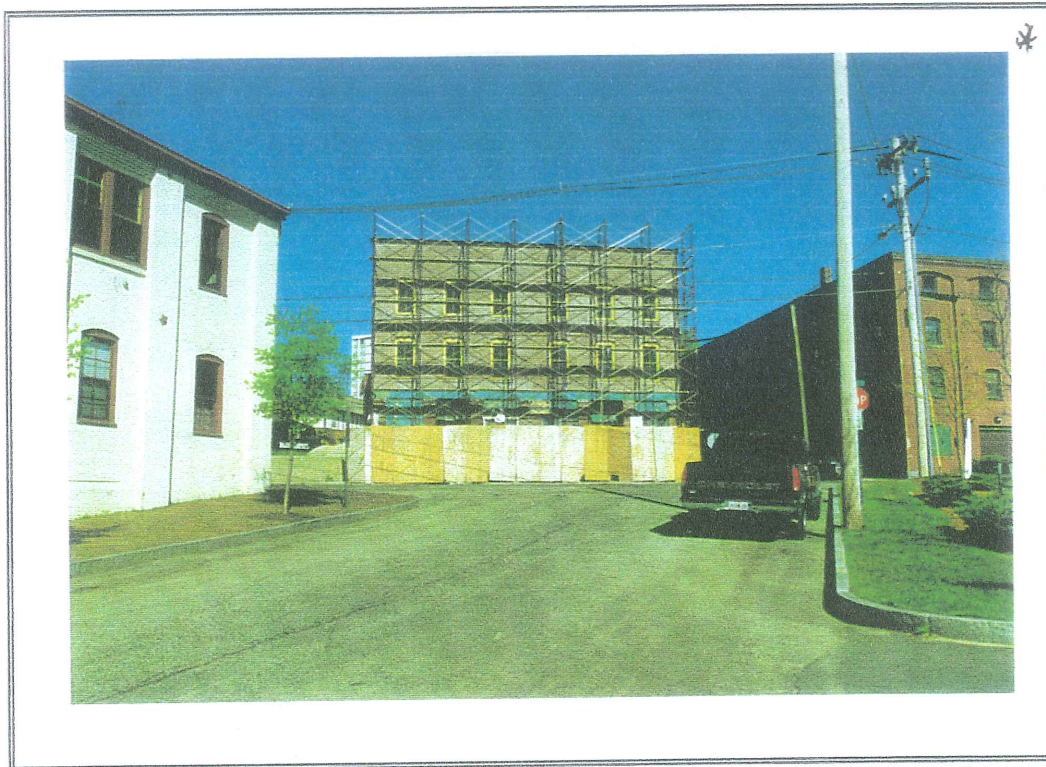
SUBJECT PHOTOGRAPHS



3) Southeasterly view down Maple St. toward Commercial St, subject at right



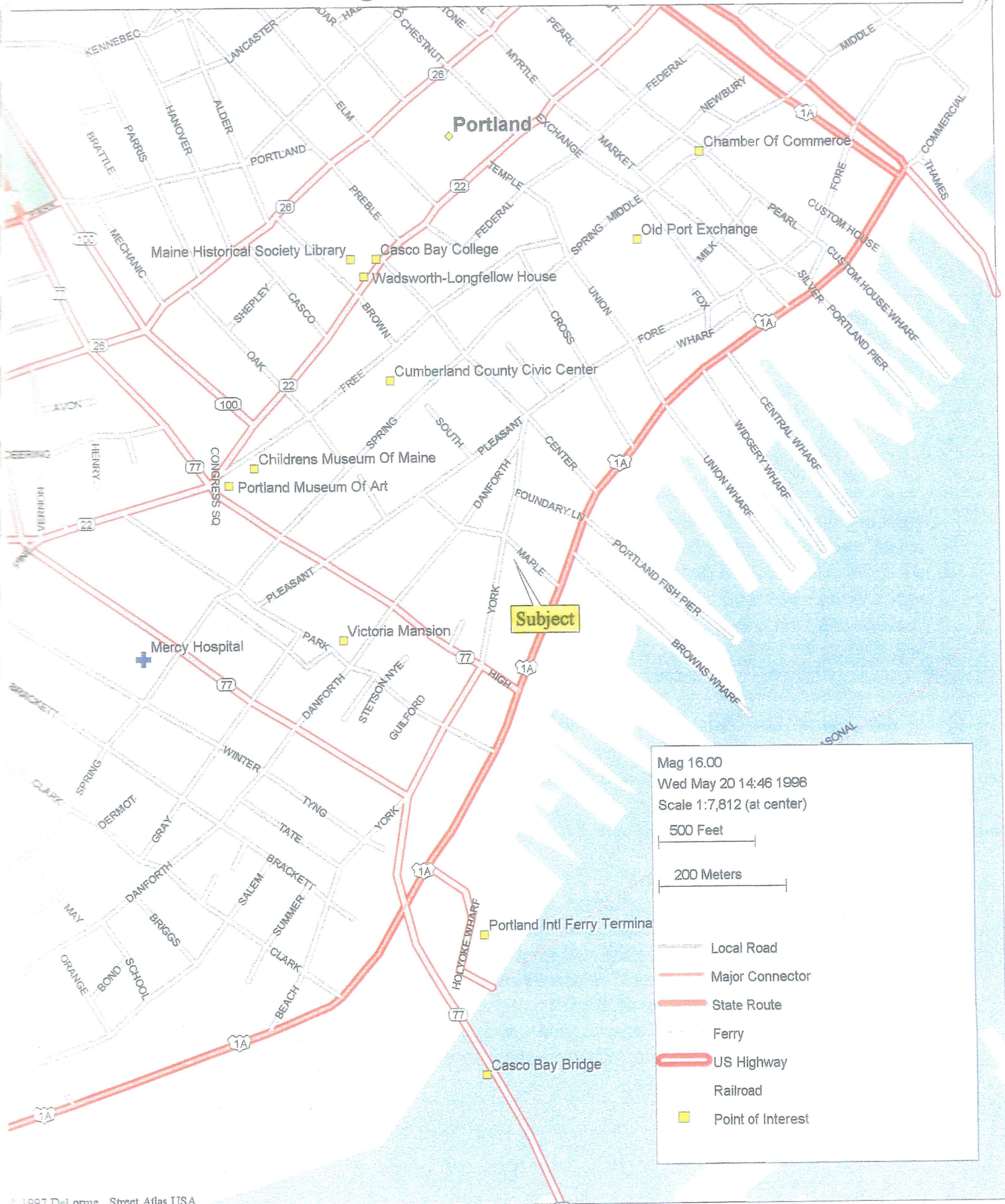
4) Westerly view of south elevation of subject (driveway behind bldg)



N2) Ongoing rehabilitation/revitalization of the subject neighborhood (subject at left)

Neighborhood Map

D.3



Mag 16.00
Wed May 20 14:46 1998
Scale 1:7,812 (at center)

500 Feet
200 Meters

- Local Road
- Major Connector
- State Route
- Ferry
- US Highway
- Railroad
- Point of Interest

SUBJECT SITE DESCRIPTION

Location

The subject site enjoys a corner lot location at the apex of York and Maple Streets in Portland Maine. This location receives heavy traffic flow along York Street as this is the major connector route from the Portland CBD to the Casco Bay Bridge providing access to the City of South Portland. The site is also within close proximity to the Portland Waterfront District with marine, transit, and tourist related land uses prevalent.

Land Area

The subject site, according to the legal description provided, contains an estimated land area of 8,712±sf in fee simple, plus an additional passageway area measuring 15' x 85' (1,275±sf) for a total of 9,987± sf. The survey plan also provided to the appraiser, showed an area of 8,447±sf, although it is not clear that this is not the footprint of the building rather than the site. For purposes of this appraisal, this is not a large issue as the site is considered fully built out and the leased fee interest has been appraised. The client is urged to consult with a surveyor and/or title attorney to clarify such matters further prior to making decisions which may be based upon land area estimates.

Shape/Frontage

The shape of the site is characterized as irregular. The parcel is a corner lot with an almost trapezoidal shape due to its frontage on two streets. According to the legal description provided, the site has 125'± of frontage along Maple Street plus another 100'± of frontage along York Street.

Access/Visibility

The subject site is considered to have good access and visibility characteristics. The site enjoys high traffic counts approximating 12,000 cars per day according to MDOT traffic volumes 1996. York Street feeds directly into/from the new Casco Bay Bridge (replaced the Million Dollar Bridge last year) and is a direct link to the Portland CBD and Old Port Districts. Commercial Street to the south is also a major commuter travel artery and serves the working waterfront section of Portland. Maple Street is a small connector road which traverses the two.

Topography/Soils

The topography of the site is characterized as sloping in an easterly direction down from its York Street frontage towards Commercial Street and the Portland Waterfront. No on site soils investigation was performed by the appraiser. The Cumberland County Soils Survey classifies this area as Urban, Cut and Fill land. It was reported that a portion of the subject (original construction) dates back to 1820 and survived the Great Fire of 1936 which leveled much of downtown Portland and gave way to the filling in along the waterfront and creation of Commercial Street in 1950. Given the age and varied uses of the site over time, the client is urged to conduct due diligence in regards to environmental issues to their own satisfaction. The appraised value herein assumes no adverse conditions related to environmental issues which may or may not affect the subject site. Reference is made to the Statement of Assumptions and Limiting Conditions in this regard.

Utilities

The subject site is serviced with all public utilities including water, sewer, natural gas, electricity, telephone and cable TV. Municipal services such as fire and police protection and sanitation are deemed to be adequate.

Easements

Reference is made to the attached legal description and title report contained in the Addenda section of this report for an overview of the various limitations of title to the subject property. As discussed, part of the site is encumbered as a passageway in common with others. There were no other adverse easements or encroachments noted on the date of inspection/value herein which would limit the use and/or value of the subject property under its current land use program.

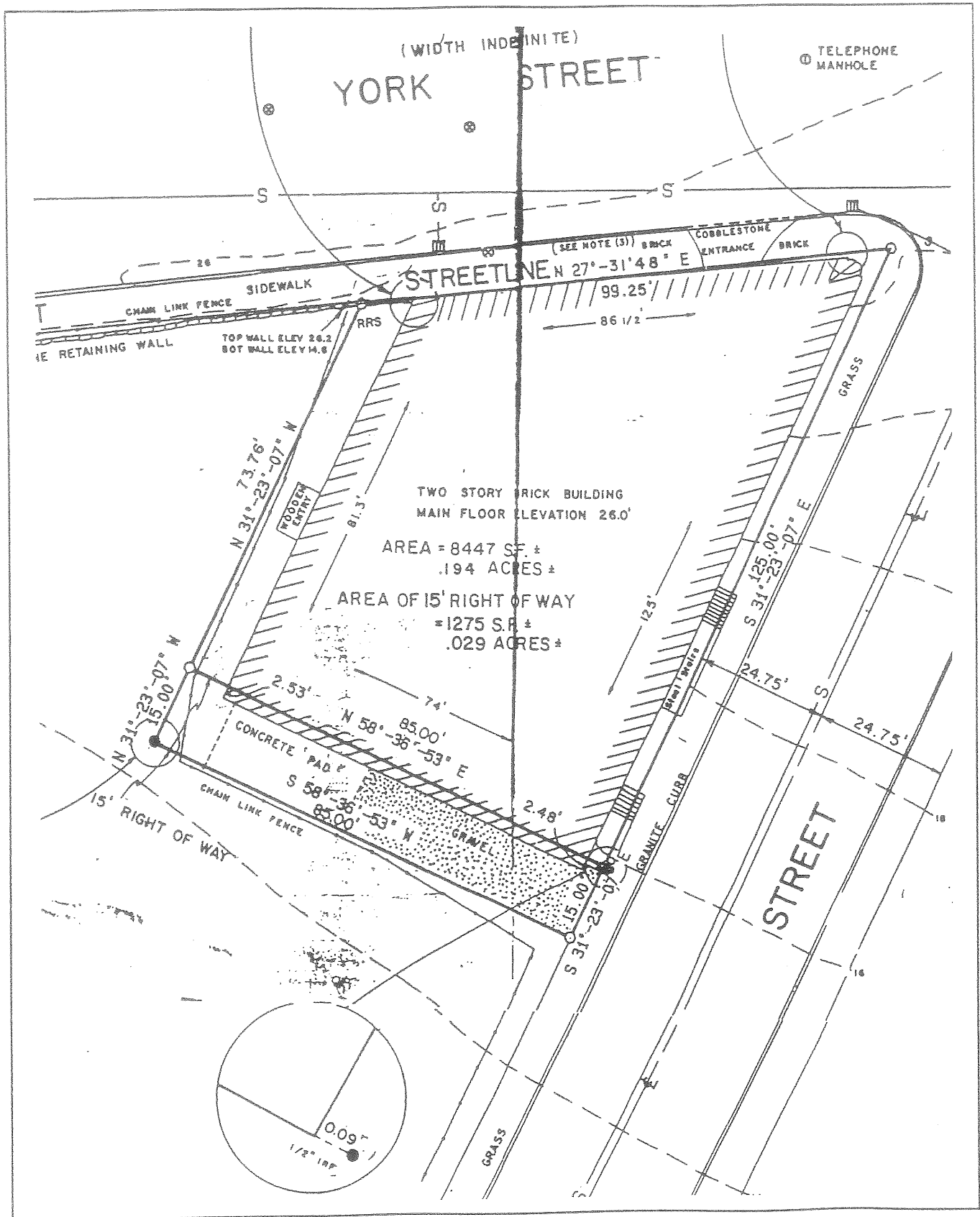
Functional Utility

The subject site is considered to offer average functional utility for development purposes. If vacant and available, it would likely be assembled with abutting lands for a larger scale development. The site does offer good exposure for commercial purposes and adequate frontage for access. It is zoned for business use and is located in an area of revitalization. An interim use for surface parking would be an immediate use of the site.

Marketability/Appeal

For the reasons cited above, the subject land parcel is considered very marketable and would be anticipated to sell quickly if vacant and available.

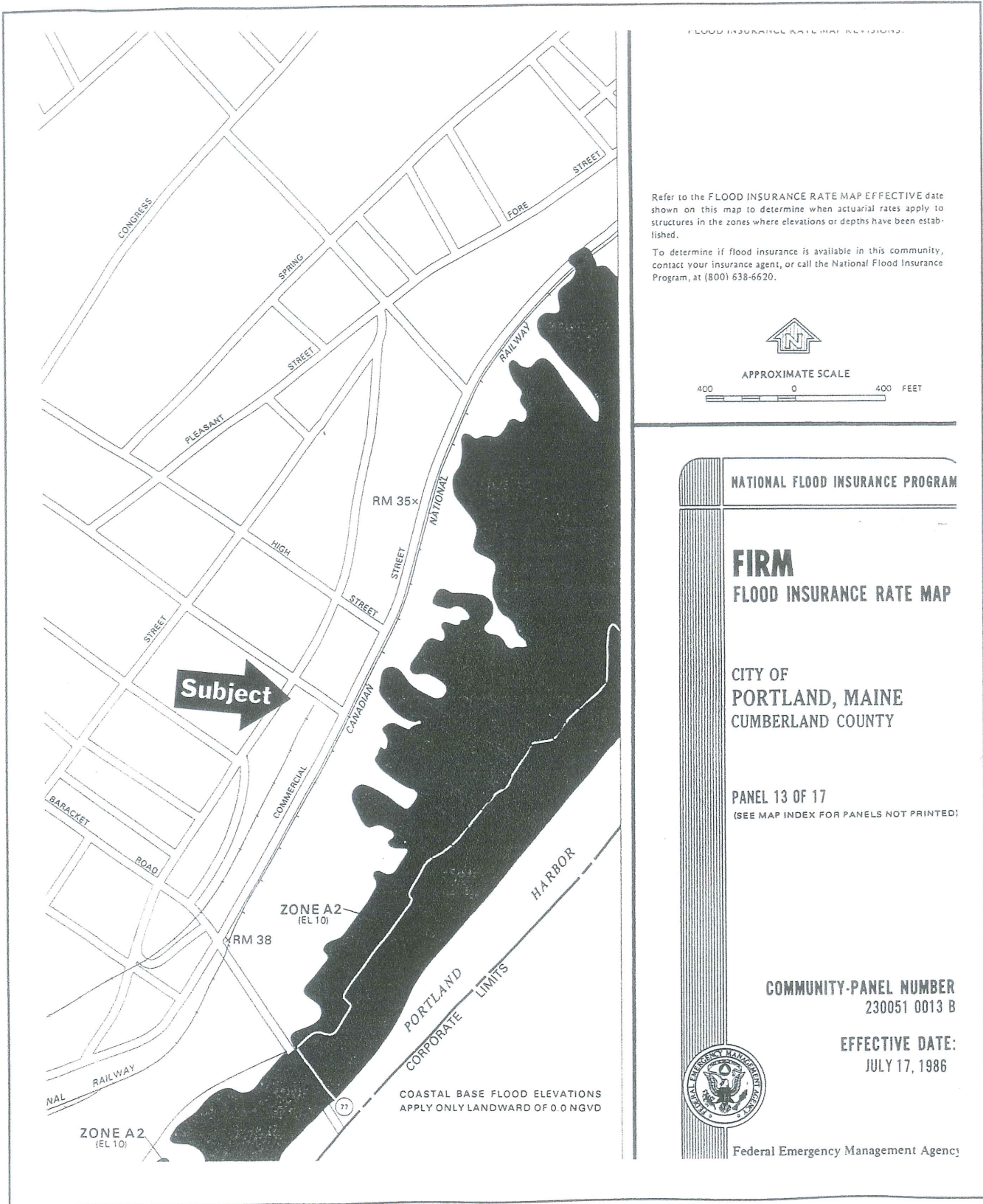
SUBJECT SITE PLAN
(not to scale)

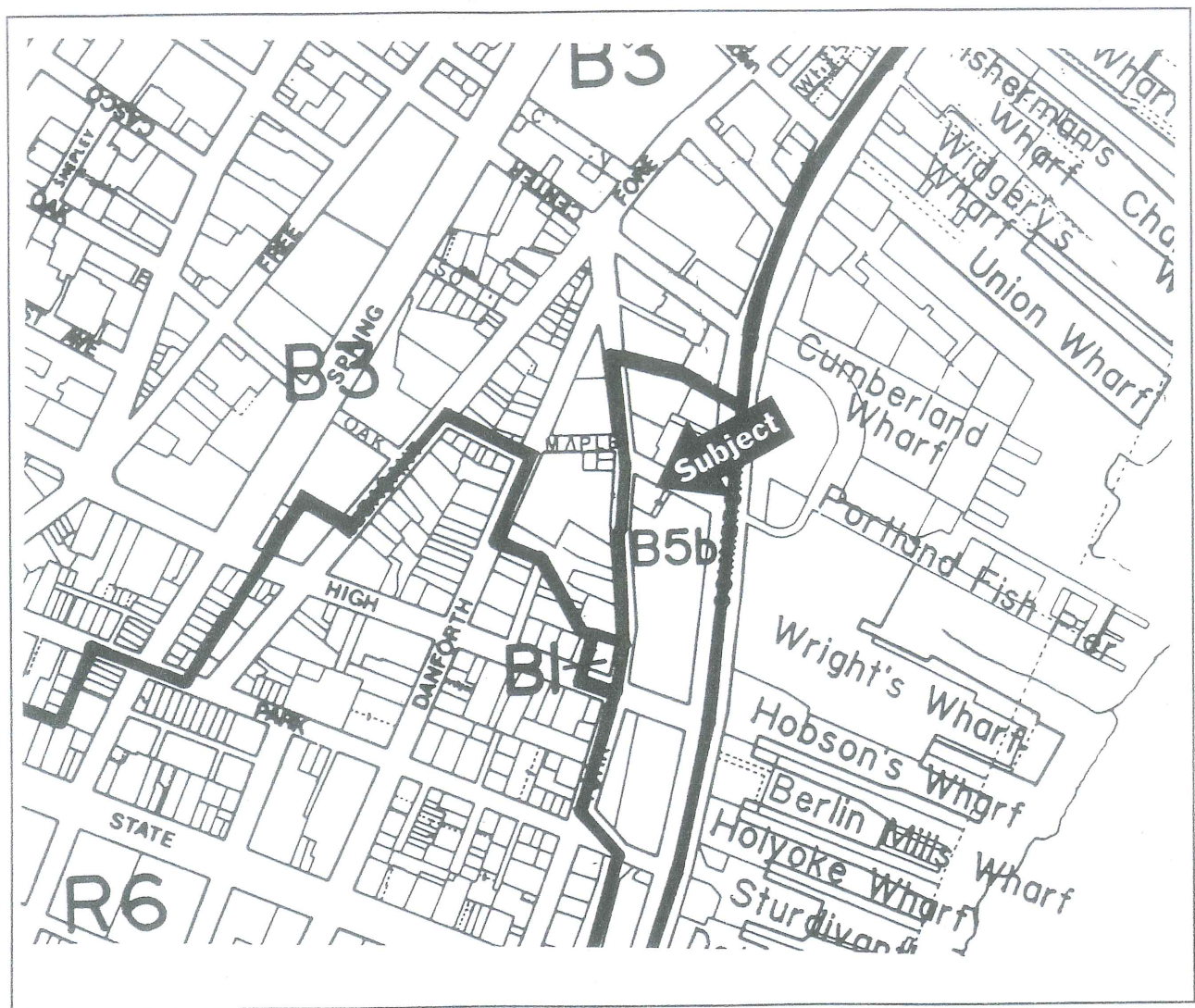


E.3

FEDERAL EMERGENCY MANAGEMENT FLOOD HAZARD ZONE

The subject property is not located in a federally designated 100-year flood hazard zone, according to the FEMA Map presented below:





Picture

Ceilings

The ceilings were primarily suspended acoustic panel tiles and in good condition, with recessed fluorescent lighting throughout.

Roof Structure & Cover

The roof structure is of a flat, shed style with a covering of rubber membrane dating to 1989 according to the present owner.

Electrical

The electrical entrance to the building consists of two primary services of four-wire, three phase, 240 volt capacity with a 400-amp rating. Distribution is via six 100 amp and 1 200-amp panels with circuit breaker protection. Electricity is separately metered to each tenant excepting unit 102 which is on the house meter.

Plumbing

The building is serviced with municipal water and sewer. The main water supply enters in the basement sprinkler room and has a backflow preventer. Where visible, plumbing distribution piping is primarily copper with some PVC and cast iron waste lines noted. There is also a recent, gas fired domestic hot water heater in the basement.

H.V.A.C.

The building is serviced with a natural gas heater with individual tenant meters, excepting unit 102 which is on the house. Heat is provided through the building with a combination of ceiling and wall hung gas heaters. There is a central, gas-fired boiler with five zones noted on the first floor as well as some electric baseboard heat interspersed.

Elevators

There are no elevators currently in the building.

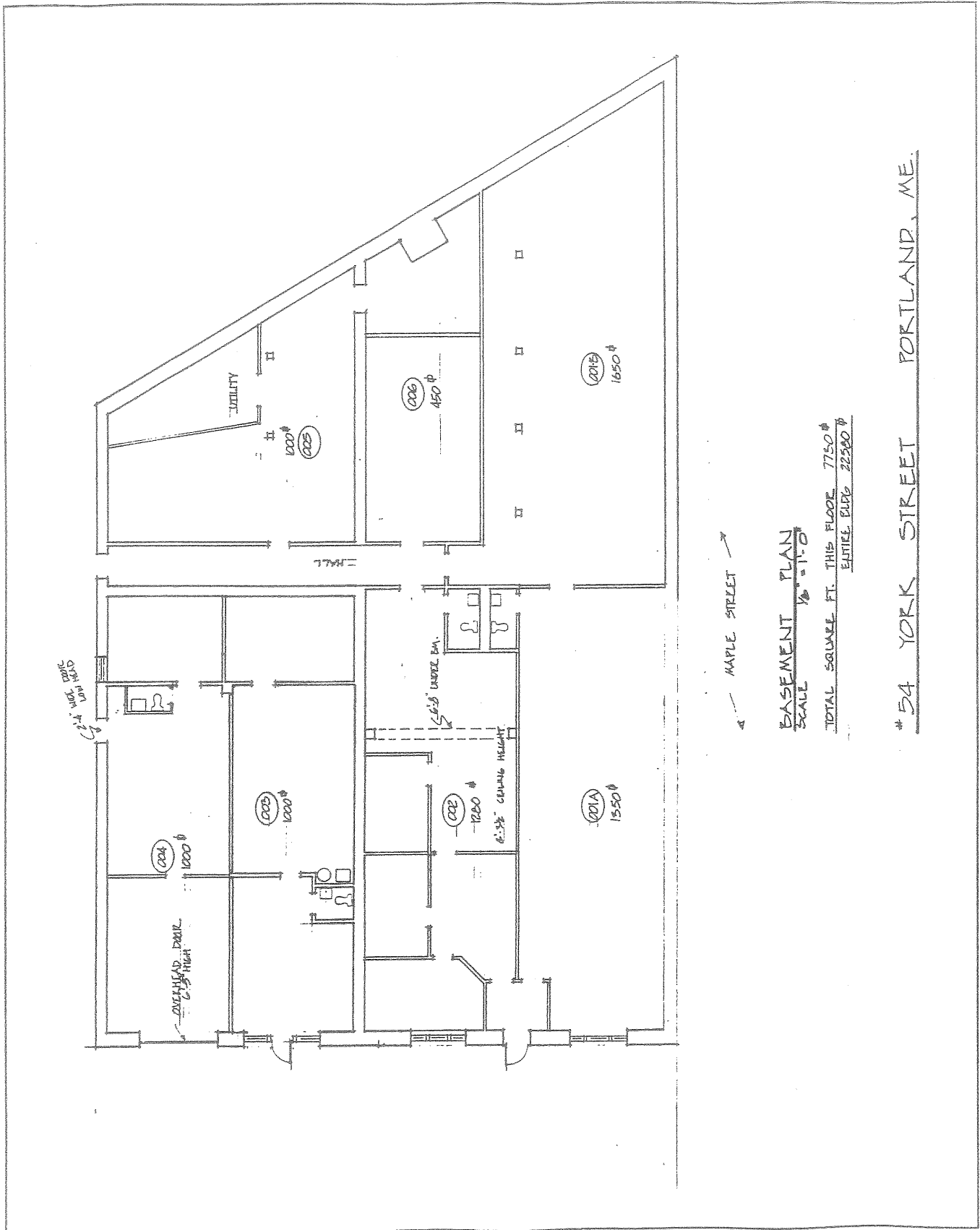
Sprinkler System

The building is provided with a dry type sprinkler system.

Overall Condition

The overall condition of the improvements is rated as average to good.

SUBJECT IMPROVEMENTS SKETCH - Basement Level
(not to scale)



MAPLE STREET →

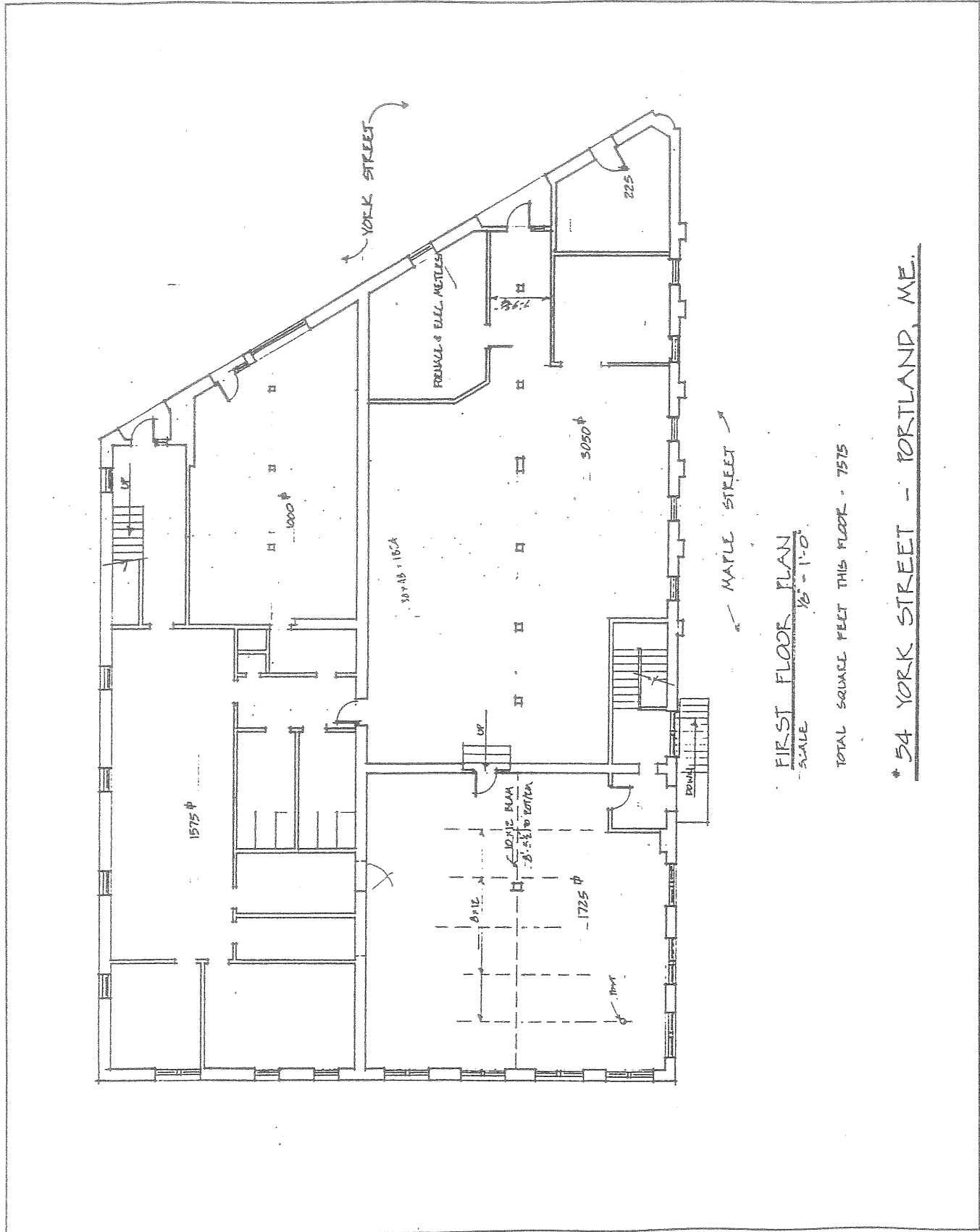
BASEMENT PLAN
SCALE 1/8" = 1'-0"

TOTAL SQUARE FT. THIS FLOOR 7750 sq ft
ENTIRE BLDG 22500 sq ft

54 YORK STREET PORTLAND, ME.

SUBJECT IMPROVEMENTS SKETCH - First Floor Level
(not to scale)

8.3

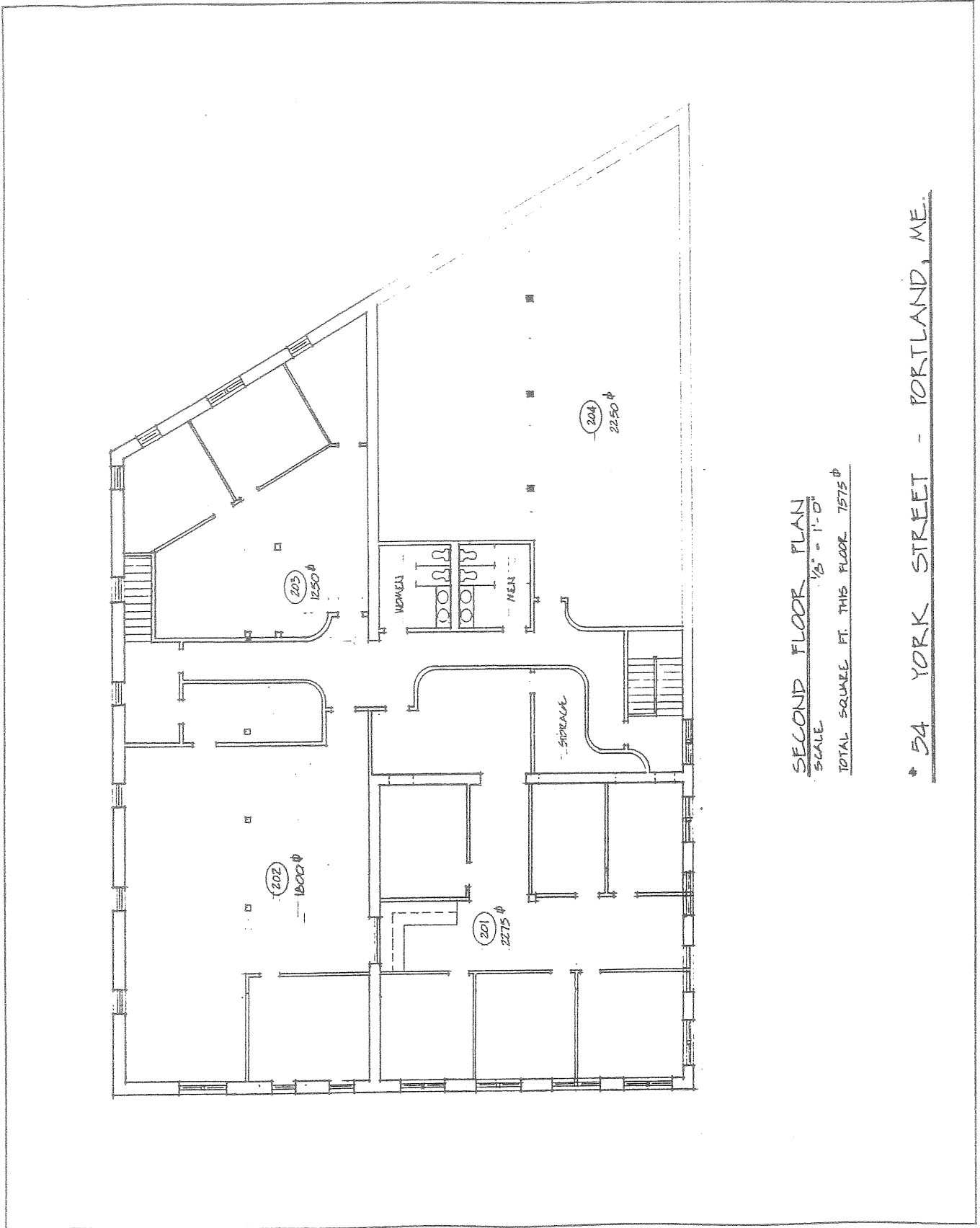


FIRST FLOOR PLAN
SCALE 1/8" = 1'-0"

TOTAL SQUARE FEET THIS FLOOR - 7575

* 54 YORK STREET - PORTLAND, ME.

SUBJECT IMPROVEMENTS SKETCH - Top Floor Level
(not to scale)



SECOND FLOOR PLAN
SCALE 1/8" = 1'-0"
TOTAL SQUARE FT. THIS FLOOR 7575 sq. ft.

54 YORK STREET - PORTLAND, ME.

SURVEYOR'S REPORT

Fill out and sign with all Surveys in accordance with the Instructions on the reverse side.

To: LAWYERS TITLE INSURANCE CORPORATION

Richmond, Virginia.

THIS IS TO CERTIFY, that on December 3, 19 87, I made an accurate survey of the premises standing in the name of William J. Dowd

situated at Portland Cumberland Maine City County State

briefly described as: Commercial Structure

and shown on the accompanying survey entitled: Standard Boundary Survey for William J. Dowd

I made a careful inspection of said premises and of the buildings located thereon at the time of making such survey, and again on December 17, 19 87, and at the time of such latter inspection I found William J. Dowd to be in possession of said premises as owner

(tenant) or (owner)

I further certify as to the existence or non-existence of the following at the time of my last inspection:

1. Rights of way, old highways, or abandoned roads, lanes or driveways, drains, sewer, water, gas or oil pipe lines across said premises (Include any such matters shown on the recorded plat of subdivision): 15 foot Right of Way to land now or formerly of Rufus Deering Location of sewer connection uncertain

2. Springs, streams, rivers, ponds, or lakes located, bordering on or running through said premises: None observed

3. Cemeteries or family burying grounds located on said premises. (Show location on plat): None observed

4. Telephone, telegraph or electric power poles, wires or lines located on, under, overhanging or crossing said premises and serving other property or properties: None observed above ground below grade lines uncertain

5. Joint driveways or walkways; party walls or rights of support; porches, steps or roofs used in common or joint garages: 15 foot strip above mentioned apparently owned by Dowd subject to use by abutter.

6. Disputed boundaries, encroachments or overhanging projections. (If the buildings, projections or cornices thereof, or signs affixed thereto, fences or other indications of occupancy encroach upon or overhang adjoining properties or easement areas, or the like encroach upon or overhang surveyed premises, specify all such): Encroachment of building onto 15 foot Right of Way, Sign overhangs York Street, Fire Escapes overhang Maple Street, Fence of Abutter encroaches on said property.

7. Physical evidence of boundary lines on all sides. (Be specific): Iron pins found at two corners (see plan)

8. Is the property improved? Yes (a) Building is: Brick (X); Clapboard (); other () Specify (b) Building is: One story (); Two story (x); split-level (); other () Specify

9. Indications of building construction, alterations or repairs within recent months: Exterior brick cleaned, repainted, new doors

(a) If new improvements under construction, how far have they progressed? 50 per cent completed more or less

10. Changes in street lines either completed or officially proposed: ing York Streetline City in progress of determin-

(a) Are there indications of recent street or sidewalk construction or repairs? None

11. Are all abutting streets or roads maintained by public authorities? Yes

(a) Is access to such streets or roads limited? NO

12. If the surveyed premises are subject to restrictive covenants, do the improvements, use and occupancy comply with such? (If the premises are subject to restrictive covenants, have the examining attorney furnish you verbatim copy of them) Note: Origination Deed 1555/24 requires Grantee to build and maintain fence. Existing fence appears to have been constructed by abutter.

William A. Knappfield Jr. Civil Engineer or Surveyor

NOTE: In all cases where there are encroachments, easements, party walls, etc., they should also be denoted upon the map of your survey. Also, be certain map complies with Instructions on reverse side. Particular attention is directed to Paragraph 3 of these instructions.

(Be sure to answer each item. If the property is not subject to any such of items 1 to 6 inclusive, insert the word "none" following the item.)

BEAGLE, PEARCE & RIDGE, LLC
ATTORNEYS AT LAW

C. ALAN BEAGLE
MICHAEL J. PEARCE
MARTIN J. RIDGE
JACQUELYN M. KURZ
PAULA M. CRAIGHEAD

24 CITY CENTER
P.O. BOX 7044
PORTLAND, MAINE 04112
(207) 773-1751
FAX (207) 775-3382

November 30, 1998

Mr. William M. Moody, Jr., President
Rufus Deering Company
383 Commercial Street
P.O. Box 880
Portland, ME 04112-0880

Sally Deering
Bo

Re: Rufus Deering Company/Peoples Heritage Bank/York
Street, Inc.

Dear Bill:

Enclosed please find the following original documents:

1. Warranty Deed from York Street, Inc. to Rufus Deering Company. This Warranty Deed has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 13.
2. Assignment of Leases and Security Deposits from York Street, Inc. to Rufus Deering Company. This Assignment has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 36.

If you have any questions, please feel free to contact me.

Very truly yours,

Robyn A. Pearce

Robyn A. Pearce
Assistant to Michael J. Pearce

enclosure

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.
By: Werner Schnetzer
Werner Schnetzer
Its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,
Paul S. Bunker
Notary Public/Attorney-at-Law
Print Name: Paul S. Bunker

122895/PSB/SKLF
91401.4926

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4: 48

CUMBERLAND COUNTY

John B. O'Brien

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4: 52

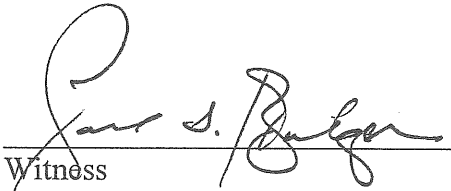
CUMBERLAND COUNTY

John B O'Brien

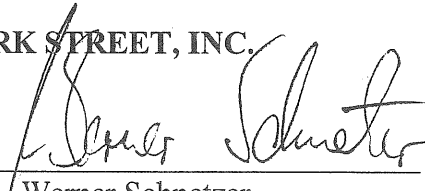
ASSIGNMENT OF LEASES AND SECURITY DEPOSITS

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04106, for consideration, does hereby **ASSIGN** to **RUFUS DEERING COMPANY**, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, its successors and assigns, all its right, title and interest in and to any and all leases, subleases and/or tenancies of the premises located on or about 54-62 York Street in the City of **PORTLAND**, County of **CUMBERLAND** and State of **MAINE**, and more particularly described on **EXHIBIT A** attached hereto, including, without limitation, any and all tenant security deposits relating thereto.

IN WITNESS WHEREOF, the said **YORK STREET, INC.** has caused this instrument to be signed on its behalf by Werner Schnetzer, its duly authorized President, on this 1st day of September, 1998.



Witness

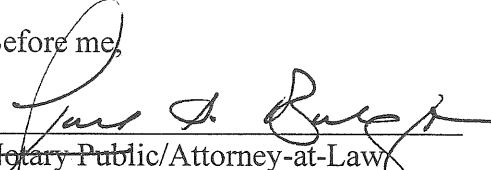
YORK STREET, INC.
By: 

Werner Schnetzer
Its: President

STATE OF MAINE
CUMBERLAND, SS.

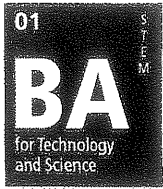
September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of **YORK STREET, INC.** and acknowledged the foregoing instrument to be his free act and deed in said capacity and the free act and deed of said **YORK STREET, INC.**

Before me,


Notary Public/Attorney-at-Law
Print Name: Paul S. Bulger

122937/PSB/SKLF
91401.4926



Baxter Academy
for Technology and Science

January 9, 2013

John Peverada
Parking Manager
City of Portland
Portland, ME 04101

LETTER OF INTENT TO LEASE SPRING STREET GARAGE

Dear Mr. Peverada,

On behalf of Baxter Academies of Maine, I would like inform you of our intent to lease 15 parking spaces in the Spring Street Garage beginning August 1, 2013.

This letter is to acknowledge that we understand that these spaces are available now and you will inform us and give us the chance to rent earlier if the number of available spaces falls below 15. I understand that these spots will not be held if we fail to act upon the notification with in (7) business day.

Best regards,

A handwritten signature in black ink that reads 'John Jaques'.

John Jaques

Executive Director


A handwritten signature in black ink that reads 'John Peverada'.

John Peverada

Parking Manager City of Portland



Memorandum

Project: 07383
To: Shukria Wiar
From: Steve Sawyer, P.E., Vice President, Transportation Services 
Date: January 9, 2013
Subject: Baxter Academy for Technology and Science - Traffic Signage Plan
cc: Dan LaBrie, John Jaques

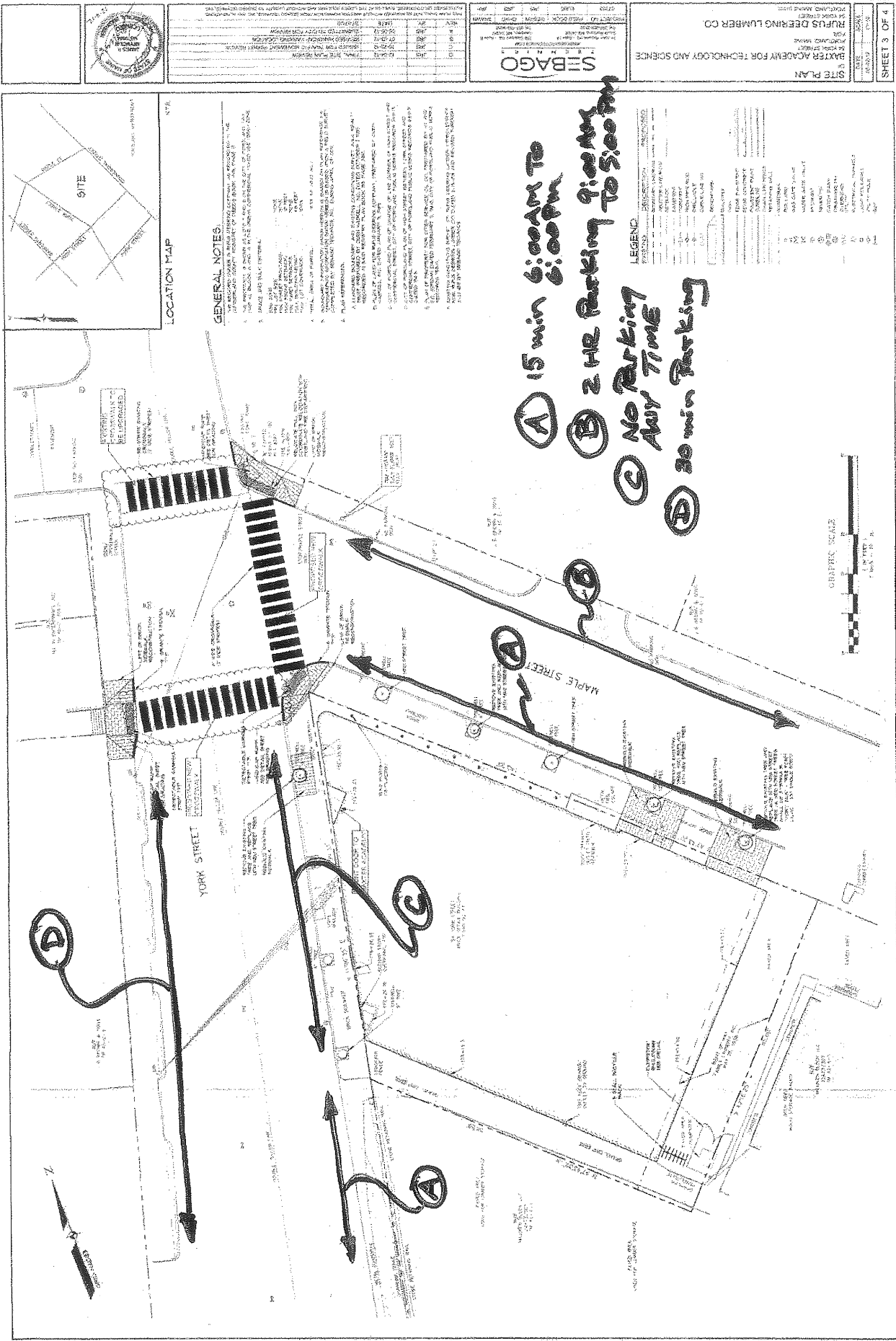
Per your email request of January 4, we are hereby submitting our proposed Traffic Signage Plan. I have attached Figure 1 that shows existing signage around the perimeter of our site. In our opinion, the only change to what exists today would be the addition of two "School Bus Drop Off Zone" signs in addition to the existing "No Parking" signs in front of the school on the south side of York Street.

The 15 minute parking on the west side of Maple Street is appropriate for parent drop off and pick up as planned, and the two 15 minute parking spaces to the west of the school on the south side of York Street are appropriate for parent drop off or pick up, as well. The existing 30 minute spaces on the north side of York Street are generally unoccupied during the times that they may be needed by parents for picking up and dropping off, so we see no need to change these restrictions.

Let us know if you require any further information from us in this regard.

SSS:sss/kn
Att.

FIGURE 1



NO.	DATE	DESCRIPTION
1	10/1/01	PRELIMINARY
2	10/1/01	REVISION
3	10/1/01	REVISION
4	10/1/01	REVISION
5	10/1/01	REVISION
6	10/1/01	REVISION
7	10/1/01	REVISION
8	10/1/01	REVISION
9	10/1/01	REVISION
10	10/1/01	REVISION



SITE PLAN

RIFUS DEERING LUMBER CO.
BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE

SHEET 3 OF 4



Memorandum

Project: 07383
To: Shukria Wiar
From: Steve Sawyer
Date: January 3, 2013
Subject: Baxter Academy for Technology and Science
– Sidewalk Waiver Request
CC: Dan LaBrie, John Jaques

Steve

This memorandum is a follow up to our meeting with you and the staff of Public Services on December 11, 2012. At this meeting it was explained that if the applicant did not agree with staff's recommendation to construct a new sidewalk on the south side of York Street between High Street and Maple Street that we should apply for a "sidewalk waiver" under the provisions Section 14-506 (b). Under this provision in the City's Ordinances, the Portland Planning Board may grant a waiver from the requirement for a sidewalk providing two or more conditions listed in the Ordinance are met. We have prepared the following to satisfy these guidelines.

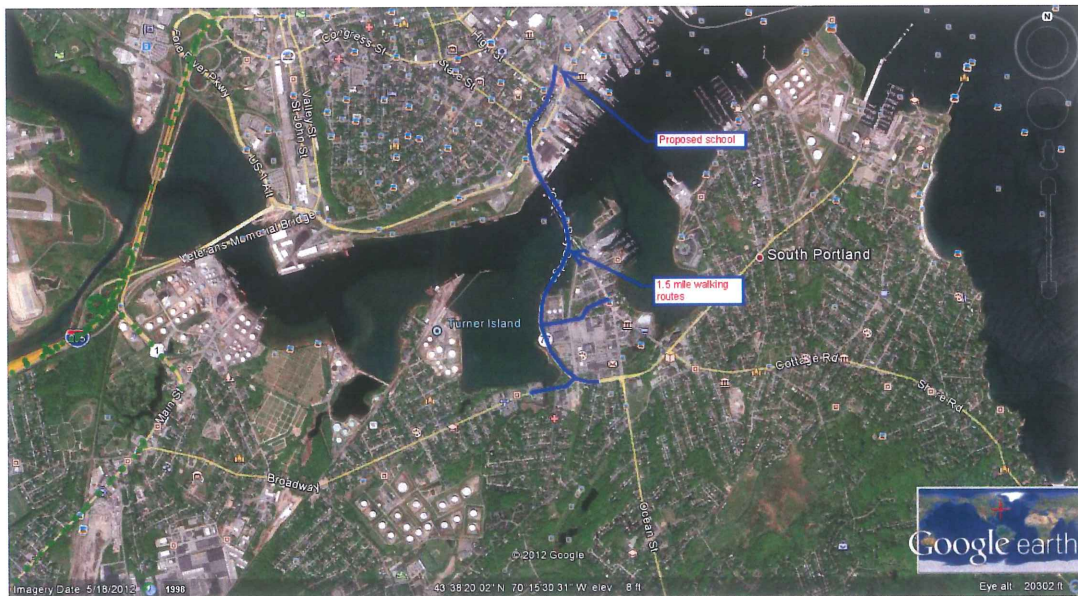
Sidewalks Condition 1 - There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site.

We provided you as part of a December 3, 2012 memo our viewpoint on the usage of a sidewalk on the south side of York Street for the proposed school. This explanation is repeated here for the Planning Board's reference.

Baxter Academy has presented an enrollment profile as part of its application materials. In Year 3 (full build-out) there is an expectation because of its urban setting that as many as 16 students and staff will walk to school. If we look at a map, see Google image below, we see that Portland's closest residential neighborhoods (within 0.75 miles) are all located on the north side of York Street in the general direction of the West End. There is an existing sidewalk on the north side of York Street that is available to walkers originating from these neighborhoods, and the school is proposing to upgrade the crosswalks in front of the school to permit safe crossings of York Street at the school's main entrance.



Who then would use a sidewalk if it were to be constructed on the south side of York Street from High Street to Maple Street? We have heard that it would be students and staff crossing the Casco Bay Bridge. If this is the case let's look at the route this would involve. From the Google image below you can see that to access the nearest residential neighborhoods in South Portland involves a walking route that would exceed 1.5 miles. In our view, it is unlikely that students would walk in excess of 1.5 miles across the bridge to get to and from school on a regular basis. Therefore, we do not believe that the Baxter Academy will be attracting walkers from South Portland given the distance involved. As such, we do not see the nexus between the school and the continuation of the sidewalk on the south side of York Street to High Street. Without one, we believe it is unreasonable to burden our project with this expense.



Condition 3 - A safe alternative-walking route is reasonably and safely available, for example, by way of a sidewalk on the other side of the street that is lightly traveled.

See the response to Condition 1 above - there is an existing sidewalk on the north side of York Street in this location.

In addition to a "sidewalk waiver", the applicant has also reviewed the conditions for satisfying a "curbing waiver" in the Ordinance. Here we find that Condition 1 refers to the cost to construct the curbing as being less than 5% of the overall project cost. The City has filed a MaineDOT grant application for the sidewalk on York Street in the amount of \$110,000. At our meeting on December 12, we were presented an estimate of \$85,000 for this construction, not including survey and engineering. Furthermore, staff requested that the applicant be required to construct a new brick sidewalk on Maple Street from its project to Commercial Street at an estimated cost of \$13,000. Combined, the staff is asking for sidewalk construction that is estimated by their figures to cost \$98,000 to build, not including survey and engineering. In our view, the total cost is more like \$120-125,000. The cost of the proposed project is expected to run in the range of \$185,000, which would put the requested sidewalk costs at 65-68% of the total project cost, which is far greater than the Ordinance figure of 5%. This expense seems considerably out of proportion and supports our position for a waiver of these requirements.



Memorandum

Project: 07383

To: Shukria Wiar

From: Steve Sawyer *Steve*

Date: January 10, 2013

Subject: Baxter Academy for Technology and Science
Summary Package for Traffic Movement Permit (TMP)

CC: Dan LaBrie, John Jaques

In preparation for our upcoming meeting before the Planning Board in connection with the proposed school, we have put together the following chronology of events leading up to this point with regard to the TMP process. Attached to this memo are all related and relevant documents.

October 23, 2012 - Submission of Section 7 of the TMP application. This document presented a revised traffic pattern in front of the school based on feedback received from your traffic engineer. Student pick-ups and drop-offs were buses on the west side of Maple Street and parents on the north side of York Street.

November 27, 2013 - Planning Board (PB) Workshop. The Planning Board offered some suggestions on the location of the student pick-ups and drop-offs, which we thought were improvements to our plan so we modified our proposal to have buses load and unload in front of the school on York Street in front of the school and have parents load and unload on the west side of Maple Street. Furthermore, a suggestion was made to add a crosswalk across York Street in front of the school that would facilitate walkers or students that might be dropped off on York Street to safely cross the street.

December 3, 2012 - Response memo to City following PB Workshop. This document revised our student loading and unloading plan, included at Traffic Operations and Management Plan, included a TDM Plan, and addressed a number of other comments that had been raised at the meeting.

December 11, 2012 - Meeting with City Public Services Staff. This meeting was to discuss our position related to the need for constructing a sidewalk on the south side of York Street up to High Street to serve the school. We disagree on this being the school's responsibility and subsequently submitted a sidewalk waiver request. The other matter discussed at this meeting was the need for us to gain approval from the City's Crosswalk Committee for the new crosswalk across York Street in front of the school.

January 3, 2013 - Meeting with City's Crosswalk Committee. We received approval from this Committee for the proposed crosswalk across York Street in front of the school.

January 3, 2013 - Submission of Sidewalk Waiver Request.

January 9, 2013 - Submitted Traffic Signage Plan.

January 10, 2013 - Submission of this Summary Package, which includes a commitment letter to lease off-site parking spaces for use by school staff.

To our knowledge this package contains all current and relevant information associated with the proposed school project from a traffic and parking perspective. Site plan information is contained in a separate transmittal.

Attachments:

October 23, 2012 - Submission of Section of the TMP Application

December 3, 2012 - Response Memo to City following PB Workshop

January 3, 2013 - Submission of Sidewalk Waiver Request

January 9, 2013 - Submitted Traffic Signage Plan

January 10, 2013 - Commitment letter to lease off-site parking spaces for use by school staff.



Memorandum

Project: 07383
To: Tom Errico, Shukria Wiar
From: Steve Sawyer
Date: December 3, 2012
Subject: Baxter Academy for Technology and Science TMP
cc: Dan LaBrie, John Jaques

This memorandum is a follow up to our meeting with the Portland Planning Board in a Workshop Session on November 27, 2012 and in response to a November 20 review memo by Tom Errico on our previous Section 7 submittal dated October 23, 2012.

Item 1 – Student drop-off and pick-up locations. The Planning Board and Mr. Errico have raised concerns over the proposed location for student drop-offs by parents being on the north side of York Street opposite the School’s entrance. Under this scenario, students would need to cross traffic to get to the school. Furthermore, during the winter months the sidewalk on the north side of York Street may not always be cleared by the City at the time of school arrivals and snow banks may exist. Since the School would not control this sidewalk area, students would have no safe place to go but into the street once they get out of their parent’s cars. For these reasons, it was suggested that we reconsider our Plan to include locating the bus drop-off on York Street in front of the School’s entrance and direct parents to use the west side of Maple Street adjacent to the existing sidewalk abutting the School. In this manner no one would need to cross any street to get into and out of the School. The matter of the buses stopping traffic on York Street during loading and unloading was discussed but the Board felt that since there were only three buses this would be less overall delay to traffic on York Street than what was previously proposed and the situation would be much safer. **ACTION: We agree with this rationale and will modify our Traffic Operations Plan accordingly.**

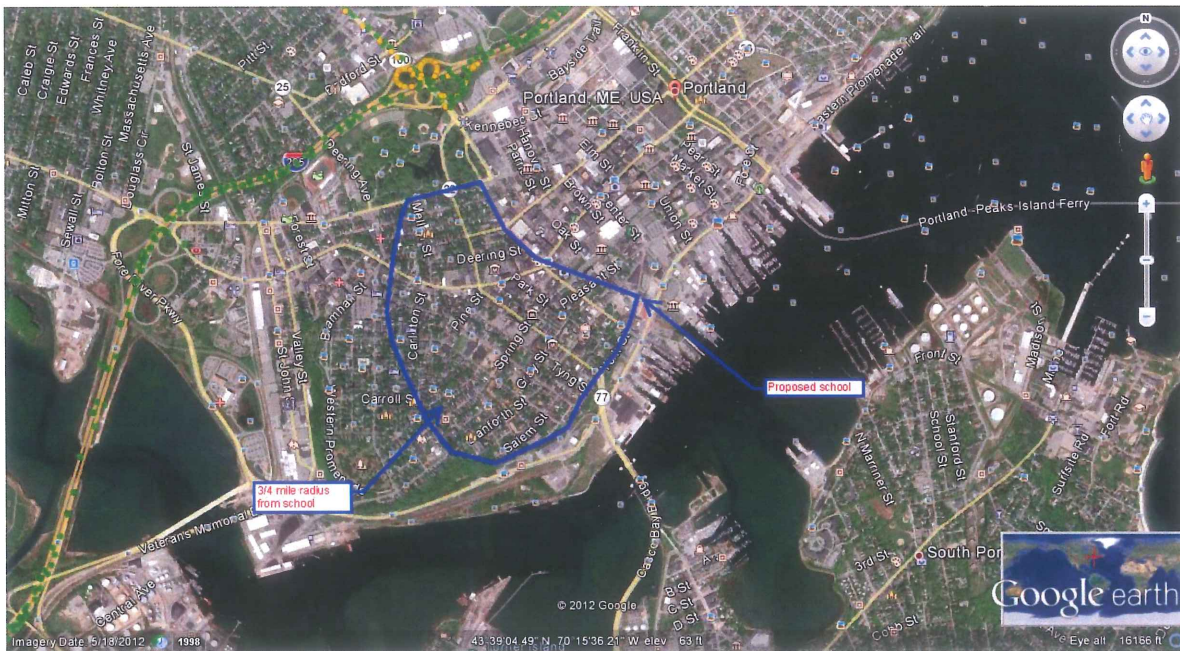
Item – Provide a TDM Plan for the Project. ACTION: A TDM Plan is attached.

Item 3 – Off-Site Parking. The School will enter a lease arrangement with either the City’s Spring Street garage or with the Holiday Inn By The Bay for a limited number of spaces for staff. Space is readily available in both of these locations presently. It is premature, though, for the School to consummate these arrangements until they hire the required staff and know where they all live and what their need for parking may be. **ACTION: Nothing further until more is known about the staffing.**

L.1

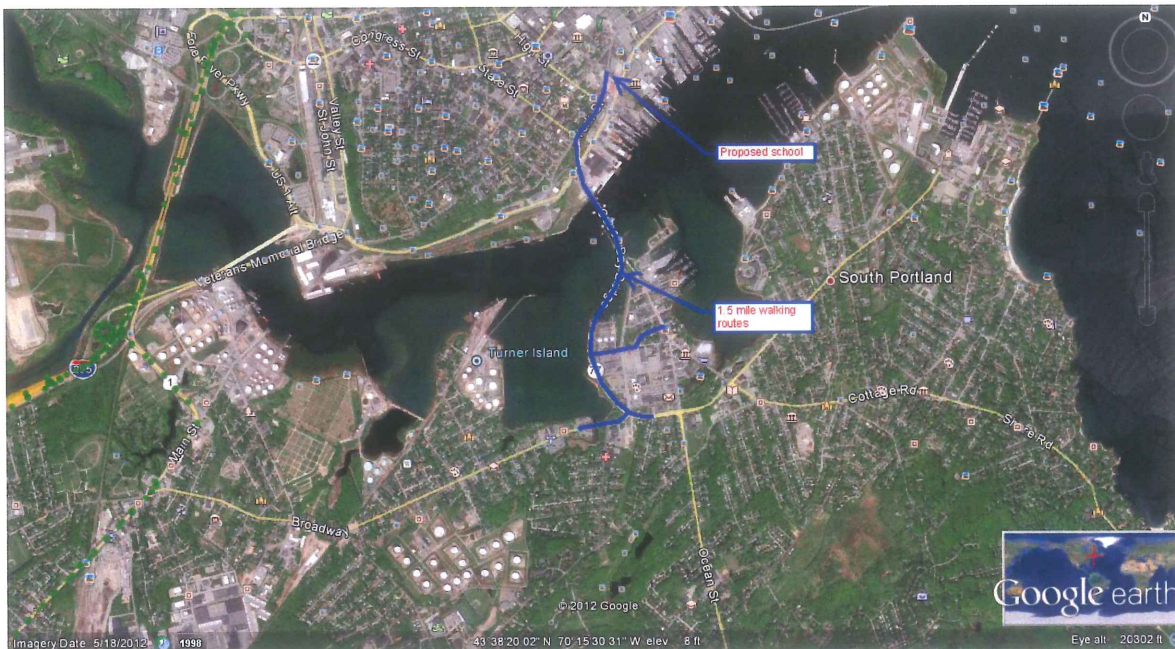
Item 4 – The sidewalk on the south side of York Street from High Street to the School site. This is not a simple matter as has been discussed with the Planning Board at our workshop meeting. We understand that the City has filed a Quality Community Grant application with MaineDOT for funding for this particular improvement, which has been estimated at \$110,000. In speaking with the Project Manager of this program at MaineDOT, we learned that the likelihood of this project receiving funding support as part of MaineDOT’s 2014-15 Work Plan is slim at best. This project is only one of 10 submitted by the City and the statewide competitiveness for these funds is extremely fierce. Therefore, this \$110,000 matter needs further discussion, because the proposed non-profit, non-tuition, public high school cannot absorb this cost.

Baxter Academy has presented an enrollment profile as part of its application materials. In Year 3 (full build-out) there is an expectation because of its urban setting that as many as 16 students and staff will walk to school. If we look at a map, see Google image below, we see that Portland’s closest residential neighborhoods (within 0.75 miles) are all located on the north side of York Street in the general direction of the West End. There is an existing sidewalk on the north side of York Street that is available to walkers originating from these neighborhoods, and the school is proposing to upgrade the crosswalks in front of the school to permit safe crossings of York Street at the school’s main entrance.



Who then would use a sidewalk if it were to be constructed on the south side of York Street from High Street to Maple Street? We have heard that it would be students and staff crossing the Casco Bay Bridge. If this is the case let’s look at the route this would involve. From the Google image below you can see that to access the nearest residential neighborhoods in South Portland involves a walking route that would exceed 1.5 miles. In our view, it is unlikely that students would walk in excess of 1.5 miles across the bridge to get to and from school on a regular basis. Therefore, we do not believe that the Baxter Academy will be attracting walkers from South Portland given the distance involved. As such, we do not see the nexus between the school and the continuation of the sidewalk on the south side of York Street to High Street. Without one, we believe it is unreasonable to burden our project with this expense.

L.2



Item 5 – Maple Street parking restrictions for student unloading and loading. Currently the west side of Maple Street is designated 15 minute parking from 6:00 AM to 6:00 PM. We contacted John Peverada and discussed this matter with him as well as the need for erecting 5 minute School Drop-Off Signs adjacent to the School site similar to those posted in front of Portland High School on Cumberland Avenue. He directed us to the City’s Traffic Division. Please advise us on how to proceed with this matter.

Item 6 – Crosswalk Pavement Markings. Our plans have been changed to show block style crosswalk markings for the intersection of York and Maple Streets.

Item 7 – Bicycle Racks. Our previous plan showed 5 bike racks in the rear alley and 5 bike posts along Maple Street in the sidewalk for a total of 15 spaces. Per the reaction of the Planning Board, we have increased the number of bike posts from 5 to 9 bringing the total spaces to 23.

Item 8 – Handicapped Parking Spaces on York Street. No HC spaces are proposed on York Street.

Item 9 – Intersection of High and Commercial Streets. This intersection was identified in our Section 7 materials as a High Crash Location. However, our TMP application is for less than 200 pce’s, so our Study Area should be limited to the immediate driveway or intersection, i.e. York and Maple Streets. We do not believe we should be subject to any contribution towards a safety concern at the High/Commercial Street intersection given that it does not border our site and any impacts from our project on this location will be very minimal.

We look forward to our meeting with you and Public Services on the 11th to bring this TMP application to closure before we meet again with the Planning Board. Thank you in advance for your cooperation.

Attachments:

Updated Site Plan

Updated Traffic Operations and Management Plan

TDM Plan

**Baxter Academy for Technology and Science
Traffic Operations and Management Plan
Revised November 28, 2012**

Arrivals and departures from the Baxter Academy are to be well-orchestrated and staff managed events. Because students will be arriving via various modes of transportation the School's Plan includes a number of elements. A key one will be that a majority of the students attending school at Baxter will be able to exit their transportation mode directly onto sidewalks adjacent to the school and maintained by the school during the winter months, so student safety is maximized.

Arrivals

The three charter buses will arrive at school and park on the south side of York Street heading in town directly in front of the school. Students will depart onto the existing York Street sidewalk in front of the main entrance to the school. Buses will be equipped with school flashing lights, so traffic on York Street will be stopped in both directions during this operation. The unloading for each bus is estimated to be 2 minutes. School staff will ensure that the three buses do not overly interrupt operations on York Street during the student discharge operation. This should not be an issue as the three buses will not likely arrive at the same time.

Students using public transit (Metro or South Portland Bus) will depart buses at existing stops on the north side of York Street at either High Street or Fore Street. Students will then proceed to school on existing sidewalks and cross York Street using the upgraded crosswalks at the intersection of York and Maple Streets. The School will have a staff person supervising these crosswalks for the 45 minutes preceding school's commencement to ensure safe student passage.

Parent drop-offs will occur on the west side of Maple Street adjacent to the existing sidewalk abutting the school. A staff person will supervise this operation to keep vehicles orderly, and students will walk up to York Street where they will enter the front door of the school.

Staff members who elect to drive their own vehicles to school will be parking in a nearby off-site lot or garage and will walk to school via existing sidewalks.

Students who elect to drive their own vehicles to school will likely park on Commercial Street in the unrestricted parking spaces adjacent to the end of Maple Street and will walk to school via existing sidewalks.

Students who elect to ride their bikes to school will be able to store their bikes in bike racks, which will be provided by the school adjacent to the building on Maple Street and in the back alley.

In addition to the staff members on-site managing the student arrival process, the school proposes to:

- place a School Crossing sign in the middle of the York and Maple Street crosswalks in front of the school to keep speeds on York Street under control during peak student usage.
- erect School Drop-Off Zone signs on west side of Maple Street for parent drop-offs.

See examples below.



“Crosswalk sign used by Waynflete”



“School Drop-Off Zone Sign at Portland High”
Departures

Charter buses will arrive at school and park on the south side of York Street, either directly in front of the school entrance or in the two parking spaces to the west of the school if they arrive early. At 2:00 PM when regular school is over students who are not participating in after school programs will load the buses from in front of the School as they did during the morning arrivals. The school buses will activate their school flashers during the loading operations, so traffic on York Street will be halted during these times.

Students using public transit (Metro or South Portland Bus) will depart the school for the bus stops on the north side of York Street at either High Street or Fore Street depending on individual choice and route schedules. Staff will be present to aid in crossing the street for these students.

Parent pick-ups will occur on the west side of Maple Street adjacent to the existing sidewalk. The schedule of these pick-ups will not be as concentrated as in the morning because of after school activities. The school is projecting that up to one half of students will be participating in after school programs, which would mean that parent pick-ups in Year 3 would be in the range of 34 at 2:00 PM and the other 34 would be spread over the subsequent 2 hours. Given the curb space adjacent to the school site this does not appear to be an issue.

Food Deliveries

Food service to the school will be provided by an off-site vendor. Deliveries will be made curbside on the west side of Maple Street, similar to where the parents will load and unload.

Proposed Signage Changes

Maple Street: The west side of the street is currently posted as 15-minute parking. We believe this would need to be modified to allow for parent drop-offs and pick-ups between 7:30 and 8:00 AM and between 1:45 and 2:15 PM. We have spoken with John Peverada on this matter and he saw no issue, but directed us to the City's traffic division.

York Street: No signage changes are anticipated on York Street.

On-Going Plan Refinement

The Baxter Academy realizes that their Traffic Management Plan is subject to individual student family preferences from year to year. As such, the school administration is committed to refining their operation each year to reflect the modal choices made by their enrollees. Given that Years 1 and 2 are ramping up years in terms of the student population, the school will have adequate time to fine-tune operations prior to reaching the ultimate goal of 320 students in Year 3.

**Baxter Academy for Technology and Science
Transportation Demand Management (TDM) Plan
November 29, 2012**

1. Transportation Narrative

The Baxter Academy for Technology and Science is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. The school's vision is to create a full 4-year high school over the next 3 years in the existing building at 54 York Street - the corner of York and Maple Streets. Total enrollment is planned for 320 students by 2015. In the opening Year (2013), 160 - first and second year students are envisioned. In Year 2, Grades 9, 10, and 11 will be provided, and in year three all four years of high school will be offered. It is estimated that 50% of the enrollment will come from Portland, South Portland and Westbrook, with the balance originating from communities outside of the urban core. School operations will run from 8:00 AM to 2:00 PM, resulting in peak traffic associated with the school being between 7-8:00 AM and 2-3:00 PM.

The School will manage the transportation and parking impacts of its employees and students through a continuous and consistent distribution of information. This message will highlight the variety of transportation options available to students and staff other than utilizing single occupant vehicles as their primary means of access to and from school. These options will include the provision of charter buses for students and staff living outside the urban core serviced by METRO or South Portland Bus services, encouraging car pooling for families that choose to transport their children to school, providing incentives for students and staff serviced by local transit, not providing parking for students as a discouragement to driving their own vehicles, and encouraging biking and walking to school for those within a reasonable distance from the facility. All of these strategies are aimed at reducing the traffic and parking impacts associated with the proposed facility.

2. TDM Coordinator

John Jaques, Founder/Executive Director of Baxter Academy, will serve as the TDM Coordinator. He can be reached at 54 York Street, Portland, Tel. (207) 318-8248, or at john@baxteracademy.org.

3. Parent and Teacher Surveys

One of the first steps taken by the School's administration in formulating their Transportation Plan was to reach out to prospective student families via a SurveyMonkey survey aimed at gauging transportation preferences. Much useful information was gained from this effort. Recognizing, though, that attitudes shift from year to year the School is committed to resurveying their population annually in conjunction with the registration process as a means of informing families of their transportation options and encouraging their participation in both trip reduction and parking reduction strategies given the location of the School's facility within Portland's downtown district. This survey will be accompanied by an information packet that will include relevant information on School-sponsored charter buses for the outlying areas; transit routes within the urban core with the School's subsidy program described; car pooling options and information about the School-sponsored ride-matching program; bicycling and walking options; and information on the lack of free parking in the immediate area of the school.

4. Parking and Trip Reduction Targets

ITE's Trip Generation, 7th Edition, Land Use Code 530 - High School, was consulted to determine industry standards for typical trip generation associated with facilities of this nature. Using a base school population of 320 students and 30 employees (representing full build out in Year 3), it is expected that AM trip generation would be 154 trips. Similarly, in Year 1 with 160 students and 15 employees this figure is estimated to be 77 trips. The School has established a target of 137 trips in Year 3 and 73 in Year 1 based on its anticipated enrollment and its Transportation Plan. This represents a reduction from published industry standards of 5% in Year 1 and 11% by Year 3.

ITE's Parking Generation, 3rd Edition, Land Use Code 530 - High School (Urban), was consulted to determine industry standards for typical parking associated with facilities of this nature. Unfortunately, this document only had three samples and they were all of much larger school populations, and thus this data is suspect for our purposes. In any event, using this published data and the expected school enrollments of 320 in Year 3 and 160 in Year 1, the parking requirements are estimated to be 29 in Year 3 and 14 in Year 1. The School's policy will be to provide only a limited number of off-site parking spaces for its staff and none for its students. This will be made clear in the School's registration materials as a discouragement to student drivers.

5. Parking and Trip Reduction Strategies

The School's parking reduction strategy is not to provide any off-site parking for its students. This should not be an issue in the initial year of operation, since the enrollment will only be 9th and 10th graders, which rarely have driver's licenses. In the later years, students may elect to use their cars for transportation, but "free" parking in the proximity of the school will be at a premium and should be a deterrent. For those students who want to bring their own vehicles to school, they will be required to register them with the school and park legally within the surrounding area. This is similar to Waynflete's policy. The school will monitor these situations, and work with the City to address any violators should they be brought to the school's attention.

As has been stated previously, trip reduction strategies employed by the School will include:

- Providing charter buses for outlying communities
- Encouraging the use of public transit within the urban core with subsidies
- Encouraging and facilitating car pooling/ride sharing
- Encouraging bicycling through the provision of on-site bike racks
- Encouraging walking by upgrading crosswalks in the vicinity of the School

6. Education

Registration materials provided by the School to prospective families will include transportation information about the various options that are available to students depending on their location of residency. Annual surveys will be conducted by the School to gauge family preferences, so that adjustments can be made in the various programs if need be. It will be made clear in these materials that the School is committed to supporting the City's goals for transportation and environmental sustainability within its urban core by discouraging single occupant vehicle usage and promoting the use of public transportation, ride sharing, biking and walking.

The School will operate its own ride matching program for families that would like to car pool as part of dropping-off and picking-up students on a regular basis. Information on this program will be included in the School's newsletters that go out to families.

For those students that can avail themselves of the METRO and South Portland Bus services, the School will provide a 50-cent subsidy per ride to incentivize the use of this mode of transportation.

7. Monitoring

Baxter Academy's TDM Plan will be monitored by its TDM Coordinator - the Executive Director. This effort will include:

- Annual family surveys regarding school transportation preferences
- Ridership figures on School-sponsored Charter Buses
- Public Transit usage through subsidy information
- The number of families participating in the School's ride-share/matching program
- The number of students biking to school
- The number of students registering their private vehicles with the School
- The number of staff utilizing off-site parking
- The number of staff utilizing public transportation or other travel options

This data will be compiled and compared against the targets established for both parking and vehicle trip production contained in Item 4 above. A summary report will be produced annually and submitted to the City's TDM Manager for review and comment.

8. Project Specific Standards

Infrastructure:

Public Transit Access - METRO and South Portland Bus have existing bus stops on the north side of York Street east and west of the School - at Fore and High Streets. Improved crosswalks are included in the School's Site Plan to ensure safe and convenient access to these two stop locations.

Bicycle Parking - A total of 23 bike parking stations are being provided initially as part of the School's Site Plan. If demand warrants additional facilities, the School is committed to providing them.

TDM Bulletin Board - School will erect a transportation bulletin board in a prominent space within the School with information on various transportation options, including ride matching possibilities.

TRAFFIC MOVEMENT
PERMIT APPLICATION

SECTION 7

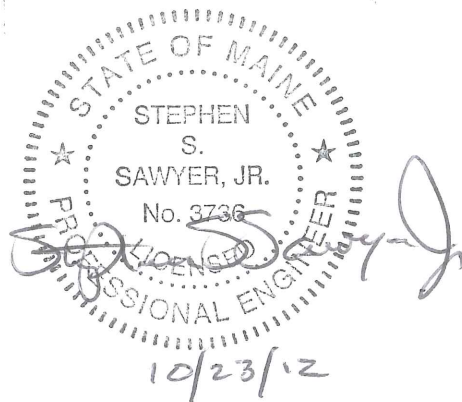


FOR

PROPOSED
CHARTER HIGH SCHOOL
54 YORK STREET
PORTLAND, ME

ON BEHALF OF

RUFUS DEERING LUMBER
COMPANY
384 COMMERCIAL STREET
PORTLAND, ME



OCTOBER 23, 2012

M.1

Section 7. Traffic Study

1. Site Plan.

Rufus Deering Lumber Company, 384 Commercial Street, Portland is proposing to remodel their existing building at 54 York Street (the corner of York and Maple Streets) to accommodate the new Baxter Academy for Technology and Science Charter High School. The current property houses the existing Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses of offices, labs, and a dance studio. This 0.22 acre (9,583 s.f.) lot is fully developed with an existing 7,540 s.f., 3-story building in the City's B-5b (Urban Business) Zone. This property is also located within the Old Port Historic District. See the Site Plan in the back of this submission.

2. Existing and Proposed Site Uses.

The existing building is currently developed with the following land uses:

- An exercise studio offering Zumba classes
- A Buddhist Center
- Administrative offices
- Global Biotech
- Deering Lumber's Kitchen and Millwork Showroom

Once the remodeling is complete the building will accommodate the Baxter Academy for Technology and Science Charter School as the primary tenant. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. The school's vision is to convert the entire building into a full 4-year high school over the next 3 years. Total enrollment is planned for 320 students by 2015. In the opening year (2013), 160 - first and second year students are envisioned. In year two, Grades 9, 10, and 11 will be provided, and in year three all four years of high school will be offered. It is estimated that 50% of the enrollment will come from Portland, South Portland and Westbrook, with the balance originating from communities outside of the urban core. School operations will run from 8:00 AM to 2:00 PM, resulting in peak traffic associated with the school being between 7-8:00 AM and 2-3:00 PM.

3. Site and Vicinity Boundaries.

A regional map showing the development site and its relationship to the adjacent street network as defined in MaineDOT's Chapter 305 of the General Rules of the Department of Transportation (Section 5B) is presented as part of the Site Plan located at the end of this submission.

4. Other Proposed Uses in the Vicinity of the Proposed Development.

A discussion with the City of Portland's Planning Department revealed that there are no other developments currently under review or have been permitted but not built in the vicinity of this Project at this time.

5. Trip generation.

ITE's Trip Generation, 7th Edition, was consulted to determine what industry data suggests for this type of land use. LUC 530 - High School, which includes both public and private institutions was deemed the closest category listed in this reference. Believing that the City's greatest traffic concern would be during the AM peak hour because it will coincide with the morning commute, we have focused our attention on this time period only. Using a base school population of 320 students and 30 employees, we find the following for the AM peak hour using ITE information:

<i>LUC 530</i>	<i>Variable</i>	<i>Total Trips</i>
By Student: $\text{Ln}(T) = 0.77\text{Ln}(X) + 0.69$	320 (students)	169
By Employee: Average Rate = 4.63	30 (employees)	139
Average of Above		154

The above trip generation assumes that there is no existing traffic associated with the site today, which is not entirely true. Currently there is an exercise studio (Nancy's) that offers among other things, Zumba classes, and a Buddhist Center (Nagaloka), and the administrative office for the Baxter School operating in the building. However, given the operating hours of these three uses, we have not taken a credit for any existing trips during the AM peak hour (7:00 AM to 8:00 AM).

From the data above it is estimated that the peak trip generation from the proposed development will be 154 trips (all primary), which will put the project in the Traffic Movement Permit (TMP) category of 100-200 Passenger Car Equivalent (PCE) trips.

6. Trip Distribution.

The ITE Trip Generation Handbook was also consulted with regard to the directional distribution of site generated trips during the AM peak period. ITE data suggests that during the AM peak period trips at the site will be distributed roughly 70% entering and 30% exiting regardless of which method of generation is used, i.e. "student" or "employee".

7. Trip Assignment.

While ITE suggests that traffic generation from the proposed facility could be in the range of new 150 trips in the AM peak hour, the Baxter Academy plans to adopt a series of travel demand management strategies for minimizing this impact on the surrounding street system. Typically, students at four-year public high schools have a variety of transportation choices that must be addressed in the school's planning. These range from parent drop-off and pick-up, to buses, and finally to student drivers with their own cars. The actual mix usually depends on the school's location within the community and the amount of available land surrounding the school. In the case of the Baxter Academy, the setting is very urban with no surplus land for off-street parking. Accordingly, the school's administration has formulated the following Transportation Plan.

First of all, it should be understood that in the initial year of operation, only 9th and 10th graders will be attending the Baxter Academy, so student drivers will not be part of the mix. However, in subsequent years this will be a consideration.

Not knowing exactly what to expect from prospective student families, Baxter Academy’s administration conducted a Parent Survey to gauge their transportation preferences. A total of 24 families participated in this survey, which was performed in June of 2012. See the Appendix. The results indicated that approximately 25% of the families preferred to transport their children to and from school either by individual or car pool means. 70% envisioned using bus transportation, either charter service provided by the school or public transportation because they reside within Greater Portland. The remaining 5% indicated they would walk to and from the school. Before running any numbers on this basis, contact was made with a few similar facilities in the region – the Academy for Science and Design in Nashua, NH – a 410 student charter school serving Grades 6-12 in the business district of Nashua; Waynflete in Portland, a private K-12 school with 550 students in the west end; and Portland High School on Cumberland Avenue with 920 students. Table-1 presents the results from this research.

Table 1 – Modal Choice by Families of 4-year Urban High Schools

<i>School</i>	<i>% Parent Drop-Off</i>	<i>% Transit</i>	<i>% Student Drivers</i>	<i>% Walkers & Bikers</i>
Academy for Science & Design	65	34	1	0
Portland High School	50	23	16	11
Waynflete School	50	30	14	6
Baxter Parent Survey	25	70	-	5
Percentages Used Adopted	30	50	15	5

From the data in Table 1, there is no clear model that works for all cases – each school’s experience seems to be unique depending on the school’s setting and its market service area. Therefore, the modal split percentages adopted for Baxter are a reasonable blend of parent feedback and other surveyed school’s experiences. Waynflete’s parent drop-offs appeared to be influenced by the younger population K-8. Portland High’s numbers are skewed due to the students all residing within the City where we expect many parents work. The Academy for Science & Design does not provide bus service but relies on the City’s School District for busing. The school’s setting is not conducive to walking or biking, and they only have 5 seniors with licenses at this point – hence the high percentage of parent drop-offs. Finally, the proposed Baxter Academy is planning to provide charter busing to outlying communities within its service area to reduce the need for parent shuttles.

The AM peak hour forecast below for the Baxter Academy is based on a student population of 320 students and 30 employees by Year 3 and 160 students and 15 employees in Year 1:

Year 1 (2013) – 160 students

- 30% parent drop off (assumed split of 50% car pooling with 3 students and 50% single occupant): 30% X 160 students = 48 students*
- 24 students in single occupant vehicles = 48 trips*
- 24 students in car pools of 3 = 16 trips*
- 55% of students (88) ride 3 charter buses (1.5 PCE per bus) = 9 trips*
- 10% of students (16) ride existing public transportation = 0 trips*
- 5% of students (8) as walkers = 0 trips*
- 15 employees @ 67% auto use = 10 trips**
- Total estimated AM peak hour trips in Year 1 = 73 trips*
- Anticipated Distribution = 37 arriving and 36 departing*

Year 3 (2015) - 320 students

30% parent drop off (assumed split of 50% car pooling with 3 students and 50% single occupant): 30% X 320 students = 96 students

48 students in single occupant vehicles = 96 trips

48 students in car pools of 3 = 32 trips

40% of students (128) ride 3 charter buses (1.5 PCE per bus) = 9 trips

*15% of students (48) drive their own cars = 0 trips**

10% of students (32) ride existing public transportation = 0 trips

5% of students (16) as walkers = 0 trips

*30 employees @ 67% auto use = 20 trips**

Total estimated AM peak hour trips in Year 3 = 137 trips

Anticipated Distribution = 69 arriving and 68 departing

*Because the site cannot accommodate it, the School is not planning to provide on-site parking for students or employees. A total of 20 off-site parking spaces are initially going to be leased for employees, but since they are off-site their impact will not be felt at the site itself.

The School is proposing a full Traffic Management Plan to organize student arrivals and departures, which is detailed later herein. From a Trip Assignment perspective, though, this Plan will route arriving school buses from York Street onto Maple Street and load/unload curbside on the west side of the street in a designated School Drop Off Zone. The buses will depart the school using Maple Street to Commercial Street. Arriving parents dropping off their students will be directed to the drop off zone on westbound York Street opposite the front door of the school, which can be accessed by either leg of Maple Street and from York Street coming from the Old Port. Figures 2A and 2B illustrate the forecasted school traffic during the AM peak hour for both Years 1 and 3 at the York/Maple intersection under this traffic scenario. It is assumed that student arrivals will originate from all directions (south, west, and north) of the site and thus these trips have been assigned to the street network accordingly. Figure 3 illustrates the combined (existing and forecasted) trips at the York/Maple Street intersection during the two analysis periods.

8. Existing and Projected Traffic Volumes

Existing Conditions

Sebago staff performed a traffic turning movement count at the intersection of York Street and Maple Street during the hours of 7:00 AM and 8:00 AM on June 27, 2012 to observe traffic operations in and around the site during the time period when children would be arriving for the school day. Figure 1 presents the results of this peak hour count. It is readily evident that the volumes are quite small during this time period.

In addition to this data collection effort, several general observations were made while conducting the count. These were:

1. Only one on-street parking space was used on Maple Street between Commercial Street and York Street during the one hour time period observed. This vehicle was a van that had parked there overnight.

2. The paved parking lot for “The Cannery” that is accessed off Maple Street only had 8 cars enter it between 7:00 AM and 8:00 AM.
3. Maple Street is barely used during this time period. Total traffic during the hour observed was 34 vehicles. Never was a queue experienced waiting to enter onto York Street, and with the exception of one overnight parked vehicle, no on-street parking spaces were occupied.
4. The two on-street parking spaces to the west of the proposed school building on York Street were not occupied during the observation period.
5. While the traffic on York Street was predominantly in-bound, it appeared that the time period 7:00 AM to 8:00 AM might be before the actual peak morning commute hour.

Proposed Volumes

Traffic volumes for Year’s 1 and 3, as mentioned above, are presented in Figures 2A and 2B, with combined existing and proposed volumes illustrated in Figure 3.

9. Capacity Analyses

Synchro/SimTraffic was used to analyze existing traffic operations at the intersection of York and Maple Streets and it was determined that the Level of Service (LOS) was A.

Year 1 - 160 students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection’s LOS in Year 1. The LOS was found to be A and only slightly less than it is today under existing conditions. We modeled the parent drop-offs using appropriate peak hour factors.

Year 3 - 320 students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection’s LOS in Year 3. The LOS was found to still be A. We modeled the parent drop-offs using appropriate peak hour factors.

Based on the above analyses, capacity does not appear to be an issue during the AM in the vicinity of the school in either Year 1 or Year 3.

10. Traffic Signals

The applicant is not requesting the installation of a traffic signal in connection with its application, nor is one warranted at the York/Maple intersection.

11. Sight Distances

York Street has a posted speed limit of 25 mph. According to MaineDOT guidelines, the intersection sight distance for this speed is 200 feet. Maple Street as it intersects York Street from both directions is stop

controlled. Sight distances looking in both directions along York Street from both Maple Street approaches exceeds this 200 foot requirement. Vehicles exiting Maple Street onto York Street can clearly see both adjacent intersections - High Street is approximately 600 feet away and Danforth Street is roughly 400 feet away. Hence, sight distance at this intersection is not a problem.

12. Traffic Accidents

MaineDOT's Accident Records Unit was contacted to obtain the most recent 3-year crash information for the area in the vicinity of the proposed project. See attached materials from the Department in the Appendix. In summary, of the 12 intersections surveyed, only one is defined as a High Crash Location (HCL), i.e. a total of 8 crashes in three years and a Critical Rate Factor (CRF) greater than 1.0. This intersection is Commercial Street at High Street with a CRF of 1.50.

Road segments were also examined as evidenced by the enclosed Department data, and no segment immediately abutting the proposed project is identified as a High Crash Location.

Given that there is no HCL in the vicinity of the Project, crash history does not seem to be an influencing factor for this application.

13. Traffic Operations and Management Plan

Arrivals and departures from the Baxter Academy are to be well-orchestrated and staff managed events. Because students will be arriving via various modes of transportation the School's Plan will have a number of elements.

Arrivals

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. Students will depart directly onto the Maple Street sidewalk and proceed to the front door of the School on York Street.

Students using public transit (Metro or South Portland Bus) will depart buses at existing stops on the north side of York Street at either High Street or Fore Street. Students will then proceed to school on existing sidewalks and cross York Street at a repainted formal crosswalk at the intersection of York and Maple Streets. The School will have a staff person supervising this crosswalk for the 45 minutes preceding school's commencement to ensure safe student passage.

Parent drop-offs will occur on the north side of York Street opposite the front door of the school. The school will have a staff person supervising this operation to keep vehicles orderly, and students will be directed to the intersection of Maple and York, where there will be another staff member acting as a crossing guard for safe passage to the front entrance of the school.

Staff members who elect to drive their own vehicles to school will be parking in the Gulf of Maine parking lot off Commercial Street or the parking garage at the Holiday Inn By-The-Bay and will walk to school via existing sidewalks.

Students who elect to drive their own vehicles to school will likely park on Commercial Street in the unrestricted parking spaces adjacent to the end of Maple Street and will walk to school via existing sidewalks.

Students who elect to ride their bikes to school will be able to store their bikes in bike racks, which will be provided by the school adjacent to the building on Maple Street.

In addition to the staff members on-site managing the student arrival process, the school proposes to:

- use traffic cones to block off the curbside in front of the school to prevent vehicles from unloading on the south side of York Street
- place a School Crossing sign in the middle of the York Street crosswalk in front of the school to keep speeds on York Street under control during peak student usage
- erect School Drop Off Zone signs on Maple Street for the buses on the west side of the street and on the north side of York Street for parent drop-offs.

See examples below.



“Crosswalk sign used by Waynflete”



“School Drop-Off Zone Sign at Portland High”

Departures

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. At 2:00 PM when regular school is over students who are not participating in after school programs will load the buses from the Maple Street sidewalk as they did during arrivals.

Students using public transit (Metro or South Portland Bus) will depart the school for the bus stops on the north side of York Street at either High Street or Fore Street depending on individual choice and route schedules.

Parent pick-ups will occur on the north side of York Street opposite the front door of the school. The schedule of these pick-ups will not be as concentrated as in the morning because of after school activities. The school is projecting that up to one half of students will be participating in after school programs, which would mean that parent pick-ups in Year 3 would be in the range of 34 at 2:00 PM and the other 34 would be spread over the subsequent 2 hours. Given the 8 on-street spaces available this does not appear to be an issue.

Food Deliveries

Food service to the school will be provided by an off-site vendor. Deliveries will be made curbside on the west side of Maple Street, similar to where the buses will load and unload.

Proposed Signage Changes

Maple Street: The west side of the street is currently posted as 15-minute parking. This would need to be modified to allow for school bus parking between 7:30 and 8:00 AM and between 1:45 and 2:15 PM.

York Street: The north side of York Street opposite the school is currently posted as 30-minute parking. This would need to be modified to allow for 5 minute school drop-off and pick-up between 7:30 and 8:00 AM and between 2:00 PM and 4:00 PM.

On-Going Plan Refinement

The Baxter Academy realizes that their Traffic Management Plan is subject to individual student family preferences from year to year. As such, the school administration is committed to refining their operation each year to reflect the modal choices made by their enrollees. Given that Years 1 and 2 are ramping up years in terms of the student population, the school will have adequate time to fine-tune operations prior to reaching the ultimate goal of 320 students in Year 3.

14. Parking.

From the information presented above, the number of students and employees that may require parking in the vicinity of the school is 10 staff members in Year 1 and by Year 3 this may grow to 20 staff members and 48 students. The School has made leasing arrangements for between 20-40 off-street parking spaces in the Gulf of Maine lot off Commercial Street. Other options in surrounding parking garages also exist if this is not possible. The Baxter Academy is committed to reducing the number of students driving their cars to school. In this regard the School's informational packets will encourage families to use car pooling, public transportation, school provided charter buses, walking and biking to access the facility. To further support this policy, the school will be offering an incentive to students in the form of a 50-cent subsidy on public transit fares for those students within the Metro/South Portland Bus network.

For those students in Years 2 and beyond that do not choose to avail themselves of public transportation, and want to bring their own vehicles to school, they will be required to register their vehicles with the school and park legally within the surrounding area. This is similar to Waynflete's policy. The school will monitor these situations, and work with the City to address any violators should they be brought to the school's attention.

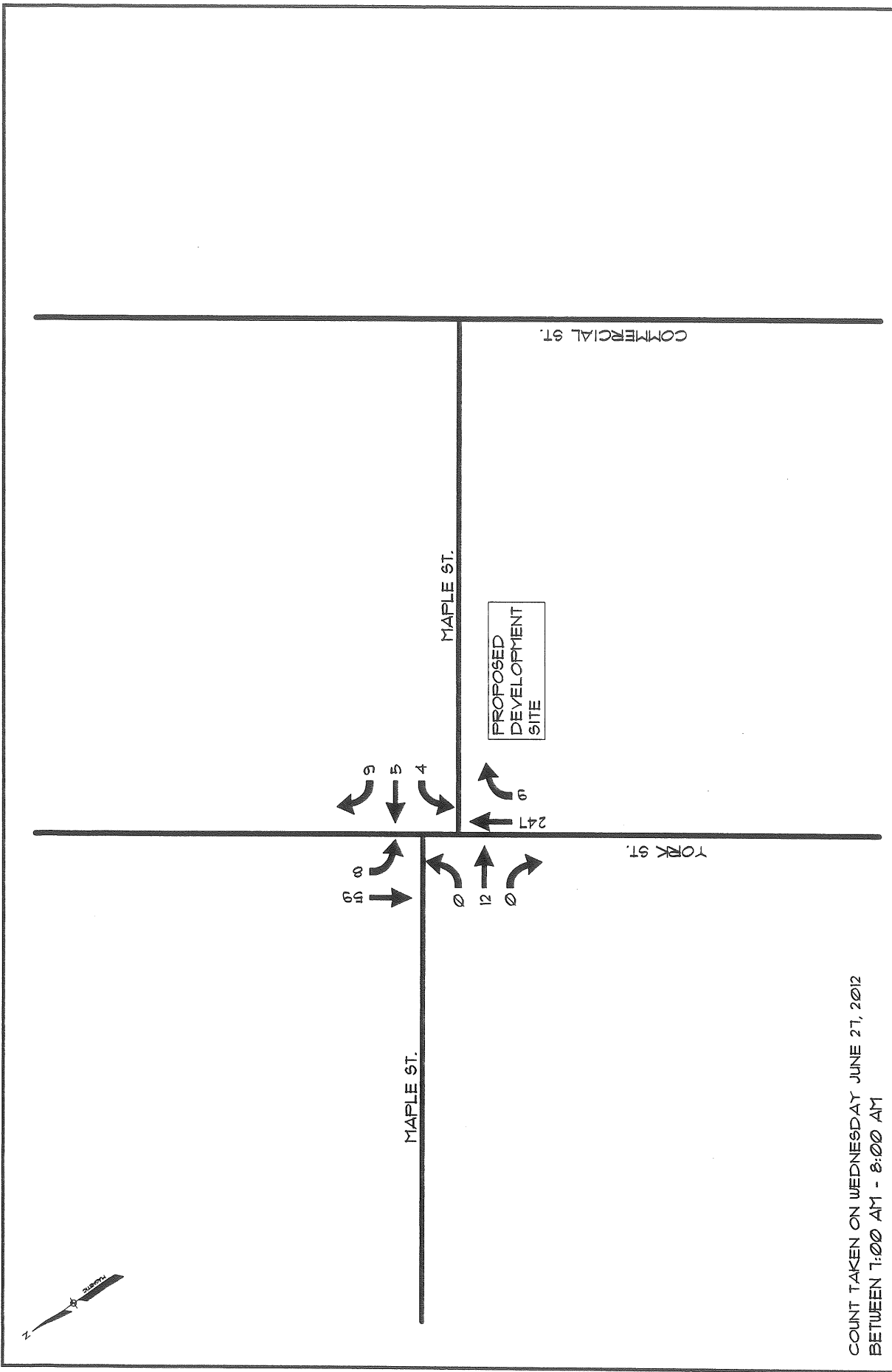
15. Recommendations

From the enclosed information, the proposed Baxter Academy at the corner of York and Maple Streets does not present a capacity or safety issue for the City of Portland. The applicant has presented not only a thorough traffic analysis to support these findings, but also a Traffic Management Plan as well as an off-site Parking Plan to further minimize its traffic impact on the surrounding neighborhood. On this basis, it is the professional opinion of Sebago Technics that the City should grant a Traffic Movement Permit to this applicant.

16. Conclusion

Given the findings presented herein regarding the proposed charter school at York and Maple Streets, Sebago Technics believes that a Traffic Movement Permit should be issued for this development.

M.10



COUNT TAKEN ON WEDNESDAY JUNE 21, 2012
 BETWEEN 1:00 AM - 8:00 AM

SCALE:	NTS
DATE:	10/23/12
FIGURE:	1

EXISTING TRAFFIC VOLUMES
 YORK ST. @ MAPLE ST., PORTLAND, ME

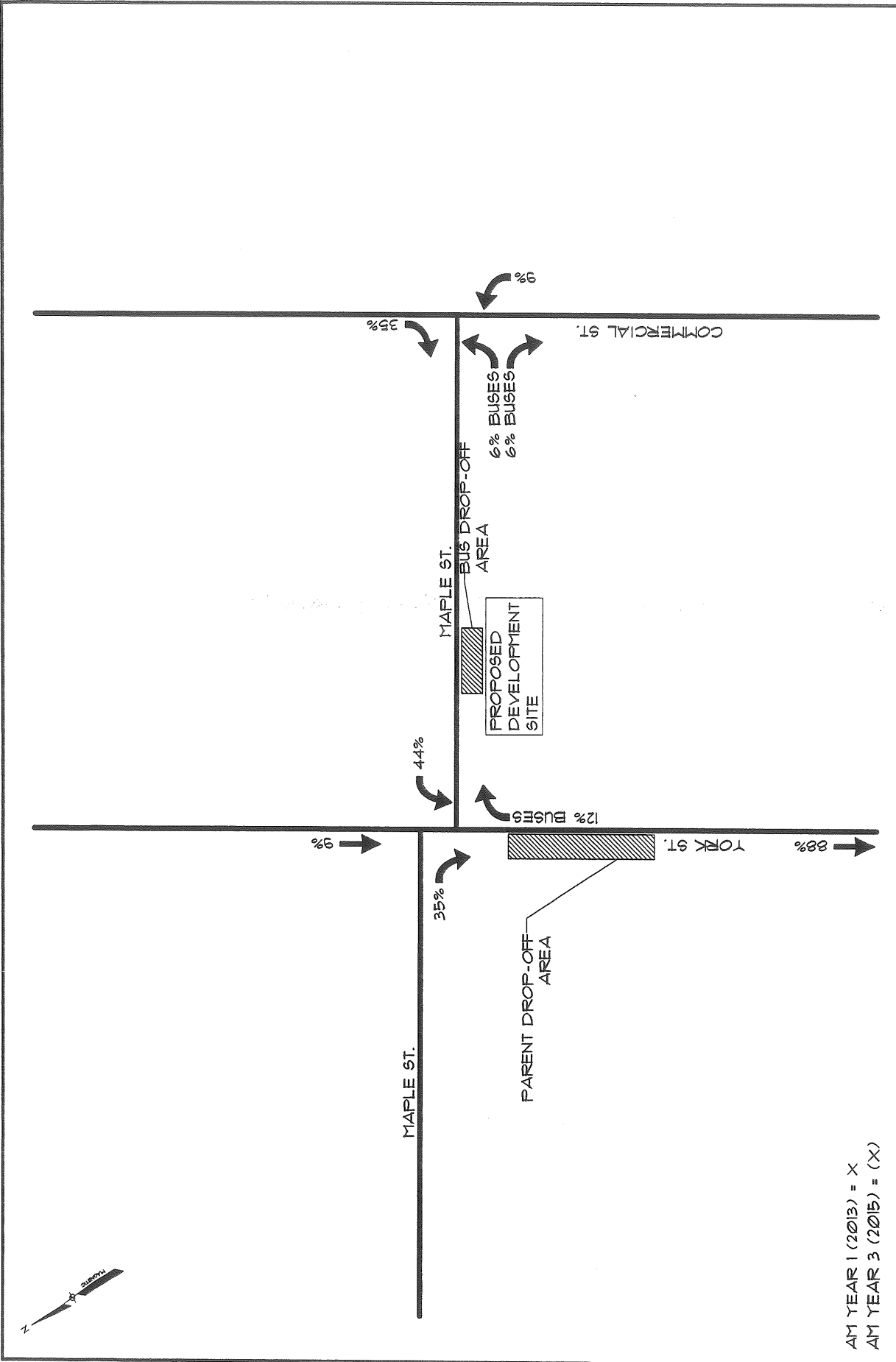
FOR: RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

LOCATION:
 YORK ST. @ MAPLE ST.
 PORTLAND, ME

SEBAGO
 TECHNICALS
 WWW.SEBAGO/TECHNICALS.COM

75 John Roberts Rd. - Suite 1A - Suite B
 South Portland, ME 04106
 Tel. 207-783-5656

M.11



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

SEBAGO
 TECHNICALS

WWW.SEBAGO/TECHNICALS.COM
 75 John Roberts Rd. - Suite 1A - 250 Goodard Rd. - Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel. 207-200-2100 Tel. 207-785-5656

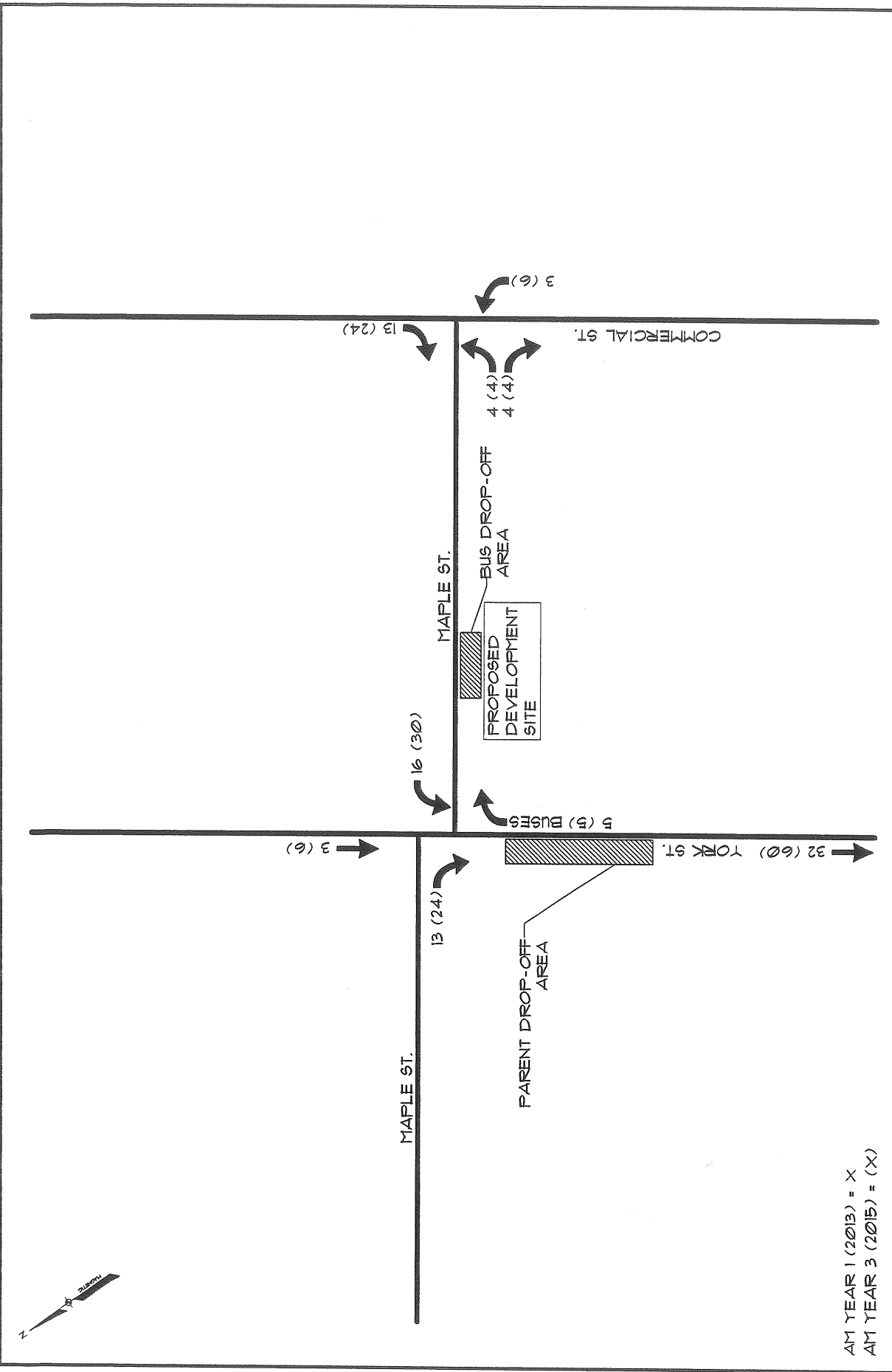
AM NEW PRIMARY TRIPS - % ASSIGNMENT
 YORK ST. @ MAPLE ST., PORTLAND, ME

LOCATION:
 YORK ST. @ MAPLE ST.
 PORTLAND, ME

FOR:
 RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

SCALE:	NTS
DATE:	10/23/12
FIGURE:	2A

M.12



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

SEBAGO
 TECHNICALS

WWW.SEBAGOTECHNICALS.COM
 75 John Roberts Rd. - Suite 1A 250 Goddard Rd. - Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel. 207-200-2100 Tel. 207-783-5656

AM NEW PRIMARY TRIPS - VOLUMES
 YORK ST. @ MAPLE ST., PORTLAND, ME

LOCATION:
 YORK ST. @ MAPLE ST.
 PORTLAND, ME

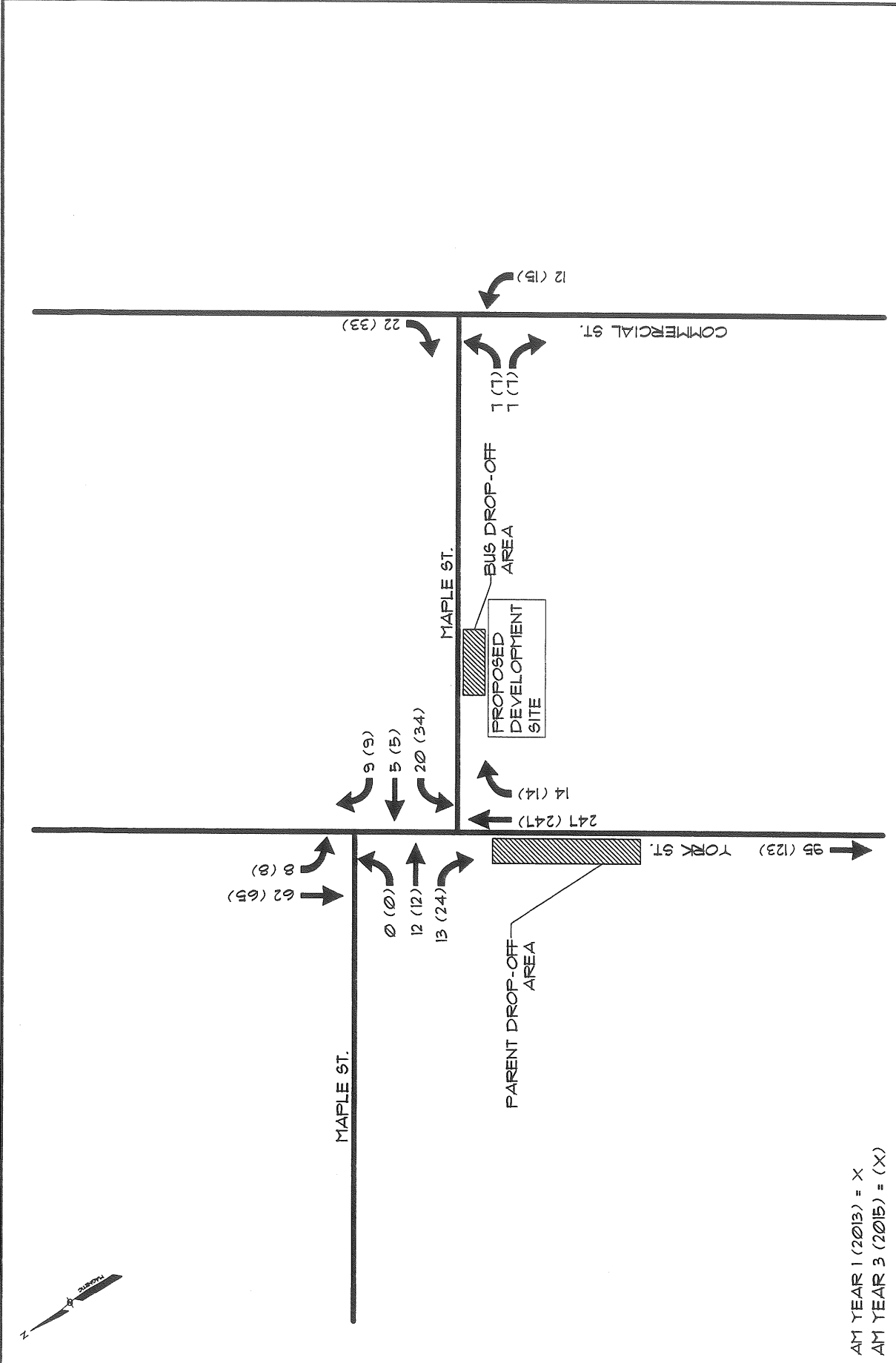
FOR:
 RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

SCALE: NTS

DATE: 10/23/12

FIGURE: 2B

M.13



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

SEBAGO
 TECHNICALS
 WWW.SEAGOTECHNICALS.COM
 75 John Roberts Rd. - Suite 1A
 South Portland, ME 04240
 Tel: 207-200-2100

COMBINED AM TRIPS
 YORK ST. @ MAPLE ST., PORTLAND, ME
 LOCATION: YORK ST. @ MAPLE ST., PORTLAND, ME

SCALE:	NTS
DATE:	10/23/12
FIGURE:	3
FOR:	RUFUS DEERING LUMBER CO. 383 COMMERCIAL ST. PORTLAND, ME

M. 14

Maple Street @ York Street, Portland ME
Existing Conditions

10/23/2012

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Delay / Veh (s)	0.5	0.3	2.7	0.2	6.2	6.5	2.7	6.1	0.8

Total Network Performance

Delay / Veh (s)	1.1								
-----------------	-----	--	--	--	--	--	--	--	--

M.15

Maple Street @ York Street, Portland ME
Proposed Year 1 Conditions

10/23/2012

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	SBR	All
Delay / Veh (s)	0.5	0.3	3.6	0.2	6.5	10.2	2.9	6.0	3.1	1.2

Total Network Performance

Delay / Veh (s)	1.5									
-----------------	-----	--	--	--	--	--	--	--	--	--

M.16

Maple Street @ York Street, Portland ME
Proposed Year 3 Conditions

10/23/2012

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	SBR	All
Delay / Veh (s)	0.5	0.3	3.6	0.3	6.6	7.8	3.4	6.0	2.8	1.4

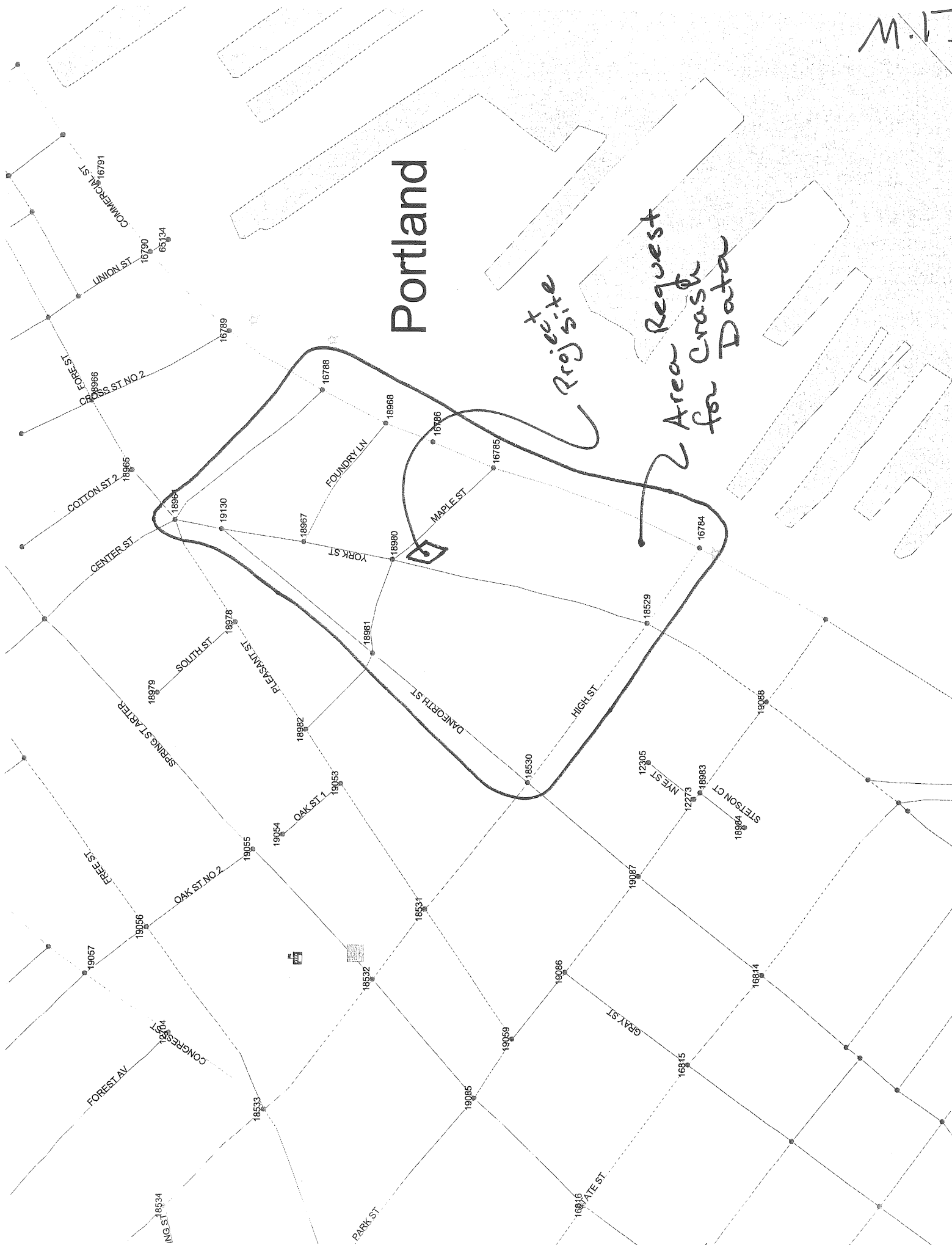
Total Network Performance

Delay / Veh (s)	1.8									
-----------------	-----	--	--	--	--	--	--	--	--	--

M.17

Portland

Project
Area Request
for Crash
Data



Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary Report

Report Selections and Input Parameters

REPORT SELECTIONS

Crash Summary I Section Detail Crash Summary II 1320 Included 1320 & Driver Report Included

REPORT DESCRIPTION

York St area

REPORT PARAMETERS

Year 2009, Start Month 1 through Year 2011 End Month: 12

Route: 0560122	Start Node: 16788 End Node: 18964	Start Offset: 0 End Offset: 0	<input type="checkbox"/> Exclude First Node <input type="checkbox"/> Exclude Last Node
Route: 3200508	Start Node: 18964 End Node: 18529	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0001A	Start Node: 16784 End Node: 16788	Start Offset: 0 End Offset: 0	<input type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0560365	Start Node: 16784 End Node: 18529	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0077W	Start Node: 18529 End Node: 18530	Start Offset: 0 End Offset: 0	<input type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0560191	Start Node: 19130 End Node: 18530	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input type="checkbox"/> Exclude Last Node
Route: 0560475	Start Node: 18980 End Node: 18981	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0560475	Start Node: 16785 End Node: 18980	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0560291	Start Node: 18967 End Node: 18968	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node

M.18

Crash Summary I

Notes

Node	Route - MP	Node Description	U/R	Total Crashes	K	A	B	C	PD	Injury	Percent Annual M Ent-Veh	Crash Rate	Critical Rate	CRF
16788	0560122 - 0	Int of CENTER ST, COMMERCIAL ST	9	8	0	0	1	1	6	25.0	5.645	0.47	1.08	0.00
											Statewide	Crash Rate:	0.62	
18964	0560122 - 0.10	Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST	9	3	0	0	0	1	2	33.3	3.777	0.26	1.18	0.00
											Statewide	Crash Rate:	0.62	
19130	3200508 - 0.02	Int of DANFORTH ST YORK ST	9	1	0	0	0	0	1	0.0	2.346	0.14	1.31	0.00
											Statewide	Crash Rate:	0.62	
18967	3200508 - 0.06	0509387 POR, YORK, FOUNDRY LA.	2	0	0	0	0	0	0	0.0	0.930	0.00	0.51	0.00
											Statewide	Crash Rate:	0.13	
18980	3200508 - 0.10	0509400 POR, MAPLE, YORK ST.	2	0	0	0	0	0	0	0.0	1.849	0.00	0.43	0.00
											Statewide	Crash Rate:	0.13	
16784	0001A - 11.15	Int of COMMERCIAL ST HIGH ST	2	8	0	0	1	2	5	37.5	5.816	0.46	0.30	1.50
											Statewide	Crash Rate:	0.12	
16785	0001A - 11.27	Int of COMMERCIAL ST MAPLE ST	2	2	0	0	0	1	1	50.0	5.507	0.12	0.31	0.00
											Statewide	Crash Rate:	0.12	
16786	0001A - 11.30	Non Int COMMERCIAL ST	2	0	0	0	0	0	0	0.0	5.382	0.00	0.31	0.00
											Statewide	Crash Rate:	0.12	
18968	0001A - 11.33	Int of COMMERCIAL ST FOUNDRY LN	2	0	0	0	0	0	0	0.0	5.466	0.00	0.31	0.00
											Statewide	Crash Rate:	0.12	
18529	0077W - 1.39	Int of HIGH ST YORK ST	9	10	0	0	0	3	6	33.3	3.377	0.99	1.21	0.00
											Statewide	Crash Rate:	0.62	
18981	0560191 - 0.10	Int of DANFORTH ST MAPLE ST	2	0	0	0	0	0	0	0.0	0.877	0.00	0.45	0.00
											Statewide	Crash Rate:	0.11	
18530	0560191 - 0.20	Int of DANFORTH ST HIGH ST	9	18	0	0	1	5	12	33.3	6.348	0.95	1.06	0.00
											Statewide	Crash Rate:	0.62	

Study Years: 3.00

NODE TOTALS: 50 0 0 0 3 13 33 32.0 47.320 0.35 0.47 0.75

M.19

Crash Summary I

Sections

Start Node	End Node	Element	Offset Begin - End	Route - MP	Section U/R Length	Total Crashes	Injury Crashes A B C	PD	Percent Injury	Annual HMVM	Crash Rate	Critical Rate	CRF				
16788	18964	192464	0 - 0.10	0560122 - 0 RD INV 05 60122	0.10	2	1	0	0	0	1	0	100.0	0.00121	275.07	982.08	0.00
		Int of CENTER ST, COMMERCIAL ST													Statewide Crash Rate: 336.22		
18529	18980	194048	0 - 0.14	3200508 - -0.04 RD INV 3200508	0.14	2	2	0	0	0	2	0	100.0	0.00243	274.26	820.78	0.00
		Int of HIGH ST, YORK ST													Statewide Crash Rate: 336.22		
18964	19130	3106830	0 - 0.02	3200508 - 0 RD INV 3200508	0.02	2	1	0	0	0	1	0	0.0	0.00046	721.13	607.32	1.19
		Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST													Statewide Crash Rate: 142.40		
18967	19130	194651	0 - 0.04	3200508 - 0.02 RD INV 3200508	0.04	2	0	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00
		0509387 POR, YORK, FOUNDRY LA.													Statewide Crash Rate: 336.22		
18967	18980	194650	0 - 0.04	3200508 - 0.06 RD INV 3200508	0.04	2	0	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00
		0509387 POR, YORK, FOUNDRY LA.													Statewide Crash Rate: 336.22		
16784	16785	3116402	0 - 0.12	0001A - 11.15 US 1A	0.12	2	6	0	0	0	1	5	16.7	0.00645	309.87	388.11	0.00
		Int of COMMERCIAL ST, HIGH ST													Statewide Crash Rate: 171.50		
16785	16786	3130488	0 - 0.03	0001A - 11.27 US 1A	0.03	2	3	0	0	1	0	2	33.3	0.00163	614.36	551.86	1.11
		Int of COMMERCIAL ST, MAPLE ST													Statewide Crash Rate: 171.50		
16788	18968	3119256	0 - 0.04	0001A - 11.29 US 1A	0.04	2	1	0	0	0	0	1	0.0	0.00216	154.31	513.40	0.00
		Int of CENTER ST, COMMERCIAL ST													Statewide Crash Rate: 171.50		
16786	18968	3118839	0 - 0.03	0001A - 11.30 US 1A	0.03	2	1	0	0	0	1	0	0.0	0.00160	208.12	554.11	0.00
		Non Int COMMERCIAL ST													Statewide Crash Rate: 171.50		
16784	18529	3115419	0 - 0.05	0560365 - 0 RD INV 05 60365	0.05	2	0	0	0	0	0	0	0.0	0.00063	0.00	683.39	0.00
		Int of COMMERCIAL ST, HIGH ST													Statewide Crash Rate: 171.50		
18529	18530	3106683	0 - 0.10	0077W - 1.39 ST RTE 77W	0.10	2	10	0	0	0	0	10	0.0	0.00383	870.25	446.50	1.95
		Int of HIGH ST, YORK ST													Statewide Crash Rate: 173.49		
18981	19130	3106832	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	2	0	0	0	0	2	0.0	0.00071	935.70	573.37	1.63
		Int of PANFORTH ST, MAPLE ST													Statewide Crash Rate: 142.40		
18530	18981	3106686	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	4	0	0	0	0	4	0.0	0.00083	1609.24	557.81	2.88
		Int of PANFORTH ST, HIGH ST													Statewide Crash Rate: 142.40		
18980	18981	194666	0 - 0.05	0560475 - 0.07 RD INV 05 60475	0.05	2	0	0	0	0	0	0	0.0	0.00004	0.00	558.41	0.00
		0509400 POR, MAPLE, YORK ST.													Statewide Crash Rate: 336.22		
16785	18980	192461	0 - 0.07	0560475 - 0 RD INV 05 60475	0.07	2	2	0	0	0	0	2	0.0	0.00015	4537.85	1451.68	3.13
		Int of COMMERCIAL ST, MAPLE ST													Statewide Crash Rate: 336.22		
18967	18968	194649	0 - 0.07	0560291 - 0 RD INV 05 60291	0.07	2	0	0	0	0	0	0	0.0	0.00013	0.00	1448.34	0.00
		0509387 POR, YORK, FOUNDRY LA.													Statewide Crash Rate: 336.22		
Study Years:	3.00				1.10	33	0	0	0	1	4	28	15.2	0.02360	466.01	338.32	1.38
Grand Totals:					1.10	83	0	0	0	4	17	61	25.3	0.02360	1172.09	461.67	2.54

M.20

Crash Summary

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	Injury Crashes			Crash Report	Crash Date	Crash Mile Point	Injury Degree		
						K	A	B					C	PD
16788	18964	192464	0 - 0.10	0560122 - 0	1	0	0	0	1	0	2010-5651C	03/14/2010	0.01	C
18964	19130	3106830	0 - 0.02	3200508 - 0	1	0	0	0	0	1	2011-7755C	04/15/2011	0.01	PD
18967	19130	194651	0 - 0.04	3200508 - 0.02	0	0	0	0	0	0				
18967	18980	194650	0 - 0.04	3200508 - 0.06	0	0	0	0	0	0				
18529	18980	194048	0 - 0.14	3200508 - 0.10	2	0	0	0	2	0	2010-3489C	02/06/2010	0.11	C
											2011-9159	08/31/2011	0.20	C
16784	16785	3116402	0 - 0.12	0001A - 11.15	6	0	0	0	1	5	2011-8884	08/28/2011	11.16	C
											2011-6597	08/03/2011	11.16	PD
											2009-32880C	12/29/2009	11.16	PD
											2009-11594C	05/22/2009	11.17	PD
											2010-14757C	07/20/2010	11.17	PD
											2010-12965C	05/31/2010	11.18	PD
16785	16786	3130488	0 - 0.03	0001A - 11.27	3	0	0	1	0	2	2010-30463C	12/26/2010	11.28	B
											2009-10518C	05/13/2009	11.28	PD
											2010-15225C	06/21/2010	11.28	PD
16786	18968	3118839	0 - 0.03	0001A - 11.30	1	0	0	0	0	1	2009-19621C	08/31/2009	11.31	PD
16788	18968	3119256	0 - 0.04	0001A - 11.33	1	0	0	0	0	1	2010-14622C	07/13/2010	11.36	PD
16784	18529	3115419	0 - 0.05	0560365 - 0	0	0	0	0	0	0				
18529	18530	3106683	0 - 0.10	0077W - 1.39	10	0	0	0	0	10	2009-6432C	03/04/2009	1.40	PD
											2011-1699C	01/30/2011	1.40	PD
											2011-3653	06/29/2011	1.41	PD
											2010-9558C	05/05/2010	1.43	PD
											2011-16796	11/28/2011	1.45	PD
											2009-1917C	01/09/2009	1.45	PD
											2011-5217	07/19/2011	1.46	PD
											2009-18444C	08/16/2009	1.48	PD
											2011-710C	01/07/2011	1.48	PD
											2010-14209C	07/10/2010	1.48	PD
18981	19130	3106832	0 - 0.10	0560191 - 0	2	0	0	0	0	2	2009-6337C	02/20/2009	0.01	PD
											2010-16740C	08/11/2010	0.02	PD
18530	18981	3106686	0 - 0.10	0560191 - 0.10	4	0	0	0	0	4	2011-5063	07/15/2011	0.11	PD
											2010-24444C	11/05/2010	0.15	PD
											2010-1886C	01/26/2010	0.15	PD
											2010-26329C	11/30/2010	0.19	PD

M.21

Crash Summary

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	Injury Crashes				Crash Report	Crash Date	Crash Mile Point	Injury Degree	
						K	A	B	C					PD
18980	18981	194666	0 - 0.05	0560475 - 0.07	0	0	0	0	0	0				
16785	18980	192461	0 - 0.07	0560475 - 0	2	0	0	0	0	2	2010-9693C	05/14/2010	0.04	PD
18967	18968	194649	0 - 0.07	0560291 - 0	0	0	0	0	0	0	2011-2927C	02/04/2011	0.06	PD
Totals:					33	0	0	1	4	28				

M.22

Maine Department of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Day and Hour

Day of Week	Hour of Day												Un	Tot												
	AM						PM																			
	12	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6	7	8	9	10	11		
SUNDAY	1	3	0	1	0	0	0	1	0	1	2	0	0	0	0	2	1	0	0	0	0	2	0	0	0	14
MONDAY	0	0	0	0	0	0	0	0	1	0	2	2	0	1	2	1	2	2	0	0	0	1	0	0	0	14
TUESDAY	0	1	0	0	0	0	0	1	1	1	0	1	1	0	0	2	0	1	0	0	1	0	0	0	0	10
WEDNESDAY	0	0	0	0	0	0	0	0	1	1	1	0	2	1	0	0	3	1	2	0	0	0	0	0	0	12
THURSDAY	0	0	0	0	0	0	0	1	1	0	1	0	2	0	0	1	1	0	1	0	1	0	0	0	0	8
FRIDAY	1	0	0	0	0	0	0	2	0	1	1	2	2	2	1	2	2	2	0	2	0	0	0	0	0	20
SATURDAY	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1	1	0	0	1	0	0	0	0	5
Totals	2	4	0	1	0	0	0	5	4	4	4	7	5	7	4	3	9	10	8	2	3	3	2	0	0	83

Vehicle Counts by Type

Unit Type	Total	Unit Type	Total
1-Passenger Car	103	23-Bicyclist	2
2-(Sport) Utility Vehicle	30	24-Witness	13
3-Passenger Van	13	25-Other	3
4-Cargo Van (10K lbs or Less)	0	Total	193
5-Pickup	20		
6-Motor Home	0		
7-School Bus	0		
8-Transit Bus	0		
9-Motor Coach	0		
10-Other Bus	0		
11-Motorcycle	1		
12-Moped	0		
13-Low Speed Vehicle	0		
14-Autocycle	0		
15-Experimental	0		
16-Other Light Trucks (10,000 lbs or Less)	0		
17-Medium/Heavy Trucks (More than 10,000 lbs)	6		
18-ATV - (4 wheel)	0		
20-ATV - (2 wheel)	0		
21-Snowmobile	0		
22-Pedestrian	2		

M.23

Crash Summary II - Characteristics

Crashes by Driver Action at Time of Crash

Driver Action at Time of Crash	Dr1	Dr2	Dr3	Dr4	Dr5	Other	Total
No Contributing Action	9	5	1	0	0	0	15
Ran Off Roadway	1	0	0	0	0	0	1
Failed to Yield Right-of-Way	8	9	0	0	0	0	17
Ran Red Light	1	1	0	0	0	0	2
Ran Stop Sign	0	0	0	0	0	0	0
Disregarded Other Traffic Sign	0	0	0	0	0	0	0
Disregarded Other Road Markings	0	0	0	0	0	0	0
Exceeded Posted Speed Limit	1	2	1	0	0	0	4
Drove Too Fast For Conditions	0	0	0	0	0	0	0
Improper Turn	3	2	0	0	0	0	5
Improper Backing	2	1	0	0	0	0	3
Improper Passing	1	2	0	0	0	0	3
Wrong Way	0	0	0	0	0	0	0
Followed Too Closely	4	7	1	0	0	0	12
Failed to Keep in Proper Lane	1	0	0	0	0	0	1
Operated Motor Vehicle in Erratic, Reckless, Careless, Negligent or Aggressive Manner	0	0	0	0	0	0	0
Swerved or Avoided Due to Wind, Slippery Surface, Motor Vehicle, Object, Non-Motorist in Roadway	0	0	0	0	0	0	0
Over-Correcting/Over-Steering	0	0	0	0	0	0	0
Other Contributing Action	0	1	0	0	0	0	1
Unknown	0	1	0	0	0	0	1
Total	31	31	3	0	0	0	65

Crashes by Apparent Physical Condition And Driver

Apparent Physical Condition	Dr1	Dr2	Dr3	Dr4	Dr5	Other	Total
Apparently Normal	72	72	10	1	0	2	157
Physically Impaired or Handicapped	0	0	0	0	0	0	0
Emotional(Depressed, Angry, Disturbed, etc.)	0	0	0	0	0	0	0
Ill (Sick)	0	0	0	0	0	0	0
Asleep or Fatigued	0	0	0	1	0	0	1
Under the Influence of Medications/Drugs/Alcohol	3	3	0	0	0	0	6
Other	1	2	0	0	0	0	3
Total	76	77	10	2	0	2	167

Driver Age by Unit Type

Age	Driver	Bicycle	Snow/Mobile	Pedestrian	ATV	Total
09-Under	0	0	0	0	0	0
10-14	0	0	0	0	0	0
15-19	14	0	0	0	0	14
20-24	39	0	0	0	0	39
25-29	22	0	0	0	0	22
30-39	26	0	0	0	0	26
40-49	18	0	0	0	0	18
50-59	28	0	0	0	0	28
60-69	14	0	0	0	0	14
70-79	5	0	0	0	0	5
80-Over	3	0	0	0	0	3
Unknown	7	2	0	2	0	11
Total	176	2	0	2	0	180

M.24

Crash Summary II - Characteristics

Most Harmful Event		Most Harmful Event	
Most Harmful Event	Total	Most Harmful Event (wall, building, tunnel, etc.)	Total
1-Overturn / Rollover	0	38-Other Fixed Object	0
2-Fire / Explosion	0	39-Unknown	3
3-Immersion	0	40-Gate or Cable	0
4-Jackknife	0	41-Pressure Ridge	0
5-Cargo / Equipment Loss Or Shift	0	Total	31
6-Fell / Jumped from Motor Vehicle	0		
7-Thrown or Falling Object	0		
8-Other Non-Collision	0		
9-Pedestrian	1		
10-Pedalcycle	0		
11-Railway Vehicle - Train, Engine	0		
12-Animal	0		
13-Motor Vehicle in Transport	24		
14-Parked Motor Vehicle	2		
15-Struck by Falling, Shifting Cargo or Anything Set in Motion by Motor Vehicle	0		
16-Work Zone / Maintenance Equipment	0		
17-Other Non-Fixed Object	0		
18-Impact Attenuator / Crash Cushion	0		
19-Bridge Overhead Structure	0		
20-Bridge Pier or Support	0		
21-Bridge Rail	0		
22-Cable Barrier	0		
23-Culvert	0		
24-Curb	0		
25-Ditch	0		
26-Embankment	0		
27-Guardrail Face	0		
28-Guardrail End	0		
29-Concrete Traffic Barrier	0		
30-Other Traffic Barrier	0		
31-Tree (Standing)	0		
32-Utility Pole / Light Support	0		
33-Traffic Sign Support	0		
34-Traffic Signal Support	1		
35-Fence	0		
36-Mailbox	0		
37-Other Post Pole or Support	0		

Injury Data		
Severity Code	Injury Crashes	Number Of Injuries
K	0	0
A	0	0
B	4	6
C	17	23
PD	61	0
Total	82	29

Road Character	
Road Grade	Total
1-Level	39
2-On Grade	41
3-Top of Hill	2
4-Bottom of Hill	1
5-Other	0
Total	83

Traffic Control Devices	
Traffic Control Device	Total
1-Traffic Signals (Stop & Go)	41
2-Traffic Signals (Flashing)	2
3-Advisory/Warning Sign	0
4-Stop Signs - All Approaches	0
5-Stop Signs - Other	8
6-Yield Sign	0
7-Curve Warning Sign	0
8-Officer, Flagman, School Patrol	0
9-School Bus Stop Arm	0
10-School Zone Sign	0
11-R.R. Crossing Device	0
12-No Passing Zone	0
13-None	28
14-Other	3
Total	82

Light	
Light Condition	Total
1-Daylight	57
2-Dawn	1
3-Dusk	4
4-Dark - Lighted	21
5-Dark - Not Lighted	0
6-Dark - Unknown Lighting	0
7-Unknown	0
Total	83

M.25

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Year and Month

Month	2009	2010	2011	Total
JANUARY	3	1	2	6
FEBRUARY	3	2	4	9
MARCH	1	4	0	5
APRIL	2	3	3	8
MAY	3	5	2	10
JUNE	1	2	2	5
JULY	2	4	6	12
AUGUST	4	1	5	10
SEPTEMBER	1	0	0	1
OCTOBER	2	4	0	6
NOVEMBER	1	3	4	8
DECEMBER	2	1	0	3
Total	25	30	28	83

Report is limited to the last 10 years of data.

M.26

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Crash Type and Type of Location

Crash Type	Straight Road	Curved Road	Three Leg Intersection	Four Leg Intersection	Five or More Leg Intersection	Driveways	Bridges	Interchanges	Other	Parking Lot	Private Way	Cross Over	Railroad Crossing	Total
Object in Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rear End / Sideswipe	26	0	12	15	0	2	0	0	0	0	0	0	0	55
Head-on / Sideswipe	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Intersection Movement	0	0	2	13	1	5	0	0	0	0	0	0	0	21
Pedestrians	0	0	2	0	0	0	0	0	0	0	0	0	0	2
Train	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Went Off Road	0	0	0	2	1	0	0	0	0	0	0	0	0	3
All Other Animal	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bicycle	0	0	1	1	0	0	0	0	0	0	0	0	0	2
Other	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Jackknife	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rollover	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Fire	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Submersion	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Thrown or Falling Object	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bear	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Deer	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Moose	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Turkey	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	26	0	17	31	2	7	0	0	0	0	0	0	0	83

M.27

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Blowing Sand, Soil, Dirt												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Blowing Snow												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Clear												
Dark - Lighted	13	0	0	0	0	0	0	2	0	0	0	15
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	36	1	0	0	0	0	0	0	0	0	1	38
Dusk	2	0	0	0	0	0	0	0	0	0	0	2
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Cloudy												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	8	0	0	0	0	0	0	0	0	0	3	11
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

M.28

Maine Department of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Fog, Smog, Smoke												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	1	0	0	0	0	0	0	0	0	0	0	1
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Other												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Rain												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	4	4
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	1	1
Daylight	0	0	0	0	0	0	0	0	0	0	5	5
Dusk	0	0	0	0	0	0	0	0	0	0	1	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Severe Crosswinds												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

M.29

Maine Department of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

M.30

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Street, Hail (Freezing Rain or Drizzle)												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Snow												
Dark - Lighted	0	0	0	0	0	0	0	1	0	0	1	2
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	2	0	0	0	2
Dusk	0	0	0	0	0	0	0	1	0	0	0	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL	60	1	0	0	0	0	0	6	0	0	0	83

You have a BASIC account | To remove the limits of a BASIC account and get unlimited questions, upgrade now!

Parent Survey June 22

Education

Design Survey Collect Responses **Analyze Results**

- View Summary
- Browse Responses
- Filter Responses
- Crosstab Responses
- Download Responses
- Share Responses

Default Report + Add Report

Response Summary

Total Started Survey: 26
Total Finished Survey: 26 (100%)

PAGE: 1

1. What is your preferred method of transportation to school?

Create Chart Download

	Response Percent	Response Count
walking	3.8%	1
bicycle	0.0%	0
public transportation	11.5%	3
parent ride	19.2%	5
parent carpool	11.5%	3
school bus	53.8%	14
answered question		26
skipped question		0

2. Would you prefer a parent carpool if Baxter Academy were to help facilitate?

Create Chart Download

	Response Percent	Response Count
yes	52.0%	13
no	48.0%	12
answered question		25
skipped question		1

3. Would you be willing to drive as part of a carpool?

Create Chart Download

Response Percent	Response Count
---------------------	-------------------

M.32

yes	70.8%	17
no	29.2%	7
answered question		24
skipped question		2

4. Would you be willing to pay a monthly bus fee?

[Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	80.8%	21
no	19.2%	5
answered question		26
skipped question		0

5. Would you be picking your child up from school at dismissal at 2pm?

[Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	36.0%	9
no	64.0%	16
answered question		25
skipped question		1

6. Would you want your child to ride a school bus back to your hometown? (outside of Portland only)

[Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	65.0%	13
no	35.0%	7
answered question		20
skipped question		6

7. Would you be picking up your child later in the day from the Portland Public Library, or from some other downtown location?

[Create Chart](#) [Download](#)

	Response Percent	Response Count
--	------------------	----------------

SurveyMonkey - Survey Results

yes	36.0%	9
no	64.0%	16
answered question		25
skipped question		1

Follow Us: [Facebook](#) • [Twitter](#) • [LinkedIn](#) • [Our Blog](#)

Help: [Tutorials](#) • [Answers & FAQs](#) • [Contact Support](#)

About Us: [Management Team](#) • [Board of Directors](#) • [Partners](#) • [Newsroom](#) • [Contact Us](#) • [We're Hiring](#) • [Sitemap](#)

Policies: [Terms of Use](#) • [Privacy Policy](#) • [Anti-Spam Policy](#) • [Security Statement](#) • [Email Opt-Out](#)

[Dansk](#) • [Deutsch](#) • [English](#) • [Español](#) • [Français](#) • [한국어](#) • [Italiano](#) • [Nederlands](#) • [日本語](#) • [Norsk](#) • [Português](#) • [Русский](#) • [Suomi](#) • [Svenska](#) • [中文\(繁體\)](#)

Copyright © 1999-2012 SurveyMonkey

Shukria Wiar - RE: Crosswalk Committee

From: "Steve Sawyer" <ssawyer@sebagotechnics.com>
To: "Shukria Wiar" <SHUKRIAW@portlandmaine.gov>
Date: 12/13/2012 11:06 AM
Subject: RE: Crosswalk Committee
CC: "Dan LaBrie" <Dlabrie@rufusdeering.com>, "John Jaques" <john@baxteracade...>
Attachments: 07383 Crosswalk Plan.pdf

Shukria -

Thanks for your follow up. Attached is the crosswalk plan that we submitted to you in advance of our meeting this week. Given the conditions at this site, we believe that the new crosswalk location across York Street is optimum as shown and look forward to the Crosswalk Committee's feedback.

As for the number of walkers to the school. We expect that about 5% of the students will walk to and from school. In year 1 this would be roughly 8 and in year 3 this would be 16. Given the proximity of the nearby residential neighborhoods within a reasonable walking distance these students should all be coming from the West End on the north side of York Street and thus will need to cross the street to get to the school's front door. In addition, should any parents drop off kids on the north side of the York Street, this new crosswalk would provide a more direct safe passage across the street than utilizing the existing crosswalk at the Maple Street intersection.

Thank you for your consideration of this request, and we look forward to receiving your feedback. Let us know if you require anything further from us in this regard.

Stephen S. Sawyer, Jr. P.E.
Vice President Transportation Services



www.sebagotechnics.com

An Employee Owned Company

New Address (effective 5/21/12):
75 John Roberts Road - Suite 1A
South Portland, ME 04106-6963

Office: 207.200.2100
Direct Dial: 207.200.2082
Mobile: 207.232.1281
Fax: 207.856.2206

This notice and disclaimer regards the transmittal and use of electronic files generated and forwarded by Sebago Technics, Inc. This message contains confidential information and is intended only for the individual named. If you are not the named addressee, you should not disseminate, distribute or copy this email. Please notify the sender immediately by email if you have received this email by mistake and delete this email from your system. Email transmission cannot be guaranteed to be secure or error-free, as information could be intercepted, corrupted, lost, destroyed, arrive late or incomplete, or contain viruses. The sender, therefore, does not accept liability for any errors or omissions in the contents of this message which arise as a result of email transmission. If verification is required, please request a hard-copy version. The information contained in these electronic files is privileged and confidential and is intended only for the use of the individual or entity stated in the address above. Any dissemination, distribution or copying of these electronic files is strictly prohibited. The recipient of the electronic file acknowledges and consents to the fact that the sender, Sebago Technics, Inc. makes no claim that electronic files are current, accurate or correct. Any use or reuse of the electronic file in any manner whatsoever is at the user's sole risk and the user agrees, to the fullest extent permitted by law, to hold Sebago Technics, Inc. harmless and to indemnify it from all claims, losses, damages, expenses and costs including all reasonable attorneys fees that may arise from its use or reuse.

From: Shukria Wiar [mailto:SHUKRIAW@portlandmaine.gov]
Sent: Thursday, December 13, 2012 10:34 AM
To: Steve Sawyer
Cc: John Jaques; dlabrie@rufusdeering.com
Subject: Crosswalk Committee

Hello Steve:

I was suppose to get some information on the crosswalks from you. I have talked to Bruce Hyman at Public Services to get your project on for the January agenda. There may be room but I will confirm with him and let you know. In the meantime could you send me the site plan with the proposed crosswalks and the descriptive language about who and how many will be walking.

Thanks,

Shukria

*Shukria Wiar, Planner
Planning Division, City of Portland
389 Congress Street
Portland, ME 04101
Ph: 207-756-8083
Fax: 207-756-8258*

Notice: Under Maine law, documents - including e-mails - in the possession of public officials or city employees about government business may be classified as public records. There are very few exceptions. As a result, please be advised that what is written in an e-mail could be released to the public and/or the media if requested.



PLANNING BOARD REPORT PORTLAND, MAINE

Baxter Academy for Technology and Science
54 York Street
Level II and Traffic Movement Permit
2012-519
Rufus Deering Lumber Company, Applicant

Submitted to: Portland Planning Board: Public Hearing Date: January 22, 2013	Prepared by: Shukria Wiar Date: January 18, 2013 Planning Board Report Number: 05-2013
--	--

I. INTRODUCTION

Dan Labrie, on behalf of Rufus Deering Lumber Company, has requested a public hearing with the Planning Board for a change of use at 54 York Street and a traffic movement permit. The proposal is to convert most of the building to the Baxter Academy for Technology and Science (a charter high school), which is an institutional use. The building has three floors and the current uses include Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses for offices, labs, and dance studio. The site for the proposed Charter School consists of 0.22 acre (9,583sq.ft.) that is fully developed and the existing three-story building is located in the Urban Commercial Business B-5b zone and the Old Port Historic District.

The Level II site plan application was submitted in July of 2012. At that time, the applicant sought to phase the project over three years: 9th grade in the first year, 10th and 11th grade in the second year, 12th in the third year. The building space was to be converted in phases as well to accommodate the incoming grades with the anticipated start date of September of 2012. On July 17, 2012, Baxter Academy was given conditional approval by the Maine Charter School Commission to open a charter school in Fall of 2013. The applicant is seeking a Traffic Movement Permit for the full enrollment, which generates more than peak 100 trips. On November 16, 2012, Baxter Academy received final approval from the Commission. The School is the first public charter school focusing on science, technology, engineering and mathematics.

The project had a workshop meeting with the Planning Board on November 21, 2012. At the meeting, the concerns that were discussed are the following:

1. Sidewalk on York Street
2. The drop-off zones on York and Maple Streets
3. Midblock Crosswalk on York Street

The City has the delegated review for the Traffic Movement Permit and the Board will review the submitted Level II site plan for the change of use that is less than 20,000 square feet. The applicant is also requesting a waiver of sidewalks on York and Maple Streets along the frontage of the proposed site. The requirement for the sidewalks is from the Site Plan Ordinance, Section 14-526 (a) (2) c.

One Hundred and eighty-eight (188) notices were sent to area residents and interested citizen's list. A notice also appeared in the January 14th and January 15th editions of the *Portland Press Herald*.

Applicant Name
Consultants

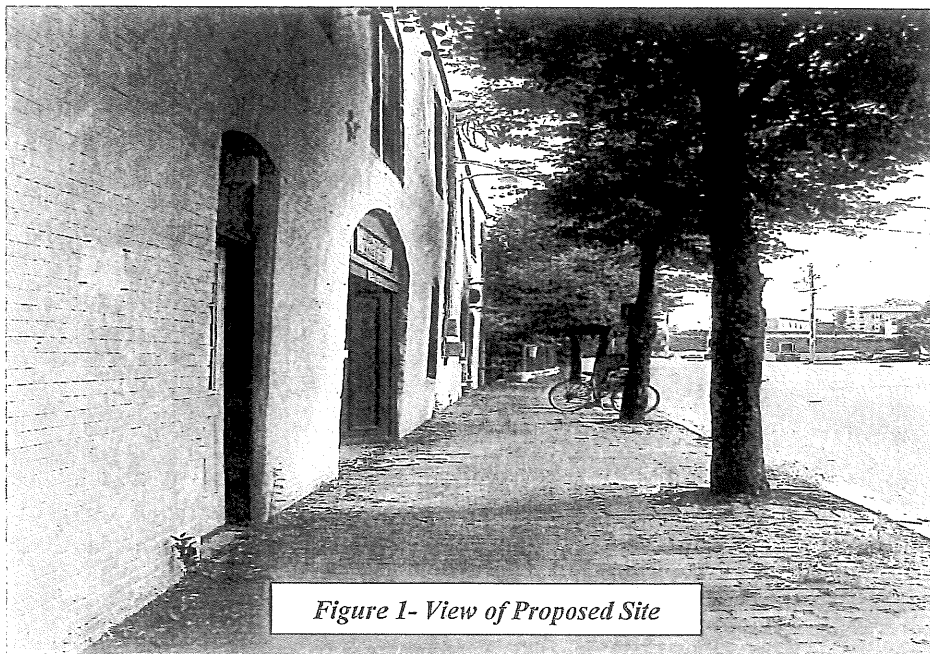
Dan Labrie, Rufus Deering Lumber Company
Steve Sawyer and Jim Seymour, PE Sebago Technics

II. PROJECT DATA

Existing Zoning:	Urban Commercial Business B-5b
Existing Use:	Rufus Lumber Milling Display Store with Offices, Labs, and Dance Studio
Proposed Use:	Institutional (Charter School)
Existing number of lots:	One
Proposed number of lots:	One
Parcel Size:	0.22 acre (9.583 s.f.)
Impervious Surface Area:	
Existing:	9,715 sq. ft.
Proposed:	Zero
Net Change:	Zero
Building Area:	7,540 sq. ft.
Parking Spaces:	
Existing:	12
Proposed:	Applicant is looking into leasing parking spaces; properties in the B-5b zone and historic districts are exempt from providing off-street parking requirements
Bicycle Parking Spaces:	
Existing:	Zero
Proposed:	5
Estimated Cost of Project:	\$185,000
Uses in Vicinity:	Commercial; restaurants; school/research; lumber yard

III. EXISTING CONDITIONS

The 7,540 square foot office building at the northern corner of York and Maple Streets is owned by Rufus Deering, but is not part of the commercial lumber company and lumber yard operation. Surface parking and Portland Pie Company are



located across York Street and surface parking is across Maple Street. The JB Brown proposal for a hotel and other uses, which is currently under review, is proposed for the corner of Maple and Commercial Streets. The lumber yard surrounds the building to the east and south.

There is an existing curb cut and three street trees along the building frontage on Maple Street. There are no sidewalks on the lower portion of Maple Street or across the street on Maple Street. There are two street trees and no curb cuts on the York Street frontage. The building is currently developed with the following land uses:

- An exercise studio;
- A Buddhist Center;
- Administrative offices;
- Global Biotech;
- Deering Lumber's Kitchen and Millwork Showroom

Once the conversion is complete, the building will house the Baxter Academy for Technology and Science Charter School as the primary tenant. According to the applicant, either the Global Biotech or the Deering Lumber's Kitchen will remain in the basement.



Figure 2- View of York Street

IV. PROPOSED DEVELOPMENT

Rufus Deering Lumber Company is proposing to renovate and convert their existing building at 54 York Street (the corner of York and Maple Streets) to house the new Baxter Academy for Technology and Science Charter High School. The lot is fully developed with an existing 7,540 square foot, three-story building. The current uses consist of the Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses of offices, labs, and a dance studio.

The Baxter Academy is proposed as a public charter high school focusing in the fields of mathematics, technology, and science. The school's proposal is to convert much the building into a full four-year high school over the next two years. The total enrollment is planned for approximately 320 students by 2014. In the opening year which is September of 2013, an estimated 160 freshmen and sophomore year students are projected to enroll. In the second year, grade 9 through 11 will be provided and all four grades in the third year. It is projected that half of the total enrollment will come from Portland, South Portland and Westbrook, with the remaining coming from communities outside of the urban core. School operations will be from 8:00 AM to 2:00 PM, resulting in peak traffic generation associated with the school being between 7-8:00 AM and 2-3:00 PM.



Figure 3- View of Maple Street

V. PUBLIC COMMENT

Since the application is a Level II review, a neighborhood meeting is not required. As of the date of

writing this report, no public comments have been submitted. As part of the Traffic Movement Permit, the direct

abutters were notified and notices for the workshop and public hearing were sent to property owners within 500 feet and to the interested citizen's list. The legal ad appeared in the Portland Press Herald on January 14th and 15th editions.

VI. RIGHT, TITLE AND INTEREST AND FINANCIAL/TECHNICAL CAPACITY

a. The owner of the property is Rufus Deering Company. The applicant has provided a copy of a warranty deed, recorded at the Cumberland County Registry of Deeds (Book 14115 Page 13), which demonstrates their right, title and interest in the property.

b. The estimated cost of the development is \$185,000. The applicant has not submitted a financial letter as demonstration of their financial and technical capacity to complete the proposed development.

A potential condition of approval:

Prior to the issuance of a building permit, the applicant shall submit a financial capacity letter for review and approval by the Planning Authority.

VII. ZONING ASSESSMENT

Following are the review comments submitted by Marge Schmuckal, Zoning Administrator:

I have reviewed the most current submission. The applicant wants to change the use from offices to a new school (a public charter high school). The property is located in a B-5b zone which allows the proposed use under, "Public or private schools of any type". There is no proposed addition to the building with the exception of an ADA ramp into the City's ROW along York Street. All changes would be interior to the existing building. There are two floors and a basement.

The B-5b zone has no minimum setbacks required. The 10' maximum setback along street lines is currently being met. There are no changes to change this existing condition. The maximum building height of 65' is more than being met with the two story and daylight basement in the existing building. Section 14-332.1(g) exempts the B-5 & B-5b zones from providing off-street parking requirements in the zone.

All HVAC equipment shall meet the sound level requirements listed under 14-230.5(b). Separate permits are required through Inspection Services for HVAC systems. Such application(s) shall provide HVAC dBA equipment levels. Separate permits are required for any new exterior signs. Signs are also subject to Historic review before issuance.

VIII. SITE PLAN SUBMISSION REQUIREMENTS (Section 14-527)

The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and meets the submission requirements.

A. SITE PLAN STANDARDS (Section 14-526)

The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and applicable regulations. Staff comments are listed below.

1. Transportation Standards

The proposed charter school will generate approximately 154 trips and therefore requires a Traffic Movement Permit (TMP), which the City may issue under delegated review. The applicant had a scoping meeting on October 10, 2012 with Tom Errico, Consultant Traffic Engineer. Mr. Errico finds the TMP acceptable, please see below comment:

- i. I generally agree with the trip generation estimate. I would note that the estimate assumes that students will not be driving their own vehicles. It is likely that some students will drive, but in my professional opinion this added traffic would not be expected to alter the conclusions of the traffic analysis. I would note that the school will not be providing parking for students and therefore it is expected that students will seek on-street parking spaces where time limit parking regulation do not exist.

- a. **Status: I have no further comment.**

a. *Impact on Surrounding Street Systems/ Access and Circulation*

At the workshop meeting, the applicant was proposing that the route for school buses will be arriving from York Street onto Maple Street and load/unload curbside on the west side of the street in a designated School Drop-Off Zone. The route for arriving parents to drop off their students will be directed to enter Maple Street from Commercial Street. The parents would then continue to York Street where they would make a left onto York Street in order to utilize the parking spaces for a school drop-off zone proposed on the north side of York Street (opposite the front door of the school). Due to traffic concerns, the Planning Board members suggested that the bus and parent drop off zones be reconfigured. The bus drop-off is now proposed on York Street in front of the building and the proposed parents drop-off area will be on Maple Street.

Mr. Errico has reviewed the School Drop-Off Zones and offers the following comments:

- i. The project is proposing parent drop-off activity to occur on the north side of York Street, just west of Maple Street. I'm concerned about whether parents will drop off students in this area (particularly those originating from Casco Bay Bridge and points from the west). Additionally, the York Street crosswalk is not likely to be used by students being dropped off, given the indirectness of walking to the crosswalk. It is unclear how much curb space will be designated as school drop-off.

- a. **Status: The applicant in coordination with the City is proposing that bus drop-off activity occur on York Street in front of the school and parent drop-off activity occur on Maple Street. I find this plan to be acceptable.**

- ii. While Maple Street is a relatively low volume street, plans for a nearby hotel/restaurant will increase traffic volume and activity. Details on specific bus loading/unloading activity shall be provided including curb space requirements, flashing light restrictions and how all street activities will be accommodated within the curb-to-curb width of Maple Street.

- a. **Status: The applicant in coordination with the City is proposing that bus drop-off activity occur on York Street in front of the school and parent drop-off activity occur on Maple Street. I find this plan to be acceptable. I would note that conditions shall be monitored within three months following opening of the school and a report shall be provided to the City summarizing existing conditions. Adjustments to the Plan may be required following this monitoring exercise. I would note that additional annual monitoring will be required in conjunction with the TDM Plan.**

A potential condition of approval:

- b. *The bus and parent loading/unloading conditions shall be monitored within three months following the opening of the school and a report shall be provided to the City summarizing existing conditions. Adjustments to the Plan may be required following this monitoring exercise. Additional annual monitoring shall be required in conjunction with the TDM Plan.*

The school intends to have a staff person overseeing the parent drop off operation to keep vehicles orderly on Maple Street. Another staff person will be acting as a crossing guard at the intersection of Maple and York Street where the students will be directed to the front entrance of the school.

The applicant states that some of the students, as well as teachers, will be walking and biking to the site. There is a sidewalk only along the York Street frontage of the building, but it does not extend between this site to the High Street intersection. There is a sidewalk on Maple Street only along the building frontage. All sidewalks are in brick. Please refer to Paragraph 3a for further discussion on the sidewalks.

According to the traffic study, the intersection of Commercial and High Streets is a high crash location. Tom Errico states that the “City has plans for implementing improvements at this location and has been requesting monetary contributions for the implementation of improvements. Based upon standard cost methods, the applicant shall contribute \$2,200.00 towards future improvements at the Commercial Street/High Street intersection”.

A potential condition of approval:

The applicant shall contribute \$2,200.00 towards future improvements at the Commercial Street/High Street intersection prior to the issuance of a building permit.

b. Parking

According to the applicant, staff members who elect to drive their own vehicles to school will be parking in the City of Portland parking garage on Spring Street and will walk to school via existing sidewalks. The applicant has submitted a Letter of Intent to Lease for the parking spaces at the garage (Attachment H). Initially, the applicant proposed securing twenty (20) parking spaces, but the recent submission cites fifteen (15) spaces.

The Consultant Traffic Engineer recommends:

- i. The applicant shall provide information on the location and the number of off-site parking spaces to be allocated to the school. The traffic study notes that 20 parking spaces will be initially leased for the project, but I would suggest that full-build parking plans be a consideration, for planning purposes.
 - a. **Status:** The applicant is leasing 15 parking spaces at the Spring Street garage. It is recommended that in conjunction with their TDM Plan, a parking demand evaluation be conducted and in consultation with the City revisions to the number of off-site parking spaces be identified. The parking evaluation should be conducted annually until one year after the school is fully occupied.

A potential condition of approval:

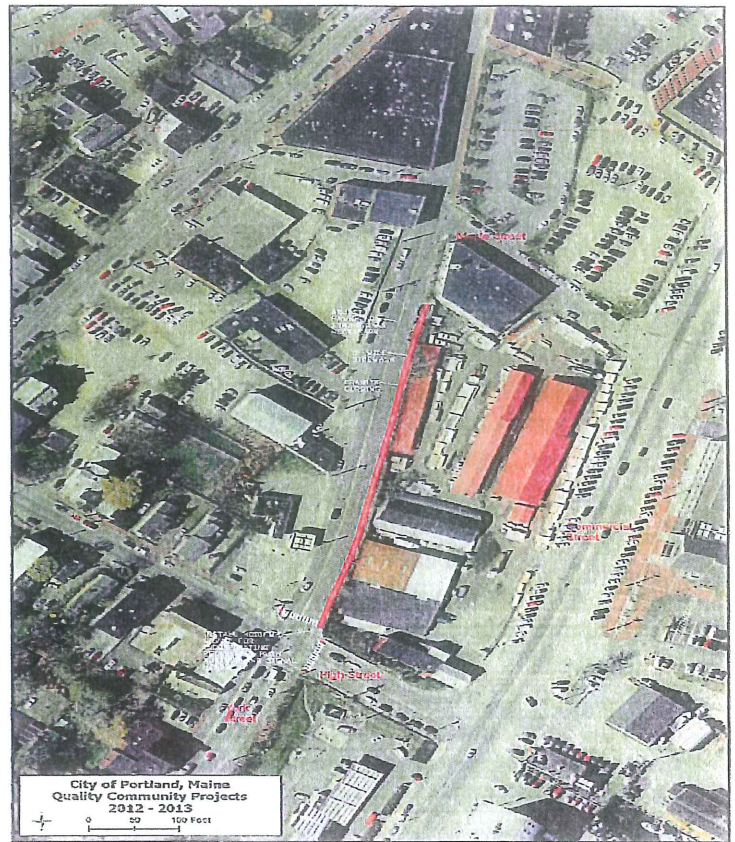
As presented in the application, the applicant proposes to lease fifteen (15) off-site parking spaces at the Spring Street garage. As part of the TDM plan, the parking demand evaluation shall be conducted and in consultation with the City and that any revisions to the number of off-site parking spaces needed, shall be identified. The parking evaluation shall be conducted one year after the school is fully occupied and then annually.

c. Bicycle Parking

Currently there is no bicycle parking on site. The applicant is proposing to install five bicycle parking spaces at the back of the building in the alleyway and eighteen (18) on Maple Street along the building. The applicant is required to install three bicycle parking spaces for the twenty (15) parking spaces being proposed. Mr. Errico has stated that “a parking demand evaluation be conducted and in consultation with the City revisions to the number of off-site parking spaces be identified. The parking evaluation should be conducted annually until one year after the school is fully occupied”.

d. *Transportation Demand Management (TDM)*
 A transportation demand management plan has been submitted. According to the plan (Attachment K), the School is encouraging variety of transportation options for students and staff. These options include charter buses, providing incentives for commuters on the METRO or South Portland Bus services, and encouraging carpooling and walking and biking. The School believes if they do not provide parking for students, the students will be discouraged from driving their own vehicles. Tom Errico has reviewed the plan and recommends an annual monitoring plan, please see his comments below:

- i. A TDM study shall be prepared for the project. I will review the details of the study when provided. I would note that it will be my recommendation that an annual TDM/Traffic Management Plan be provided to the City for review and comment.
 - a. **Status: I have reviewed the TDM Plan and generally find it to be acceptable. It is important to note that a detailed annual monitoring plan (with required action items) is required to access pick-up and drop-off activities, off-site parking needs, and bicycle storage needs.**



YORK STREET SIDEWALK TE PROJECT

A potential condition of approval:

Annually (from the date of receiving a certificate of occupancy) applicant shall submit a detailed monitoring plan that addresses access for the bus and parent pick-up and drop-off activities, off-site parking needs, and bicycle storage needs.

2. Environmental Quality Standards

a. *Landscaping*

The final site plan must be submitted for review that incorporates Jeff Tarling, City Arborist review comments (Attachment 5). Mr. Tarling recommends the replanting of one street tree on Maple Street to replace the dead tree and recommends that the tree type be either 'Armstrong' Red Maple or other upright species such as upright Pin Oak, or English Oak.

A potential condition of approval:

A final site plan must be submitted for review that incorporates Jeff Tarling, City Arborist review comments from his memo dated July 12, 2012.

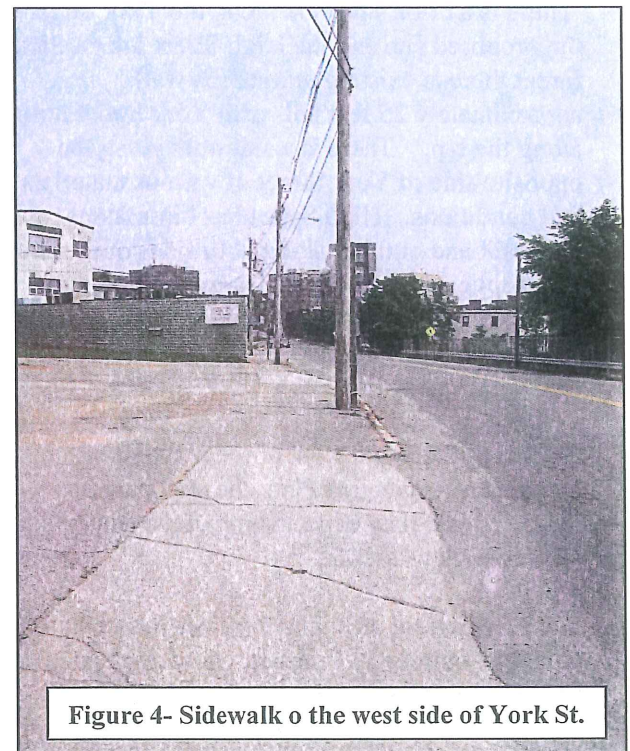


Figure 4- Sidewalk o the west side of York St.

b. *Water Quality, Storm Water Management and Erosion Control*

The project consists of converting the existing Rufus Deering Lumber Milling Display Store into the Baxter Academy for Technology and Science; no building additions are anticipated.

The existing site is completely impervious; therefore, the project will not create any new impervious areas.

3. Public Infrastructure and Community Safety Standards

a. *Sidewalk*

This lot is a separate lot, but is under the same ownership as the Rufus Lumber Yard and thus is considered as one tract of land. Therefore this site encompasses an entire city block. The site plan ordinance (Section 14-526 (a) (2) c. i) states

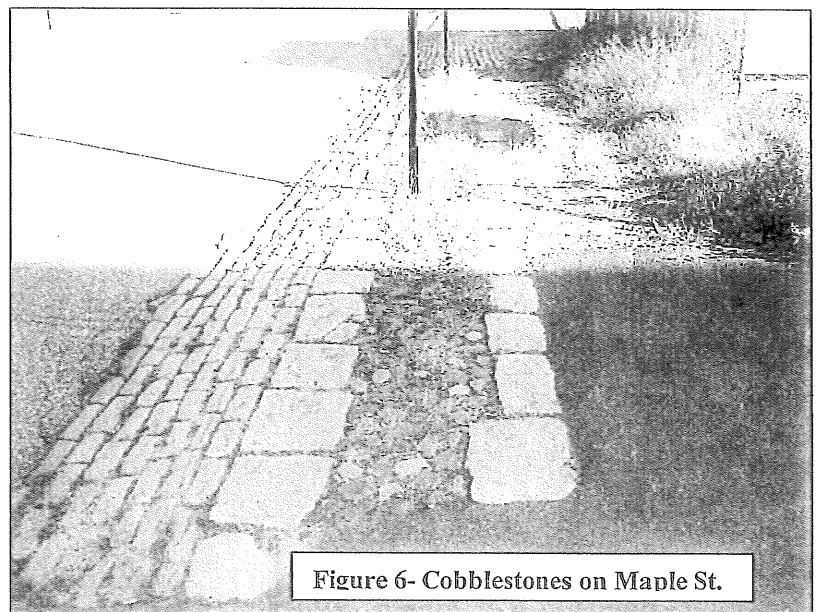
“all developments shall provide sidewalks along all frontages in accordance with Section 14-498 and 14-499 of the City Code, installed to City specifications as described in Section 1 of the Technical Manual. An applicant may request a waiver from curb and sidewalk installation requirements if they meet applicable waiver criteria listed in Section 14-506 (b) of the code.”

Thus the above provision applies to all four frontages of the site. Currently, there are existing brick sidewalks along the Maple Street building frontage, but there is no sidewalk between the proposed site and Commercial Street. There are two driveway cuts on the side of Rufus Deering Lumber Yard. The lower portion of the right-of way does have historic cobble stones, please see photos.

There is a brick sidewalk along the York Street building frontage, but again, there is no sidewalk or curbing between the proposed site and the High Street intersection (approximately 620 feet of frontage). Along this portion of York Street there is existing granite sea wall, approximately 25 feet tall, with York Street running along the top. There is a sidewalk along the opposite side of York Street of various materials and conditions. High Street has bituminous sidewalk and curbing along both sides of the street. The applicant has requested waivers from the sidewalk and curb requirements to install brick sidewalk along York and Maple Streets frontage (see Attachment I).

As part of the application and the Transportation Demand Management Plan, the applicant has indicated that they expect some of the students and staff to walk to the site.

The Department of Public Services has made a Quality Community Program Grant application to the MaineDOT this past August to install a sidewalk at this location. The decision of the application is not expected until Spring 2013 and if it is funded, the



federal portion of the funding becomes available in October 2013 at the earliest. The total local cash match for the project is \$22,000. The application includes costs of the sidewalk, improvement to ADA accessibility at the intersection of York and High Streets, including curb ramps and pedestrian signals.

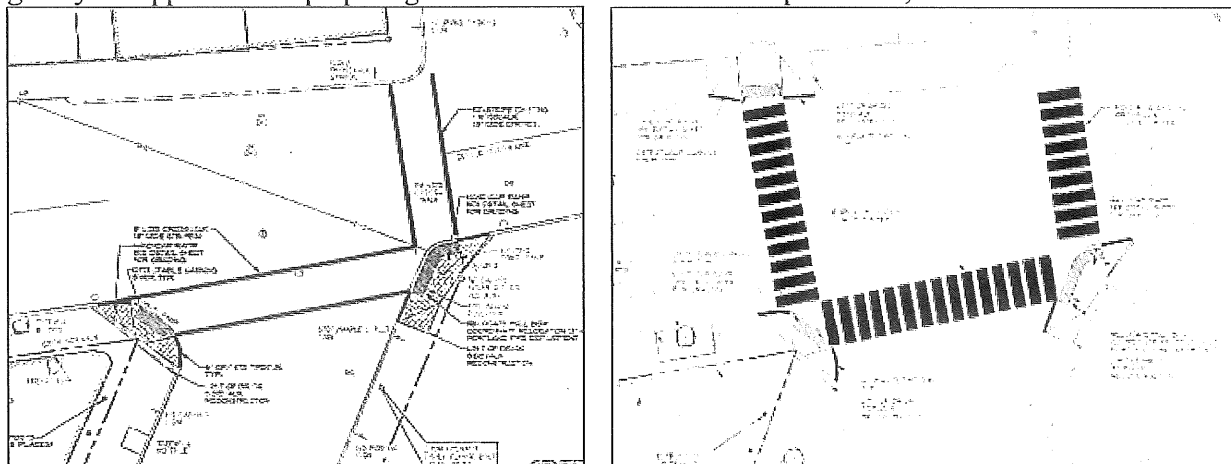
Based upon the anticipated pedestrian activity of the high school students, staff and parents, the City staff and the consulting traffic engineer do not support the requested sidewalk waivers. The staff recommend that both the Maple and York Street sidewalk segments should be constructed. Department of Public Services and Planning Staff recommend that applicant make the contribution of the local match of \$22,000. If the City does not receive the grant, it will be the responsibility of the applicant to construct the sidewalk. The City Staff has recommended that the cobble stones along Maple Street remain and be integrated into the sidewalk. With the retention of the historic cobble stones, curbing will not be needed and could be waived along Maple Street.

Mr. Errico has reviewed the final plans and does not recommend waiving the sidewalk waivers for both York and Maple Streets based on the following comments:

- i. In my professional opinion, the provision of a sidewalk along the south side of York Street between High Street and the school is critically important for this project as students are expected to walk to school. The City has submitted a grant application for the noted sidewalk, but it is unknown if funding will be secured. A similar grant application was submitted previously and the City was not successful. If funding is obtained, it is unlikely a sidewalk will be available by the time school opens in September 2013. I strongly suggest that a sidewalk be installed before the school opens, given anticipated pedestrian activity.
 - a. **Status: The City does not believe this application meets the sidewalk waiver policy and therefore a sidewalk shall be constructed. Fundamental to not meeting the waiver criteria is the expectation of pedestrian usage along the south side of York Street. Pedestrians currently walk in the road and in my opinion there is expectation that some Baxter Academy students will walk along the south side of York Street. I would note that if the City is successful in obtaining a grant application the applicant would only be responsible for providing funds for the City's required local match.**
- ii. The applicant has submitted a waiver for construction of a short section of sidewalk on Maple Street near Commercial Street. The City does not believe this application meets the sidewalk waiver policy and therefore a sidewalk shall be constructed. Pedestrian activity is currently present and pedestrian demand along this sidewalk is expected.

b. *Crosswalks*

Originally the applicant was proposing the crosswalks at York and Maple Streets, as seen in the first below diagram:



During the workshop meeting, Board members suggested that there be a crosswalk a midblock crossing near the entrance of the building. The applicant went to the Crosswalk Committee on January 3, 2013 and were approved. The applicant is providing an improvement plan that upgrades the existing crosswalk and sidewalk ramps and installs a new crosswalk. The second diagram aboveshow the revised approved crosswalk design.

c. *Public Safety and Fire Prevention*

Captain Chris Pirone of Fire Department has reviewed the submitted site plans and finds the proposed site plan to be acceptable.

d. *Availability and Adequate Capacity of Public Utilities*

The current building has public utilities that are available at the site. The school does not intend on having a kitchen and lunch will be outsourced to a local vender.

4. Site Design Standards

a. *Snow and Ice Loading*

Snow and Ice Loading area has not been shown on the site plan. It has been discussed with the School that the applicant will be responsible for snow removal on site.

A potential condition of approval:

It shall be the responsibility of the applicant for all snow removal on site so to have pedestrian pathway clear for the use of the students and staff.

b. *Historic Resources*

The project site is located in the Old Port Historic District. The project was reviewed and approved by the Historic Preservation Board on January 16, 2013.

c. *Exterior Lighting*

A lighting plan has not been submitted for review. Staff will need to review and approve the photometric plan and catalogue cuts of proposed lighting fixtures.

A potential condition of approval:

A final lighting plan, including a photometric plan and catalogue cuts for the proposed exterior lighting fixture meeting the City's technical standards, shall be submitted for review and approval by the Planning Authority prior to the issuance of a building permit.

d. *Signage and Wayfinding*

A traffic signage and wayfinding plan has been submitted for review. Mr. Errico finds the plan acceptable with "Specific details on implementation will need to be coordinated and approved by the City before installation".

A condition of approval:

Specific details on implementation of traffic signage shall be coordinated and approved by the Planning Authority before installation.

e. *Zoning Related Design Standards*

The proposal is a reuse of an existing building. There are no additions or alterations being proposed to the building.

IX. PLANNING BOARD PROPOSED MOTIONS

WAIVERS

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report # 05-2013 for application # 2012-519 relevant to the Portland Technical and Design Standards and Guidelines and other regulations and the testimony presented at the Planning Board hearing:

- i. The Planning Board **waives / does not waive** the requirement of Section 14-526 (a) (2) (c) of the Site Plan Ordinance that all development shall provide sidewalk and curbing along all frontages, to allow no sidewalk and curbing on the east side of the York Street (Attachment Plan 3 to Planning Board Report # 05.2013).
- ii. The Planning Board **waives / does not waive** the requirement of Section 14-526 (a) (2) (c) of the Site Plan Ordinance that all development shall provide sidewalk and curbing along all frontages, to allow no sidewalk and curbing on the lower east side on Maple Street (Attachment Plan 3 to Planning Board Report # 05.2013).

1. TRAFFIC MOVEMENT PERMIT

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #05-2013 for application # 2012-519 relevant to the Traffic Movement Permit and the testimony presented at the Planning Board hearing, the Planning Board finds the plan **is not** in conformance with the standards for Traffic Movement Permit application for Baxter Academy for Technology and Science and **grants/does not grant** a permit subject to the following condition:

- i. As part of the TDM plan, the parking demand evaluation shall be conducted and in consultation with the City that revisions to the number of off-site parking spaces shall be identified. The parking evaluation shall be conducted annually until one year after the school is fully occupied.

2. SITE PLAN REVIEW

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #05-2013 for application # 2012-519 relevant to the Site Plan and other regulations, and the testimony presented at the Planning Board hearing, the Planning Board finds that the plan **is / is not** in conformance with the site plan standards of the land use code, subject to the following conditions to be met prior to the issuance of a building permit unless otherwise stated:

- i. The applicant shall submit a financial capacity letter for review and approval by the Planning Authority;
- ii. The bus and parent loading/unloading conditions shall be monitored within three months following the opening of the school and a report shall be provided to the City summarizing existing conditions. Adjustments to the Plan may be required following this monitoring exercise. Additional annual monitoring shall be required in conjunction with the TDM Plan;
- iii. The applicant shall contribute \$2,200.00 towards future improvements at the Commercial Street/High Street intersection;
- iv. As presented in the application, the applicant proposes to lease fifteen (15) off-site parking spaces at the Spring Street garage. As part of the TDM plan, the parking demand evaluation shall be conducted and in consultation with the City and that any revisions to the number of off-site parking spaces needed, shall be identified. The parking evaluation shall be conducted one year after the school is fully occupied and then annually;

- v. On an annual basis, that applicant shall submit a detailed monitoring plan that addresses access the bus/ Annually (from the date of receiving a certificate of occupancy) applicant shall submit a detailed monitoring plan that addresses access for the bus and parent pick-up and drop-off activities, off-site parking needs, and bicycle storage needs;
- vi. It shall be the responsibility of the applicant for all snow removal on site, so to have pedestrian pathways clear for the use of the students and staff;
- vii. A final site plan must be submitted for review that incorporates Jeff Tarling, City Arborist review comments from his memo dated July 12, 2012;
- viii. A final lighting plan, including a photometric plan and catalogue cuts for the proposed exterior lighting fixture meeting the City's technical standards, shall be submitted for review and approval by the Planning Authority prior to the issuance of a building permit;
- ix. Specific details on implementation of traffic signage shall be coordinated and approved by the Planning Authority before installation.

If the waivers are not be granted:

- x. The applicant shall submit the plans for the York and Maple Streets sidewalk for review and approval prior to the release of a building permit. The monetary contribution for York Street, if applicable, or the sidewalks shall be installed prior to the issuance of a certificate of occupancy.

ATTACHMENTS:

PLANNING BOARD MEMO ATTACHMENTS

- 1. Marge Schmuckal Review 06.18.2012 and 01.06.2013
- 2. Tom Errico Review 01.17.2013
- 3. David Senus Review 01/15/2013
- 4. Jeff Tarling Review 07.12.2012
- 5. Bruce Hyman, Crosswalk Committee 01.03.2013

APPLICANT'S SUBMITTAL

- A. Written Description of Project
- B. Application
- C. Description of Charter School
- D. Photos of Existing Site
- E. Development Description
- F. Surveyor's Report
- G. Warranty Deed
- H. Intent to Lease Agreement
- I. Traffic Signage Plan
- J. Sidewalk Waiver Request
- K. Summary Package for Traffic Movement Permit (TMP)
- L. Baxter Academy for Technology and Science (TMP)
- M. Traffic Movement Permit Application

PLANS

- Plan 1 Cover Sheet
- Plan 2 Existing Conditions Plan
- Plan 3 Site Plan
- Plan 4 Details
- Plan 5 Code Summary
- Plan 6 Existing Lower Level Plan
- Plan 7 Existing First Floor Plan with Proposed Changes
- Plan 8 Existing Second Floor Plan with Proposed Changes
- Plan 9 Door and Hardware Schedules Wall Details and Stair Railings
- Plan10 York Street Elevation

Shukria Wiar - 54 York Street

From: Marge Schmuckal
To: Shukria Wiar
Date: 1/16/2013 3:57 PM
Subject: 54 York Street

Hi Shukria,

I do not have any zoning concerns for this project. My comments and requirements for HVAC and signs are still in force. Inspection Services will require a change of use permit prior to use.

Marge

Shukria Wiar - 54 York St - Baxter Academy

From: Marge Schmuckal
To: Shukria Wiar
Date: 6/18/2012 3:20 PM
Subject: 54 York St - Baxter Academy

54 York Street - 042-A-7 & 8 - B-5b Zone & Historic Overlay
2012-519 Level II site plan review
June 18, 2012

I have reviewed the most current submission. The applicant wants to change the use from offices to a new school (a public charter high school). The property is located in a B-5b zone which allows the proposed use under, "Public or private schools of any type". There is no proposed addition to the building with the exception of an ADA ramp into the City's ROW along York Street. All changes would be interior to the existing building. There are two floors and a basement.

The B-5b zone has no minimum setbacks required. The 10' maximum setback along street lines is currently being met. There are no changes to change this existing condition.

The maximum building height of 65' is more than being met with the two story and daylight basement in the existing building.

Section 14-332.1(g) exempts the B-5 & B-5b zones from providing off-street parking requirements in the zone.

All HVAC equipment shall meet the sound level requirements listed under 14-230.5(b). Separate permits are required through Inspection Services for HVAC systems. Such application(s) shall provide HVAC dBA equipment levels.

Separate permits are required for any new exterior signs. Signs are also subject to Historic review before issuance.

Marge Schmuckal
Zoning Administrator

Shukria Wiar - 54 York Street - Baxter Academy

From: Tom Errico <thomas.errico@tylin.com>
 To: Shukria Wiar <SHUKRIA.W@portlandmaine.gov>
 Date: 1/17/2013 11:18 AM
 Subject: 54 York Street - Baxter Academy
 CC: David Margolis-Pineo <DMP@portlandmaine.gov>, Katherine Earley <KAS@port...

Shukria – The following represents my final comments.

- The project is proposing parent drop-off activity to occur on the north side of York Street, just west of Maple Street. I'm concerned about whether parents will drop off students in this area (particularly those originating from Casco Bay Bridge and points from the west). Additionally, the York Street crosswalk is not likely to be used by students being dropped off, given the indirectness of walking to the crosswalk. It is unclear how much curb space will be designated as school drop-off.
Status: The applicant in coordination with the City is proposing that bus drop-off activity occur on York Street in front of the school and parent drop-off activity occur on Maple Street. I find this plan to be acceptable.
- I generally agree with the trip generation estimate. I would note that the estimate assumes that students will not be driving their own vehicles. It is likely that some students will drive, but in my professional opinion this added traffic would not be expected to alter the conclusions of the traffic analysis. I would note that the school will not be providing parking for students and therefore it is expected that students will seek on-street parking spaces where time limit parking regulation do not exist.
Status: I have no further comment.
- A TDM study shall be prepared for the project. I will review the details of the study when provided. I would note that it will be my recommendation that an annual TDM/Traffic Management Plan be provided to the City for review and comment.
Status: I have reviewed the TDM Plan and generally find it to be acceptable. It is important to note that a detailed annual monitoring plan (with required action items) is required to access pick-up and drop-off activities, off-site parking needs, and bicycle storage needs.
- The applicant shall provide information on the location and the number of off-site parking spaces to be allocated to the school. The traffic study notes that 20 parking spaces will be initially leased for the project, but I would suggest that full-build parking plans be a consideration, for planning purposes.
Status: The applicant is leasing 15 parking spaces at the Spring Street garage. It is recommended that in conjunction with their TDM Plan, a parking demand evaluation be conducted and in consultation with the City revisions to the number of off-site parking spaces be identified. The parking evaluation should be conducted annually until one year after the school is fully occupied.
- In my professional opinion, the provision of a sidewalk along the south side of York Street between High Street and the school is critically important for this project as students are expected to walk to school. The City has submitted a grant application for the noted sidewalk, but it is unknown if funding will be secured. A similar grant application was submitted previously and the City was not successful. If funding is obtained, it is unlikely a sidewalk will be available by the time school opens in September 2013. I strongly suggest that a sidewalk be installed before the school opens, given anticipated pedestrian activity.
Status: The City does not believe this application meets the sidewalk waiver policy and therefore a sidewalk shall be constructed. Fundamental to not meeting the waiver criteria is the expectation of pedestrian usage along the south side of York Street. Pedestrians currently walk in the road and in my opinion there is expectation that some Baxter Academy students will walk along the south side of York Street. I would note that if the City is successful in obtaining a grant application the applicant would only be responsible for

providing funds for the City's required local match.

- While Maple Street is a relatively low volume street, plans for a nearby hotel/restaurant will increase traffic volume and activity. Details on specific bus loading/unloading activity shall be provided including curb space requirements, flashing light restrictions and how all street activities will be accommodated within the curb-to-curb width of Maple Street.

Status: The applicant in coordination with the City is proposing that bus drop-off activity occur on York Street in front of the school and parent drop-off activity occur on Maple Street. I find this plan to be acceptable. I would note that conditions shall be monitored within three months following opening of the school and a report shall be provided to the City summarizing existing conditions. Adjustments to the Plan may be required following this monitoring exercise. I would note that additional annual monitoring will be required in conjunction with the TDM Plan.
- The crosswalk on York Street shall be upgraded to "block" style painted markings. The sidewalk ramps may need to be upgraded to comply with City/ADA standards. Detailed recommendations on the sidewalk will be provided by DPS.

Status: The applicant has provided an improvement plan that upgrades the existing crosswalk and sidewalk ramps and installs a new crosswalk. I find this plan to be acceptable.
- The specific location of bicycle racks shall be identified (the study notes they will be located on Maple Street). The original application indicated 5 bicycle parking locations to be provided. Given the type of activity expected, I would suggest additional bicycle spaces be allocated.

Status: The applicant shall be required to provide additional bicycle racks as determined through the conclusions of the annual monitoring program of the TDM Plan.
- The proposed "5 MIN. SCHOOL DROP-OFF ZONE" parking regulations will need to be coordinated with John Peverada. My initial conversation with John is that this is workable, although specific regulations and time limits need to be evaluated. It should be noted that some parking regulation changes may require approval by the City Council for a parking schedule revision. If required, the applicant will be responsible for providing materials in support of the parking schedule change.

Status: The applicant has submitted a signage plan that I find acceptable. Specific details on implementation will need to be coordinated and approved by the City before installation.
- It is unclear what is being proposed along York Street in front of the subject property. Prior site plans have indicated that handicapped parking spaces will be provided with a sidewalk ramp connection to the street. Previous comments by the City have been in opposition to the provision of handicapped parking spaces on the street.

Status: This comment is no longer relevant. York Street in the subject area is proposed for bus drop-off activity.
- As noted in the traffic study, the intersection of Commercial Street and High Street is a High Crash Location. The City has plans for implementing improvements at this location and has been requesting monetary contributions for the implementation of improvements. I will provide information on the contribution amount in the future.

Status: Based upon standard cost methods the applicant shall contribute \$2,200.00 towards future improvements at the Commercial Street/High Street intersection.
- The applicant has submitted a waiver for construction of a short section of sidewalk on Maple Street near Commercial Street. The City does not believe this application meets the sidewalk waiver policy and therefore a sidewalk shall be constructed. Pedestrian activity is currently present and pedestrian demand along this sidewalk is expected.

If you have any questions, please contact me.

From: "David Senus" <dsenus@woodardcurran.com>
To: "Shukria Wiar" <SHUKRIAW@portlandmaine.gov>
Date: 1/15/2013 12:40 PM
Subject: RE: Baxter Charter School Final Application

We will not have any comments.

Thanks,
Dave

-----Original Message-----

From: Shukria Wiar [mailto:SHUKRIAW@portlandmaine.gov]
Sent: Tuesday, January 15, 2013 9:34 AM
To: Chris Pirone; David Margolis-Pineo; John Peverada; Jeff Tarling;
Marge Schmuckal; Thomas Errico; David Senus
Subject: Baxter Charter School Final Application

Hello:

Baxter School has submitted their final application and is uploaded into eplan. This project is on for a public hearing on Jan. 22 so the report is going out on Friday. Please get me your final comments by Thursday. If you do not have any comments, please state that.

Thanks you.

Shukria

Notice: Under Maine law, documents - including e-mails - in the possession of public officials or city employees about government business may be classified as public records. There are very few exceptions. As a result, please be advised that what is written in an e-mail could be released to the public and/or the media if requested.

--

Shukria Wiar - 54 York Street Project

From: Jeff Tarling
To: Barbara Barhydt ; Shukria Wiar
Date: 7/12/2012 12:16 PM
Subject: 54 York Street Project
CC: David Margolis-Pineo

Hi Shukria -

In review of the landscape portion of the proposed 54 York Street project I would recommend the replanting of one street tree on Maple Street to replace the dead tree.

Recommended tree type would be 'Armstrong' Red Maple or other upright species such as upright Pin Oak, or English Oak.

Jeff Tarling

Shukria Wiar - Baxter Academy: Crosswalk Committee Action

From: Bruce Hyman
To: Barbara Barhydt; Shukria Wiar; Tom Errico
Date: 1/3/2013 10:14 AM
Subject: Baxter Academy: Crosswalk Committee Action
CC: David Margolis-Pineo; Jeremiah Bartlett; Katherine Earley

Good morning, Shukria,

This morning, the city's Crosswalk Committee deliberated on the proposed curb ramp and crosswalk configurations as shown in the Baxter Academy site plan dated 12/14/12 per Sebago Technics (sheet 3 of 4). The Crosswalk Committee approves the new crosswalk across York Street on the west side of Maple Street, as shown. The Committee understood that there is to be new exterior lighting on the Baxter Academy building as well the existing exterior lighting on the Portland Pie building to adequately light the new crossing during the nighttime.

Please do not hesitate to contact me if you require any additional information.

Best regards,

Bruce

Bruce Hyman
Bicycle & Pedestrian Program Coordinator

City of Portland, Maine
Dept. of Public Services
bhyman@portlandmaine.gov
207-874-8833



January 10, 2013

07383

Shukria Wiar, Planner
City of Portland Planning Division
City Hall 4th Floor
389 Congress St.
Portland, ME 04101

Site Plan Application Submittal – Change of Use Permit-Final Plans
Proposed Baxter Academy for Technology and Science –Charter School
54 York Street, Portland, ME

Dear Ms. Wiar:

Please find a copy of the Final Site Plan packet and associated reports that we have prepared for the Baxter Academy for Technology and Science on property owned by Rufus Deering Lumber Company at 54 York Street, located on the corner of York Street and Maple Street in Portland Maine. The parcel proposed for development is the existing Rufus Deering Lumber Milling display store with other mixed uses for offices, labs, and dance studios. The property consists of a 0.22-acre lot fully developed with an existing 7,540 square foot (SF) footprint for a 3-story building zoned in the B-5b (Urban Commercial Business Zone). The proposed use will entail a floor area conversion of over 10,000 SF change of use and will require a review by the Planning Staff. It is also within the Old Port Historical District.

The overall improvement of the site will not add any additional impervious areas, as the site now is completely impervious. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. Their vision is to convert the entire building into a full 4 year high school over the next 3 to 4 years. Due to the Baxter Academy's plan to convert the existing structure into a school, all the retrofitting will occur with the interior spacing, and no building additions are anticipated to the exterior. Therefore, items such as stormwater and existing utilities services will not be altered as a result of the change of use. No stormwater management is proposed due to the lack of site work required on site; therefore, we request a waiver of the standard.

Rufus Deering Lumber Company will provide the lease for the Baxter Academy and will be incorporating all necessary interior building improvements for the Charter School. We anticipate that the Baxter Academy will be a long-term lease as the Charter School envisions an enrollment of 320 students by 2014. However, in the first year, the expectation is an enrollment near 160 students. Our primary issue on the site has to deal with the transportation of students and parking arrangements for faculty and administrators.

Based on the City's parking requirements, school parking requirements are based on age. One regulation is based on parking for schools with students 15 years of age and under, and one deals with students 16 years and older. Baxter Academy has a letter of intent to lease parking spaces in the City's Spring Street Parking Garage and would expand that as the school expands in the future. Options did exist with the Gulf of Maine Research Institute parking off Commercial Street but due to zoning issues, that parking was not fully approved. If that parking becomes acceptable, the School may acquire leased spaces from there in the future.

In addition, the Director, John Jacques, has indicated that the Charter School intends to contract for bus transportation for three full sized buses for student transportation in year one, which is the equivalent of upwards of 160 students. Other students could take other modes of public transportation or be dropped

off by parents. Student parking will not be encouraged or provide by the school. York Street has been designated as a location for bus delivery and pick up of students directly in front of the school's entrance. Buses will be loaded/unloaded from the existing sidewalk adjacent to the proposed school building. A full Traffic Management Demand Plan and Traffic Report have been submitted under separate cover. We have been coordinating with staff on this plan along with the Sidewalk Committee to resolve final suggested crosswalks to accommodate student pedestrian foot traffic across Maple and York Streets. The Sidewalk Committee agreed to the proposed location of a sidewalk from the upper end of Maple Street to a midpoint location in York Street which appears a natural extension and will provide convenient crossings for the school students. The increase in trips potentially impacting the site due to the change of use will require a Traffic Movement Permit (TMP), which the City has delegated authority to approve from the State of Maine Department of Transportation with Planning Board review and approval.

We are requesting a waiver of the sidewalk and curbing construction requirement along York Street. A separate request is included in the TMP package explaining our position for the waiver under the City's Sidewalk and Curbing Construction requirements. In addition, the State has determined that the site does not need to provide a dedicated handicap parking space. Americans with Disabilities Act accessibility will be provided through the main entrance off York Street. Baxter Academy and/or Rufus Deering will clear all sidewalks on the lots frontage of York and Maple Streets and will contract to have snow removed as there is no appreciable location for storage. It is the Academy's interest to maintain a clean and safe sidewalk for its students and staff as well as other public pedestrians.

Rufus Deering Lumber Company is very anxious to start construction immediately as the Charter School wishes to begin this 2013 school year. Rufus Deering has provided the Historical Preservation Committee with information as to their planned exterior revisions for lighting, signs, and general improvements. We have provided architectural elevation views and plans, and site improvements plans also for your review. Our understanding is a hearing on that will be held next week. We intend to provide the Planning Department results or conditions of the committee's findings prior to our proposed hearing.

The applicant is still coordinating and reviewing HVAC systems, and will prefer to provide these as a condition of approval. Lighting is being provided by the owner and is to mimic the lighting similar to Portland Pie Co. directly across the street where the lighting is reflected back into the building and sidewalk from a curled pipe extension and shield light pan. We will provide those cut sheets once we know they have been accepted by the Historical Committee.

We look forward to meeting with the Planning Board on January 22nd to further discuss the TMP and TMD as well as the general improvements planned for the Baxter Academy especially with parking and transportation needs.

If you have any questions, please feel free to contact me at our office (207-200 2083). We look to working with the Planning Division staff and City engineers to facilitate the opening of this new opportunity for a Charter School in the Downtown Portland area.

Sincerely,

SEBAGO TECHNICS, INC.


James R. Seymour, P.E.
Project Manager

JRS:jrs/kn
Enc.

cc: Dan Labrie, President, Rufus Deering Lumber Company
John Jacques, Executive Director, Baxter Academy for Technology & Science

Effective August 18, 2010



Level II – Preliminary and Final Site Plans Development Review Application Portland, Maine

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Level II: Preliminary or Final Site Plan. Please note that Portland has delegated review from the State of Maine for reviews under the Site Location of Development Act, Chapter 500 Stormwater Permits, and Traffic Movement Permits. General information pertaining to the thresholds of review, public noticing procedures and the fee structure is contained in the Notice to Developer's Packet.

Level II: Site Plan Development includes:

- New structures with a total floor area of less than 10,000 sf except in Industrial Zones.
- New structures with a total floor area of less than 20,000 sf in Industrial Zones.
- Any new temporary or permanent parking area, paving of an unpaved surface parking area, or creation of other impervious surface area greater than 7,500 sf.
- Building addition(s) with a total floor area of less than 10,000 sf (cumulatively within a 3 year period) except in Industrial Zones.
- Building addition(s) with a total floor area of less than 20,000 sf in Industrial Zones.
- Park improvements: New structures or buildings with a total floor area of less than 10,000 sf, facilities encompassing an area of greater than 7,500 sf and less than 20,000 sf (excludes rehabilitation or replacement of existing facilities).
- New piers, docks, wharves, bridges, retaining walls, and other structures within the Shoreland Zone.
- Land disturbance between 1 and 3 acres (includes stripping, grading, grubbing, filling or excavation).
- A change in the use of a total floor area between 10,000 and 20,000 sf in any existing building (cumulatively within a 3 year period).
- Construction of a lodging house, bed and breakfast facility, emergency shelter or special needs independent living unit.
- Signage subject to approval pursuant to Section 14-526 (d) 8.a. (IV) of the Land Use Code.
- Any new major or minor auto service station with less than 10,000 sf of building area that is outside the B-2 or B-5 zones.
- The creation of day care or home babysitting facilities to serve more than 12 children in a residential zone (not permitted as a home occupation under section 14-410) in any principal structure that has not been used as a residence within the 5 years preceding the application.

The Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division
Fourth Floor, City Hall
389 Congress Street
(207) 874-8721 or 874-8719

Office Hours
Monday thru Friday
8:00 a.m. – 4:30 p.m.

B.1

PROJECT NAME: BAXTER ACADEMY for TECHNOLOGY & SCIENCE

PROPOSED DEVELOPMENT ADDRESS:
54 YORK ST.

PROJECT DESCRIPTION:
INTERIOR CHANGE OF USE FROM OFFICES TO
CHARTER (PUBLIC) SCHOOL > 10,000 SF FLOOR SPACE.

CHART/BLOCK/LOT: MAP 42 - BLOCK A PRELIMINARY PLAN 6/5/12 (date)
LOTS 7 & 8 FINAL PLAN _____ (date)

CONTACT INFORMATION:

Applicant's Contact for electronic plans
Name: JAMES SEYMOUR P.E.
e-mail: Jseymour@sebagotech.com
work #: 200-2083

Applicant - must be owner, Lessee or Buyer
Name: DAN LABRIE
Business Name, if applicable: RUFUS DEERING LUMBER
Address: 383 COMMERCIAL ST. Co.
City/State: PORTLAND, ME Zip Code: 04101

Applicant Contact Information
Work # 772-6505
Home# 767-1055
Cell # 838-0444 Fax# 772-6981
e-mail: Dlabrie@rufusdeering.com

Owner - (if different from Applicant)
Name:
Address: SAME
City/State: Zip Code:

Owner Contact Information
Work #
Home#
Cell # Fax#
e-mail:

Agent/ Representative
Name:
Address: N/A
City/State: Zip Code:

Agent/Representative Contact information
Work #
Cell #
e-mail:

Billing Information
Name: APPLICANT.
Address:
City/State: Zip Code:

Billing Information
Work #
Cell # Fax#
e-mail:

Engineer SEBAGO TECHNICS INC Name: JAMES SEYMOUR, P.E. Address: 75 JOHN ROBERTS ROAD, SUITE 1A City/State: So. PORTLAND, ME Zip Code: 04106	Engineer Contact Information Work # 200-2083 Cell # 632-1199 Fax# 856-2256 e-mail: jseymour@sebagotechnics.com
Surveyor SEBAGO TECHNICS INC Name: WILLIAM SHIPPEN PLS Address: 75 JOHN ROBERTS RD, SUITE 1A City/State: So. PORTLAND, ME Zip Code: 04106	Surveyor Contact Information Work # 200-2084 Cell # Fax# 856-2256 e-mail: wshippen@sebagotechnics.com
Architect RICK RENNER ARCHITECTURE Name: CHARLES YOUNG Address: 35 Pleasant St. City/State: PORTLAND, ME Zip Code: 04101	Architect Contact Information Work # 773-9699 x 203 Cell # Fax# e-mail: cyoung@rrennerarchitects
Attorney PEARCE & DOW Name: MIKE PEARCE Address: 50 MONUMENT SQ. City/State: PORTLAND, ME Zip Code: 04101	Attorney Contact Information Work # 822-9900 Cell # Fax# 822-9901 e-mail:

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

Level II Development (check applicable reviews) <input checked="" type="checkbox"/> Less than 10,000 sq. ft. (\$400.00) <input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)	Fees Paid (office use) _____ _____	Other Reviews (check applicable reviews) <input type="checkbox"/> Traffic Movement (\$1,000) <input type="checkbox"/> Stormwater Quality (\$250) <input type="checkbox"/> Section 14-403 Review (\$400 + \$25/lot) # of Lots _____ x \$25/lot = _____ <input type="checkbox"/> Other _____ <input checked="" type="checkbox"/> Change of Use <input type="checkbox"/> Flood Plain <input type="checkbox"/> Shoreland <input type="checkbox"/> Design Review <input type="checkbox"/> Housing Replacement <input type="checkbox"/> Historic Preservation	Fees Paid (office use) _____ _____
The City invoices separately for the following: <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) Third party review is assessed separately.			
Plan Amendments (check applicable reviews) <input type="checkbox"/> Planning Staff Review (\$250) <input type="checkbox"/> Planning Board Review (\$500)	Fees Paid (office use) _____ _____		

APPLICATION SUBMISSION

All site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Application submissions shall include one (1) paper packet with folded plans containing the following materials:

Numbers below changed- can't seem to highlight

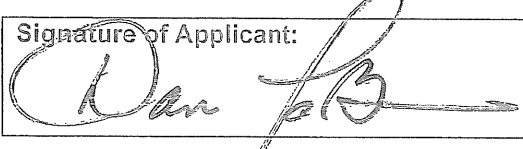
1. One (1) full size site plans that must be folded.
 One (1) set of all written materials or as follows, unless otherwise noted:
 - a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
2. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet.
3. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
4. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
5. One (1) set of plans reduced to 11 x 17.

Refer to the application checklist for a detailed list of submittal requirements.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level II Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: 	Date: 6/5/12
--	-----------------

PROJECT DATA

The following information is required where applicable, in order complete the application

Total Site Area	9715 sq. ft.
Proposed Total Disturbed Area of the Site	N/A sq. ft.
(If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)	
IMPERVIOUS SURFACE AREA	
• Proposed Total Paved Area	N/A sq. ft.
• Existing Total Impervious Area	9715 sq. ft.
• Proposed Total Impervious Area	N/A sq. ft.
• Proposed Impervious Net Change	0 sq. ft.
BUILDING AREA	
• Proposed Building Footprint	-NA- sq. ft.
• Proposed Building Footprint Net change	0 sq. ft.
• Existing Total Building Floor Area	sq. ft.
• Proposed Total Building Floor Area	0 sq. ft.
• Proposed Building Floor Area Net Change	0 sq. ft.
• New Building	(yes or <input checked="" type="radio"/> no)
ZONING	
• Existing	B5b
• Proposed, if applicable	SAME
LAND USE	
• Existing	MIXED USE -
• Proposed	OFFICES · RETAIL · LABS · ZUMBA CHARTER SCHOOL
RESIDENTIAL, IF APPLICABLE	
• Proposed Number of Affordable Housing Units	N/A
• Proposed Number of Residential Units to be Demolished	↓
• Existing Number of Residential Units	↓
• Proposed Number of Residential Units	↓
• Subdivision, Proposed Number of Lots	↓
PARKING SPACES	
• Existing Number of Parking Spaces	12
• Proposed Number of Parking Spaces	10 (LEASED)
• Number of Handicapped Parking Spaces	2 (YORK ST.)
• Proposed Total Parking Spaces	1
BICYCLE PARKING SPACES	
• Existing Number of Bicycle Parking Spaces	0
• Existing Number of Bicycle Parking Spaces	0
• Proposed Number of Bicycle Parking Spaces	5
• Total Bicycle Parking Spaces	5
ESTIMATED COST OF PROJECT	\$185,000.00

**General Submittal Requirements – Preliminary Plan (Optional)
Level II Site Plan
Preliminary Plan Phase Check list (if elected by applicant)**

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Completed application form
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Application fees
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written description of project
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Evidence of right, title and interest.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Copies of required State and/or Federal permits.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of proposed project's compliance with applicable zoning requirements.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written description of existing and proposed easements or other burdens.
<input checked="" type="checkbox"/> NA	<input type="checkbox"/>	1	Written requests for waivers from individual site plan and/or technical standards, where applicable.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Traffic analysis (may be preliminary, in nature, during the preliminary plan phase).
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written summary of significant natural features located on the site.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written summary of project's consistency with related city master plans.
Applicant Checklist	Planner Checklist	Number of Copies	Site Plan Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>		Boundary Survey meeting the requirements of Section 13 of the City of Portland Technical Manual.
<input type="checkbox"/>	<input type="checkbox"/>	1	Preliminary Site Plan including the following: (*information provided may be preliminary in nature during preliminary plan phase):
<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/> Existing and proposed structures with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/> Location of adjacent streets and intersections and approximate location of structures on abutting properties.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/> Proposed site access and circulation.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/> Proposed grading and contours.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/> Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways.
<input type="checkbox"/> NA	<input type="checkbox"/>		<input type="checkbox"/> Preliminary landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
<input type="checkbox"/> NA	<input type="checkbox"/>		<input type="checkbox"/> Existing and proposed utilities (preliminary layout).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/> Preliminary infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, transit infrastructure, roadway improvements).
<input checked="" type="checkbox"/> NA	<input type="checkbox"/>		<input type="checkbox"/> Preliminary stormwater management and erosion control plan.
<input type="checkbox"/> NA	<input type="checkbox"/>		<input type="checkbox"/> Existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b) 1. of the Land Use Code).
<input type="checkbox"/> NA	<input type="checkbox"/>		<input type="checkbox"/> Proposed alterations to and protection measures for significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important

NA

<i>natural features listed in Section 14-526 (b)1. of the Land Use Code).</i>
▪ <i>Existing and proposed easements or public or private rights of way.</i>

General Submittal Requirements – Final Plan (Required)
Level II Site Plan
Final Plan Phase Check list (including items listed above in General Requirements for Preliminary Plan, if applicant did not elect to submit for a preliminary plan review)

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirement
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of financial and technical capacity.
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of utilities' capacity to serve the development.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of fire safety (referencing NFPA fire code and Section 3 of the City of Portland Technical Manual).
<input type="checkbox"/>	<input type="checkbox"/>	1	Construction management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Traffic Plan (if development will (1) generate 100 or more PCE or (2) generate 25 or more PCE and is located on an arterial, within 1/2 mile of a high crash location, and/or within ¼ mile of an intersection identified in a previous traffic study as a failing intersection).
<input checked="" type="checkbox"/> NA	<input type="checkbox"/>	1	Stormwater management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of solid waste generation and proposed management of solid waste.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of conformity with applicable design standards.
<input type="checkbox"/>	<input type="checkbox"/>	1	Manufacturer's verification that HVAC and manufacturing equipment meets applicable state and federal emissions requirements.

Final Plan Phase			
<input type="checkbox"/>	<input type="checkbox"/>	1	Final Site Plan including the following
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Existing and proposed structures on the site with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Location of adjacent streets and intersections and approximate location of structures on abutting properties.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed site access and circulation.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed grading and contours.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways. Proposed curb lines must be shown.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed loading and servicing areas, including applicable turning templates for delivery vehicles</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed snow storage areas or snow removal plan.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed trash and recycling facilities.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Existing and proposed utilities.</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Location and details of proposed infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, public transit infrastructure, roadway improvements).</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed septic system, if not connecting to municipal sewer. (Portland Waste Water Application included in this application)</i>
<input type="checkbox"/>	<input type="checkbox"/>		▪ <i>Proposed finish floor elevation (FFE).</i>

-
-
-
-
-

-

-
-
-
-

-
-
-
-
-

-

-
-
-
-

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Exterior building elevation(s) (showing all 4 sides).
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Proposed stormwater management and erosion controls.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Exterior lighting plan, including street lighting improvements..
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Proposed signage.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Identification of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code). Wetlands must be delineated.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Proposed alterations to and protection measures for of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code).
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Total area and limits of proposed land disturbance.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Soil type and location of test pits and borings.
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Details of proposed pier rehabilitation (Shoreland areas only).
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/> Existing and proposed easements or public or private rights of way.

Review Staff
City of Portland
Portland, Me 04101

Dear Staff Members:

Baxter Academy for Technology and Science
54 York Street

HISTORY

The building is located on the corner of York and Maple (see attached map) in Portland. Fifty-four York Street has had numerous uses over the years. Most recently approximately 20,000 sq. ft. was leased to Media Power. In April of 2010 Media Power needed more space and moved to a building on Foden Road in South Portland.

In mid-2011 John Jaques, future Executive Director of Baxter Academy, and Daniel LaBrie, Senior Vice President of Rufus Deering Lumber Company, began discussing a possible partnership by leasing 54 York Street to them for a charter school.

OBJECTIVE

The school aims to link students with local businesses, scientists, researchers and other professionals working in fields of science, technology, engineering and mathematics. The Portland location offers easy access to the Gulf of Maine Research Institute, Maine Medical Center, the University of Southern Maine and Southern Maine Community College.

The school hopes to get a charter from the state by July 1st and open in September. It hopes to be the first of its kind in Portland.

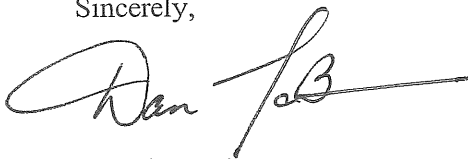
PROPOSAL

The fit up to prepare the building for a school consists of upgrading HVAC, Security system, Handicap Accessibility, new carpets and paint. The building as it stands today does not have an elevator. An exterior elevator will be installed on the southwest side of the building on a 15' easement.

CLOSING

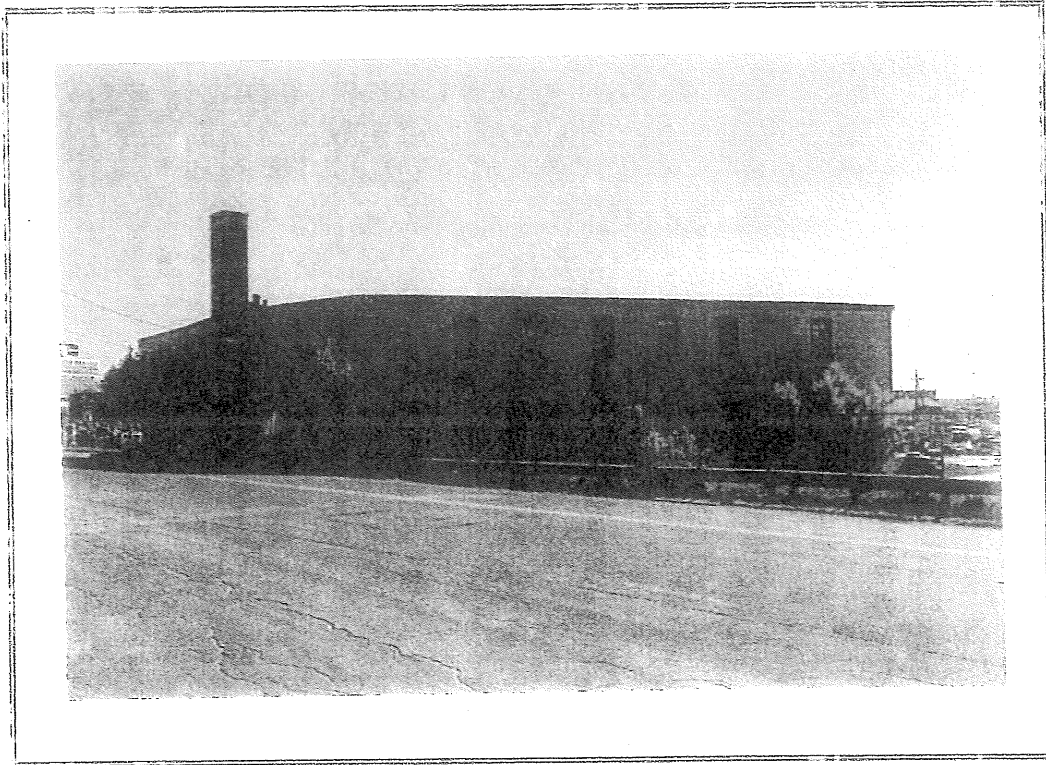
I believe this is a perfect location for a charter school. It is wonderful to be able to offer our youth an alternative education to the usual public school system. These charter schools have been very successful all over the country. Time is of the essence, I would ask that you move this process along and grant the permit in a timely manner.

Sincerely,

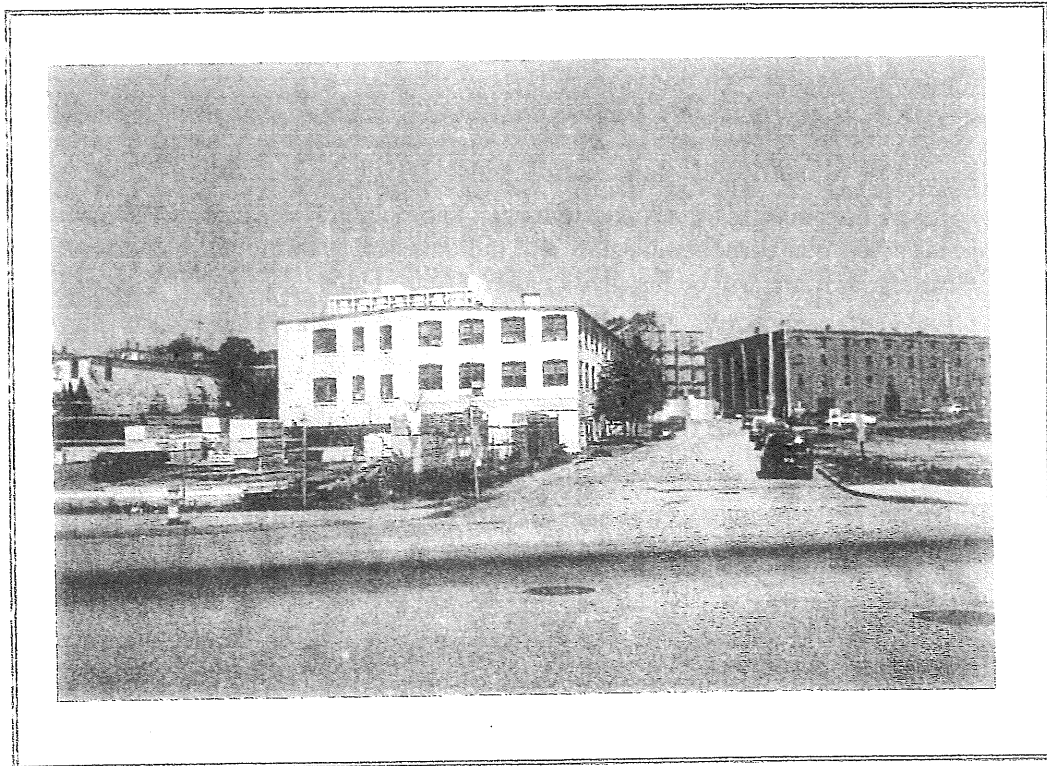


Daniel LaBrie
Senior Vice President

SUBJECT PHOTOGRAPHS



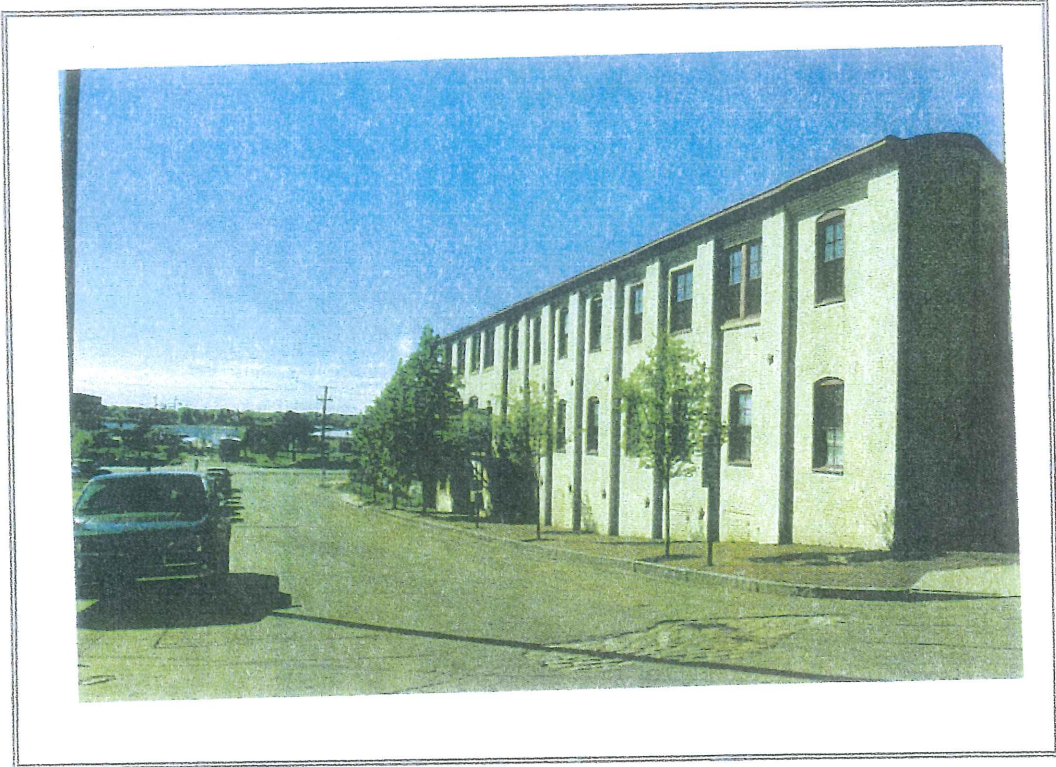
1) Easterly view of subject from York Street



2) Westerly view of subject from Commercial Street, looking up Maple Street

D.1

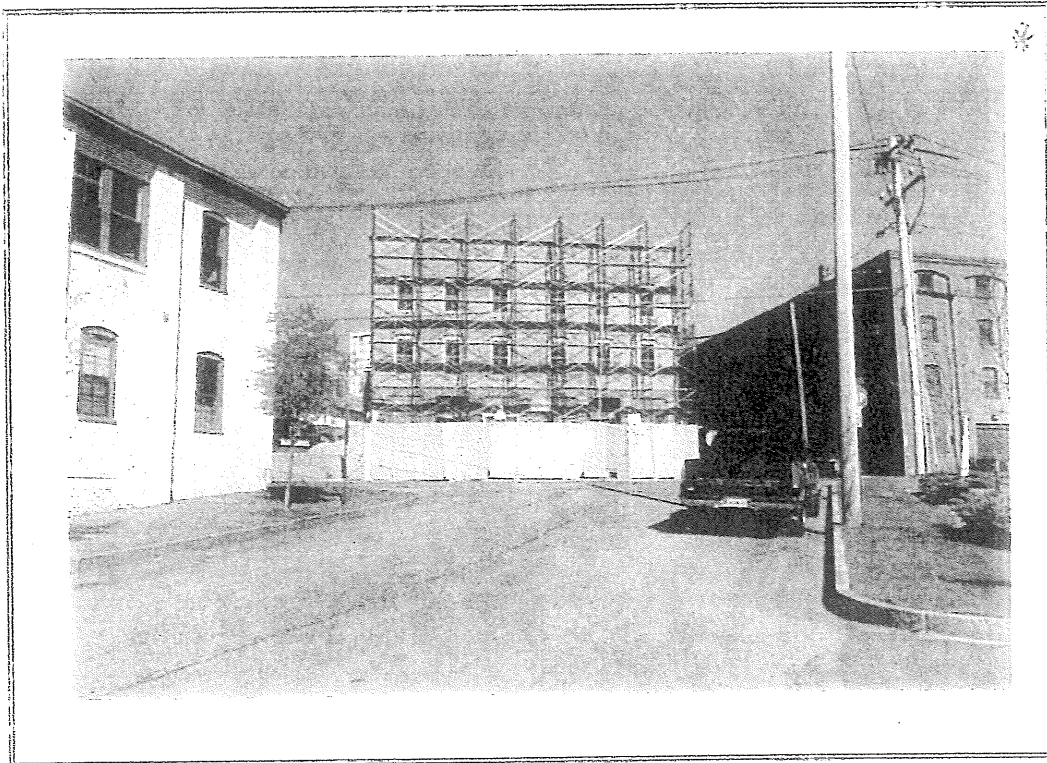
SUBJECT PHOTOGRAPHS



3) Southeasterly view down Maple St. toward Commercial St, subject at right



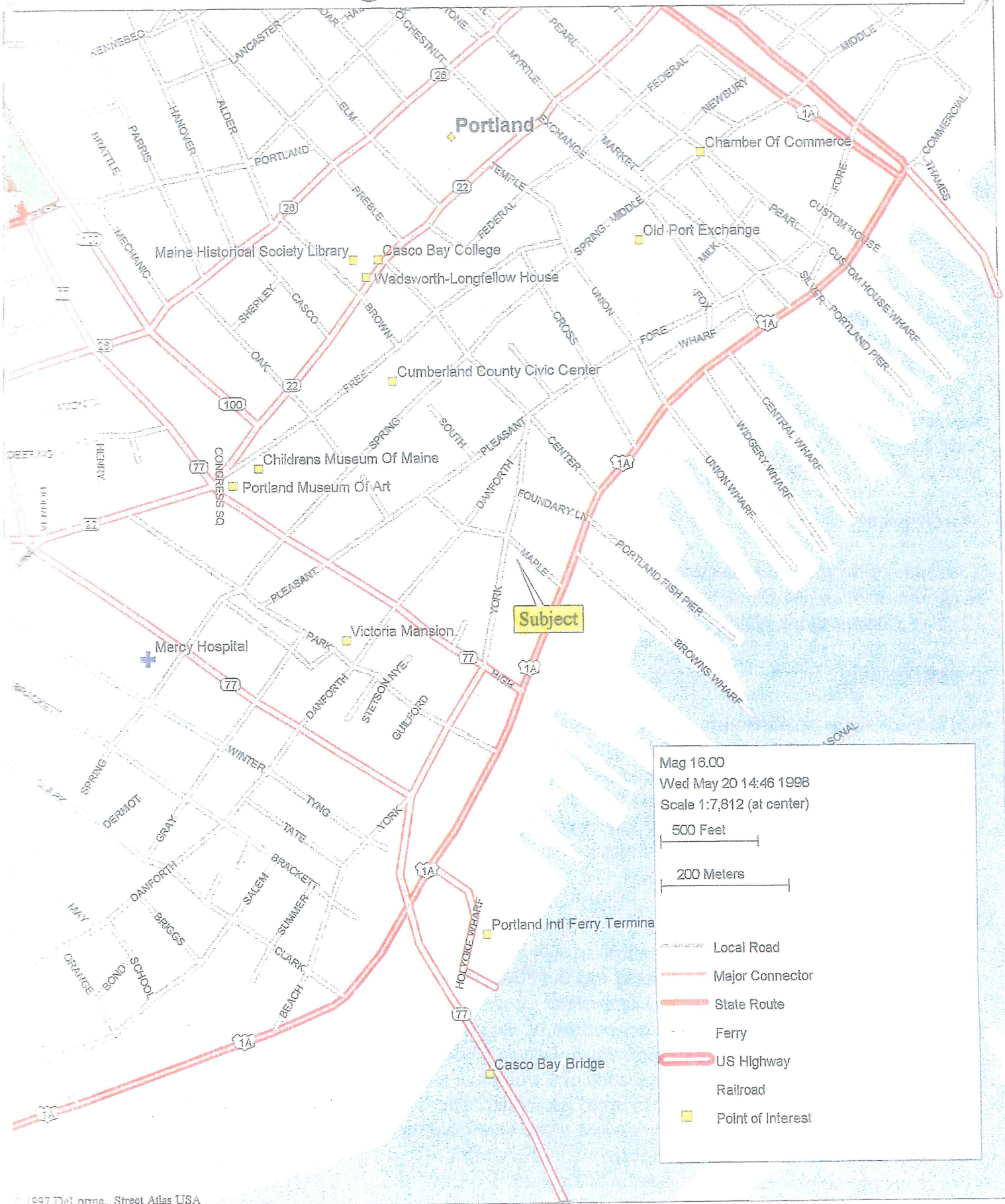
4) Westerly view of south elevation of subject (driveway behind bldg)



N2) Ongoing rehabilitation/revitalization of the subject neighborhood (subject at left)

Neighborhood Map

D.3



SUBJECT SITE DESCRIPTION

Location

The subject site enjoys a corner lot location at the apex of York and Maple Streets in Portland Maine. This location receives heavy traffic flow along York Street as this is the major connector route from the Portland CBD to the Casco Bay Bridge providing access to the City of South Portland. The site is also within close proximity to the Portland Waterfront District with marine, transit, and tourist related land uses prevalent.

Land Area

The subject site, according to the legal description provided, contains an estimated land area of 3,712±sf in fee simple, plus an additional passageway area measuring 15' x 85' (1,275±sf) for a total of 9,987± sf. The survey plan also provided to the appraiser, showed an area of 3,447±sf, although it is not clear that this is not the footprint of the building rather than the site. For purposes of this appraisal, this is not a large issue as the site is considered fully built out and the leased fee interest has been appraised. The client is urged to consult with a surveyor and/or title attorney to clarify such matters further prior to making decisions which may be based upon land area estimates.

Shape/Frontage

The shape of the site is characterized as irregular. The parcel is a corner lot with an almost trapezoidal shape due to its frontage on two streets. According to the legal description provided, the site has 125'± of frontage along Maple Street plus another 100'± of frontage along York Street.

Access/Visibility

The subject site is considered to have good access and visibility characteristics. The site enjoys high traffic counts approximating 12,000 cars per day according to MDOT traffic volumes 1996. York Street feeds directly into/from the new Casco Bay Bridge (replaced the Million Dollar Bridge last year) and is a direct link to the Portland CBD and Old Port Districts. Commercial Street to the south is also a major commuter travel artery and serves the working waterfront section of Portland. Maple Street is a small connector road which traverses the two.

Topography/Soils

The topography of the site is characterized as sloping in an easterly direction down from its York Street frontage towards Commercial Street and the Portland Waterfront. No on site soils investigation was performed by the appraiser. The Cumberland County Soils Survey classifies this area as Urban, Cut and Fill land. It was reported that a portion of the subject (original construction) dates back to 1820 and survived the Great Fire of 1936 which leveled much of downtown Portland and gave way to the filling in along the waterfront and creation of Commercial Street in 1950. Given the age and varied uses of the site over time, the client is urged to conduct due diligence in regards to environmental issues to their own satisfaction. The appraised value herein assumes no adverse conditions related to environmental issues which may or may not affect the subject site. Reference is made to the Statement of Assumptions and Limiting Conditions in this regard.

Utilities

The subject site is serviced with all public utilities including water, sewer, natural gas, electricity, telephone and cable TV. Municipal services such as fire and police protection and sanitation are deemed to be adequate.

Easements

Reference is made to the attached legal description and title report contained in the Addenda section of this report for an overview of the various limitations of title to the subject property. As discussed, part of the site is encumbered as a passageway in common with others. There were no other adverse easements or encroachments noted on the date of inspection/value herein which would limit the use and/or value of the subject property under its current land use program.

Functional Utility

The subject site is considered to offer average functional utility for development purposes. If vacant and available, it would likely be assembled with abutting lands for a larger scale development. The site does offer good exposure for commercial purposes and adequate frontage for access. It is zoned for business use and is located in an area of revitalization. An interim use for surface parking would be an immediate use of the site.

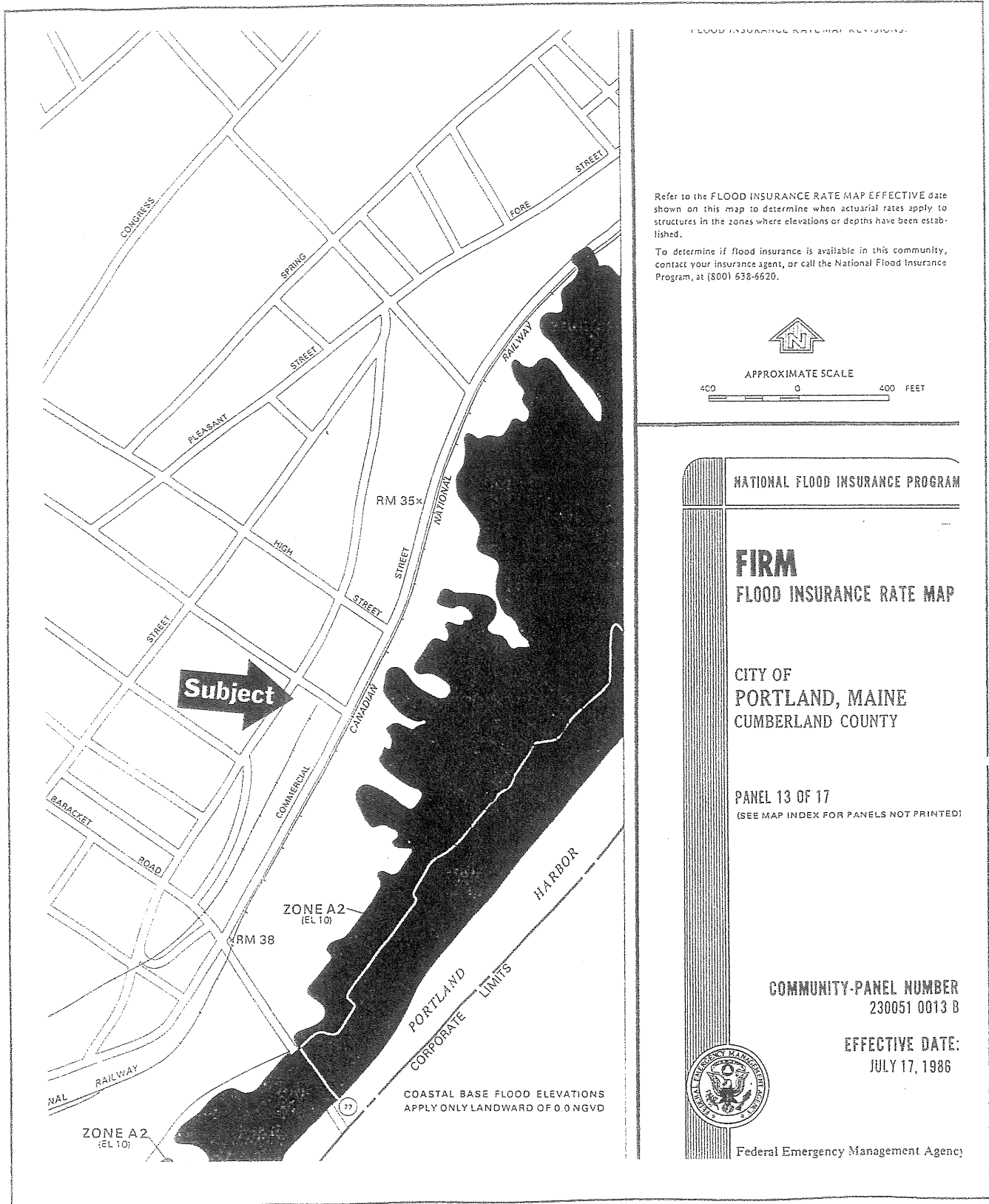
Marketability/Appeal

For the reasons cited above, the subject land parcel is considered very marketable and would be anticipated to sell quickly if vacant and available.

E.3

FEDERAL EMERGENCY MANAGEMENT FLOOD HAZARD ZONE

The subject property is not located in a federally designated 100-year flood hazard zone, according to the FEMA Map presented below:



FLOOD INSURANCE RATE MAP REVISIONS:

Refer to the FLOOD INSURANCE RATE MAP EFFECTIVE date shown on this map to determine when actuarial rates apply to structures in the zones where elevations or depths have been established.

To determine if flood insurance is available in this community, contact your insurance agent, or call the National Flood Insurance Program, at (800) 638-6620.

APPROXIMATE SCALE
400 0 400 FEET

NATIONAL FLOOD INSURANCE PROGRAM


FIRM
FLOOD INSURANCE RATE MAP

CITY OF
PORTLAND, MAINE
CUMBERLAND COUNTY

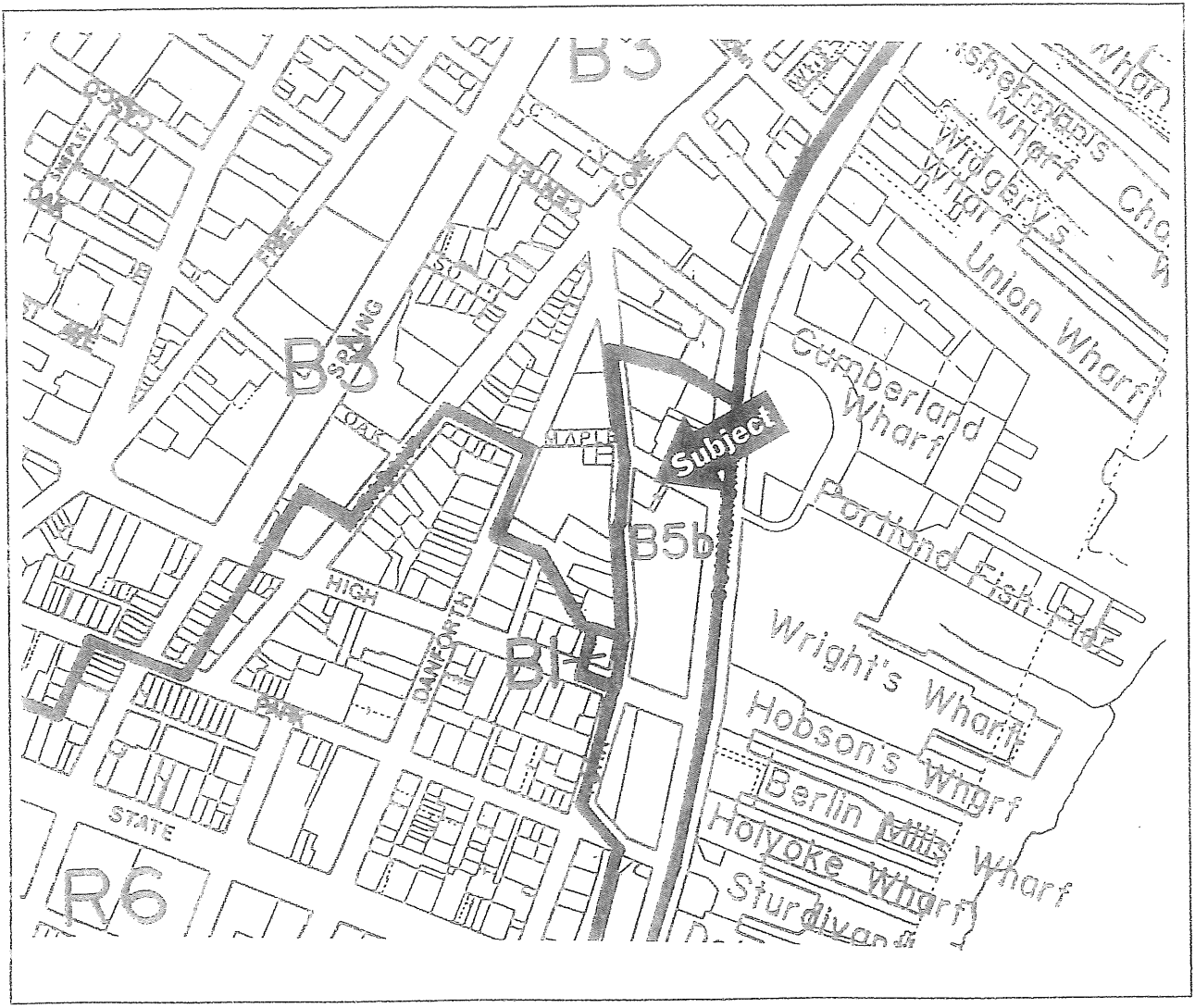
PANEL 13 OF 17
(SEE MAP INDEX FOR PANELS NOT PRINTED)

COMMUNITY-PANEL NUMBER
230051 0013 B

EFFECTIVE DATE:
JULY 17, 1986



Federal Emergency Management Agency



Picture

SUBJECT IMPROVEMENTS DESCRIPTION

Year Built

The subject building improvements were built in the late 1800's, and subsequent to a fire, was rebuilt and expanded around the 1920's.

Gross Building Area

The building is estimated to contain approximately 21,297±sf of gross building area, all of which has been considered net rentable area as per existing leases in place (common areas prorated).

Foundation

The foundation walls are of mortared brick and appear to be in sound condition.

Frame

The frame of the building is heavy timber columns and wooden floors, some supported with brick columns and steel girders.

Exterior Walls

The exterior walls are of painted brick.

Floors

The top two floors in the building were generally of wood underlayment with a concrete slab floor at the daylight basement level.

Floor Covering

Floor coverings ranged from low cost to medium grade commercial carpeting in the office areas, some composition tile and/or vinyl floor finishes in the bathroom areas. The lower level was mostly painted concrete with some finished areas carpeted.

Partitions

Interior partitions were mostly of wood stud walls and sheetrocked panels. Some paneling and matchboard wainscoting were also noted.

Layout

The layout is fairly heavily partitioned into numerous offices and suites for multi-tenant occupancy.

Ceilings

The ceilings were primarily suspended acoustic panel tiles and in good condition, with recessed fluorescent lighting throughout.

Roof Structure & Cover

The roof structure is of a flat, shed style with a covering of rubber membrane dating to 1989 according to the present owner.

Electrical

The electrical entrance to the building consists of two primary services of four-wire, three phase, 240 volt capacity with a 400-amp rating. Distribution is via six 100 amp and 1 200-amp panels with circuit breaker protection. Electricity is separately metered to each tenant excepting unit 102 which is on the house meter.

Plumbing

The building is serviced with municipal water and sewer. The main water supply enters in the basement sprinkler room and has a backflow preventer. Where visible, plumbing distribution piping is primarily copper with some PVC and cast iron waste lines noted. There is also a recent, gas fired domestic hot water heater in the basement.

H.V.A.C.

The building is serviced with a natural gas heater with individual tenant meters, excepting unit 102 which is on the house. Heat is provided through the building with a combination of ceiling and wall hung gas heaters. There is a central, gas-fired boiler with five zones noted on the first floor as well as some electric baseboard heat interspersed.

Elevators

There are no elevators currently in the building.

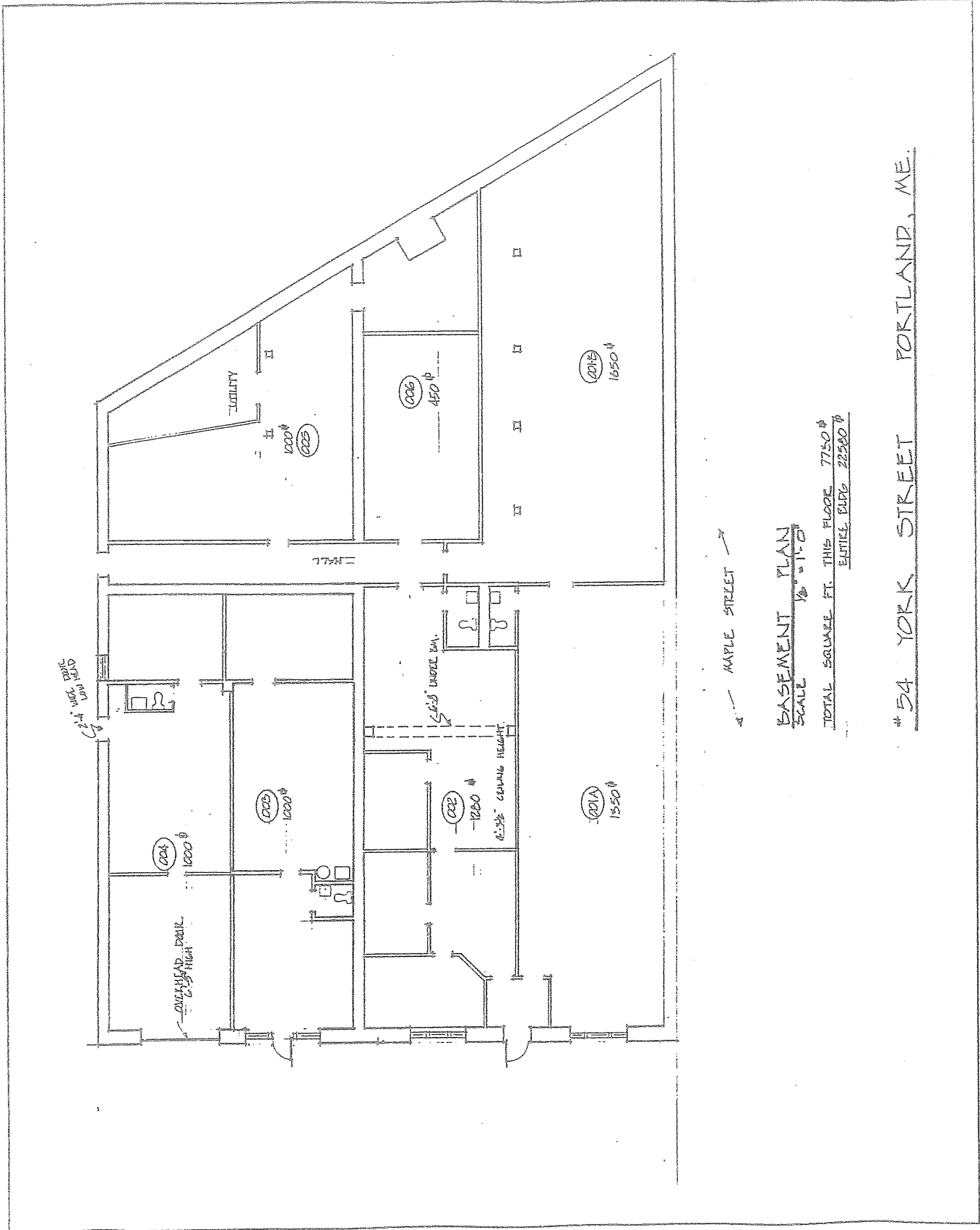
Sprinkler System

The building is provided with a dry type sprinkler system.

Overall Condition

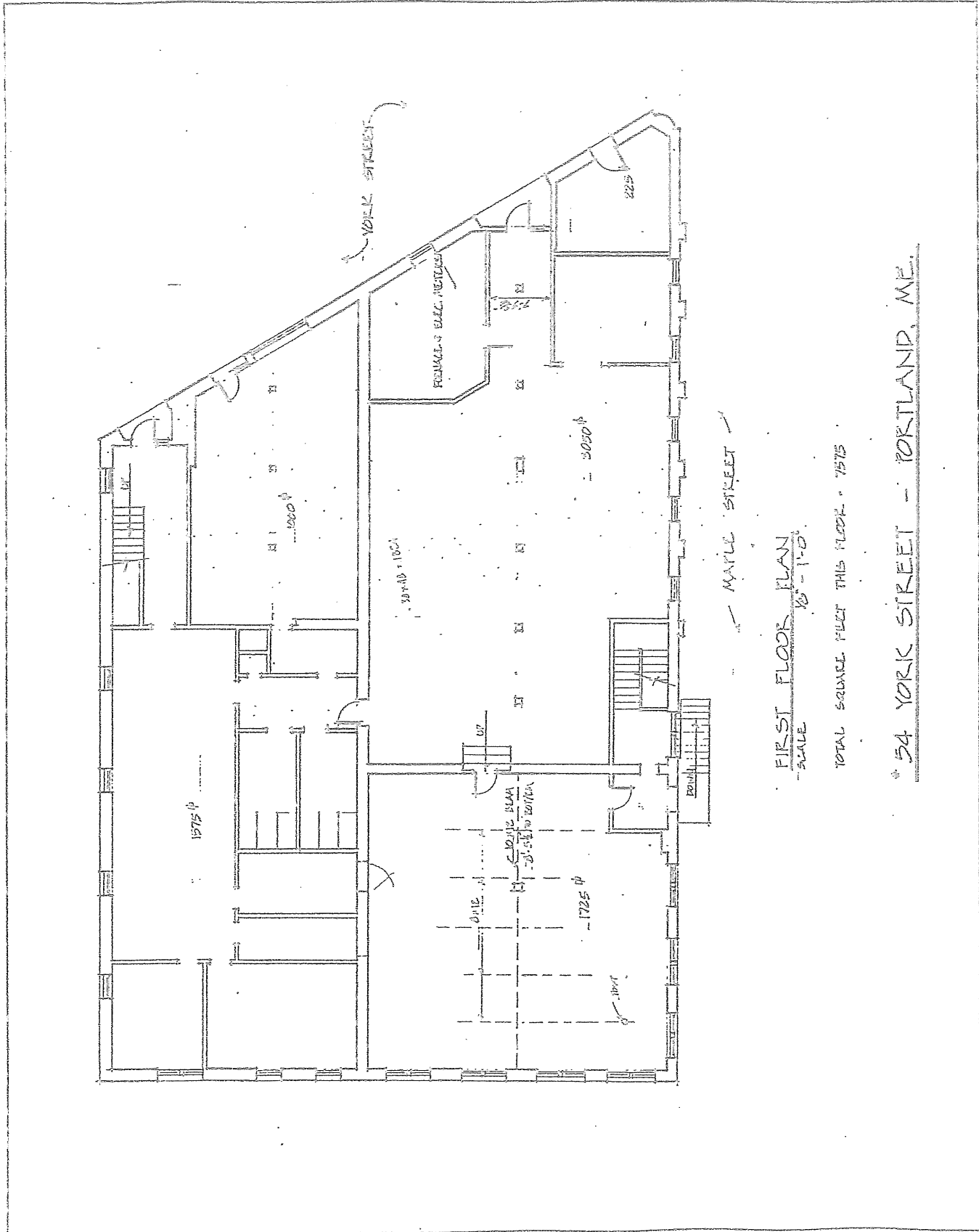
The overall condition of the improvements is rated as average to good.

SUBJECT IMPROVEMENTS SKETCH - Basement Level
(not to scale)

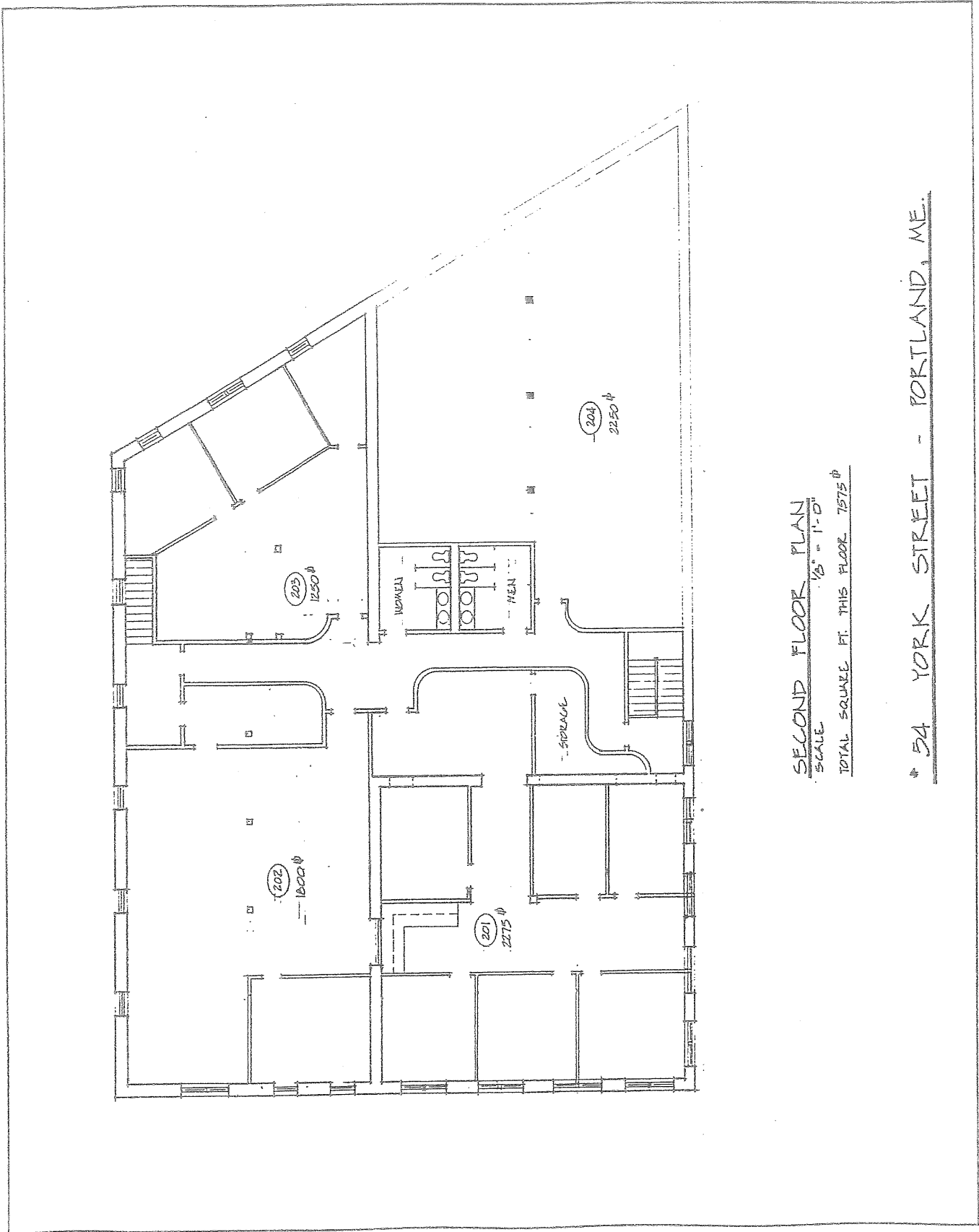


SUBJECT IMPROVEMENTS SKETCH - First Floor Level
(not to scale)

3.8



SUBJECT IMPROVEMENTS SKETCH - Top Floor Level
(not to scale)



SECOND FLOOR PLAN
SCALE 1/8" = 1'-0"
TOTAL SQUARE FT. THIS FLOOR 7575 sq ft

54 YORK STREET - PORTLAND, ME.

SURVEYOR'S REPORT

Fill out and sign with all Surveys in accordance with the Instructions on the reverse side.

To: LAWYERS TITLE INSURANCE CORPORATION

Richmond, Virginia.

THIS IS TO CERTIFY that on December 3, 19 87, I made an accurate survey of the premises standing in the name of William J. Dowd situated at Portland Cumberland Maine City County State

briefly described as: Commercial Structure and shown on the accompanying survey entitled: Standard Boundary Survey for William J. Dowd

I made a careful inspection of said premises and of the buildings located thereon at the time of making such survey, and again on December 17, 19 87 and at the time of such latter inspection I found William J. Dowd to be in possession of said premises as owner (tenant) or (owner)

I further certify as to the existence or non-existence of the following at the time of my last inspection:

1. Rights of way, old highways, or abandoned roads, lanes or driveways, drains, sewer, water, gas or oil pipe lines across said premises (Include any such matters shown on the recorded plat of subdivision): 15 foot Right of Way to land now or formerly of Rufus Deering Location of sewer connection uncertain

2. Springs, streams, rivers, ponds, or lakes located, bordering on or running through said premises: None observed

3. Cemeteries or family burying grounds located on said premises. (Show location on plat): None observed

4. Telephone, telegraph or electric power poles, wires or lines located on, under, overhanging or crossing said premises and serving other property or properties: None observed above ground below grade lines uncertain

5. Joint driveways or walkways; party walls or rights of support; porches, steps or roofs used in common or joint garages: 15 foot strip above mentioned apparently owned by Dowd subject to use by abutter.

6. Disputed boundaries, encroachments or overhanging projections. (If the buildings, projections or cornices thereof, or signs affixed thereto, fences or other indications of occupancy encroach upon or overhang adjoining properties or easement areas, or the like encroach upon or overhang surveyed premises, specify all such): Encroachment of building onto 15 foot Right of Way, Sign overhangs York Street, Fire Escapes overhang Maple Street, Fence of Abutter encroaches on said property.

7. Physical evidence of boundary lines on all sides. (Be specific): Iron pins found at two corners (see plan)

8. Is the property improved? Yes (a) Building is: Brick (X); Clapboard (); other () Specify (b) Building is: One story (); Two story (X); split-level (); other () Specify

9. Indications of building construction, alterations or repairs within recent months: Exterior brick cleaned, repainted, new doors (a) If new improvements under construction, how far have they progressed? 50 per cent completed more or less

10. Changes in street lines either completed or officially proposed: ing. York Streetline City in progress of determining (a) Are there indications of recent street or sidewalk construction or repairs? None

11. Are all abutting streets or roads maintained by public authorities? Yes (a) Is access to such streets or roads limited? NO

12. If the surveyed premises are subject to restrictive covenants, do the improvements, use and occupancy comply with such? (If the premises are subject to restrictive covenants, have the examining attorney furnish you verbatim copy of them) Note: Origination Deed 1555/24 requires Grantee to build and maintain fence, Existing fence appears to have been constructed by abutter.

William A. Kempfield, Jr. Civil Engineer & Surveyor

(Be sure to answer each item. If the property is not subject to any such of items 1 to 6 inclusive, insert the word "none" following the item.)

NOTE: In all cases where there are encroachments, easements, party walls, etc., they should also be denoted upon diagram of your survey, and be made more complete with instructions on reverse side. Particular attention is directed to Paragraph 5 of these instructions.

BEAGLE, PEARCE & RIDGE, LLC
ATTORNEYS AT LAW

C. ALAN BEAGLE
MICHAEL J. PEARCE
MARTIN J. RIDGE
JACQUELYN M. KURZ
PAULA M. CRAIGHEAD

24 CITY CENTER
P.O. BOX 7044
PORTLAND, MAINE 04112
(207) 773-1751
FAX (207) 775-3382

November 30, 1998

Sally Dwyer
302

Mr. William M. Moody, Jr., President
Rufus Deering Company
383 Commercial Street
P.O. Box 880
Portland, ME 04112-0880

Re: Rufus Deering Company/Peoples Heritage Bank/York
Street, Inc.

Dear Bill:

Enclosed please find the following original documents:

1. Warranty Deed from York Street, Inc. to Rufus Deering Company. This Warranty Deed has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 13.
2. Assignment of Leases and Security Deposits from York Street, Inc. to Rufus Deering Company. This Assignment has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 36.

If you have any questions, please feel free to contact me.

Very truly yours,

Robyn A. Pearce
Robyn A. Pearce
Assistant to Michael J. Pearce

enclosure

G.1

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.
By: Werner Schnetzer
Werner Schnetzer
Its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,
James S. Bunker
Notary Public/Attorney-at-Law
Print Name: James S. Bunker

122895/PSB/SKLF
91401.4926

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4: 48

CUMBERLAND COUNTY

John B. O'Brien

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

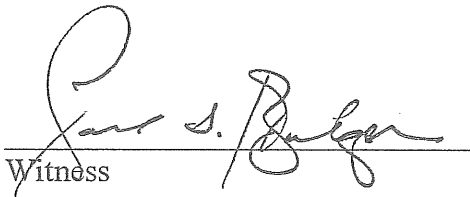
There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

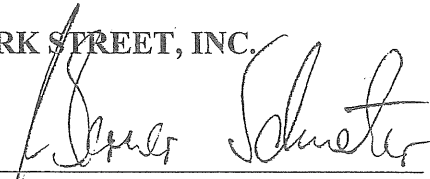
RECEIVED
RECORDED REGISTRY OF DEEDS
1998 SEP -1 PM 4:52
CUMBERLAND COUNTY
John B O'Brien

ASSIGNMENT OF LEASES AND SECURITY DEPOSITS

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04106, for consideration, does hereby **ASSIGN** to **RUFUS DEERING COMPANY**, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, its successors and assigns, all its right, title and interest in and to any and all leases, subleases and/or tenancies of the premises located on or about 54-62 York Street in the City of **PORTLAND**, County of **CUMBERLAND** and State of **MAINE**, and more particularly described on **EXHIBIT A** attached hereto, including, without limitation, any and all tenant security deposits relating thereto.

IN WITNESS WHEREOF, the said **YORK STREET, INC.** has caused this instrument to be signed on its behalf by Werner Schnetzer, its duly authorized President, on this 1st day of September, 1998.



Witness

YORK STREET, INC.
By: 
Werner Schnetzer
Its: President

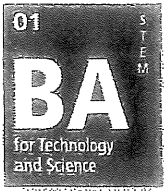
STATE OF MAINE
CUMBERLAND, SS.

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of **YORK STREET, INC.** and acknowledged the foregoing instrument to be his free act and deed in said capacity and the free act and deed of said **YORK STREET, INC.**

Before me,

Notary Public/Attorney-at-Law
Print Name: Paul S. Bulger

122937/PSB/SKLF
91401.4926



Baxter Academy
for Technology and Science

January 9, 2013

John Peverada
Parking Manager
City of Portland
Portland, ME 04101

LETTER OF INTENT TO LEASE SPRING STREET GARAGE

Dear Mr. Peverada,

On behalf of Baxter Academies of Maine, I would like inform you of our intent to lease 15 parking spaces in the Spring Street Garage beginning August 1, 2013.

This letter is to acknowledge that we understand that these spaces are available now and you will inform us and give us the chance to rent earlier if the number of available spaces falls below 15. I understand that these spots will not be held if we fail to act upon the notification with in (7) business day.

Best regards,

John Jaques


Executive Director

John Peverada

Parking Manager City of Portland



Memorandum

Project: 07383
To: Shukria Wiar
From: Steve Sawyer, P.E., Vice President, Transportation Services 
Date: January 9, 2013
Subject: Baxter Academy for Technology and Science – Traffic Signage Plan
cc: Dan LaBrie, John Jaques


Per your email request of January 4, we are hereby submitting our proposed Traffic Signage Plan. I have attached Figure 1 that shows existing signage around the perimeter of our site. In our opinion, the only change to what exists today would be the addition of two “School Bus Drop Off Zone” signs in addition to the existing “No Parking” signs in front of the school on the south side of York Street.

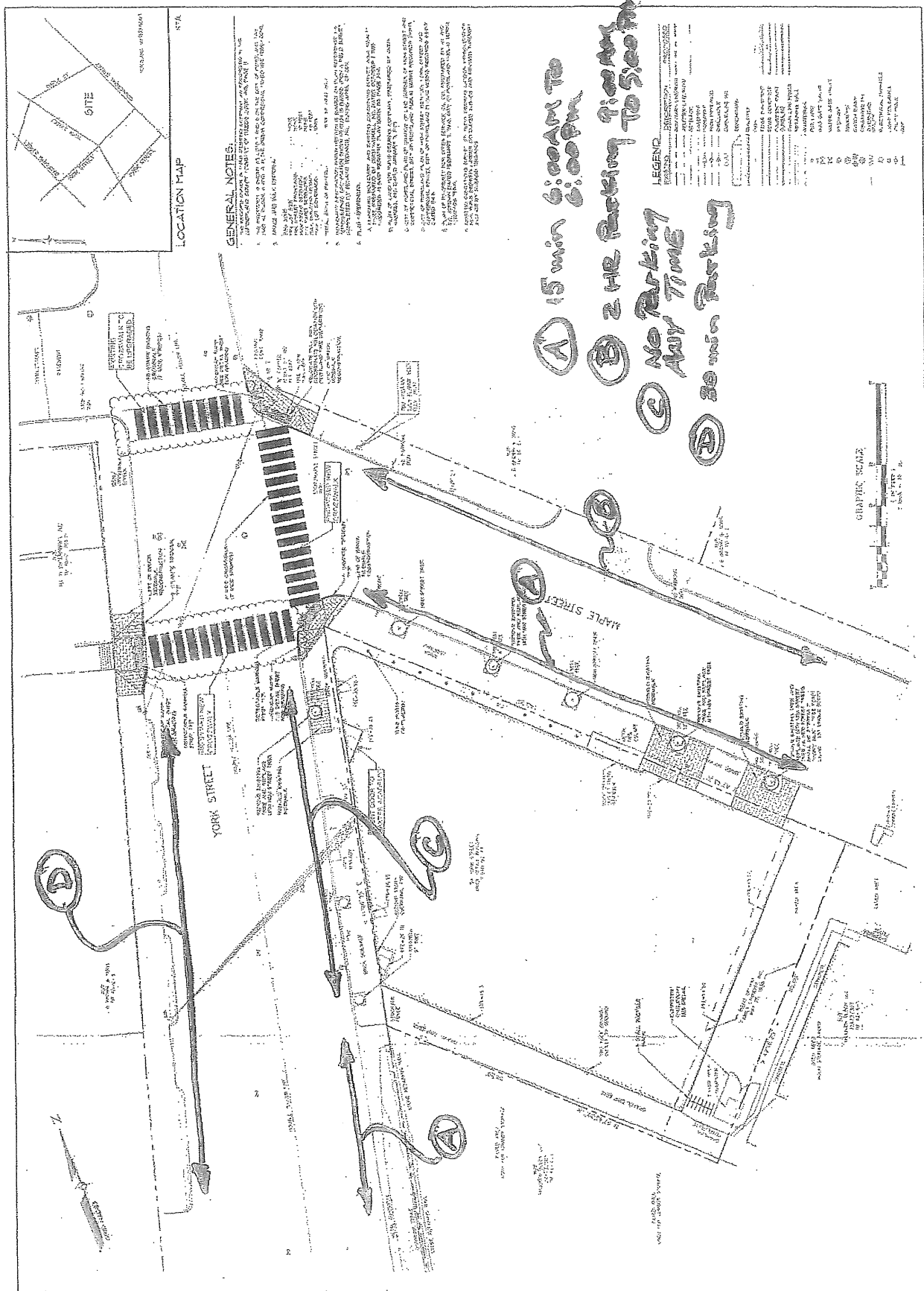
The 15 minute parking on the west side of Maple Street is appropriate for parent drop off and pick up as planned, and the two 15 minute parking spaces to the west of the school on the south side of York Street are appropriate for parent drop off or pick up, as well. The existing 30 minute spaces on the north side of York Street are generally unoccupied during the times that they may be needed by parents for picking up and dropping off, so we see no need to change these restrictions.

Let us know if you require any further information from us in this regard.

SSS:sss/kn
Att.

FIGURE 1

	SHEET'S OF 4 NO. 1 NO. 2 NO. 3 NO. 4
	SITE PLAN RUPAS ENGINEERING LUMBER CO. RUPAS ENGINEERING LUMBER CO. RUPAS ENGINEERING LUMBER CO.





Memorandum

Project: 07383
To: Shukria Wiar
From: Steve Sawyer
Date: January 3, 2013
Subject: Baxter Academy for Technology and Science
- Sidewalk Waiver Request
CC: Dan LaBrie, John Jaques

Steve

This memorandum is a follow up to our meeting with you and the staff of Public Services on December 11, 2012. At this meeting it was explained that if the applicant did not agree with staff's recommendation to construct a new sidewalk on the south side of York Street between High Street and Maple Street that we should apply for a "sidewalk waiver" under the provisions Section 14-506 (b). Under this provision in the City's Ordinances, the Portland Planning Board may grant a waiver from the requirement for a sidewalk providing two or more conditions listed in the Ordinance are met. We have prepared the following to satisfy these guidelines.

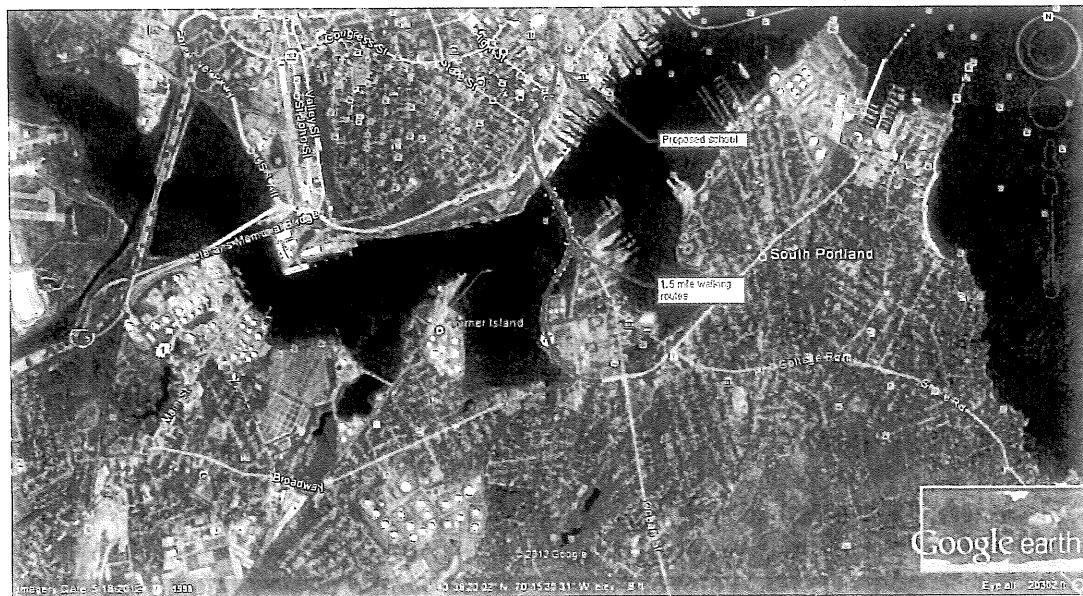
Sidewalks Condition 1 - There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site.

We provided you as part of a December 3, 2012 memo our viewpoint on the usage of a sidewalk on the south side of York Street for the proposed school. This explanation is repeated here for the Planning Board's reference.

Baxter Academy has presented an enrollment profile as part of its application materials. In Year 3 (full build-out) there is an expectation because of its urban setting that as many as 16 students and staff will walk to school. If we look at a map, see Google image below, we see that Portland's closest residential neighborhoods (within 0.75 miles) are all located on the north side of York Street in the general direction of the West End. There is an existing sidewalk on the north side of York Street that is available to walkers originating from these neighborhoods, and the school is proposing to upgrade the crosswalks in front of the school to permit safe crossings of York Street at the school's main entrance.



Who then would use a sidewalk if it were to be constructed on the south side of York Street from High Street to Maple Street? We have heard that it would be students and staff crossing the Casco Bay Bridge. If this is the case let's look at the route this would involve. From the Google image below you can see that to access the nearest residential neighborhoods in South Portland involves a walking route that would exceed 1.5 miles. In our view, it is unlikely that students would walk in excess of 1.5 miles across the bridge to get to and from school on a regular basis. Therefore, we do not believe that the Baxter Academy will be attracting walkers from South Portland given the distance involved. As such, we do not see the nexus between the school and the continuation of the sidewalk on the south side of York Street to High Street. Without one, we believe it is unreasonable to burden our project with this expense.



J.2

Shukria Wiar Memo

January 3, 2013

Condition 3 - A safe alternative-walking route is reasonably and safely available, for example, by way of a sidewalk on the other side of the street that is lightly traveled.

See the response to Condition 1 above - there is an existing sidewalk on the north side of York Street in this location.

In addition to a "sidewalk waiver", the applicant has also reviewed the conditions for satisfying a "curbing waiver" in the Ordinance. Here we find that Condition 1 refers to the cost to construct the curbing as being less than 5% of the overall project cost. The City has filed a MaineDOT grant application for the sidewalk on York Street in the amount of \$110,000. At our meeting on December 12, we were presented an estimate of \$85,000 for this construction, not including survey and engineering. Furthermore, staff requested that the applicant be required to construct a new brick sidewalk on Maple Street from its project to Commercial Street at an estimated cost of \$13,000. Combined, the staff is asking for sidewalk construction that is estimated by their figures to cost \$98,000 to build, not including survey and engineering. In our view, the total cost is more like \$120-125,000. The cost of the proposed project is expected to run in the range of \$185,000, which would put the requested sidewalk costs at 65-68% of the total project cost, which is far greater than the Ordinance figure of 5%. This expense seems considerably out of proportion and supports our position for a waiver of these requirements.



CIVIL ENGINEERING · SURVEYING · LANDSCAPE ARCHITECTURE

Memorandum

Project: 07383

To: Shukria Wiar

From: Steve Sawyer

Date: January 10, 2013

Subject: Baxter Academy for Technology and Science
Summary Package for Traffic Movement Permit (TMP)

CC: Dan LaBrie, John Jaques

In preparation for our upcoming meeting before the Planning Board in connection with the proposed school, we have put together the following chronology of events leading up to this point with regard to the TMP process. Attached to this memo are all related and relevant documents.

October 23, 2012 - Submission of Section 7 of the TMP application. This document presented a revised traffic pattern in front of the school based on feedback received from your traffic engineer. Student pick-ups and drop-offs were buses on the west side of Maple Street and parents on the north side of York Street.

November 27, 2013 - Planning Board (PB) Workshop. The Planning Board offered some suggestions on the location of the student pick-ups and drop-offs, which we thought were improvements to our plan so we modified our proposal to have buses load and unload in front of the school on York Street in front of the school and have parents load and unload on the west side of Maple Street. Furthermore, a suggestion was made to add a crosswalk across York Street in front of the school that would facilitate walkers or students that might be dropped off on York Street to safely cross the street.

December 3, 2012 - Response memo to City following PB Workshop. This document revised our student loading and unloading plan, included at Traffic Operations and Management Plan, included a TDM Plan, and addressed a number of other comments that had been raised at the meeting.

December 11, 2012 - Meeting with City Public Services Staff. This meeting was to discuss our position related to the need for constructing a sidewalk on the south side of York Street up to High Street to serve the school. We disagree on this being the school's responsibility and subsequently submitted a sidewalk waiver request. The other matter discussed at this meeting was the need for us to gain approval from the City's Crosswalk Committee for the new crosswalk across York Street in front of the school.

January 3, 2013 - Meeting with City's Crosswalk Committee. We received approval from this Committee for the proposed crosswalk across York Street in front of the school.

January 3, 2013 - Submission of Sidewalk Waiver Request.

January 9, 2013 - Submitted Traffic Signage Plan.

January 10, 2013 - Submission of this Summary Package, which includes a commitment letter to lease off-site parking spaces for use by school staff.

To our knowledge this package contains all current and relevant information associated with the proposed school project from a traffic and parking perspective. Site plan information is contained in a separate transmittal.

Attachments:

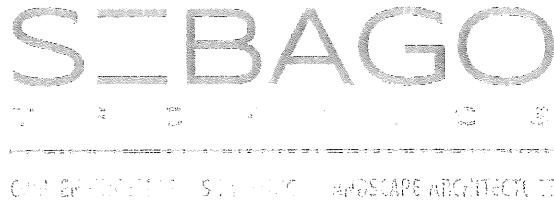
October 23, 2012 - Submission of Section of the TMP Application

December 3, 2012 - Response Memo to City following PB Workshop

January 3, 2013 - Submission of Sidewalk Waiver Request

January 9, 2013 - Submitted Traffic Signage Plan

January 10, 2013 - Commitment letter to lease off-site parking spaces for use by school staff.



Memorandum

Project: 07383
 To: Tom Errico, Shukria Wiar
 From: Steve Sawyer
 Date: December 3, 2012
 Subject: Baxter Academy for Technology and Science TMP
 cc: Dan LaBrie, John Jaques

This memorandum is a follow up to our meeting with the Portland Planning Board in a Workshop Session on November 27, 2012 and in response to a November 20 review memo by Tom Errico on our previous Section 7 submittal dated October 23, 2012.

Item 1 – Student drop-off and pick-up locations. The Planning Board and Mr. Errico have raised concerns over the proposed location for student drop-offs by parents being on the north side of York Street opposite the School's entrance. Under this scenario, students would need to cross traffic to get to the school. Furthermore, during the winter months the sidewalk on the north side of York Street may not always be cleared by the City at the time of school arrivals and snow banks may exist. Since the School would not control this sidewalk area, students would have no safe place to go but into the street once they get out of their parent's cars. For these reasons, it was suggested that we reconsider our Plan to include locating the bus drop-off on York Street in front of the School's entrance and direct parents to use the west side of Maple Street adjacent to the existing sidewalk abutting the School. In this manner no one would need to cross any street to get into and out of the School. The matter of the buses stopping traffic on York Street during loading and unloading was discussed but the Board felt that since there were only three buses this would be less overall delay to traffic on York Street than what was previously proposed and the situation would be much safer. **ACTION: We agree with this rationale and will modify our Traffic Operations Plan accordingly.**

Item – Provide a TDM Plan for the Project. ACTION: A TDM Plan is attached.

Item 3 – Off-Site Parking. The School will enter a lease arrangement with either the City's Spring Street garage or with the Holiday Inn By The Bay for a limited number of spaces for staff. Space is readily available in both of these locations presently. It is premature, though, for the School to consummate these arrangements until they hire the required staff and know where they all live and what their need for parking may be. **ACTION: Nothing further until more is known about the staffing.**

L-1

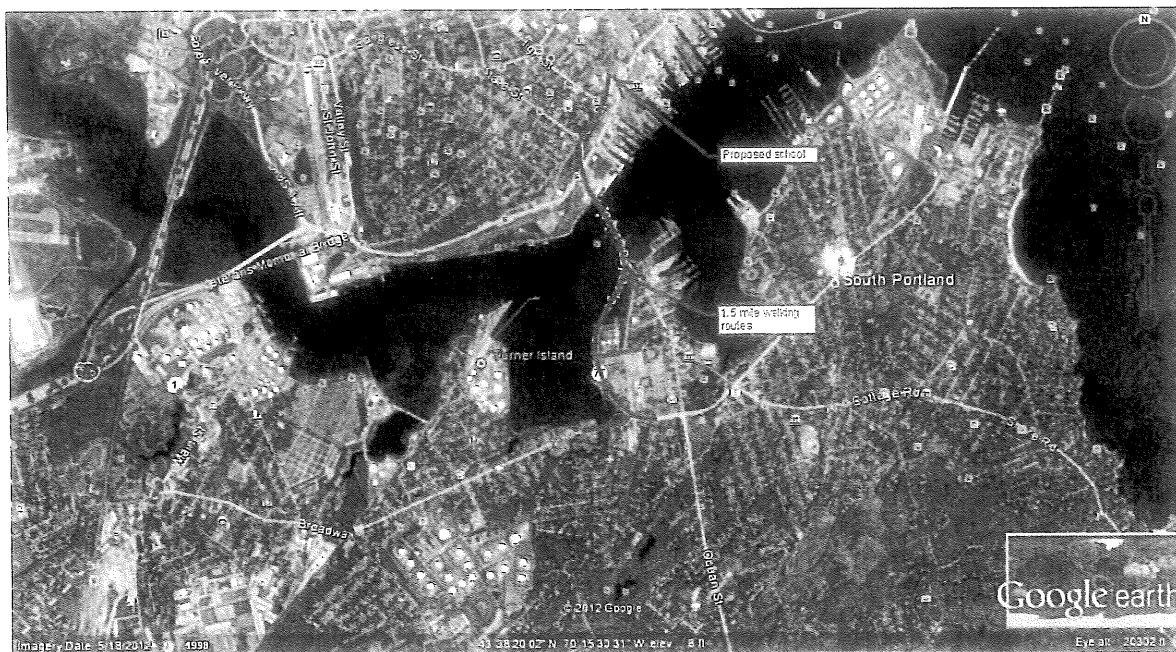
Item 4 – The sidewalk on the south side of York Street from High Street to the School site. This is not a simple matter as has been discussed with the Planning Board at our workshop meeting. We understand that the City has filed a Quality Community Grant application with MaineDOT for funding for this particular improvement, which has been estimated at \$110,000. In speaking with the Project Manager of this program at MaineDOT, we learned that the likelihood of this project receiving funding support as part of MaineDOT’s 2014-15 Work Plan is slim at best. This project is only one of 10 submitted by the City and the statewide competitiveness for these funds is extremely fierce. Therefore, this \$110,000 matter needs further discussion, because the proposed non-profit, non-tuition, public high school cannot absorb this cost.

Baxter Academy has presented an enrollment profile as part of its application materials. In Year 3 (full build-out) there is an expectation because of its urban setting that as many as 16 students and staff will walk to school. If we look at a map, see Google image below, we see that Portland’s closest residential neighborhoods (within 0.75 miles) are all located on the north side of York Street in the general direction of the West End. There is an existing sidewalk on the north side of York Street that is available to walkers originating from these neighborhoods, and the school is proposing to upgrade the crosswalks in front of the school to permit safe crossings of York Street at the school’s main entrance.



Who then would use a sidewalk if it were to be constructed on the south side of York Street from High Street to Maple Street? We have heard that it would be students and staff crossing the Casco Bay Bridge. If this is the case let’s look at the route this would involve. From the Google image below you can see that to access the nearest residential neighborhoods in South Portland involves a walking route that would exceed 1.5 miles. In our view, it is unlikely that students would walk in excess of 1.5 miles across the bridge to get to and from school on a regular basis. Therefore, we do not believe that the Baxter Academy will be attracting walkers from South Portland given the distance involved. As such, we do not see the nexus between the school and the continuation of the sidewalk on the south side of York Street to High Street. Without one, we believe it is unreasonable to burden our project with this expense.

L.2



Item 5 – Maple Street parking restrictions for student unloading and loading. Currently the west side of Maple Street is designated 15 minute parking from 6:00 AM to 6:00 PM. We contacted John Peverada and discussed this matter with him as well as the need for erecting 5 minute School Drop-Off Signs adjacent to the School site similar to those posted in front of Portland High School on Cumberland Avenue. He directed us to the City’s Traffic Division. Please advise us on how to proceed with this matter.

Item 6 – Crosswalk Pavement Markings. Our plans have been changed to show block style crosswalk markings for the intersection of York and Maple Streets.

Item 7 – Bicycle Racks. Our previous plan showed 5 bike racks in the rear alley and 5 bike posts along Maple Street in the sidewalk for a total of 15 spaces. Per the reaction of the Planning Board, we have increased the number of bike posts from 5 to 9 bringing the total spaces to 23.

Item 8 – Handicapped Parking Spaces on York Street. No HC spaces are proposed on York Street.

Item 9 – Intersection of High and Commercial Streets. This intersection was identified in our Section 7 materials as a High Crash Location. However, our TMP application is for less than 200 pce’s, so our Study Area should be limited to the immediate driveway or intersection, i.e. York and Maple Streets. We do not believe we should be subject to any contribution towards a safety concern at the High/Commercial Street intersection given that it does not border our site and any impacts from our project on this location will be very minimal.

We look forward to our meeting with you and Public Services on the 11th to bring this TMP application to closure before we meet again with the Planning Board. Thank you in advance for your cooperation.

Attachments:

Updated Site Plan

Updated Traffic Operations and Management Plan

TDM Plan

SEBAGO
SEBAGO ENGINEERING & ARCHITECTURE, INC.
15 NEW HAVEN BLVD., SUITE 104
NEW HAVEN, CT 06511
TEL: (203) 786-0000
WWW.SEBAAGO.COM

PROJECT INFORMATION

PROJECT NO: 07383
DATE: 05-22-12
CLIENT: SEBAGO ENGINEERING & ARCHITECTURE, INC.
LOCATION: 15 NEW HAVEN BLVD., SUITE 104, NEW HAVEN, CT 06511

DESIGN PHASES

PHASE: CONCEPT DESIGN
DATE: 05-22-12

REVISIONS

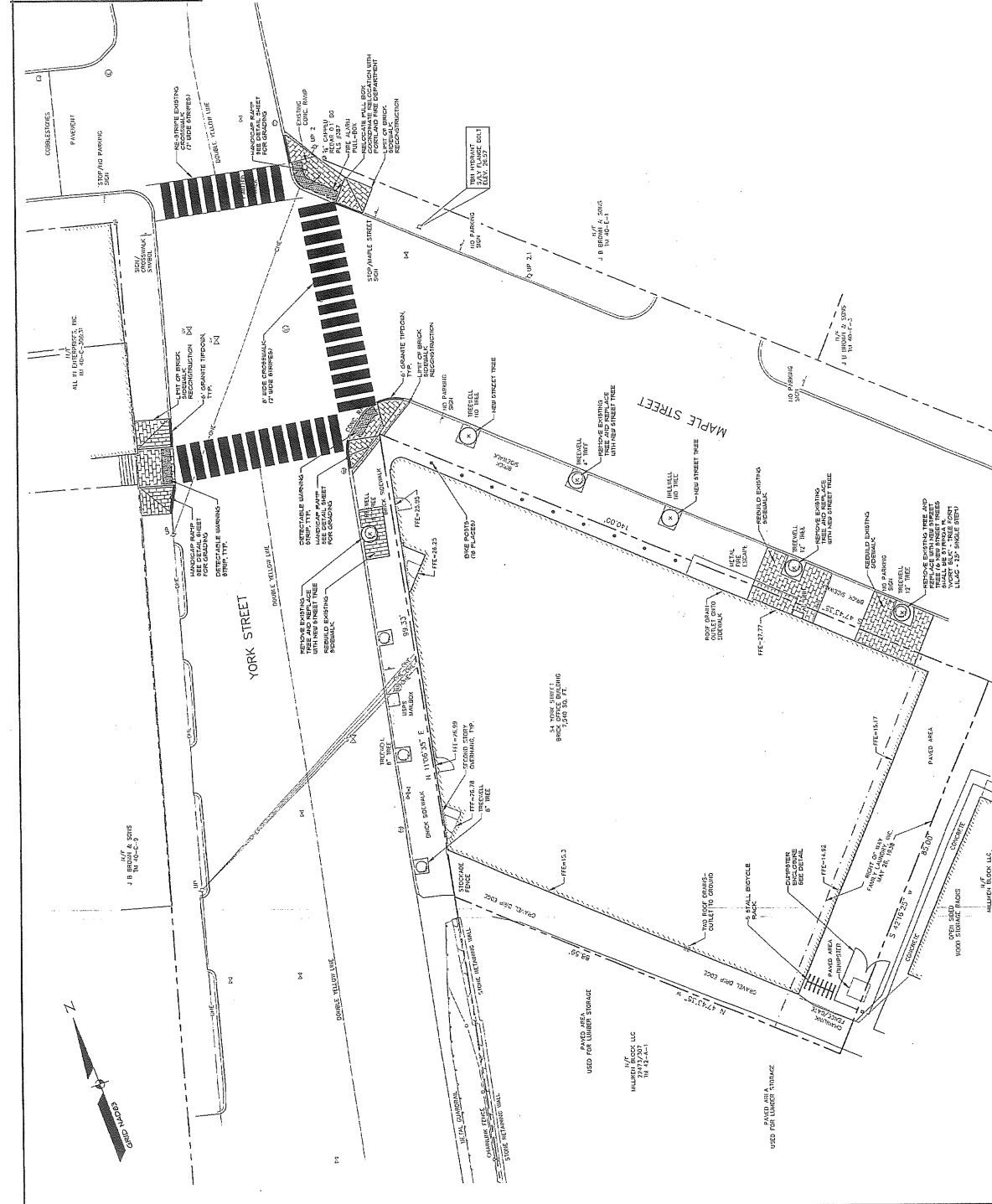
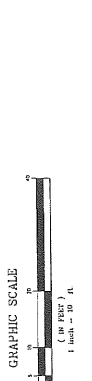
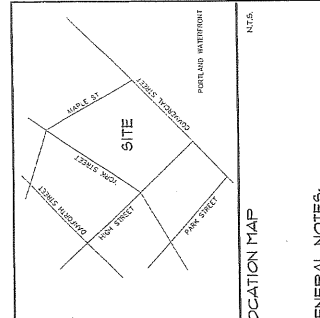
NO.	DATE	DESCRIPTION
1	05-22-12	ISSUED FOR TRAFFIC MOVEMENT REVIEW
2	05-22-12	ISSUED FOR TRAFFIC MOVEMENT REVIEW

LEGEND

SYMBOL	DESCRIPTION	PROPOSED
---	BOUNDARY LINES	---
---	EXISTING DRIVEWAYS	---
---	EXISTING SIDEWALKS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---
---	EXISTING DRIVEWAYS	---

GENERAL NOTES:

- THE BELOW NOTES SHALL BE CONSIDERED AS NECESSARY TO THE PROJECT. ALL NOTES SHALL BE CHECKED AGAINST THE PLAN AND THE LOCATION MAP TO DETERMINE ACCURACY.
- THIS PROJECT IS THE PROPERTY OF SEBAGO ENGINEERING & ARCHITECTURE, INC. AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF SEBAGO ENGINEERING & ARCHITECTURE, INC.
- THIS PLAN IS A PRELIMINARY DESIGN AND IS NOT TO BE USED FOR CONSTRUCTION WITHOUT THE WRITTEN PERMISSION OF SEBAGO ENGINEERING & ARCHITECTURE, INC.
- ALL DIMENSIONS ARE TO THE CENTERLINE OF THE DRIVEWAY OR SIDEWALK UNLESS OTHERWISE NOTED.
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE INTERNATIONAL BUILDING CODES (IBC) AND THE INTERNATIONAL PLUMBING CODES (IPC).
- ALL MATERIALS AND METHODS SHALL BE APPROVED BY THE APPLICABLE AGENCIES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.
- ALL UTILITIES SHALL BE LOCATED AND DEPTH DETERMINED PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AT ALL TIMES.
- ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL MAINTAIN PROPER RECORDS OF ALL CONSTRUCTION ACTIVITIES.
- ALL WORK SHALL BE COMPLETED WITHIN THE SPECIFIED TIME FRAME.
- ALL COSTS OF PERMITS AND APPROVALS SHALL BE THE RESPONSIBILITY OF THE CLIENT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.
- ALL UTILITIES SHALL BE LOCATED AND DEPTH DETERMINED PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AT ALL TIMES.
- ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL MAINTAIN PROPER RECORDS OF ALL CONSTRUCTION ACTIVITIES.
- ALL WORK SHALL BE COMPLETED WITHIN THE SPECIFIED TIME FRAME.
- ALL COSTS OF PERMITS AND APPROVALS SHALL BE THE RESPONSIBILITY OF THE CLIENT.



L.4

**Baxter Academy for Technology and Science
Traffic Operations and Management Plan
Revised November 28, 2012**

Arrivals and departures from the Baxter Academy are to be well-orchestrated and staff managed events. Because students will be arriving via various modes of transportation the School's Plan includes a number of elements. A key one will be that a majority of the students attending school at Baxter will be able to exit their transportation mode directly onto sidewalks adjacent to the school and maintained by the school during the winter months, so student safety is maximized.

Arrivals

The three charter buses will arrive at school and park on the south side of York Street heading in town directly in front of the school. Students will depart onto the existing York Street sidewalk in front of the main entrance to the school. Buses will be equipped with school flashing lights, so traffic on York Street will be stopped in both directions during this operation. The unloading for each bus is estimated to be 2 minutes. School staff will ensure that the three buses do not overly interrupt operations on York Street during the student discharge operation. This should not be an issue as the three buses will not likely arrive at the same time.

Students using public transit (Metro or South Portland Bus) will depart buses at existing stops on the north side of York Street at either High Street or Fore Street. Students will then proceed to school on existing sidewalks and cross York Street using the upgraded crosswalks at the intersection of York and Maple Streets. The School will have a staff person supervising these crosswalks for the 45 minutes preceding school's commencement to ensure safe student passage.

Parent drop-offs will occur on the west side of Maple Street adjacent to the existing sidewalk abutting the school. A staff person will supervise this operation to keep vehicles orderly, and students will walk up to York Street where they will enter the front door of the school.

Staff members who elect to drive their own vehicles to school will be parking in a nearby off-site lot or garage and will walk to school via existing sidewalks.

Students who elect to drive their own vehicles to school will likely park on Commercial Street in the unrestricted parking spaces adjacent to the end of Maple Street and will walk to school via existing sidewalks.

Students who elect to ride their bikes to school will be able to store their bikes in bike racks, which will be provided by the school adjacent to the building on Maple Street and in the back alley.

In addition to the staff members on-site managing the student arrival process, the school proposes to:

- place a School Crossing sign in the middle of the York and Maple Street crosswalks in front of the school to keep speeds on York Street under control during peak student usage.
- erect School Drop-Off Zone signs on west side of Maple Street for parent drop-offs.

See examples below.



“Crosswalk sign used by Waynflete”



“School Drop-Off Zone Sign at Portland High”

Departures

Charter buses will arrive at school and park on the south side of York Street, either directly in front of the school entrance or in the two parking spaces to the west of the school if they arrive early. At 2:00 PM when regular school is over students who are not participating in after school programs will load the buses from in front of the School as they did during the morning arrivals. The school buses will activate their school flashers during the loading operations, so traffic on York Street will be halted during these times.

Students using public transit (Metro or South Portland Bus) will depart the school for the bus stops on the north side of York Street at either High Street or Fore Street depending on individual choice and route schedules. Staff will be present to aid in crossing the street for these students.

Parent pick-ups will occur on the west side of Maple Street adjacent to the existing sidewalk. The schedule of these pick-ups will not be as concentrated as in the morning because of after school activities. The school is projecting that up to one half of students will be participating in after school programs, which would mean that parent pick-ups in Year 3 would be in the range of 34 at 2:00 PM and the other 34 would be spread over the subsequent 2 hours. Given the curb space adjacent to the school site this does not appear to be an issue.

Food Deliveries

Food service to the school will be provided by an off-site vendor. Deliveries will be made curbside on the west side of Maple Street, similar to where the parents will load and unload.

Proposed Signage Changes

Maple Street: The west side of the street is currently posted as 15-minute parking. We believe this would need to be modified to allow for parent drop-offs and pick-ups between 7:30 and 8:00 AM and between 1:45 and 2:15 PM. We have spoken with John Peverada on this matter and he saw no issue, but directed us to the City's traffic division.

York Street: No signage changes are anticipated on York Street.

On-Going Plan Refinement

The Baxter Academy realizes that their Traffic Management Plan is subject to individual student family preferences from year to year. As such, the school administration is committed to refining their operation each year to reflect the modal choices made by their enrollees. Given that Years 1 and 2 are ramping up years in terms of the student population, the school will have adequate time to fine-tune operations prior to reaching the ultimate goal of 320 students in Year 3.

**Baxter Academy for Technology and Science
Transportation Demand Management (TDM) Plan
November 29, 2012**

1. Transportation Narrative

The Baxter Academy for Technology and Science is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. The school's vision is to create a full 4-year high school over the next 3 years in the existing building at 54 York Street - the corner of York and Maple Streets. Total enrollment is planned for 320 students by 2015. In the opening Year (2013), 160 - first and second year students are envisioned. In Year 2, Grades 9, 10, and 11 will be provided, and in year three all four years of high school will be offered. It is estimated that 50% of the enrollment will come from Portland, South Portland and Westbrook, with the balance originating from communities outside of the urban core. School operations will run from 8:00 AM to 2:00 PM, resulting in peak traffic associated with the school being between 7-8:00 AM and 2-3:00 PM.

The School will manage the transportation and parking impacts of its employees and students through a continuous and consistent distribution of information. This message will highlight the variety of transportation options available to students and staff other than utilizing single occupant vehicles as their primary means of access to and from school. These options will include the provision of charter buses for students and staff living outside the urban core serviced by METRO or South Portland Bus services, encouraging car pooling for families that choose to transport their children to school, providing incentives for students and staff serviced by local transit, not providing parking for students as a discouragement to driving their own vehicles, and encouraging biking and walking to school for those within a reasonable distance from the facility. All of these strategies are aimed at reducing the traffic and parking impacts associated with the proposed facility.

2. TDM Coordinator

John Jaques, Founder/Executive Director of Baxter Academy, will serve as the TDM Coordinator. He can be reached at 54 York Street, Portland, Tel. (207) 318-8248, or at john@baxteracademy.org.

3. Parent and Teacher Surveys

One of the first steps taken by the School's administration in formulating their Transportation Plan was to reach out to prospective student families via a SurveyMonkey survey aimed at gauging transportation preferences. Much useful information was gained from this effort. Recognizing, though, that attitudes shift from year to year the School is committed to resurveying their population annually in conjunction with the registration process as a means of informing families of their transportation options and encouraging their participation in both trip reduction and parking reduction strategies given the location of the School's facility within Portland's downtown district. This survey will be accompanied by an information packet that will include relevant information on School-sponsored charter buses for the outlying areas; transit routes within the urban core with the School's subsidy program described; car pooling options and information about the School-sponsored ride-matching program; bicycling and walking options; and information on the lack of free parking in the immediate area of the school.

4. Parking and Trip Reduction Targets

ITE's Trip Generation, 7th Edition, Land Use Code 530 - High School, was consulted to determine industry standards for typical trip generation associated with facilities of this nature. Using a base school population of 320 students and 30 employees (representing full build out in Year 3), it is expected that AM trip generation would be 154 trips. Similarly, in Year 1 with 160 students and 15 employees this figure is estimated to be 77 trips. The School has established a target of 137 trips in Year 3 and 73 in Year 1 based on its anticipated enrollment and its Transportation Plan. This represents a reduction from published industry standards of 5% in Year 1 and 11% by Year 3.

ITE's Parking Generation, 3rd Edition, Land Use Code 530 - High School (Urban), was consulted to determine industry standards for typical parking associated with facilities of this nature. Unfortunately, this document only had three samples and they were all of much larger school populations, and thus this data is suspect for our purposes. In any event, using this published data and the expected school enrollments of 320 in Year 3 and 160 in Year 1, the parking requirements are estimated to be 29 in Year 3 and 14 in Year 1. The School's policy will be to provide only a limited number of off-site parking spaces for its staff and none for its students. This will be made clear in the School's registration materials as a discouragement to student drivers.

5. Parking and Trip Reduction Strategies

The School's parking reduction strategy is not to provide any off-site parking for its students. This should not be an issue in the initial year of operation, since the enrollment will only be 9th and 10th graders, which rarely have driver's licenses. In the later years, students may elect to use their cars for transportation, but "free" parking in the proximity of the school will be at a premium and should be a deterrent. For those students who want to bring their own vehicles to school, they will be required to register them with the school and park legally within the surrounding area. This is similar to Waynflete's policy. The school will monitor these situations, and work with the City to address any violators should they be brought to the school's attention.

As has been stated previously, trip reduction strategies employed by the School will include:

- Providing charter buses for outlying communities
- Encouraging the use of public transit within the urban core with subsidies
- Encouraging and facilitating car pooling/ride sharing
- Encouraging bicycling through the provision of on-site bike racks
- Encouraging walking by upgrading crosswalks in the vicinity of the School

6. Education

Registration materials provided by the School to prospective families will include transportation information about the various options that are available to students depending on their location of residency. Annual surveys will be conducted by the School to gauge family preferences, so that adjustments can be made in the various programs if need be. It will be made clear in these materials that the School is committed to supporting the City's goals for transportation and environmental sustainability within its urban core by discouraging single occupant vehicle usage and promoting the use of public transportation, ride sharing, biking and walking.

The School will operate its own ride matching program for families that would like to car pool as part of dropping-off and picking-up students on a regular basis. Information on this program will be included in the School's newsletters that go out to families.

For those students that can avail themselves of the METRO and South Portland Bus services, the School will provide a 50-cent subsidy per ride to incentivize the use of this mode of transportation.

7. Monitoring

Baxter Academy's TDM Plan will be monitored by its TDM Coordinator - the Executive Director. This effort will include:

- Annual family surveys regarding school transportation preferences
- Ridership figures on School-sponsored Charter Buses
- Public Transit usage through subsidy information
- The number of families participating in the School's ride-share/matching program
- The number of students biking to school
- The number of students registering their private vehicles with the School
- The number of staff utilizing off-site parking
- The number of staff utilizing public transportation or other travel options

This data will be compiled and compared against the targets established for both parking and vehicle trip production contained in Item 4 above. A summary report will be produced annually and submitted to the City's TDM Manager for review and comment.

8. Project Specific Standards

Infrastructure:

Public Transit Access - METRO and South Portland Bus have existing bus stops on the north side of York Street east and west of the School - at Fore and High Streets. Improved crosswalks are included in the School's Site Plan to ensure safe and convenient access to these two stop locations.

Bicycle Parking - A total of 23 bike parking stations are being provided initially as part of the School's Site Plan. If demand warrants additional facilities, the School is committed to providing them.

TDM Bulletin Board - School will erect a transportation bulletin board in a prominent space within the School with information on various transportation options, including ride matching possibilities.

TRAFFIC MOVEMENT
PERMIT APPLICATION

SECTION 7

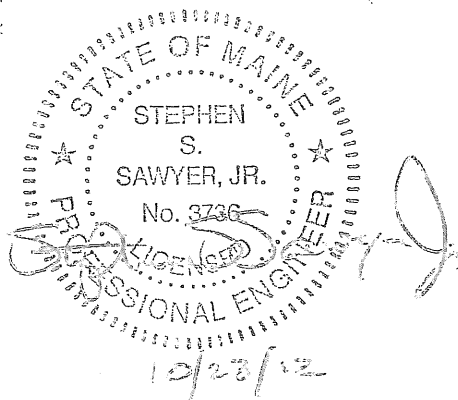


FOR

PROPOSED
CHARTER HIGH SCHOOL
54 YORK STREET
PORTLAND, ME

ON BEHALF OF

RUFUS DEERING LUMBER
COMPANY
384 COMMERCIAL STREET
PORTLAND, ME



OCTOBER 23, 2012

M.1

Section 7. Traffic Study

1. Site Plan.

Rufus Deering Lumber Company, 384 Commercial Street, Portland is proposing to remodel their existing building at 54 York Street (the corner of York and Maple Streets) to accommodate the new Baxter Academy for Technology and Science Charter High School. The current property houses the existing Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses of offices, labs, and a dance studio. This 0.22 acre (9,583 s.f.) lot is fully developed with an existing 7,540 s.f., 3-story building in the City's B-5b (Urban Business) Zone. This property is also located within the Old Port Historic District. See the Site Plan in the back of this submission.

2. Existing and Proposed Site Uses.

The existing building is currently developed with the following land uses:

- An exercise studio offering Zumba classes
- A Buddhist Center
- Administrative offices
- Global Biotech
- Deering Lumber's Kitchen and Millwork Showroom

Once the remodeling is complete the building will accommodate the Baxter Academy for Technology and Science Charter School as the primary tenant. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. The school's vision is to convert the entire building into a full 4-year high school over the next 3 years. Total enrollment is planned for 320 students by 2015. In the opening year (2013), 160 - first and second year students are envisioned. In year two, Grades 9, 10, and 11 will be provided, and in year three all four years of high school will be offered. It is estimated that 50% of the enrollment will come from Portland, South Portland and Westbrook, with the balance originating from communities outside of the urban core. School operations will run from 8:00 AM to 2:00 PM, resulting in peak traffic associated with the school being between 7-8:00 AM and 2-3:00 PM.

3. Site and Vicinity Boundaries.

A regional map showing the development site and its relationship to the adjacent street network as defined in MaineDOT's Chapter 305 of the General Rules of the Department of Transportation (Section 5B) is presented as part of the Site Plan located at the end of this submission.

4. Other Proposed Uses in the Vicinity of the Proposed Development.

A discussion with the City of Portland's Planning Department revealed that there are no other developments currently under review or have been permitted but not built in the vicinity of this Project at this time.

5. Trip generation.

ITE's Trip Generation, 7th Edition, was consulted to determine what industry data suggests for this type of land use. LUC 530 - High School, which includes both public and private institutions was deemed the closest category listed in this reference. Believing that the City's greatest traffic concern would be during the AM peak hour because it will coincide with the morning commute, we have focused our attention on this time period only. Using a base school population of 320 students and 30 employees, we find the following for the AM peak hour using ITE information:

<i>LUC 530</i>	<i>Variable</i>	<i>Total Trips</i>
By Student: $\ln(T) = 0.77\ln(X) + 0.69$	320 (students)	169
By Employee: Average Rate = 4.63	30 (employees)	139
Average of Above		154

The above trip generation assumes that there is no existing traffic associated with the site today, which is not entirely true. Currently there is an exercise studio (Nancy's) that offers among other things, Zumba classes, and a Buddhist Center (Nagaloka), and the administrative office for the Baxter School operating in the building. However, given the operating hours of these three uses, we have not taken a credit for any existing trips during the AM peak hour (7:00 AM to 8:00 AM).

From the data above it is estimated that the peak trip generation from the proposed development will be 154 trips (all primary), which will put the project in the Traffic Movement Permit (TMP) category of 100-200 Passenger Car Equivalent (PCE) trips.

6. Trip Distribution.

The ITE Trip Generation Handbook was also consulted with regard to the directional distribution of site generated trips during the AM peak period. ITE data suggests that during the AM peak period trips at the site will be distributed roughly 70% entering and 30% exiting regardless of which method of generation is used, i.e. "student" or "employee".

7. Trip Assignment.

While ITE suggests that traffic generation from the proposed facility could be in the range of new 150 trips in the AM peak hour, the Baxter Academy plans to adopt a series of travel demand management strategies for minimizing this impact on the surrounding street system. Typically, students at four-year public high schools have a variety of transportation choices that must be addressed in the school's planning. These range from parent drop-off and pick-up, to buses, and finally to student drivers with their own cars. The actual mix usually depends on the school's location within the community and the amount of available land surrounding the school. In the case of the Baxter Academy, the setting is very urban with no surplus land for off-street parking. Accordingly, the school's administration has formulated the following Transportation Plan.

First of all, it should be understood that in the initial year of operation, only 9th and 10th graders will be attending the Baxter Academy, so student drivers will not be part of the mix. However, in subsequent years this will be a consideration.

Not knowing exactly what to expect from prospective student families, Baxter Academy's administration conducted a Parent Survey to gauge their transportation preferences. A total of 24 families participated in this survey, which was performed in June of 2012. See the Appendix. The results indicated that approximately 25% of the families preferred to transport their children to and from school either by individual or car pool means. 70% envisioned using bus transportation, either charter service provided by the school or public transportation because they reside within Greater Portland. The remaining 5% indicated they would walk to and from the school. Before running any numbers on this basis, contact was made with a few similar facilities in the region – the Academy for Science and Design in Nashua, NH – a 410 student charter school serving Grades 6-12 in the business district of Nashua; Waynflete in Portland, a private K-12 school with 550 students in the west end; and Portland High School on Cumberland Avenue with 920 students. Table-1 presents the results from this research.

Table 1 – Modal Choice by Families of 4-year Urban High Schools

<i>School</i>	<i>% Parent Drop-Off</i>	<i>% Transit</i>	<i>% Student Drivers</i>	<i>% Walkers & Bikers</i>
Academy for Science & Design	65	34	1	0
Portland High School	50	23	16	11
Waynflete School	50	30	14	6
Baxter Parent Survey	25	70	-	5
Percentages Used Adopted	30	50	15	5

From the data in Table 1, there is no clear model that works for all cases – each school's experience seems to be unique depending on the school's setting and its market service area. Therefore, the modal split percentages adopted for Baxter are a reasonable blend of parent feedback and other surveyed school's experiences. Waynflete's parent drop-offs appeared to be influenced by the younger population K-8. Portland High's numbers are skewed due to the students all residing within the City where we expect many parents work. The Academy for Science & Design does not provide bus service but relies on the City's School District for busing. The school's setting is not conducive to walking or biking, and they only have 5 seniors with licenses at this point – hence the high percentage of parent drop-offs. Finally, the proposed Baxter Academy is planning to provide charter busing to outlying communities within its service area to reduce the need for parent shuttles.

The AM peak hour forecast below for the Baxter Academy is based on a student population of 320 students and 30 employees by Year 3 and 160 students and 15 employees in Year 1:

Year 1 (2013) – 160 students

- 30% parent drop off (assumed split of 50% car pooling with 3 students and 50% single occupant): 30% X 160 students = 48 students*
- 24 students in single occupant vehicles = 48 trips*
- 24 students in car pools of 3 = 16 trips*
- 55% of students (88) ride 3 charter buses (1.5 PCE per bus) = 9 trips*
- 10% of students (16) ride existing public transportation = 0 trips*
- 5% of students (8) as walkers = 0 trips*
- 15 employees @ 67% auto use = 10 trips**
- Total estimated AM peak hour trips in Year 1 = 73 trips*
- Anticipated Distribution = 37 arriving and 36 departing*

Year 3 (2015) - 320 students

30% parent drop off (assumed split of 50% car pooling with 3 students and 50% single occupant): $30\% \times 320$ students = 96 students

48 students in single occupant vehicles = 96 trips

48 students in car pools of 3 = 32 trips

40% of students (128) ride 3 charter buses (1.5 PCE per bus) = 9 trips

15% of students (48) drive their own cars = 0 trips*

10% of students (32) ride existing public transportation = 0 trips

5% of students (16) as walkers = 0 trips

30 employees @ 67% auto use = 20 trips*

Total estimated AM peak hour trips in Year 3 = 137 trips

Anticipated Distribution = 69 arriving and 68 departing

*Because the site cannot accommodate it, the School is not planning to provide on-site parking for students or employees. A total of 20 off-site parking spaces are initially going to be leased for employees, but since they are off-site their impact will not be felt at the site itself.

The School is proposing a full Traffic Management Plan to organize student arrivals and departures, which is detailed later herein. From a Trip Assignment perspective, though, this Plan will route arriving school buses from York Street onto Maple Street and load/unload curbside on the west side of the street in a designated School Drop Off Zone. The buses will depart the school using Maple Street to Commercial Street. Arriving parents dropping off their students will be directed to the drop off zone on westbound York Street opposite the front door of the school, which can be accessed by either leg of Maple Street and from York Street coming from the Old Port. Figures 2A and 2B illustrate the forecasted school traffic during the AM peak hour for both Years 1 and 3 at the York/Maple intersection under this traffic scenario. It is assumed that student arrivals will originate from all directions (south, west, and north) of the site and thus these trips have been assigned to the street network accordingly. Figure 3 illustrates the combined (existing and forecasted) trips at the York/Maple Street intersection during the two analysis periods.

8. Existing and Projected Traffic Volumes

Existing Conditions

Sebago staff performed a traffic turning movement count at the intersection of York Street and Maple Street during the hours of 7:00 AM and 8:00 AM on June 27, 2012 to observe traffic operations in and around the site during the time period when children would be arriving for the school day. Figure 1 presents the results of this peak hour count. It is readily evident that the volumes are quite small during this time period.

In addition to this data collection effort, several general observations were made while conducting the count. These were:

1. Only one on-street parking space was used on Maple Street between Commercial Street and York Street during the one hour time period observed. This vehicle was a van that had parked there overnight.

2. The paved parking lot for “The Cannery” that is accessed off Maple Street only had 8 cars enter it between 7:00 AM and 8:00 AM.
3. Maple Street is barely used during this time period. Total traffic during the hour observed was 34 vehicles. Never was a queue experienced waiting to enter onto York Street, and with the exception of one overnight parked vehicle, no on-street parking spaces were occupied.
4. The two on-street parking spaces to the west of the proposed school building on York Street were not occupied during the observation period.
5. While the traffic on York Street was predominantly in-bound, it appeared that the time period 7:00 AM to 8:00 AM might be before the actual peak morning commute hour.

Proposed Volumes

Traffic volumes for Year’s 1 and 3, as mentioned above, are presented in Figures 2A and 2B, with combined existing and proposed volumes illustrated in Figure 3.

9. Capacity Analyses

Synchro/SimTraffic was used to analyze existing traffic operations at the intersection of York and Maple Streets and it was determined that the Level of Service (LOS) was A.

Year 1 - 160 students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection’s LOS in Year 1. The LOS was found to be A and only slightly less than it is today under existing conditions. We modeled the parent drop-offs using appropriate peak hour factors.

Year 3 - 320 students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection’s LOS in Year 3. The LOS was found to still be A. We modeled the parent drop-offs using appropriate peak hour factors.

Based on the above analyses, capacity does not appear to be an issue during the AM in the vicinity of the school in either Year 1 or Year 3.

10. Traffic Signals

The applicant is not requesting the installation of a traffic signal in connection with its application, nor is one warranted at the York/Maple intersection.

11. Sight Distances

York Street has a posted speed limit of 25 mph. According to MaineDOT guidelines, the intersection sight distance for this speed is 200 feet. Maple Street as it intersects York Street from both directions is stop

M.6

controlled. Sight distances looking in both directions along York Street from both Maple Street approaches exceeds this 200 foot requirement. Vehicles exiting Maple Street onto York Street can clearly see both adjacent intersections - High Street is approximately 600 feet away and Danforth Street is roughly 400 feet away. Hence, sight distance at this intersection is not a problem.

12. Traffic Accidents

MaineDOT's Accident Records Unit was contacted to obtain the most recent 3-year crash information for the area in the vicinity of the proposed project. See attached materials from the Department in the Appendix. In summary, of the 12 intersections surveyed, only one is defined as a High Crash Location (HCL), i.e. a total of 8 crashes in three years and a Critical Rate Factor (CRF) greater than 1.0. This intersection is Commercial Street at High Street with a CRF of 1.50.

Road segments were also examined as evidenced by the enclosed Department data, and no segment immediately abutting the proposed project is identified as a High Crash Location.

Given that there is no HCL in the vicinity of the Project, crash history does not seem to be an influencing factor for this application.

13. Traffic Operations and Management Plan

Arrivals and departures from the Baxter Academy are to be well-orchestrated and staff managed events. Because students will be arriving via various modes of transportation the School's Plan will have a number of elements.

Arrivals

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. Students will depart directly onto the Maple Street sidewalk and proceed to the front door of the School on York Street.

Students using public transit (Metro or South Portland Bus) will depart buses at existing stops on the north side of York Street at either High Street or Fore Street. Students will then proceed to school on existing sidewalks and cross York Street at a repainted formal crosswalk at the intersection of York and Maple Streets. The School will have a staff person supervising this crosswalk for the 45 minutes preceding school's commencement to ensure safe student passage.

Parent drop-offs will occur on the north side of York Street opposite the front door of the school. The school will have a staff person supervising this operation to keep vehicles orderly, and students will be directed to the intersection of Maple and York, where there will be another staff member acting as a crossing guard for safe passage to the front entrance of the school.

Staff members who elect to drive their own vehicles to school will be parking in the Gulf of Maine parking lot off Commercial Street or the parking garage at the Holiday Inn By-The-Bay and will walk to school via existing sidewalks.

Students who elect to drive their own vehicles to school will likely park on Commercial Street in the unrestricted parking spaces adjacent to the end of Maple Street and will walk to school via existing sidewalks.

Students who elect to ride their bikes to school will be able to store their bikes in bike racks, which will be provided by the school adjacent to the building on Maple Street.

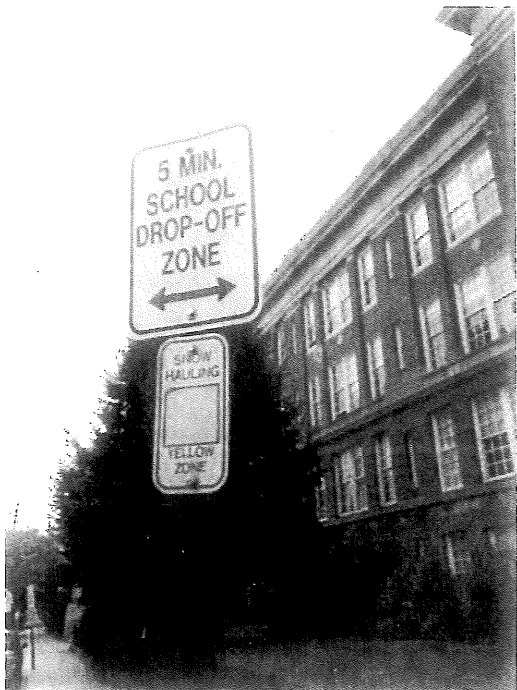
In addition to the staff members on-site managing the student arrival process, the school proposes to:

- use traffic cones to block off the curbside in front of the school to prevent vehicles from unloading on the south side of York Street
- place a School Crossing sign in the middle of the York Street crosswalk in front of the school to keep speeds on York Street under control during peak student usage
- erect School Drop Off Zone signs on Maple Street for the buses on the west side of the street and on the north side of York Street for parent drop-offs.

See examples below.



“Crosswalk sign used by Waynflete”



“School Drop-Off Zone Sign at Portland High”

Departures

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. At 2:00 PM when regular school is over students who are not participating in after school programs will load the buses from the Maple Street sidewalk as they did during arrivals.

Students using public transit (Metro or South Portland Bus) will depart the school for the bus stops on the north side of York Street at either High Street or Fore Street depending on individual choice and route schedules.

Parent pick-ups will occur on the north side of York Street opposite the front door of the school. The schedule of these pick-ups will not be as concentrated as in the morning because of after school activities. The school is projecting that up to one half of students will be participating in after school programs, which would mean that parent pick-ups in Year 3 would be in the range of 34 at 2:00 PM and the other 34 would be spread over the subsequent 2 hours. Given the 8 on-street spaces available this does not appear to be an issue.

Food Deliveries

Food service to the school will be provided by an off-site vendor. Deliveries will be made curbside on the west side of Maple Street, similar to where the buses will load and unload.

Proposed Signage Changes

Maple Street: The west side of the street is currently posted as 15-minute parking. This would need to be modified to allow for school bus parking between 7:30 and 8:00 AM and between 1:45 and 2:15 PM.

York Street: The north side of York Street opposite the school is currently posted as 30-minute parking. This would need to be modified to allow for 5 minute school drop-off and pick-up between 7:30 and 8:00 AM and between 2:00 PM and 4:00 PM.

On-Going Plan Refinement

The Baxter Academy realizes that their Traffic Management Plan is subject to individual student family preferences from year to year. As such, the school administration is committed to refining their operation each year to reflect the modal choices made by their enrollees. Given that Years 1 and 2 are ramping up years in terms of the student population, the school will have adequate time to fine-tune operations prior to reaching the ultimate goal of 320 students in Year 3.

14. Parking.

From the information presented above, the number of students and employees that may require parking in the vicinity of the school is 10 staff members in Year 1 and by Year 3 this may grow to 20 staff members and 48 students. The School has made leasing arrangements for between 20-40 off-street parking spaces in the Gulf of Maine lot off Commercial Street. Other options in surrounding parking garages also exist if this is not possible. The Baxter Academy is committed to reducing the number of students driving their cars to school. In this regard the School's informational packets will encourage families to use car pooling, public transportation, school provided charter buses, walking and biking to access the facility. To further support this policy, the school will be offering an incentive to students in the form of a 50-cent subsidy on public transit fares for those students within the Metro/South Portland Bus network.

For those students in Years 2 and beyond that do not choose to avail themselves of public transportation, and want to bring their own vehicles to school, they will be required to register their vehicles with the school and park legally within the surrounding area. This is similar to Waynflete's policy. The school will monitor these situations, and work with the City to address any violators should they be brought to the school's attention.

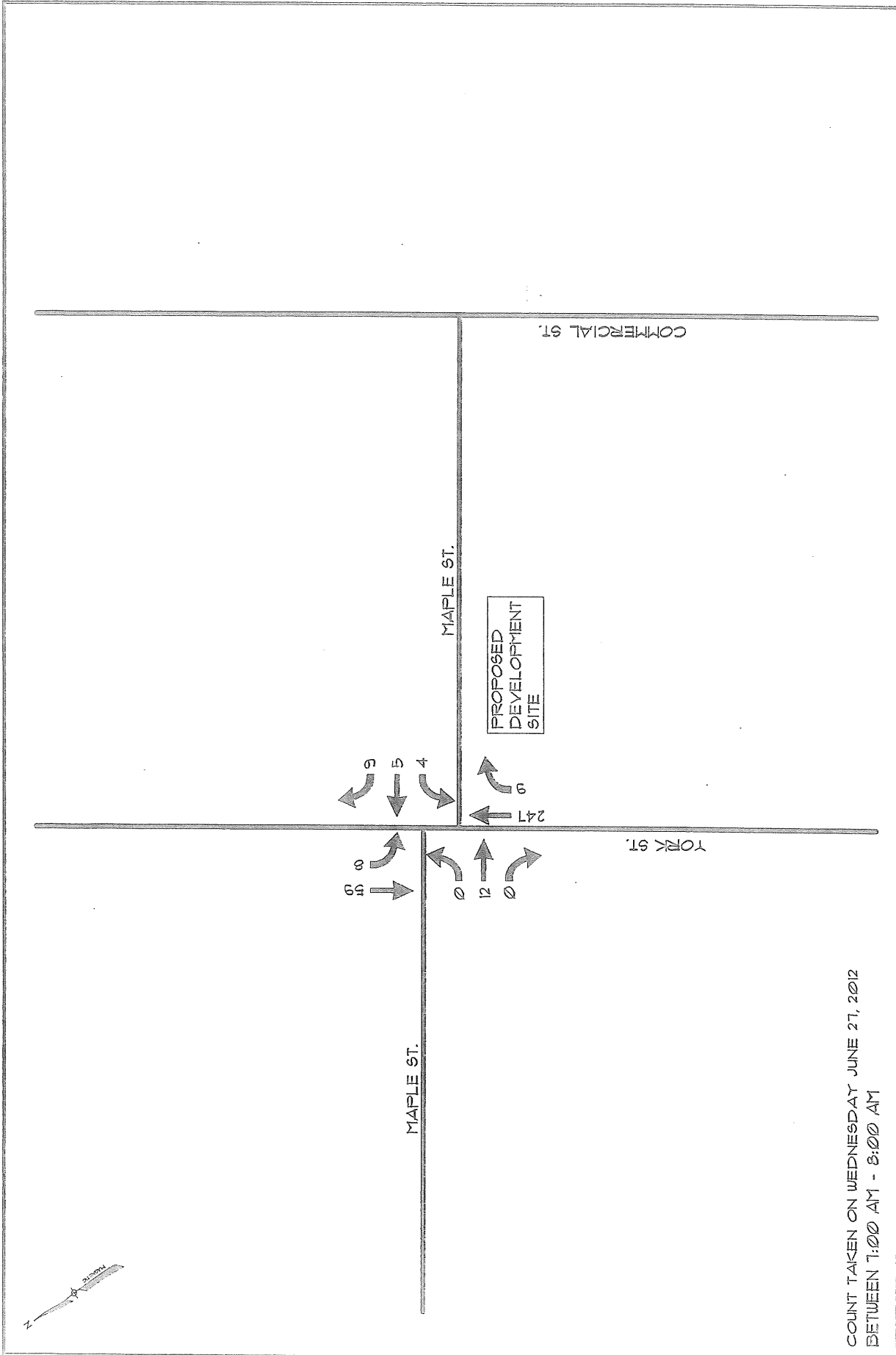
15. Recommendations

From the enclosed information, the proposed Baxter Academy at the corner of York and Maple Streets does not present a capacity or safety issue for the City of Portland. The applicant has presented not only a thorough traffic analysis to support these findings, but also a Traffic Management Plan as well as an off-site Parking Plan to further minimize its traffic impact on the surrounding neighborhood. On this basis, it is the professional opinion of Sebago Technics that the City should grant a Traffic Movement Permit to this applicant.

16. Conclusion

Given the findings presented herein regarding the proposed charter school at York and Maple Streets, Sebago Technics believes that a Traffic Movement Permit should be issued for this development.

M.10



COUNT TAKEN ON WEDNESDAY JUNE 27, 2012
 BETWEEN 1:00 AM - 8:00 AM

SEBAGO
 TECHNICAL SERVICES

WWW.SEAGOTECHNICALS.COM

75 John Roberts Rd. - Suite 1A 250 Goddard Rd. - Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel. 207-200-2100 Tel. 207-783-5858

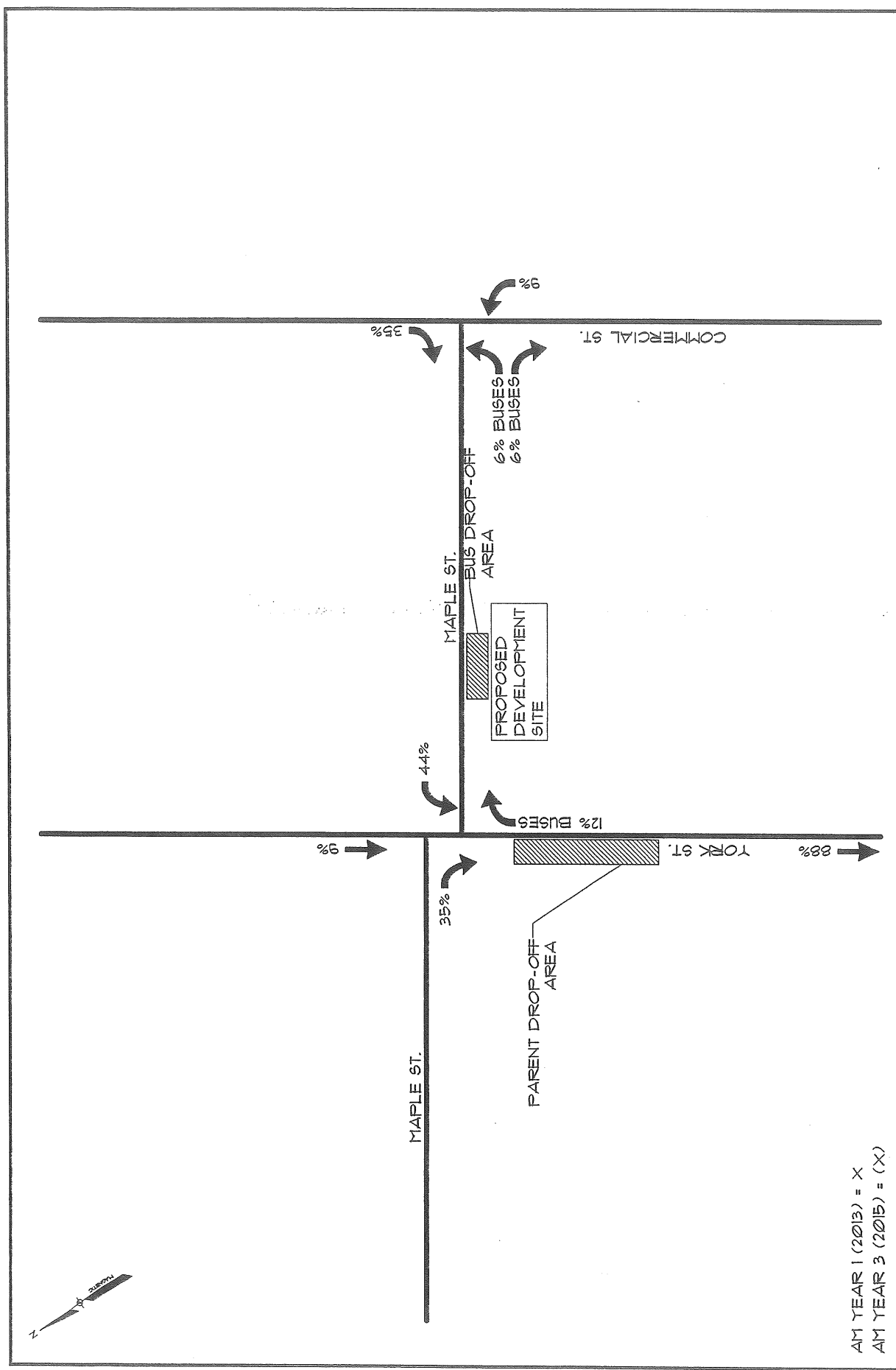
EXISTING TRAFFIC VOLUMES
 YORK ST. @ MAPLE ST., PORTLAND, ME

LOCATION: YORK ST. @ MAPLE ST.
 PORTLAND, ME

FOR: RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

SCALE: NTS
 DATE: 10/23/12
 FIGURE: 1

M.11



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

SCALE: NTS
 DATE: 10/23/12
 FIGURE: 2A

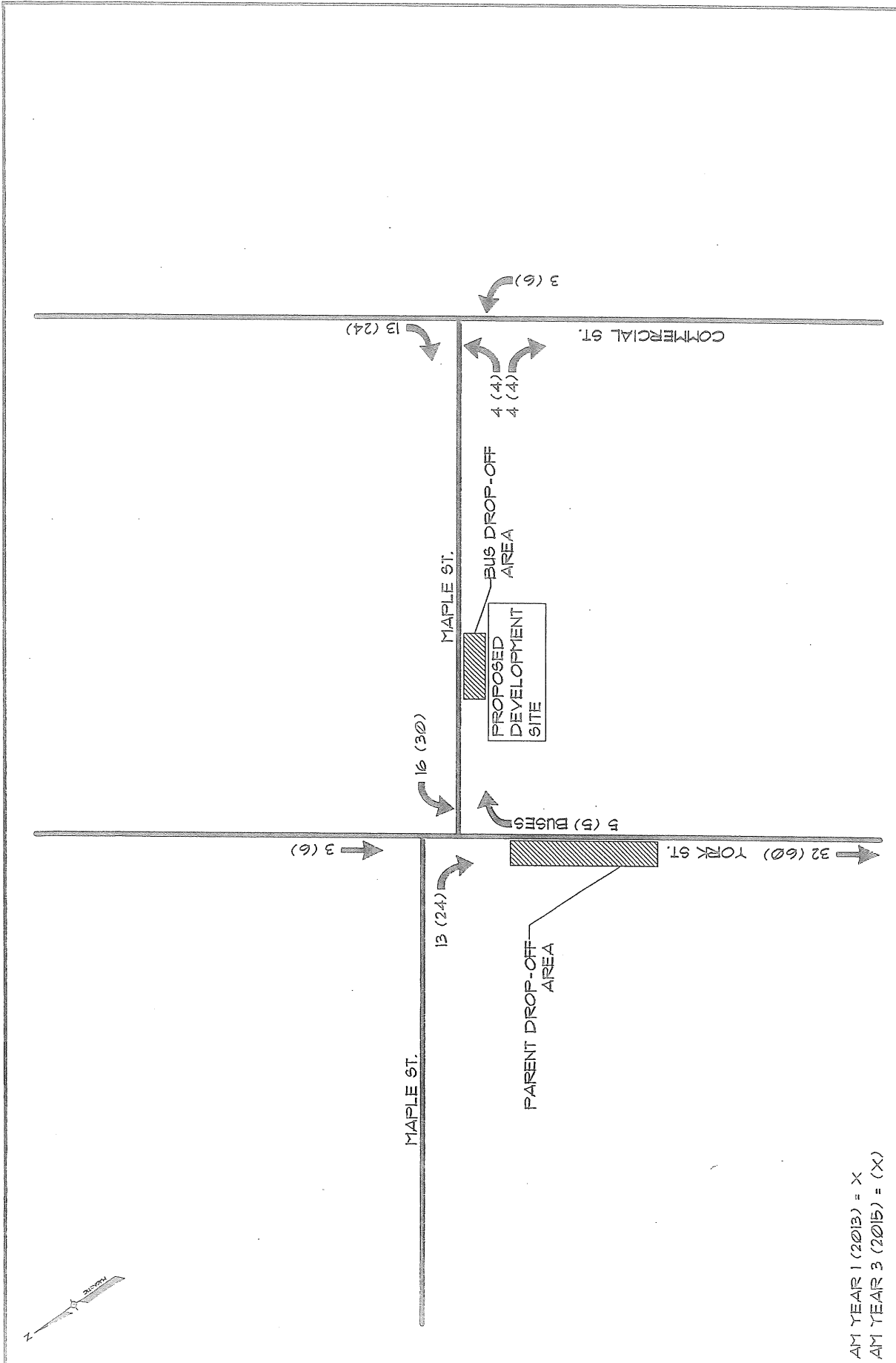
AM NEW PRIMARY TRIPS - % ASSIGNMENT
 YORK ST. @ MAPLE ST., PORTLAND, ME

LOCATION: YORK ST. @ MAPLE ST.
 PORTLAND, ME

FOR: RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

SEBAGO
 TECHNICALS
 WWW.SEBAGOTECHNICALS.COM
 75 John Roberts Rd., Suite 1A - 250 Goodard Rd., Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel. 207-200-2100 Tel. 207-785-5656

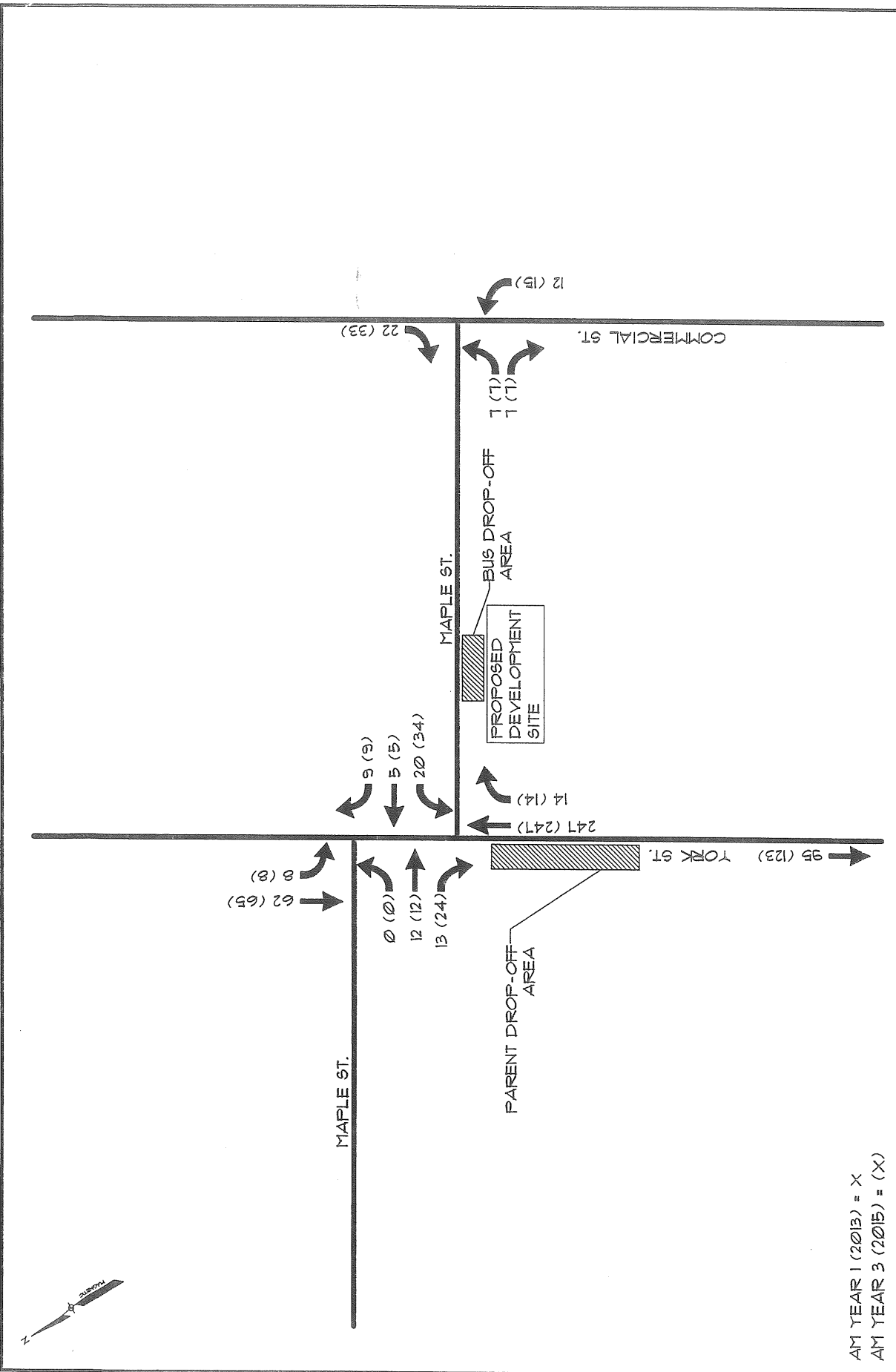
M.12



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

<p>SEBAGO <small>T E C H N I C S</small> <small>WWW.SEBAGOTECHNICS.COM</small> 75 John Roberts Rd. - Suite 1A South Portland, ME 04240 Tel. 207-200-2100</p>		<p>AM NEW PRIMARY TRIPS - VOLUMES YORK ST. @ MAPLE ST., PORTLAND, ME</p>		<p>SCALE: NTS</p>
<p>LOCATION: YORK ST. @ MAPLE ST. PORTLAND, ME</p>		<p>FOR: RUFUS DEERING LUMBER CO. 383 COMMERCIAL ST. PORTLAND, ME</p>		<p>DATE: 10/23/12</p>
				<p>FIGURE: 2B</p>

M.13



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

SCALE:	NTS
DATE:	10/23/12
FIGURE:	3

FOR: RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

LOCATION: YORK ST. @ MAPLE ST.
 PORTLAND, ME

SEBAGO
 TECHNICALS
 WWW.SEBAGOTECHNICS.COM
 75 John Roberts Rd., Suite 1A 250 Goddard Rd., Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel. 207-200-2100

M.14

Maple Street @ York Street, Portland ME
Existing Conditions

10/23/2012

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Delay / Veh (s)	0.5	0.3	2.7	0.2	6.2	6.5	2.7	6.1	0.8

Total Network Performance

Delay / Veh (s)	1.1								
-----------------	-----	--	--	--	--	--	--	--	--

M.15

Maple Street @ York Street, Portland ME
Proposed Year 1 Conditions

10/23/2012

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	SBR	All
Delay / Veh (s)	0.5	0.3	3.6	0.2	6.5	10.2	2.9	6.0	3.1	1.2

Total Network Performance

Delay / Veh (s)	1.5
-----------------	-----

M.16

Maple Street @ York Street, Portland ME
Proposed Year 3 Conditions

10/23/2012

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	SBR	All
Delay / Veh (s)	0.5	0.3	3.6	0.3	6.6	7.8	3.4	6.0	2.8	1.4

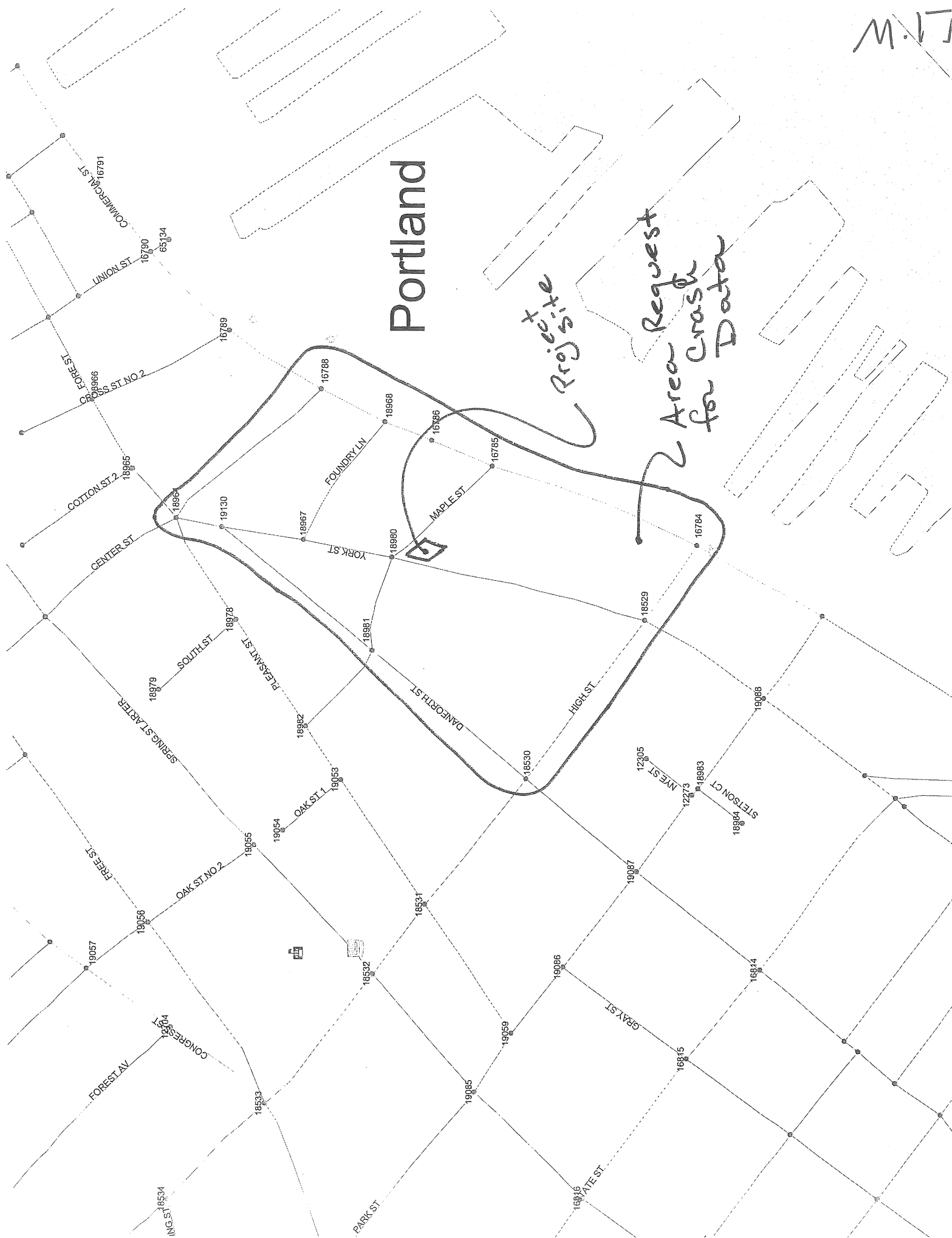
Total Network Performance

Delay / Veh (s)	1.8									
-----------------	-----	--	--	--	--	--	--	--	--	--

M.I.T

Portland

Project Site
Area Request for Crash Data



Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary Report

Report Selections and Input Parameters

REPORT SELECTIONS

Crash Summary I Section Detail Crash Summary II 1320 Included 1320 & Driver Report Included

REPORT DESCRIPTION

York St area

REPORT PARAMETERS

Year 2009, Start Month 1 through Year 2011 End Month: 12

Route: 0560122	Start Node: 16788 End Node: 18964	Start Offset: 0 End Offset: 0	<input type="checkbox"/> Exclude First Node <input type="checkbox"/> Exclude Last Node
Route: 3200508	Start Node: 18964 End Node: 18529	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0001A	Start Node: 16784 End Node: 16788	Start Offset: 0 End Offset: 0	<input type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0560365	Start Node: 16784 End Node: 18529	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0077W	Start Node: 18529 End Node: 18530	Start Offset: 0 End Offset: 0	<input type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0560191	Start Node: 19130 End Node: 18530	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input type="checkbox"/> Exclude Last Node
Route: 0560475	Start Node: 18980 End Node: 18981	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0560475	Start Node: 16785 End Node: 18980	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node
Route: 0560291	Start Node: 18967 End Node: 18968	Start Offset: 0 End Offset: 0	<input checked="" type="checkbox"/> Exclude First Node <input checked="" type="checkbox"/> Exclude Last Node

M.18

Maine Department Of Transportation - Traffic Engineering, Crash Records Section

Crash Summary I

Notes

Node	Route - MP	Node Description	U/R	Total Crashes	K	A	B	C	PD	Injury Crashes	Percent Annual M Injury Ent-Veh	Crash Rate	Critical Rate	CRF
16788	0560122 - 0	Int of CENTER ST, COMMERCIAL ST	9	8	0	0	1	1	6	25.0	5.645	0.47	1.08	0.00
												Statewide Crash Rate:	0.62	
18964	0560122 - 0.10	Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST	9	3	0	0	0	1	2	33.3	3.777	0.26	1.18	0.00
												Statewide Crash Rate:	0.62	
19130	3200508 - 0.02	Int of DANFORTH ST YORK ST	9	1	0	0	0	0	1	0.0	2.346	0.14	1.31	0.00
												Statewide Crash Rate:	0.62	
18967	3200508 - 0.06	0509387 POR, YORK, FOUNDRY LA.	2	0	0	0	0	0	0	0.0	0.930	0.00	0.51	0.00
												Statewide Crash Rate:	0.13	
18980	3200508 - 0.10	0509400 POR, MAPLE, YORK ST.	2	0	0	0	0	0	0	0.0	1.849	0.00	0.43	0.00
												Statewide Crash Rate:	0.13	
16784	0001A - 11.15	Int of COMMERCIAL ST HIGH ST	2	8	0	0	1	2	5	37.5	5.816	0.46	0.30	1.50
												Statewide Crash Rate:	0.12	
16785	0001A - 11.27	Int of COMMERCIAL ST MAPLE ST	2	2	0	0	0	1	1	50.0	5.507	0.12	0.31	0.00
												Statewide Crash Rate:	0.12	
16786	0001A - 11.30	Non Int COMMERCIAL ST	2	0	0	0	0	0	0	0.0	5.382	0.00	0.31	0.00
												Statewide Crash Rate:	0.12	
18968	0001A - 11.33	Int of COMMERCIAL ST FOUNDRY LN	2	0	0	0	0	0	0	0.0	5.466	0.00	0.31	0.00
												Statewide Crash Rate:	0.12	
18529	0077W - 1.39	Int of HIGH ST YORK ST	9	10	0	0	0	3	6	33.3	3.377	0.99	1.21	0.00
												Statewide Crash Rate:	0.62	
18981	0560191 - 0.10	Int of DANFORTH ST MAPLE ST	2	0	0	0	0	0	0	0.0	0.877	0.00	0.45	0.00
												Statewide Crash Rate:	0.11	
18530	0560191 - 0.20	Int of DANFORTH ST HIGH ST	9	18	0	0	1	5	12	33.3	6.348	0.95	1.06	0.00
												Statewide Crash Rate:	0.62	
Study Years: 3.00				50	0	0	3	13	33	32.0	47.320	0.35	0.47	0.75
NODE TOTALS:				50	0	0	3	13	33	32.0	47.320	0.35	0.47	0.75

M.19

Crash Summary I

M.20

Start Node	End Node	Element	Offset Begin - End	Route - MP	Section U/R Length	Total Crashes	Injury Crashes					Percent Injury	Annual HMVM	Crash Rate	Critical Rate	CRF		
							K	A	B	C	PD							
16788	18964	192464	0 - 0.10	0560122 - 0 RD INV 05 60122	0.10	2	1	0	0	1	0	100.0	0.00121	275.07	982.08	0.00		
		Int of CENTER ST, COMMERCIAL ST												Statewide Crash Rate: 336.22				
18529	18980	194048	0 - 0.14	3200508 - 0.04 RD INV 3200508	0.14	2	2	0	0	2	0	100.0	0.00243	274.26	820.78	0.00		
		Int of HIGH ST, YORK ST												Statewide Crash Rate: 336.22				
18964	19130	3106830	0 - 0.02	3200508 - 0 RD INV 3200508	0.02	2	1	0	0	0	1	0.0	0.00046	721.13	607.32	1.19		
		Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST												Statewide Crash Rate: 142.40				
18967	19130	194651	0 - 0.04	3200508 - 0.02 RD INV 3200508	0.04	2	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00		
		0509387 POR, YORK, FOUNDRY LA.												Statewide Crash Rate: 336.22				
18967	18980	194650	0 - 0.04	3200508 - 0.06 RD INV 3200508	0.04	2	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00		
		0509387 POR, YORK, FOUNDRY LA.												Statewide Crash Rate: 336.22				
16784	16785	3116402	0 - 0.12	0001A - 11.15 US 1A	0.12	2	6	0	0	1	5	16.7	0.00645	309.87	388.11	0.00		
		Int of COMMERCIAL ST, HIGH ST												Statewide Crash Rate: 171.50				
16785	16786	3130488	0 - 0.03	0001A - 11.27 US 1A	0.03	2	3	0	0	1	2	33.3	0.00163	614.36	551.86	1.11		
		Int of COMMERCIAL ST, MAPLE ST												Statewide Crash Rate: 171.50				
16788	18968	3119256	0 - 0.04	0001A - 11.29 US 1A	0.04	2	1	0	0	0	1	0.0	0.00216	154.31	513.40	0.00		
		Int of CENTER ST, COMMERCIAL ST												Statewide Crash Rate: 171.50				
16786	18968	3118839	0 - 0.03	0001A - 11.30 US 1A	0.03	2	1	0	0	0	1	0.0	0.00160	208.12	554.11	0.00		
		Non Int COMMERCIAL ST												Statewide Crash Rate: 171.50				
16784	18529	3115419	0 - 0.05	0560365 - 0 RD INV 05 60365	0.05	2	0	0	0	0	0	0.0	0.00063	0.00	683.39	0.00		
		Int of COMMERCIAL ST, HIGH ST												Statewide Crash Rate: 171.50				
18529	18530	3106683	0 - 0.10	0077W - 1.39 ST RTE 77W	0.10	2	10	0	0	0	10	0.0	0.00383	870.25	446.50	1.95		
		Int of HIGH ST, YORK ST												Statewide Crash Rate: 173.49				
18981	19130	3106832	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	2	0	0	0	2	0.0	0.00071	935.70	573.37	1.63		
		Int of DANFORTH ST, MAPLE ST												Statewide Crash Rate: 142.40				
18530	18981	3106686	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	4	0	0	0	4	0.0	0.00083	1609.24	557.81	2.88		
		Int of DANFORTH ST, HIGH ST												Statewide Crash Rate: 142.40				
18980	18981	194666	0 - 0.05	0560475 - 0.07 RD INV 05 60475	0.05	2	0	0	0	0	0	0.0	0.00004	0.00	558.41	0.00		
		0509490 POR, MAPLE, YORK ST.												Statewide Crash Rate: 336.22				
16785	18980	192461	0 - 0.07	0560475 - 0 RD INV 05 60475	0.07	2	2	0	0	0	2	0.0	0.00015	4537.85	1451.68	3.13		
		Int of COMMERCIAL ST, MAPLE ST												Statewide Crash Rate: 336.22				
18967	18968	194649	0 - 0.07	0560291 - 0 RD INV 05 60291	0.07	2	0	0	0	0	0	0.0	0.00013	0.00	1448.34	0.00		
		0509387 POR, YORK, FOUNDRY LA.												Statewide Crash Rate: 336.22				
Study Years: 3.00						Section Totals:	1.10	33	0	0	1	4	28	15.2	0.02360	466.01	338.32	1.38
Grand Totals:						Grand Totals:	1.10	83	0	0	4	17	61	25.3	0.02360	1172.09	461.67	2.54

Crash Summary

M.21

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	Injury Crashes				Crash Report	Crash Date	Crash Mile Point	Injury Degree	
						K	A	B	C					PD
16788	18964	192464	0 - 0.10	0560122 - 0	1	0	0	0	1	0	2010-5651C	03/14/2010	0.01	C
18964	19130	3106830	0 - 0.02	3200508 - 0	1	0	0	0	1	0	2011-7755C	04/15/2011	0.01	PD
18967	19130	194651	0 - 0.04	3200508 - 0.02	0	0	0	0	0	0				
18967	18980	194650	0 - 0.04	3200508 - 0.06	0	0	0	0	0	0				
18529	18980	194048	0 - 0.14	3200508 - 0.10	2	0	0	0	2	0	2010-3489C	02/06/2010	0.11	C
											2011-9159	08/31/2011	0.20	C
16784	16785	3116402	0 - 0.12	0001A - 11.15	6	0	0	0	1	5	2011-8884	08/28/2011	11.16	C
											2011-6597	08/03/2011	11.16	PD
											2009-32880C	12/29/2009	11.16	PD
											2009-11594C	05/22/2009	11.17	PD
											2010-14757C	07/20/2010	11.17	PD
											2010-12965C	05/31/2010	11.18	PD
16785	16786	3130488	0 - 0.03	0001A - 11.27	3	0	0	1	0	2	2010-30463C	12/26/2010	11.28	B
											2009-10518C	05/13/2009	11.28	PD
											2010-15225C	06/21/2010	11.28	PD
16786	18968	3118839	0 - 0.03	0001A - 11.30	1	0	0	0	0	1	2009-19621C	08/31/2009	11.31	PD
16788	18968	3119256	0 - 0.04	0001A - 11.33	1	0	0	0	0	1	2010-14622C	07/13/2010	11.36	PD
16784	18529	3115419	0 - 0.05	0560365 - 0	0	0	0	0	0	0				
18529	18530	3106683	0 - 0.10	0077W - 1.39	10	0	0	0	0	10	2009-6432C	03/04/2009	1.40	PD
											2011-1699C	01/30/2011	1.40	PD
											2011-3653	06/29/2011	1.41	PD
											2010-9558C	05/05/2010	1.43	PD
											2011-16796	11/28/2011	1.45	PD
											2009-1917C	01/09/2009	1.45	PD
											2011-5217	07/19/2011	1.46	PD
											2009-18444C	08/16/2009	1.48	PD
											2011-710C	01/07/2011	1.48	PD
18981	19130	3106832	0 - 0.10	0560191 - 0	2	0	0	0	0	2	2010-14209C	07/10/2010	1.48	PD
											2009-6337C	02/20/2009	0.01	PD
											2010-16740C	08/11/2010	0.02	PD
18530	18981	3106686	0 - 0.10	0560191 - 0.10	4	0	0	0	0	4	2011-5063	07/15/2011	0.11	PD
											2010-24444C	11/05/2010	0.15	PD
											2010-1886C	01/26/2010	0.15	PD
											2010-26329C	11/30/2010	0.19	PD

M.22

Crash Summary

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	Injury Crashes			Crash Report	Crash Date	Crash Mile Point	Injury Degree	
						K	A	B					C
18980	18981	194666	0 - 0.05	0560475 - 0.07	0	0	0	0	0				
16785	18980	192461	0 - 0.07	0560475 - 0	2	0	0	0	2	2010-9693C	05/14/2010	0.04	PD
18967	18968	194649	0 - 0.07	0560291 - 0	0	0	0	0	0	2011-2927C	02/04/2011	0.06	PD

Totals: 33 0 0 1 4 28

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Day and Hour

Day Of Week	Hour of Day												Un	Tot												
	AM						PM																			
	12	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6	7	8	9	10	11		
SUNDAY	1	3	0	1	0	0	0	1	0	1	2	0	0	0	0	2	1	0	0	0	0	2	0	0	0	14
MONDAY	0	0	0	0	0	0	0	0	1	0	2	2	0	1	2	1	2	2	0	0	0	1	0	0	0	14
TUESDAY	0	1	0	0	0	0	0	1	1	1	0	1	1	0	2	0	2	0	1	0	0	1	0	0	0	10
WEDNESDAY	0	0	0	0	0	0	0	0	1	1	1	0	2	1	0	0	3	1	2	0	0	0	0	0	0	12
THURSDAY	0	0	0	0	0	0	0	1	1	0	1	0	2	0	0	0	1	1	0	1	0	1	0	0	0	8
FRIDAY	1	0	0	0	0	0	0	2	0	1	1	2	2	2	1	2	2	2	0	2	0	2	0	0	0	20
SATURDAY	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1	1	0	0	1	0	0	0	5	
Totals	2	4	0	1	0	0	0	5	4	4	7	5	7	4	3	9	10	8	2	2	3	2	0	0	83	

Vehicle Counts by Type

Unit Type	Total	Unit Type	Total
1-Passenger Car	103	23-Bicyclist	2
2-(Sport) Utility Vehicle	30	24-Witness	13
3-Passenger Van	13	25-Other	3
4-Cargo Van (10K lbs or Less)	0	Total	193
5-Pickup	20		
6-Motor Home	0		
7-School Bus	0		
8-Transit Bus	0		
9-Motor Coach	0		
10-Other Bus	0		
11-Motorcycle	1		
12-Moped	0		
13-Low Speed Vehicle	0		
14-Autocycle	0		
15-Experimental	0		
16-Other Light Trucks (10,000 lbs or Less)	0		
17-Medium/Heavy Trucks (More than 10,000 lbs)	6		
18-ATV - (4 wheel)	0		
20-ATV - (2 wheel)	0		
21-Snowmobile	0		
22-Pedestrian	2		

M.23

Maine Department of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Driver Action at Time of Crash

Driver Action at Time of Crash	Dr 1	Dr 2	Dr 3	Dr 4	Dr 5	Other	Total
No Contributing Action	9	5	1	0	0	0	15
Ran Off Roadway	1	0	0	0	0	0	1
Failed to Yield Right-of-Way	8	9	0	0	0	0	17
Ran Red Light	1	1	0	0	0	0	2
Ran Stop Sign	0	0	0	0	0	0	0
Disregarded Other Traffic Sign	0	0	0	0	0	0	0
Disregarded Other Road Markings	0	0	0	0	0	0	0
Exceeded Posted Speed Limit	1	2	1	0	0	0	4
Drove Too Fast For Conditions	0	0	0	0	0	0	0
Improper Turn	3	2	0	0	0	0	5
Improper Backing	2	1	0	0	0	0	3
Improper Passing	1	2	0	0	0	0	3
Wrong Way	0	0	0	0	0	0	0
Followed Too Closely	4	7	1	0	0	0	12
Failed to Keep in Proper Lane	1	0	0	0	0	0	1
Operated Motor Vehicle in Erratic, Reckless, Careless, Negligent or Aggressive Manner	0	0	0	0	0	0	0
Swerved or Avoided Due to Wind, Slippery Surface, Motor Vehicle, Object, Non-Motorist in Roadway	0	0	0	0	0	0	0
Over-Correcting/Over-Steering	0	0	0	0	0	0	0
Other Contributing Action	0	1	0	0	0	0	1
Unknown	0	1	0	0	0	0	1
Total	31	31	3	0	0	0	65

Crashes by Apparent Physical Condition And Driver

Apparent Physical Condition	Dr 1	Dr 2	Dr 3	Dr 4	Dr 5	Other	Total
Apparently Normal	72	72	10	1	0	2	157
Physically Impaired or Handicapped	0	0	0	0	0	0	0
Emotional (Depressed, Angry, Disturbed, etc.)	0	0	0	0	0	0	0
Ill (Sick)	0	0	0	0	0	0	0
Asleep or Fatigued	0	0	0	1	0	0	1
Under the Influence of Medications/Drugs/Alcohol	3	3	0	0	0	0	6
Other	1	2	0	0	0	0	3
Total	76	77	10	2	0	2	167

Driver Age by Unit Type

Age	Driver	Bicycle	Snowmobile	Pedestrian	ATV	Total
09-Under	0	0	0	0	0	0
10-14	0	0	0	0	0	0
15-19	14	0	0	0	0	14
20-24	39	0	0	0	0	39
25-29	22	0	0	0	0	22
30-39	26	0	0	0	0	26
40-49	18	0	0	0	0	18
50-59	28	0	0	0	0	28
60-69	14	0	0	0	0	14
70-79	5	0	0	0	0	5
80-Over	3	0	0	0	0	3
Unknown	7	2	0	2	0	11
Total	176	2	0	2	0	180

M.24

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Year and Month

Month	2009	2010	2011	Total
JANUARY	3	1	2	6
FEBRUARY	3	2	4	9
MARCH	1	4	0	5
APRIL	2	3	3	8
MAY	3	5	2	10
JUNE	1	2	2	5
JULY	2	4	6	12
AUGUST	4	1	5	10
SEPTEMBER	1	0	0	1
OCTOBER	2	4	0	6
NOVEMBER	1	3	4	8
DECEMBER	2	1	0	3
Total	25	30	28	83

Report is limited to the last 10 years of data.

M.26

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Crash Type and Type of Location

Crash Type	Straight Road	Curved Road	Three Leg Intersection	Four Leg Intersection	Five or More Leg Intersection	Driveways	Bridges	Interchanges	Other	Parking Lot	Private Way	Cross Over	Railroad Crossing	Total
Object in Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rear End / Sideswipe	26	0	12	15	0	2	0	0	0	0	0	0	0	55
Head-on / Sideswipe	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Intersection Movement	0	0	2	13	1	5	0	0	0	0	0	0	0	21
Pedestrians	0	0	2	0	0	0	0	0	0	0	0	0	0	2
Train	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Went Off Road	0	0	0	2	1	0	0	0	0	0	0	0	0	3
All Other Animal	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bicycle	0	0	1	1	0	0	0	0	0	0	0	0	0	2
Other	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Jackknife	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rollover	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Fire	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Submerston	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Thrown or Falling Object	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bear	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Deer	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Moose	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Turkey	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	26	0	17	31	2	7	0	0	0	0	0	0	0	83

M.2

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

M.28

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Blowing Sand, Soil, Dirt												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Blowing Snow												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Clear												
Dark - Lighted	13	0	0	0	0	0	0	2	0	0	0	15
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	36	1	0	0	0	0	0	0	0	0	1	38
Dusk	2	0	0	0	0	0	0	0	0	0	0	2
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Cloudy												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	8	0	0	0	0	0	0	0	0	0	3	11
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Fog, Smog, Smoke												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	1	0	0	0	0	0	0	0	0	0	0	1
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Other												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Rain												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	4	4
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	1	1
Daylight	0	0	0	0	0	0	0	0	0	0	5	5
Dusk	0	0	0	0	0	0	0	0	0	0	1	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Severe Crosswinds												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

M.29

Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

M.30

Weather	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Light												
Steel, Hail (Freezing Rain or Drizzle)	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Snow												
Dark - Lighted	0	0	0	0	0	0	0	1	0	0	1	2
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	2	0	0	0	2
Dusk	0	0	0	0	0	0	0	1	0	0	0	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL	60	1	0	0	0	0	0	6	0	0	0	83

You have a BASIC account | To remove the limits of a BASIC account and get unlimited questions, upgrade now!

Parent Survey June 22

Education

Design Survey Collect Responses Analyze Results

- [View Summary](#)
- [Browse Responses](#)
- [Filter Responses](#)
- [Crosstab Responses](#)
- [Download Responses](#)
- [Share Responses](#)

Default Report + Add Report

Response Summary

Total Started Survey: 26
Total Finished Survey: 26 (100%)

PAGE: 1

1. What is your preferred method of transportation to school?

Create Chart Download

	Response Percent	Response Count
walking	3.8%	1
bicycle	0.0%	0
public transportation	11.5%	3
parent ride	19.2%	5
parent carpool	11.5%	3
school bus	53.8%	14
answered question		26
skipped question		0

2. Would you prefer a parent carpool if Baxter Academy were to help facilitate?

Create Chart Download

	Response Percent	Response Count
yes	52.0%	13
no	48.0%	12
answered question		25
skipped question		1

3. Would you be willing to drive as part of a carpool?

Create Chart Download

	Response Percent	Response Count
--	---------------------	-------------------

SurveyMonkey - Survey Results

M.32

yes	70.8%	17
no	29.2%	7
answered question		24
skipped question		2

4. Would you be willing to pay a monthly bus fee?

		Create Chart	Download
		Response	Response
		Percent	Count
yes	80.8%	21	
no	19.2%	5	
answered question		26	
skipped question		0	

5. Would you be picking your child up from school at dismissal at 2pm?

		Create Chart	Download
		Response	Response
		Percent	Count
yes	36.0%	9	
no	64.0%	16	
answered question		25	
skipped question		1	

6. Would you want your child to ride a school bus back to your hometown? (outside of Portland only)

		Create Chart	Download
		Response	Response
		Percent	Count
yes	65.0%	13	
no	35.0%	7	
answered question		20	
skipped question		6	

7. Would you be picking up your child later in the day from the Portland Public Library, or from some other downtown location?

		Create Chart	Download
		Response	Response
		Percent	Count

SurveyMonkey – Survey Results

yes	36.0%	9
no	64.0%	16

answered question 25

skipped question 1

Follow Us: [Facebook](#) • [Twitter](#) • [LinkedIn](#) • [Our Blog](#)

Help: [Tutorials](#) • [Answers & FAQs](#) • [Contact Support](#)

About Us: [Management Team](#) • [Board of Directors](#) • [Partners](#) • [Newsroom](#) • [Contact Us](#) • [We're Hiring](#) • [Sitemap](#)

Policies: [Terms of Use](#) • [Privacy Policy](#) • [Anti-Spam Policy](#) • [Security Statement](#) • [Email Opt-Out](#)

[Dansk](#) • [Deutsch](#) • [English](#) • [Español](#) • [Français](#) • [한국어](#) • [Italiano](#) • [Nederlands](#) • [日本語](#) • [Norsk](#) • [Português](#) • [Русский](#) • [Suomi](#) • [Svenska](#) • [中文\(繁體\)](#)

Copyright © 1999-2012 SurveyMonkey

Shukria Wiar - Baxter Academy: Crosswalk Committee Action

From: Bruce Hyman
To: Barbara Barhydt; Shukria Wiar; Tom Errico
Date: 1/3/2013 10:14 AM
Subject: Baxter Academy: Crosswalk Committee Action
CC: David Margolis-Pineo; Jeremiah Bartlett; Katherine Earley

Good morning, Shukria,

This morning, the city's Crosswalk Committee deliberated on the proposed curb ramp and crosswalk configurations as shown in the Baxter Academy site plan dated 12/14/12 per Sebago Technics (sheet 3 of 4). The Crosswalk Committee approves the new crosswalk across York Street on the west side of Maple Street, as shown. The Committee understood that there is to be new exterior lighting on the Baxter Academy building as well the existing exterior lighting on the Portland Pie building to adequately light the new crossing during the nighttime.

Please do not hesitate to contact me if you require any additional information.

Best regards,

Bruce

Bruce Hyman
Bicycle & Pedestrian Program Coordinator

City of Portland, Maine
Dept. of Public Services
bhyman@portlandmaine.gov
207-874-8833

**Memorandum
Planning and Urban Development Department
Planning Division**



To: Carol Morrisette, Chair and Members of the Portland Planning Board
From: Shukria Wiar, Planner
Date: November 21, 2012
Re: Baxter Academy for Technology and Science. 54 York Street, Rufus Deering Lumber Company
Project #: 2012-519 **CBL:** 42-A-007
Meeting Date: November 27, 2012

I. INTRODUCTION

Dan Labrie of behalf of Rufus Deering Lumber Company has requested a workshop meeting with the Planning Board for a change of use at 54 York Street and a traffic movement permit. The proposal is to convert most of the building to the Baxter Academy for Technology and Science (a charter high school), which is an institutional use. The proposed school is the Baxter Academy for Technology and Science and will be a charter school. The building has three of floors and the current uses include Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses for offices, labs, and dance studio. The site for the proposed Charter School consists of 0.22 acre (9,583sq.ft.) that is fully developed and the existing three-story building is located in the Urban Commercial Business B-5b zone and the Old Port Historic District.

The Level II site plan application was submitted in July of this year. At that time, the applicant sought to phase the project over three years: 9th grade in the first year, 10th and 11th grade in the second year, 12th in the third year. The building space was to be converted in phases as well to accommodate the incoming grades with the anticipated start date of September of 2012. On July 17, 2012, Baxter Academy was given conditional approval by the Maine Charter School Commission to open a charter school in Fall of 2013. Since the charter school opening was delayed for another year, the applicant is now proposing to phase the school in two years instead of the three years: 9th 10th and 11th grade in year one (Fall of 2013) and 12th grade in year two (Fall of 2014). This new phased concept triggers a Traffic Movement Permit (TMP), since the project will be generating more than 100 trips. On November 16, 2012, Baxter Academy received final approval from the Commission. The School is the first public charter school focusing on science, technology, engineering and mathematics.

The City has the delegated review for the Traffic Movement Permit and the Board will review the submitted Level II site plan for the change of use that is less than 20,000 square feet.

Applicant Name Dan Labrie, Rufus Deering Lumber Company
Consultants Steve Sawyer and Jim Seymour, PE Sebago Technics

II. PROJECT DATA

Existing Zoning:	Urban Commercial Business B-5b
Existing Use:	Rufus Lumber Milling Display Store with Offices, Labs, and Dance Studio
Proposed Use:	Institutional (Charter School)

Existing number of lots:	One
Proposed number of lots:	One
Parcel Size:	0.22 acre (9.583 s.f.)
Impervious Surface Area:	
Existing:	9,715 sq. ft.
Proposed:	Zero
Net Change:	Zero
Building Area:	7,540 sq. ft.
Parking Spaces:	
Existing:	12
Proposed:	Applicant is looking into leasing parking spaces; properties in the B-5b zone and historic districts are exempt from providing off-street parking requirements
Bicycle Parking Spaces:	
Existing:	Zero
Proposed:	5
Estimated Cost of Project:	\$185,000
Uses in Vicinity:	Commercial; restaurants; school/research; lumber yard

III. EXISTING CONDITIONS

The 7,540 square foot office building at the northern corner of York and Maple Streets is owned by Rufus Deering, but is not part of the commercial lumber company and lumber yard operation. Surface parking and Portland Pie Company are located across York Street and surface parking is across Maple Street. The JB Brown proposal for a hotel and other uses, which is currently under review, is proposed for the corner of Maple and Commercial Streets. The lumber yard surrounds the building to the east and south.

There is an existing curb cut and three street trees along the building frontage on Maple Street. There are no sidewalks on the lower portion of Maple Street or across the street on Maple Street. There are two street trees and no



Figure 2- View of York Street



Figure 1- View of Proposed Site

curb cuts on the York Street frontage. The building is currently developed with the following land uses:

- An exercise studio;
- A Buddhist Center;
- Administrative offices;
- Global Biotech;
- Deering Lumber's Kitchen and Millwork Showroom

Once the conversion is complete, the building will house the Baxter Academy for Technology and Science Charter School as the primary tenant. According to the applicant, either the Global Biotech or the Deering Lumber's Kitchen will remain in the basement.

IV. PROPOSED DEVELOPMENT

Rufus Deering Lumber Company is proposing to renovate and convert their existing building at 54 York Street (the corner of York and Maple Streets) to house the new Baxter Academy for Technology and Science Charter High School. The lot is fully developed with an existing 7,540 square foot, three-story building. The current uses consist of the Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses of offices, labs, and a dance studio.



The Baxter Academy is proposed as a public charter high school focusing in the fields of mathematics, technology, and science. The school's proposal is to convert the entire building into a full 4-year high school over the next 2 years. The total enrollment is planned for approximately 320 students by 2014. In the opening year which is September of 2013, an estimated 160 freshmen and sophomore year students are projected to enroll. In the second year, grades 11 and 12 will be provided. It is projected that half of the total enrollment will come from Portland, South Portland and Westbrook, with the remaining coming from communities outside of the urban core. School will operations will be from 8:00 AM to 2:00 PM, resulting in peak traffic generation associated with the school being between 7-8:00 AM and 2-3:00 PM.

V. PUBLIC COMMENT

Since the application is a Level II review, a neighborhood meeting is not required. As of the date of writing this report, no public comments have been submitted.

VI. RIGHT, TITLE AND INTEREST AND FINANCIAL/TECHNICAL CAPACITY

a. The owner of the property is Rufus Deering Company. The applicant has provided a copy of a warranty deed, recorded at the Cumberland County Registry of Deeds (Book 14115 Page 13), which demonstrates their right, title and interest in the property.

b. The estimated cost of the development is \$185,000. The applicant has not submitted a financial letter as demonstration of their financial and technical capacity to complete the proposed development.

VII. ZONING ASSESSMENT

A. ZONING REVIEW

Following are the review comments submitted by Marge Schmuckal, Zoning Administrator:

I have reviewed the most current submission. The applicant wants to change the use from offices to a new school (a public charter high school). The property is located in a B-5b zone which allows the proposed use under,

"Public or private schools of any type". There is no proposed addition to the building with the exception of an ADA ramp into the City's ROW along York Street. All changes would be interior to the existing building. There are two floors and a basement.

The B-5b zone has no minimum setbacks required. The 10' maximum setback along street lines is currently being met. There are no changes to change this existing condition. The maximum building height of 65' is more than being met with the two story and daylight basement in the existing building. Section 14-332.1(g) exempts the B-5 & B-5b zones from providing off-street parking requirements in the zone.

All HVAC equipment shall meet the sound level requirements listed under 14-230.5(b). Separate permits are required through Inspection Services for HVAC systems. Such application(s) shall provide HVAC dBA equipment levels. Separate permits are required for any new exterior signs. Signs are also subject to Historic review before issuance.

B. SITE PLAN SUBMISSION REQUIREMENTS (Section 14-527)

The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and applicable regulations. Staff comments are listed below.

C. SITE PLAN STANDARDS (Section 14-526)

The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and applicable regulations. Staff comments are listed below.

1. Transportation Standards

The proposed charter school will generate approximately 154 trips and therefore will need a Traffic Movement Permit (TMP). The applicant has a scoping meeting on October 10, 2012 with Tom Errico, Consultant Traffic Engineer. The City has delegated review of the TMP and Mr. Errico is in the process of reviewing and will make final recommendations to the Board at the public hearing.

a. Impact on Surrounding Street Systems/ Access and Circulation

The drop off zones for the school will be on Maple Street. The route for school buses will be arriving from York Street onto Maple Street and load/unload curbside on the west side of the street in a designated School Drop-Off Zone. The buses will depart the school using Maple Street to Commercial Street. The route for arriving parents that will drop off their students will be directed to enter Maple Street from Commercial Street. The parents will then continue to York Street where they would make a left onto York Street in order to utilize the parking spaces for a school drop-off zone on the north side of York Street (opposite the front door of the school). The drop off location is proposed on the north side on York rather than the south side, so that traffic traveling east on York Street from the Casco Bay Bridge is not blocked.

Mr. Errico has reviewed the School Drop-Off Zones and offers the following preliminary comments:

- i. The project is proposing parent drop-off activity to occur on the north side of York Street, just west of Maple Street. I'm concerned about whether parents will drop off students in this area (particularly those originating from Casco Bay Bridge and points from the west). Additionally, the York Street crosswalk is not likely to be used by students being dropped off, given the indirectness of walking to the crosswalk. It is unclear how much curb space will be designated as school drop-off.
- ii. While Maple Street is a relatively low volume street, plans for a nearby hotel/restaurant will increase traffic volume and activity. Details on specific bus loading/unloading activity shall be provided including curb space requirements, flashing light restrictions and how all street activities will be accommodated within the curb-to-curb width of Maple Street.

- iii. The proposed “5 MIN. SCHOOL DROP-OFF ZONE” parking regulations will need to be coordinated with John Peverada. My initial conversation with John is that this is workable, although specific regulations and time limits need to be evaluated. It should be noted that some parking regulation changes may require approval by the City Council for a parking schedule revision. If required, the applicant will be responsible for providing materials in support of the parking schedule change.
- iv. It is unclear what is being proposed along York Street in front of the subject property. Prior site plans have indicated that handicapped parking spaces will be provided with a sidewalk ramp connection to the street. Previous comments by the City have been in opposition to the provision of handicapped parking spaces on the street.

The school intends to have a staff person overseeing the parent drop off operation to keep vehicles orderly on York Street. Another staff person will be acting as a crossing guard at the intersection of Maple and York Street where the students will be directed to the front entrance of the school.

The applicant states that some of the students, as well as teachers, will be walking and biking to the site. There is a sidewalk only along the York Street frontage of the building, but it does not extend between this site to the High Street intersection. There is a sidewalk on Maple Street only along the building frontage. All sidewalks are in brick.

According to the traffic study, the intersection of Commercial and High Streets is a high crash location. Tom Errico states that the “City has plans for implementing improvements at this location and has been requesting monetary contributions for the implementation of improvements. I will provide information on the contribution amount in the future”.

b. Parking

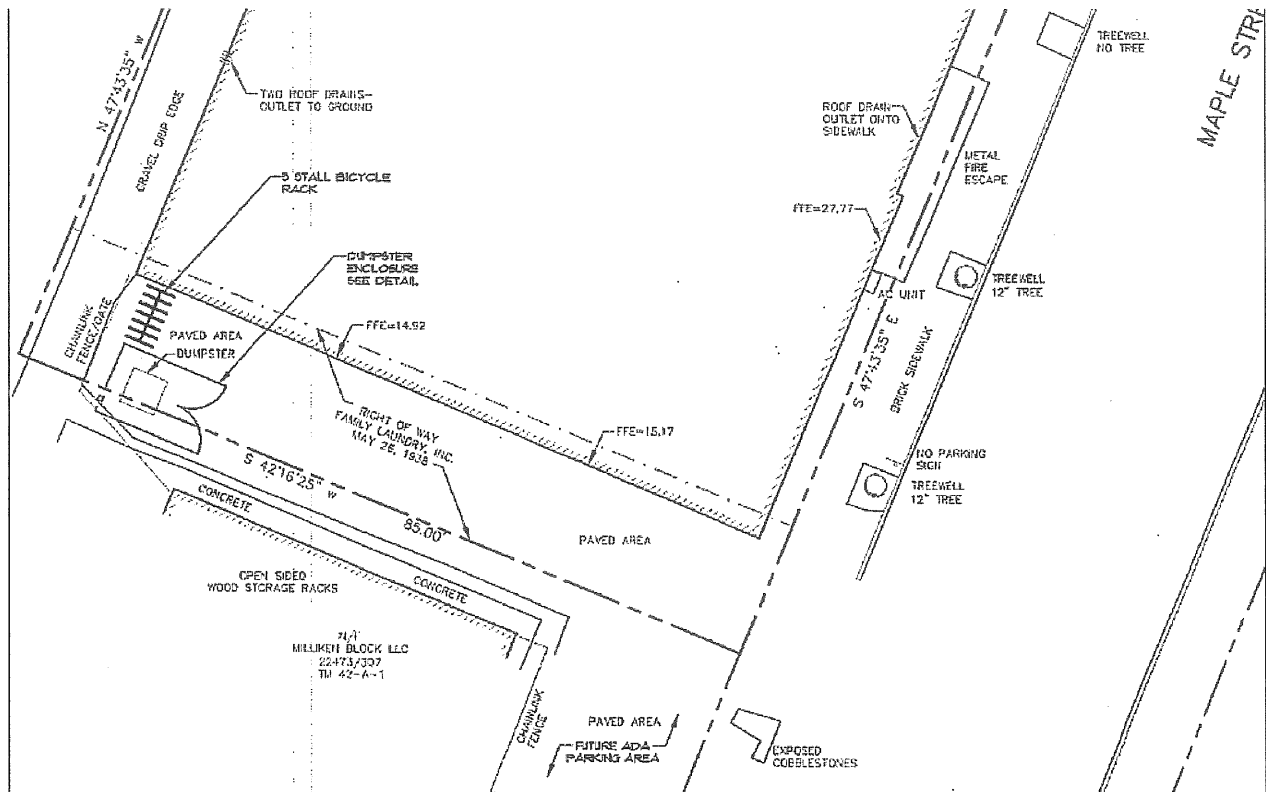
According to the applicant, staff members who elect to drive their own vehicles to school will be parking in the Gulf of Maine parking lot off Commercial Street or the parking garage at the Holiday Inn By-The-Bay and will walk to school via existing sidewalks. The gravel area at the Gulf of Maine is not an approved parking lot and the Planning Staff have met with the facilities manager for the site to discuss the required site plan process for a proposed parking lot. The applicant will need submit more information on exactly where the parking will be provided.

The Consultant Traffic Engineer recommends:

- i. The applicant shall provide information on the location and the number of off-site parking spaces to be allocated to the school. The traffic study notes that 20 parking spaces will be initially leased for the project, but I would suggest that full-build parking plans be a consideration, for planning purposes.
- ii. I generally agree with the trip generation estimate. I would note that the estimate assumes that students will not be driving their own vehicles. It is likely that some students will drive, but in my professional opinion this added traffic would not be expected to alter the conclusions of the traffic analysis. I would note that the school will not be providing parking for students and therefore it is expected that students will seek on-street parking spaces where time limit parking regulation do not exist.

c. *Bicycle Parking*

Currently there is no bicycle parking on site. The applicant originally proposed to install five bicycle parking spaces at the back of the building in the alleyway. The traffic study identifies that it will be on Maple Street. Mr. Errico recommends that the “specific location of bicycle racks shall be identified....Given the type of activity expected, I would suggest additional bicycle spaces be allocated”. The applicant is required to install four bicycle parking for the twenty (20) parking spaces being proposed.



d. *Transportation Demand Management (TDM)*

While the scoping session has occurred, the TDM has not been submitted yet and will need to be reviewed as part of the final plans. The review of the TDM will be conducted by Tom Errico, Consulting Traffic Engineer. He recommends that “an annual TDM/Traffic Management Plan be provided to the City for review and comment”.

2. Environmental Quality Standards

a. *Landscaping*

A landscaping plans needs to be submitted for review. Street trees are a requirement of site plan review and need to be proposed as part of the project. The site is a reuse of an existing building and any opportunity to add landscaping should be considered. Jeff Tarling, City Arborist recommends the replanting of one street tree on Maple Street to replace the dead tree and recommends that the tree type be either 'Armstrong' Red Maple or other upright species such as upright Pin Oak, or English Oak.

b. *Water Quality, Storm Water Management and Erosion Control*

The project consists of converting the existing Rufus Deering Lumber Milling Display Store into the Baxter Academy for Technology and Science; no building additions are anticipated. The existing site is completely impervious; therefore, the project will not create any new impervious areas.

3. Public Infrastructure and Community Safety Standards

a. *Sidewalk*

There are existing brick sidewalks along the Maple Street Maple Street frontage and along the York Street frontage. There is no sidewalk or granite curbing along York Street from the site to the intersection of High Street. There is sidewalk along the opposite side of York Street of various materials and conditions. High Street has bituminous sidewalk and curbing along both sides of the street.

The Department of Public Services has made a Quality Community Program Grant application to the MaineDOT this past August to install a sidewalk at this location. The decision of the application is not expected until Spring 2013 and if it is funded, the federal portion of the funding becomes available in Oct 2013 at the earliest.

The total local cash match for the project is \$22,000. The application includes costs of the sidewalk, improvement to ADA accessibility at the intersection of York and High Streets, including curb ramps and pedestrian signals.



YORK STREET SIDEWALK TE PROJECT

In discussion with the applicant, it has been stated that students and staff will be walking to the site. Therefore, the City staff and the consulting traffic engineer feel this segment of the sidewalk will need to be constructed. Staff recommends that applicant make the contribution of the local match of \$22,000. If the City does not get the grant, it will be the responsibility of the applicant to construct the sidewalk.

Mr. Errico has reviewed the site and the submitted plans and recommends:

- i. In my professional opinion, the provision of a sidewalk along the south side of York Street between High Street and the school is critically important for this project as students are expected to walk to school. The City has submitted a grant application for the noted sidewalk, but it is unknown if funding will be secured. A similar grant application was submitted previously and the City was not successful. If funding is obtained, it is unlikely a sidewalk will be available by the time school opens in September 2013. I strongly suggest that a sidewalk be installed before the school opens, given anticipated pedestrian activity.
- ii. The crosswalk on York Street shall be upgraded to “block” style painted markings. The sidewalk ramps may need to be upgraded to comply with City/ADA standards. Detailed recommendations on the sidewalk will be provided by DPS.

b. *Public Safety and Fire Prevention*

Captain Chris Pirone of Fire Department has reviewed the submitted site plans and finds the proposed site plan to be acceptable.

c. *Availability and Adequate Capacity of Public Utilities*

The current building has public utilities that are available at the site. The school does not intend on having a kitchen and lunch will be outsourced to a local vender.

4. Site Design Standards

a. *Snow and Ice Loading*

Snow and Ice Loading area must be shown on the site plan. If snow is to be removed, a detail plan will need to be submitted as how it will be handles.

b. *Exterior Lighting*

This site is located in Old Port Historic District. Any exterior improvements and alteration including exterior lighting and signs will need to be reviewed by the Historic Preservation. A catalogue cut and specifications of proposed lighting fixtures will need to be provided. The applicant has met with Deb Andrews, Historic Preservation Program Manager, and will be submitting plans for review.

c. *Signage and Wayfinding*

A traffic signage and wayfinding plan will need to be submitted for review. Any building signage will require Historic Preservation review.

d. *Zoning Related Design Standards*

The proposal is a reuse of an existing building. There are no additions or alterations being proposed to the building.

VIII. NEXT STEPS

Next steps include:

- Address comments of the Transportation Engineering Reviewer Tom Errico
- Submit further information on the outstanding items mentioned in this memo
- Address Planning Board comments
- Submit final application

ATTACHMENTS:

PLANNING BOARD MEMO ATTACHMENTS

1. Marge Schmuckal Review 06.18.2012
2. Tom Errico Review 11.20.2012
3. David Sensus Review 06.12.2012
4. David Margolis-Pineo Review 07.12.2012
5. Jeff Tarling Review 07.12.2012

APPLICANT'S SUBMITTAL

- A. Written Description of Project
- B. Application
- C. Description of Charter School
- D. Photos of Existing Site

- E. Development Description
- F. Surveyor's Report
- G. Warranty Deed
- H. Traffic Movement Permit, dated 09.28.2012
- I. Amendment to Section 7 of the Traffic Movement Permit, 10.23.2012

PLANS

- Plan 1 Cover Sheet
- Plan 2 Existing Conditions Plan
- Plan 3 Site Plan
- Plan 4 Details
- Plan 5 Rendering of Building from York Street

Shukria Wiar - 54 York St - Baxter Academy

From: Marge Schmuckal
To: Shukria Wiar
Date: 6/18/2012 3:20 PM
Subject: 54 York St - Baxter Academy

54 York Street - 042-A-7 & 8 - B-5b Zone & Historic Overlay
2012-519 Level II site plan review
June 18, 2012

I have reviewed the most current submission. The applicant wants to change the use from offices to a new school (a public charter high school). The property is located in a B-5b zone which allows the proposed use under, "Public or private schools of any type". There is no proposed addition to the building with the exception of an ADA ramp into the City's ROW along York Street. All changes would be interior to the existing building. There are two floors and a basement.

The B-5b zone has no minimum setbacks required. The 10' maximum setback along street lines is currently being met. There are no changes to change this existing condition.

The maximum building height of 65' is more than being met with the two story and daylight basement in the existing building.

Section 14-332.1(g) exempts the B-5 & B-5b zones from providing off-street parking requirements in the zone.

All HVAC equipment shall meet the sound level requirements listed under 14-230.5(b). Separate permits are required through Inspection Services for HVAC systems. Such application(s) shall provide HVAC dBA equipment levels.

Separate permits are required for any new exterior signs. Signs are also subject to Historic review before issuance.

Marge Schmuckal
Zoning Administrator

Shukria Wiar - 54 York Street - Baxter School

From: Tom Errico <thomas.errico@tylin.com>
To: Shukria Wiar <SHUKRIAW@portlandmaine.gov>
Date: 11/20/2012 5:54 PM
Subject: 54 York Street - Baxter School
CC: "Margolis-Pineo, David" <DMP@portlandmaine.gov>, Jeff Tarling <JST@portl...>

Shukria – I have reviewed the traffic study conducted for the above noted project and I have the following preliminary comments.

- The project is proposing parent drop-off activity to occur on the north side of York Street, just west of Maple Street. I'm concerned about whether parents will drop off students in this area (particularly those originating from Casco Bay Bridge and points from the west). Additionally, the York Street crosswalk is not likely to be used by students being dropped off, given the indirectness of walking to the crosswalk. It is unclear how much curb space will be designated as school drop-off.
- I generally agree with the trip generation estimate. I would note that the estimate assumes that students will not be driving their own vehicles. It is likely that some students will drive, but in my professional opinion this added traffic would not be expected to alter the conclusions of the traffic analysis. I would note that the school will not be providing parking for students and therefore it is expected that students will seek on-street parking spaces where time limit parking regulation do not exist.
- A TDM study shall be prepared for the project. I will review the details of the study when provided. I would note that it will be my recommendation that an annual TDM/Traffic Management Plan be provided to the City for review and comment
- The applicant shall provide information on the location and the number of off-site parking spaces to be allocated to the school. The traffic study notes that 20 parking spaces will be initially leased for the project, but I would suggest that full-build parking plans be a consideration, for planning purposes.
- In my professional opinion, the provision of a sidewalk along the south side of York Street between High Street and the school is critically important for this project as students are expected to walk to school. The City has submitted a grant application for the noted sidewalk, but it is unknown if funding will be secured. A similar grant application was submitted previously and the City was not successful. If funding is obtained, it is unlikely a sidewalk will be available by the time school opens in September 2013. I strongly suggest that a sidewalk be installed before the school opens, given anticipated pedestrian activity.
- While Maple Street is a relatively low volume street, plans for a nearby hotel/restaurant will increase traffic volume and activity. Details on specific bus loading/unloading activity shall be provided including curb space requirements, flashing light restrictions and how all street activities will be accommodated within the curb-to-curb width of Maple Street.
- The crosswalk on York Street shall be upgraded to "block" style painted markings. The sidewalk ramps may need to be upgraded to comply with City/ADA standards. Detailed recommendations on the sidewalk will be provided by DPS.
- The specific location of bicycle racks shall be identified (the study notes they will be located on Maple Street). The original application indicated 5 bicycle parking locations to be provided. Given the type of activity expected, I would suggest additional bicycle spaces be allocated.
- The proposed "5 MIN. SCHOOL DROP-OFF ZONE" parking regulations will need to be coordinated with John Peverada. My initial conversation with John is that this is workable, although specific regulations and time limits need to be evaluated. It should be noted that some parking regulation changes may require approval by the City Council for a parking schedule revision. If required, the applicant will be responsible for providing materials in support of the parking schedule change.
- It is unclear what is being proposed along York Street in front of the subject property. Prior site plans have indicated that handicapped parking spaces will be provided with a sidewalk ramp connection to the street. Previous comments by the City have been in opposition to the provision of handicapped parking spaces on the

street.

- As noted in the traffic study, the intersection of Commercial Street and High Street is a High Crash Location. The City has plans for implementing improvements at this location and has been requesting monetary contributions for the implementation of improvements. I will provide information on the contribution amount in the future.

If you have any questions, please contact me.

Best regards,

Thomas A. Errico, PE
Senior Associate
Traffic Engineering Director

TYLIN INTERNATIONAL

12 Northbrook Drive
Falmouth, ME 04105
207.347.4354 direct
207.400.0719 mobile
207.781.4753 fax

thomas.errico@tylin.com
Visit us online at www.tylin.com

"One Vision, One Company"
Please consider the environment before printing.

**COMMITMENT & INTEGRITY
DRIVE RESULTS**41 Hutchins Drive
Portland, Maine 04102
www.woodardcurran.comT 800.426.4262
T 207.774.2112
F 207.774.6635**MEMORANDUM**

TO: Shukria Wiar, Planner
FROM: David Senus, P.E. & Ashley Auger, E.I.T.
DATE: June 12, 2012
RE: Baxter Academy, Preliminary Level II Site Plan Application

Woodard & Curran has reviewed the Preliminary Level II Site Plan Application for Baxter Academy located at 54 York Street in Portland, Maine. The project consists of converting the existing Rufus Deering Lumber Milling Display Store into the Baxter Academy for Technology and Science; no building additions are anticipated. The existing site is completely impervious; therefore, the project will not create any new impervious areas.

Documents Provided By Applicant

- Site Plan Application and attachments dated June 5, 2012, prepared by Sebago Technics, on behalf of Rufus Deering Lumber Co.
- Engineering Plans, Sheets 1-4, revised June 5, 2012, prepared by Sebago Technics, on behalf of Rufus Deering Lumber Co.

Comments

- 1) The Brick Sidewalk Detail and Vertical Granite Curb Detail indicate an esplanade, which doesn't appear on the site plan. Please clarify.
- 2) The Brick Sidewalk Detail should be modified to reflect the gravel depth requirements of Figure I-10 of the City of Portland Technical Manual.

July 12, 2012

TO: Barbara Barhydt
Shukria Wiar
FROM: David Margolis-Pineo Dept. of Public Services
RE: Review Comments: Proposed Baxter School – 54 York Street

The Department of Public Services has the following comments on the above referenced project.

1. The applicant is requested to rebuild the sidewalk ramp at the corner on York and Maple Streets to add an ADA handicap warning panel. Please add note to the plans that states: “The proposed sidewalk ADA handicap ramp and crosswalk layout at the corner of York and Maple streets shall be reviewed and approved prior to construction by Bruce Hyman (400-9243) Portland’s Bike/Ped Coordinator.”

This department has no further comments at this time.

Applicant's Submittal



June 5, 2012
07383

Barbara Barhydt
City of Portland Planning Division
City Hall 4th Floor
389 Congress St.
Portland, ME 04101

**Minor Site Plan Level II Application Submittal – Change of Use Permit
Proposed Baxter Academy for Technology and Science –Charter School
54 York Street, Portland, ME**

Dear Ms. Barhydt:

Enclosed please find a copy of the Level II Site Plan Application and associated site drawings that we have prepared for the Baxter Academy for Technology and Science on property owned by Rufus Deering Lumber Company at 54 York Street, Portland, Maine (located on the corner of York Street and Maple Street). The parcel proposed for development is the existing Rufus Deering Lumber Milling display store with other mixed uses for offices, labs, and dance studios. The property consists of a 0.22 acre lot fully developed with an existing 7,540 square foot (SF) footprint for a three story building zoned in the B-5b (Urban Commercial Business Zone). The proposed use will entail a floor area conversion of over 10,000 SF change of use and will require a Level II Site Plan review by the Planning Staff. It is also within the Old Port Historical District.

The overall improvement of the site will not add any additional impervious areas, as the site now is completely impervious. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. Their vision is to convert the entire building into a full 4 year high school over the next 3 to 4 years. Due to the Baxter Academy's plan to convert the existing structure into a school, all the retrofitting will occur with the interior spacing, and no building additions are anticipated to the exterior. Therefore, items such as stormwater and existing utilities services will not be altered as a result of the change of use. However, the need for Americans with Disabilities Act (ADA) access into the building will require a handicap ramp access to be extended into the City's right-of-way (ROW) at their York Street entrance, along with street space to be dedicated for handicap parking and access. This work will require rebuilding a small portion of the sidewalk and curbing along a section of York Street.

Rufus Deering Lumber Company will provide the lease for Baxter Academy and will be incorporating all necessary interior building improvements for the Charter School. We anticipate that the Baxter Academy will be a long-term lease as the Charter School envisions an enrollment of 320 students by 2014. However, in the first year the expectation is an enrollment of nearly 160 students. Our primary issue on the site has to deal with the transportation of students and parking arrangements for faculty and administrators.

Based on the City's parking requirements, school parking requirements are based on age. One regulation is based on parking for schools with students 15 years of age and under and one deal with students 16 years and older. This facility will be handling students in each grouping equally given the high school ages. We calculated that 42 spaces would be required for the leased space in year one, and that the

requirements will increase slightly with complete full building lease and build-out by 2014. Baxter Academy has a contract for parking spaces with JB Brown for their parking lot located opposite of the school off Maple Street and would expand that as the school expands in the future.

In addition, the Director, John Jacques, has indicated that the Charter School intends to contract for bus transportation for three full-size buses for student transportation in year one, which is the equivalent of upwards of 162 students. Other students could take other modes of public transportation, or be dropped off by parents. Student parking will not be encouraged or provide by the school. Maple Street has been designated as a location for bus delivery and pick-up of students. Busses are intended to be turning down Maple Street off of York Street such that students will be loaded/unloaded from the existing sidewalk adjacent to the proposed school building. Although the school has a requirement for several spaces, the school does not feel that they will have a need for the required spaces, but could retain JB Brown's lot on an event basis for special activities or conferences which may be held at the school above its base lease for faculty and administrators.

We are proposing that the City allow us to reconstruct a portion of the sidewalk along York Street, as well as dedicate two on-street parking spaces to be allowed for ADA compliant parking to meet our current demand. Both will be constructed to allow van access and direct ramps to the sidewalk and access into the York Street main entrance. In the final year of building renovation, when the bottom floor of the existing building (currently the Deering Lumber Milling Store) is occupied by the school, a second location off Maple Street could be dedicated for additional handicap spaces.

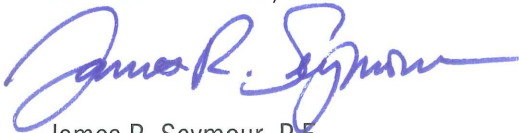
Rufus Deering Lumber Company is very anxious to start construction in early July. We are fully aware of the allocated review time, but would like to be proactive with your staff and be responsive to facilitate approvals in coordination with Portland Site Plan approval. We will be providing Architectural Plans and Site Improvements Plans to address ADA compliance issues for ramps and handicap parking. However, based on the ownerships desire to schedule for a September opening, we feel the Site Plan process must begin immediately.

In the interim, we are available at your request to hold any type of application review meeting or to meet the City's review staff to present the project on an informal basis. We feel given site design parameters that this would be beneficial for both the City Planners and the Baxter Academy design team, especially with parking and transportation needs.

If you have any questions please feel free to contact me at our office (207-200-2083). We look to working with the Planning Division Staff and City Engineers to facilitate the opening of this new opportunity for a charter school in the Downtown Portland area.

Sincerely,

SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager

JRS:jrs/kn
Enc.

cc: Dan Labrie, President, Rufus Deering Lumber Company
John Jacques, Executive Director, Baxter Academy for Technology & Science

Effective August 18, 2010



Level II – Preliminary and Final Site Plans Development Review Application Portland, Maine

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Level II: Preliminary or Final Site Plan. Please note that Portland has delegated review from the State of Maine for reviews under the Site Location of Development Act, Chapter 500 Stormwater Permits, and Traffic Movement Permits. General information pertaining to the thresholds of review, public noticing procedures and the fee structure is contained in the Notice to Developer's Packet.

Level II: Site Plan Development includes:

- New structures with a total floor area of less than 10,000 sf except in Industrial Zones.
- New structures with a total floor area of less than 20,000 sf in Industrial Zones.
- Any new temporary or permanent parking area, paving of an unpaved surface parking area, or creation of other impervious surface area greater than 7,500 sf.
- Building addition(s) with a total floor area of less than 10,000 sf (cumulatively within a 3 year period) except in Industrial Zones.
- Building addition(s) with a total floor area of less than 20,000 sf in Industrial Zones.
- Park improvements: New structures or buildings with a total floor area of less than 10,000 sf, facilities encompassing an area of greater than 7,500 sf and less than 20,000 sf (excludes rehabilitation or replacement of existing facilities).
- New piers, docks, wharves, bridges, retaining walls, and other structures within the Shoreland Zone.
- Land disturbance between 1 and 3 acres (includes stripping, grading, grubbing, filling or excavation).
- A change in the use of a total floor area between 10,000 and 20,000 sf in any existing building (cumulatively within a 3 year period).
- Construction of a lodging house, bed and breakfast facility, emergency shelter or special needs independent living unit.
- Signage subject to approval pursuant to Section 14-526 (d) 8.a. (IV) of the Land Use Code.
- Any new major or minor auto service station with less than 10,000 sf of building area that is outside the B-2 or B-5 zones.
- The creation of day care or home babysitting facilities to serve more than 12 children in a residential zone (not permitted as a home occupation under section 14-410) in any principal structure that has not been used as a residence within the 5 years preceding the application.

The Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division
Fourth Floor, City Hall
389 Congress Street
(207) 874-8721 or 874-8719

Office Hours
Monday thru Friday
8:00 a.m. – 4:30 p.m.

PROJECT NAME: BAXTER ACADEMY for TECHNOLOGY & SCIENCE

PROPOSED DEVELOPMENT ADDRESS:

54 YORK ST.

PROJECT DESCRIPTION:

INTERIOR CHANGE OF USE FROM OFFICES TO
CHARTER (PUBLIC) SCHOOL > 10,000 SF FLOOR SPACE.

CHART/BLOCK/LOT: MAP 42 - BLOCK A
LOTS 7 & 8

PRELIMINARY PLAN 6/5/12 (date)
FINAL PLAN _____ (date)

CONTACT INFORMATION:

	Applicant's Contact for electronic plans Name: <u>JAMES SEYMOUR P.E.</u> e-mail: <u>Jseymour@sebagotechnics.com</u> work #: <u>200-2083</u>
Applicant – must be owner, Lessee or Buyer Name: <u>DAN LABRIE</u> Business Name, if applicable: <u>RUFUS DEERING LUMBER Co.</u> Address: <u>383 COMMERCIAL ST.</u> City/State: <u>PORTLAND, ME</u> Zip Code: <u>04101</u>	Applicant Contact Information Work # <u>772-6505</u> Home# <u>767-1055</u> Cell # <u>838-0444</u> Fax# <u>772-6981</u> e-mail: <u>Dlabrie@rufusdeering.com</u>
Owner – (if different from Applicant) Name: Address: <u>SAME</u> City/State: Zip Code:	Owner Contact Information Work # Home# Cell # Fax# e-mail:
Agent/ Representative Name: <u>N/A</u> Address: City/State: Zip Code:	Agent/Representative Contact information Work # Cell # e-mail:
Billing Information Name: <u>APPLICANT.</u> Address: City/State: Zip Code:	Billing Information Work # Cell # Fax# e-mail:

Engineer SEBAGO TECHNICS INC Name: JAMES SEYMOUR, P.E. Address: 75 JOHN ROBERTS ROAD, SUITE 1A City/State: So. PORTLAND, ME Zip Code: 04106	Engineer Contact Information Work # 200-2083 Cell # 632-1199 Fax# 856-2256 e-mail: jseymour@sebagotechnics.com
Surveyor SEBAGO TECHNICS INC Name: WILLIAM SHIPPEN PLS Address: 75 JOHN ROBERTS RD, SUITE 1A City/State: So. PORTLAND, ME Zip Code: 04106	Surveyor Contact Information Work # 200-2084 Cell # Fax# 856-2256 e-mail: wshippen@sebagotechnics.com
Architect RICK RENNER ARCHITECTURE Name: CHARLES YOUNG Address: 35 Pleasant St. City/State: PORTLAND, ME Zip Code: 04101	Architect Contact Information Work # 773-9699 x 203 Cell # Fax# e-mail: cyoung@rennerarchitects
Attorney PEARCE & DOW Name: MIKE PEARCE Address: 50 MONUMENT SQ. City/State: PORTLAND, ME Zip Code: 04101	Attorney Contact Information Work # 822-9900 Cell # Fax# 822-9901 e-mail:

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

Level II Development (check applicable reviews) <input checked="" type="checkbox"/> Less than 10,000 sq. ft. (\$400.00) <input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)	Fees Paid (office use) — —	Other Reviews (check applicable reviews) <input type="checkbox"/> Traffic Movement (\$1,000) <input type="checkbox"/> Stormwater Quality (\$250) <input type="checkbox"/> Section 14-403 Review (\$400 + \$25/lot) # of Lots ___ x \$25/lot = _____ <input type="checkbox"/> Other _____ <input checked="" type="checkbox"/> Change of Use <input type="checkbox"/> Flood Plain <input type="checkbox"/> Shoreland <input type="checkbox"/> Design Review <input type="checkbox"/> Housing Replacement <input type="checkbox"/> Historic Preservation	Fees Paid (office use) — —
The City invoices separately for the following: <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) Third party review is assessed separately.			
Plan Amendments (check applicable reviews) <input type="checkbox"/> Planning Staff Review (\$250) <input type="checkbox"/> Planning Board Review (\$500)	Fees Paid (office use) — —		

APPLICATION SUBMISSION

All site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Application submissions shall include one (1) paper packet with folded plans containing the following materials:

Numbers below changed- can't seem to highlight

1. One (1) full size site plans that must be folded.

One (1) set of all written materials or as follows, unless otherwise noted:

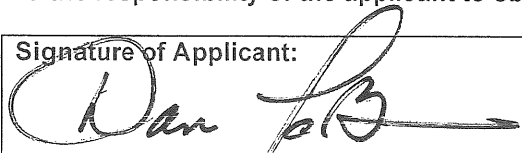

- a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
2. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet.
 3. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
 4. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
 5. One (1) set of plans reduced to 11 x 17.

Refer to the application checklist for a detailed list of submittal requirements.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level II Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: 	Date: 
--	---

PROJECT DATA

The following information is required where applicable, in order complete the application

Total Site Area	9715 sq. ft.
Proposed Total Disturbed Area of the Site	N/A sq. ft.
(If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)	
IMPERVIOUS SURFACE AREA	
• Proposed Total Paved Area	N/A sq. ft.
• Existing Total Impervious Area	9715 sq. ft.
• Proposed Total Impervious Area	N/A sq. ft.
• Proposed Impervious Net Change	0 sq. ft.
BUILDING AREA	
• Proposed Building Footprint	-NA- sq. ft.
• Proposed Building Footprint Net change	0 sq. ft.
• Existing Total Building Floor Area	sq. ft.
• Proposed Total Building Floor Area	0 sq. ft.
• Proposed Building Floor Area Net Change	0 sq. ft.
• New Building	(yes or <input checked="" type="radio"/> no)
ZONING	
• Existing	B5b
• Proposed, if applicable	SAME
LAND USE	
• Existing	MIXED USE -
• Proposed	OFFICES · RETAIL · LABS · ZUMBA CHARTER SCHOOL
RESIDENTIAL, IF APPLICABLE	
• Proposed Number of Affordable Housing Units	N/A
• Proposed Number of Residential Units to be Demolished	↓
• Existing Number of Residential Units	↓
• Proposed Number of Residential Units	↓
• Subdivision, Proposed Number of Lots	↓
PARKING SPACES	
• Existing Number of Parking Spaces	12
• Proposed Number of Parking Spaces	10 (LEASED)
• Number of Handicapped Parking Spaces	2 (YORK ST.)
• Proposed Total Parking Spaces	1
BICYCLE PARKING SPACES	
• Existing Number of Bicycle Parking Spaces	0
• Existing Number of Bicycle Parking Spaces	0
• Proposed Number of Bicycle Parking Spaces	5
• Total Bicycle Parking Spaces	5
ESTIMATED COST OF PROJECT	\$185,000.00

General Submittal Requirements – Preliminary Plan (Optional)

Level II Site Plan

Preliminary Plan Phase Check list (if elected by applicant)

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Completed application form
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Application fees
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written description of project
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Evidence of right, title and interest.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Copies of required State and/or Federal permits.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of proposed project's compliance with applicable zoning requirements.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written description of existing and proposed easements or other burdens.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written requests for waivers from individual site plan and/or technical standards, where applicable.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Traffic analysis (may be preliminary, in nature, during the preliminary plan phase).
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written summary of significant natural features located on the site.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written summary of project's consistency with related city master plans.
Applicant Checklist	Planner Checklist	Number of Copies	Site Plan Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>		Boundary Survey meeting the requirements of Section 13 of the City of Portland Technical Manual.
<input type="checkbox"/>	<input type="checkbox"/>	1	Preliminary Site Plan Including the following: (*information provided may be preliminary in nature during preliminary plan phase):
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed structures with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Location of adjacent streets and intersections and approximate location of structures on abutting properties.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Proposed site access and circulation.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Proposed grading and contours.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Preliminary landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Existing and proposed utilities (preliminary layout).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Preliminary infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, transit infrastructure, roadway improvements).
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Preliminary stormwater management and erosion control plan.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b) 1. of the Land Use Code).
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Proposed alterations to and protection measures for significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important

NA

natural features listed in Section 14-526 (b)1. of the Land Use Code).

▪ Existing and proposed easements or public or private rights of way.

General Submittal Requirements – Final Plan (Required)

Level II Site Plan

Final Plan Phase Check list (including items listed above in General Requirements for Preliminary Plan, if applicant did not elect to submit for a preliminary plan review)

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirement
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of financial and technical capacity.
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of utilities' capacity to serve the development.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of fire safety (referencing NFPA fire code and Section 3 of the City of Portland Technical Manual).
<input type="checkbox"/>	<input type="checkbox"/>	1	Construction management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Traffic Plan (if development will (1) generate 100 or more PCE or (2) generate 25 or more PCE and is located on an arterial, within 1/2 mile of a high crash location, and/or within ¼ mile of an intersection identified in a previous traffic study as a failing intersection).
<input checked="" type="checkbox"/> NA	<input type="checkbox"/>	1	Stormwater management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of solid waste generation and proposed management of solid waste.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of conformity with applicable design standards.
<input type="checkbox"/>	<input type="checkbox"/>	1	Manufacturer's verification that HVAC and manufacturing equipment meets applicable state and federal emissions requirements.

Final Plan Phase

<input type="checkbox"/>	<input type="checkbox"/>	1	Final Site Plan Including the following
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed structures on the site with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Location of adjacent streets and intersections and approximate location of structures on abutting properties.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed site access and circulation.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed grading and contours.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways. Proposed curb lines must be shown.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed loading and servicing areas, including applicable turning templates for delivery vehicles
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed snow storage areas or snow removal plan.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed trash and recycling facilities.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed utilities.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Location and details of proposed infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, public transit infrastructure, roadway improvements).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed septic system, if not connecting to municipal sewer. (Portland Waste Water Application included in this application)
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed finish floor elevation (FFE).

-
-
-
-
-
-
-
-
-
-
-
-
-
-

-
-
-
-
-
-
-
-
-
-
-
-
-
-

<ul style="list-style-type: none"> ▪ Exterior building elevation(s) (showing all 4 sides).
<ul style="list-style-type: none"> ▪ Proposed stormwater management and erosion controls.
<ul style="list-style-type: none"> ▪ Exterior lighting plan, including street lighting improvements..
<ul style="list-style-type: none"> ▪ Proposed signage.
<ul style="list-style-type: none"> ▪ Identification of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code). Wetlands must be delineated.
<ul style="list-style-type: none"> ▪ Proposed alterations to and protection measures for of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code).
<ul style="list-style-type: none"> ▪ Total area and limits of proposed land disturbance.
<ul style="list-style-type: none"> ▪ Soil type and location of test pits and borings.
<ul style="list-style-type: none"> ▪ Details of proposed pier rehabilitation (Shoreland areas only).
<ul style="list-style-type: none"> ▪ Existing and proposed easements or public or private rights of way.

Review Staff
City of Portland
Portland, Me 04101

Dear Staff Members:

Baxter Academy for Technology and Science
54 York Street

HISTORY

The building is located on the corner of York and Maple (see attached map) in Portland. Fifty-four York Street has had numerous uses over the years. Most recently approximately 20,000 sq. ft. was leased to Media Power. In April of 2010 Media Power needed more space and moved to a building on Foden Road in South Portland.

In mid-2011 John Jaques, future Executive Director of Baxter Academy, and Daniel LaBrie, Senior Vice President of Rufus Deering Lumber Company, began discussing a possible partnership by leasing 54 York Street to them for a charter school.

OBJECTIVE

The school aims to link students with local businesses, scientists, researchers and other professionals working in fields of science, technology, engineering and mathematics. The Portland location offers easy access to the Gulf of Maine Research Institute, Maine Medical Center, the University of Southern Maine and Southern Maine Community College.

The school hopes to get a charter from the state by July 1st and open in September. It hopes to be the first of its kind in Portland.

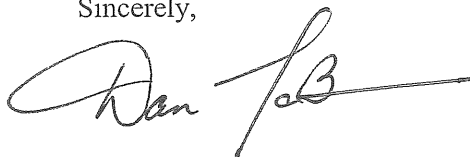
PROPOSAL

The fit up to prepare the building for a school consists of upgrading HVAC, Security system, Handicap Accessibility, new carpets and paint. The building as it stands today does not have an elevator. An exterior elevator will be installed on the southwest side of the building on a 15' easement.

CLOSING

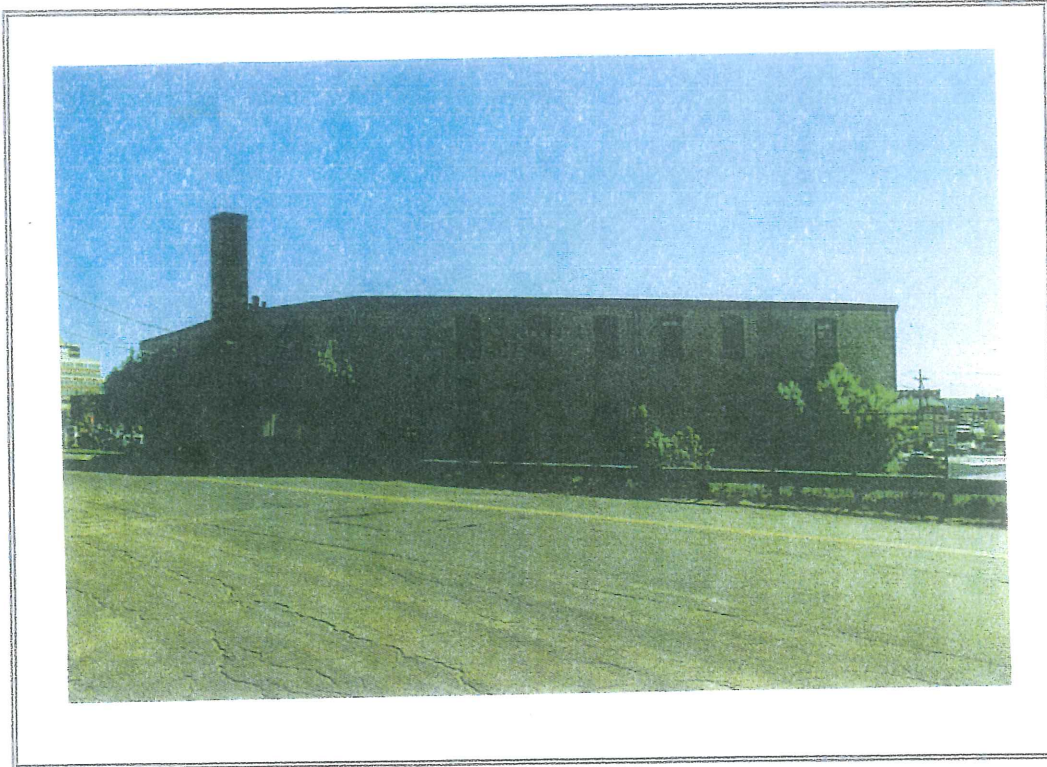
I believe this is a perfect location for a charter school. It is wonderful to be able to offer our youth an alternative education to the usual public school system. These charter schools have been very successful all over the country. Time is of the essence, I would ask that you move this process along and grant the permit in a timely manner.

Sincerely,

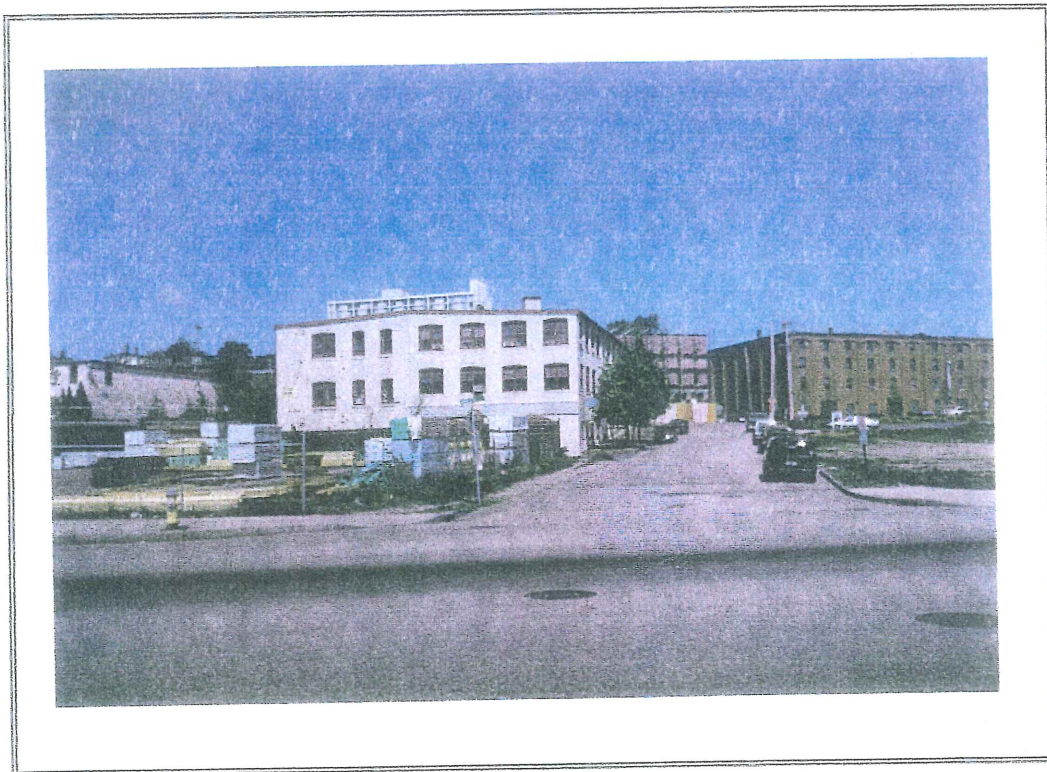


Daniel LaBrie
Senior Vice President

SUBJECT PHOTOGRAPHS

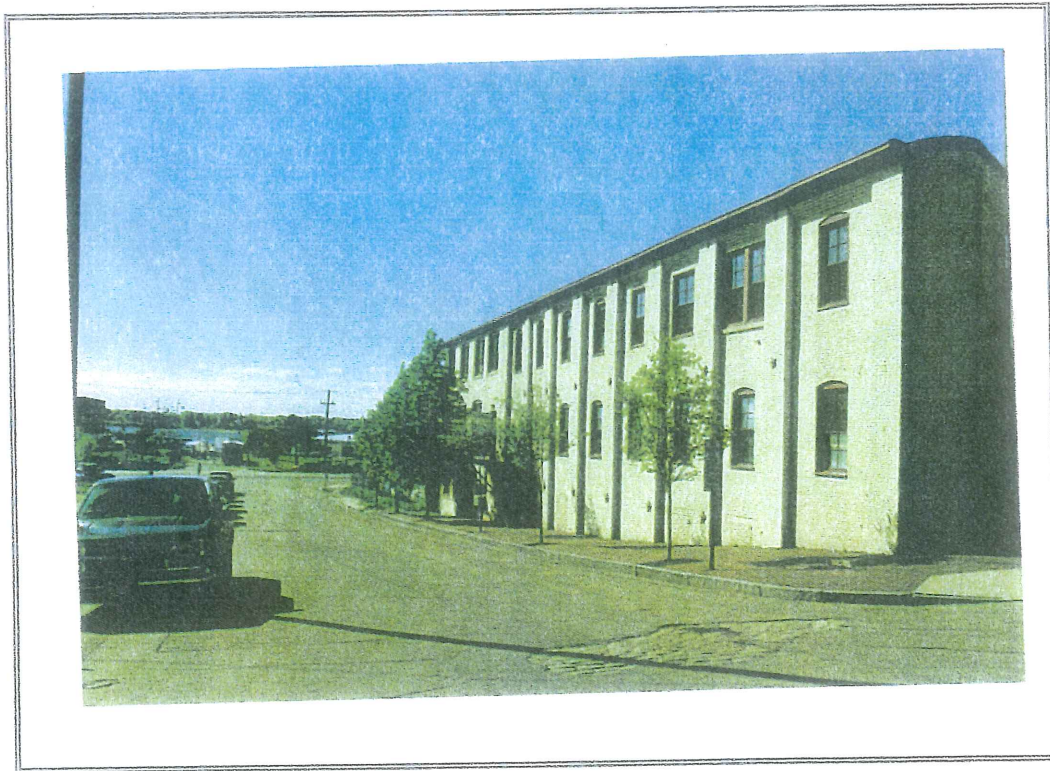


1) Easterly view of subject from York Street

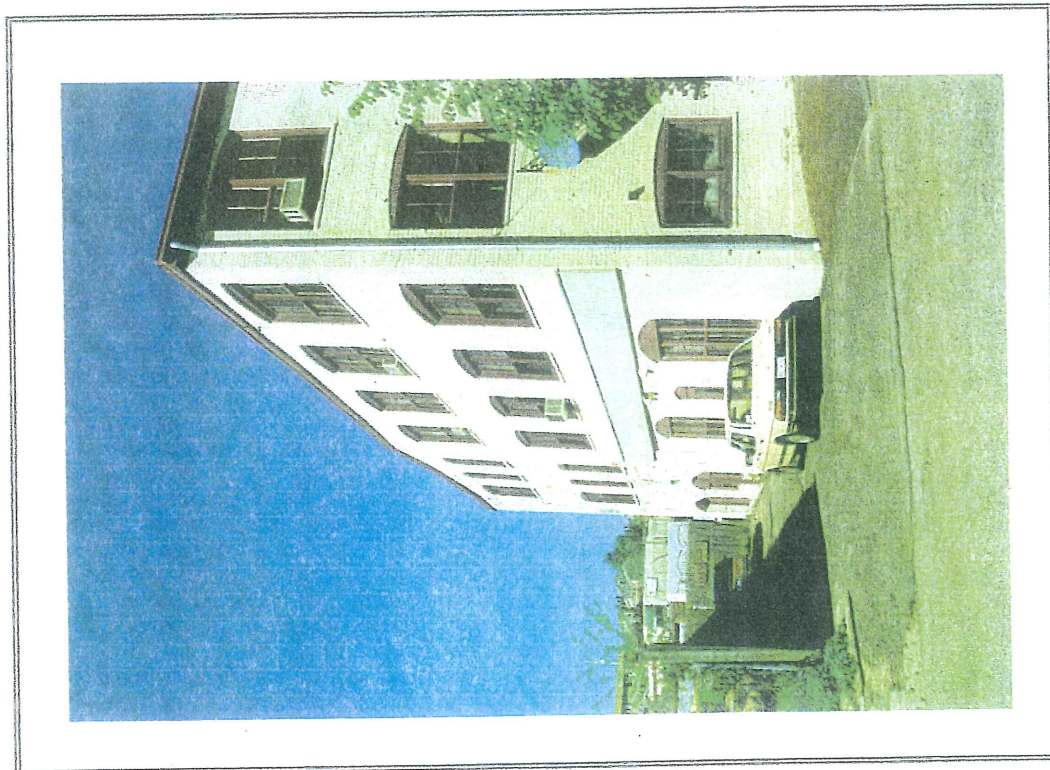


2) Westerly view of subject from Commercial Street, looking up Maple Street

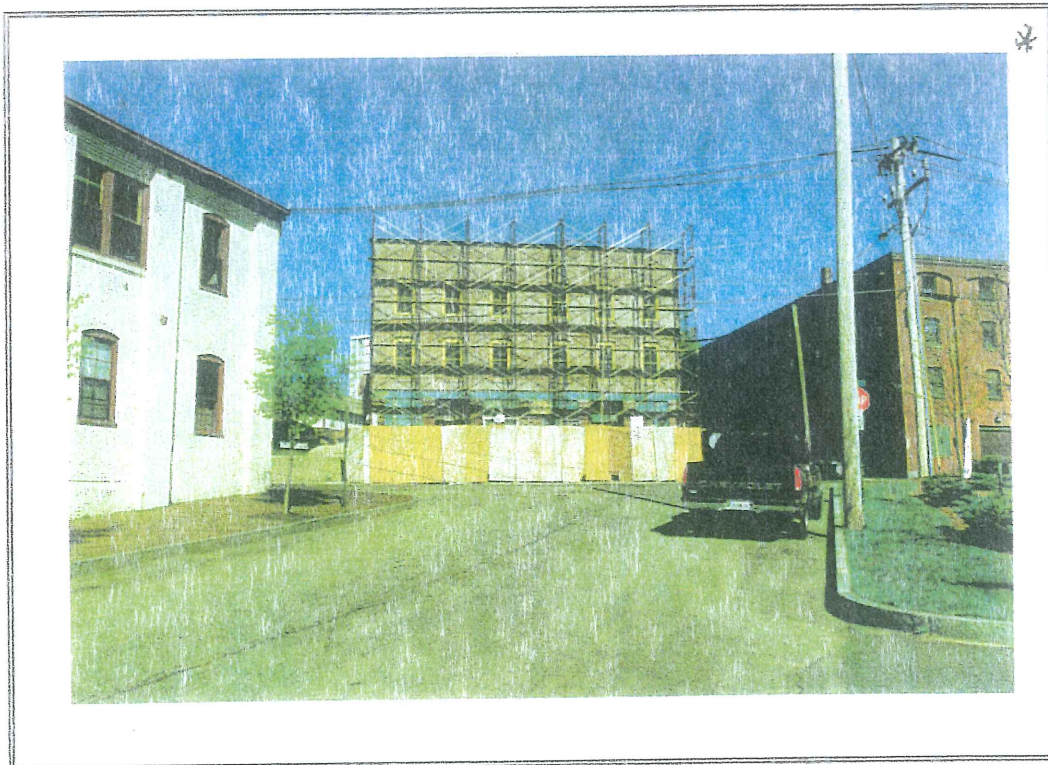
SUBJECT PHOTOGRAPHS



3) Southeasterly view down Maple St. toward Commercial St, subject at right

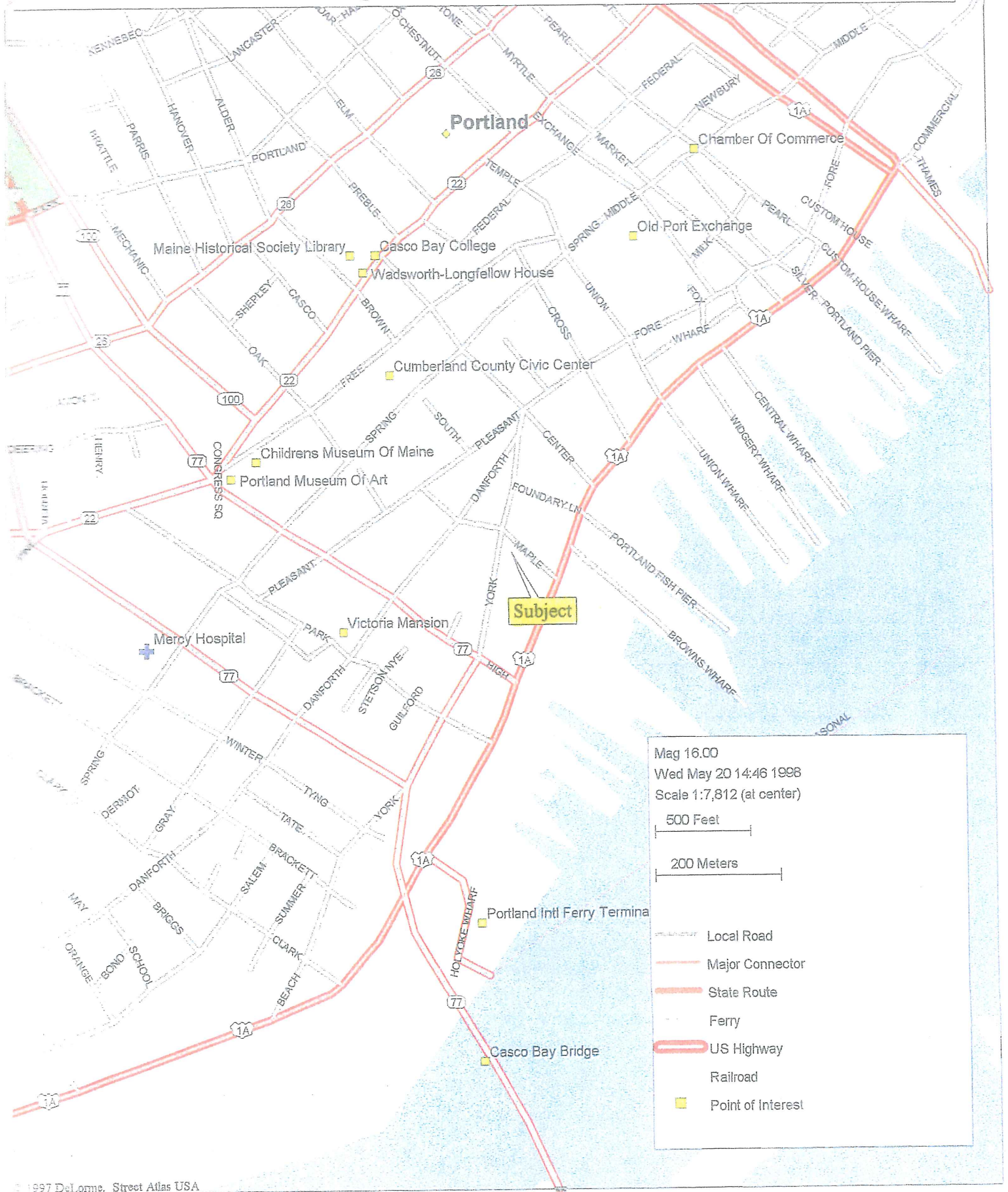


4) Westerly view of south elevation of subject (driveway behind bldg)



N2) Ongoing rehabilitation/revitalization of the subject neighborhood (subject at left)

Neighborhood Map



SUBJECT SITE DESCRIPTION

Location

The subject site enjoys a corner lot location at the apex of York and Maple Streets in Portland Maine. This location receives heavy traffic flow along York Street as this is the major connector route from the Portland CBD to the Casco Bay Bridge providing access to the City of South Portland. The site is also within close proximity to the Portland Waterfront District with marine, transit, and tourist related land uses prevalent.

Land Area

The subject site, according to the legal description provided, contains an estimated land area of 8,712±sf in fee simple, plus an additional passageway area measuring 15' x 85' (1,275±sf) for a total of 9,987± sf. The survey plan also provided to the appraiser, showed an area of 8,447±sf, although it is not clear that this is not the footprint of the building rather than the site. For purposes of this appraisal, this is not a large issue as the site is considered fully built out and the leased fee interest has been appraised. The client is urged to consult with a surveyor and/or title attorney to clarify such matters further prior to making decisions which may be based upon land area estimates.

Shape/Frontage

The shape of the site is characterized as irregular. The parcel is a corner lot with an almost trapezoidal shape due to its frontage on two streets. According to the legal description provided, the site has 125'± of frontage along Maple Street plus another 100'± of frontage along York Street.

Access/Visibility

The subject site is considered to have good access and visibility characteristics. The site enjoys high traffic counts approximating 12,000 cars per day according to MDOT traffic volumes 1996. York Street feeds directly into/from the new Casco Bay Bridge (replaced the Million Dollar Bridge last year) and is a direct link to the Portland CBD and Old Port Districts. Commercial Street to the south is also a major commuter travel artery and serves the working waterfront section of Portland. Maple Street is a small connector road which traverses the two.

Topography/Soils

The topography of the site is characterized as sloping in an easterly direction down from its York Street frontage towards Commercial Street and the Portland Waterfront. No on site soils investigation was performed by the appraiser. The Cumberland County Soils Survey classifies this area as Urban, Cut and Fill land. It was reported that a portion of the subject (original construction) dates back to 1820 and survived the Great Fire of 1936 which leveled much of downtown Portland and gave way to the filling in along the waterfront and creation of Commercial Street in 1950. Given the age and varied uses of the site over time, the client is urged to conduct due diligence in regards to environmental issues to their own satisfaction. The appraised value herein assumes no adverse conditions related to environmental issues which may or may not affect the subject site. Reference is made to the Statement of Assumptions and Limiting Conditions in this regard.

Utilities

The subject site is serviced with all public utilities including water, sewer, natural gas, electricity, telephone and cable TV. Municipal services such as fire and police protection and sanitation are deemed to be adequate.

Easements

Reference is made to the attached legal description and title report contained in the Addenda section of this report for an overview of the various limitations of title to the subject property. As discussed, part of the site is encumbered as a passageway in common with others. There were no other adverse easements or encroachments noted on the date of inspection/value herein which would limit the use and/or value of the subject property under its current land use program.

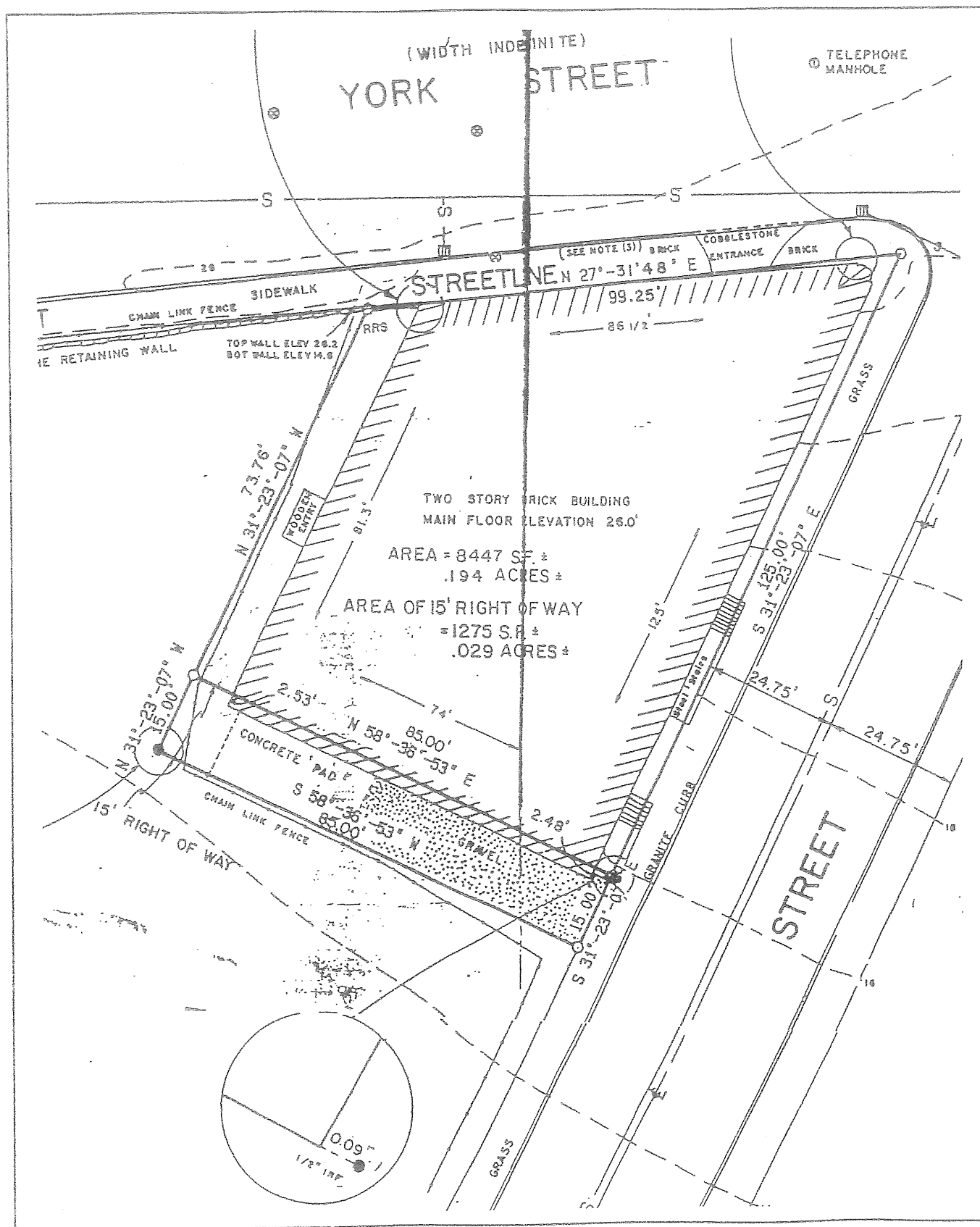
Functional Utility

The subject site is considered to offer average functional utility for development purposes. If vacant and available, it would likely be assembled with abutting lands for a larger scale development. The site does offer good exposure for commercial purposes and adequate frontage for access. It is zoned for business use and is located in an area of revitalization. An interim use for surface parking would be an immediate use of the site.

Marketability/Appeal

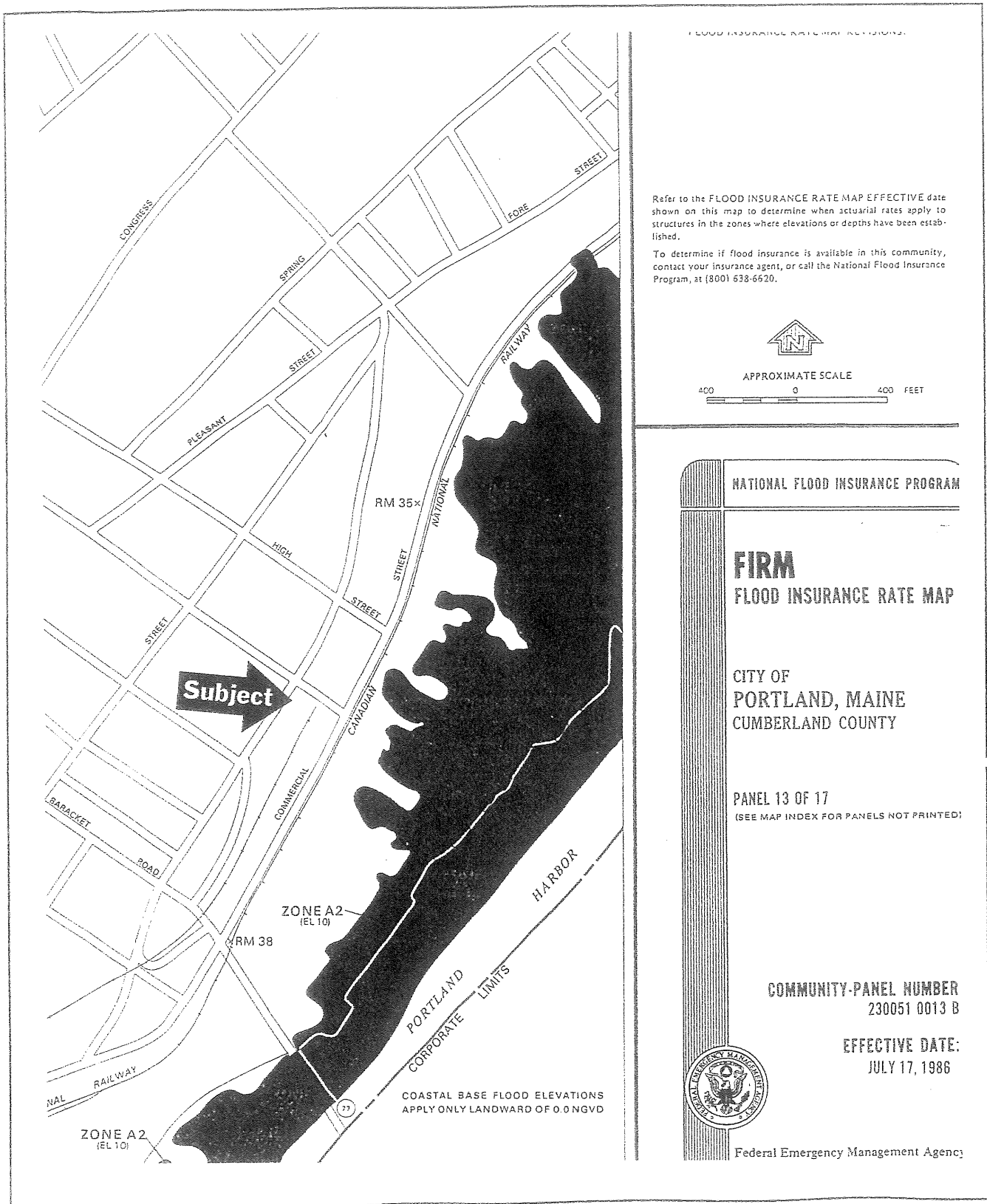
For the reasons cited above, the subject land parcel is considered very marketable and would be anticipated to sell quickly if vacant and available.

SUBJECT SITE PLAN
(not to scale)



FEDERAL EMERGENCY MANAGEMENT FLOOD HAZARD ZONE

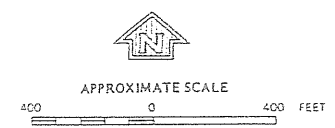
The subject property is not located in a federally designated 100-year flood hazard zone, according to the FEMA Map presented below:



FLOOD INSURANCE RATE MAP

Refer to the FLOOD INSURANCE RATE MAP EFFECTIVE date shown on this map to determine when actuarial rates apply to structures in the zones where elevations or depths have been established.

To determine if flood insurance is available in this community, contact your insurance agent, or call the National Flood Insurance Program, at (800) 638-6620.



NATIONAL FLOOD INSURANCE PROGRAM


FIRM
FLOOD INSURANCE RATE MAP

CITY OF
PORTLAND, MAINE
CUMBERLAND COUNTY

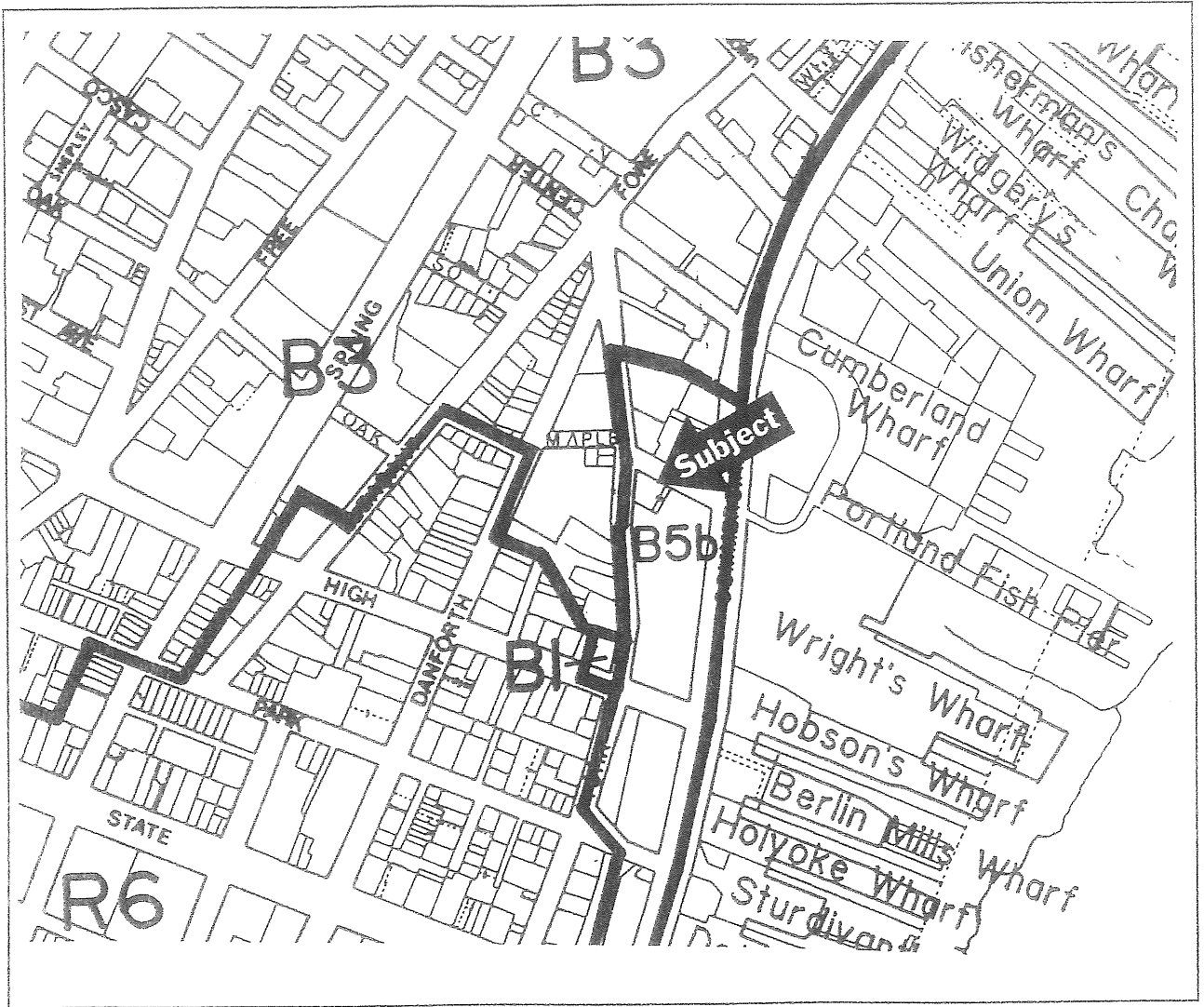
PANEL 13 OF 17
(SEE MAP INDEX FOR PANELS NOT PRINTED)

COMMUNITY-PANEL NUMBER
230051 0013 B

EFFECTIVE DATE:
JULY 17, 1986



Federal Emergency Management Agency



Pictur

SUBJECT IMPROVEMENTS DESCRIPTION

Year Built

The subject building improvements were built in the late 1800's, and subsequent to a fire, was rebuilt and expanded around the 1920's.

Gross Building Area

The building is estimated to contain approximately 21,297±sf of gross building area, all of which has been considered net rentable area as per existing leases in place (common areas prorated).

Foundation

The foundation walls are of mortared brick and appear to be in sound condition.

Frame

The frame of the building is heavy timber columns and wooden floors, some supported with brick columns and steel girders.

Exterior Walls

The exterior walls are of painted brick.

Floors

The top two floors in the building were generally of wood underlayment with a concrete slab floor at the daylight basement level.

Floor Covering

Floor coverings ranged from low cost to medium grade commercial carpeting in the office areas, some composition tile and/or vinyl floor finishes in the bathroom areas. The lower level was mostly painted concrete with some finished areas carpeted.

Partitions

Interior partitions were mostly of wood stud walls and sheetrocked panels. Some paneling and matchboard wainscoting were also noted.

Layout

The layout is fairly heavily partitioned into numerous offices and suites for multi-tenant occupancy.

Ceilings

The ceilings were primarily suspended acoustic panel tiles and in good condition, with recessed fluorescent lighting throughout.

Roof Structure & Cover

The roof structure is of a flat, shed style with a covering of rubber membrane dating to 1989 according to the present owner.

Electrical

The electrical entrance to the building consists of two primary services of four-wire, three phase, 240 volt capacity with a 400-amp rating. Distribution is via six 100 amp and 1 200-amp panels with circuit breaker protection. Electricity is separately metered to each tenant excepting unit 102 which is on the house meter.

Plumbing

The building is serviced with municipal water and sewer. The main water supply enters in the basement sprinkler room and has a backflow preventer. Where visible, plumbing distribution piping is primarily copper with some PVC and cast iron waste lines noted. There is also a recent, gas fired domestic hot water heater in the basement.

H.V.A.C.

The building is serviced with a natural gas heater with individual tenant meters, excepting unit 102 which is on the house. Heat is provided through the building with a combination of ceiling and wall hung gas heaters. There is a central, gas-fired boiler with five zones noted on the first floor as well as some electric baseboard heat interspersed.

Elevators

There are no elevators currently in the building.

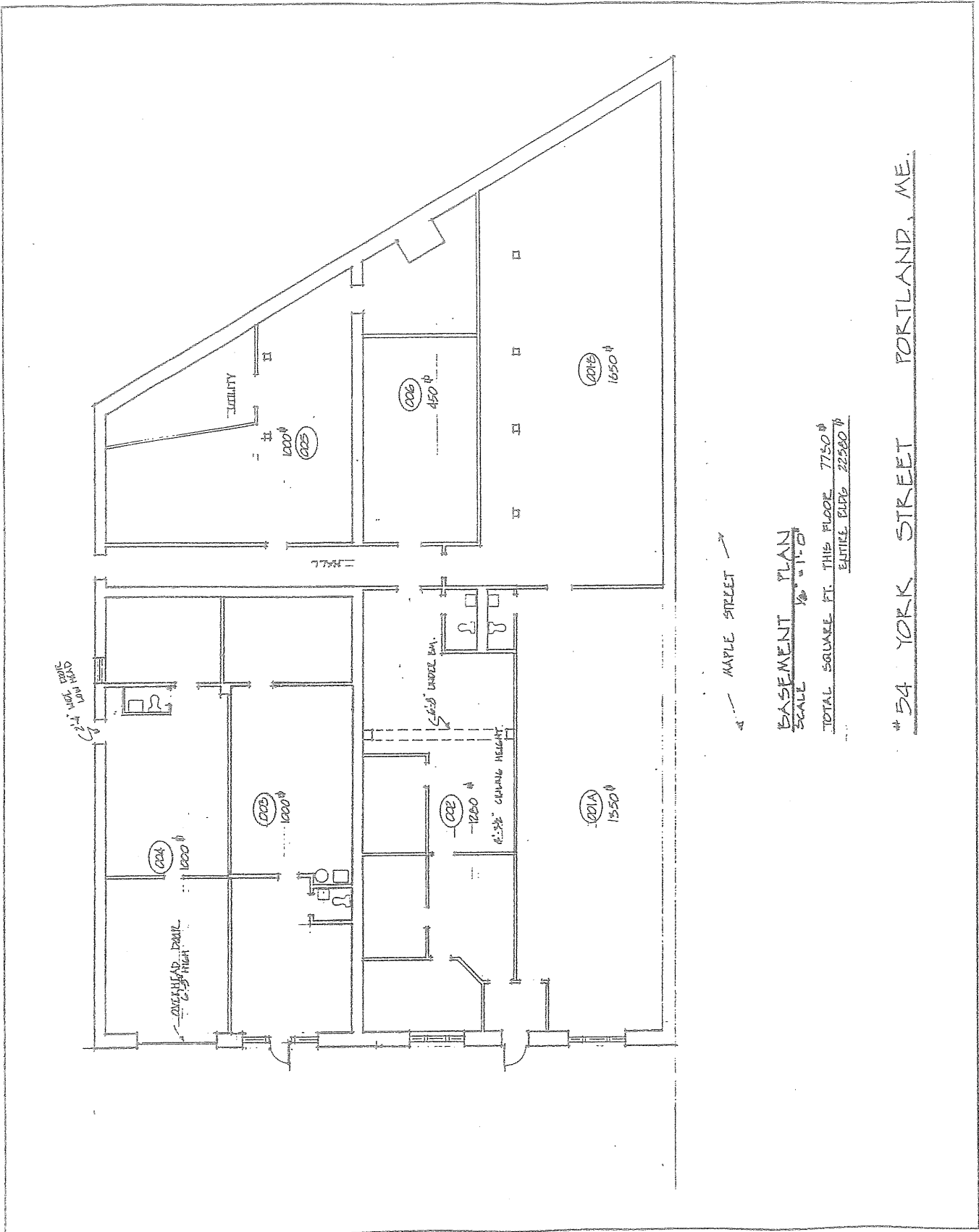
Sprinkler System

The building is provided with a dry type sprinkler system.

Overall Condition

The overall condition of the improvements is rated as average to good.

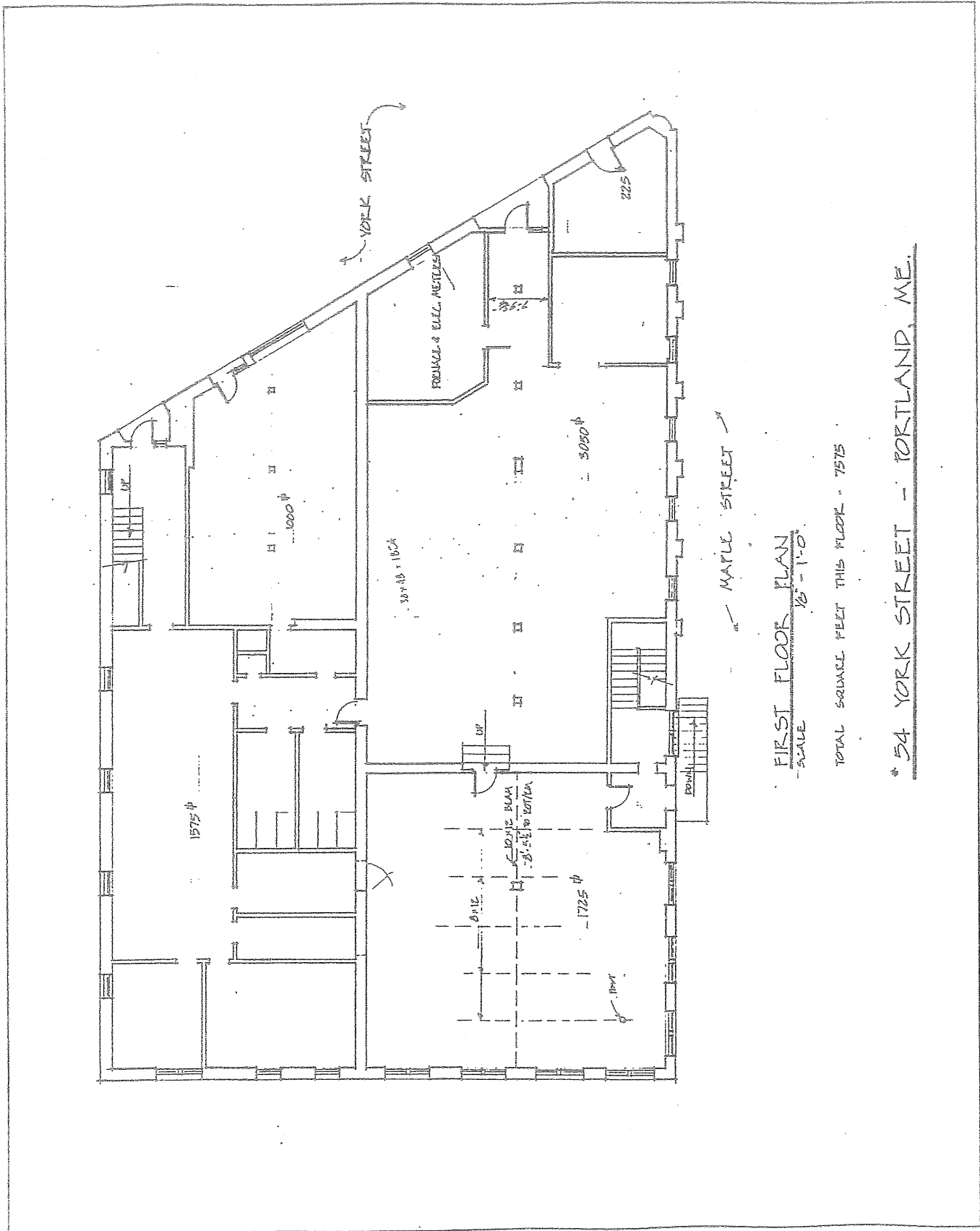
SUBJECT IMPROVEMENTS SKETCH - Basement Level
(not to scale)



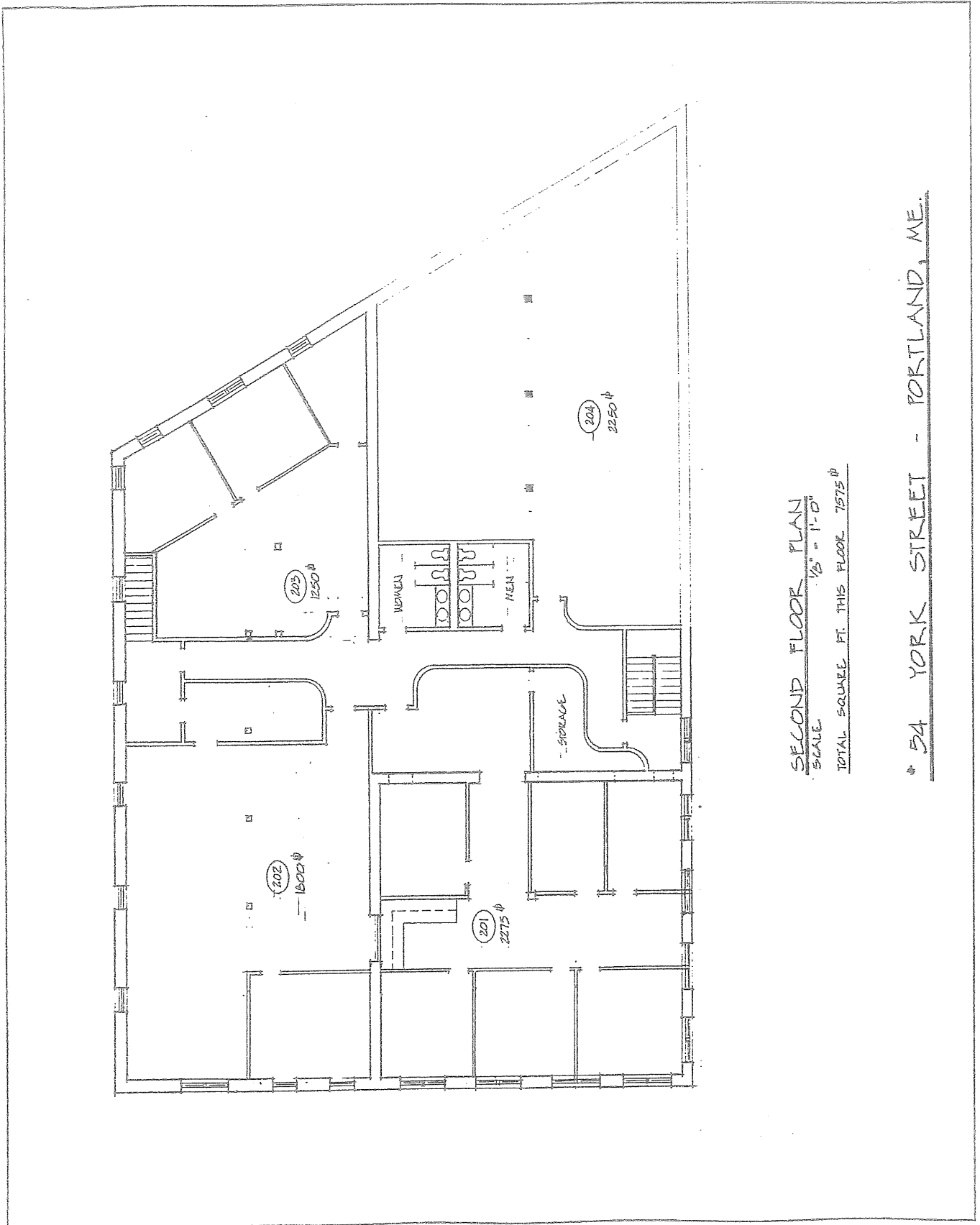
BASEMENT PLAN
SCALE 1/8" = 1'-0"

TOTAL SQUARE FT. THIS FLOOR 7750' [#]
ENTIRE BLDG 22580' [#]

SUBJECT IMPROVEMENTS SKETCH - First Floor Level
(not to scale)



SUBJECT IMPROVEMENTS SKETCH - Top Floor Level
(not to scale)



SECOND FLOOR PLAN
SCALE 1/2" = 1'-0"
TOTAL SQUARE FT. THIS FLOOR 7575 sq

54 YORK STREET - PORTLAND, ME.

SURVEYOR'S REPORT

Fill out and sign with all Surveys in accordance with the Instructions on the reverse side.

To: LAWYERS TITLE INSURANCE CORPORATION

Richmond, Virginia.

THIS IS TO CERTIFY, that on December 3, 1987, I made an accurate survey of the premises standing in the name of William J. Dowd situated at Portland Cumberland Maine City County State

briefly described as: Commercial Structure and shown on the accompanying survey entitled: Standard Boundary Survey for William J. Dowd

I made a careful inspection of said premises and of the buildings located thereon at the time of making such survey, and again on December 17, 1987 and at the time of such latter inspection I found William J. Dowd to be in possession of said premises as owner (tenant) or (owner)

I further certify as to the existence or non-existence of the following at the time of my last inspection:

1. Rights of way, old highways, or abandoned roads, lanes or driveways, drains, sewer, water, gas or oil pipe lines across said premises (Include any such matters shown on the recorded plat of subdivision): 15 foot Right of Way to land now or formerly of Rufus Deering Location of sewer connection uncertain

2. Springs, streams, rivers, ponds, or lakes located, bordering on or running through said premises: None observed

3. Cemeteries or family burying grounds located on said premises. (Show location on plat): None observed

4. Telephone, telegraph or electric power poles, wires or lines located on, under, overhanging or crossing said premises and serving other property or properties: None observed above ground below grade lines uncertain

5. Joint driveways or walkways; party walls or rights of support; porches, steps or roofs used in common or joint garages: 15 foot strip above mentioned apparently owned by Dowd subject to use by abutter.

6. Disputed boundaries, encroachments or overhanging projections. (If the buildings, projections or cornices thereof, or signs affixed thereto, fences or other indications of occupancy encroach upon or overhang adjoining properties or easement areas, or the like encroach upon or overhang surveyed premises, specify all such): Encroachment of building onto 15 foot Right of Way, Sign overhangs York Street, Fire Escapes overhang Maple Street, Fence of Abutter encroaches on said property.

7. Physical evidence of boundary lines on all sides. (Be specific): Iron pins found at two corners (see plan)

8. Is the property improved? Yes (a) Building is: Brick (X); Clapboard (); other () Specify (b) Building is: One story (); Two story (X); split-level (); other () Specify

9. Indications of building construction, alterations or repairs within recent months: Exterior brick cleaned, repainted, new doors (a) If new improvements under construction, how far have they progressed? 50 per cent completed more or less

10. Changes in street lines either completed or officially proposed: ing York Streetline City in progress of determining (a) Are there indications of recent street or sidewalk construction or repairs? NONE

11. Are all abutting streets or roads maintained by public authorities? Yes (a) Is access to such streets or roads limited? NO

12. If the surveyed premises are subject to restrictive covenants, do the improvements, use and occupancy comply with such? (If the premises are subject to restrictive covenants, have the examining attorney furnish you verbatim copy of them) Note: Origination Deed 1555/24 requires Grantee to build and maintain fence, Existing fence appears to have been constructed by abutter.

William A. Kenfield Jr. Civil Engineer or Surveyor

(Be sure to answer each item. If the property is not subject to any such of items 1 to 6 inclusive, insert the word "none" following the item.)

NOTE: In all cases where there are encroachments, easements, party walls, etc., they should also be denoted upon the map of your survey. Also, be certain map complies with Instructions on reverse side. Particular attention is directed to Paragraph 3 of these Instructions.

BEAGLE, PEARCE & RIDGE, LLC
ATTORNEYS AT LAW

C. ALAN BEAGLE
MICHAEL J. PEARCE
MARTIN J. RIDGE
JACQUELYN M. KURZ
PAULA M. CRAIGHEAD

24 CITY CENTER
P.O. BOX 7044
PORTLAND, MAINE 04112
(207) 773-1751
FAX (207) 775-3382

November 30, 1998

Sally Dwyer
Boe

Mr. William M. Moody, Jr., President
Rufus Deering Company
383 Commercial Street
P.O. Box 880
Portland, ME 04112-0880

Re: Rufus Deering Company/Peoples Heritage Bank/York
Street, Inc.

Dear Bill:

Enclosed please find the following original documents:

1. Warranty Deed from York Street, Inc. to Rufus Deering Company. This Warranty Deed has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 13.
2. Assignment of Leases and Security Deposits from York Street, Inc. to Rufus Deering Company. This Assignment has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 36.

If you have any questions, please feel free to contact me.

Very truly yours,

Robyn A. Pearce
Robyn A. Pearce

Assistant to Michael J. Pearce

enclosure

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.
By: Werner Schnetzer
Werner Schnetzer
Its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,
Paul S. Zucker
Notary Public/Attorney-at-Law
Print Name: Paul S. Zucker

122895/PSB/SKLF
91401.4926

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4: 48

CUMBERLAND COUNTY

John B. Bruin

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4:52

CUMBERLAND COUNTY

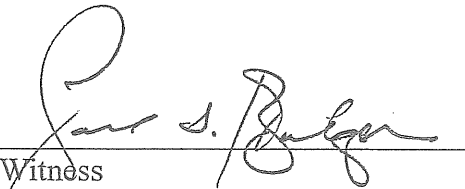
John B. O'Brien

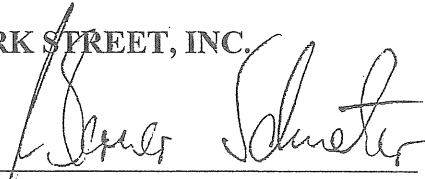
61274

ASSIGNMENT OF LEASES AND SECURITY DEPOSITS

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04106, for consideration, does hereby ASSIGN to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, its successors and assigns, all its right, title and interest in and to any and all leases, subleases and/or tenancies of the premises located on or about 54-62 York Street in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto, including, without limitation, any and all tenant security deposits relating thereto.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by Werner Schnetzer, its duly authorized President, on this 1st day of September, 1998.

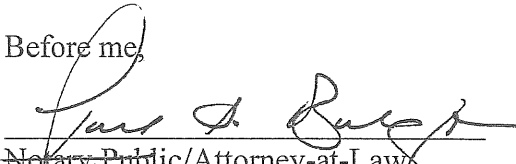

Witness

YORK STREET, INC.
By: 
Werner Schnetzer
Its: President

STATE OF MAINE
CUMBERLAND, SS.

September 1, 1998

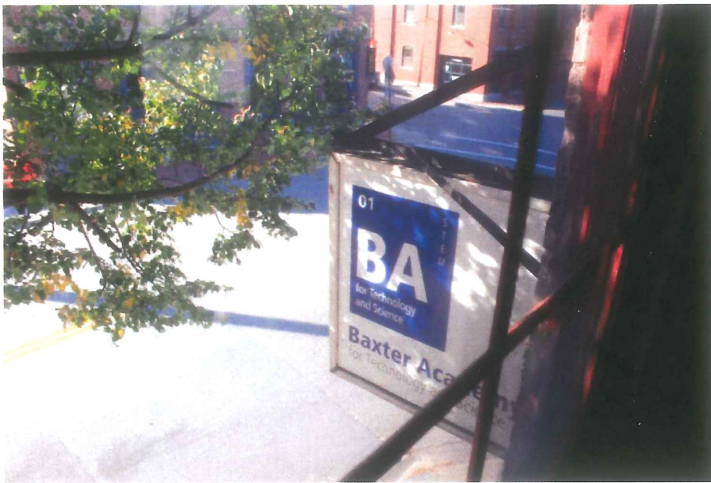
Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in said capacity and the free act and deed of said YORK STREET, INC.

Before me,

Notary Public/Attorney-at-Law
Print Name: Paul S. Bulger

122937/PSB/SKLF
91401.4926

TRAFFIC MOVEMENT
PERMIT APPLICATION

REQUEST FOR
SCOPING MEETING
SECTIONS 1-6



FOR

PROPOSED
CHARTER HIGH SCHOOL
54 YORK STREET
PORTLAND, ME

ON BEHALF OF

RUFUS DEERING LUMBER
COMPANY
384 COMMERCIAL STREET
PORTLAND, ME

SEPTEMBER 28, 2012

Department of Transportation
Traffic Engineering Division
16 State House Station
Augusta, Maine 04333
Telephone: 207-287-3774

FOR MDOT USE 1/2000
ID #
Total Fees:
Date Received:

PERMIT APPLICATION - TRAFFIC TRAFFIC MOVEMENT PERMIT, 23 M.R.S.A. § 704 - A

Please type or print:

This application is for: Traffic 100-200 PCE's Traffic 200+ PCE's

Name of Applicant: Rufus Deering Lumber Company

Address: 384 Commercial Street, Portland, ME 04101 Telephone: (207) 772-6505

Name of local contact or agent: Stephen S. Sawyer, Jr., P.E., Sebago Technics, Inc.

Address: 75 John Roberts Road, Suite 1A, South Portland, ME 04106 Telephone: (207) 200-2082

Name and type of development: Baxter Academy for Technology and Science, Charter High School

Location of development including road, street, or nearest route number: 54 York Street, Portland, ME 04101

City/Town/Plantation: Portland, County: Cumberland, Tax Map # 42 Block A, Lot # 7 & 8

Do you want a consolidated review with DEP pursuant to 23 M.R.S.A. § 704-A (7)? Yes No

Was this development started prior to obtaining a traffic permit? No

Is the project located in an area designated as a growth area (as defined in M.R.S.A. title 30 - A, chapter 187)?

Yes No

Is this project located within a compact area of an urban compact municipality? Yes

Is this development or any portion of the site currently subject to state or municipal enforcement action?
No

Existing DEP or MDOT permit number (if applicable): N/A

Name(s) of DOT staff person(s) contacted concerning this application: _____

Name(s) of DOT staff person(s) present at the scoping meeting for 200+ applications: _____

CERTIFICATION

The traffic engineer responsible for preparing this application and/or attaching pertinent site and traffic information hereto, by signing below, certifies that the application for traffic approval is complete and accurate to the best of his/her knowledge.

Signature: Stephen Sawyer, Jr. Re/Cert/Lic No.: _____

Name (print): Stephen S. Sawyer, Jr., P.E.

Date: 9/27/12



If the signature below is not the applicant's signature, attach letter of agent authorization signed by applicant.

"I certify under penalty of law that I have personally examined the information submitted in this document and all attachments thereto and that, based on my inquiry of those individuals immediately responsible for obtaining the information, I believe the information is true, accurate, and complete. I authorize the Department to enter the property that is the subject of this application, at reasonable hours, including buildings, structures or conveyances on the property, to determine the accuracy of any information provided herein. I am aware there are significant penalties for submitting false information, including the possibility of fine and imprisonment."

Wan [Signature]
Signature of applicant

9/27/12
Date

NOTICE OF INTENT TO FILE

Please take notice that Rufus Deering Lumber Company
384 Commercial Street
Portland, ME 04101
(207) 772-6506

is intending to file a Traffic Movement Permit application with the Maine Department of Transportation and City of Portland pursuant to the provisions of 23 M.R.S.A. § 704 - A on or about September 28, 2012

The application is for the development of a 4-year Charter High School focusing in the areas of mathematics, technology, and science. The proposed development will generate 154 AM and 125 PM trips. Construction is anticipated for Fall 2012 at the following location:

54 York Street
Portland, ME 04101

A request for a public hearing must be received by the City, in writing, no later than 20 days after the application is found by the City to be complete and is accepted for processing. Public comment on the application will be accepted throughout the processing of the application.

The application will be filed for public inspection at the Department of Transportation Division office in Scarborough during normal working hours. A copy of the application may also be seen at the municipal offices in Portland, Maine.

Written public comments may be sent to the Department of Transportation, Traffic Engineering Division, 16 State House Station, Augusta, Maine 04333 or the City of Portland Planning Department, 389 Congress Street, Portland, ME 04101-3509.

JN 07383
Baxter Academy Charter School
TMP Abutters List

J. B. Brown and Sons
P.O. Box 207
Portland, ME 04112

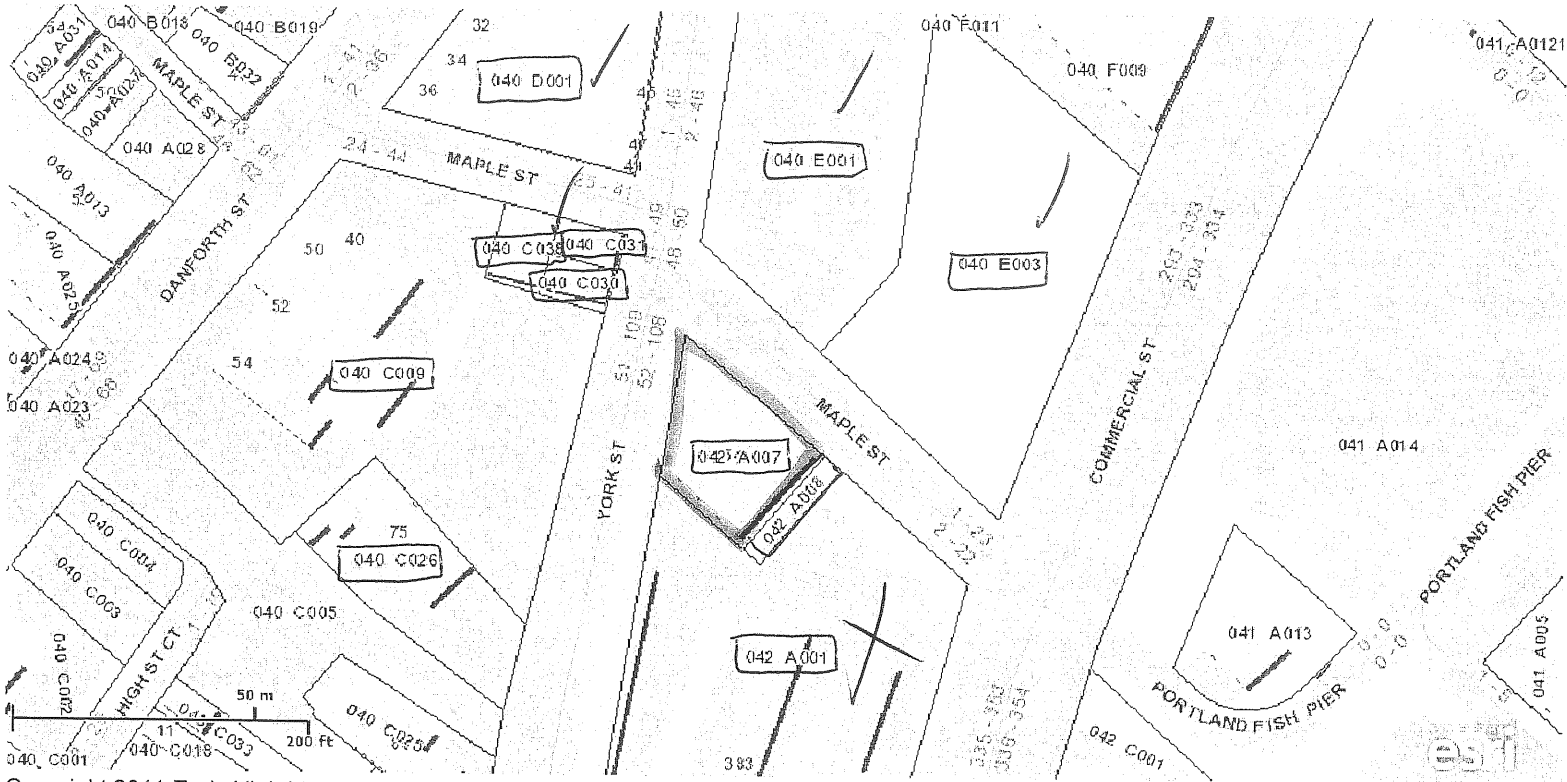
All In Enterprises LLC
5 Lincoln Avenue
Scarborough, ME 04074

75 York Street LLC
15 Franklin Street
Portland, ME 04101

My Map

07383

rd



Copyright 2011 Esri. All rights reserved. Thu Sep 27 2012 09:59:18 AM.

Section 1. Site and Traffic Information

A. Site Plan.

Rufus Deering Lumber Company, 384 Commercial Street, Portland is proposing to remodel their existing building at 54 York Street (the corner of York and Maple Streets) to accommodate the new Baxter Academy for Technology and Science Charter High School. The current property houses the existing Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses of offices, labs, and a dance studio. This 0.22 acre (9,583 s.f.) lot is fully developed with an existing 7,540 s.f., 3-story building in the City's B-5b (Urban Business) Zone. This property is also located within the Old Port Historic District. See the Site Plan in the back of this submission.

B. Existing and Proposed Site Uses.

The existing building is currently developed with the following land uses:

- An exercise studio offering Zumba classes
- A Buddhist Center
- Administrative offices
- Global Biotech
- Deering Lumber's Kitchen and Millwork Showroom

Once the remodeling is complete the building will accommodate the Baxter Academy for Technology and Science Charter School as the primary tenant. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. The school's vision is to convert the entire building into a full 4-year high school over the next 3 years. Total enrollment is planned for 320 students by 2015. In the opening year (2013), 160 - first and second year students are envisioned. In year two, grades 9, 10, and 11 will be provided, and in year three all four years of high school will be offered. It is estimated that 50% of the enrollment will come from Portland, South Portland and Westbrook, with the balance originating from communities outside of the urban core. School operations will run from 8:00 AM to 2:00 PM, resulting in peak traffic associated with the school being between 7-8:00 AM and 2-3:00 PM.

C. Site and Vicinity Boundaries.

A regional map showing the development site and its relationship to the adjacent street network as defined in MaineDOT's Chapter 305 of the General Rules of the Department of Transportation (Section 5B) is presented as part of the Site Plan located at the end of this submission.

D. Other Proposed Uses in the Vicinity of the Proposed Development.

A discussion with the City of Portland's Planning Department revealed that there are no other developments currently under review or have been permitted but not built in the immediate proximity of this Project at this time.

E. Trip generation.

ITE's Trip Generation, 7th Edition, was consulted to determine what industry data suggests for this type of land use. LUC 530 - High School, which includes both public and private institutions was deemed the closest category listed in this reference. Believing that the City's greatest traffic concern would be during the AM peak hour because it will coincide with the morning commute, we have focused our attention on this time period only. Using a base school population of 320 students and 30 employees, we find the following for the AM peak hour using ITE information:

<i>LUC 530</i>	<i>Variable</i>	<i>Total Trips</i>
By Student: $\ln(T) = 0.77\ln(X) + 0.69$	320 (students)	169
By Employee: Average Rate = 4.63	30 (employees)	139
Average of Above		154

The above trip generation assumes that there is no existing traffic associated with the site today, which is not entirely true. Currently there is an exercise studio (Nancy's) that offers among other things, Zumba classes, and a Buddhist Center (Nagaloka), and the administrative office for the Baxter School operating in the building. However, given the operating hours of these three uses, we have not taken a credit for any existing trips during the AM peak hour (7:00 AM to 8:00 AM).

From the data above it is estimated that the peak trip generation from the proposed development will be 154 trips (all primary), which will put the project in the Traffic Movement Permit (TMP) category of 100--200 Passenger Car Equivalent (PCE) trips.

F. Trip Distribution.

The ITE Trip Generation Handbook was also consulted with regard to the directional distribution of site generated trips during the AM peak period. ITE data suggests that during the AM peak period trips at the site will be distributed roughly 70% entering and 30% exiting regardless of which method of generation is used, i.e. "student" or "employee".

G. Trip assignment.

While ITE suggests that traffic generation from the proposed facility could be in the range of new 150 trips in the AM peak hour, the Baxter Academy plans to adopt a series of travel demand management strategies for minimizing this impact on the surrounding street system. Typically, students at four-year public high schools have a variety of transportation choices that must be addressed in the school's planning. These range from parent drop-off and pick-up, to buses, and finally to student drivers with their own cars. The actual mix usually depends on the school's location within the community and the amount of available land surrounding the school. In the case of the Baxter Academy, the setting is very urban with no surplus land for off-street parking. Accordingly, the school's administration has formulated the following Transportation Plan.

First of all, it should be understood that in the initial year of operation, only 9th and 10th graders will be attending the Baxter Academy, so student drivers will not be part of the mix. However, in subsequent years this will need to be a consideration.

Not knowing exactly what to expect from prospective student families, Baxter Academy's administration conducted a Parent Survey to gauge their transportation preferences. A total of 24 families participated in

this survey, which was performed in June of 2012. See the Appendix. The results indicated that approximately 25% of the families preferred to transport their children to and from school either by individual or car pool means. 70% envisioned using bus transportation, either charter service provided by the school or public transportation because they reside within Greater Portland. The remaining 5% indicated they would walk to and from the school. Before running any numbers on this basis, contact was made with a few similar facilities in the region – the Academy for Science and Design in Nashua, NH – a 410 student charter school serving grades 6-12 in the business district of Nashua; Waynflete in Portland, a private K-12 school with 550 students in the west end; and Portland High School on Cumberland Avenue with 920 students. Table 1 presents the results from this research.

Table 1 – Modal Choice by Families of 4-year Urban High Schools

<i>School</i>	<i>% Parent Drop-Off</i>	<i>% Transit</i>	<i>% Student Drivers</i>	<i>% Walkers & Bikers</i>
Academy for Science & Design	65	34	1	0
Portland High School	50	23	16	11
Waynflete School	50	30	14	6
Baxter Parent Survey	25	70	-	5
Percentages Used Adopted	30	50	15	5

From the data in Table 1, there is no clear model that works for all cases – each school’s experience seems to be unique depending on the school’s setting and its market service area. Therefore, the modal split percentages adopted for Baxter are a reasonable blend of parent feedback and other surveyed school’s experiences. Waynflete’s parent drop-offs appeared to be influenced by the younger population K-8. Portland High’s numbers are skewed due to the students all residing within the City where we expect many parents work. The Academy for Science & Design does not provide bus service but relies on the City’s School District for busing, the school’s setting is not conducive to walking or biking, and they only have 5 seniors with licenses at this point – hence the high percentage of parent drop-offs. Finally, the proposed Baxter Academy is planning to provide charter busing to outlying communities within its service area to reduce the need for parent shuttles.

The AM peak hour forecast below for the Baxter Academy is based on a student population of 320 and 30 employees by Year 3 and 160 students and 15 employees in Year 1:

Year 1 (2013) – 160 Students

30% parent drop-off (assumed split of 50% car pooling with 3 students and 50% single occupant): 30% X 160 students = 48 students
24 students in single occupant vehicles = 48 trips
24 students in car pools of 3 = 16 trips
55% of students (88) ride 3 charter buses (1.5 PCE per bus) = 9 trips
10% of students (16) ride existing public transportation = 0 trips
5% of students (8) as walkers = 0 trips
*15 employees @ 67% auto use = 10 trips**
Total estimated AM peak hour trips in Year 1 = 73 trips
Anticipated Distribution = 37 arriving and 36 departing

Year 3 (2015) - 320 Students

30% parent drop-off (assumed split of 50% car pooling with 3 students and 50% single occupant): 30% X 320 students = 96 students

48 students in single occupant vehicles = 96 trips

48 students in car pools of 3 = 32 trips

40% of students (128) ride 3 charter buses (1.5 PCE per bus) = 9 trips

*15% of students (48) drive their own cars = 0 trips**

10% of students (32) ride existing public transportation = 0 trips

5% of students (16) as walkers = 0 trips

*30 employees @ 67% auto use = 20 trips**

Total estimated AM peak hour trips in Year 3 = 137 trips

Anticipated Distribution = 69 arriving and 68 departing

*Because the site will not allow it, the School is not planning to provide any on-site parking for students or employees. A total of 20 off-site parking spaces are initially going to be leased for employees, but since they are off-site their impact will not be felt at the site itself.

The School is proposing a full Traffic Management Plan to organize student arrivals and departures, which is detailed later herein. From a Trip Assignment perspective, though, this Plan will route arriving school buses from York Street onto Maple Street and load/unload curbside on the west side of the street in a designated School Drop-Off Zone. The buses will depart the school using Maple Street to Commercial Street. Arriving parents dropping off their students will be directed to enter Maple Street from Commercial Street, proceed to York Street where they would make a left onto so that they can utilize the designated 8-space School Drop-Off Zone on the north side of the street opposite the front door of the school. Figure 2 illustrates the forecasted school traffic during the AM peak hour for both Years 1 and 3 at the York/Maple intersection under this traffic scenario. Figure 3 illustrates the combined (existing and forecasted) trips at the York/Maple Street intersection.

H. Traffic Analysis

Existing Conditions

Sebago staff performed a traffic turning movement count at the intersection of York Street and Maple Street during the hours of 7:00 AM and 8:00 AM on June 27, 2012 to observe traffic operations in and around the site during the time period when children would be arriving for the school day. Figure 1 presents the results of this peak hour count. It is readily evident that the volumes are quite small during this time period. Synchro/Simtraffic was used to analyze existing traffic operations at the intersection and it was determined that the Level of Service (LOS) was A.

In addition to this data collection and analysis effort, several general observations were made while conducting the count. These were:

1. Only one on-street parking space was used on Maple Street between Commercial Street and York Street during the one hour time period observed. This vehicle was a van that had parked there overnight.
2. The paved parking lot for "The Cannery" that is accessed off Maple Street only had 8 cars enter it between 7:00 AM and 8:00 AM.

3. Maple Street is barely used during this time period. Total traffic during the hour observed was 34 vehicles. Never was a queue experienced waiting to enter onto York Street, and with the exception of one overnight parked vehicle, no on-street parking spaces were occupied.
4. The two on-street parking spaces to the west of the proposed school building on York Street were not occupied during the observation period.
5. While the traffic on York Street was predominantly in-bound, it appeared that the time period 7:00 AM to 8:00 AM might be before the actual peak morning commute hour.

Year 1 – 160 Students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection's LOS in Year 1. The LOS was found to be A and only slightly less than it is today under existing conditions. Furthermore, the resulting "maximum" queues at York Street from the additional Parent Drop-Off traffic, which we assumed would occur within the 30-minute timeframe prior to school commencing at 8:00 AM was found to be 37 feet, or two cars. We modeled the parent drop-offs using appropriate peak hour factors.

Year 3 – 320 Students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection's LOS in Year 3. The LOS was found to still be A. Furthermore, the resulting "maximum" queues at York Street from the additional Parent Drop-Off traffic, which we assumed would occur within the 30-minute timeframe prior to school commencing at 8:00 AM was found to be 57 feet, or three cars. We modeled the parent drop-offs using appropriate peak hour factors.

Based on the above analysis, neither capacity nor queuing appear to be an issue during the AM on Maple Street in either Year 1 or Year 3.

I. Traffic Operations and Management Plan

Arrivals and departures from the Baxter Academy are to be well-orchestrated and staff managed events. Because students will be arriving via various modes of transportation the School's Plan will have a number of elements.

Arrivals

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. Students will depart directly onto the Maple Street sidewalk and proceed to the front door of the School on York Street.

Students using public transit (Metro or South Portland Bus) will depart buses at existing stops on the north side of York Street at either High Street or Fore Street. Students will then proceed to school on existing sidewalks and cross York Street at a repainted formal crosswalk at the intersection of York and Maple Streets. The School will have a staff person supervising this crosswalk for the 45 minutes preceding school's commencement to ensure safe student passage.

Parent drop-offs will occur on the north side of York Street opposite the front door of the school. Parents will be routed onto Commercial Street from their origins so that they can enter Maple Street heading northbound where they would make a left turn onto York Street and pull off into 8 designated drop-off

parking spaces for unloading onto the adjacent northerly sidewalk. The school will have a staff person supervising this operation to keep vehicles orderly, and students will be directed to the intersection of Maple and York, where there will be another staff member acting as a crossing guard for safe passage to the front entrance of the school.

Staff members who elect to drive their own vehicles to school will be parking in the Gulf of Maine parking lot off Commercial Street or the parking garage at the Holiday Inn By-The-Bay and will walk to school via existing sidewalks.

Students who elect to drive their own vehicles to school will likely park on Commercial Street in the unrestricted parking spaces adjacent to the end of Maple Street and will walk to school via existing sidewalks.

Students who elect to ride their bikes to school will be able to store their bikes in bike racks, which will be provided by the school adjacent to the building on Maple Street.

In addition to the staff members on-site managing the student arrival process, the school proposes to:

- use traffic cones to block off the curbside in front of the school to prevent vehicles from unloading on the south side of York Street
- place a School Crossing sign in the middle of the York Street crosswalk in front of the school to keep speeds on York Street under control during peak student usage
- erect School Drop-Off Zone signs on Maple Street for the buses on the west side of the street and on the north side of York Street for parent drop-offs.

See examples below.



“Crosswalk sign used by Waynflete”



“School Drop-Off Zone Sign at Portland High”

Departures

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. At 2:00 PM when regular school is over students who are not participating in after school programs will load the buses from the Maple Street sidewalk as they did during arrivals.

Students using public transit (Metro or South Portland Bus) will depart the school for the bus stops on the north side of York Street at either High Street or Fore Street depending on individual choice and route schedules.

Parent pick-ups will occur on the north side of York Street opposite the front door of the school. The schedule of these pick-ups will not be as concentrated as in the morning because of after school activities. The school is projecting that up to one half of students will be participating in after school programs, which would mean that parent pick-ups in Year 3 would be in the range of 32 at 2:00 PM and the other 32 would be spread over the subsequent 2 hours. Given the 8 on-street spaces available this does not appear to be an issue.

Food Deliveries

Food service to the school will be provided by an off-site vendor. Deliveries will be made curbside on the west side of Maple Street, similar to where the buses will load and unload.

Proposed Signage Changes

Maple Street: The west side of the street is currently posted as 15-minute parking. This would need to be modified to allow for school bus parking between 7:30 and 8:00 AM and between 1:45 and 2:15 PM.

York Street: The north side of York Street opposite the school is currently posted as 30-minute parking. This would need to be modified to allow for 5 minute school drop-off and pick-up between 7:30 and 8:00 AM and between 2:00 PM and 4:00 PM.

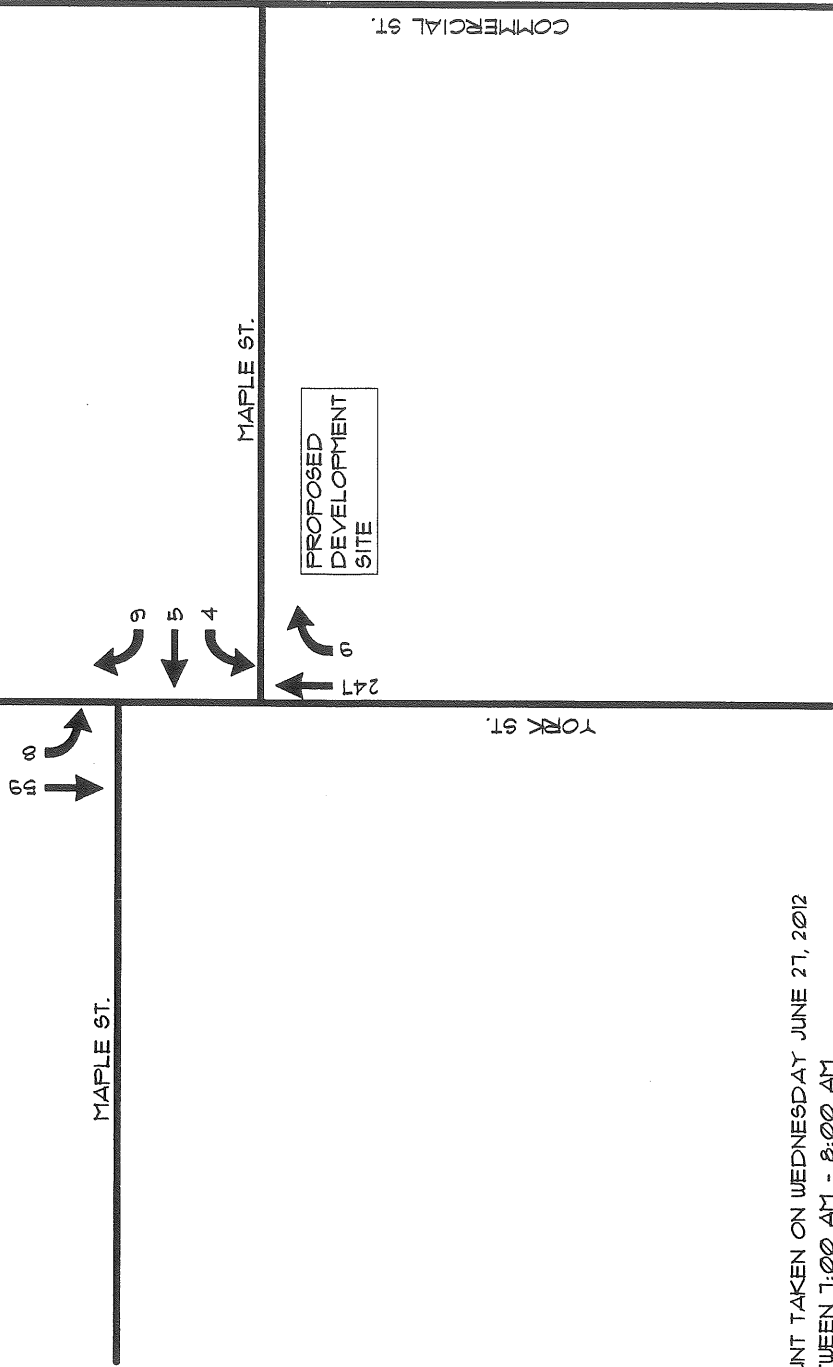
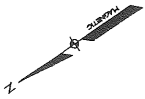
On-Going Plan Refinement

The Baxter Academy realizes that their Traffic Management Plan is subject to individual student family preferences from year to year. As such, the school administration is committed to refining their operation each year to reflect the modal choices made by their enrollees. Given that Years 1 and 2 are ramping up years in terms of the student population, the school will have adequate time to fine-tune operations prior to reaching the ultimate goal of 320 students in Year 3.

J. Parking.

From the information presented above, the number of students and employees that may require parking in the vicinity of the school is 10 staff members in Year 1 and by Year 3 this may grow to 20 staff members and 48 students. The School has made leasing arrangements for between 20-40 off-street parking spaces in the Gulf of Maine lot off Commercial Street. Other options in surrounding parking garages also exist if this is not possible. The Baxter Academy is committed to reducing the number of students driving their cars to school. In this regard the School's informational packets will encourage families to use car pooling, public transportation, school provided charter buses, walking and biking to access the facility. To further support this policy, the school is prepared to offer an incentive to students in the form of a 50-cent subsidy on public transit fares for those students within the Metro/South Portland Bus network.

For those students in Years 2 and beyond that do not choose to avail themselves of public transportation, and want to bring their own vehicles to school, they will be required to register their vehicles with the school and park legally within the surrounding area. This is similar to Waynflete's policy. The school will monitor these situations, and work with the City to address any violators should they be brought to the school's attention.



COUNT TAKEN ON WEDNESDAY JUNE 27, 2012
 BETWEEN 1:00 AM - 8:00 AM

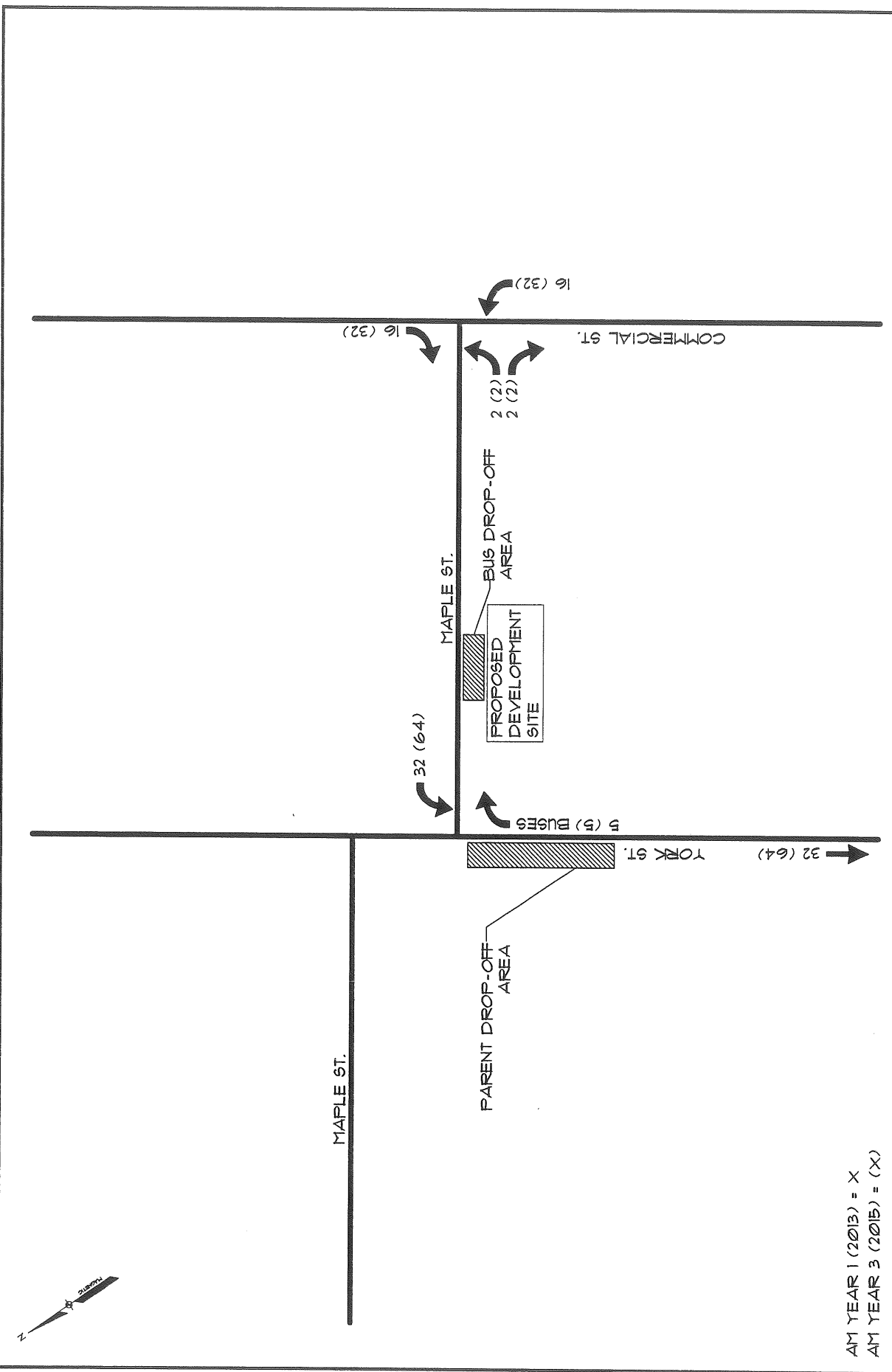
SEBAGO
 TECHNICALS
 WWW.SEBAGOTECHNICALS.COM
 75 John Roberts Rd., Suite 1A 250 Goddard Rd., Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel: 207-200-2100 Tel: 207-653-5656

EXISTING TRAFFIC VOLUMES
 YORK ST. @ MAPLE ST., PORTLAND, ME

LOCATION:
 YORK ST. @ MAPLE ST.
 PORTLAND, ME

FOR:
 RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

SCALE:	NTS
DATE:	9/26/12
FIGURE:	1



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

SEBAGO
 TECHNICS
 WWW.SEBAGOTECHNICS.COM
 75 John Roberts Rd. - Suite 1A 250 Goddard Rd. - Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel. 207-200-2100 Tel. 207-783-5656

AM NEW PRIMARY TRIPS
 YORK ST. @ MAPLE ST., PORTLAND, ME

LOCATION:
 YORK ST. @ MAPLE ST.
 PORTLAND, ME

FOR:
 RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

SCALE:	NTS
DATE:	9/26/12
FIGURE:	2

Section 2. Traffic Accidents

MaineDOT's Accident Records Unit was contacted to obtain the most recent 3-year crash information for the area in the vicinity of the proposed project. See attached materials from the Department in the Appendix. In summary, of the 12 intersections surveyed, only one is defined as a High Crash Location, i.e. a total of 8 crashes in three years and a Critical Rate Factor (CRF) greater than 1.0. This intersection is Commercial Street at High Street with a CRF of 1.50.

Road segments were also examined as evidenced by the enclosed Department data, and no segment immediately abutting the proposed project is identified as a High Crash Location.

Section 3. Development Entrances and Exits

The proposed development is to occur at 54 York Street – the corner of York and Maple Streets. The main walk-in entrance to the school will be located on York Street with Maple Street used as the curbside location for school bus drop-offs and pick-ups, as well as food service deliveries. See the proposed Site Plan in the Appendix.

Section 4. Title, Right or Interest

Please see the following Deed and supporting documents.

SURVEYOR'S REPORT

Fill out and sign with all Surveys in accordance with the Instructions on the reverse side.

To: LAWYERS TITLE INSURANCE CORPORATION

Richmond, Virginia. THIS IS TO CERTIFY, that on December 3, 1987, I made an accurate survey of the premises standing in the name of William J. Dowd situated at Portland Cumberland Maine

City County State briefly described as: Commercial Structure and shown on the accompanying survey entitled: Standard Boundary Survey for William J. Dowd

I made a careful inspection of said premises and of the buildings located thereon at the time of making such survey, and again on December 17, 1987 and at the time of such latter inspection I found William J. Dowd to be in possession of said premises as owner

I further certify as to the existence or non-existence of the following at the time of my last inspection: (tenant) or (owner)

1. Rights of way, old highways, or abandoned roads, lanes or driveways, drains, sewer, water, gas or oil pipe lines across said premises (Include any such matters shown on the recorded plat of subdivision): 15 foot Right of Way to land now or formerly of Rufus Deering Location of sewer connection uncertain

2. Springs, streams, rivers, ponds, or lakes located, bordering on or running through said premises: None observed

3. Cemeteries or family burying grounds located on said premises. (Show location on plat): None observed

4. Telephone, telegraph or electric power poles, wires or lines located on, under, overhanging or crossing said premises and serving other property or properties: None observed above ground below grade lines uncertain

5. Joint driveways or walkways; party walls or rights of support; porches, steps or roofs used in common or joint garages: 15 foot strip above mentioned apparently owned by Dowd subject to use by abutter.

6. Disputed boundaries, encroachments or overhanging projections. (If the buildings, projections or cornices thereof, or signs affixed thereto, fences or other indications of occupancy encroach upon or overhang adjoining properties or easement areas, or the like encroach upon or overhang surveyed premises, specify all such): Encroachment of building onto 15 foot Right of Way, Sign overhangs York Street, Fire Escapes overhang Maple Street, Fence of Abutter encroaches on said property.

7. Physical evidence of boundary lines on all sides. (Be specific): Iron pins found at two corners (see plan)

8. Is the property improved? Yes (a) Building is: Brick (x); Clapboard (); other () (b) Building is: One story (); Two story (x); split-level (); other () Specify

9. Indications of building construction, alterations or repairs within recent months: Exterior brick cleaned, repainted, new doors (a) If new improvements under construction, how far have they progressed? 50 per cent completed more or less Specify

10. Changes in street lines either completed or officially proposed: City in progress of determining York Streetline (a) Are there indications of recent street or sidewalk construction or repairs? None

11. Are all abutting streets or roads maintained by public authorities? Yes (a) Is access to such streets or roads limited? No

12. If the surveyed premises are subject to restrictive covenants, do the improvements, use and occupancy comply with such? (If the premises are subject to restrictive covenants, have the examining attorney furnish you verbatim copy of them) Note: Origination Deed 1555/24 requires Grantee to build and maintain fence. Existing fence appears to have been constructed by abutter.

William A. Beaufort Civil Engineer of Surveyor

NOTE: In all cases where there are encroachments, easements, party walls, etc., they should also be denoted upon the map of your survey. Also, be certain map complies with Instructions on reverse side. Particular attention is directed to Paragraph 3 of these instructions.

(Be sure to answer each item. If the property is not subject to any such of items 1 to 6 inclusive, insert the word "none" following the item.)

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4:48

CUMBERLAND COUNTY

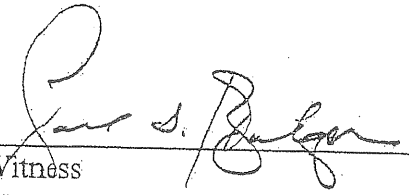
John B. O'Brien

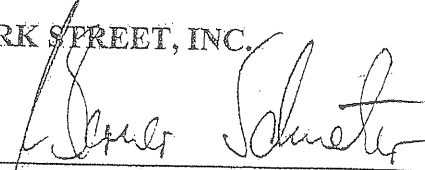
61274

ASSIGNMENT OF LEASES AND SECURITY DEPOSITS

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04106, for consideration, does hereby ASSIGN to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, its successors and assigns, all its right, title and interest in and to any and all leases, subleases and/or tenancies of the premises located on or about 54-62 York Street in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto, including, without limitation, any and all tenant security deposits relating thereto.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by Werner Schnetzer, its duly authorized President, on this 1st day of September, 1998.

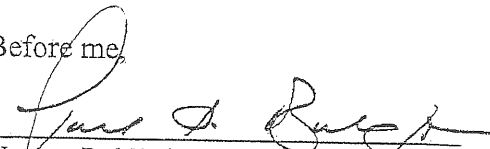

Witness

YORK STREET, INC.
By: 
Werner Schnetzer
Its: President

STATE OF MAINE
CUMBERLAND, SS.

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in said capacity and the free act and deed of said YORK STREET, INC.

Before me

Notary Public/Attorney-at-Law
Print Name: Paul S. Bussler

122937/PSB/SKLF
91401.4926

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4:52

CUMBERLAND COUNTY

John B. O'Brien

Section 5. Public or private Rights-of-Way

There is a 15' wide Right-of-Way across the southerly boundary of this parcel for a passageway in common with Portland Terminal Company. See the Existing Conditions Survey Plan in the Appendix.

Section 6. Schedule

Building remodeling is planned to begin in the fall of 2012.

TRAFFIC MOVEMENT
PERMIT APPLICATION

SECTION 7



FOR

PROPOSED
CHARTER HIGH SCHOOL
54 YORK STREET
PORTLAND, ME

ON BEHALF OF

RUFUS DEERING LUMBER
COMPANY
384 COMMERCIAL STREET
PORTLAND, ME

STATE OF MAINE
STEPHEN
S.
SAWYER, JR.
No. 3736
PROFESSIONAL ENGINEER
10/23/12

OCTOBER 23, 2012

Section 7. Traffic Study

1. Site Plan.

Rufus Deering Lumber Company, 384 Commercial Street, Portland is proposing to remodel their existing building at 54 York Street (the corner of York and Maple Streets) to accommodate the new Baxter Academy for Technology and Science Charter High School. The current property houses the existing Rufus Deering Lumber Kitchen and Millwork Showroom with other mixed uses of offices, labs, and a dance studio. This 0.22 acre (9,583 s.f.) lot is fully developed with an existing 7,540 s.f., 3-story building in the City's B-5b (Urban Business) Zone. This property is also located within the Old Port Historic District. See the Site Plan in the back of this submission.

2. Existing and Proposed Site Uses.

The existing building is currently developed with the following land uses:

- An exercise studio offering Zumba classes
- A Buddhist Center
- Administrative offices
- Global Biotech
- Deering Lumber's Kitchen and Millwork Showroom

Once the remodeling is complete the building will accommodate the Baxter Academy for Technology and Science Charter School as the primary tenant. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. The school's vision is to convert the entire building into a full 4-year high school over the next 3 years. Total enrollment is planned for 320 students by 2015. In the opening year (2013), 160 - first and second year students are envisioned. In year two, Grades 9, 10, and 11 will be provided, and in year three all four years of high school will be offered. It is estimated that 50% of the enrollment will come from Portland, South Portland and Westbrook, with the balance originating from communities outside of the urban core. School operations will run from 8:00 AM to 2:00 PM, resulting in peak traffic associated with the school being between 7-8:00 AM and 2-3:00 PM.

3. Site and Vicinity Boundaries.

A regional map showing the development site and its relationship to the adjacent street network as defined in MaineDOT's Chapter 305 of the General Rules of the Department of Transportation (Section 5B) is presented as part of the Site Plan located at the end of this submission.

4. Other Proposed Uses in the Vicinity of the Proposed Development.

A discussion with the City of Portland's Planning Department revealed that there are no other developments currently under review or have been permitted but not built in the vicinity of this Project at this time.

5. Trip generation.

ITE's Trip Generation, 7th Edition, was consulted to determine what industry data suggests for this type of land use. LUC 530 - High School, which includes both public and private institutions was deemed the closest category listed in this reference. Believing that the City's greatest traffic concern would be during the AM peak hour because it will coincide with the morning commute, we have focused our attention on this time period only. Using a base school population of 320 students and 30 employees, we find the following for the AM peak hour using ITE information:

<i>LUC 530</i>	<i>Variable</i>	<i>Total Trips</i>
By Student: $\ln(T) = 0.77\ln(X) + 0.69$	320 (students)	169
By Employee: Average Rate = 4.63	30 (employees)	139
Average of Above		154

The above trip generation assumes that there is no existing traffic associated with the site today, which is not entirely true. Currently there is an exercise studio (Nancy's) that offers among other things, Zumba classes, and a Buddhist Center (Nagaloka), and the administrative office for the Baxter School operating in the building. However, given the operating hours of these three uses, we have not taken a credit for any existing trips during the AM peak hour (7:00 AM to 8:00 AM).

From the data above it is estimated that the peak trip generation from the proposed development will be 154 trips (all primary), which will put the project in the Traffic Movement Permit (TMP) category of 100-200 Passenger Car Equivalent (PCE) trips.

6. Trip Distribution.

The ITE Trip Generation Handbook was also consulted with regard to the directional distribution of site generated trips during the AM peak period. ITE data suggests that during the AM peak period trips at the site will be distributed roughly 70% entering and 30% exiting regardless of which method of generation is used, i.e. "student" or "employee".

7. Trip Assignment.

While ITE suggests that traffic generation from the proposed facility could be in the range of new 150 trips in the AM peak hour, the Baxter Academy plans to adopt a series of travel demand management strategies for minimizing this impact on the surrounding street system. Typically, students at four-year public high schools have a variety of transportation choices that must be addressed in the school's planning. These range from parent drop-off and pick-up, to buses, and finally to student drivers with their own cars. The actual mix usually depends on the school's location within the community and the amount of available land surrounding the school. In the case of the Baxter Academy, the setting is very urban with no surplus land for off-street parking. Accordingly, the school's administration has formulated the following Transportation Plan.

First of all, it should be understood that in the initial year of operation, only 9th and 10th graders will be attending the Baxter Academy, so student drivers will not be part of the mix. However, in subsequent years this will be a consideration.

Not knowing exactly what to expect from prospective student families, Baxter Academy’s administration conducted a Parent Survey to gauge their transportation preferences. A total of 24 families participated in this survey, which was performed in June of 2012. See the Appendix. The results indicated that approximately 25% of the families preferred to transport their children to and from school either by individual or car pool means. 70% envisioned using bus transportation, either charter service provided by the school or public transportation because they reside within Greater Portland. The remaining 5% indicated they would walk to and from the school. Before running any numbers on this basis, contact was made with a few similar facilities in the region – the Academy for Science and Design in Nashua, NH – a 410 student charter school serving Grades 6-12 in the business district of Nashua; Waynflete in Portland, a private K-12 school with 550 students in the west end; and Portland High School on Cumberland Avenue with 920 students. Table-1 presents the results from this research.

Table 1 – Modal Choice by Families of 4-year Urban High Schools

<i>School</i>	<i>% Parent Drop-Off</i>	<i>% Transit</i>	<i>% Student Drivers</i>	<i>% Walkers & Bikers</i>
Academy for Science & Design	65	34	1	0
Portland High School	50	23	16	11
Waynflete School	50	30	14	6
Baxter Parent Survey	25	70	-	5
Percentages Used Adopted	30	50	15	5

From the data in Table 1, there is no clear model that works for all cases – each school’s experience seems to be unique depending on the school’s setting and its market service area. Therefore, the modal split percentages adopted for Baxter are a reasonable blend of parent feedback and other surveyed school’s experiences. Waynflete’s parent drop-offs appeared to be influenced by the younger population K-8. Portland High’s numbers are skewed due to the students all residing within the City where we expect many parents work. The Academy for Science & Design does not provide bus service but relies on the City’s School District for busing. The school’s setting is not conducive to walking or biking, and they only have 5 seniors with licenses at this point – hence the high percentage of parent drop-offs. Finally, the proposed Baxter Academy is planning to provide charter busing to outlying communities within its service area to reduce the need for parent shuttles.

The AM peak hour forecast below for the Baxter Academy is based on a student population of 320 students and 30 employees by Year 3 and 160 students and 15 employees in Year 1:

Year 1 (2013) – 160 students

30% parent drop off (assumed split of 50% car pooling with 3 students and 50% single occupant): 30% X 160 students = 48 students
24 students in single occupant vehicles = 48 trips
24 students in car pools of 3 = 16 trips
55% of students (88) ride 3 charter buses (1.5 PCE per bus) = 9 trips
10% of students (16) ride existing public transportation = 0 trips
5% of students (8) as walkers = 0 trips
*15 employees @ 67% auto use = 10 trips**
Total estimated AM peak hour trips in Year 1 = 73 trips
Anticipated Distribution = 37 arriving and 36 departing

Year 3 (2015) - 320 students

30% parent drop off (assumed split of 50% car pooling with 3 students and 50% single occupant): 30% X 320 students = 96 students

48 students in single occupant vehicles = 96 trips

48 students in car pools of 3 = 32 trips

40% of students (128) ride 3 charter buses (1.5 PCE per bus) = 9 trips

*15% of students (48) drive their own cars = 0 trips**

10% of students (32) ride existing public transportation = 0 trips

5% of students (16) as walkers = 0 trips

*30 employees @ 67% auto use = 20 trips**

Total estimated AM peak hour trips in Year 3 = 137 trips

Anticipated Distribution = 69 arriving and 68 departing

*Because the site cannot accommodate it, the School is not planning to provide on-site parking for students or employees. A total of 20 off-site parking spaces are initially going to be leased for employees, but since they are off-site their impact will not be felt at the site itself.

The School is proposing a full Traffic Management Plan to organize student arrivals and departures, which is detailed later herein. From a Trip Assignment perspective, though, this Plan will route arriving school buses from York Street onto Maple Street and load/unload curbside on the west side of the street in a designated School Drop Off Zone. The buses will depart the school using Maple Street to Commercial Street. Arriving parents dropping off their students will be directed to the drop off zone on westbound York Street opposite the front door of the school, which can be accessed by either leg of Maple Street and from York Street coming from the Old Port. Figures 2A and 2B illustrate the forecasted school traffic during the AM peak hour for both Years 1 and 3 at the York/Maple intersection under this traffic scenario. It is assumed that student arrivals will originate from all directions (south, west, and north) of the site and thus these trips have been assigned to the street network accordingly. Figure 3 illustrates the combined (existing and forecasted) trips at the York/Maple Street intersection during the two analysis periods.

8. Existing and Projected Traffic Volumes

Existing Conditions

Sebago staff performed a traffic turning movement count at the intersection of York Street and Maple Street during the hours of 7:00 AM and 8:00 AM on June 27, 2012 to observe traffic operations in and around the site during the time period when children would be arriving for the school day. Figure 1 presents the results of this peak hour count. It is readily evident that the volumes are quite small during this time period.

In addition to this data collection effort, several general observations were made while conducting the count. These were:

1. Only one on-street parking space was used on Maple Street between Commercial Street and York Street during the one hour time period observed. This vehicle was a van that had parked there overnight.

2. The paved parking lot for “The Cannery” that is accessed off Maple Street only had 8 cars enter it between 7:00 AM and 8:00 AM.
3. Maple Street is barely used during this time period. Total traffic during the hour observed was 34 vehicles. Never was a queue experienced waiting to enter onto York Street, and with the exception of one overnight parked vehicle, no on-street parking spaces were occupied.
4. The two on-street parking spaces to the west of the proposed school building on York Street were not occupied during the observation period.
5. While the traffic on York Street was predominantly in-bound, it appeared that the time period 7:00 AM to 8:00 AM might be before the actual peak morning commute hour.

Proposed Volumes

Traffic volumes for Year’s 1 and 3, as mentioned above, are presented in Figures 2A and 2B, with combined existing and proposed volumes illustrated in Figure 3.

9. Capacity Analyses

Synchro/SimTraffic was used to analyze existing traffic operations at the intersection of York and Maple Streets and it was determined that the Level of Service (LOS) was A.

Year 1 – 160 students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection’s LOS in Year 1. The LOS was found to be A and only slightly less than it is today under existing conditions. We modeled the parent drop-offs using appropriate peak hour factors.

Year 3 – 320 students

From the traffic volumes presented in Figure 3, a Synchro/SimTraffic model was used to calculate the York/Maple Street intersection’s LOS in Year 3. The LOS was found to still be A. We modeled the parent drop-offs using appropriate peak hour factors.

Based on the above analyses, capacity does not appear to be an issue during the AM in the vicinity of the school in either Year 1 or Year 3.

10. Traffic Signals

The applicant is not requesting the installation of a traffic signal in connection with its application, nor is one warranted at the York/Maple intersection.

11. Sight Distances

York Street has a posted speed limit of 25 mph. According to MaineDOT guidelines, the intersection sight distance for this speed is 200 feet. Maple Street as it intersects York Street from both directions is stop

controlled. Sight distances looking in both directions along York Street from both Maple Street approaches exceeds this 200 foot requirement. Vehicles exiting Maple Street onto York Street can clearly see both adjacent intersections - High Street is approximately 600 feet away and Danforth Street is roughly 400 feet away. Hence, sight distance at this intersection is not a problem.

12. Traffic Accidents

MaineDOT's Accident Records Unit was contacted to obtain the most recent 3-year crash information for the area in the vicinity of the proposed project. See attached materials from the Department in the Appendix. In summary, of the 12 intersections surveyed, only one is defined as a High Crash Location (HCL), i.e. a total of 8 crashes in three years and a Critical Rate Factor (CRF) greater than 1.0. This intersection is Commercial Street at High Street with a CRF of 1.50.

Road segments were also examined as evidenced by the enclosed Department data, and no segment immediately abutting the proposed project is identified as a High Crash Location.

Given that there is no HCL in the vicinity of the Project, crash history does not seem to be an influencing factor for this application.

13. Traffic Operations and Management Plan

Arrivals and departures from the Baxter Academy are to be well-orchestrated and staff managed events. Because students will be arriving via various modes of transportation the School's Plan will have a number of elements.

Arrivals

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. Students will depart directly onto the Maple Street sidewalk and proceed to the front door of the School on York Street.

Students using public transit (Metro or South Portland Bus) will depart buses at existing stops on the north side of York Street at either High Street or Fore Street. Students will then proceed to school on existing sidewalks and cross York Street at a repainted formal crosswalk at the intersection of York and Maple Streets. The School will have a staff person supervising this crosswalk for the 45 minutes preceding school's commencement to ensure safe student passage.

Parent drop-offs will occur on the north side of York Street opposite the front door of the school. The school will have a staff person supervising this operation to keep vehicles orderly, and students will be directed to the intersection of Maple and York, where there will be another staff member acting as a crossing guard for safe passage to the front entrance of the school.

Staff members who elect to drive their own vehicles to school will be parking in the Gulf of Maine parking lot off Commercial Street or the parking garage at the Holiday Inn By-The-Bay and will walk to school via existing sidewalks.

Students who elect to drive their own vehicles to school will likely park on Commercial Street in the unrestricted parking spaces adjacent to the end of Maple Street and will walk to school via existing sidewalks.

Students who elect to ride their bikes to school will be able to store their bikes in bike racks, which will be provided by the school adjacent to the building on Maple Street.

In addition to the staff members on-site managing the student arrival process, the school proposes to:

- use traffic cones to block off the curbside in front of the school to prevent vehicles from unloading on the south side of York Street
- place a School Crossing sign in the middle of the York Street crosswalk in front of the school to keep speeds on York Street under control during peak student usage
- erect School Drop Off Zone signs on Maple Street for the buses on the west side of the street and on the north side of York Street for parent drop-offs.

See examples below.



“Crosswalk sign used by Waynflete”



“School Drop-Off Zone Sign at Portland High”

Departures

Charter buses will arrive at school and park on the west side of Maple Street heading towards Commercial Street. At 2:00 PM when regular school is over students who are not participating in after school programs will load the buses from the Maple Street sidewalk as they did during arrivals.

Students using public transit (Metro or South Portland Bus) will depart the school for the bus stops on the north side of York Street at either High Street or Fore Street depending on individual choice and route schedules.

Parent pick-ups will occur on the north side of York Street opposite the front door of the school. The schedule of these pick-ups will not be as concentrated as in the morning because of after school activities. The school is projecting that up to one half of students will be participating in after school programs, which would mean that parent pick-ups in Year 3 would be in the range of 34 at 2:00 PM and the other 34 would be spread over the subsequent 2 hours. Given the 8 on-street spaces available this does not appear to be an issue.

Food Deliveries

Food service to the school will be provided by an off-site vendor. Deliveries will be made curbside on the west side of Maple Street, similar to where the buses will load and unload.

Proposed Signage Changes

Maple Street: The west side of the street is currently posted as 15-minute parking. This would need to be modified to allow for school bus parking between 7:30 and 8:00 AM and between 1:45 and 2:15 PM.

York Street: The north side of York Street opposite the school is currently posted as 30-minute parking. This would need to be modified to allow for 5 minute school drop-off and pick-up between 7:30 and 8:00 AM and between 2:00 PM and 4:00 PM.

On-Going Plan Refinement

The Baxter Academy realizes that their Traffic Management Plan is subject to individual student family preferences from year to year. As such, the school administration is committed to refining their operation each year to reflect the modal choices made by their enrollees. Given that Years 1 and 2 are ramping up years in terms of the student population, the school will have adequate time to fine-tune operations prior to reaching the ultimate goal of 320 students in Year 3.

14. Parking.

From the information presented above, the number of students and employees that may require parking in the vicinity of the school is 10 staff members in Year 1 and by Year 3 this may grow to 20 staff members and 48 students. The School has made leasing arrangements for between 20-40 off-street parking spaces in the Gulf of Maine lot off Commercial Street. Other options in surrounding parking garages also exist if this is not possible. The Baxter Academy is committed to reducing the number of students driving their cars to school. In this regard the School's informational packets will encourage families to use car pooling, public transportation, school provided charter buses, walking and biking to access the facility. To further support this policy, the school will be offering an incentive to students in the form of a 50-cent subsidy on public transit fares for those students within the Metro/South Portland Bus network.

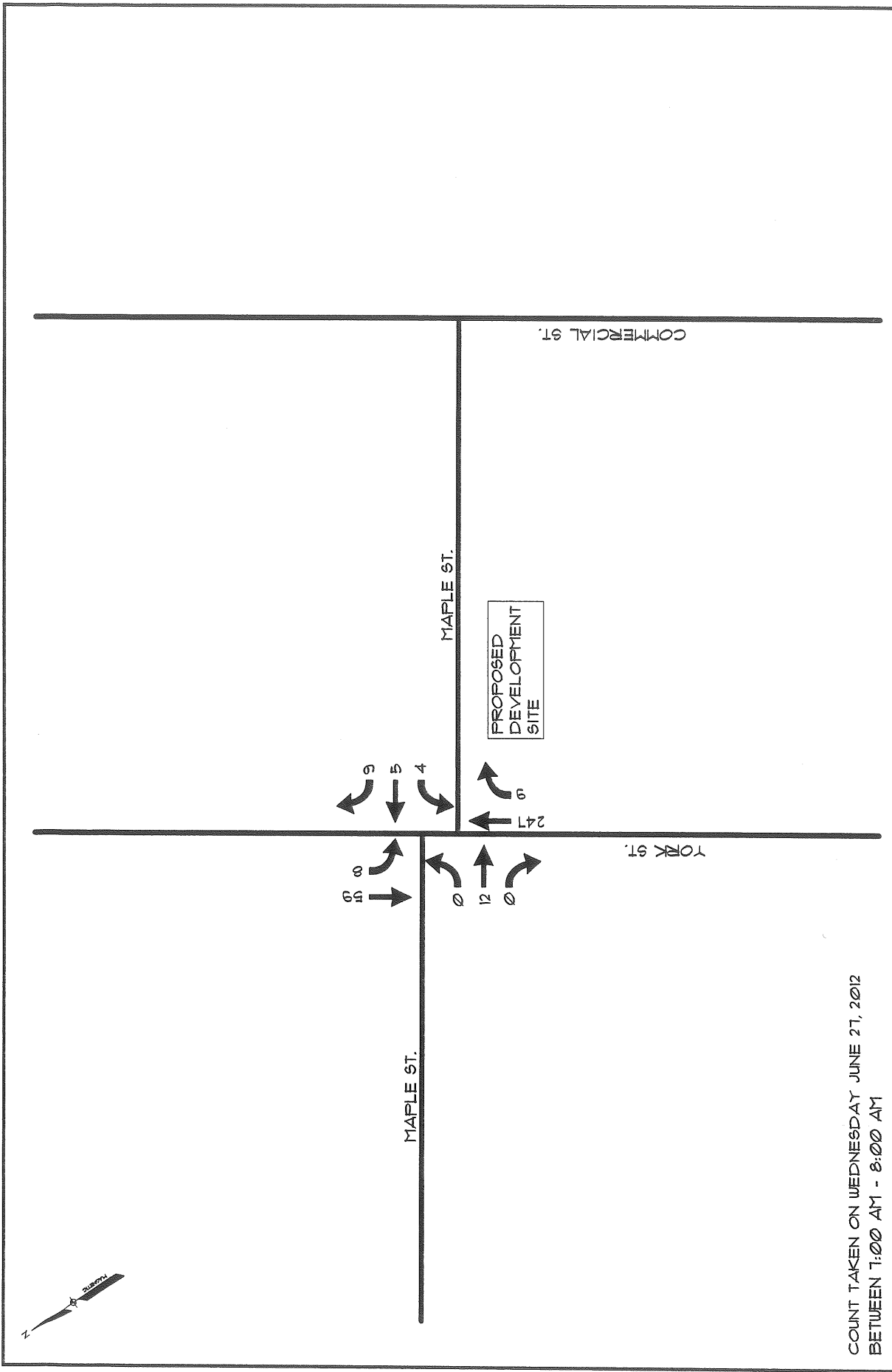
For those students in Years 2 and beyond that do not choose to avail themselves of public transportation, and want to bring their own vehicles to school, they will be required to register their vehicles with the school and park legally within the surrounding area. This is similar to Waynflete's policy. The school will monitor these situations, and work with the City to address any violators should they be brought to the school's attention.

15. Recommendations

From the enclosed information, the proposed Baxter Academy at the corner of York and Maple Streets does not present a capacity or safety issue for the City of Portland. The applicant has presented not only a thorough traffic analysis to support these findings, but also a Traffic Management Plan as well as an off-site Parking Plan to further minimize its traffic impact on the surrounding neighborhood. On this basis, it is the professional opinion of Sebago Technics that the City should grant a Traffic Movement Permit to this applicant.

16. Conclusion

Given the findings presented herein regarding the proposed charter school at York and Maple Streets, Sebago Technics believes that a Traffic Movement Permit should be issued for this development.



COUNT TAKEN ON WEDNESDAY JUNE 27, 2012
 BETWEEN 1:00 AM - 8:00 AM

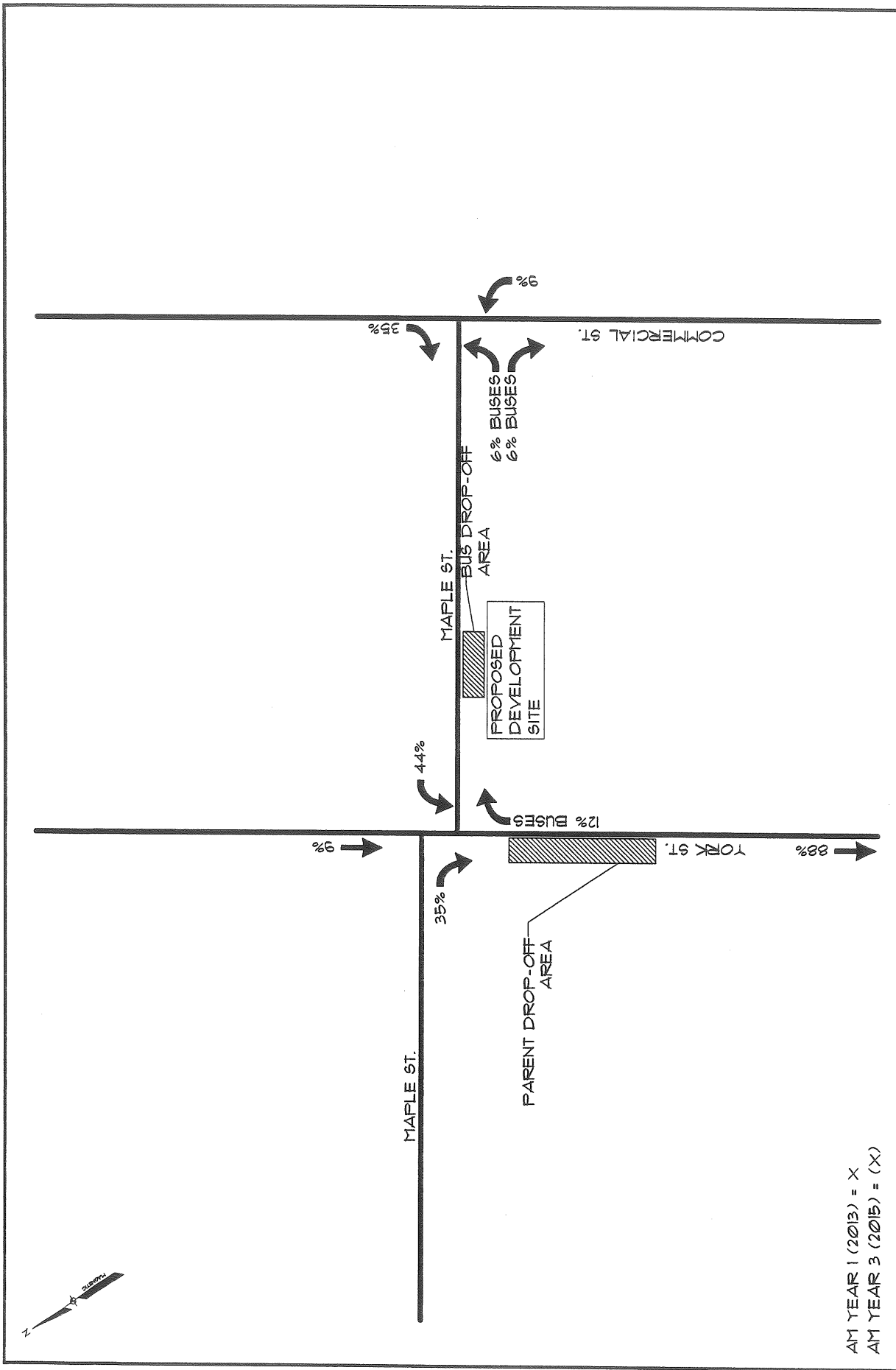
SEBAGO
 TECHNICS
 WWW.SEBAGOTECHNICS.COM
 75 John Roberts Rd. - Suite 1A 250 Goddard Rd. - Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel: 207-200-2100 Tel: 207-785-5656

EXISTING TRAFFIC VOLUMES
 YORK ST. @ MAPLE ST., PORTLAND, ME

LOCATION:
 YORK ST. @ MAPLE ST.
 PORTLAND, ME

FOR:
 RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

SCALE:	NTS
DATE:	10/23/12
FIGURE:	1



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

SCALE:	NTS
DATE:	10/23/12
FIGURE:	2A

AM NEW PRIMARY TRIPS - % ASSIGNMENT
 YORK ST. @ MAPLE ST., PORTLAND, ME

LOCATION:
 YORK ST. @ MAPLE ST.
 PORTLAND, ME

FOR:
 RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

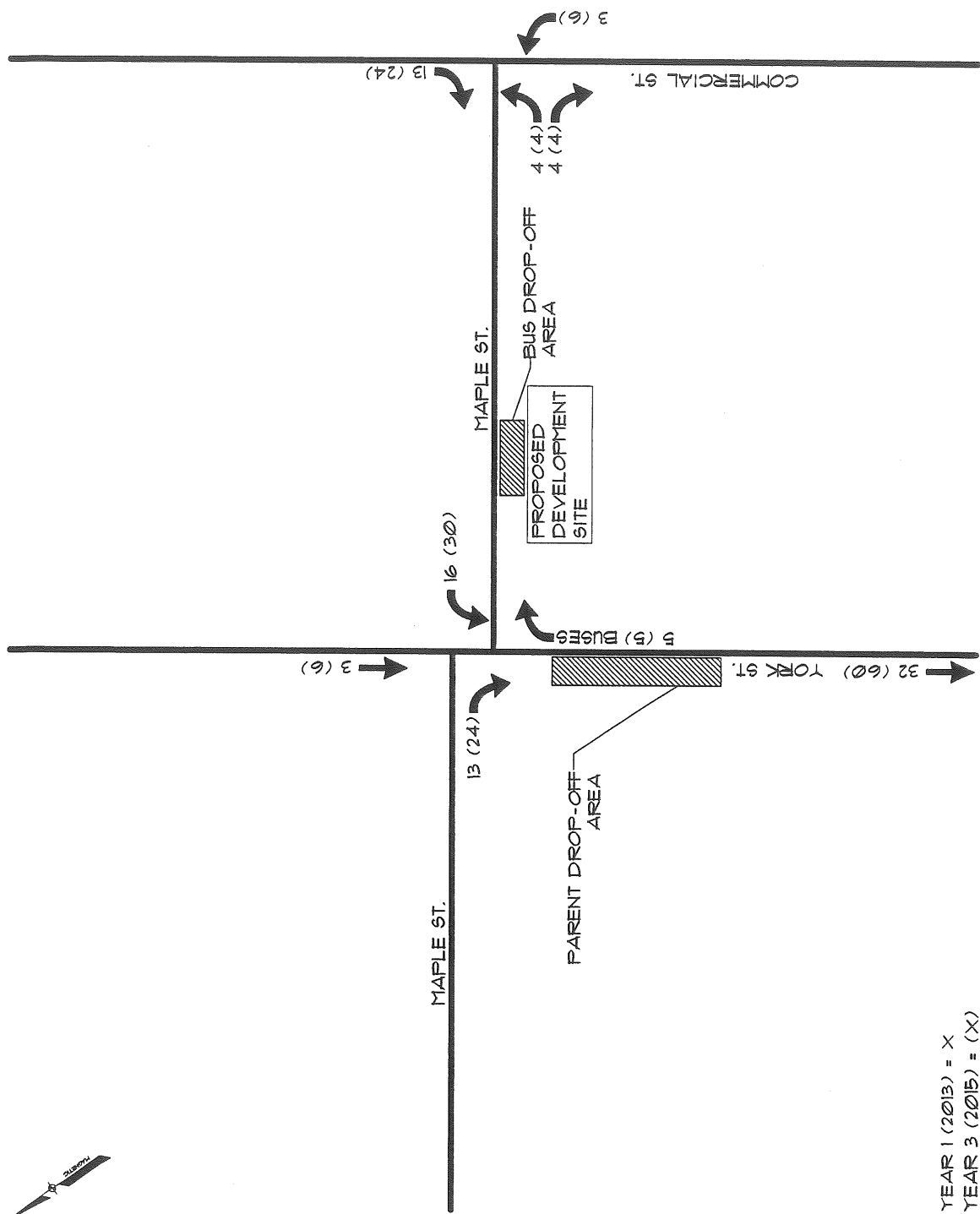
SEBAGO
 TECHNICALS

WWW.SEAGOTECHNICALS.COM

75 John Roberts Rd. - Suite 1A - 250 Goodard Rd. - Suite B
 South Portland, ME 04106 - Lewiston, ME 04240
 Tel. 207-200-2100



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

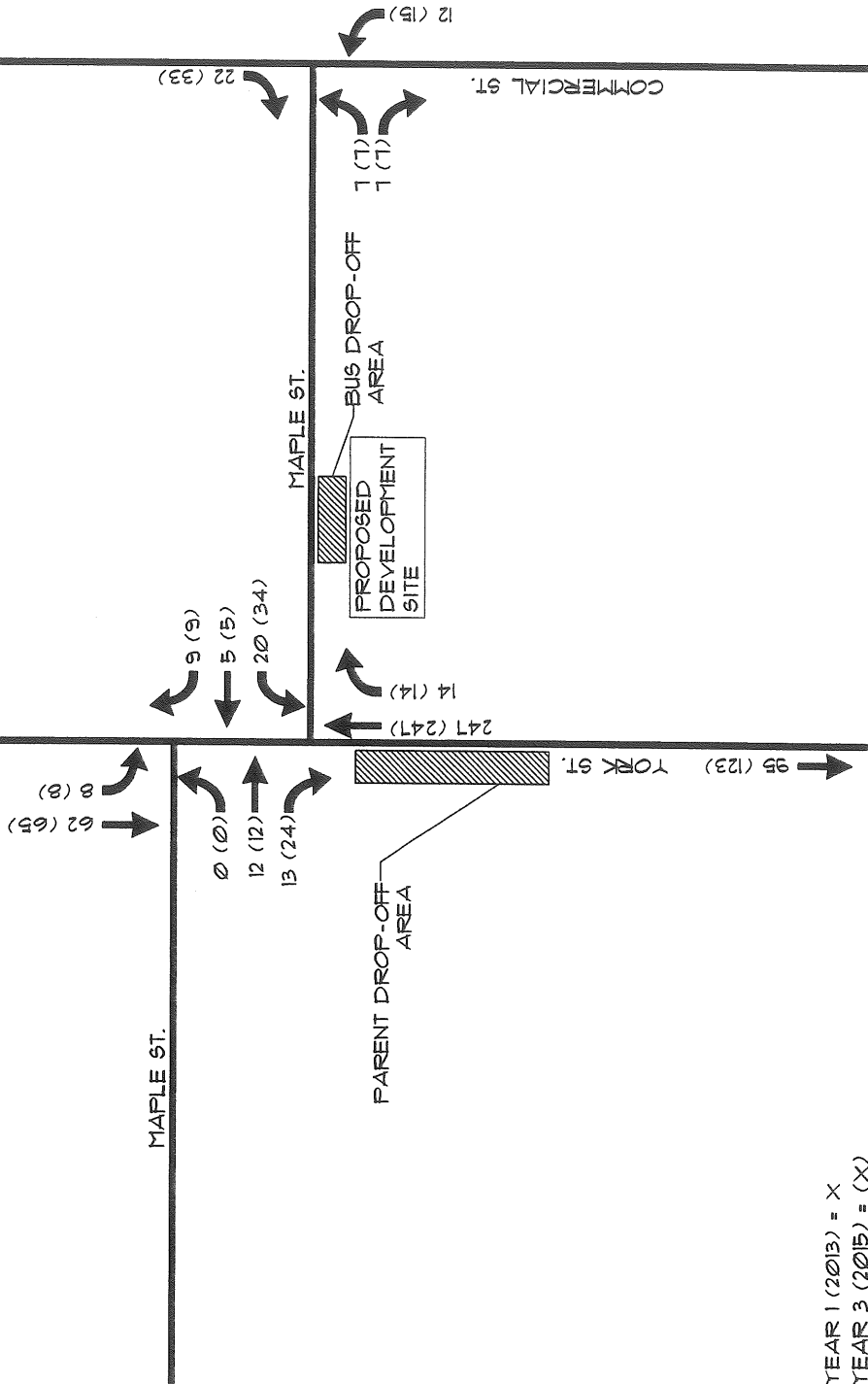
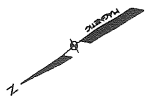


SCALE: NTS
 DATE: 10/23/12
 FIGURE: 2B

FOR: RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

LOCATION: YORK ST. @ MAPLE ST.
 PORTLAND, ME

SEBAGO
 TECHNICALS
 WWW.SEBAGOTECHNICALS.COM
 75 John Roberts Rd. - Suite 1A 250 Coddard Rd. - Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel. 207-200-2100 Tel. 207-783-5656



AM YEAR 1 (2013) = X
 AM YEAR 3 (2015) = (X)

SEBAGO
 TECHNICALS
 WWW.SEBAGOTECHNICALS.COM
 75 John Roberts Rd., Suite 1A 250 Goodard Rd., Suite B
 South Portland, ME 04106 Lewiston, ME 04240
 Tel: 207-200-2100 Tel: 207-785-5656

COMBINED AM TRIPS
 YORK ST. @ MAPLE ST., PORTLAND, ME

LOCATION: YORK ST. @ MAPLE ST.
 PORTLAND, ME

FOR: RUFUS DEERING LUMBER CO.
 383 COMMERCIAL ST.
 PORTLAND, ME

SCALE:	NTS
DATE:	10/23/12
FIGURE:	3

Shukria Wiar - Re: Baxter Academy Request for Sidewalk Waiver

From: Shukria Wiar
To: Steve Sawyer
Date: 1/4/2013 9:22 AM
Subject: Re: Baxter Academy Request for Sidewalk Waiver
CC: Barbara Barhydt; Dan LaBrie; James Seymour; John Jaques

Hello Steve:

Thank you for the sidewalk waiver memo. I have forwarded the document to Tom Errico and David Margolis- Pineo for their review.

There were a couple of other items that we needed to submit before the public hearing:

1. Traffic Signage Plan showing the propose signage on York and Maple Streets. We need to specifically as to what kind of sign that the applicant wants to put up in regard to drop off area, limitation of hours, etc.
2. 'Intent to Lease' agreement for the staff parking will need to be submitted before the public hearing.

The original application that you had submitted was for a preliminary plan dated 06.05.2012. We will need a final application to be submitted for the public hearing. There have been many changes since the preliminary plan and we need to have all these changes in one final packet. This needs to be submitted asap.

If you have any questions, please let me know.

Thank you.

Shukria

>>> "Steve Sawyer" <ssawyer@sebagotechnics.com> 1/3/2013 1:29 PM >>>
 Shukria -

Attached is a memo requesting a "sidewalk waiver" for the subject project. Please include this in your packet to the Planning Board, and confirm for us that we are on your agenda for the 22nd. Thanks!

Stephen S. Sawyer, Jr. P.E.
 Vice President Transportation Services



www.sebagotechnics.com
 An Employee Owned Company

New Address (effective 5/21/12):
 75 John Roberts Road - Suite 1A
 South Portland, ME 04106-6963

Office: 207.200.2100
 Direct Dial: 207.200.2082

Memorandum

Project: 07383
To: Shukria Wiar
From: Steve Sawyer
Date: January 3, 2013
Subject: Baxter Academy for Technology and Science
- Sidewalk Waiver Request
CC: Dan LaBrie, John Jaques

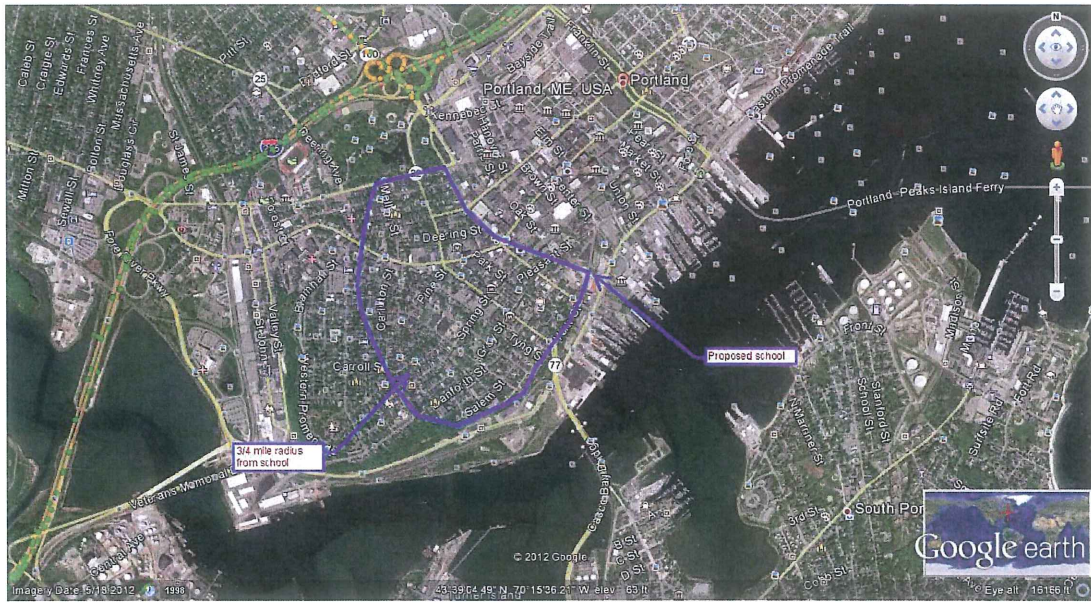
Steve

This memorandum is a follow up to our meeting with you and the staff of Public Services on December 11, 2012. At this meeting it was explained that if the applicant did not agree with staff's recommendation to construct a new sidewalk on the south side of York Street between High Street and Maple Street that we should apply for a "sidewalk waiver" under the provisions Section 14-506 (b). Under this provision in the City's Ordinances, the Portland Planning Board may grant a waiver from the requirement for a sidewalk providing two or more conditions listed in the Ordinance are met. We have prepared the following to satisfy these guidelines.

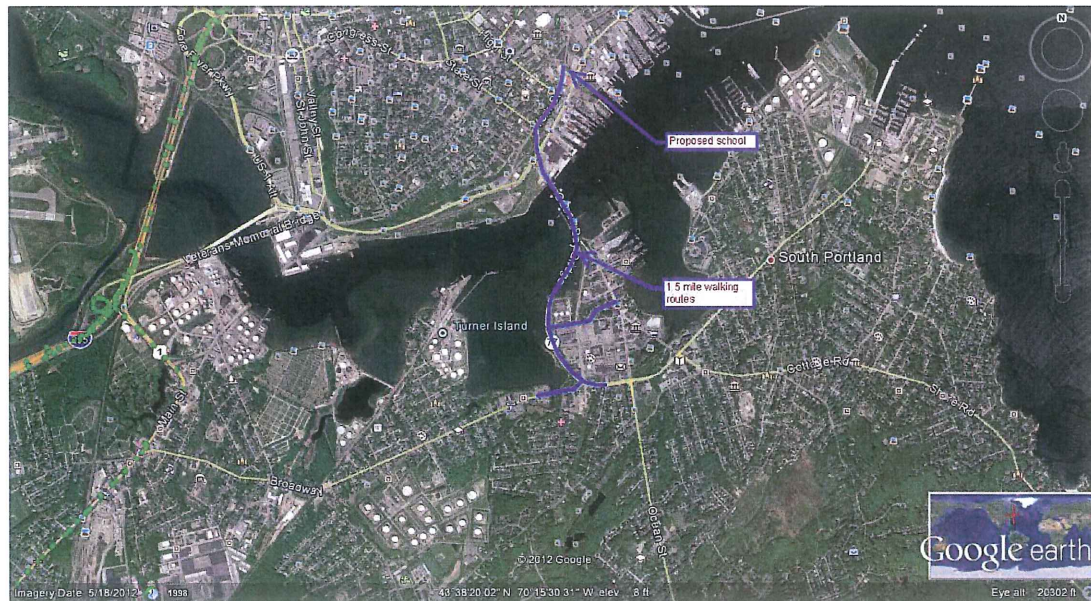
Sidewalks Condition 1 - There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site.

We provided you as part of a December 3, 2012 memo our viewpoint on the usage of a sidewalk on the south side of York Street for the proposed school. This explanation is repeated here for the Planning Board's reference.

Baxter Academy has presented an enrollment profile as part of its application materials. In Year 3 (full build-out) there is an expectation because of its urban setting that as many as 16 students and staff will walk to school. If we look at a map, see Google image below, we see that Portland's closest residential neighborhoods (within 0.75 miles) are all located on the north side of York Street in the general direction of the West End. There is an existing sidewalk on the north side of York Street that is available to walkers originating from these neighborhoods, and the school is proposing to upgrade the crosswalks in front of the school to permit safe crossings of York Street at the school's main entrance.



Who then would use a sidewalk if it were to be constructed on the south side of York Street from High Street to Maple Street? We have heard that it would be students and staff crossing the Casco Bay Bridge. If this is the case let's look at the route this would involve. From the Google image below you can see that to access the nearest residential neighborhoods in South Portland involves a walking route that would exceed 1.5 miles. In our view, it is unlikely that students would walk in excess of 1.5 miles across the bridge to get to and from school on a regular basis. Therefore, we do not believe that the Baxter Academy will be attracting walkers from South Portland given the distance involved. As such, we do not see the nexus between the school and the continuation of the sidewalk on the south side of York Street to High Street. Without one, we believe it is unreasonable to burden our project with this expense.



Condition 3 - A safe alternative-walking route is reasonably and safely available, for example, by way of a sidewalk on the other side of the street that is lightly traveled.

See the response to Condition 1 above - there is an existing sidewalk on the north side of York Street in this location.

In addition to a "sidewalk waiver", the applicant has also reviewed the conditions for satisfying a "curbing waiver" in the Ordinance. Here we find that Condition 1 refers to the cost to construct the curbing as being less than 5% of the overall project cost. The City has filed a MaineDOT grant application for the sidewalk on York Street in the amount of \$110,000. At our meeting on December 12, we were presented an estimate of \$85,000 for this construction, not including survey and engineering. Furthermore, staff requested that the applicant be required to construct a new brick sidewalk on Maple Street from its project to Commercial Street at an estimated cost of \$13,000. Combined, the staff is asking for sidewalk construction that is estimated by their figures to cost \$98,000 to build, not including survey and engineering. In our view, the total cost is more like \$120-125,000. The cost of the proposed project is expected to run in the range of \$185,000, which would put the requested sidewalk costs at 65-68% of the total project cost, which is far greater than the Ordinance figure of 5%. This expense seems considerably out of proportion and supports our position for a waiver of these requirements.

Item 1 – Student drop-off and pick-up locations.

- Signage for bus parking on York Street- School Parking limited to 7:30 to 8:00 AM and 2:00 to 2:30 PM - *Parking signage on Maple Street*
- Anti-Idling Ordinance (?) - *signage needs UP.*
- Condition of Approval- monitoring every three month and after a year, monitoring on annual basis.
- The applicant will be responsible for snow removal on York and Maple Streets for the student drop off purposes. - *Applicant agrees to snow removal / Rufus Deering will do it.*
- Re-stripe the center line on York Street. This will give a little more room on the Baxter Academy school side. *May be needed.*

Item 2 – Provide a TDM Plan for the Project.

- *Discount on METRO for students.*

Item 3 – Off-Site Parking. →

- We need an 'Intent to Lease' agreement before a public hearing
- Holiday Inn may be a better option
- Present the routes from the proposed parking structures
- There needs to be more parking spaces proposed for the staff: 20 for the first year, 20 for the second year. The parking will also be part of the monitoring conditions.
- As for the student parking, we need to know where exactly the parking will be and what routes the students will travel to get to the school. With the proposal of a hotel on corner of Maple and Commercial Streets, some of the on-street parking will be regulated.

Item 4 – The sidewalk on the south side of York Street from High Street to the School site.

- Sidewalk on York Street
- There is a need for the sidewalk on York Street.
- Sidewalk Waiver

Item 5 – Maple Street parking restrictions for student unloading and loading.

- It is recommended that these parking spaces stay 15 minutes parking.

Item 6 – Crosswalk Pavement Markings.

- Crosswalks on York and Maple Streets
- As proposed by the applicant is satisfactory
- The crosswalks need to be reviewed by the Crosswalk Committee

Item 7 – Bicycle Racks.

Relocate the some of the bicycle racks closer to the main entry. The racks are allowed to be located on the City's ROW. The current location is far and students may not use them.

Item 8 – Handicapped Parking Spaces on York Street. -no comment needed

Item 9 – Intersection of High and Commercial Streets. → *contribution towards this intersection.*

↳ *Applicant cannot contribute an amount.*

City of Portland

TRAFFIC MOVEMENT PERMIT SCOPING MEETING

Permit Category: X 100 - 200 PCEs 200 + PCEs
 New Modification

Attendance: _____ Date: 10/10/2012

Tom Errico , Barbara Barhydt, Shukria Wiar, Steve Sawyer,
 John , Jim Seymour
 27th of Oct. - public notice
 neighborhood notified.

Meeting Location: Planning Department, City Hall

Project Name: Charter High School Project
Address: 54 York Street

Applicant: Rufus Deering Lumber Company
Address: 384 Commercial Street

Project Engineer: Steve Sawyer, Sebago Technics
Address: _____

Portland Traffic Engineer (reviewer): Tom Errico

Filings: Has the Applicant notified abutters via certified mail at least 7 days prior to this meeting? _____
Has the "Notice of Intent to File" been published in a local newspaper? _____

Section 1 - Site and Traffic Information

1A. Site Plan

Size of development parcel (acres): 0.22 acres (9,583 sq. ft) .
Size of development parcel to be left non-vegetated (acres): _____

1B. Existing and Proposed Site Uses

Type of development:

<u>USE</u>	<u>SIZE</u>
High School	7,540 sf
_____	_____
_____	_____

1C. Site and Vicinity Boundaries

- Boundary or title survey signed and sealed by a professional land surveyor
- Vicinity map provided

1D. Proposed Uses in the Vicinity of Proposed Development

Other approved development(s): _____

1E. Trip Generation

Summary tables for each land use code:

ITE (LUC)	Weekday	AM Peak	PM Peak	Saturday Peak
_____	_____	_____	_____	_____
_____	_____	_____	_____	_____
_____	_____	_____	_____	_____
_____	_____	_____	_____	_____

Trip rates obtained from other sources:

Number of locations where driveway counts taken: _____

Dates & time periods when driveway counts taken: _____

Locations where driveway counts were taken: _____

1F. Trip Distribution

_____ Stick diagram for each major intersection on either side of the development driveway(s)?

_____ ITE trip generation handbook

_____ Actual survey done? Location? _____

_____ Other. Explain. _____

Comments: _____

1G. Trip Assignment

_____ Stick diagram for each major intersection on either side of the development driveway(s)?

Percent primary trips _____

Percent passer-by trips _____

Percent diverted trips _____

Traffic Movement Permit Scoping Meeting

Project Name: Charter High School, 54 York Street

Basis for using above-listed percentages:

_____ Existing traffic patterns of adjacent street

_____ Zip code survey

_____ Gravity model

_____ Other. Explain. _____

Comments: _____

Section 2 - Traffic Crashes

3 Year(s) of MDOT crash records for study area

1 Number of high crash locations

_____ Collision diagrams provided

Mitigation provided for each high crash location: _____

Other traffic problems: _____

Comments: _____

Section 3 - Development Entrances and Exits

3A. Entrance and Exit Locations

_____ Distance to nearest intersecting road or town line (to the nearest hundredth of a mile)

1 Number, width and surface of each proposed entrance/exit

3B. Plan View of Each Intersection Created

Names of intersecting road _____

Posted speed limits _____

Entrance/Exit Sight Distance:

Left

Right

Usage and location of all driveways and roads located adjacent to the development site:

3C. Entrance/Exit Design

- Lighting
- Driveway spacing and corner clearance (Access Management)
- Adequate sight distance for vehicles exiting development
- Entrance grade (see fact sheet)
- Entrance/exit widths less than 42 feet (12.8 meters)
- Left turn lane
- Right turn lane
- Signal warranted
- Lane encroachment
- Separation islands (see fact sheet)
- Drainage study using 50 year storm for culverts and to connect to MDOT system
- Capacity Analysis
- Queuing Analysis
- Study to be submitted to MDOT Division Engineer

Comments: _____

Section 4 - Title, Right or Interest

- Title, right or interest in project site
- Title, right or interest in entrance/exits
- Title, right or interest in drainage easement affecting MDOT

Section 5 - Public or Private Rights-of-Way

- Location and width of proposed streets, easements and other public or private rights-of-way
- No signs, structures or pavement connected to the entrance

Section 6 - Schedule

Project schedule: September 2013 (After of Labor Day to Middle of June - depending on snow days)

Further study items required (\$500.00 additional fee): _____

Section 7 - Full Traffic Study

_____ \$1,500.00 additional fee required

Build-out year (Phase 1): _____
Build-out year (Full Occupancy): 4 yrs

FS 1. Time Period(s) for Traffic Engineering Analysis

_____ AM weekday peak hour of adjacent street.
_____ Noontime weekday peak hour of adjacent street
_____ PM weekday peak hour of adjacent street.
_____ Noontime Saturday peak hour of adjacent street.
_____ Other. Explain. _____

FS 2. Background Annual Traffic Growth Rate

Type of counts taken:
_____ Peak hour
_____ AADT
_____ Base counts less than 2 years old?

FS 3. Study Area to Include the Following Intersections

York K Street Intersections

Additional intersection(s) required if one hour volumes from development are:
25 vehicles in left turn only lane
35 vehicles in through, right turn lane or combined through and right turn
35 vehicles (multiplying the left turn volume by 1.5) in a combined left turn and through lane, or a combined left turn, through and right turn lane

FS 4. Intersection Capacity Analysis

_____ Isolated
_____ Interconnected. Intersection(s) _____
Software package (isolated): _____
Software package (interconnected): _____

FS 5. Analyze or Evaluate the Following

	<u>Location</u>
_____ Left turn lane warranted	_____
_____ Right turn lane warranted	_____
_____ Traffic signal warranted	_____
_____ Sight distance evaluation	_____
_____ Truck climbing lane warranted	_____
_____ Truck/RV turning radii evaluation	_____
_____ Investigation of HCL	_____

Traffic Movement Permit Scoping Meeting
Project Name: Charter High School, 54 York Street

FS 6. Other Development Traffic To Be Included in Study

None

FS 7. List Location(s) and Date(s) of Completion of Other Projects and Their Traffic Engineer

Other Items:

City of Portland

TRAFFIC MOVEMENT PERMIT SCOPING MEETING

Permit Category: X 100 - 200 PCEs 200 + PCEs
 New Modification

Attendance: _____ Date: 10/10/2012

Meeting Location: Planning Department, City Hall

Project Name: Charter High School Project

Address: 54 York Street

Applicant: Rufus Deering Lumber Company

Address: 384 Commercial Street

Project Engineer: Steve Sawyer, Sebago Technics

Address: _____

Portland Traffic Engineer (reviewer): Tom Errico

Filings: Has the Applicant notified abutters via certified mail at least 7 days prior to this meeting? _____
Has the "Notice of Intent to File" been published in a local newspaper? _____

Section 1 - Site and Traffic Information

1A. Site Plan

Size of development parcel (acres): _____

Size of development parcel to be left non-vegetated (acres): _____

1B. Existing and Proposed Site Uses

Type of development:

<u>USE</u>	<u>SIZE</u>
High School	7,540 sf
_____	_____
_____	_____

1C. Site and Vicinity Boundaries

- Boundary or title survey signed and sealed by a professional land surveyor
- Vicinity map provided

1D. Proposed Uses in the Vicinity of Proposed Development

Other approved development(s): _____

1E. Trip Generation

Summary tables for each land use code:

ITE (LUC)	Weekday	AM Peak	PM Peak	Saturday Peak
_____	_____	_____	_____	_____
_____	_____	_____	_____	_____
_____	_____	_____	_____	_____
_____	_____	_____	_____	_____

Trip rates obtained from other sources:

Number of locations where driveway counts taken: _____

Dates & time periods when driveway counts taken: _____

Locations where driveway counts were taken: _____

1F. Trip Distribution

_____ Stick diagram for each major intersection on either side of the development driveway(s)?

_____ ITE trip generation handbook

_____ Actual survey done? Location? _____

_____ Other. Explain. _____

Comments: _____

1G. Trip Assignment

_____ Stick diagram for each major intersection on either side of the development driveway(s)?

Percent primary trips _____

Percent passer-by trips _____

Percent diverted trips _____

Traffic Movement Permit Scoping Meeting
Project Name: Charter High School, 54 York Street

Basis for using above-listed percentages:

- _____ Existing traffic patterns of adjacent street
- _____ Zip code survey
- _____ Gravity model
- _____ Other. Explain. _____

Comments: _____

Section 2 - Traffic Crashes

- _____ Year(s) of MDOT crash records for study area
- _____ Number of high crash locations
- _____ Collision diagrams provided

Mitigation provided for each high crash location: _____

Other traffic problems: _____

Comments: _____

Section 3 - Development Entrances and Exits

3A. Entrance and Exit Locations

- _____ Distance to nearest intersecting road or town line (to the nearest hundredth of a mile)
- _____ Number, width and surface of each proposed entrance/exit

3B. Plan View of Each Intersection Created

Names of intersecting road _____

Posted speed limits _____

Entrance/Exit Sight Distance:	Left	Right
_____	_____	_____
_____	_____	_____
_____	_____	_____
_____	_____	_____

Usage and location of all driveways and roads located adjacent to the development site:

3C. Entrance/Exit Design

- _____ Lighting
- _____ Driveway spacing and corner clearance (Access Management)
- _____ Adequate sight distance for vehicles exiting development
- _____ Entrance grade (see fact sheet)
- _____ Entrance/exit widths less than 42 feet (12.8 meters)
- _____ Left turn lane
- _____ Right turn lane
- _____ Signal warranted
- _____ Lane encroachment
- _____ Separation islands (see fact sheet)
- _____ Drainage study using 50 year storm for culverts and to connect to MDOT system
- _____ Capacity Analysis
- _____ Queuing Analysis
- _____ Study to be submitted to MDOT Division Engineer

Comments: _____

Section 4 - Title, Right or Interest

- _____ Title, right or interest in project site
- _____ Title, right or interest in entrance/exits
- _____ Title, right or interest in drainage easement affecting MDOT

Section 5 - Public or Private Rights-of-Way

- _____ Location and width of proposed streets, easements and other public or private rights-of-way
- _____ No signs, structures or pavement connected to the entrance

Section 6 - Schedule

Project schedule: _____

Further study items required (\$500.00 additional fee): _____

Section 7 - Full Traffic Study

_____ **\$1,500.00 additional fee required**

Build-out year (Phase 1): _____

Build-out year (Full Occupancy): _____

FS 1. Time Period(s) for Traffic Engineering Analysis

_____ AM weekday peak hour of adjacent street.

_____ Noontime weekday peak hour of adjacent street

_____ PM weekday peak hour of adjacent street.

_____ Noontime Saturday peak hour of adjacent street.

_____ Other. Explain. _____

FS 2. Background Annual Traffic Growth Rate

Type of counts taken:

_____ Peak hour

_____ AADT

_____ Base counts less than 2 years old?

FS 3. Study Area to Include the Following Intersections

Additional intersection(s) required if one hour volumes from development are:

25 vehicles in left turn only lane

35 vehicles in through, right turn lane or combined through and right turn

35 vehicles (multiplying the left turn volume by 1.5) in a combined left turn and through lane, or a combined left turn, through and right turn lane

FS 4. Intersection Capacity Analysis

_____ Isolated

_____ Interconnected. Intersection(s) _____

Software package (isolated): _____

Software package (interconnected): _____

FS 5. Analyze or Evaluate the Following

Location

_____ Left turn lane warranted _____

_____ Right turn lane warranted _____

_____ Traffic signal warranted _____

_____ Sight distance evaluation _____

_____ Truck climbing lane warranted _____

_____ Truck/RV turning radii evaluation _____

_____ Investigation of HCL _____

FS 6. Other Development Traffic To Be Included in Study

FS 7. List Location(s) and Date(s) of Completion of Other Projects and Their Traffic Engineer

Other Items:

MaineDOT Quality Community Programs 2012-2013 Application
SECTION 3.1 - Estimated Cost of Infrastructure Project

PROJECT: York Street - High Street to Maple Street

ITEM NO.	ITEM	QUANTITY	UNIT	UNIT PRICE	SUBTOTAL
1	Environmental Evaluation (N/A for this project)	0	LS		\$0
2	Preliminary/Final Design Engineering (assume 15% of Item No 5)	1	LS		\$16,275
3	MaineDOT Proj Design Admin (5% of Item No 2, per application instructions)	1	LS		\$814
4	Right of Way/Land Acquisition (N/A for this project)	0	LS		\$0
5	Construction				
	5.1 Mobilization-Traffic Control Etc	1	LS	\$12,000.00	\$12,000
	5.2 Brick Sidewalk	345	SY	\$120.00	\$41,400
	5.3 Vertical Curb	620	EA	\$35.00	\$21,700
	5.4 Drainage Adjustments	1	LS	\$3,500.00	\$3,500
	5.5 Pipe Replacement	20	LF	\$45.00	\$900
	5.6 ADA Ramp w/Truncated Domes	1	EA	\$3,000.00	\$3,000
	5.7 Pavement Markings	1	LS	\$1,500.00	\$1,500
	5.8 Pedestrian Signals	1	LS	\$2,800.00	\$2,800
	Subtotal:				\$86,800
	Contingency (assume 25%):				\$21,700
	SUBTOTAL, Construction:				\$108,500
6	Construction Engineering (assume 10% of Item No 5)				\$10,850
7	MaineDOT Constr Admin (10% of Item No 6, per application instructions)				\$1,085
8	Total Value of Project (Lines 1 thru 6)				\$136,439
	Note: Estimate does not include utility relocations.				
CLIENT:	City of Portland				
DATE:	June 30, 2010				
BY:	GV-City of Portland				



MaineDOT

Maine Department of Transportation QUALITY COMMUNITY PROGRAMS Fiscal Years 2012-2013 Application

Date Application Received
(For MaineDOT Use Only)

Quality Community Program Eligibility:

Please indicate which Quality Community Program(s) your project is eligible for. Please place an "x" next to all that apply:

- Transportation Enhancement Program (Up to 80% of total project value)
- Safe Routes to School Program (Up to 100% of total project value, within 2 miles of a K-8 school)
- Other Program (please list):

Section 1: General Information

Applicant Name(s): City of Portland, Maine		
Contact Person: Katherine Earley, P.E., City Engineer		
Mailing Address: Department of Public Services, 55 Portland Street		
City: Portland	State: ME	Zip: 04101
Daytime Phone No.: 207-874-8830	Email: kas@portlandmaine.gov	
NOTE: The following sections of this application request specific project related information. If warranted, pictures, maps, exhibits, diagrams, survey summaries, etc., should be included with the application. Please be concise. If additional space is required, please attach supplemental sheets.		

Section 2: Project Brief

Location of Project: Provide town/city, street name(s) and additional project location references. Attach map showing location of the proposed improvements. Indicate area affected or linked to the proposed improvement(s) especially noting valued community resources, including neighborhoods, schools within two (2) miles, businesses, and village areas.

City of Portland; York Street between High St and Maple St in Downtown Portland commercial district/neighborhood. See attached map.

Project Summary (*Outline proposed improvements in 30 words or less*): Project will fill strategic gap in sidewalk/pedestrian signal network that provides regional connectivity between Casco Bay Bridge/South Portland and Downtown Portland. Striping and signing will also improve bikeway network.

Section 3: Project Details

Section 3.1 - Estimated Cost of Infrastructure Project

Environmental Evaluation:	1. \$ 0
Preliminary/Final Design Engineering:	2. \$ 16,275

MaineDOT Project Design Administration (5% of Preliminary and Final Design line above)	3.	\$ 814
Right of Way/Land Acquisition:	4.	\$ 0
Construction:	5.	\$ 108,500
Construction Engineering:	6.	\$ 10,850
MaineDOT Construction Administration (10% of Construction Engineering line above)	7.	\$ 1,085
Total Value of Project (add lines 1 through 6):	8.	\$ 136,439

Local Match/Voluntary Contributions (Note: All match/voluntary contributions must be detailed. Bonus consideration may be given to applications that offer additional match/voluntary contributions beyond any applicable required local match.)

- Cash: 9. \$ 27,288
- In-kind, right of way, or other contributions: 10. \$ 0
- Please describe local match in detail:

Total Local Match/Voluntary Contributions: % 20 11. \$ 27,288

Funds Requested from MaineDOT: 12. \$ 109,151

Note: The sum of Lines 11 and 12 should equal Line 8 above.

Note: Section 3.2 of this form also requires further details on costs. The amount shown on line 8 on the above estimate should match the total outline of costs in Section 3.2, or an explanation must be given for the discrepancy.

Section 3.2 - Detailed Description of Infrastructure Project and Cost (if applicable)

Provide a detailed description of project work items and/or construction costs from Section 3.1. Note that all infrastructure improvements must comply with the requirements of the Americans with Disabilities Act (ADA). See attached.

Section 3.3 - Project Description and Demonstrated Needs

Describe the proposed project and scope: *Attach diagrams, maps and/or correspondence that will help provide a clear description of the proposed scope.*

This project includes the following:

- Installation of granite curb and 5' wide brick sidewalk within existing street
- Establish crosswalks with signals for ADA crossing alignment compliance
- Adjust drainage to new curb line
- Establish shoulder for bicycle traffic
- Installation of sidewalk ramps with truncated domes
- Repaint crosswalks

See attached graphic.

Describe why this project is important to your community/region and worthy of funding:

If funded, this project will leverage other funds to support its efforts, thus resulting in both projects having a much broader and deeper community impact. This project supports and it will be supported by efforts under Portland's Communities

Putting Prevention to Work (CPPW) grant. Portland has received \$1.8 million for obesity prevention efforts under the America's Recovery and Reinvestment Act and the workplan calls for the creation of a Walk and Bike to School Policy as well as the promotion of Safe Routes to School. Portland will be able to leverage CPPW funds to support signage and promotional materials for parents and families; however the Quality Community funds are needed to support the infrastructure necessary to result in more children walking and biking to school. Additionally, the CPPW grant is funding a staff person to facilitate Portland's Safe Routes to School Committee. This Committee has been meeting in preparation for this application and the support of a staff person from CPPW will ensure that this committee remains robust and is able to support the projects outlined in this Quality Communities application. Finally, Portland's CPPW grant has made a commitment in time and resources to ensure that the City adopts policy and environmental changes that make sure our City's roads take in to account the needs of all users. These CPPW funds will allow for the creation of such policies and plans so that we are prepared to implement infrastructure improvements as funding becomes available. These Quality Communities funds would in turn provide those funds for infrastructure improvements.

Describe the Transportation Values: *Each project should serve primarily transportation purposes, as opposed to recreation purposes. A project serves valid transportation purposes if it serves as a connection between origins and destinations, increases safety, or enhances the use of the transportation system and the transportation environment.* By filling a significant gap in the sidewalk network, this project will provide a needed safe, convenient and attractive pedestrian connection between Downtown Portland and the Casco Bay Bridge. Many pedestrians per day rely on this connection for job access between the two communities.

Describe the Positive Impacts on Community:

There will be significant health impacts on the community as a result of this Quality Community Program. In Cumberland County 58% of adults are overweight or obese, and 1 in 3 kindergarteners are above a healthy weight. Lack of regular physical activity is scientifically proven as one of the major contributors to this epidemic rise in our obesity rates. The efforts of this Quality Community Grant will be working in collaboration with the Communities Putting Prevention to Work grant in order to increase access to safe and attractive places for our community residents and visitors to bike, walk and be active. The projects proposed in this application are critical to this progress and goal. We need to make the healthy choice the easy choice for our community, and the projects proposed in this grant will make it easier for more residents to commute on all modes of transportation, including foot and bike.

Describe Regional Benefits:

This project will complete the pedestrian link between the Knightville/Millcreek area of South Portland and the Portland Peninsula/Downtown via the Casco Bay Bridge. It will also provide a quality linkage between the two communities' trail networks.

Describe the Demonstrated Needs:

Large numbers of pedestrians cross the Casco Bay Bridge daily to commute, shop and recreate. This sidewalk and pedestrian crossing at High/York Streets will significantly improve their safety and enjoyment of their trip.

Describe How the Project Brings New Opportunities for Public Benefit:

Filling this gap in the transportation network will attract new users to alternative transportation in the region, increase physical activity, and help spur economic development in this revitalizing portion of the Portland Downtown.

Describe How Your Project Meets State Planning Goals/Economic/Tourism Benefits: *A project that will enhance or enable a livable, compact community that has the potential to improve quality of life, thereby enhancing the economic vibrancy of a community or region.*

York Street, between Maple and High Street, travels through the periphery of an emerging retail and commercial district that transitions from historic 19th century warehouses to the east, high density historic housing to the west, and the Arts District to the north. York Street serves as a primary route for pedestrians and cyclists traveling to the Old Port from the Casco Bay Bridge and from Portland's West End neighborhoods. Pedestrians accessing this district to or from the Casco Bay Bridge are forced to cross high volume traffic because of the interruption in the sidewalk network along York Street behind the Rufus Deering lumber yard. By introducing a sidewalk and bike lane along the southerly sideline of York Street, non-automotive travelers, tourists, and commuters are provided with safe quality experience with access to local and regional corridors and destinations.

Describe the Potential Safety Improvements:

The York Street sidewalk will provide a quality pedestrian connection between the Downtown/Portland Peninsula and the Casco Bay Bridge, including new pedestrian signals and curb ramps/crosswalks at the High Street/York Street intersection.

Section 4: Community Support

Provide information on the consultation and support for the project by the community: *Identify public discussion processes that occurred in the development of the proposal. Identify organizations that pledged their support of the project and the roles they plan to play in project implementation. Possible project partners may include school officials, local traffic engineers, community members, municipal officials, law enforcement agencies, public health agencies or organizations, local elected officials, and other not-for-profit community groups. Include as attachments letters of support.* City and School Dept. staff, and our local Safe Routes To School contact, Sarah Cushman; engaged several partners in the course of brainstorming our candidate applications. Representation from the following groups helped form the initial proposal to our City Council's Transportation Committee:

- Bicycle/Pedestrian Advisory Committee
- Portland School Department (Health Coordinator)
- Portland Trails
- PACTS
- City Departments include: Executive-Office of Transportation Policy, Planning & Urban Development, Public Services, and Health & Human Services

We held three 'brainstorming' sessions with various representatives of these groups and developed a list of proposals. That list was presented to our City Council Transportation Committee on May 13, 2010 during their public meeting. The Transportation Committee supported all but one of the proposed projects (it was duplicative of a previous application to PACTS for STIP funding) and added five (5) additional projects for consideration. Our Transportation Committee understands that if projects are successful, their may be a local match commitment.

Is a municipal official where the project is located certified to locally administer the project? Yes No

Is the municipality willing to become LAP Certified? Yes No

A municipal/state agreement with the Maine Department of Transportation is required for the development, design, and construction of the project in accordance with Federal, State, and Local requirements.

Note: Information on Locally Administered Project (LAP) requirements can be found at: <http://www.maine.gov/mdot/lap/lpa.php>

Section 5: Sensible Transportation Planning Bonus Points

Please Note: Your community may receive additional bonus points if it has met some of the criteria listed below. To be eligible for these bonus points, please answer the following questions:

Does your community have an approved Comprehensive Plan or Community Transportation Plan that has been found consistent by the State Planning Office or MaineDOT? Yes No **If yes, please list the year it was found consistent:**

If applicable, please outline relevant ordinances and policies that have been enacted to support the Comprehensive or Transportation Plan by promoting and encouraging compact development patterns in growth areas.

The City of Portland, through its history, recent planning and intentional expressed policies promotes the community as a business and population center committed to responsible growth promoting a vibrant urban environment. The City's Comprehensive Plan has been found consistent with State Growth Management goals in 2001 and 2005 and the following planning and ordinance initiatives specifically demonstrate Portland's commitment to retaining its position as the cultural, transportation, business, service, and population center of Northern New England.

Policies and Ordinances Promoting Density –The following housing and transportation planning initiatives promote true urban scale development and density for Portland:

Housing

Adoption of a Housing Plan (Sustaining Portland's Future, 2002) established general policies for increased density and specific goals for growth both on and off the Portland peninsula. The Housing Plan is implemented by: the creation of the "R-7" high density residential overlay zone, small lot infill provisions for high and medium density neighborhoods in the R-6 and R-5 zones, and higher residential density allowances in the B-2 "community" business zones (pending.)

Transportation

Adoption of the Portland Transportation Plan (Time of Change, 1993) established the City as a leader in connecting transportation and land use decision-making by linking policies for smarter growth with the provision of transportation choice and urban livability. Additional recent Smart Transportation initiatives include:

-- Peninsula Transit Study - Report and Action Plan, as implemented by Bayside neighborhood TDM requirements, reduced parking requirements for on-peninsula residential projects and affordable housing projects, adoption of a Peninsula Fee – Fee In-Lieu of Parking ordinance, and creation of a Sustainable Transportation Fund all promote land use policies linking urban density with transportation choice.

Master Planning and Zoning for Density.

Portland's commitment to expanding livable and attractive urban development is shown with:

-- Adoption of the Bayside Vision and Eastern Waterfront redevelopment master plans, both adopted components of the Comprehensive Plan. Both Bayside and the Eastern Waterfront are planned for multi-story, mixed use, transit oriented development set in a traditional urban street grid.

If applicable, please describe any regional efforts with surrounding municipalities that your community has undertaken to promote and encourage compact development patterns in growth areas.

Regional Efforts – In addition to on-going working relationships with regional community partners at GPCOG and PACTS, Portland has engaged in the following multi-jurisdictional planning efforts promoting density and transportation supporting density:

-- The Peninsula Transit Study oversight committee included South Portland representation to ensure coordination between transit planning between the two communities.

-- The Gorham East-West Corridor Study is an on-going regional transportation corridor evaluation where Portland's participation promoted a compact development scenario for evaluation to demonstrate the advantages of higher density development policies at the community level for regional transportation infrastructure needs and enhance regional livability.

Section 6: Authorized Signatures

An authorized representative of the city/town and applicant/sponsor must sign the application.

Municipal Official: (check one) Applicant Endorsement

Name: _____ Title: _____

Phone#: _____ Email: _____

Signature

Date

Applicant/Sponsor (if different)

Name: _____ Title: _____

Phone#: _____ Email: _____

Signature

Date

These signatures indicate the willingness/ability to provide the designated level of matching funds and a willingness to enter into a municipal/State agreement with the Department requiring the municipality/applicant/sponsor to administer the development, design, and construction of the project abiding to federal, State, and local requirements. The applicant will also be responsible for future maintenance of the completed project. Note that design should meet all applicable federal and State Standards and ADA Guidelines.

Send all application information as follows: Applications must be received no later than 4:00 PM (EST) on July 1, 2010. Seven complete hardcopy application packages (including all attachments and all required documents) must be submitted to:

MaineDOT
Attn: Dan Stewart
Bureau of Transportation Systems Planning
16 State House Station
Child Street
Augusta, ME 04333-0016

12/10/2012

Maple St Sidewalk (Commercial St and connect to existing brick sidewalk)

Item No	Qty.	UOM	Description	Unit Cost	Ext.
608.15	45	SY	Brick Sidewalk with Bituminous Base	\$150.00	\$6,750.00
609.3811	55	LF	Reset Granite Boarder	\$50.00	\$2,750.00
615.08	80	SY	Loam, Seed and Mulch	\$20.00	\$1,600.00
Subtotal					\$11,100.00
20% Contingency					\$2,220.00
Total Estimate					\$13,320.00

5' x 80'

Note: No granite curb being installed

Leave granite cobbles at edge of road

Install 5' brick walk- reset large granite blocks as boarder to brick walk

Loam and seed sides as necessary

DAG

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Delay / Veh (s)	0.5	0.3	2.7	0.2	6.2	6.5	2.7	6.1	0.8

Total Network Performance

Delay / Veh (s)	1.1
-----------------	-----

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	SBR	All
Delay / Veh (s)	0.5	0.3	3.6	0.2	6.5	10.2	2.9	6.0	3.1	1.2

Total Network Performance

Delay / Veh (s)	1.5
-----------------	-----

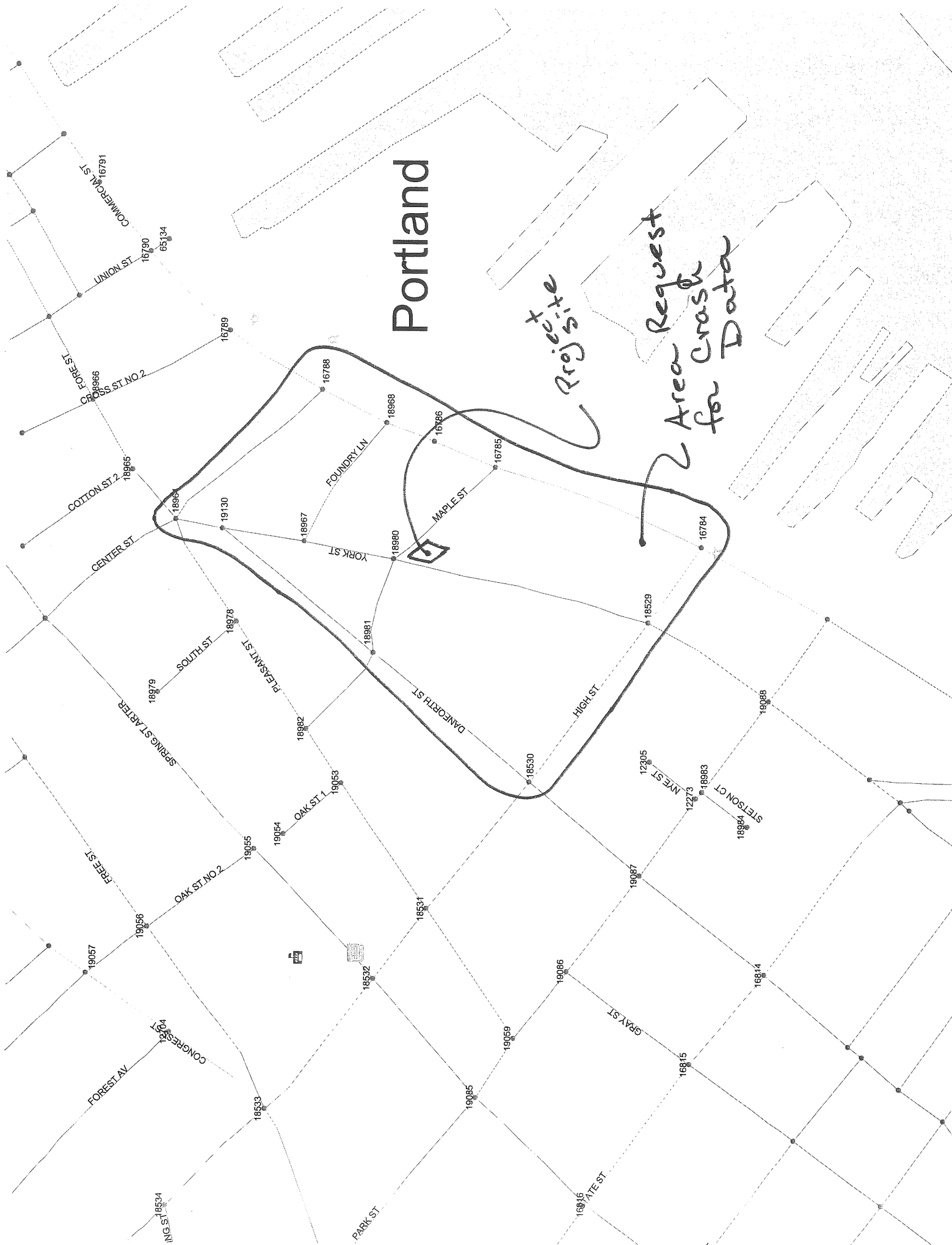
1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	SBR	All
Delay / Veh (s)	0.5	0.3	3.6	0.3	6.6	7.8	3.4	6.0	2.8	1.4

Total Network Performance

Delay / Veh (s)	1.8									
-----------------	-----	--	--	--	--	--	--	--	--	--

Portland



Project Site

Area Request for Crash Data



Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary Report

Report Selections and Input Parameters

REPORT SELECTIONS

Crash Summary I Section Detail Crash Summary II 1320 Included 1320 & Driver Report Included

REPORT DESCRIPTION

York St area

REPORT PARAMETERS

Year 2009, Start Month 1 through Year 2011 End Month: 12

Route: 0560122

Start Node: 16788
End Node: 18964

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 3200508

Start Node: 18964
End Node: 18529

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0001A

Start Node: 16784
End Node: 16788

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560365

Start Node: 16784
End Node: 18529

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0077W

Start Node: 18529
End Node: 18530

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560191

Start Node: 19130
End Node: 18530

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560475

Start Node: 18980
End Node: 18981

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560475

Start Node: 16785
End Node: 18980

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560291

Start Node: 18967
End Node: 18968

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Crash Summary I

Nodes

Node	Route - MP	Node Description	U/R	Total Crashes	K	A	B	C	PD	Injury Crashes	Percent Annual M Ent-Veh	Crash Rate	Critical Rate	CRF
16788	0560122 - 0	Int of CENTER ST, COMMERCIAL ST	9	8	0	0	1	1	6	25.0	5.645	0.47	1.08	0.00
											Statewide Crash Rate:	0.62		
18964	0560122 - 0.10	Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST	9	3	0	0	0	1	2	33.3	3.777	0.26	1.18	0.00
											Statewide Crash Rate:	0.62		
19130	3200508 - 0.02	Int of DANFORTH ST YORK ST	9	1	0	0	0	0	1	0.0	2.346	0.14	1.31	0.00
											Statewide Crash Rate:	0.62		
18967	3200508 - 0.06	0509387 POR, YORK, FOUNDRY LA.	2	0	0	0	0	0	0	0.0	0.930	0.00	0.51	0.00
											Statewide Crash Rate:	0.13		
18980	3200508 - 0.10	0509400 POR, MAPLE, YORK ST.	2	0	0	0	0	0	0	0.0	1.849	0.00	0.43	0.00
											Statewide Crash Rate:	0.13		
16784	0001A - 11.15	Int of COMMERCIAL ST HIGH ST	2	8	0	0	1	2	5	37.5	5.816	0.46	0.30	1.50
											Statewide Crash Rate:	0.12		
16785	0001A - 11.27	Int of COMMERCIAL ST MAPLE ST	2	2	0	0	0	1	1	50.0	5.507	0.12	0.31	0.00
											Statewide Crash Rate:	0.12		
16786	0001A - 11.30	Non Int COMMERCIAL ST	2	0	0	0	0	0	0	0.0	5.382	0.00	0.31	0.00
											Statewide Crash Rate:	0.12		
18968	0001A - 11.33	Int of COMMERCIAL ST FOUNDRY LN	2	0	0	0	0	0	0	0.0	5.466	0.00	0.31	0.00
											Statewide Crash Rate:	0.12		
18529	0077VW - 1.39	Int of HIGH ST YORK ST	9	10	0	0	0	3	6	33.3	3.377	0.99	1.21	0.00
											Statewide Crash Rate:	0.62		
18981	0560191 - 0.10	Int of DANFORTH ST MAPLE ST	2	0	0	0	0	0	0	0.0	0.877	0.00	0.45	0.00
											Statewide Crash Rate:	0.11		
18530	0560191 - 0.20	Int of DANFORTH ST HIGH ST	9	18	0	0	1	5	12	33.3	6.348	0.95	1.06	0.00
											Statewide Crash Rate:	0.62		

Study Years: 3.00

NODE TOTALS: 50 0 0 0 3 13 33 32.0 47.320 0.35 0.47 0.75

Crash Summary I

Sections

Start Node	End Node	Element	Offset Begin - End	Route - MP	Section U/R Length	Total Crashes	K	A	B	C	PD	Injury Crashes	Percent Injury	Annual HMVM	Crash Rate	Critical Rate	CRF
16788 Int of CENTER ST, COMMERCIAL ST	18964 192464	192464	0 - 0.10	0560122 - 0 RD INV 05 60122	0.10	2	1	0	0	0	0	1	100.0	0.00121	275.07	982.08	0.00
18529 Int of HIGH ST, YORK ST	18980 194048	194048	0 - 0.14	3200508 - 0.04 RD INV 3200508	0.14	2	2	0	0	0	0	2	100.0	0.00243	274.26	820.78	0.00
18964 Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST	19130 3106830	3106830	0 - 0.02	3200508 - 0 RD INV 3200508	0.02	2	1	0	0	0	1	0	0.0	0.00046	721.13	607.32	1.19
18967 0509387 POR, YORK, FOUNDRY LA.	19130 194651	194651	0 - 0.04	3200508 - 0.02 RD INV 3200508	0.04	2	0	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00
18967 0509387 POR, YORK, FOUNDRY LA.	194650 194650	194650	0 - 0.04	3200508 - 0.06 RD INV 3200508	0.04	2	0	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00
16784 Int of COMMERCIAL ST, HIGH ST	16785 3116402	3116402	0 - 0.12	0001A - 11.15 US 1A	0.12	2	6	0	0	0	1	5	16.7	0.00645	309.87	388.11	0.00
16785 Int of COMMERCIAL ST, MAPLE ST	16786 3130488	3130488	0 - 0.03	0001A - 11.27 US 1A	0.03	2	3	0	0	1	0	2	33.3	0.00163	614.36	551.86	1.11
16788 Int of CENTER ST, COMMERCIAL ST	18968 3119256	3119256	0 - 0.04	0001A - 11.29 US 1A	0.04	2	1	0	0	0	1	0	0.0	0.00216	154.31	513.40	0.00
16786 Non Int COMMERCIAL ST	18968 3118839	3118839	0 - 0.03	0001A - 11.30 US 1A	0.03	2	1	0	0	0	1	0	0.0	0.00160	208.12	554.11	0.00
16784 Int of COMMERCIAL ST, HIGH ST	18529 3115419	3115419	0 - 0.05	0560365 - 0 RD INV 05 60365	0.05	2	0	0	0	0	0	0	0.0	0.00063	0.00	683.39	0.00
18529 Int of HIGH ST, YORK ST	18530 3106683	3106683	0 - 0.10	0077W - 1.39 ST RTE 77W	0.10	2	10	0	0	0	0	10	0.0	0.00383	870.25	446.50	1.95
18981 Int of DANFORTH ST, MAPLE ST	19130 3106832	3106832	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	2	0	0	0	2	0	0.0	0.00071	935.70	573.37	1.63
18530 Int of DANFORTH ST, HIGH ST	18981 3106686	3106686	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	4	0	0	0	4	0	0.0	0.00083	1609.24	557.81	2.88
18980 0509400 POR, MAPLE, YORK ST.	18981 194666	194666	0 - 0.05	0560475 - 0.07 RD INV 05 60475	0.05	2	0	0	0	0	0	0	0.0	0.00004	0.00	558.41	0.00
16785 Int of COMMERCIAL ST, MAPLE ST	18980 192461	192461	0 - 0.07	0560475 - 0 RD INV 05 60475	0.07	2	2	0	0	0	2	0	0.0	0.00015	4537.85	1451.68	3.13
18967 0509387 POR, YORK, FOUNDRY LA.	18968 194649	194649	0 - 0.07	0560291 - 0 RD INV 05 60291	0.07	2	0	0	0	0	0	0	0.0	0.00013	0.00	1448.34	0.00

Study Years:	3.00	Section Totals:	1.10	33	0	0	1	4	28	15.2	0.02360	466.01	338.32	1.38
Grand Totals:	1.10	83	0	0	4	17	61	25.3	0.02360	1172.09	461.67	2.54		

Crash Summary

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	Injury Crashes					Crash Report	Crash Date	Crash Mile Point	Injury Degree
						K	A	B	C	PD				
16788	18964	192464	0 - 0.10	0560122 - 0	1	0	0	0	1	0	2010-5651C	03/14/2010	0.01	C
18964	19130	3106830	0 - 0.02	3200508 - 0	1	0	0	0	1	0	2011-7755C	04/15/2011	0.01	PD
18967	19130	194651	0 - 0.04	3200508 - 0.02	0	0	0	0	0	0				
18967	18980	194650	0 - 0.04	3200508 - 0.06	0	0	0	0	0	0				
18529	18980	194048	0 - 0.14	3200508 - 0.10	2	0	0	0	2	0	2010-3489C	02/06/2010	0.11	C
											2011-9159	08/31/2011	0.20	C
16784	16785	3116402	0 - 0.12	0001A - 11.15	6	0	0	0	1	5	2011-8884	08/28/2011	11.16	C
											2011-6597	08/03/2011	11.16	PD
											2009-32880C	12/29/2009	11.16	PD
											2009-11594C	05/22/2009	11.17	PD
											2010-14757C	07/20/2010	11.17	PD
											2010-12965C	05/31/2010	11.18	PD
16785	16786	3130488	0 - 0.03	0001A - 11.27	3	0	0	1	0	2	2010-30463C	12/26/2010	11.28	B
											2009-10518C	05/13/2009	11.28	PD
											2010-15225C	06/21/2010	11.28	PD
16786	18968	3118839	0 - 0.03	0001A - 11.30	1	0	0	0	0	1	2009-19621C	08/31/2009	11.31	PD
16788	18968	3119256	0 - 0.04	0001A - 11.33	1	0	0	0	0	1	2010-14622C	07/13/2010	11.36	PD
16784	18529	3115419	0 - 0.05	0560365 - 0	0	0	0	0	0	0				
18529	18530	3106683	0 - 0.10	0077W - 1.39	10	0	0	0	0	10	2009-6432C	03/04/2009	1.40	PD
											2011-1699C	01/30/2011	1.40	PD
											2011-3653	06/29/2011	1.41	PD
											2010-9558C	05/05/2010	1.43	PD
											2011-16796	11/28/2011	1.45	PD
											2009-1917C	01/09/2009	1.45	PD
											2011-5217	07/19/2011	1.46	PD
											2009-18444C	08/16/2009	1.48	PD
											2011-710C	01/07/2011	1.48	PD
											2010-14209C	07/10/2010	1.48	PD
18981	19130	3106832	0 - 0.10	0560191 - 0	2	0	0	0	0	2	2009-6337C	02/20/2009	0.01	PD
											2010-16740C	08/11/2010	0.02	PD
18530	18981	3106686	0 - 0.10	0560191 - 0.10	4	0	0	0	0	4	2011-5063	07/15/2011	0.11	PD
											2010-24444C	11/05/2010	0.15	PD
											2010-1886C	01/26/2010	0.15	PD
											2010-26329C	11/30/2010	0.19	PD

Crash Summary

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	K	Injury Crashes			Crash Report	Crash Date	Crash Mile Point	Injury Degree
						A	B	C	PD				
18980	18981	194666	0 - 0.05	0560475 - 0.07	0	0	0	0	0				
16785	18980	192461	0 - 0.07	0560475 - 0	2	0	0	0	2	2010-9693C	05/14/2010	0.04	PD
18967	18968	194649	0 - 0.07	0560291 - 0	0	0	0	0	0	2011-2927C	02/04/2011	0.06	PD

Totals: 33 0 0 1 4 28

Maine Department of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Day and Hour

Day of Week	Hour of Day												Un	Tot													
	AM						PM																				
	12	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6	7	8	9	10	11			
SUNDAY	1	3	0	1	0	0	0	1	0	1	2	0	0	0	0	0	2	1	0	0	0	2	0	0	0	0	14
MONDAY	0	0	0	0	0	0	0	0	1	0	2	2	0	1	2	1	2	2	0	0	0	1	0	0	0	0	14
TUESDAY	0	1	0	0	0	0	0	1	1	1	0	1	1	0	0	2	0	1	0	0	1	0	0	0	0	0	10
WEDNESDAY	0	0	0	0	0	0	0	0	1	1	1	0	2	1	0	0	3	1	2	0	0	0	0	0	0	0	12
THURSDAY	0	0	0	0	0	0	0	1	1	0	1	0	2	0	0	1	1	0	1	0	1	0	0	0	0	0	8
FRIDAY	1	0	0	0	0	0	0	2	0	1	1	2	2	2	1	2	2	2	0	2	0	0	0	0	0	0	20
SATURDAY	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1	1	0	0	1	0	0	0	0	0	5
Totals	2	4	0	1	0	0	0	5	4	4	4	7	5	7	4	3	9	10	8	2	3	2	0	0	0	83	

Vehicle Counts by Type

Unit Type	Total	Unit Type	Total
1-Passenger Car	103	23-Bicyclist	2
2-(Sport) Utility Vehicle	30	24-Witness	13
3-Passenger Van	13	25-Other	3
4-Cargo Van (10K lbs or Less)	0	Total	193
5-Pickup	20		
6-Motor Home	0		
7-School Bus	0		
8-Transit Bus	0		
9-Motor Coach	0		
10-Other Bus	0		
11-Motorcycle	1		
12-Moped	0		
13-Low Speed Vehicle	0		
14-Autocycle	0		
15-Experimental	0		
16-Other Light Trucks (10,000 lbs or Less)	0		
17-Medium/Heavy Trucks (More than 10,000 lbs)	6		
18-ATV - (4 wheel)	0		
20-ATV - (2 wheel)	0		
21-Snowmobile	0		
22-Pedestrian	2		

Maine Department of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Driver Action at Time of Crash

Driver Action at Time of Crash	Dr 1	Dr 2	Dr 3	Dr 4	Dr 5	Other	Total
No Contributing Action	9	5	1	0	0	0	15
Ran Off Roadway	1	0	0	0	0	0	1
Failed to Yield Right-of-Way	8	9	0	0	0	0	17
Ran Red Light	1	1	0	0	0	0	2
Ran Stop Sign	0	0	0	0	0	0	0
Disregarded Other Traffic Sign	0	0	0	0	0	0	0
Disregarded Other Road Markings	0	0	0	0	0	0	0
Exceeded Posted Speed Limit	1	2	1	0	0	0	4
Drove Too Fast For Conditions	0	0	0	0	0	0	0
Improper Turn	3	2	0	0	0	0	5
Improper Backing	2	1	0	0	0	0	3
Improper Passing	1	2	0	0	0	0	3
Wrong Way	0	0	0	0	0	0	0
Followed Too Closely	4	7	1	0	0	0	12
Failed to Keep in Proper Lane	1	0	0	0	0	0	1
Operated Motor Vehicle in Erratic, Reckless, Careless, Negligent or Aggressive Manner	0	0	0	0	0	0	0
Swerved or Avoided Due to Wind, Slippery Surface, Motor Vehicle, Object, Non-Motorist in Roadway	0	0	0	0	0	0	0
Over-Correcting/Over-Steering	0	0	0	0	0	0	0
Other Contributing Action	0	1	0	0	0	0	1
Unknown	0	1	0	0	0	0	1
Total	31	31	3	0	0	0	65

Crashes by Apparent Physical Condition And Driver

Apparent Physical Condition	Dr 1	Dr 2	Dr 3	Dr 4	Dr 5	Other	Total
Apparently Normal	72	72	10	1	0	2	157
Physically Impaired or Handicapped	0	0	0	0	0	0	0
Emotional(Depressed, Angry, Disturbed, etc.)	0	0	0	0	0	0	0
Ill (Sick)	0	0	0	0	0	0	0
Asleep or Fatigued	0	0	0	1	0	0	1
Under the Influence of Medications/Drugs/Alcohol	3	3	0	0	0	0	6
Other	1	2	0	0	0	0	3
Total	76	77	10	2	0	2	167

Driver Age by Unit Type

Age	Driver	Bicycle	Snow/Mobile	Pedestrian	ATV	Total
09-Under	0	0	0	0	0	0
10-14	0	0	0	0	0	0
15-19	14	0	0	0	0	14
20-24	39	0	0	0	0	39
25-29	22	0	0	0	0	22
30-39	26	0	0	0	0	26
40-49	18	0	0	0	0	18
50-59	28	0	0	0	0	28
60-69	14	0	0	0	0	14
70-79	5	0	0	0	0	5
80-Over	3	0	0	0	0	3
Unknown	7	2	0	2	0	11
Total	176	2	0	2	0	180

Crash Summary II - Characteristics

Most Harmful Event		Most Harmful Event		Injury Data		
Most Harmful Event	Total	Most Harmful Event (wall, building, tunnel, etc.)	Total	Severity Code	Injury Crashes	Number Of Injuries
1-Overturn / Rollover	0	38-Other Fixed Object	0	K	0	0
2-Fire / Explosion	0	39-Unknown	3	A	0	0
3-Immersion	0	40-Gate or Cable	0	B	4	6
4-Jackknife	0	41-Pressure Ridge	0	C	17	23
5-Cargo / Equipment Loss Or Shift	0	Total	31	PD	61	0
6-Fell / Jumped from Motor Vehicle	0			Total	82	29
7-Thrown or Falling Object	0					
8-Other Non-Collision	0					
9-Pedestrian	1					
10-Pedalcycle	0					
11-Railway Vehicle - Train, Engine	0					
12-Animal	0					
13-Motor Vehicle in Transport	24					
14-Parked Motor Vehicle	2					
15-Struck by Falling, Shifting Cargo or Anything Set in Motion by Motor Vehicle	0					
16-Work Zone / Maintenance Equipment	0					
17-Other Non-Fixed Object	0					
18-Impact Attenuator / Crash Cushion	0					
19-Bridge Overhead Structure	0					
20-Bridge Pier or Support	0					
21-Bridge Rail	0					
22-Cable Barrier	0					
23-Culvert	0					
24-Curb	0					
25-Ditch	0					
26-Embankment	0					
27-Guardrail Face	0					
28-Guardrail End	0					
29-Concrete Traffic Barrier	0					
30-Other Traffic Barrier	0					
31-Tree (Standing)	0					
32-Utility Pole / Light Support	0					
33-Traffic Sign Support	0					
34-Traffic Signal Support	1					
35-Fence	0					
36-Mailbox	0					
37-Other Post Pole or Support	0					

Road Character		Light	
Road Grade	Total	Light Condition	Total
1-Level	39	1-Daylight	57
2-On Grade	41	2-Dawn	1
3-Top of Hill	2	3-Dusk	4
4-Bottom of Hill	1	4-Dark - Lighted	21
5-Other	0	5-Dark - Not Lighted	0
Total	83	6-Dark - Unknown Lighting	0
		7-Unknown	0
		Total	83

Traffic Control Devices	
Traffic Control Device	Total
1-Traffic Signals (Stop & Go)	41
2-Traffic Signals (Flashing)	2
3-Advisory/Warning Sign	0
4-Stop Signs - All Approaches	0
5-Stop Signs - Other	8
6-Yield Sign	0
7-Curve Warning Sign	0
8-Officer, Flagman, School Patrol	0
9-School Bus Stop Arm	0
10-School Zone Sign	0
11-R.R. Crossing Device	0
12-No Passing Zone	0
13-None	28
14-Other	3
Total	82

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Year and Month

Month	2009	2010	2011	Total
JANUARY	3	1	2	6
FEBRUARY	3	2	4	9
MARCH	1	4	0	5
APRIL	2	3	3	8
MAY	3	5	2	10
JUNE	1	2	2	5
JULY	2	4	6	12
AUGUST	4	1	5	10
SEPTEMBER	1	0	0	1
OCTOBER	2	4	0	6
NOVEMBER	1	3	4	8
DECEMBER	2	1	0	3
Total	25	30	28	83

Report is limited to the last 10 years of data.

Crash Summary II - Characteristics

Crashes by Crash Type and Type of Location

Crash Type	Straight Road	Curved Road	Three Leg Intersection	Four Leg Intersection	Five or More Leg Intersection	Driveways	Bridges	Interchanges	Other	Parking Lot	Private Way	Cross Over	Railroad Crossing	Total
Object in Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rear End / Sideswipe	26	0	12	15	0	2	0	0	0	0	0	0	0	55
Head-on / Sideswipe	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Intersection Movement	0	0	2	13	1	5	0	0	0	0	0	0	0	21
Pedestrians	0	0	2	0	0	0	0	0	0	0	0	0	0	2
Train	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Went Off Road	0	0	0	2	1	0	0	0	0	0	0	0	0	3
All Other Animal	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bicycle	0	0	1	1	0	0	0	0	0	0	0	0	0	2
Other	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Jackknife	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Rollover	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Fire	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Submersion	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Thrown or Falling Object	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bear	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Deer	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Moose	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Turkey	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total	26	0	17	31	2	7	0	0	0	0	0	0	0	83

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Blowing Sand, Soil, Dirt												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Blowing Snow												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Clear												
Dark - Lighted	13	0	0	0	0	0	0	2	0	0	0	15
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	36	1	0	0	0	0	0	0	0	0	1	38
Dusk	2	0	0	0	0	0	0	0	0	0	0	2
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Cloudy												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	8	0	0	0	0	0	0	0	0	0	3	11
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Fog, Smog, Smoke												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	1	0	0	0	0	0	0	0	0	0	0	1
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Other												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Rain												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	4	4
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	1	1
Daylight	0	0	0	0	0	0	0	0	0	0	5	5
Dusk	0	0	0	0	0	0	0	0	0	0	1	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Severe Crosswinds												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Sleet, Hail (Freezing Rain or Drizzle)												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Snow												
Dark - Lighted	0	0	0	0	0	0	0	1	0	0	1	2
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	2	0	0	0	2
Dusk	0	0	0	0	0	0	0	1	0	0	0	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL	60	1	0	0	0	0	0	6	0	0	0	83

You have a BASIC account | To remove the limits of a BASIC account and get unlimited questions, upgrade now!

Parent Survey June 22

Education

Design Survey Collect Responses **Analyze Results**

- [View Summary](#)
- [Browse Responses](#)
- [Filter Responses](#)
- [Crosstab Responses](#)
- [Download Responses](#)
- [Share Responses](#)

Default Report + Add Report

Response Summary

Total Started Survey: 26
Total Finished Survey: 26 (100%)

PAGE: 1

1. What is your preferred method of transportation to school?

Create Chart Download

	Response Percent	Response Count
walking	3.8%	1
bicycle	0.0%	0
public transportation	11.5%	3
parent ride	19.2%	5
parent carpool	11.5%	3
school bus	53.8%	14
answered question		26
skipped question		0

2. Would you prefer a parent carpool if Baxter Academy were to help facilitate?

Create Chart Download

	Response Percent	Response Count
yes	52.0%	13
no	48.0%	12
answered question		25
skipped question		1

3. Would you be willing to drive as part of a carpool?

Create Chart Download

	Response Percent	Response Count
--	---------------------	-------------------

SurveyMonkey – Survey Results

yes	70.8%	17
no	29.2%	7
answered question		24
skipped question		2

4. Would you be willing to pay a monthly bus fee? [Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	80.8%	21
no	19.2%	5
answered question		26
skipped question		0

5. Would you be picking your child up from school at dismissal at 2pm? [Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	36.0%	9
no	64.0%	16
answered question		25
skipped question		1

6. Would you want your child to ride a school bus back to your hometown? (outside of Portland only) [Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	65.0%	13
no	35.0%	7
answered question		20
skipped question		6

7. Would you be picking up your child later in the day from the Portland Public Library, or from some other downtown location? [Create Chart](#) [Download](#)

Response Percent	Response Count
---------------------	-------------------

SurveyMonkey - Survey Results

yes	36.0%	9
no	64.0%	16
answered question		25
skipped question		1

Follow Us: [Facebook](#) · [Twitter](#) · [LinkedIn](#) · [Our Blog](#)

Help: [Tutorials](#) · [Answers & FAQs](#) · [Contact Support](#)

About Us: [Management Team](#) · [Board of Directors](#) · [Partners](#) · [Newsroom](#) · [Contact Us](#) · [We're Hiring](#) · [Sitemap](#)

Policies: [Terms of Use](#) · [Privacy Policy](#) · [Anti-Spam Policy](#) · [Security Statement](#) · [Email Opt-Out](#)

[Dansk](#) · [Deutsch](#) · [English](#) · [Español](#) · [Français](#) · [한국어](#) · [Italiano](#) · [Nederlands](#) · [日本語](#) · [Norsk](#) · [Português](#) · [Русский](#) · [Suomi](#) · [Svenska](#) · [中文\(繁體\)](#)

Copyright © 1999-2012 SurveyMonkey

Maple Street @ York Street, Portland ME
Existing Conditions

9/24/2012

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Total Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.1
Delay / Veh (s)	0.5	0.3	2.7	0.2	6.2	6.5	2.7	6.1	0.8
Total Stops	0	0	2	0	3	6	7	12	30
Stop/Veh	0.00	0.00	0.40	0.00	1.00	1.00	1.00	1.00	0.08
Vehicles Entered	245	11	5	64	3	6	7	12	353

Total Network Performance

Total Delay (hr)	0.1
Delay / Veh (s)	1.1
Total Stops	30
Stop/Veh	0.08
Vehicles Entered	353

Maple Street @ York Street, Portland ME
Existing Conditions

9/24/2012

Intersection: 1: York Street & Maple Street

Movement	WB	NB	SB
Directions Served	LT	LTR	LTR
Maximum Queue (ft)	18	24	34
Average Queue (ft)	2	6	9
95th Queue (ft)	13	19	32
Link Distance (ft)	422	345	186
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Network Summary

Network wide Queuing Penalty: 0

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.0	0.0	0.1
Delay / Veh (s)	0.6	0.4	3.0	0.2	5.9	8.6	3.3	6.1	1.4
Total Stops	0	0	2	0	35	3	10	13	63
Stop/Veh	0.00	0.00	0.33	0.00	1.00	1.00	1.00	1.00	0.16
Vehicles Entered	242	17	6	59	36	3	10	13	386

Total Network Performance

Total Delay (hr)	0.2
Delay / Veh (s)	1.7
Total Stops	63
Stop/Veh	0.16
Vehicles Entered	386

1: York Street & Maple Street Performance by movement

Movement	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBT	All
Total Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.0	0.0	0.2
Delay / Veh (s)	0.5	0.3	2.9	0.2	6.6	8.4	4.2	6.6	1.9
Total Stops	0	0	2	0	70	6	11	13	102
Stop/Veh	0.00	0.00	0.33	0.00	1.00	1.00	1.00	1.00	0.24
Vehicles Entered	241	16	6	56	70	6	11	13	419

Total Network Performance

Total Delay (hr)	0.3
Delay / Veh (s)	2.4
Total Stops	102
Stop/Veh	0.24
Vehicles Entered	419

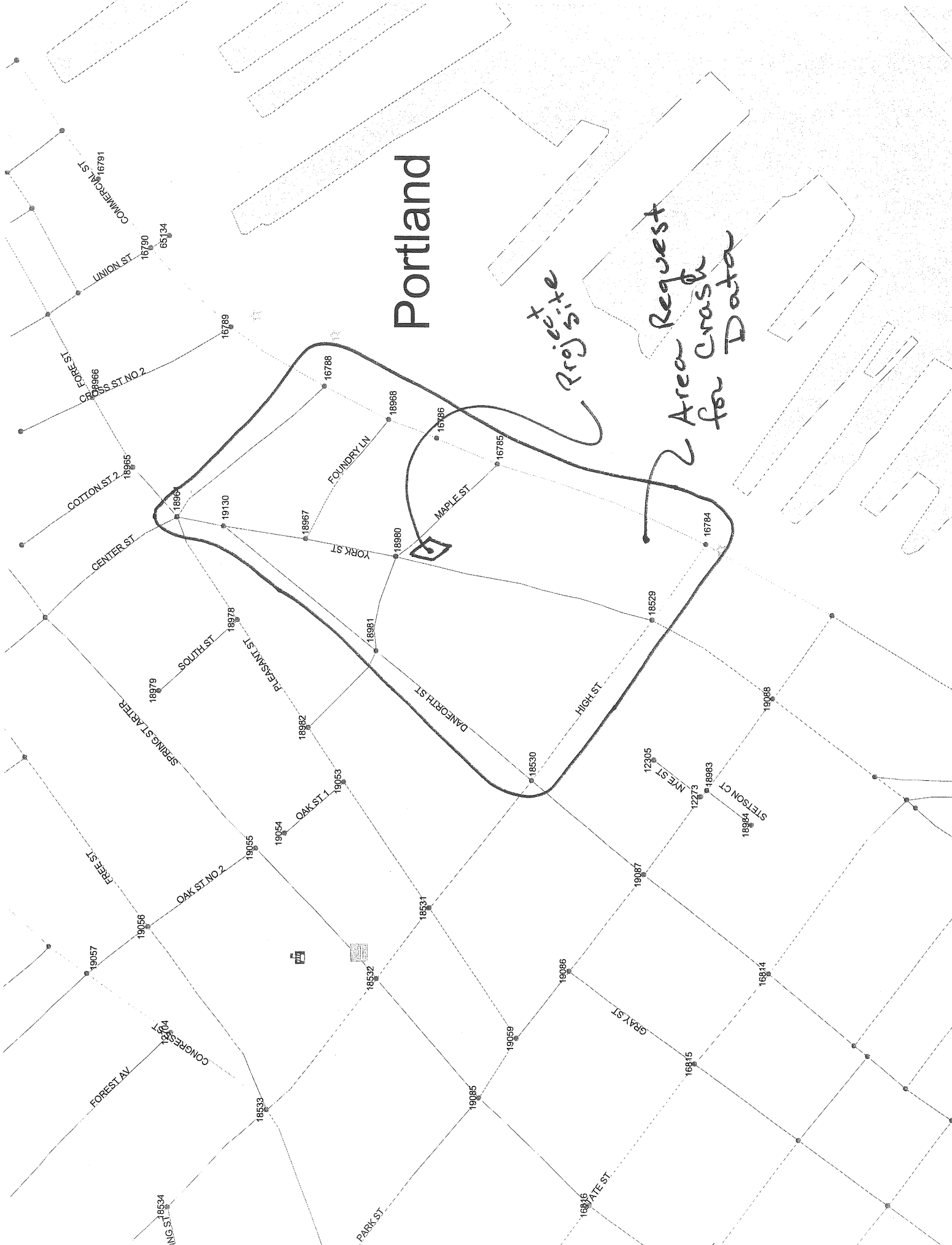
Intersection: 1: York Street & Maple Street

Movement	WB	NB	SB
Directions Served	LT	LTR	LTR
Maximum Queue (ft)	18	57	39
Average Queue (ft)	2	20	10
95th Queue (ft)	12	41	34
Link Distance (ft)	422	345	186
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Network Summary

Network wide Queuing Penalty: 0

Portland



Project Site

Area Request for Crash for Data

Crash Summary Report

Report Selections and Input Parameters

REPORT SELECTIONS

Crash Summary I Section Detail Crash Summary II 1320 Included 1320 & Driver Report Included

REPORT DESCRIPTION

York St area

REPORT PARAMETERS

Year 2009, Start Month 1 through Year 2011 End Month: 12

Route: 0560122

Start Node: 16788
End Node: 18964

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 3200508

Start Node: 18964
End Node: 18529

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0001A

Start Node: 16784
End Node: 16788

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560365

Start Node: 16784
End Node: 18529

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0077W

Start Node: 18529
End Node: 18530

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560191

Start Node: 19130
End Node: 18530

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560475

Start Node: 18980
End Node: 18981

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560475

Start Node: 16785
End Node: 18980

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Route: 0560291

Start Node: 18967
End Node: 18968

Start Offset: 0
End Offset: 0

Exclude First Node
 Exclude Last Node

Maine Department Of Transportation - Traffic Engineering, Crash Records Section

Crash Summary I

Nodes

Node	Route - MP	Node Description	U/R	Total Crashes	K	A	B	C	PD	Injury	Percent Annual M Ent-Veh	Crash Rate	Critical Rate	CRF
16788	0560122 - 0	Int of CENTER ST, COMMERCIAL ST	9	8	0	0	1	1	6	25.0	5.645	0.47	1.08	0.00
												Statewide Crash Rate:	0.62	
18964	0560122 - 0.10	Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST	9	3	0	0	0	1	2	33.3	3.777	0.26	1.18	0.00
												Statewide Crash Rate:	0.62	
19130	3200508 - 0.02	Int of DANFORTH ST YORK ST	9	1	0	0	0	0	1	0.0	2.346	0.14	1.31	0.00
												Statewide Crash Rate:	0.62	
18967	3200508 - 0.06	0509387 POR, YORK, FOUNDRY LA.	2	0	0	0	0	0	0	0.0	0.930	0.00	0.51	0.00
												Statewide Crash Rate:	0.13	
18980	3200508 - 0.10	0509400 POR, MAPLE, YORK ST.	2	0	0	0	0	0	0	0.0	1.849	0.00	0.43	0.00
												Statewide Crash Rate:	0.13	
16784	0001A - 11.15	Int of COMMERCIAL ST HIGH ST	2	8	0	0	1	2	5	37.5	5.816	0.46	0.30	1.50
												Statewide Crash Rate:	0.12	
16785	0001A - 11.27	Int of COMMERCIAL ST MAPLE ST	2	2	0	0	0	1	1	50.0	5.507	0.12	0.31	0.00
												Statewide Crash Rate:	0.12	
16786	0001A - 11.30	Non Int COMMERCIAL ST	2	0	0	0	0	0	0	0.0	5.382	0.00	0.31	0.00
												Statewide Crash Rate:	0.12	
18968	0001A - 11.33	Int of COMMERCIAL ST FOUNDRY LN	2	0	0	0	0	0	0	0.0	5.466	0.00	0.31	0.00
												Statewide Crash Rate:	0.12	
18529	0077W - 1.39	Int of HIGH ST YORK ST	9	10	0	0	0	3	6	33.3	3.377	0.99	1.21	0.00
												Statewide Crash Rate:	0.62	
18981	0560191 - 0.10	Int of DANFORTH ST MAPLE ST	2	0	0	0	0	0	0	0.0	0.877	0.00	0.45	0.00
												Statewide Crash Rate:	0.11	
18530	0560191 - 0.20	Int of DANFORTH ST HIGH ST	9	18	0	0	1	5	12	33.3	6.348	0.95	1.06	0.00
												Statewide Crash Rate:	0.62	
Study Years:	3.00			50	0	0	0	3	13	33	47.320	0.35	0.47	0.75

Crash Summary I

Sections

Start Node	End Node	Element	Offset Begin - End	Route - MP	Section U/R Length	Total Crashes	K	A	B	C	PD	Injury Crashes	Percent Injury	Annual HMVM	Crash Rate	Critical Rate	CRF
16788	18964	192464	0 - 0.10	0560122 - 0 RD INV 05 60122	0.10	2	1	0	0	0	1	0	100.0	0.00121	275.07	982.08	0.00
		Int of CENTER ST, COMMERCIAL ST													Statewide Crash Rate: 336.22		
18529	18980	194048	0 - 0.14	3200508 - -0.04 RD INV 3200508	0.14	2	2	0	0	2	0	0	100.0	0.00243	274.26	820.78	0.00
		Int of HIGH ST, YORK ST													Statewide Crash Rate: 336.22		
18964	19130	3106830	0 - 0.02	3200508 - 0 RD INV 3200508	0.02	2	1	0	0	0	1	0	0.0	0.00046	721.13	607.32	1.19
		Int of CENTER ST, FORE ST, PLEASANT ST, YORK ST													Statewide Crash Rate: 142.40		
18967	19130	194651	0 - 0.04	3200508 - 0.02 RD INV 3200508	0.04	2	0	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00
		0509387 POR, YORK, FOUNDRY LA.													Statewide Crash Rate: 336.22		
18967	18980	194650	0 - 0.04	3200508 - 0.06 RD INV 3200508	0.04	2	0	0	0	0	0	0	0.0	0.00067	0.00	1142.24	0.00
		0509387 POR, YORK, FOUNDRY LA.													Statewide Crash Rate: 336.22		
16784	16785	3116402	0 - 0.12	0001A - 11.15 US 1A	0.12	2	6	0	0	0	1	5	16.7	0.00645	309.87	388.11	0.00
		Int of COMMERCIAL ST, HIGH ST													Statewide Crash Rate: 171.50		
16785	16786	3130488	0 - 0.03	0001A - 11.27 US 1A	0.03	2	3	0	0	1	2	2	33.3	0.00163	614.36	551.86	1.11
		Int of COMMERCIAL ST, MAPLE ST													Statewide Crash Rate: 171.50		
16788	18968	3119256	0 - 0.04	0001A - 11.29 US 1A	0.04	2	1	0	0	0	1	0	0.0	0.00216	154.31	513.40	0.00
		Int of CENTER ST, COMMERCIAL ST													Statewide Crash Rate: 171.50		
16786	18968	3118839	0 - 0.03	0001A - 11.30 US 1A	0.03	2	1	0	0	0	1	0	0.0	0.00160	208.12	554.11	0.00
		Non Int COMMERCIAL ST													Statewide Crash Rate: 171.50		
16784	18529	3115419	0 - 0.05	0560365 - 0 RD INV 05 60365	0.05	2	0	0	0	0	0	0	0.0	0.00063	0.00	683.39	0.00
		Int of COMMERCIAL ST, HIGH ST													Statewide Crash Rate: 171.50		
18529	18530	3106683	0 - 0.10	0077W - 1.39 ST RTE 77W	0.10	2	10	0	0	0	10	0	0.0	0.00383	870.25	446.50	1.95
		Int of HIGH ST, YORK ST													Statewide Crash Rate: 173.49		
18981	19130	3106832	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	2	0	0	0	2	0	0.0	0.00071	935.70	573.37	1.63
		Int of DANFORTH ST, MAPLE ST													Statewide Crash Rate: 142.40		
18530	18981	3106686	0 - 0.10	0560191 - 0 RD INV 05 60191	0.10	2	4	0	0	0	4	0	0.0	0.00083	1609.24	557.81	2.88
		Int of DANFORTH ST, HIGH ST													Statewide Crash Rate: 142.40		
18980	18981	194666	0 - 0.05	0560475 - 0.07 RD INV 05 60475	0.05	2	0	0	0	0	0	0	0.0	0.00004	0.00	558.41	0.00
		0509400 POR, MAPLE, YORK ST.													Statewide Crash Rate: 336.22		
16785	18980	192461	0 - 0.07	0560475 - 0 RD INV 05 60475	0.07	2	2	0	0	0	2	0	0.0	0.00015	4537.85	1451.68	3.13
		Int of COMMERCIAL ST, MAPLE ST													Statewide Crash Rate: 336.22		
18967	18968	194649	0 - 0.07	0560291 - 0 RD INV 05 60291	0.07	2	0	0	0	0	0	0	0.0	0.00013	0.00	1448.34	0.00
		0509387 POR, YORK, FOUNDRY LA.													Statewide Crash Rate: 336.22		
Study Years:	3.00				1.10	33	0	0	0	1	4	28	15.2	0.02360	466.01	338.32	1.38
Grand Totals:					1.10	83	0	0	0	4	17	61	25.3	0.02360	1172.09	461.67	2.54

Crash Summary

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes	Injury Crashes				Crash Report	Crash Date	Crash Mile Point	Injury Degree
						K	A	B	C				
16788	18964	192464	0 - 0.10	0560122 - 0	1	0	0	1	0	2010-5651C	03/14/2010	0.01	C
18964	19130	3106830	0 - 0.02	3200508 - 0	1	0	0	0	1	2011-7755C	04/15/2011	0.01	PD
18967	19130	194651	0 - 0.04	3200508 - 0.02	0	0	0	0	0				
18967	18980	194650	0 - 0.04	3200508 - 0.06	0	0	0	0	0				
18529	18980	194048	0 - 0.14	3200508 - 0.10	2	0	0	2	0	2010-3489C	02/06/2010	0.11	C
										2011-9159	08/31/2011	0.20	C
16784	16785	3116402	0 - 0.12	0001A - 11.15	6	0	0	1	5	2011-8884	08/28/2011	11.16	C
										2011-6597	08/03/2011	11.16	PD
										2009-32880C	12/29/2009	11.16	PD
										2009-11594C	05/22/2009	11.17	PD
										2010-14757C	07/20/2010	11.17	PD
										2010-12965C	05/31/2010	11.18	PD
16785	16786	3130488	0 - 0.03	0001A - 11.27	3	0	0	1	2	2010-30463C	12/26/2010	11.28	B
										2009-10518C	05/13/2009	11.28	PD
										2010-15225C	06/21/2010	11.28	PD
16786	18968	3118839	0 - 0.03	0001A - 11.30	1	0	0	0	1	2009-19621C	08/31/2009	11.31	PD
16788	18968	3119256	0 - 0.04	0001A - 11.33	1	0	0	0	1	2010-14622C	07/13/2010	11.36	PD
16784	18529	3115419	0 - 0.05	0560365 - 0	0	0	0	0	0				
18529	18530	3106683	0 - 0.10	0077W - 1.39	10	0	0	0	10	2009-6432C	03/04/2009	1.40	PD
										2011-1699C	01/30/2011	1.40	PD
										2011-3653	06/29/2011	1.41	PD
										2010-9558C	05/05/2010	1.43	PD
										2011-16796	11/28/2011	1.45	PD
										2009-1917C	01/09/2009	1.45	PD
										2011-5217	07/19/2011	1.46	PD
										2009-18444C	08/16/2009	1.48	PD
										2011-710C	01/07/2011	1.48	PD
										2010-14209C	07/10/2010	1.48	PD
18981	19130	3106832	0 - 0.10	0560191 - 0	2	0	0	0	2	2009-6337C	02/20/2009	0.01	PD
										2010-16740C	08/11/2010	0.02	PD
18530	18981	3106686	0 - 0.10	0560191 - 0.10	4	0	0	0	4	2011-5063	07/15/2011	0.11	PD
										2010-24444C	11/05/2010	0.15	PD
										2010-1886C	01/26/2010	0.15	PD
										2010-26329C	11/30/2010	0.19	PD

Crash Summary

Section Details

Start Node	End Node	Element	Offset Begin - End	Route - MP	Total Crashes			Injury Crashes			Crash Report	Crash Date	Crash Mile Point	Injury Degree
					K	A	B	C	PD					
18980	18981	194666	0 - 0.05	0560475 - 0.07	0	0	0	0	0	0				
16785	18980	192461	0 - 0.07	0560475 - 0	2	0	0	0	0	2	2010-9693C	05/14/2010	0.04	PD
18967	18968	194649	0 - 0.07	0560291 - 0	0	0	0	0	0	0	2011-2927C	02/04/2011	0.06	PD

Totals: 33 0 0 1 4 28

Maine Department Of Transportation - Traffic Engineering, Crash Records Section
Crash Summary II - Characteristics

Crashes by Weather, Light Condition and Road Surface

Weather Light	Dry	Ice/Frost	Mud, Dirt, Gravel	Oil	Other	Sand	Slush	Snow	Unknown	Water (Standing, Moving)	Wet	Total
Sleet, Hail (Freezing Rain or Drizzle)												
Dark - Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	0	0	0	0	0
Dusk	0	0	0	0	0	0	0	0	0	0	0	0
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
Snow												
Dark - Lighted	0	0	0	0	0	0	0	1	0	0	1	2
Dark - Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0
Dark - Unknown Lighting	0	0	0	0	0	0	0	0	0	0	0	0
Dawn	0	0	0	0	0	0	0	0	0	0	0	0
Daylight	0	0	0	0	0	0	0	2	0	0	0	2
Dusk	0	0	0	0	0	0	0	1	0	0	0	1
Unknown	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL	60	1	0	0	0	0	0	6	0	0	0	83

You have a BASIC account | To remove the limits of a BASIC account and get unlimited questions, upgrade now!

Parent Survey June 22

Education

Design Survey Collect Responses **Analyze Results**

- View Summary
- Browse Responses
- Filter Responses
- Crosstab Responses
- Download Responses
- Share Responses

Default Report + Add Report

Response Summary

Total Started Survey: 26
Total Finished Survey: 26 (100%)

PAGE: 1

1. What is your preferred method of transportation to school?	Create Chart	Download
	Response Percent	Response Count
walking	3.8%	1
bicycle	0.0%	0
public transportation	11.5%	3
parent ride	19.2%	5
parent carpool	11.5%	3
school bus	53.8%	14
answered question		26
skipped question		0

2. Would you prefer a parent carpool if Baxter Academy were to help facilitate?	Create Chart	Download
	Response Percent	Response Count
yes	52.0%	13
no	48.0%	12
answered question		25
skipped question		1

3. Would you be willing to drive as part of a carpool?	Create Chart	Download
	Response Percent	Response Count

SurveyMonkey – Survey Results

yes	70.8%	17
no	29.2%	7
answered question		24
skipped question		2

4. Would you be willing to pay a monthly bus fee?

[Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	80.8%	21
no	19.2%	5
answered question		26
skipped question		0

5. Would you be picking your child up from school at dismissal at 2pm?

[Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	36.0%	9
no	64.0%	16
answered question		25
skipped question		1

6. Would you want your child to ride a school bus back to your hometown? (outside of Portland only)

[Create Chart](#) [Download](#)

	Response Percent	Response Count
yes	65.0%	13
no	35.0%	7
answered question		20
skipped question		6

7. Would you be picking up your child later in the day from the Portland Public Library, or from some other downtown location?

[Create Chart](#) [Download](#)

	Response Percent	Response Count
--	------------------	----------------

SurveyMonkey – Survey Results

yes	36.0%	9
no	64.0%	16

answered question	25
skipped question	1

Follow Us: [Facebook](#) · [Twitter](#) · [LinkedIn](#) · [Our Blog](#)

Help: [Tutorials](#) · [Answers & FAQs](#) · [Contact Support](#)

About Us: [Management Team](#) · [Board of Directors](#) · [Partners](#) · [Newsroom](#) · [Contact Us](#) · [We're Hiring](#) · [Sitemap](#)

Policies: [Terms of Use](#) · [Privacy Policy](#) · [Anti-Spam Policy](#) · [Security Statement](#) · [Email Opt-Out](#)

[Dansk](#) · [Deutsch](#) · [English](#) · [Español](#) · [Français](#) · [한국어](#) · [Italiano](#) · [Nederlands](#) · [日本語](#) · [Norsk](#) · [Português](#) · [Русский](#) · [Suomi](#) · [Svenska](#) · [中文\(繁體\)](#)

Copyright © 1999-2012 SurveyMonkey

Jobs Cars Real Estate Maine Yellow Pages Facebook Twitter Blogs

Subscribe Today

41°

Sunny
RealFeel® 43°
High: 48° | Low: 35°
High Tide: 4:18PM
Low Tide: 10:40PM



TUESDAY, NOVEMBER 20, 2012

News Sports Politics Business Opinion Life & Culture Obituaries Connect With Us Classified Ads

Save This Story E-mail This Story Print This Story Large Type

November 16

Baxter Academy for Technology and Science approved as charter school

The Baxter Academy for Technology and Science received approval Friday to open in Portland next fall as Maine's third charter school.

By Edward D. Murphy emurphy@mainetoday.com
Staff Writer

PORTLAND – The Baxter Academy for Technology and Science received approval Friday to open next fall as Maine's third charter school.

The Maine Charter School Commission said Baxter Academy will become the first public charter school focusing on science, technology, engineering and mathematics.

While the school expects to emphasize the sciences, it will also offer other courses typically offered by high schools, such as social studies and English, said John Jaques, the school's executive director. He said it will also offer Mandarin Chinese as a foreign language.

Jaques said the classes will feature "technology-rich project-based learning."

The school, which will be based at 54 York St., will begin accepting applications on Jan. 1 for students to be 9th and 10th graders when the doors open on Sept. 3, 2013.

The school hopes to enroll at least 150 students for the first year and will add another grade level for each of the two years following the first year to create a four-grade high school.

In charter schools, the "dollars follow the child," meaning the money for each student's education that would have gone to their local school will instead go to the charter school, which will accept students from throughout the state. The schools are also expected to do fund-raising to augment the tax dollars.

Under Maine's charter school law, the commission can authorize up to 10 public charter schools over the next decade.

Were you interviewed for this story? If so, please fill out our [accuracy form](#)

[Send Question/Comment to the Publisher](#)

This portion of the requested page has been blocked.
[Click here for details.](#)

[Tweet](#)

Further Discussion

Here at [PressHerald.com](#) we value our readers and are committed to growing our community by encouraging you to add to the discussion. To ensure conscientious dialogue we have implemented a strict no-bullying policy. To participate, you must follow our [Terms of Use](#).

Questions about the article? Add them below and we'll try to answer them or do a follow-up post as soon as we can.

Technical problems? [Email them](#) to us with an exact description of the problem. Make sure to include:

Type of computer or mobile device you are using

Exact operating system and browser you are viewing the site on (TIP: You can easily determine your operating system [here](#).)

Most...

READ

SHARED

[Musician Billy Scott dies at 70](#)

[3 people found dead in central Maine home](#)

[Fatal Maine plane crash raises new questions on safety](#)

[General's wife speaks out on his misconduct probe](#)

This portion of the requested page has been blocked.
[Click here for details.](#)

News

[Kennebec Journal](#)
[Morning Sentinel](#)
[Nation & World](#)
[Maine Crime](#)
[The Coastal Journal](#)
[RSS Feed](#)

Sports

[HS Football Picks Contest](#)
[Maine High School Sports](#)
[HS Players of the Year](#)
[New England Patriots](#)
[Portland Pirates](#)
[Maine Outdoors](#)

Politics

[Election 2012](#)
[Presidential](#)
[Senate](#)
[House](#)
[State](#)
[Same-Sex Marriage](#)
[Off the Trail](#)

Business

Opinion

[Editorials](#)
[Letters to the Editor](#)

Life & Culture

[Audience](#)
[Food & Dining](#)
[Go entertainment](#)
[Movies](#)
[Home & Garden](#)
[Close to Home](#)
[Religion & Values](#)
[MaineToday.com](#)
[Today's Crossword Puzzle](#)
[Today's Sudoku Puzzle](#)
[TV Listings](#)

Obituaries

[Maine Obituaries - Press Herald](#)
[Augusta Obituaries - KJ](#)
[Waterville Obituaries - MS](#)

Connect With Us

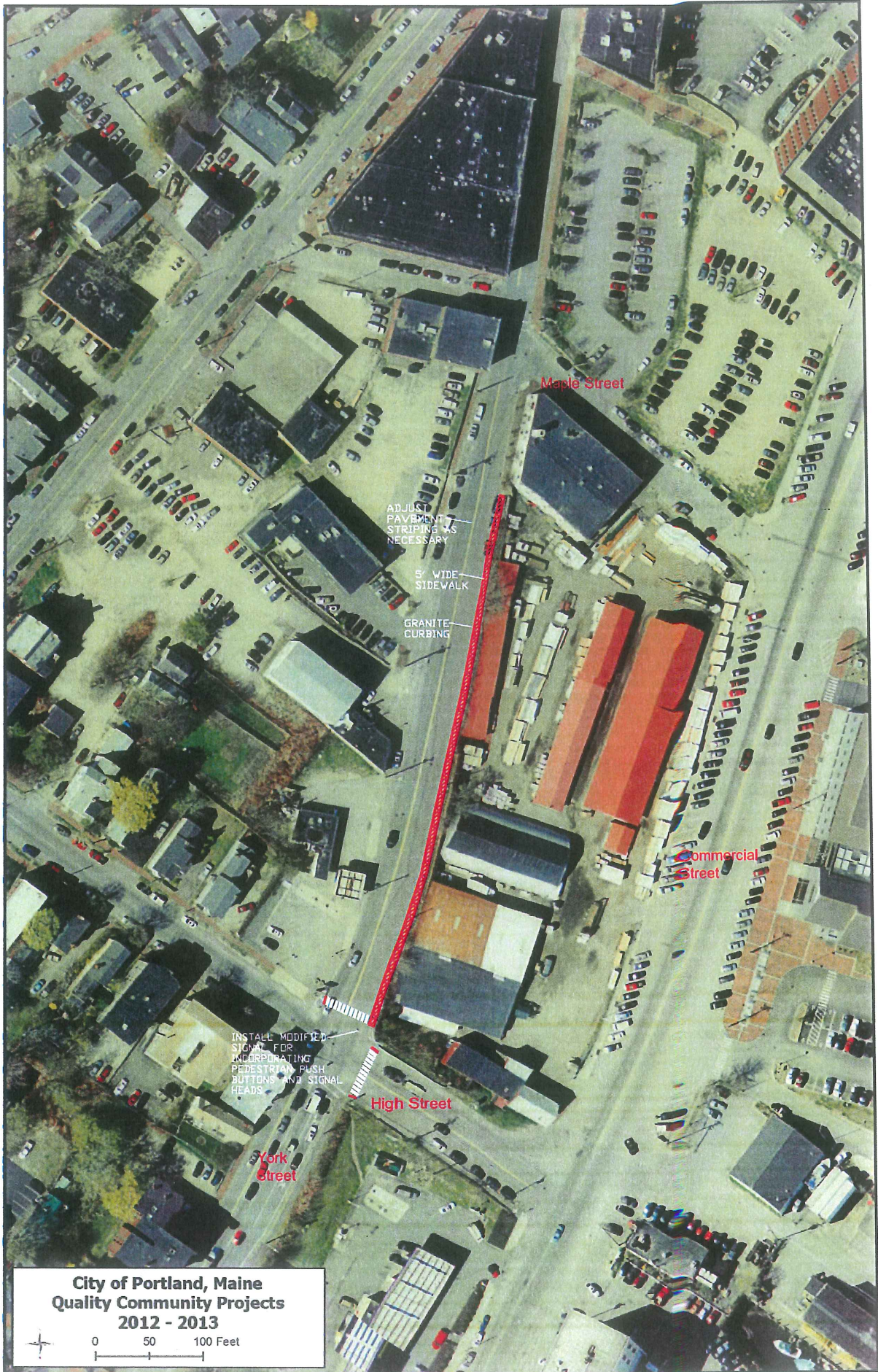
[Contact Us](#)
[Mobile Apps](#)
[Sign up for Text Alerts](#)
[Sign up for E-Mail Alerts](#)
[Reader Services](#)
[Access the e-edition](#)
[Facebook](#)
[Twitter](#)
[Circulation/Newspaper Delivery](#)
[Advertising](#)

Classified Ads

[Maine Jobs | Monster.com](#)
[Real Estate](#)
[Open House](#)
[Cars](#)
[Marketplace](#)
[Garage Sales](#)
[Business](#)
[Maine Yellow Pages](#)
[Special Sections](#)

[Place a Classified Ad](#)
[Legals](#)
[Search Maine Public Notices](#)

[Place an Online Ad](#) | [Place a Print Ad](#) | [Place a Classified Ad](#) | [Privacy Policy](#) | [Reader Services](#) | [Contact us](#)
Copyright ©2012 MaineToday Media, Inc.



City of Portland, Maine
Quality Community Projects
2012 - 2013

0 50 100 Feet

YORK STREET SIDEWALK TE PROJECT

Shukria Wiar - 54 York Street Project

From: Jeff Tarling
To: Barbara Barhydt ; Shukria Wiar
Date: 7/12/2012 12:16 PM
Subject: 54 York Street Project
CC: David Margolis-Pineo

Hi Shukria -

In review of the landscape portion of the proposed 54 York Street project I would recommend the replanting of one street tree on Maple Street to replace the dead tree.

Recommended tree type would be 'Armstrong' Red Maple or other upright species such as upright Pin Oak, or English Oak.

Jeff Tarling



PORTLAND MAINE

Strengthening a Remarkable City, Building a Community for Life • www.portlandmaine.gov

Planning & Urban Development Department
Gregory A. Mitchell, Acting Director

Planning Division
Alexander Jaegerman, Director

July 5, 2012

Dan Labrie
Rufus Deering Lumber Company
383 Commercial Street
Portland, ME 04101

Sebago Technics, Inc
James Seymour, PE
75 John Roberts Road, Suite 1A
South Portland, ME 04106

RE: Staff Review Comments for Level II Preliminary Site Plan

Project Name: Baxter Academy for Technology & Science Project ID: 2012-519
Address: 54 York Street CBL: 042-A-007
Applicant: Rufus Deering Lumber Company
Planner: Shukria Wiar

Dear Mr. Labrie;

Thank you for submitting an application for change of use for offices to Charter school at 54 York Street. The project consists of converting the existing Rufus Deering Lumber Milling Display Store into the Baxter Academy for Technology and Science; no building additions are proposed. This proposal is being reviewed as a preliminary plan for administrative review subject to the Site Plan Ordinance, Article V of Land Use Code provisions.

Zoning: Marge Schmuckal

I have reviewed the most current submission. The applicant wants to change the use from offices to a new school (a public charter high school). The property is located in a B-5b zone which allows the proposed use under, "Public or private schools of any type". There is no proposed addition to the building with the exception of an ADA ramp into the City's ROW along York Street. All changes would be interior to the existing building. There are two floors and a basement.

The B-5b zone has no minimum setbacks required. The 10' maximum setback along street lines is currently being met. There are no changes to change this existing condition. The maximum building height of 65' is more than being met with the two story and daylight basement in the existing building.

Section 14-332.1(g) exempts the B-5 & B-5b zones from providing off-street parking requirements in the zone. All HVAC equipment shall meet the sound level requirements listed under 14-230.5(b). Separate permits are required through Inspection Services for HVAC systems. Such application(s) shall provide HVAC dBA equipment levels.

Separate permits are required for any new exterior signs. Signs are also subject to Historic review before issuance.

Staff Review Comments for Level II Preliminary Plan:

1. Transportation Standards

- a. I would suggest that the traffic generation estimate be conducted for the full build-out of the school. Based upon the results of the trip generation estimate provided by the applicant, the project may require a Traffic Movement Permit. The applicant should provide all back-up survey information for review and approval. At this time I do not approve the results of the survey. As I noted previously to the applicant (at our meeting in Room 24 at City Hall), an actual traffic survey at a similar school is suggested.
- b. The applicant should base parking demand for the school on other similar sites. A demand for ten parking spaces does not seem to be sufficient for future full build-out conditions. The applicant should also provide parking options given the proposal for a development on the JB Brown parking lot identified for use by the school.
- c. The City does not support the provision of sidewalk ramps at the proposed handicap parking spaces on York Street. The applicant should document the need for these spaces. In general public curb side parking is not typically used for meeting handicapped parking requirements.
- d. The applicant should specify how pick-up and drop-off activity will occur for both students and buses.
- e. It is suggested that the applicant construct a sidewalk along the south side of York Street between High Street and the site, so students can safely walk to the school.
- f. The existing sidewalk infrastructure abutting the building should be reviewed for being ADA compliant and whether it meets City standards. I'm not sure if DPS staff has conducted this evaluation.

2. Environmental Quality Standards

- a. A landscaping plans needs to be submitted for review. Street trees are a requirement of site plan review and need to be proposed as part of the project. The site is reuse of an existing building and any opportunity to add landscaping should be considered.
- b. Water Quality, Storm Water Management and Erosion Control

3. Public Infrastructure and Community Safety Standards

- a. A boundary survey will need to be submitted for review.
- b. Availability and Adequate Capacity of Public Utilities- the final capacity letters will need to be submitted.
- c. Financial and technical letters will need to be submitted.
- d. The Brick Sidewalk Detail and Vertical Granite Curb Detail indicate an esplanade, which doesn't appear on the site plan. Please clarify.
- e. The Brick Sidewalk Detail should be modified to reflect the gravel depth requirements of Figure I-10 of the City of Portland Technical Manual.

4. Site Design Standards

- a. Snow and Ice Loading area will be to be shown on the site plan. If snow is to be removed, a detail plan will need to be submitted as how it will be handles.
- b. Historic Resources - this site is located in West End Historic District. Any exterior improvements and alteration, and signs will need to be reviewed by the Historic Preservation.
- c. Exterior Lighting- any lighting being proposed on the site or building? We will need to see a photometric plan, catalogue cuts, and specification on all proposed lighting.
- d. Noise and Vibration- Any HVAC and mechanical equipment being proposed need to be shown on the site plan and specification to be submitted.
- e. Signage and Wayfinding- a traffic signage plan needs to be submitted for review.

The Planning Authority may request additional information during the continued review of the proposal according to applicable laws, ordinances and regulations.

Please submit one (1) complete paper set of revised final plans and upload the digital plans and documents to address staff comments. Upon receipt of the revised material, the City of Portland will review the additional plans and information for conformance with applicable ordinances. Please be aware that an application expires within 120 days of the date upon which this written request for additional information was made.

If you have any questions, feel free to contact me at 756-8083 or by email at shukriaw@portlandmaine.gov

Sincerely,

Shukria Wiar
Planner

Electronic Distribution:

Alexander Jaegerman, Planning Division Director
Barbara Barhydt, Development Review Services Manager
Danielle West-Chuhta, Associate Corporation Counsel
Marge Schmuckal, Zoning Administrator
Katherine Earley, City Engineer, Public Services
David Margolis-Pineo, Deputy City Engineer
Captain Keith Gautreau, Fire
Jeff Tarling, City Arborist
Tom Errico, P.E., TY Lin Associates
Dan Goyette, P.E., Woodard & Curran

Parking and Transportation Plan

Baxter Academy for Technology and Science (BA) anticipates an enrollment of 160 14 and 15-year-old students for the 2012-13 school year. We will serve only grades 9 and 10 in year one, year two grade 9,10 & 11, and in year three we will serve grades 9-12. Students will be riding three buses 54 passenger school buses to and from the school located at 54 York Street in Portland. Our buses will serve the communities of the greater Portland region. The northern bus route will begin transporting students beginning at 7:00 am. at the Topsham Fair Mall parking lot. The southern bus route will begin in Kennebunk, and the western bus route will begin in Windham. All buses will arrive and depart from on Maple Street. Busses will arrive between 7:45 and 8:00 during the 180 day school year from mid September through mid June. Busses will depart at 2:00 pm.

Students from Portland, South Portland, Westbrook and Falmouth will be able to access the school via public transportation on the Metro system. We anticipate that 50% of our students will either walk, bike or use public transportation. Portland students who live on the peninsula will be within walking and biking distance to the school. BA will be providing bike racks at the school and encourage students from Portland to walk or bike to school. The school is also located within walking distance from Portland High School. In South Portland students will be encouraged to use the bike path leads to the Casco Bay Bridge. The school is less than a mile from the Casco Bay Bridge.

Since the school is located in the Old Port Historic District we will be providing parking only for a limited number of staff members. Leased parking spaces will be provided to staff members at the lot on the corner of Commercial and Maple Streets, this lot is owned by J.B. Brown and Sons. Next year we anticipate the needed number parking spots to be 10 for staff members. A survey of staff has indicated that the remaining staff members would walk, bike or ride public transportation to the school.

The policy for parking for students will be that if students choose to drive a vehicle to school they will be responsible for contracting a monthly parking spot. BA will not provide any student parking.

Bus drop off will be between 7:45 and 8 PM on Maple Street and a BA staff member will be outside to help manage student arrival and departure. Buses will be departing from Maple Street between 2:00 and 2:15 pm. We anticipate that morning arrival times will be staggered and no more than two buses on Maple Street at any one time.

Handicapped accessibility will be on York Street where there will be easy access to the main entrance of the building as the sidewalk and the entrance to the school level. We anticipate between 10 and 15 parents will be needing to pick up their students on a daily basis were dropping them off for doctors appointments or for late arrivals to school or early departures. We do not anticipate a large number of parental trips during the school day.

J.B. BROWN & SONS
36 Danforth Street
P.O. Box 207
Portland, ME 04112-0207
207-774-5908 (phone) 207-774-0898 (fax)

May 21, 2012

Baxter Academy
Attn: John Jacques
54 York Street
Portland, Maine 04101

Phone #: 207-318-8248
Account #: 11-baxtpk
e-mail: john@baxteracademy.org

Parking Lease Agreement

Dear John:

This document, when seen and agreed to by both parties, shall constitute a lease agreement between **J. B. Brown & Sons** (Landlord) and **Baxter Academy** (Tenant):

1. Parking Rights: Tenant shall have the right to park **ten (10)** authorized vehicles at the **321 Commercial Street**, Portland, Maine parking lot. Authorized vehicles are those whose year, make, model, color and license plate number have been provided to J.B. Brown & Son for inclusion on the parking list.

<u>Permit</u>	<u>Year</u>	<u>Color</u>	<u>Make</u>	<u>Model</u>	<u>License plate</u>
---------------	-------------	--------------	-------------	--------------	----------------------

* Descriptions must be provided prior to utilizing parking

2. Lease Terms: The parking agreement shall commence on September 1, 2012 **and it shall terminate on June 30, 2013**. The monthly rent of **Eighty dollars (\$ 80.00)** [discounted \$5.00 per spot as part of a group discount] per parking space shall be paid in advance, without notice from Landlord, on the first day of each month. **(Monthly total: \$800.00). Upon city approval for the school, a \$400 deposit will be required to hold these spaces.**

3. Tenant Responsibilities: It is the owner's responsibility to notify J.B. Brown & Sons of car make, year, color and registration number or any changes of that information. **Any car parked in the lot that is not on our tenant listing is subject to being towed at the owner's expense. Overnight parking is prohibited during the snow season. Vehicles prohibiting snow plowing or removal will be towed. All authorized vehicles MUST be operational and duly registered.**

4. Landlord's Responsibilities: J. B. Brown & Sons is responsible for snow removal but is not able to maintain consistent sanding and ice control. Parkers and guests park, walk, and drive in this lot at their own risk.

5. Hold Harmless Clause: Tenant shall hold J. B. Brown & Sons harmless for all losses or damages arising out of the use of the parking lot. J. B. Brown & Sons does not assume responsibility for Tenant's vehicle(s) nor any personal belongings.

6. Termination: Landlord and/or Tenant may cancel this parking agreement at any time by providing written notification thirty (30) days prior to the effective date of termination.

If the terms are satisfactory, please sign both documents and kindly return the one agreement to our office by **July 10, 2012**.

Sincerely,

Michelle Crowley
Office Manager

SEEN & AGREED TO BY:

For: Baxter Academy

City of Portland
Code of Ordinances
Sec. 14-332

Land Use
Chapter 14
Rev. 4-19-12

- (b) Motel: One (1) parking space for each sleeping room.
(c) Hotels: One (1) parking space for each four (4) guest rooms.

$\frac{1}{2}$ (d) Schools providing instruction for students up to and including those fifteen (15) years of age: One (1) parking space for each room used for purposes of instruction.

$\frac{1}{2}$ (e) Schools providing instruction for students sixteen (16) years of age and over: One (1) parking space for each ten (10) seats or major fraction thereof, used for purposes of instruction; if no fixed seats, one (1) parking space for each one hundred (100) square feet or major fraction thereof used for purposes of instruction.

PARKING TOTALS.

8 ROOMS PROPOSED FOR = 6805 SF

> 15yrs = 8 spaces / 2 = 4 spaces

< 15yrs = 6805/100 / 2 = 34 spaces

38 spaces *

25-50 spaces requires 2 HANDICAP PARKING

* SINCE THE PROJECT IS LOCATED IN AN HISTORIC DISTRICT
NO ON-SITE PARKING MAYBE REQUIRED.

PARKING SPACES will be provided in lease with
J.B BROWN & SONS - @ MAPLE ST. PARKING LOT.
(see attached lease doc.)

School Year 2012-2013 - Target enrollment 160 students and lease approximately 10,000 sq. ft.
 School Year 2013-2014 - Target enrollment 240 students and lease approximately 15,000 sq. ft.
 School Year 2014-2012 - Target Enrollment 320 students and lease approximately 20,000 sq. ft.

Ground-level York Street for occupancy September 2012

- Entryway 20' x 8'
 - Main office 16' x 13'
 - Media & student center 38' x 48' **1824**
 - Robotics and engineering classroom 38' x 22' **836**
 - Storage room for robotics & engineering 15' x 17'
 - Art room and Science lab 38' x 36' - room to be partitioned **1368**
 - Men's bathroom 14' x 8'
 - Women's bathroom 14 x 8'
- ≈ 4028 SF**

Top floor to be occupied for September 2012

- Server room 28' x 13'
 - Men's bathroom 12' x 9'
 - Women's bathroom 12' x 9'
 - Computer labs - 41' x 32' - room to be partitioned into two computer labs -- **1312**
 - Teachers room 15' x 8'
 - Classroom one 13' x 28' **364**
 - Classroom two 13' x 26' **338**
 - Classroom three 22' x 20' **440**
 - Classroom four 19' x 17' **323**
 - Upstairs admin office 16' x 16'
- = 2,777 SF**
- INSTRUCTIONAL FLOOR AREA = 6805 SF**

Best regards,
 John

Gulf of Maine Properties Tenant & Summary

Parking Totals:

Total Spots Available: **235**

- 48 spaces- USCG Area
- 99 spaces- gravel parking lot
- 88 spaces- GMRI paved lot

Total currently being used: **143**

Total current marine: **135** (GMRI, UMaine SMS, Fish Vet Group, Ocean Approved)

Total current non-marine: **8** (Image Works & Sgurr Energy)

Tenant Summary :

Gulf of Maine Research Institute (GMRI):

- The Gulf of Maine Research Institute is a non-profit marine science center located in Portland, Maine
 - Parking for all staff (55 people)

University of Maine School of Marine Sciences:

- Joint appointments between the University of Maine and the Gulf of Maine Research Institute expand the capacity of our interdisciplinary research team.
 - Parking for all staff (8-10 people)

Sgurr Energy:

- Sgurr Energy is a leading independent engineering consultancy specializing in renewable energy and focused on off shore wind projects.
 - Parking for 4 vehicles

Ocean Approved:

- Ocean Approved operates the first commercial kelp farm in the U.S., producing seaweed products with a commitment to sustainable practices.
 - Parking for all staff (5 vehicles)

Image Works:

- Image Works is a custom web software design and development firm founded in 1977 in Portland, Maine. The company develops web sites, Internet applications, networked educational exhibits and other interactive media for the purposes of marketing, customer service, e-commerce, content management/publishing and e-learning.

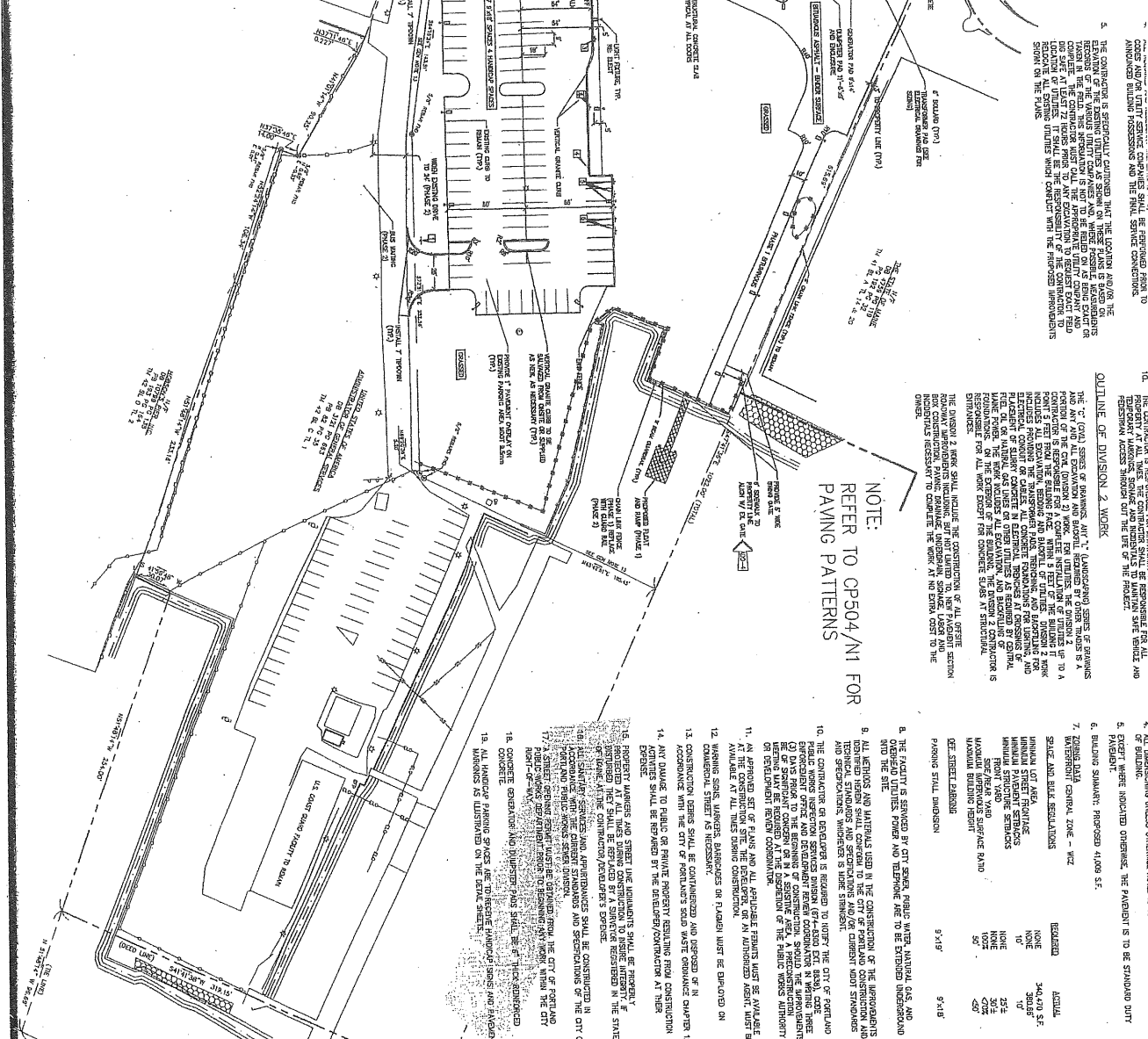
LEGEND/STIPPLE LEGEND

ROCK STRENGTH WITH BRITANNICA BASE

SCALE/TREATMENT LEGEND

ANTICIPATED VISUALS ONLY

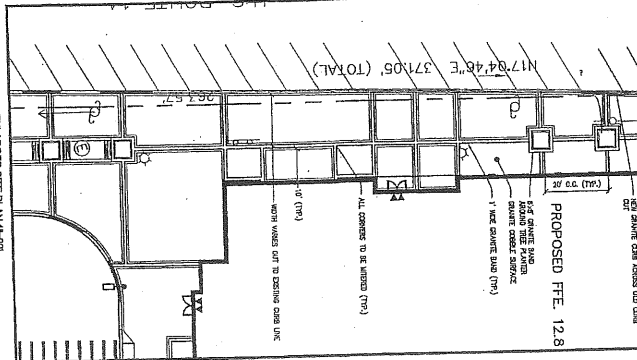
ROCK COVERS



NOTE:
REFER TO CP504/N1 FOR
PAVING PATTERNS

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION, MAINTENANCE AND REPAIR OF ALL UTILITIES AND SERVICES TO BE PROVIDED TO THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION, MAINTENANCE AND REPAIR OF ALL UTILITIES AND SERVICES TO BE PROVIDED TO THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION, MAINTENANCE AND REPAIR OF ALL UTILITIES AND SERVICES TO BE PROVIDED TO THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION, MAINTENANCE AND REPAIR OF ALL UTILITIES AND SERVICES TO BE PROVIDED TO THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION, MAINTENANCE AND REPAIR OF ALL UTILITIES AND SERVICES TO BE PROVIDED TO THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROVISION, MAINTENANCE AND REPAIR OF ALL UTILITIES AND SERVICES TO BE PROVIDED TO THE PROJECT.

ITEM	DESCRIPTION	QUANTITY	UNIT	REMARKS
1	CONCRETE	10,000	YD	
2	ASPHALT	5,000	YD	
3	GRAVEL	2,000	YD	
4	STEEL REINFORCEMENT	100	TON	
5	PAVING	10,000	SQ YD	
6	CONCRETE	5,000	YD	
7	ASPHALT	2,000	YD	
8	GRAVEL	1,000	YD	
9	STEEL REINFORCEMENT	50	TON	
10	PAVING	5,000	SQ YD	
11	CONCRETE	2,000	YD	
12	ASPHALT	1,000	YD	
13	GRAVEL	500	YD	
14	STEEL REINFORCEMENT	25	TON	
15	PAVING	2,000	SQ YD	
16	CONCRETE	1,000	YD	
17	ASPHALT	500	YD	
18	GRAVEL	250	YD	
19	STEEL REINFORCEMENT	12.5	TON	
20	PAVING	1,000	SQ YD	



PROPOSED FFL 12.8

ENLARGED VIEW PLAN 1-20'

PH

CLIENT ISSUE ST/

SM RT

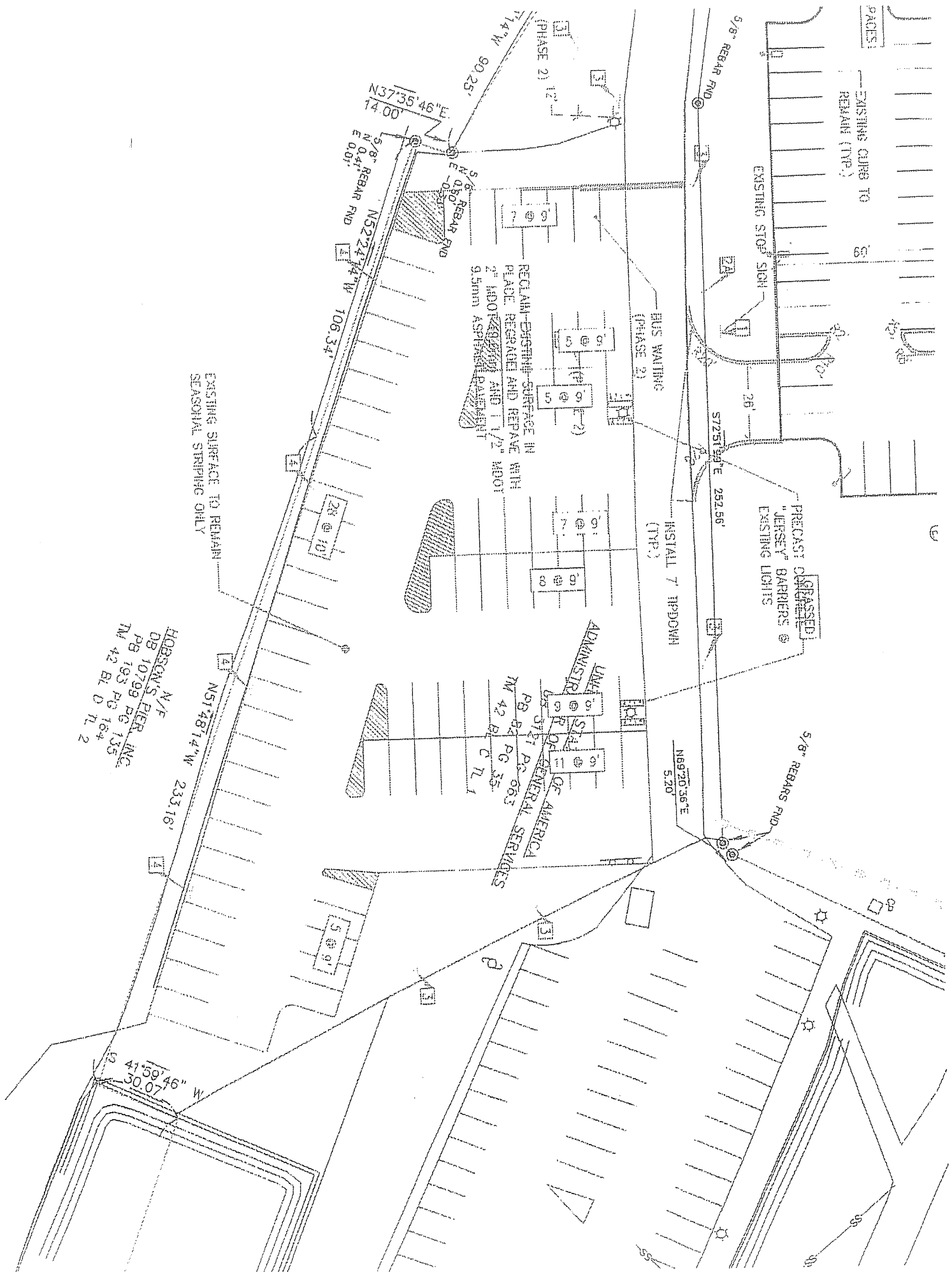
RE

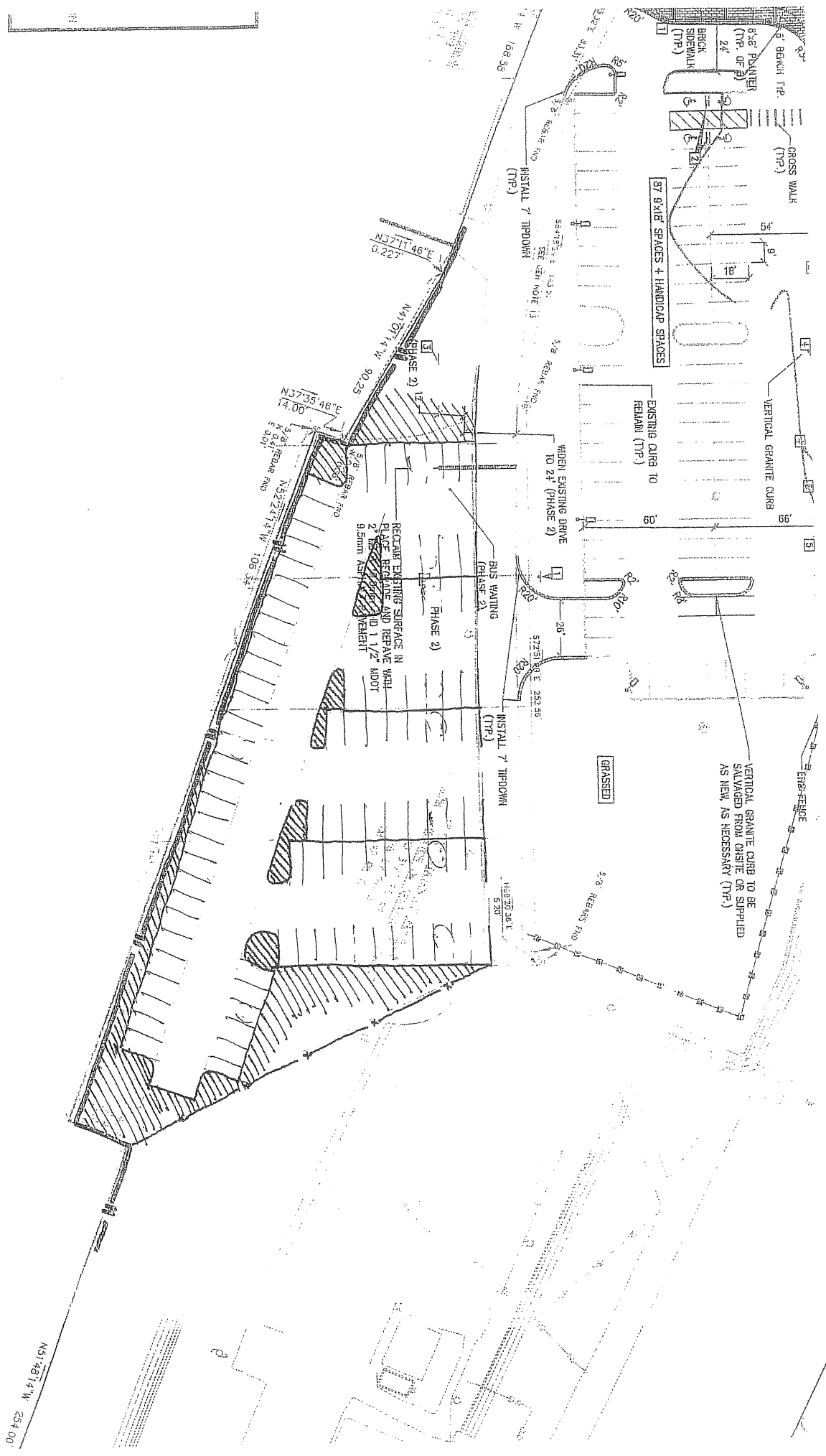
PROJECT

DATE

REV

1	ASSEMBLY
2	SMALL
3	ASSEMBLY
4	PHASE
5	SMALL
6	DESIGN
7	SECOND





BY 9"x18" SPACES + HANDICAP SPACES

VERTICAL GRANITE CURB TO BE SALVAGED FROM ON-SITE OR SUPPLIED AS NEW, AS NECESSARY (TYP.)

GRASSED

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

WIDEN EXISTING DRIVE TO 24' (PHASE 2)

BUS WAITING (PHASE 2)

RECENT EXISTING SURFACE IN CONCRETE AND REPAIR WITH 2" AGGREGATE AND 1 1/2" ADOT 9.5mm ASPHALT

PHASE 2

PHASE 2

VERTICAL GRANITE CURB

EXISTING CURB TO REMAIN (TYP.)

CROSS WALK (TYP.)

6"x6" PLASTER (TYP. OF 2)
BRICK SIDEWALK (TYP.)

VERTICAL GRANITE CURB

ENTRANCE

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

N57°48'14"W 234.00'

N37°35'48"E 14.00'

N32°27'14"W 106.34'

N37°11'46"E 0.222'

N41°07'14"W 90.25'

N37°35'48"E 14.00'

N32°27'14"W 106.34'

N37°11'46"E 0.222'

N41°07'14"W 90.25'

N37°35'48"E 14.00'

N32°27'14"W 106.34'

N37°11'46"E 0.222'

N41°07'14"W 90.25'

N37°35'48"E 14.00'

N32°27'14"W 106.34'

N37°11'46"E 0.222'

N41°07'14"W 90.25'

N37°35'48"E 14.00'

N32°27'14"W 106.34'

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

INSTALL 7' TIEDOWN (TYP.)

Parking and Transportation Plan

Baxter Academy for Technology and Science (BA) anticipates an enrollment of 160 14 and 15-year-old students for the 2012-13 school year. We will serve only grades 9 and 10 in year one, year two grade 9,10 & 11, and in year three we will serve grades 9-12. Students will be riding three buses 54 passenger school buses to and from the school located at 54 York Street in Portland. Our buses will serve the communities of the greater Portland region. The northern bus route will begin transporting students beginning at 7:00 am. at the Topsham Fair Mall parking lot. The southern bus route will begin in Kennebunk, and the western bus route will begin in Windham. All buses will arrive and depart from on Maple Street. Busses will arrive between 7:45 and 8:00 during the 180 day school year from mid September through mid June. Busses will depart at 2:00 pm.

Students from Portland, South Portland, Westbrook and Falmouth will be able to access the school via public transportation on the Metro system. We anticipate that 50% of our students will either walk, bike or use public transportation. Portland students who live on the peninsula will be within walking and biking distance to the school. BA will be providing bike racks at the school and encourage students from Portland to walk or bike to school. The school is also located within walking distance from Portland High School. In South Portland students will be encouraged to use the bike path leads to the Casco Bay Bridge. The school is less than a mile from the Casco Bay Bridge.

Since the school is located in the Old Port Historic District we will be providing parking only for a limited number of staff members. Leased parking spaces will be provided to staff members at the lot on the corner of Commercial and Maple Streets, this lot is owned by J.B. Brown and Sons. Next year we anticipate the needed number parking spots to be 10 for staff members. A survey of staff has indicated that the remaining staff members would walk, bike or ride public transportation to the school.

The policy for parking for students will be that if students choose to drive a vehicle to school they will be responsible for contracting a monthly parking spot. BA will not provide any student parking.

Bus drop off will be between 7:45 and 8 PM on Maple Street and a BA staff member will be outside to help manage student arrival and departure. Buses will be departing from Maple Street between 2:00 and 2:15 pm. We anticipate that morning arrival times will be staggered and no more than two buses on Maple Street at any one time.

Handicapped accessibility will be on York Street where there will be easy access to the main entrance of the building as the sidewalk and the entrance to the school level. We anticipate between 10 and 15 parents will be needing to pick up their students on a daily basis were dropping them off for doctors appointments or for late arrivals to school or early departures. We do not anticipate a large number of parental trips during the school day.

J.B. BROWN & SONS

36 Danforth Street
P.O. Box 207
Portland, ME 04112-0207
207-774-5908 (phone) 207-774-0898 (fax)

May 21, 2012

Baxter Academy
Attn: John Jacques
54 York Street
Portland, Maine 04101

Phone #: 207-318-8248
Account #: 11-baxtpk
e-mail: john@baxteracademy.org

Parking Lease Agreement

Dear John:

This document, when seen and agreed to by both parties, shall constitute a lease agreement between **J. B. Brown & Sons** (Landlord) and **Baxter Academy** (Tenant):

1. Parking Rights: Tenant shall have the right to park **ten (10)** authorized vehicles at the **321 Commercial Street**, Portland, Maine parking lot. Authorized vehicles are those whose year, make, model, color and license plate number have been provided to J.B. Brown & Son for inclusion on the parking list.

<u>Permit</u>	<u>Year</u>	<u>Color</u>	<u>Make</u>	<u>Model</u>	<u>License plate</u>
*	Descriptions must be provided prior to utilizing parking				

2. Lease Terms: The parking agreement shall commence on September 1, 2012 **and it shall terminate on June 30, 2013**. The monthly rent of **Eighty dollars (\$ 80.00)** [discounted \$5.00 per spot as part of a group discount] per parking space shall be paid in advance, without notice from Landlord, on the first day of each month. **(Monthly total: \$800.00)**. **Upon city approval for the school, a \$400 deposit will be required to hold these spaces.**

3. Tenant Responsibilities: It is the owner's responsibility to notify J.B. Brown & Sons of car make, year, color and registration number or any changes of that information. **Any car parked in the lot that is not on our tenant listing is subject to being towed at the owner's expense. Overnight parking is prohibited during the snow season. Vehicles prohibiting snow plowing or removal will be towed. All authorized vehicles MUST be operational and duly registered.**

4. Landlord's Responsibilities: J. B. Brown & Sons is responsible for snow removal but is not able to maintain consistent sanding and ice control. Parkers and guests park, walk, and drive in this lot at their own risk.

5. Hold Harmless Clause: Tenant shall hold J. B. Brown & Sons harmless for all losses or damages arising out of the use of the parking lot. J. B. Brown & Sons does not assume responsibility for Tenant's vehicle(s) nor any personal belongings.

6. Termination: Landlord and/or Tenant may cancel this parking agreement at any time by providing written notification thirty (30) days prior to the effective date of termination.

If the terms are satisfactory, please sign both documents and kindly return the one agreement to our office by **July 10, 2012**.

Sincerely,

Michelle Crowley
Office Manager

SEEN & AGREED TO BY:

For: Baxter Academy

City of Portland
Code of Ordinances
Sec. 14-332

Land Use
Chapter 14
Rev. 4-19-12

- (b) Motel: One (1) parking space for each sleeping room.
- (c) Hotels: One (1) parking space for each four (4) guest rooms.

$\frac{1}{2}$ (d) Schools providing instruction for students up to and including those fifteen (15) years of age: One (1) parking space for each room used for purposes of instruction.

$\frac{1}{2}$ (e) Schools providing instruction for students sixteen (16) years of age and over: One (1) parking space for each ten (10) seats or major fraction thereof, used for purposes of instruction; if no fixed seats, one (1) parking space for each one hundred (100) square feet or major fraction thereof used for purposes of instruction.

PARKING TOTALS.

8 ROOMS PROPOSED FOR = 6805 SF

> 15yrs = 8 spaces / 2 = 4 spaces

< 15yrs = 6805/100 / 2 = 34 spaces

38 spaces *

25-50 spaces requires 2 HANDICAP PARKING

* SINCE THE PROJECT IS LOCATED IN AN HISTORIC DISTRICT
NO ON-SITE PARKING MAYBE REQUIRED.

PARKING SPACES will be provided in lease with
J.B BROWN & SONS - @ MAPLE ST. PARKING LOT.
(see attached lease doc.)

School Year 2012-2013 - Target enrollment 160 students and lease approximately 10,000 sq. ft
School Year 2013-2014 - Target enrollment 240 students and lease approximately 15,000 sq. ft.
School Year 2014-2012 - Target Enrollment 320 students and lease approximately 20,000 sq. ft.

Ground-level York Street for occupancy September 2012

- Entryway 20' x 8'
 - Main office 16' x 13'
 - Media & student center 38' x 48' 1824
 - Robotics and engineering classroom 38' x 22' 836
 - Storage room for robotics & engineering 15' x 17'
 - Art room and Science lab 38' x 36' - room to be partitioned 1368
 - Men's bathroom 14' x 8'
 - Women's bathroom 14 x 8'
- 2 = 4028 SF

Top floor to be occupied for September 2012

- Server room 28' x 13'
 - Men's bathroom 12' x 9'
 - Women's bathroom 12' x 9'
 - Computer labs - 41' x 32' - room to be partitioned into two computer labs -- 1312
 - Teachers room 15' x 8'
 - Classroom one 13' x 28' 364
 - Classroom two 13' x 26' 338
 - Classroom three 22' x 20' 440
 - Classroom four 19' x 17' 323
 - Upstairs admin office 16' x 16'
- = 2,777 SF
- INSTRUCTIONAL FLOOR AREA = 6805 SF

Best regards,
John

MEMORANDUM



TO: Shukria Wiar, Planner
FROM: David Senus, P.E. & Ashley Auger, E.I.T.
DATE: June 12, 2012
RE: Baxter Academy, Preliminary Level II Site Plan Application

Woodard & Curran has reviewed the Preliminary Level II Site Plan Application for Baxter Academy located at 54 York Street in Portland, Maine. The project consists of converting the existing Rufus Deering Lumber Milling Display Store into the Baxter Academy for Technology and Science; no building additions are anticipated. The existing site is completely impervious; therefore, the project will not create any new impervious areas.

Documents Provided By Applicant

- Site Plan Application and attachments dated June 5, 2012, prepared by Sebago Technics, on behalf of Rufus Deering Lumber Co.
- Engineering Plans, Sheets 1-4, revised June 5, 2012, prepared by Sebago Technics, on behalf of Rufus Deering Lumber Co.

Comments

- 1) The Brick Sidewalk Detail and Vertical Granite Curb Detail indicate an esplanade, which doesn't appear on the site plan. Please clarify.
- 2) The Brick Sidewalk Detail should be modified to reflect the gravel depth requirements of Figure I-10 of the City of Portland Technical Manual.



Memorandum

Project: 07383

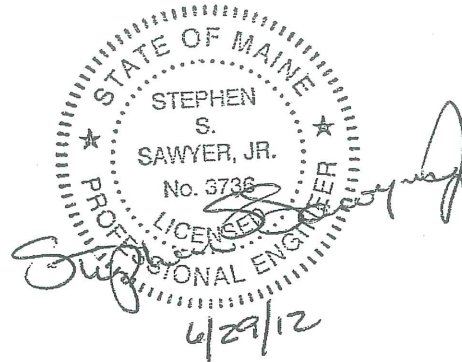
To: Tom Errico, TY Lin
Shukria Wiar, Portland Planning

From: Steve Sawyer

Date: June 29, 2012

Subject: Traffic Evaluation for Baxter Academy Charter School – York Street, Portland

CC: John Jacques, Baxter Academy
Dan LaBrie, Rufus Deering



This memorandum is to present relevant traffic information for your review with regard to the proposed Baxter Academy for Technology and Science Charter School at the corner of York Street and Maple Street in Portland.

Proposed Development Plan

The applicant is proposing a new charter school that is expected to attract between 100 and 120 14-15 year-old students (Grades 9 and 10) during its first year of operation, which is planned for the fall 2012. It is estimated that 50% of the enrollment will come from Portland, South Portland and Westbrook, with the balance originating from communities outside of the urban core. School operations will run from 8:00 AM to 2:00 PM, resulting in peak traffic associated with the school being between 7-8:00 AM and 2-3:00 PM.

Trip Generation

ITE's Trip Generation, 7th Edition, was consulted to determine what industry data suggests for this type of land use. LUC 536 - Private School (K-12) is the closest category listed in this reference. Believing that your greatest traffic concern would be during the AM peak hour because it will coincide with the morning commute, we have focused our attention on this time period only. Using a base school population of 120 students and 15 employees, we find the following for the AM peak hour using ITE information:

LUC		Rate	Variable	Total Trips
Using Average Rate per Student		0.79	120 (students)	95
Using Rate per Employee		5.31	15 (employees)	80
Average of Above				88

The above trip generation assumes that there is no existing traffic associated with the site today, which is not entirely true. Currently there is an exercise studio (Nancy's) that offers among other things, Zumba classes, and a Buddhist Center (Nagaloka), and the administrative office for the Baxter School operating in the building. However, given the operating hours of these three uses, we have not taken a credit for any existing trips during the AM peak hour (7:00 AM to 8:00 AM) in the analysis performed herein.

Parent Survey

The administration for the school conducted a Parent Survey of prospective students regarding their transportation preferences. A total of 24 families participated in this survey, which was performed earlier this month. The results indicated that approximately 25% of the families preferred to transport their children to and from school either by individual or car pool means. 70% envisioned using bus transportation, either charter service provided by the school or public transportation because they reside within Greater Portland. The remaining 5% indicated they would walk to and from the school. From this data, it can be presumed that the trip generation for this development during the AM peak hour will be less than the ITE figures presented above. Conservatively, our forecast is as follows based on a student population of 120 and 15 employees:

120 students x 35% using parent drop off (no discount for car pooling)	42 trips
15 employees @ 67% auto use	10 trips
3 buses for 60% of the students (passenger car equivalents)	5 trips
5% of students as walkers (6)	<u>0 trips</u>
Totals estimated trips	57 trips

Field Observations June 27, 2012

Sebago staff performed a traffic turning movement count at the intersection of York Street and Maple Street during the hours of 7:00 AM and 8:00 AM to observe traffic operations in and around the site during the time period when children would be arriving for the school day. Figure 1 presents the results of this peak hour count. As you can see, the volumes are quite small during this time period. Synchro/Simtraffic was used to analyze existing traffic operations at the intersection and it was determined that the Level of Service was indeed A.

In addition to this data collection and analysis effort, several general observations were made while conducting the count. These were:

1. Only one on-street parking space was used on Maple Street between Commercial Street and York Street during the one hour time period observed. This vehicle was a van that had parked there overnight.
2. The paved parking lot for "The Cannery" that is accessed off Maple Street only had 8 cars enter it between 7:00 AM and 8:00 AM.
3. Maple Street is barely used during this time period. Total traffic during the hour observed was 34 vehicles. Never was a queue experienced waiting to enter onto

York Street, and with the exception of one overnight parked vehicle, no on-street parking spaces were occupied.

4. The two on-street parking spaces to the west of the proposed school building on York Street were not occupied during the observation period.
5. While the traffic on York Street was predominantly in-bound, it appeared that the time period 7:00 AM to 8:00 AM might be before the actual peak morning commute hour.

Parking

The school has reserved 10 parking spaces for staff in a lot owned by J.B. Brown at the corner of Commercial and Maple Streets. No student parking is being provided, nor would any be expected to be needed given the age of the students.

Pick-Up and Drop-Off Locations

Given the apparent low usage of Maple Street, the school plans to locate its charter bus drop-offs and pick-ups on the westerly side of this street curbside adjacent to the school. Parent drop-offs will be directed to Maple Street, as well. The school will provide a crossing guard on Maple Street and also on York Street for those students that walk or utilize public transportation during arrivals and departures.

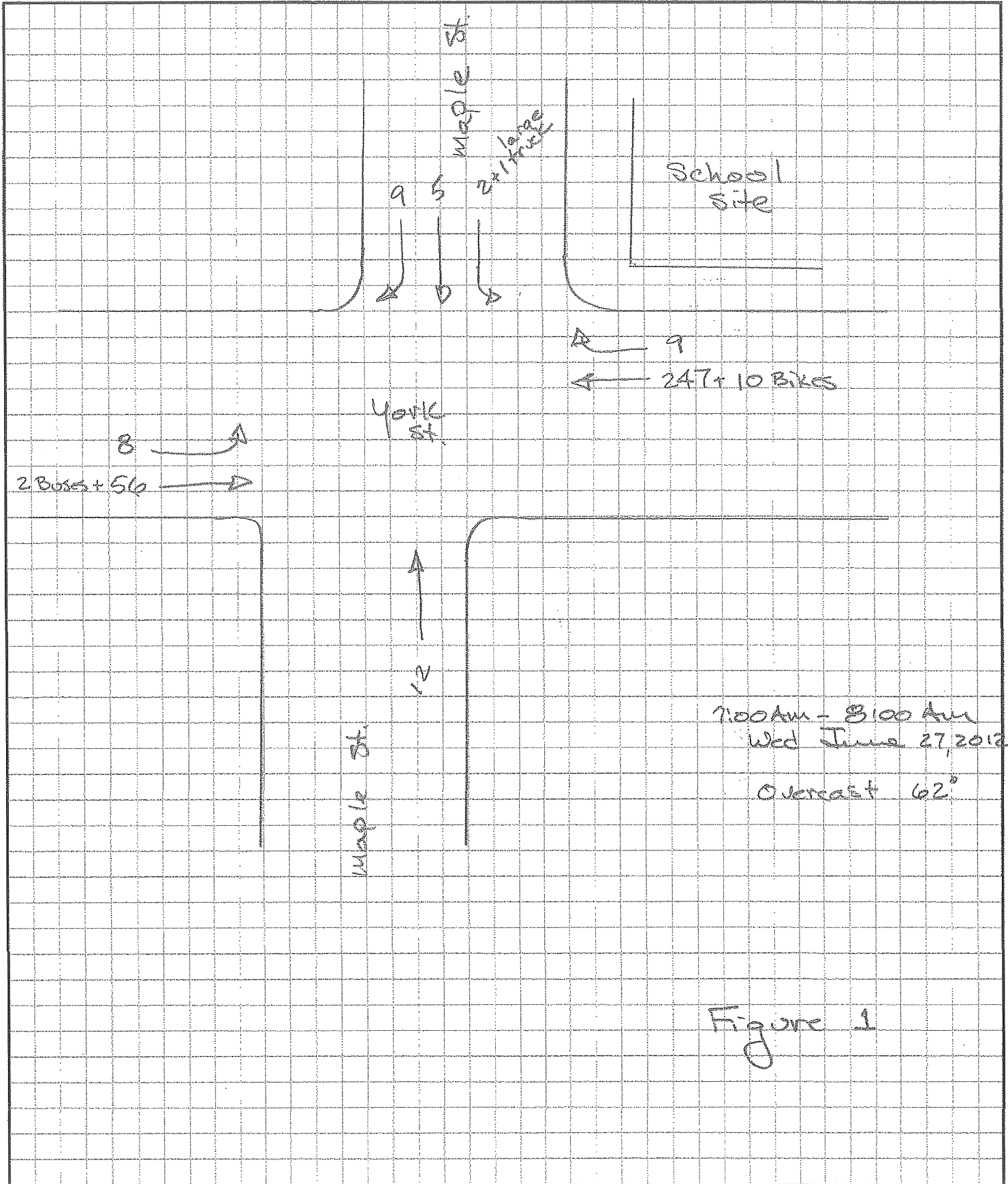
Two existing parking spaces located on York Street to the west of the school building on the south side of the street will be upgraded to handicap accessible. Sidewalk improvements will also be made to facilitate access to the school's main entrance for users of these spaces.

Conclusions

Based on our field observations and information provided by the school administration, we conclude that the establishment of a charter school as proposed by Baxter Academy at the level of 100-120 students will not create a traffic or parking concern for the City. The estimated trip generation using ITE rates is less than the threshold for a Traffic Movement Permit - our forecast is only 57 trips in the AM peak hour. Observations made at the site during the time at which students and staff would be arriving revealed relatively light traffic on York Street and virtually no traffic or parking on Maple Street. For these reasons, we do not believe that the proposed school as proposed will create a traffic or parking concern for the City.

SEBAGO TECHNICS, INC.
P.O. Box 1339
Westbrook, ME 04098
(207) 856-0277 FAX (207) 856-2206

JOB 07383 Baxter School
SHEET NO. _____ OF _____
CALCULATED BY SSS DATE 6/27/12
CHECKED BY _____ DATE _____
SCALE _____





81°
Partly Sunny
RealFeel® 95°
High: 84° | Low: 62°
High Tide: 11:32AM
Low Tide: 5:20PM

Wednesday, July 18, 2012

Subscribe today

- News
- Sports
- Business
- Opinion
- Life & Culture
- People
- Obituaries
- Connect With Us
- Classified Ads
- Jobs
- Cars
- Real Estate
- Deals
- Maine Yellow Pages
- blogs
- facebook
- twitter

Save This Story E-mail This Story Print This Story Large Type

1:00 AM

Board OKs two charter schools, one in Portland

The opening of a science-oriented high school in the city is reset to 2013 amid a time crunch and mayoral skepticism.

By Tom Bell tbell@mainetoday.com
Staff Writer

AUGUSTA – A state commission voted Tuesday to approve the opening of two charter schools, a science-oriented high school in Portland in 2013 and an elementary school in Cornville in the coming school year.

The votes mean that within 14 months, Maine will have at least three charter schools, which have been allowed in 40 other states but are new to Maine.

A law passed last year allows the new Maine Charter School Commission to authorize as many as 10 charter schools over the next 10 years.

Last month, the commission approved plans for the Maine Academy of Natural Sciences, a magnet school that has operated since September on the campus of the Good Will-Hinckley Home in Fairfield. It will now be joined by the Baxter Academy School of Technology and Science and the Cornville Regional Charter School.

"We're off to a great start with these approvals," Education Commissioner Stephen Bowen said in a prepared statement after Tuesday's votes. He said charter schools foster innovation and flexibility.

But many public school systems, including Portland's, see charter schools as a threat because they draw funding and students away from traditional public schools.

Portland Mayor Michael Brennan said the Baxter Academy proposal should have been rejected because it doesn't stand up to scrutiny. The commission approved Baxter Academy only after its proponents announced that they would open the school in 2013 rather than 2012, as initially proposed.

The delay reflects concerns that opponents raised about the school's ability to be ready by this September, Brennan said.

He said in an interview that the commission faces pressure from Gov. Paul LePage, Bowen and lawmakers to approve charter schools.

"It would have been embarrassing to the governor, the commissioner and the Legislature if no charter schools were approved this time around," he said. "I think the commission bent over backward to be accommodating, particularly with the Baxter school."

On June 11, frustrated with what he considered to be the commission's slow pace for approving charter schools, LePage sent a letter urging it to act as quickly as possible. He said members who were not up to the task should resign.

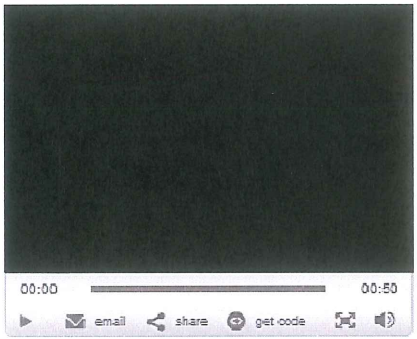
Commissioner Lynda Doyle said LePage's letter did not pressure the commission. "If anything, it was annoying," she said.

Doyle said the commission has a lot of integrity. "Nobody on that commission is in anybody's pocket," she said.

The one-year delay for Baxter Academy allayed the commission's worries that the proponents would fail to transform a former call center at 54 York St. into a school by September.

The delay gives the proponents more time to raise money, easing commissioners' concerns that the school's budget was so tight that it would risk running out of cash.

James Banks Sr., the commission's chair, said he was impressed with the group's revised application, which included a



Most...

- READ
- SHARED
- COMMENTED
- Dashboard cam captures trooper's split-second maneuvers
- Bill Nemitz: Real Maine ingenuity lost in the din of fireworks
- Ariz. sheriff insists Obama birth certificate is fake
- Teens kill disabled man, post video on Facebook

This portion of the requested page has been blocked.
[Click here for details.](#)

fundraising plan put together by Andrea Berry, who two months ago became president of the school's board of directors.

Berry was involved with a charter school in Boston and oversees fundraising for Idealware, a Portland-based nonprofit that helps other nonprofits buy software.

The school's curriculum will focus on math, science and engineering.

Shaun Meredith, who founded the software development firm InfoBridge, told the commission that technology companies in Maine will support the school because they need workers with technology skills.

Fifty-one students have applied to enroll in the school, which aims to have 160 students, starting with the ninth and 10th grades in the first year. Twenty percent of the applicants live in Portland.

In a separate vote, the commission approved the plan for a charter school in Cornville starting this fall. Parents who opposed the closure of Cornville's elementary school two years ago developed the proposal.

The commission rejected the plan two weeks ago on a 3-3 vote, but the proponents convinced the commission to reconsider after submitting a revised budget that showed a \$25,000 surplus after the first year. The school will have at least 45 students.

Proponents say they expect the school will open before Oct. 1.

Some commissioners had expressed concerns that the school's program would not be different enough from traditional public school to justify a charter.

But proponents argued that the school will be different, offering a longer school day, a personal learning plan for every student, a gardening program, and the ability to schedule social studies, science and health programs for the same period for all grades, kindergarten through sixth.

"This is not only an opportunity to bring back our community school, but build a dream school for teachers, students and parents," said Justin Belanger, a painting contractor who will receive a \$1,000 annual salary as the school's director for the first year.

Staff Writer Tom Bell can be contacted at 791-6369 or at:

tbell@pressherald.com

Twitter: TomBellPortland

Were you interviewed for this story? If so, please fill out our accuracy form

Send Question/Comment to the Publisher

This portion of the requested page has been blocked.
[Click here for details.](#)

Tweet

Further Discussion

Here at PressHerald.com we value our readers and are committed to growing our community by encouraging you to add to the discussion. To ensure conscientious dialogue we have implemented a strict no-bullying policy. To participate, you must follow our [Terms of Use](#).

Questions about the article? Add them below and we'll try to answer them or do a follow-up post as soon as we can.

Technical problems? Email them to us with an exact description of the problem. Make sure to include:

Type of computer or mobile device you are using

Exact operating system and browser you are viewing the site on (TIP: You can easily determine your operating system here.)

Like

Add New Comment

Required: Please login below to comment.

Type your comment here.

Image

Post as ...

Showing 0 comments

Sort by Popular now Subscribe by email Subscribe by RSS

Real-time updating is enabled. (Pause)

[Home](#) | [News](#) | [Weather](#) | [Sports](#) | [Business](#) | [Opinion](#) | [Blogs](#) | [Obituaries](#) | [Classified Ads](#)



[Place an Online Ad](#) | [Place a Print Ad](#) | [Place a Classified Ad](#) | [Privacy Policy](#) | [Reader Services](#) | [Contact us](#)

Copyright ©2012 MaineToday Media, Inc.

Trackback URL <http://disqus.com/forums/t>



76°
Partly Sunny
RealFeel® 87°
High: 79° | Low: 60°
High Tide: 6:06PM
Low Tide: 12:27AM

Wednesday, July 11, 2012

Subscribe today

- News
- Sports
- Business
- Opinion
- Life & Culture
- People
- Obituaries
- Connect With Us
- Classified Ads
- Jobs
- Cars
- Real Estate
- Deals
- Maine Yellow Pages
- blogs
- facebook
- twitter

Save This Story E-mail This Story Print This Story Large Type

July 3

Charter school for Portland put off for now The commission charged with approving schools seeks more information on business plans.

By Steve Mistler smistler@pressherald.com
Staff Writer

AUGUSTA — A charter school proposed in Portland will get more scrutiny before the project moves forward.

The Maine Charter School Commission delayed action Monday on the application by Baxter Academies of Maine, one of four groups that want state charters under a year-old law that allows charter schools in Maine for the first time.

An application for a 50-student charter school in Cornville failed. The commission rejected a motion to begin negotiating the charter contract if supporters demonstrated that the school could remain financially stable if it fell short of enrollment projections.

Commission members also wanted more financial information before approving contract deliberations that could allow the Baxter Academy of Technology and Science to open this year.

Both decisions reflected ambivalence by the board charged with authorizing as many as 10 charter schools over the next 10 years. The commission began reviewing applications in May with the hope of approving some for the upcoming school year.

On Monday, members repeatedly expressed concerns about the short time they have to sign off on business plans that may well determine the viability of the charter schools.

"We want good charter schools," said commission member Richard Barnes. "We're only allowed to authorize 10 in 10 years. I'm worried that if we stumble here (the schools) will stumble."

The politics of charter schools has added pressure to the commission's deliberations. Gov. Paul LePage, an avid supporter of charter schools, wrote a letter to the commission in mid-June urging it to approve applications for two virtual schools.

The governor wrote that the commission has the time -- and a mandate -- to expand educational opportunities. "If any members of the commission are not up to meeting the state's expectations, I urge their resignation," he wrote.

The commission voted to delay reviewing the virtual schools' applications until next year, rejecting LePage's assertion that its reluctance to approve applications reflected the influence of interest groups traditionally opposed to charter schools -- public school districts and teachers unions.

A charter school is a public school that receives public funding but is created and operated by parents, teachers and community leaders, and is largely exempt from the rules and regulations of the area's school district.

Supporters say charters fit niche students and can offer a tailored curriculum that public schools can't. Opponents say that many charter schools fail because of faulty business plans, and that they can hurt public schools by siphoning students and public funding.

Commission members expressed several concerns about Baxter Academies, including whether renovations to a building it would use, at 54 York St. in Portland, would carry into the school year.

A bigger concern, members said, is whether Baxter Academies is financially stable.

The charter commission delayed its final vote on the application until later this month. John Jaques, executive director of Baxter Academies, said the school could operate in 2012-13 if the commission votes on the application by July 17.

Jaques expressed frustration with the commission for "moving the target," a reference to commission members' requests for updated budget data in case the school falls short of its 160-student projection.

TODAY'S POLL: CHARTER SCHOOLS
Is the state moving too slowly to set up charter schools?

Yes

No

View Results

Most...

READ	SHARED	COMMENTED
1 man killed, 2 wounded in Portland's first homicide of 2012		
Bill Nemitz: Mortified by a geysers of trash talk from Maine's governor		
Teen's leg severed after boat strikes rock in Maine lake		
Gorham town councilor pleads not guilty to drunken driving		

Today's poll: Charter schools
Is the state moving too slowly to set up charter schools?

Yes

No

View Results

This portion of the requested page has been blocked.
[Click here for details.](#)

Baxter Academies has already presented adjusted budget numbers for 100 students.

After the meeting, Jaques said concerns about the proposed school budget are overblown. He said schools that focus on science, technology, engineering and mathematics have widespread support from national political and business leaders. Such support, he said, would ensure adequate funding for the Baxter Academy of Technology and Science.

"It's very tough to attract funding for schools that have not been approved," Jaques said.

Monday's decision followed Baxter Academies' announcement last week that it had a \$500,000 line of credit, secured by an anonymous benefactor from Tennessee, to assist with start-up costs.

Supporters say the credit should reassure state officials that the school would be financially sound. Critics say the line from SunTrust Bank of Knoxville, Tenn., obtained just two weeks ago, shows desperation and raises questions about the validity of the school's financial plan.

Jaques remained confident Monday that the academy will be approved this month.

The future was bleaker for the Cornville Regional Charter School, a proposal to educate 50 students from kindergarten through grade 6 at the former Cornville Elementary School.

Supporters had hoped the commission would be receptive to the group's 650-page application.

Some commission members said they were inclined to move the proposal into the contract phase, but others worried that the proposal focused too much on the value of small-school education and not enough on innovation.

The proposal was led by parents who hoped to reopen the elementary school after School Administrative District 54 moved to close it in 2010. Cornville residents later voted to close the school rather than pay \$597,000 to keep it open.

Despite its vote, the commission welcomed proponents of the Cornville charter school to reapply next year.

Justin Belanger, chairman of the Cornville charter board, was not optimistic. He said the board had 55 written commitments from parents who hoped to send their children to the school in the fall. Despite that support, Belanger worried that it would be difficult to convince taxpayers to pay the \$25,000 needed to keep the vacant building heated and maintained for another year.

Last week, the commission voted to begin contract deliberations for the Maine Academy of Natural Sciences, a 40-student school in Fairfield. The academy is slated to become Maine's first charter school.

State House Writer Steve Mistler can be contacted at 791-6345 or at: smistler@mainetoday.com

Were you interviewed for this story? If so, please fill out our accuracy form

Send Question/Comment to the Publisher

This portion of the requested page has been blocked.
[Click here for details.](#)

Tweet

Further Discussion

Here at [PressHerald.com](#) we value our readers and are committed to growing our community by encouraging you to add to the discussion. To ensure conscientious dialogue we have implemented a strict no-bullying policy. To participate, you must follow our [Terms of Use](#).

Questions about the article? Add them below and we'll try to answer them or do a follow-up post as soon as we can.

Technical problems? Email them to us with an exact description of the problem. Make sure to include:

Type of computer or mobile device your are using

Exact operating system and browser you are viewing the site on (**TIP:** You can easily determine your operating system [here](#).)

Like

Add New Comment

Required: Please login below to comment.

Type your comment here.

Image

Post as ...

Showing 3 comments

Sort by Popular now [Subscribe by email](#) [Subscribe by RSS](#)

Real-time updating is enabled. (Pause)

[Home](#) | [News](#) | [Weather](#) | [Sports](#) | [Business](#) | [Opinion](#) | [Blogs](#) | [Obituaries](#) | [Classified Ads](#)



[Place an Online Ad](#) | [Place a Print Ad](#) | [Place a Classified Ad](#) | [Privacy Policy](#) | [Reader Services](#) | [Contact us](#)

Copyright ©2012 MaineToday Media, Inc.



Justin Belanger 1 week ago

The school in Cornville would have served up to 120 kids. It was about taking education back, providing a safe well rounded, and student centered educational choice to parents in the surrounding area. It was about providing a positive example for a troubled school district to follow, or lose students to our competition. It was about choice, the American dream of self determination, but most importantly it was about doing what is right for the students. When will people stop thinking money money money and think about the students?

tomuchbs and 1 more liked this [Like](#) [Reply](#)



RevoGen 1 week ago

I wonder how the various charter schools business plans stack up against the one the public system has...

Thistle liked this [Like](#) [Reply](#)



tomuchbs 1 week ago

Stop dragging your feet. The public schools are a continous waste of taxpayer money and they fail our kids. The two best high schools in the state are Catholic. Charter schools will take out the dead wood and the waste in the public system. Maine needs educational alternatives to what we have been wasting our money on in these public systems.

RevoGen liked this [Like](#) [Reply](#)

Trackback URL <http://disqus.com/forums/t>



76°
Partly Sunny
RealFeel® 87°
High: 79° | Low: 60°
High Tide: 6:06PM
Low Tide: 12:27AM

Wednesday, July 11, 2012

Subscribe today

- News
- Sports
- Business
- Opinion
- Life & Culture
- People
- Obituaries
- Connect With Us
- Classified Ads
- Jobs
- Cars
- Real Estate
- Deals
- Maine Yellow Pages
- blogs
- facebook
- twitter

Save This Story E-mail This Story Print This Story Large Type

July 2

Portland charter school decision delayed

By Steve Mistler smistler@pressherald.com
Staff Writer

AUGUSTA -- A proposed charter school in downtown Portland will undergo more scrutiny before the project will move forward.

The Maine Charter School Commission delayed action on an application by Baxter Academies of Maine, one of four groups angling for state charters under a year-old law that allows charter schools in Maine for the first time.

RELATED HEADLINES
\$500,000 loan could boost charter school's bid
First Maine charter school to be in Fairfield

A second application for a 50-student charter school in Cornville failed after the commission voted down a motion to begin negotiating the charter contract with the stipulation that project supporters demonstrate that the school could remain financially stable if it fell short of enrollment projections.

Similarly, members of the authorizing commission wanted more financial information from Baxter Academies before green-lighting contract deliberations that could allow the school to open this year.

Both moves reflected ambivalence by a board charged with authorizing 10 charter schools over the next 10 years. The commission began reviewing applications in May with the hope approving some for the upcoming school year.

Board members Monday repeatedly expressed concerns about the short window to sign off on business plans that may well determine the viability of the charter schools.

"We want good charter schools," board member Richard Barnes said. "We're only allowed to authorize 10 in 10 years. I'm worried that if we stumble here, they (the schools) will stumble."

The politics of charter schools has added pressure to commission deliberations. Gov. Paul LePage, an avid supporter of charter schools, in mid-June wrote a letter to the commission urging it to approve applications for two virtual schools. The governor wrote that the board had the time -- and a mandate -- to expand educational opportunities.

"If any members of the commission are not up to meeting the state's expectations, I urge their resignation," LePage wrote.

Nonetheless, the board ultimately voted to delay reviewing the virtual school applications until next year. Commission members rejected LePage's assertion that its reluctance to approve applications reflected the influence of interest groups traditionally opposed to charter schools, public school districts and teachers unions.

In the case of Baxter Academies, members of the commission had several concerns, including whether renovations to the proposed building at 54 York St. would carry into the school year. A bigger concern, board members said, was whether Baxter Academies was financially stable.

The charter commission delayed a final vote on the application until later this month. John Jaques, executive director of Baxter Academies, said the school could still operate this year if the commission votes on the charter application by July 17.

Jaques expressed frustration with the commission for "moving the target," a reference to commission members' requests for updated budget data if the school falls short of its 160-student projections. Baxter Academies has already presented adjusted budget numbers for 100 students.

In an interview after the meeting Jaques said concerns about the proposed school budget were overblown. He said science, technology, engineering mathematics schools, or STEM schools, enjoyed widespread support from national political and business leaders. Such support, he said, would ensure that Baxter would receive adequate funding.

"It's very tough to attract funding for schools that have not been approved," Jaques said.

The board decision follows the Baxter Academies announcement last week that it secured a \$500,000 line of credit secured by an anonymous benefactor from Tennessee to assist with the start-up costs. Supporters of the Baxter Academy of Technology and Science say the anonymous loan should reassure state officials that the school would be financially sound.

Most...

- | READ | SHARED | COMMENTED |
|--|--------|-----------|
| 1 man killed, 2 wounded in Portland's first homicide of 2012 | | |
| Bill Nemitz: Mortified by a geyser of trash talk from Maine's governor | | |
| Teen's leg severed after boat strikes rock in Maine lake | | |
| Gorham town councilor pleads not guilty to drunken driving | | |

This portion of the requested page has been blocked.
[Click here for details.](#)

Critics said the loan from SunTrust Bank of Knoxville, Tenn., obtained just two weeks ago, shows desperation and raises questions about the validity of the school's financial plan. Jaques remained confident Monday that the academy would be approved later this month.

The future was more bleak for the Cornville Regional Charter School, a proposal to educate 50 students from kindergarten through grade 6 at the former Cornville Elementary school. Supporters had hoped the commission would be receptive to the group's 650-page application.

Some board members said they were inclined to move the Cornville into the contract phase. However, others worried that the proposal focused too much on the value of small-school education and not enough on innovation.

The proposal was led by parents who hoped to reopen the elementary school after School Administrative District 54 moved to close it in 2010. Cornville residents later voted to shutter the school rather than pay \$597,000.

Commission member Jana Lapoint worried that accepting the Cornville proposal would set a precedent for other applications hoping to reopen rural schools. She also expressed concerns that the charter school could hurt SAD 54.

Despite the vote the board welcomed proponents of the Cornville charter school to reapply next year. Justin Belanger, chairman of the Cornville charter board, was not optimistic.

He said the Cornville board had received 55 written commitments from parents hoping to send their children to the school in the fall. Despite that support, Belanger worried that it would be difficult to convince taxpayers to pay the \$25,000 needed to keep the shuttered building heated and maintained for another year.

"It's going to be a difficult thing to tell 55 people tonight that this isn't happening," he said. "The board worked their (butts) off, working hundreds, maybe thousands of hours on this. It was a lot of money and a lot of time."

Last week the commission voted to begin contract deliberations for the Maine Academy of Natural Sciences, a 40-student school in Hinckley, Maine. The academy is slated to become the state's first charter school.

A charter school is a public school that receives public funding but is created and operated by local parents, teachers and community leaders, yet largely exempt of the rules and regulations of the area school district.

Supporters say charters fit niche students and can offer a tailored curriculum that public schools can't. Opponents counter that many charter schools fail due to faulty business plans and that they can hurt public schools by siphoning students and public funding.

Were you interviewed for this story? If so, please fill out our accuracy form

Send Question/Comment to the Publisher

This portion of the requested page has been blocked. [Click here for details.](#)

Tweet

Further Discussion

Here at [PressHerald.com](#) we value our readers and are committed to growing our community by encouraging you to add to the discussion. To ensure conscientious dialogue we have implemented a strict no-bullying policy. To participate, you must follow our [Terms of Use](#).

Questions about the article? Add them below and we'll try to answer them or do a follow-up post as soon as we can.

Technical problems? Email them to us with an exact description of the problem. Make sure to include:

Type of computer or mobile device you are using

Exact operating system and browser you are viewing the site on (TIP: You can easily determine your operating system [here](#).)

Like 1 person liked this.

Add New Comment

Required: Please login below to comment.

Type your comment here.

Image

Post as ...

Showing 11 comments

Sort by [Popular now](#) Subscribe by email Subscribe by RSS

Real-time updates is enabled. (Pause)



 **TWMaine** 1 week ago

"A charter school is a public school that receives public money -- state, local and federal -- but is created and operated by local parents, teachers and community leaders, free of the rules and regulations of the area school district."

This is the storybook version of charter schools. If you look around the country you'll see that charter schools are often operated by for-profit businesses that operate multi-campus charter networks. These are often backed by wealthy donors and business interests who represent a very small portion of the public and have a very narrow understanding of the role of education.

 mainer58 and 4 more liked this Like Reply

 **EddyL** 1 week ago in reply to TWMaine


What I've been saying since the getgo.

 windjammer liked this Like Reply

 **Nudwick** 1 week ago

Schools for profit are a bad idea. Their main concern is their bottom line, not education.

 TWMaine and 3 more liked this Like Reply

 **EddyL** 1 week ago in reply to Nudwick

Check out the website and see how top heavy this is in management types.

 windjammer liked this Like Reply


 **prettyfoolish** 1 week ago

"If any members of the commission are not up to meeting the state's expectations, I urge their resignation," LePage wrote. 'Bullying and threatening YET again?? Why do I think it is LePage who got an anonymous fax form a special interest

 mainer58 and 3 more liked this Like Reply

 **mainer58** 1 week ago

"If any members of the commission are not up to meeting the state's expectations, I urge their resignation," LePage wrote. What a silly thing to say. To turn that around, we could say, "If any governor is not living up to our expectations, we urge their resignation." I think it is good that the commission is taking their time to check everything over and should not be on a time schedule.

 EddyL and 3 more liked this Like Reply

 **RATHayer** 1 week ago

The charter commission is just dragging its feet. That's what you get when you ask people who like the status quo to be on a commission that is responsible for initiating change. ENOUGH! Southern Maine needs a STEM school and Baxter Academy deserves a chance.

Like Reply



prettyfoolish 1 week ago in reply to RATHayer

what's the hurry? we have 10 years to get it right.

mainer58 and 3 more liked this

Like Reply



EddyL 1 week ago in reply to prettyfoolish

They have 40 applicants according to their website.

windjammer liked this

Like Reply



EddyL 1 week ago in reply to RATHayer

What is Baxter Academies and how long has it been in existence? What others schools does it operate? Why would a bank in Tennessee cough up a half million for a start-up charter school in Portland?

Answers?

mainer58 and 1 more liked this

Like Reply



Joe 1 week ago in reply to RATHayer

The children who would be students need a chance with a secure institution. They are the first priority. It is also public money being pumped in after all.

mainer58 and 1 more liked this

Like Reply

Reactions



NancyEH 1 week ago

From Twitter

MT @stevemistler: Portland #charter school decision delayed <http://t.co/cQQLK5tw> #nepolitics #MEschools



PressHerald 1 week ago

From Twitter

Portland charter school decision delayed <http://t.co/rTO4fTwk>



stevemistler 1 week ago

From Twitter

Portland charter school decision delayed | The Portland Press Herald / Maine Sunday Telegram <http://t.co/TpAydIsp> via @feedly #nepolitics

Trackback URL <http://disqus.com/forums/>



78°
Sunny
RealFeel® 83°
High: 79° | Low: 62°
High Tide: 7:36PM
Low Tide: 2:05AM

Friday, June 29, 2012

Subscribe today

- News
- Sports
- Business
- Opinion
- Life & Culture
- People
- Obituaries
- Connect With Us
- Classified Ads
- Jobs
- Cars
- Real Estate
- Deals
- Maine Yellow Pages
- blogs
- facebook
- twitter

Save This Story E-mail This Story Print This Story Large Type

Yesterday at 9:30 AM

\$500,000 loan could boost charter school's bid But critics of the Baxter Academy plan in Portland raise questions about the school's financial viability.

By Tom Bell tbell@mainetoday.com
Staff Writer

PORTLAND - A \$500,000 line of credit secured by an anonymous benefactor from Tennessee would fund the start-up costs of a charter school proposed in Portland.

Supporters of the Baxter Academy of Technology and Science say that loan should assure state officials that the school would be on sound financial footing when it opens in September.

Critics say the loan from SunTrust Bank of Knoxville, Tenn., obtained just two weeks ago, shows desperation and raises questions about the validity of the school's financial plan. They are urging a state board to reject the group's application for a charter school.

"They are frankly running around at this point, trying to put together a financial package to make it work," said Portland Mayor Michael Brennan, a critic of the proposed school, which he fears would siphon money from the city's school system, as well as its top students.

Baxter Academy, which would be run by the nonprofit Baxter Academies of Maine, describes itself as a "rigorous, college-preparatory high school."

The school would be in a building that housed a former call center on York Street. Its curriculum would focus on science, technology, engineering and mathematics. It has applications from 40 students, and hopes to have 160 students by September.

Baxter Academies of Maine is one of four groups that are vying for state charters under a year-old law that allows charter schools in Maine for the first time.

Maine is the 41st state to allow charter schools, which are public schools that don't follow all of the regulations and restrictions on traditional public schools. If a public school student enrolls in a charter school, the state and local tax dollars from the student's home school district follow the student to the charter school.

Careful analysis of any business plan is critical because a school that runs out of money would be very disruptive for students, said William Shuttleworth, vice chairman of the Maine Charter School Commission, which is reviewing Baxter Academy's application.

When a charter school fails, he said, it's usually because the first contract with the authorizing agency wasn't well thought out, he said.

"My number one job, as commissioner, is to make sure we do the due diligence so every school we authorize is guaranteed for kids," he said.

A national survey of charter schools during 2010-2011 showed that 6.2 percent closed when their charters came up for renewal and 1.5 percent closed during their charter terms, according to the National Association of Charter School Authorizers.

Friday is Maine's deadline for groups to apply for charters. Six groups have applied.

The charter commission has already ruled that two of the applicants -- both proposals for virtual schools -- are not ready for the upcoming academic year and should re-apply next year.

On Friday, the commission will decide whether to give a charter to the Maine Academy of Natural Sciences, a magnet school that has operated since September on the campus of the Good Will-Hinckley Home for Boys in the central Maine town of Fairfield.

On Monday, the commission will decide whether to give a charter to Baxter Academy and the Cornville Regional Charter School, organized by parents who opposed the closure of Cornville's elementary school two years ago.

The commission has yet to schedule a meeting on a proposal for the Fiddlehead School of Arts and Sciences at an

Most...

READ SHARED COMMENTED

- Woman arrested after shaving boyfriend's head
- Assault suspect says actual date of birth makes him a juvenile
- Hill residents worried about Mumford & Sons show
- Missing N.H. toddler found sleeping in marsh

This portion of the requested page has been blocked.
[Click here for details.](#)

existing preschool in Gray. In its first year, the school would be for children from preschool through first grade.

The Baxter proposal faces an additional financial hurdle because it's the only proposal for which there is no existing school building.

In its initial application, Baxter proposed a \$2.4 million budget to teach 160 students. Critics said the proposal was built on assumptions that seemed unlikely to materialize.

For example, the school said it anticipated \$540,000 in grants — \$360,000 in the first year — from the U.S. Department of Education.

But the average grant is \$175,000 per year, and only 15 to 19 schools are awarded grants nationwide, said Rep. Chellie Pingree, D-Maine, who made inquiries about the grant at Brennan's request.

The discrepancy is the most obvious example of unrealistic assumptions in Baxter Academy's financial plan, said John Kosinski, a lobbyist for the Maine Education Association, which supports the Good Will-Hinkley proposal but opposes Baxter Academy.

He said the plan in Portland is just too risky.

"Right now this budget is a house of cards," he said. "If one thing doesn't come through -- if this grant doesn't come through -- the whole house collapses."

John Jaques, who heads the nonprofit group behind Baxter Academy, said he is confident that the school will get the grant because it has a strong application, with letters of support from the Mitchell Institute, Gov. Paul LePage, U.S. Sen. Susan Collins, R-Maine, and Pingree.

To address criticism from the charter commission that its budget was too optimistic in its revenue and enrollment projections, Baxter Academies of Maine presented an alternative \$1.1 million budget last week for an enrollment of 100 students. The new budget assumes a federal grant of \$180,000 in the first year, half the amount projected in its initial budget.

The revised budget shows that the group plans to borrow \$400,000 this summer for start-up costs, including \$75,000 for academic software and computers, \$30,000 for classroom furniture, and \$94,000 for annual lease payments.

The first-year budget includes \$261,000 in salaries and benefits for six teachers and an educational technician, and \$225,000 for administrators.

No money is budgeted for transportation or food service.

By the end of the 2012-13 school year, the school will have received revenue to pay back the \$400,000 and have \$40,000 left over, said Jaques.

He said the loan, through SunTrust Bank, would be guaranteed by the chief executive officer of an international engineering company.

Jaques said the donor has asked to remain anonymous because he does not want to attract more requests for financial help. He said the donor is a supporter of charter schools nationwide that have science and math curriculums.

Jaques said the school's critics want to protect the status quo, which is not working for many students, and are making up accusations to prevent the school from opening.

"This is school reform that needs to happen," he said, "and it is virtually impossible to accomplish in a public school setting."

Karen Malone of North Yarmouth wants to send her 16-year-old son to the school because it can offer him an individualized education plan with a wide range of technology courses. She said she and other parents are committed to helping the school succeed, by doing things such as fundraising and volunteering their time.

"We would do whatever we had to to make sure that the school would work," she said. "These kids need the opportunity, and we can't let them down."

Staff Writer Tom Bell can be contacted at 791-6369 or at:

tbell@pressherald.com

Were you interviewed for this story? If so, please fill out our accuracy form

[Send Question/Comment to the Publisher](#)

This portion of the requested page has been blocked.
[Click here for details.](#)

Tweet

Further Discussion

the discussion. To ensure conscientious dialogue we have implemented a strict no-bullying policy. To participate, you must follow our Terms of Use.

Home | News | Weather | Sports | Business | Opinion | Blogs | Obituaries | Classified Ads

Questions about the article? Add them below and we'll try to answer them or do a follow-up post as soon as we can. Technical problems? Email them to us with an exact description of the problem. Make sure to include:



Place an Online Ad | Place a Print Ad | Place a Classified Ad | Privacy Policy | Reader Services | Contact us

Type of computer or mobile device you are using
Exact operating system and browser you are viewing the site on (TIP: You can easily determine your operating system here.)

Copyright ©2012 MaineToday Media, Inc.

Like

Add New Comment

Required: Please login below to comment.

Type your comment here.

Image

Post as ...

Showing 8 comments

Sort by Popular now Subscribe by email Subscribe by RSS

Real-time updating is enabled. (Pause)



Teetime9 1 day ago

"The first-year budget includes \$261,000 in salaries and benefits for six teachers and an educational technician, and \$225,000 for administrators."

How many administrators does one need for a school with six teachers and an ed tech? Seven employees dividing \$261,000 is \$37,285.71 each for salary and benefits. Not the kind of package that will attract experienced and qualified personnel. Assuming Mr. Jacques can justify a ratio of 3.5 employees to one administrator, that leaves a package of \$112,500 for each of two administrators. One thing this charter school will have in common with public schools seems to be too many overpaid administrators.

2 people liked this.



IslandJW 1 day ago

Cherry-picking from public schools will not help public education. The natural evolution will be that charter schools, as private schools have traditionally done, will take the very few best students and give them a separate but inherently unequal high-quality education while public schools will continue to be burdened with educating physically, mentally, and emotionally challenged kids at extremely, distractingly high costs. The few will benefit, the many will lose. The solution is not to give up on public schools, but to get back to basics, demand high-quality results, and segregate by learning ability within the institution. Sounds harsh, but it's reality. I only hope that the teaching community, the public, the unions, and the politicians on both sides of the aisle wake up before they flush the education system down the drain, and further weaken our country's ability to compete in a global economy.

mainer58 and 1 more liked this



tomuchbs 1 day ago

The plan may have financial risks but it is backed up with 500K now. Charter schools are perfect for places like Portland because the schools in Portland are frankly not that good. That's why you see long waiting lists to get in the two Catholic High Schools in the area. Mainers need more viable educational opportunities and the dead wood in the school systems all over the state have outlived their usefulness in today's educational environment. Business as usual has hurt our kids and the charter school model will move us in the right direction.

2 people liked this.



Carl Loeffler 20 hours ago in reply to tomuchbs

You are woefully and pathetically misinformed.

Like Reply



dmoore69 1 day ago in reply to tomuchbs

While you are ranting against the Portland schools, kindly tell us where you were educated. Judging from the poor spelling, fractured syntax and lack of punctuation, you shouldn't be casting aspersions on anyone in education.

Like Reply



Nudwick 1 day ago

If we can get the left wing socialist programs and right wing religious programs the hell out of public schools maybe we can get some science, math and technology with college preparation started.

 Naran Row-Spaulding and 1 more liked this Like Reply



sickofcorporatocracy 1 day ago

A line of credit from a supporter shows strength, not weakness. Support from the Mitchell Institute also shows strength. Any charter school that emphasizes science and math, over creationism say, deserves a look.

 Naran Row-Spaulding and 1 more liked this Like Reply



go2goal 21 hours ago

Is the Charter school a none-profit institution? If so, LePage & Poliquin have made itg clear no bonds (loans) for them....they dont' create jobs or serve any societal purpose. LePage says NO to any none-profit debt financing!

Like Reply

Trackback URL <http://disqus.com/forums/>



June 5, 2012
07383

Barbara Barhydt
City of Portland Planning Division
City Hall 4th Floor
389 Congress St.
Portland, ME 04101

**Minor Site Plan Level II Application Submittal - Change of Use Permit
Proposed Baxter Academy for Technology and Science -Charter School
54 York Street, Portland, ME**

Dear Ms. Barhydt:

Enclosed please find a copy of the Level II Site Plan Application and associated site drawings that we have prepared for the Baxter Academy for Technology and Science on property owned by Rufus Deering Lumber Company at 54 York Street, Portland, Maine (located on the corner of York Street and Maple Street). The parcel proposed for development is the existing Rufus Deering Lumber Milling display store with other mixed uses for offices, labs, and dance studios. The property consists of a 0.22 acre lot fully developed with an existing 7,540 square foot (SF) footprint for a three story building zoned in the B-5b (Urban Commercial Business Zone). The proposed use will entail a floor area conversion of over 10,000 SF change of use and will require a Level II Site Plan review by the Planning Staff. It is also within the Old Port Historical District.

The overall improvement of the site will not add any additional impervious areas, as the site now is completely impervious. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. Their vision is to convert the entire building into a full 4 year high school over the next 3 to 4 years. Due to the Baxter Academy's plan to convert the existing structure into a school, all the retrofitting will occur with the interior spacing, and no building additions are anticipated to the exterior. Therefore, items such as stormwater and existing utilities services will not be altered as a result of the change of use. However, the need for Americans with Disabilities Act (ADA) access into the building will require a handicap ramp access to be extended into the City's right-of-way (ROW) at their York Street entrance, along with street space to be dedicated for handicap parking and access. This work will require rebuilding a small portion of the sidewalk and curbing along a section of York Street.

Rufus Deering Lumber Company will provide the lease for Baxter Academy and will be incorporating all necessary interior building improvements for the Charter School. We anticipate that the Baxter Academy will be a long-term lease as the Charter School envisions an enrollment of 320 students by 2014. However, in the first year the expectation is an enrollment of nearly 160 students. Our primary issue on the site has to deal with the transportation of students and parking arrangements for faculty and administrators.

Based on the City's parking requirements, school parking requirements are based on age. One regulation is based on parking for schools with students 15 years of age and under and one deal with students 16 years and older. This facility will be handling students in each grouping equally given the high school ages. We calculated that 42 spaces would be required for the leased space in year one, and that the

requirements will increase slightly with complete full building lease and build-out by 2014. Baxter Academy has a contract for parking spaces with JB Brown for their parking lot located opposite of the school off Maple Street and would expand that as the school expands in the future.

In addition, the Director, John Jacques, has indicated that the Charter School intends to contract for bus transportation for three full-size buses for student transportation in year one, which is the equivalent of upwards of 162 students. Other students could take other modes of public transportation, or be dropped off by parents. Student parking will not be encouraged or provide by the school. Maple Street has been designated as a location for bus delivery and pick-up of students. Busses are intended to be turning down Maple Street off of York Street such that students will be loaded/unloaded from the existing sidewalk adjacent to the proposed school building. Although the school has a requirement for several spaces, the school does not feel that they will have a need for the required spaces, but could retain JB Brown's lot on an event basis for special activities or conferences which may be held at the school above its base lease for faculty and administrators.

We are proposing that the City allow us to reconstruct a portion of the sidewalk along York Street, as well as dedicate two on-street parking spaces to be allowed for ADA compliant parking to meet our current demand. Both will be constructed to allow van access and direct ramps to the sidewalk and access into the York Street main entrance. In the final year of building renovation, when the bottom floor of the existing building (currently the Deering Lumber Milling Store) is occupied by the school, a second location off Maple Street could be dedicated for additional handicap spaces.

Rufus Deering Lumber Company is very anxious to start construction in early July. We are fully aware of the allocated review time, but would like to be proactive with your staff and be responsive to facilitate approvals in coordination with Portland Site Plan approval. We will be providing Architectural Plans and Site Improvements Plans to address ADA compliance issues for ramps and handicap parking. However, based on the ownerships desire to schedule for a September opening, we feel the Site Plan process must begin immediately.

In the interim, we are available at your request to hold any type of application review meeting or to meet the City's review staff to present the project on an informal basis. We feel given site design parameters that this would be beneficial for both the City Planners and the Baxter Academy design team, especially with parking and transportation needs.

If you have any questions please feel free to contact me at our office (207-200-2083). We look to working with the Planning Division Staff and City Engineers to facilitate the opening of this new opportunity for a charter school in the Downtown Portland area.

Sincerely,

SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager

JRS:jrs/kn
Enc.

cc: Dan Labrie, President, Rufus Deering Lumber Company
John Jacques, Executive Director, Baxter Academy for Technology & Science



Level II – Preliminary and Final Site Plans Development Review Application Portland, Maine

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Level II: Preliminary or Final Site Plan. Please note that Portland has delegated review from the State of Maine for reviews under the Site Location of Development Act, Chapter 500 Stormwater Permits, and Traffic Movement Permits. General information pertaining to the thresholds of review, public noticing procedures and the fee structure is contained in the Notice to Developer's Packet.

Level II: Site Plan Development includes:

- New structures with a total floor area of less than 10,000 sf except in Industrial Zones.
- New structures with a total floor area of less than 20,000 sf in Industrial Zones.
- Any new temporary or permanent parking area, paving of an unpaved surface parking area, or creation of other impervious surface area greater than 7,500 sf.
- Building addition(s) with a total floor area of less than 10,000 sf (cumulatively within a 3 year period) except in Industrial Zones.
- Building addition(s) with a total floor area of less than 20,000 sf in Industrial Zones.
- Park improvements: New structures or buildings with a total floor area of less than 10,000 sf, facilities encompassing an area of greater than 7,500 sf and less than 20,000 sf (excludes rehabilitation or replacement of existing facilities).
- New piers, docks, wharves, bridges, retaining walls, and other structures within the Shoreland Zone.
- Land disturbance between 1 and 3 acres (includes stripping, grading, grubbing, filling or excavation).
- A change in the use of a total floor area between 10,000 and 20,000 sf in any existing building (cumulatively within a 3 year period).
- Construction of a lodging house, bed and breakfast facility, emergency shelter or special needs independent living unit.
- Signage subject to approval pursuant to Section 14-526 (d) 8.a. (IV) of the Land Use Code.
- Any new major or minor auto service station with less than 10,000 sf of building area that is outside the B-2 or B-5 zones.
- The creation of day care or home babysitting facilities to serve more than 12 children in a residential zone (not permitted as a home occupation under section 14-410) in any principal structure that has not been used as a residence within the 5 years preceding the application.

The Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division
Fourth Floor, City Hall
389 Congress Street
(207) 874-8721 or 874-8719

Office Hours
Monday thru Friday
8:00 a.m. – 4:30 p.m.

PROJECT NAME: BAXTER ACADEMY for TECHNOLOGY & SCIENCE

PROPOSED DEVELOPMENT ADDRESS:

54 YORK ST.

PROJECT DESCRIPTION:

INTERIOR CHANGE OF USE FROM OFFICES TO
CHARTER (PUBLIC) SCHOOL > 10,000 SF FLOOR SPACE.

CHART/BLOCK/LOT: MAP 42 - BLOCK A
LOTS 7 & 8

PRELIMINARY PLAN 6/5/12 (date)
FINAL PLAN _____ (date)

CONTACT INFORMATION:

Applicant's Contact for electronic plans

Name: JAMES SEYMOUR P.E.
e-mail: JSEYMOUR@SEBAQOTECHNICS.COM
work #: 200-2083

Applicant – must be owner, Lessee or Buyer Name: <u>DAN LABRIE</u> Business Name, if applicable: <u>RUFUS DEERING LUMBER Co.</u> Address: <u>383 COMMERCIAL ST.</u> City/State: <u>PORTLAND, ME</u> Zip Code: <u>04101</u>	Applicant Contact Information Work # <u>772-6505</u> Home# <u>767-1055</u> Cell # <u>838-0444</u> Fax# <u>772-6981</u> e-mail: <u>Dlabrie@rufusdeering.com</u>
Owner – (if different from Applicant) Name: Address: <u>SAME</u> City/State: Zip Code:	Owner Contact Information Work # Home# Cell # Fax# e-mail:
Agent/ Representative Name: Address: <u>N/A</u> City/State: Zip Code:	Agent/Representative Contact information Work # Cell # e-mail:
Billing Information Name: <u>APPLICANT.</u> Address: City/State: Zip Code:	Billing Information Work # Cell # Fax# e-mail:

Engineer SEBAGO TECHNICS INC Name: JAMES SEYMOUR, P.E. Address: 75 JOHN ROBERTS ROAD, SUITE 1A City/State: So. PORTLAND, ME Zip Code: 04106	Engineer Contact Information Work # 200-2083 Cell # 632-1199 Fax# 856-2256 e-mail: jseymour@sebagotechnics.com
Surveyor SEBAGO TECHNICS INC Name: WILLIAM SHIPPEN PLS Address: 75 JOHN ROBERTS RD, SUITE 1A City/State: So. PORTLAND, ME Zip Code: 04106	Surveyor Contact Information Work # 200-2084 Cell # Fax# 856-2256 e-mail: wshippen@sebagotechnics.com
Architect RICK RENNER ARCHITECTURE Name: CHARLES YOUNG Address: 35 Pleasant St. City/State: PORTLAND, ME Zip Code: 04101	Architect Contact Information Work # 773-9699 x 203 Cell # Fax# e-mail: cyoung@rennerarchitects
Attorney PEARCE & DOW Name: MIKE PEARCE Address: 50 MONUMENT SQ. City/State: PORTLAND, ME Zip Code: 04101	Attorney Contact Information Work # 822-9900 Cell # Fax# 822-9901 e-mail:

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

Level II Development (check applicable reviews) <input checked="" type="checkbox"/> Less than 10,000 sq. ft. (\$400.00) <input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)	Fees Paid (office use) <input type="checkbox"/> <input type="checkbox"/>	Other Reviews (check applicable reviews) <input type="checkbox"/> Traffic Movement (\$1,000) <input type="checkbox"/> Stormwater Quality (\$250) <input type="checkbox"/> Section 14-403 Review (\$400 + \$25/lot) # of Lots <input type="checkbox"/> x \$25/lot = <input type="checkbox"/> <input type="checkbox"/> Other _____ <input checked="" type="checkbox"/> Change of Use <input type="checkbox"/> Flood Plain <input type="checkbox"/> Shoreland <input type="checkbox"/> Design Review <input type="checkbox"/> Housing Replacement <input type="checkbox"/> Historic Preservation	Fees Paid (office use) <input type="checkbox"/> <input type="checkbox"/>
The City invoices separately for the following: <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) Third party review is assessed separately.			
Plan Amendments (check applicable reviews) <input type="checkbox"/> Planning Staff Review (\$250) <input type="checkbox"/> Planning Board Review (\$500)	Fees Paid (office use) <input type="checkbox"/> <input type="checkbox"/>		

APPLICATION SUBMISSION

All site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Application submissions shall include one (1) paper packet with folded plans containing the following materials:

Numbers below changed- can't seem to highlight

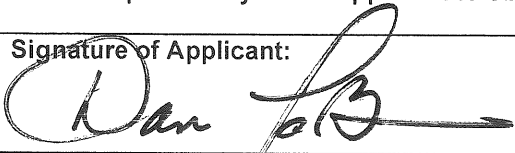

1. **One (1) full size site plans** that must be **folded**.
One (1) set of all written materials or as follows, unless otherwise noted:
 - a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
2. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet.
3. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
4. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
5. One (1) set of plans reduced to 11 x 17.

Refer to the application checklist for a detailed list of submittal requirements.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level II Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: 	Date: 
--	---

PROJECT DATA

The following information is required where applicable, in order complete the application

Total Site Area	9715 sq. ft.
Proposed Total Disturbed Area of the Site	N/A sq. ft.
(If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)	
IMPERVIOUS SURFACE AREA	
• Proposed Total Paved Area	N/A sq. ft.
• Existing Total Impervious Area	9715 sq. ft.
• Proposed Total Impervious Area	N/A sq. ft.
• Proposed Impervious Net Change	0 sq. ft.
BUILDING AREA	
• Proposed Building Footprint	-NA- sq. ft.
• Proposed Building Footprint Net change	0 sq. ft.
• Existing Total Building Floor Area	sq. ft.
• Proposed Total Building Floor Area	0 sq. ft.
• Proposed Building Floor Area Net Change	0 sq. ft.
• New Building	(yes or <u>no</u>)
ZONING	
• Existing	B5b
• Proposed, if applicable	SAME
LAND USE	
• Existing	MIXED USE -
• Proposed	OFFICES · RETAIL · LABS · ZUMBA CHARTER SCHOOL
RESIDENTIAL, IF APPLICABLE	
• Proposed Number of Affordable Housing Units	N/A
• Proposed Number of Residential Units to be Demolished	↓
• Existing Number of Residential Units	↓
• Proposed Number of Residential Units	↓
• Subdivision, Proposed Number of Lots	↓
PARKING SPACES	
• Existing Number of Parking Spaces	12
• Proposed Number of Parking Spaces	10 (LEASED)
• Number of Handicapped Parking Spaces	2 (YORK ST.)
• Proposed Total Parking Spaces	1
BICYCLE PARKING SPACES	
• Existing Number of Bicycle Parking Spaces	0
• Existing Number of Bicycle Parking Spaces	0
• Proposed Number of Bicycle Parking Spaces	5
• Total Bicycle Parking Spaces	5
ESTIMATED COST OF PROJECT	<u>\$185,000.00</u>

General Submittal Requirements – Preliminary Plan (Optional)

Level II Site Plan

Preliminary Plan Phase Check list (if elected by applicant)

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Completed application form
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Application fees
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written description of project
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Evidence of right, title and interest.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Copies of required State and/or Federal permits.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of proposed project's compliance with applicable zoning requirements.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written description of existing and proposed easements or other burdens.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written requests for waivers from individual site plan and/or technical standards, where applicable.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Traffic analysis (may be preliminary, in nature, during the preliminary plan phase).
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written summary of significant natural features located on the site.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written summary of project's consistency with related city master plans.
Applicant Checklist	Planner Checklist	Number of Copies	Site Plan Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>		Boundary Survey meeting the requirements of Section 13 of the City of Portland Technical Manual.
<input type="checkbox"/>	<input type="checkbox"/>	1	Preliminary Site Plan Including the following: (*information provided may be preliminary in nature during preliminary plan phase):
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed structures with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Location of adjacent streets and intersections and approximate location of structures on abutting properties.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Proposed site access and circulation.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Proposed grading and contours.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Preliminary landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Existing and proposed utilities (preliminary layout).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Preliminary infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, transit infrastructure, roadway improvements).
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Preliminary stormwater management and erosion control plan.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b) 1. of the Land Use Code).
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Proposed alterations to and protection measures for significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important

NA

natural features listed in Section 14-526 (b)1. of the Land Use Code).

- Existing and proposed easements or public or private rights of way.

General Submittal Requirements – Final Plan (Required)

Level II Site Plan

Final Plan Phase Check list (including items listed above in General Requirements for Preliminary Plan, if applicant did not elect to submit for a preliminary plan review)

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirement
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of financial and technical capacity.
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of utilities' capacity to serve the development.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of fire safety (referencing NFPA fire code and Section 3 of the City of Portland Technical Manual).
<input type="checkbox"/>	<input type="checkbox"/>	1	Construction management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Traffic Plan (if development will (1) generate 100 or more PCE or (2) generate 25 or more PCE and is located on an arterial, within 1/2 mile of a high crash location, and/or within ¼ mile of an intersection identified in a previous traffic study as a failing intersection).
<input checked="" type="checkbox"/> NA	<input type="checkbox"/>	1	Stormwater management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of solid waste generation and proposed management of solid waste.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of conformity with applicable design standards.
<input type="checkbox"/>	<input type="checkbox"/>	1	Manufacturer's verification that HVAC and manufacturing equipment meets applicable state and federal emissions requirements.

Final Plan Phase

<input type="checkbox"/>	<input type="checkbox"/>	1	Final Site Plan Including the following
<input type="checkbox"/>	<input type="checkbox"/>		Existing and proposed structures on the site with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
<input type="checkbox"/>	<input type="checkbox"/>		Location of adjacent streets and intersections and approximate location of structures on abutting properties.
<input type="checkbox"/>	<input type="checkbox"/>		Proposed site access and circulation.
<input type="checkbox"/>	<input type="checkbox"/>		Proposed grading and contours.
<input type="checkbox"/>	<input type="checkbox"/>		Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways. Proposed curb lines must be shown.
<input type="checkbox"/>	<input type="checkbox"/>		Proposed loading and servicing areas, including applicable turning templates for delivery vehicles
<input type="checkbox"/>	<input type="checkbox"/>		Proposed snow storage areas or snow removal plan.
<input type="checkbox"/>	<input type="checkbox"/>		Proposed trash and recycling facilities.
<input type="checkbox"/>	<input type="checkbox"/>		Landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
<input type="checkbox"/>	<input type="checkbox"/>		Existing and proposed utilities.
<input type="checkbox"/>	<input type="checkbox"/>		Location and details of proposed infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, public transit infrastructure, roadway improvements).
<input type="checkbox"/>	<input type="checkbox"/>		Proposed septic system, if not connecting to municipal sewer. (Portland Waste Water Application included in this application)
<input type="checkbox"/>	<input type="checkbox"/>		Proposed finish floor elevation (FFE).

-
-
-
-
-
-
-
-
-
-
-
-
-
-
-

-
-
-
-
-
-
-
-
-
-
-
-
-
-
-

<ul style="list-style-type: none"> ▪ Exterior building elevation(s) (showing all 4 sides).
<ul style="list-style-type: none"> ▪ Proposed stormwater management and erosion controls.
<ul style="list-style-type: none"> ▪ Exterior lighting plan, including street lighting improvements..
<ul style="list-style-type: none"> ▪ Proposed signage.
<ul style="list-style-type: none"> ▪ Identification of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code). Wetlands must be delineated.
<ul style="list-style-type: none"> ▪ Proposed alterations to and protection measures for of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code).
<ul style="list-style-type: none"> ▪ Total area and limits of proposed land disturbance.
<ul style="list-style-type: none"> ▪ Soil type and location of test pits and borings.
<ul style="list-style-type: none"> ▪ Details of proposed pier rehabilitation (Shoreland areas only).
<ul style="list-style-type: none"> ▪ Existing and proposed easements or public or private rights of way.

Review Staff
City of Portland
Portland, Me 04101

Dear Staff Members:

Baxter Academy for Technology and Science
54 York Street

HISTORY

The building is located on the corner of York and Maple (see attached map) in Portland. Fifty-four York Street has had numerous uses over the years. Most recently approximately 20,000 sq. ft. was leased to Media Power. In April of 2010 Media Power needed more space and moved to a building on Foden Road in South Portland.

In mid-2011 John Jaques, future Executive Director of Baxter Academy, and Daniel LaBrie, Senior Vice President of Rufus Deering Lumber Company, began discussing a possible partnership by leasing 54 York Street to them for a charter school.

OBJECTIVE

The school aims to link students with local businesses, scientists, researchers and other professionals working in fields of science, technology, engineering and mathematics. The Portland location offers easy access to the Gulf of Maine Research Institute, Maine Medical Center, the University of Southern Maine and Southern Maine Community College.

The school hopes to get a charter from the state by July 1st and open in September. It hopes to be the first of its kind in Portland.

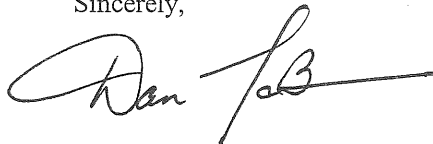
PROPOSAL

The fit up to prepare the building for a school consists of upgrading HVAC, Security system, Handicap Accessibility, new carpets and paint. The building as it stands today does not have an elevator. An exterior elevator will be installed on the southwest side of the building on a 15' easement.

CLOSING

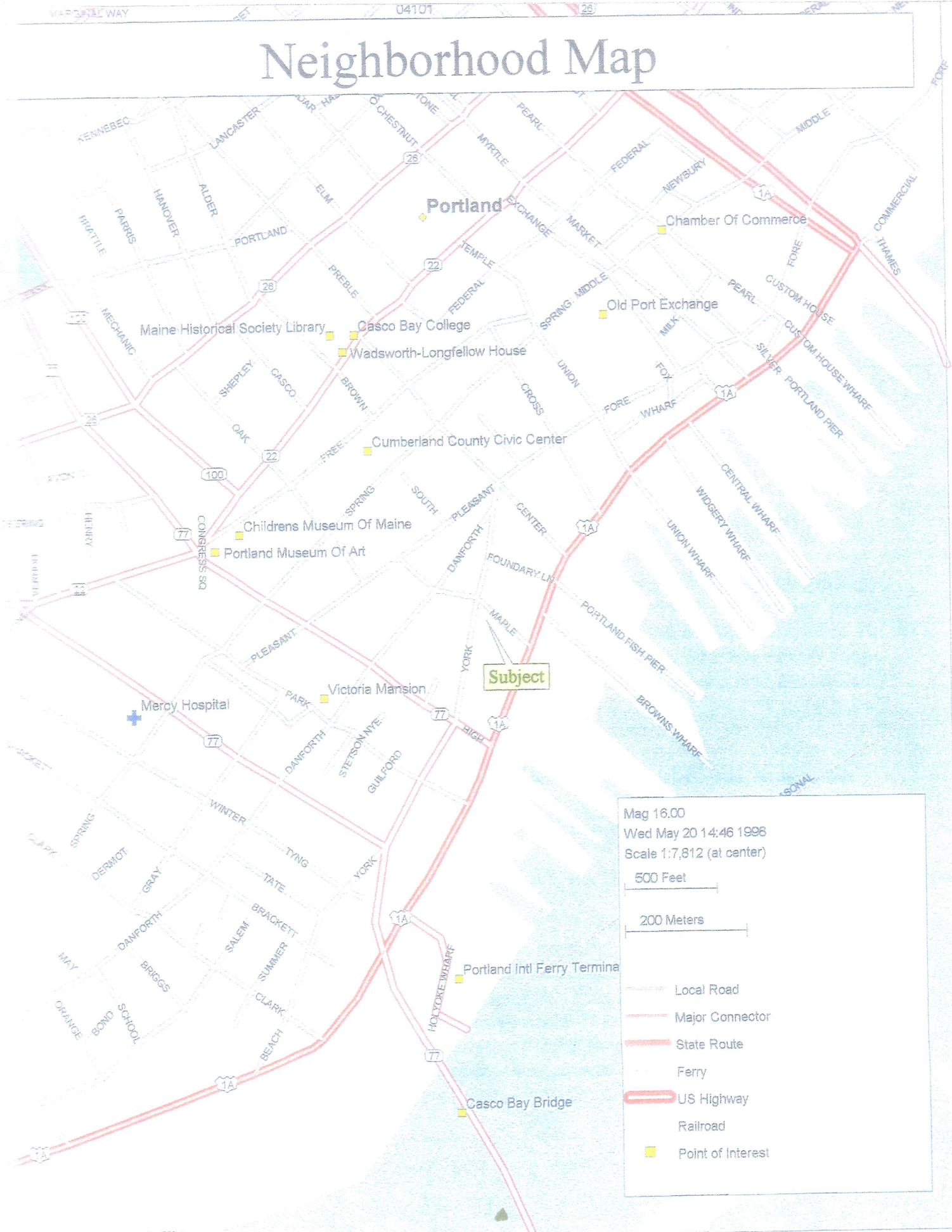
I believe this is a perfect location for a charter school. It is wonderful to be able to offer our youth an alternative education to the usual public school system. These charter schools have been very successful all over the country. Time is of the essence, I would ask that you move this process along and grant the permit in a timely manner.

Sincerely,



Daniel LaBrie
Senior Vice President

Neighborhood Map

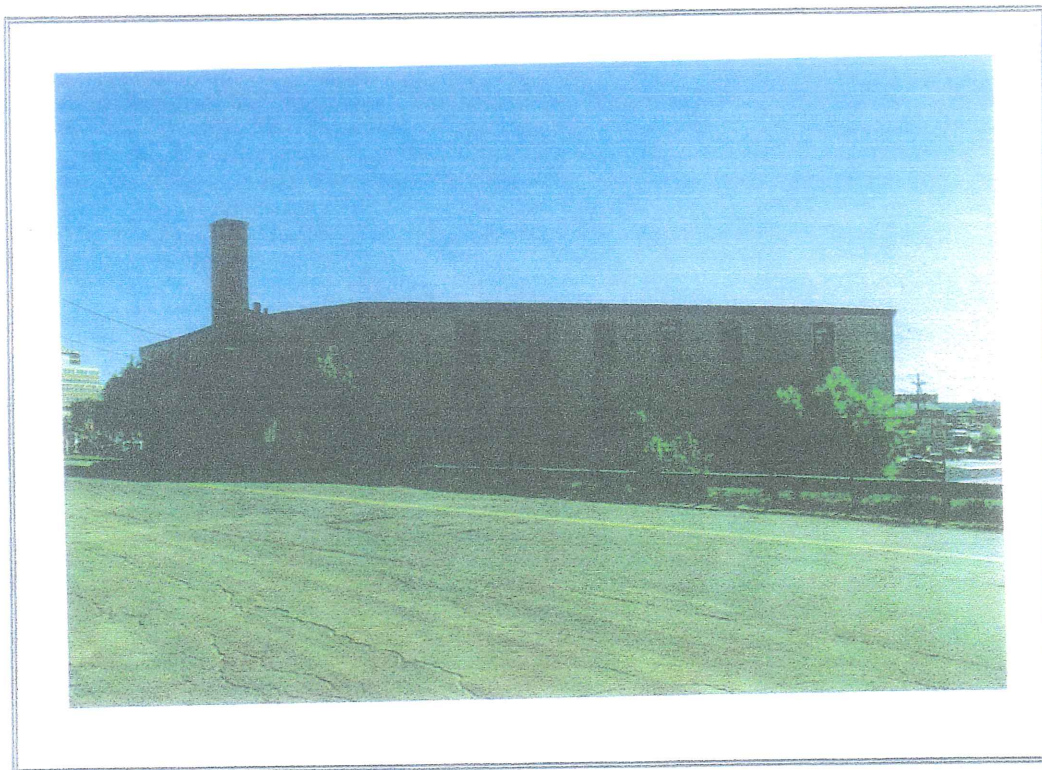


Mag 16.00
Wed May 20 14:46 1998
Scale 1:7,812 (at center)

500 Feet
200 Meters

- Local Road
- Major Connector
- State Route
- Ferry
- US Highway
- Railroad
- Point of Interest

SUBJECT PHOTOGRAPHS



1) Easterly view of subject from York Street



2) Westerly view of subject from Commercial Street, looking up Maple Street

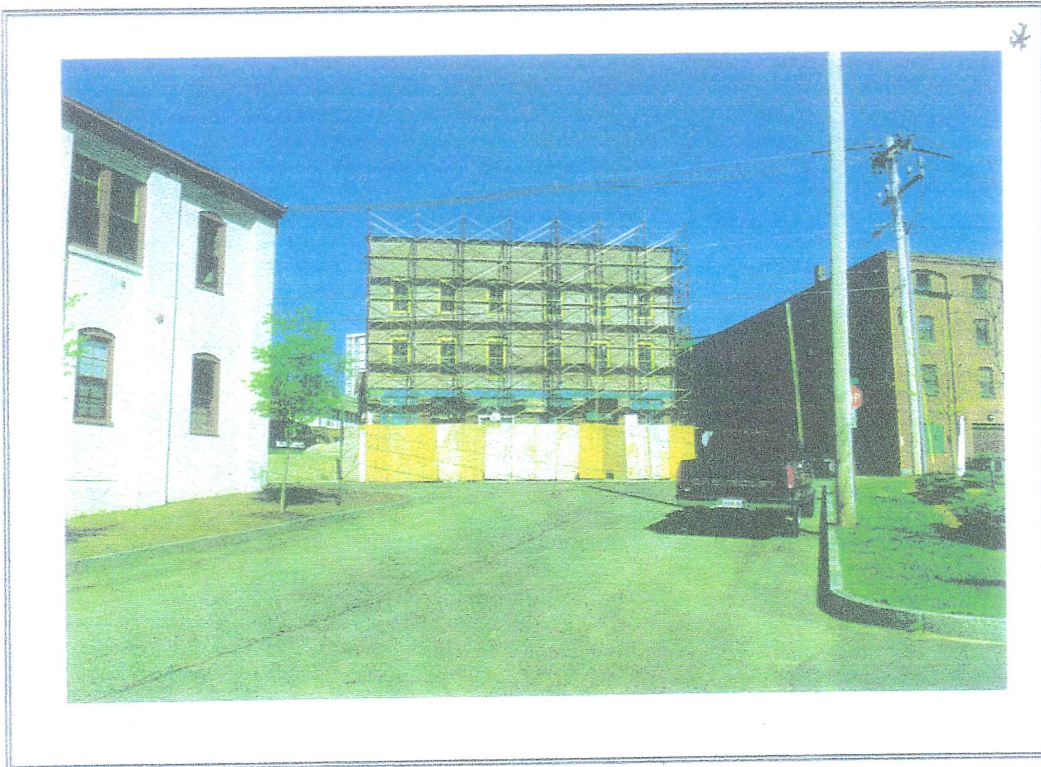
SUBJECT PHOTOGRAPHS



3) Southeasterly view down Maple St. toward Commercial St, subject at right



4) Westerly view of south elevation of subject (driveway behind bldg)



N2) Ongoing rehabilitation/revitalization of the subject neighborhood (subject at left)

SUBJECT SITE DESCRIPTION

Location

The subject site enjoys a corner lot location at the apex of York and Maple Streets in Portland Maine. This location receives heavy traffic flow along York Street as this is the major connector route from the Portland CBD to the Casco Bay Bridge providing access to the City of South Portland. The site is also within close proximity to the Portland Waterfront District with marine, transit, and tourist related land uses prevalent.

Land Area

The subject site, according to the legal description provided, contains an estimated land area of 8,712±sf in fee simple, plus an additional passageway area measuring 15' x 85' (1,275±sf) for a total of 9,987± sf. The survey plan also provided to the appraiser, showed an area of 8,447±sf, although it is not clear that this is not the footprint of the building rather than the site. For purposes of this appraisal, this is not a large issue as the site is considered fully built out and the leased fee interest has been appraised. The client is urged to consult with a surveyor and/or title attorney to clarify such matters further prior to making decisions which may be based upon land area estimates.

Shape/Frontage

The shape of the site is characterized as irregular. The parcel is a corner lot with an almost trapezoidal shape due to its frontage on two streets. According to the legal description provided, the site has 125'± of frontage along Maple Street plus another 100'± of frontage along York Street.

Access/Visibility

The subject site is considered to have good access and visibility characteristics. The site enjoys high traffic counts approximating 12,000 cars per day according to MDOT traffic volumes 1996. York Street feeds directly into/from the new Casco Bay Bridge (replaced the Million Dollar Bridge last year) and is a direct link to the Portland CBD and Old Port Districts. Commercial Street to the south is also a major commuter travel artery and serves the working waterfront section of Portland. Maple Street is a small connector road which traverses the two.

Topography/Soils

The topography of the site is characterized as sloping in an easterly direction down from its York Street frontage towards Commercial Street and the Portland Waterfront. No on site soils investigation was performed by the appraiser. The Cumberland County Soils Survey classifies this area as Urban, Cut and Fill land. It was reported that a portion of the subject (original construction) dates back to 1820 and survived the Great Fire of 1936 which leveled much of downtown Portland and gave way to the filling in along the waterfront and creation of Commercial Street in 1950. Given the age and varied uses of the site over time, the client is urged to conduct due diligence in regards to environmental issues to their own satisfaction. The appraised value herein assumes no adverse conditions related to environmental issues which may or may not affect the subject site. Reference is made to the Statement of Assumptions and Limiting Conditions in this regard.

Utilities

The subject site is serviced with all public utilities including water, sewer, natural gas, electricity, telephone and cable TV. Municipal services such as fire and police protection and sanitation are deemed to be adequate.

Easements

Reference is made to the attached legal description and title report contained in the Addenda section of this report for an overview of the various limitations of title to the subject property. As discussed, part of the site is encumbered as a passageway in common with others. There were no other adverse easements or encroachments noted on the date of inspection/value herein which would limit the use and/or value of the subject property under its current land use program.

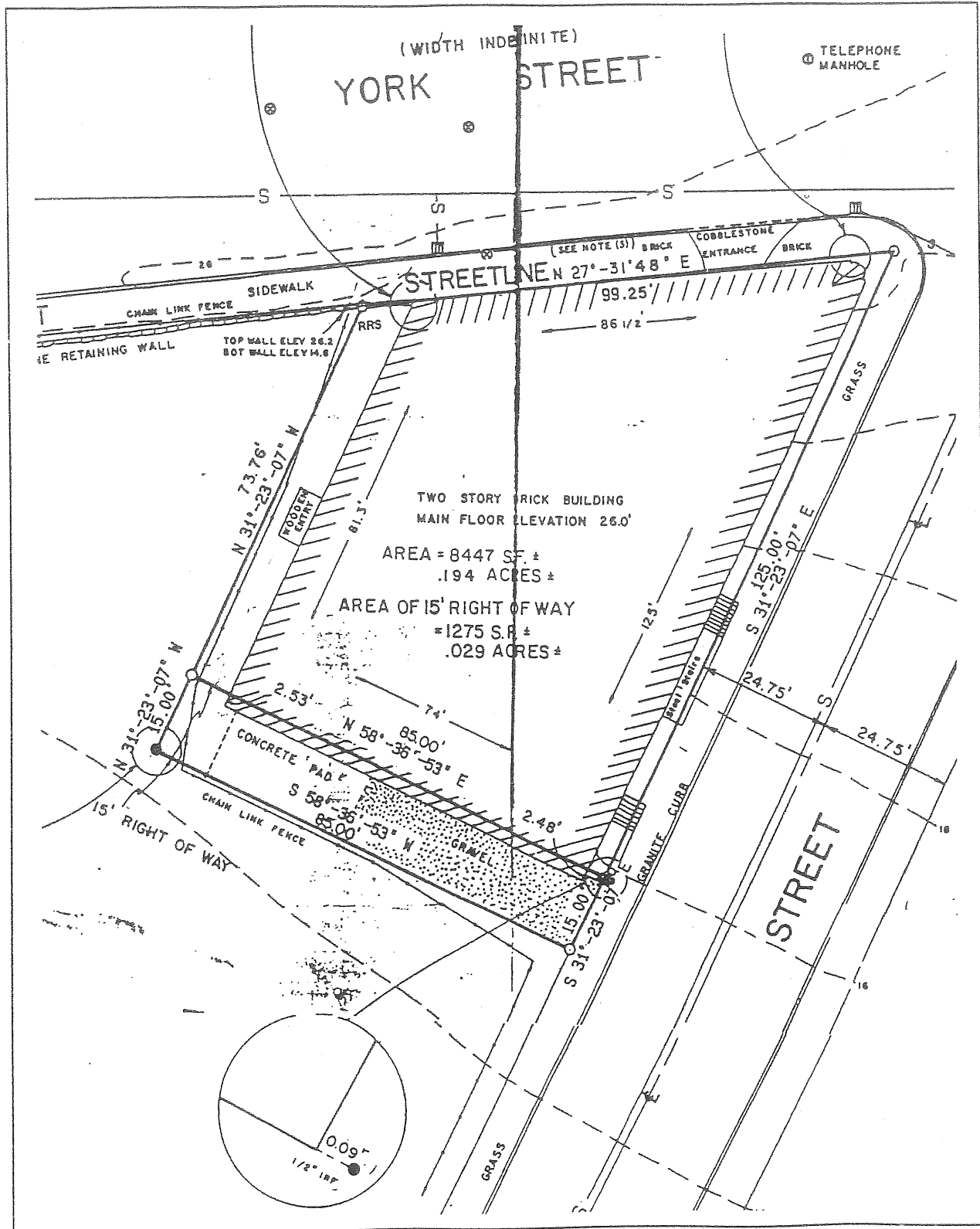
Functional Utility

The subject site is considered to offer average functional utility for development purposes. If vacant and available, it would likely be assembled with abutting lands for a larger scale development. The site does offer good exposure for commercial purposes and adequate frontage for access. It is zoned for business use and is located in an area of revitalization. An interim use for surface parking would be an immediate use of the site.

Marketability/Appeal

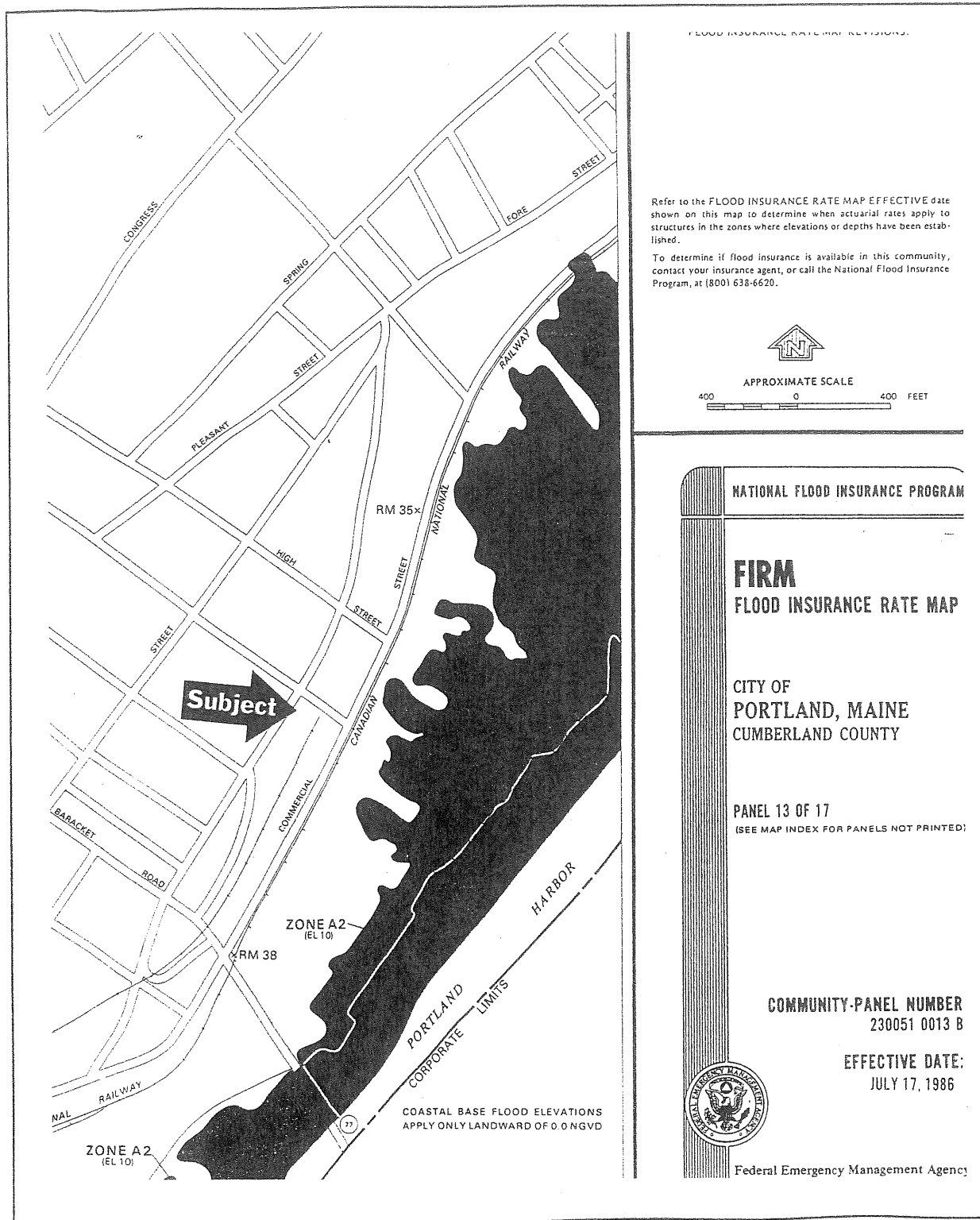
For the reasons cited above, the subject land parcel is considered very marketable and would be anticipated to sell quickly if vacant and available.

SUBJECT SITE PLAN
(not to scale)



FEDERAL EMERGENCY MANAGEMENT FLOOD HAZARD ZONE

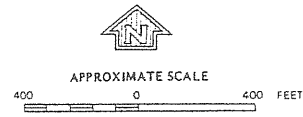
The subject property is not located in a federally designated 100-year flood hazard zone, according to the FEMA Map presented below:



FLOOD INSURANCE RATE MAP REVISIONS:

Refer to the FLOOD INSURANCE RATE MAP EFFECTIVE date shown on this map to determine when actuarial rates apply to structures in the zones where elevations or depths have been established.

To determine if flood insurance is available in this community, contact your insurance agent, or call the National Flood Insurance Program, at (800) 638-6620.



NATIONAL FLOOD INSURANCE PROGRAM

FIRM FLOOD INSURANCE RATE MAP

CITY OF PORTLAND, MAINE
CUMBERLAND COUNTY

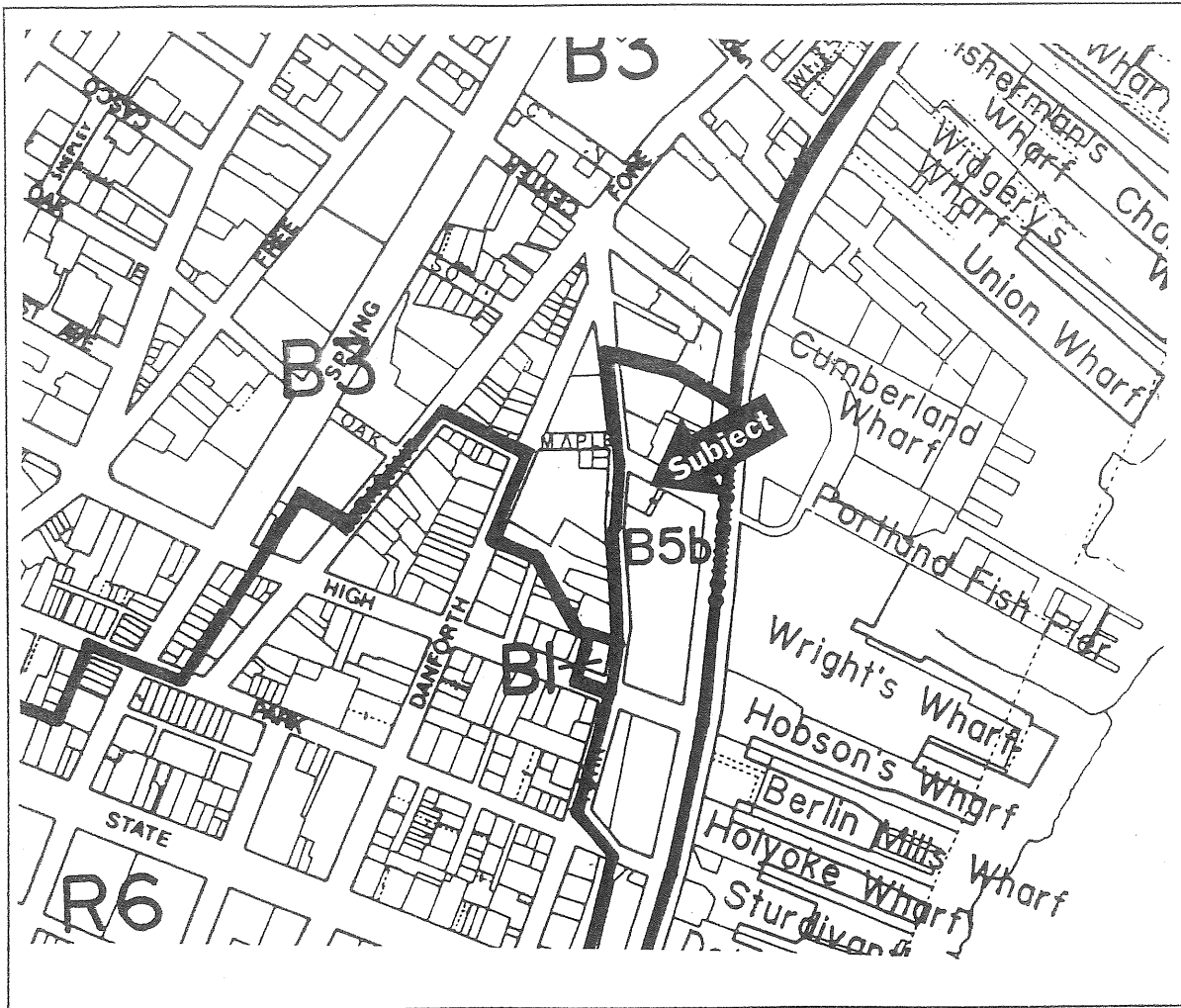
PANEL 13 OF 17
(SEE MAP INDEX FOR PANELS NOT PRINTED)

COMMUNITY-PANEL NUMBER
230051 0013 B

EFFECTIVE DATE:
JULY 17, 1986



Federal Emergency Management Agency



Pickup

SUBJECT IMPROVEMENTS DESCRIPTION

Year Built

The subject building improvements were built in the late 1800's, and subsequent to a fire, was rebuilt and expanded around the 1920's.

Gross Building Area

The building is estimated to contain approximately 21,297±sf of gross building area, all of which has been considered net rentable area as per existing leases in place (common areas prorated).

Foundation

The foundation walls are of mortared brick and appear to be in sound condition.

Frame

The frame of the building is heavy timber columns and wooden floors, some supported with brick columns and steel girders.

Exterior Walls

The exterior walls are of painted brick.

Floors

The top two floors in the building were generally of wood underlayment with a concrete slab floor at the daylight basement level.

Floor Covering

Floor coverings ranged from low cost to medium grade commercial carpeting in the office areas, some composition tile and/or vinyl floor finishes in the bathroom areas. The lower level was mostly painted concrete with some finished areas carpeted.

Partitions

Interior partitions were mostly of wood stud walls and sheetrocked panels. Some paneling and matchboard wainscoting were also noted.

Layout

The layout is fairly heavily partitioned into numerous offices and suites for multi-tenant occupancy.

Ceilings

The ceilings were primarily suspended acoustic panel tiles and in good condition, with recessed fluorescent lighting throughout.

Roof Structure & Cover

The roof structure is of a flat, shed style with a covering of rubber membrane dating to 1989 according to the present owner.

Electrical

The electrical entrance to the building consists of two primary services of four-wire, three phase, 240 volt capacity with a 400-amp rating. Distribution is via six 100 amp and 1 200-amp panels with circuit breaker protection. Electricity is separately metered to each tenant excepting unit 102 which is on the house meter.

Plumbing

The building is serviced with municipal water and sewer. The main water supply enters in the basement sprinkler room and has a backflow preventer. Where visible, plumbing distribution piping is primarily copper with some PVC and cast iron waste lines noted. There is also a recent, gas fired domestic hot water heater in the basement.

H.V.A.C.

The building is serviced with a natural gas heater with individual tenant meters, excepting unit 102 which is on the house. Heat is provided through the building with a combination of ceiling and wall hung gas heaters. There is a central, gas-fired boiler with five zones noted on the first floor as well as some electric baseboard heat interspersed.

Elevators

There are no elevators currently in the building.

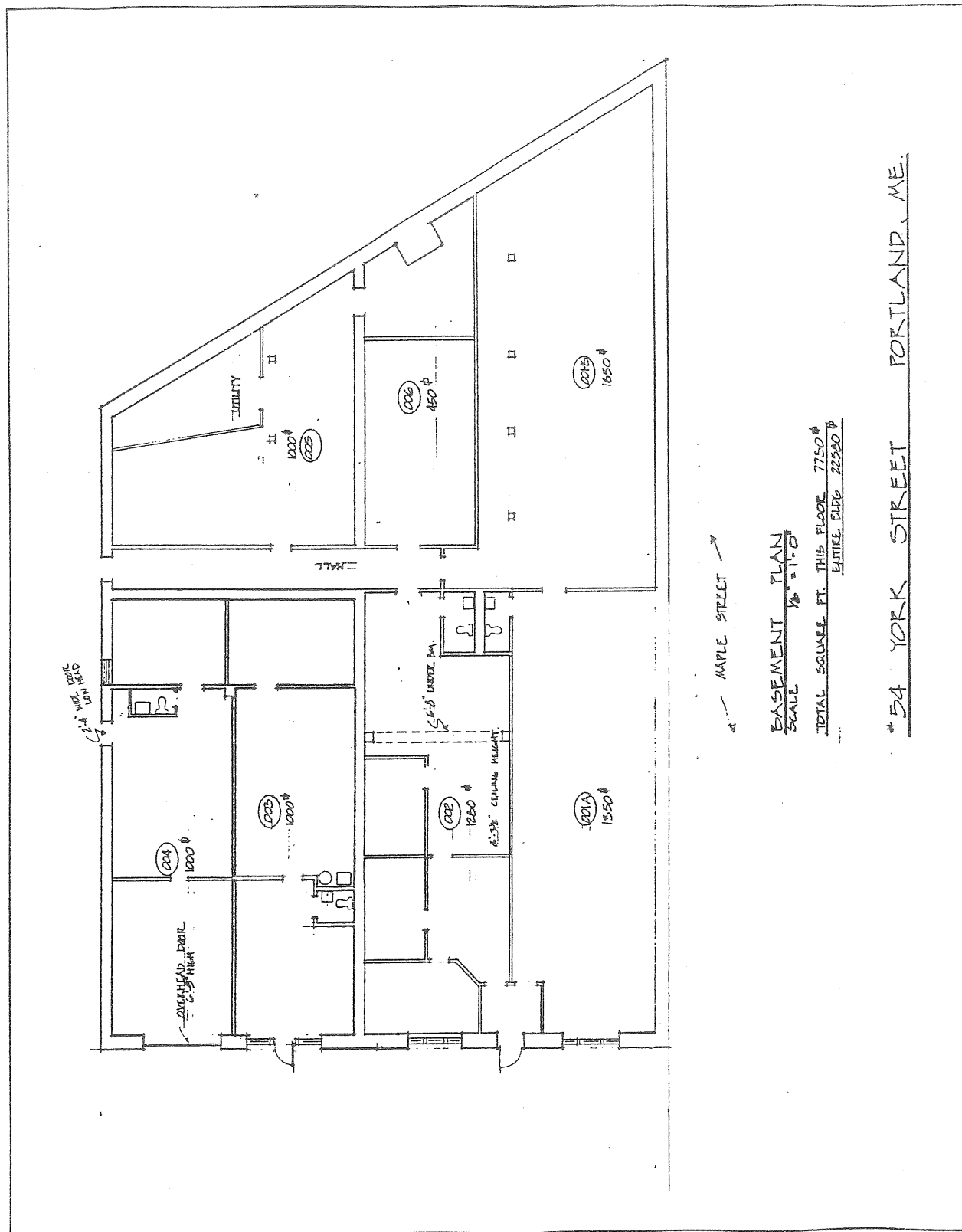
Sprinkler System

The building is provided with a dry type sprinkler system.

Overall Condition

The overall condition of the improvements is rated as average to good.

SUBJECT IMPROVEMENTS SKETCH - Basement Level
(not to scale)

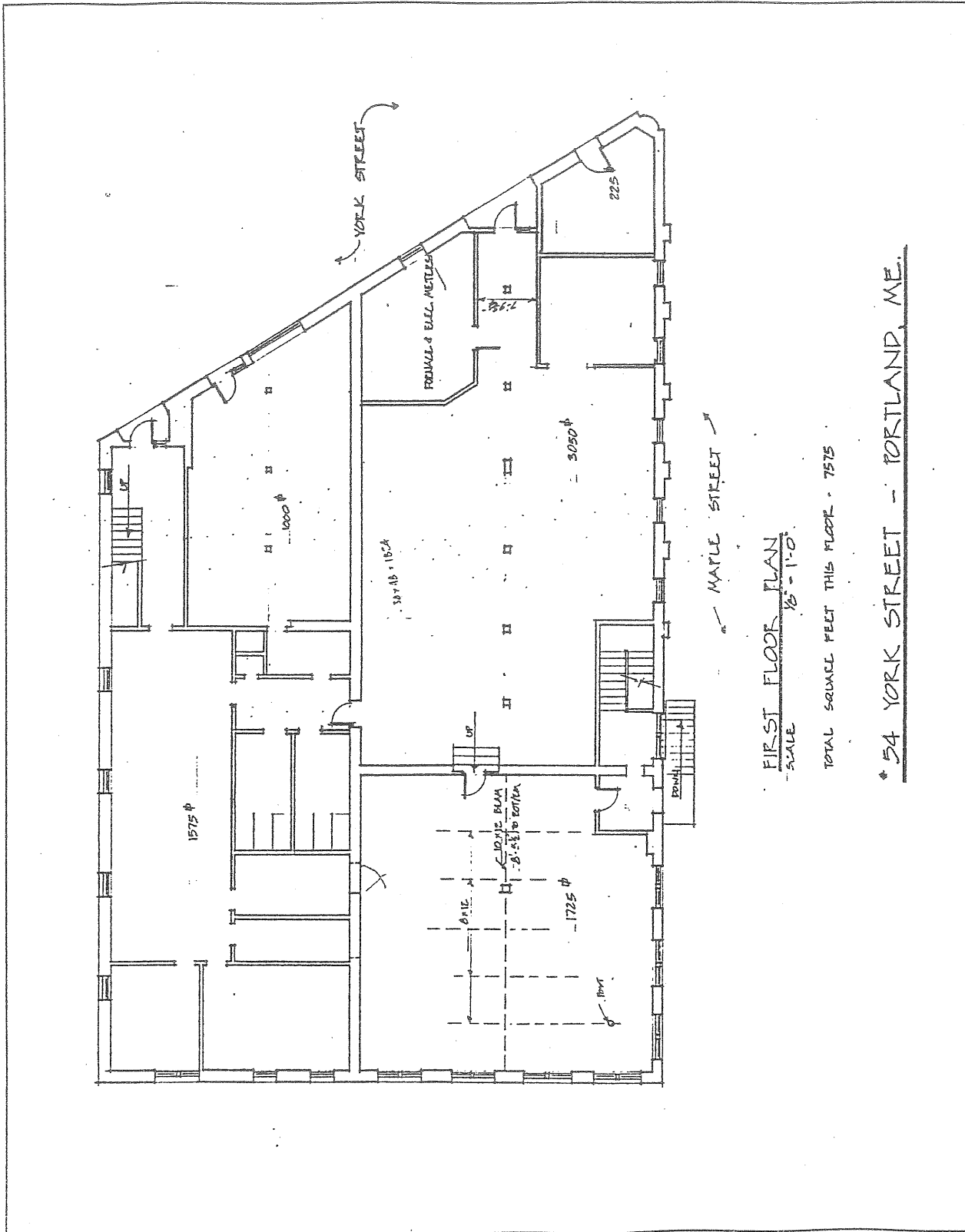


BASEMENT PLAN
SCALE 1/8" = 1'-0"

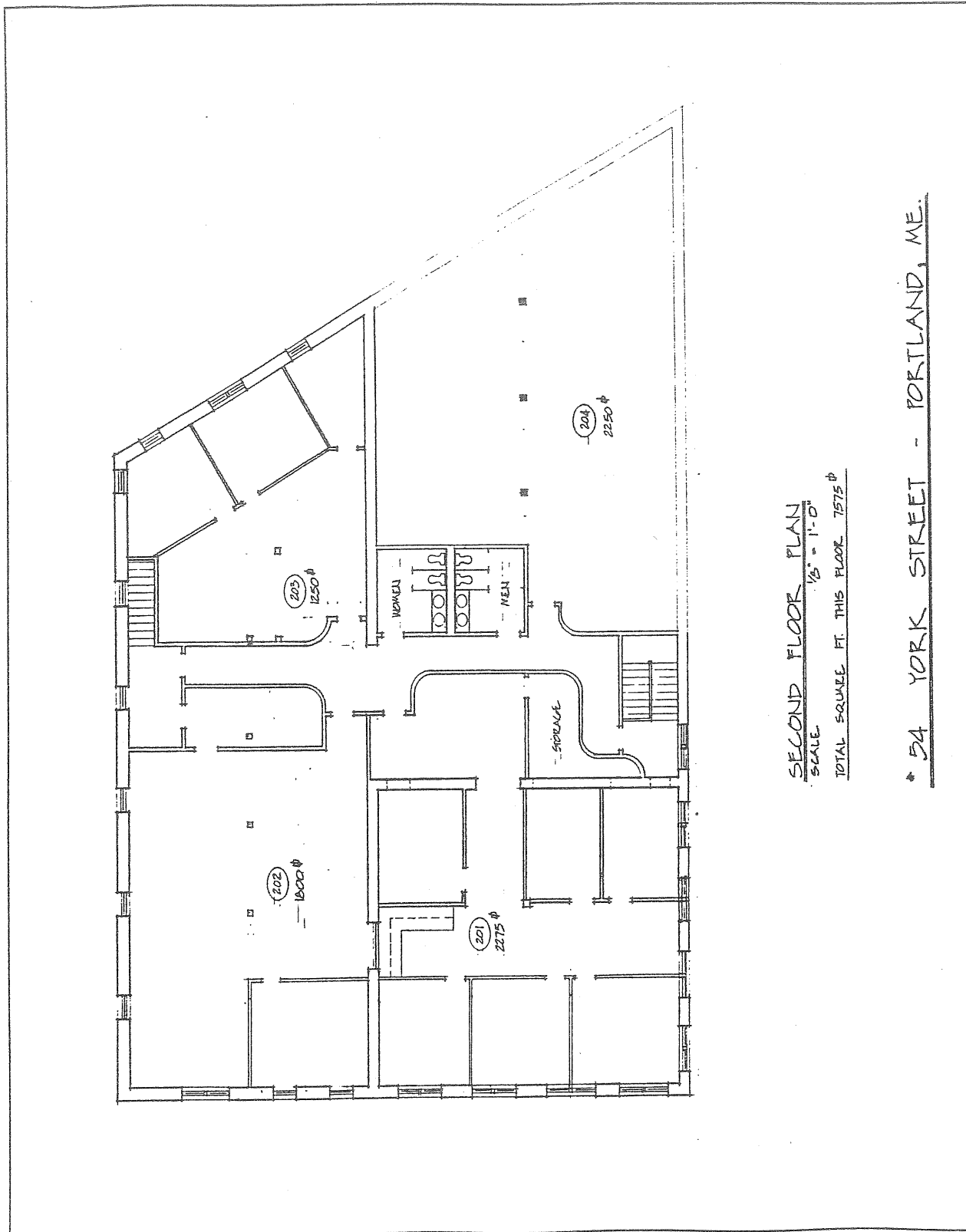
TOTAL SQUARE FT. THIS FLOOR 7750 sq ft
ENTIRE FLOOR 22500 sq ft

54 YORK STREET PORTLAND, ME.

SUBJECT IMPROVEMENTS SKETCH - First Floor Level
(not to scale)



SUBJECT IMPROVEMENTS SKETCH - Top Floor Level
(not to scale)



SECOND FLOOR PLAN
SCALE 1/8" = 1'-0"
TOTAL SQUARE FT. THIS FLOOR 7575 sq ft

54 YORK STREET - PORTLAND, ME.

SURVEYOR'S REPORT

Fill out and sign with all Surveys in accordance with the Instructions on the reverse side.

To: **LAWYERS TITLE INSURANCE CORPORATION**

Richmond, Virginia.

THIS IS TO CERTIFY, that on December 3, 19 87, I made an accurate survey of the premises standing in the name of William J. Dowd situated at Portland Cumberland Maine
City County State

briefly described as: Commercial Structure

and shown on the accompanying survey entitled: Standard Boundary Survey for William J. Dowd

I made a careful inspection of said premises and of the buildings located thereon at the time of making such survey, and again on December 17, 19 87, and at the time of such latter inspection I found William J. Dowd to be in possession of said premises as owner

(tenant) or (owner)

I further certify as to the existence or non-existence of the following at the time of my last inspection:

1. Rights of way, old highways, or abandoned roads, lanes or driveways, drains, sewer, water, gas or oil pipe lines across said premises (Include any such matters shown on the recorded plat of subdivision):

15 foot Right of Way to land now or formerly of Rufus Deering
Location of sewer connection uncertain

2. Springs, streams, rivers, ponds, or lakes located, bordering on or running through said premises: None observed

3. Cemeteries or family burying grounds located on said premises. (Show location on plat): None observed

4. Telephone, telegraph or electric power poles, wires or lines located on, under, overhanging or crossing said premises and serving other property or properties: None observed above ground below grade lines uncertain

5. Joint driveways or walkways; party walls or rights of support; porches, steps or roofs used in common or joint garages: 15 foot strip above mentioned apparently owned by Dowd subject to use by abutter.

6. Disputed boundaries, encroachments or overhanging projections. (If the buildings, projections or cornices thereof, or signs affixed thereto, fences or other indications of occupancy encroach upon or overhang adjoining properties or easement areas, or the like encroach upon or overhang surveyed premises, specify all such):
Encroachment of building onto 15 foot Right of Way, Sign overhangs York Street, Fire Escapes overhang Maple Street, Fence of Abutter encroaches on said property.

7. Physical evidence of boundary lines on all sides. (Be specific): Iron pins found at two corners (see plan)

8. Is the property improved? Yes

(a) Building is: Brick (X); Clapboard (); other ()

(b) Building is: One story (); Two story (X); split-level (); other ()
Specify

9. Indications of building construction, alterations or repairs within recent months: Exterior brick cleaned, repainted, new doors

(a) If new improvements under construction, how far have they progressed? 50 per cent completed more or less

City in progress of determin-

10. Changes in street lines either completed or officially proposed: ing York Streetline

(a) Are there indications of recent street or sidewalk construction or repairs? None

11. Are all abutting streets or roads maintained by public authorities? Yes

(a) Is access to such streets or roads limited? NO

12. If the surveyed premises are subject to restrictive covenants, do the improvements, use and occupancy comply with such? (If the premises are subject to restrictive covenants, have the examining attorney furnish you verbatim copy of them) Note: Origination Deed 1555/24 requires Grantee to build and maintain fence, Existing fence appears to have been constructed by abutter.

William A. Campbell, Jr.
Civil Engineer or Surveyor

NOTE: In all cases where there are encroachments, easements, party walls, etc., they should also be denoted upon the map of your survey. Also, be certain map complies with Instructions on reverse side. Particular attention is directed to Paragraph 3 of these instructions.

(Be sure to answer each item. If the property is not subject to any such of items 1 to 6 inclusive, insert the word "none" following the item.)

BEAGLE, PEARCE & RIDGE, LLC
ATTORNEYS AT LAW

C. ALAN BEAGLE
MICHAEL J. PEARCE
MARTIN J. RIDGE
JACQUELYN M. KURZ
PAULA M. CRAIGHEAD

24 CITY CENTER
P.O. BOX 7044
PORTLAND, MAINE 04112
(207) 773-1751
FAX (207) 775-3382

November 30, 1998

Mr. William M. Moody, Jr., President
Rufus Deering Company
383 Commercial Street
P.O. Box 880
Portland, ME 04112-0880

*Safety Deposits
Box*

Re: Rufus Deering Company/Peoples Heritage Bank/York
Street, Inc.

Dear Bill:

Enclosed please find the following original documents:

1. Warranty Deed from York Street, Inc. to Rufus Deering Company. This Warranty Deed has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 13.
2. Assignment of Leases and Security Deposits from York Street, Inc. to Rufus Deering Company. This Assignment has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 36.

If you have any questions, please feel free to contact me.

Very truly yours,

Robyn
Robyn A. Pearce

Assistant to Michael J. Pearce

enclosure

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.
By: Werner Schnetzer
Werner Schnetzer
Its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,
Paul S. Burger
Notary Public/Attorney-at-Law
Print Name: Paul S. Burger

122895/PSB/SKLF
91401.4926

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4: 48

CUMBERLAND COUNTY

John B. Bruin

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4:52

CUMBERLAND COUNTY

John B. O'Brien

61274

ASSIGNMENT OF LEASES AND SECURITY DEPOSITS

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04106, for consideration, does hereby ASSIGN to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, its successors and assigns, all its right, title and interest in and to any and all leases, subleases and/or tenancies of the premises located on or about 54-62 York Street in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto, including, without limitation, any and all tenant security deposits relating thereto.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by Werner Schnetzer, its duly authorized President, on this 1st day of September, 1998.

Paul S. Bulger
Witness

YORK STREET, INC.
By: Werner Schnetzer
Werner Schnetzer
Its: President

STATE OF MAINE
CUMBERLAND, SS.

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in said capacity and the free act and deed of said YORK STREET, INC.

Before me,
Paul S. Bulger
Notary Public/Attorney-at-Law
Print Name: Paul S. Bulger

122937/PSB/SKLF
91401.4926

Parking and Transportation Plan

Baxter Academy for Technology and Science (BA) anticipates an enrollment of 160 14 and 15-year-old students for the 2012-13 school year. We will serve only grades 9 and 10 in year one, year two grade 9,10 & 11, and in year three we will serve grades 9-12. Students will be riding three buses 54 passenger school buses to and from the school located at 54 York Street in Portland. Our buses will serve the communities of the greater Portland region. The northern bus route will begin transporting students beginning at 7:00 am. at the Topsham Fair Mall parking lot. The southern bus route will begin in Kennebunk, and the western bus route will begin in Windham. All buses will arrive and depart from on Maple Street. Busses will arrive between 7:45 and 8:00 during the 180 day school year from mid September through mid June. Busses will depart at 2:00 pm.

Students from Portland, South Portland, Westbrook and Falmouth will be able to access the school via public transportation on the Metro system. We anticipate that 50% of our students will either walk, bike or use public transportation. Portland students who live on the peninsula will be within walking and biking distance to the school. BA will be providing bike racks at the school and encourage students from Portland to walk or bike to school. The school is also located within walking distance from Portland High School. In South Portland students will be encouraged to use the bike path leads to the Casco Bay Bridge. The school is less than a mile from the Casco Bay Bridge.

Since the school is located in the Old Port Historic District we will be providing parking only for a limited number of staff members. Leased parking spaces will be provided to staff members at the lot on the corner of Commercial and Maple Streets, this lot is owned by J.B. Brown and Sons. Next year we anticipate the needed number parking spots to be 10 for staff members. A survey of staff has indicated that the remaining staff members would walk, bike or ride public transportation to the school.

The policy for parking for students will be that if students choose to drive a vehicle to school they will be responsible for contracting a monthly parking spot. BA will not provide any student parking.

Bus drop off will be between 7:45 and 8 PM on Maple Street and a BA staff member will be outside to help manage student arrival and departure. Buses will be departing from Maple Street between 2:00 and 2:15 pm. We anticipate that morning arrival times will be staggered and no more than two buses on Maple Street at any one time.

Handicapped accessibility will be on York Street where there will be easy access to the main entrance of the building as the sidewalk and the entrance to the school level. We anticipate between 10 and 15 parents will be needing to pick up their students on a daily basis were dropping them off for doctors appointments or for late arrivals to school or early departures. We do not anticipate a large number of parental trips during the school day.

J.B. BROWN & SONS
36 Danforth Street
P.O. Box 207
Portland, ME 04112-0207
207-774-5908 (phone) 207-774-0898 (fax)

May 21, 2012

Baxter Academy
Attn: John Jacques
54 York Street
Portland, Maine 04101

Phone #: 207-318-8248
Account #: 11-baxtpk
e-mail: john@baxteracademy.org

Parking Lease Agreement

Dear John:

This document, when seen and agreed to by both parties, shall constitute a lease agreement between **J. B. Brown & Sons** (Landlord) and **Baxter Academy** (Tenant):

1. Parking Rights: Tenant shall have the right to park **ten (10)** authorized vehicles at the **321 Commercial Street**, Portland, Maine parking lot. Authorized vehicles are those whose year, make, model, color and license plate number have been provided to J.B. Brown & Son for inclusion on the parking list.

<u>Permit</u>	<u>Year</u>	<u>Color</u>	<u>Make</u>	<u>Model</u>	<u>License plate</u>
---------------	-------------	--------------	-------------	--------------	----------------------

* Descriptions must be provided prior to utilizing parking

2. Lease Terms: The parking agreement shall commence on September 1, 2012 **and it shall terminate on June 30, 2013**. The monthly rent of **Eighty dollars (\$ 80.00)** [discounted \$5.00 per spot as part of a group discount] per parking space shall be paid in advance, without notice from Landlord, on the first day of each month. **(Monthly total: \$800.00). Upon city approval for the school, a \$400 deposit will be required to hold these spaces.**

3. Tenant Responsibilities: It is the owner's responsibility to notify J.B. Brown & Sons of car make, year, color and registration number or any changes of that information. **Any car parked in the lot that is not on our tenant listing is subject to being towed at the owner's expense. Overnight parking is prohibited during the snow season. Vehicles prohibiting snow plowing or removal will be towed. All authorized vehicles MUST be operational and duly registered.**

4. Landlord's Responsibilities: J. B. Brown & Sons is responsible for snow removal but is not able to maintain consistent sanding and ice control. Parkers and guests park, walk, and drive in this lot at their own risk.

5. Hold Harmless Clause: Tenant shall hold J. B. Brown & Sons harmless for all losses or damages arising out of the use of the parking lot. J. B. Brown & Sons does not assume responsibility for Tenant's vehicle(s) nor any personal belongings.

6. Termination: Landlord and/or Tenant may cancel this parking agreement at any time by providing written notification thirty (30) days prior to the effective date of termination.

If the terms are satisfactory, please sign both documents and kindly return the one agreement to our office by **July 10, 2012**.

Sincerely,

Michelle Crowley
Office Manager

SEEN & AGREED TO BY:

For: Baxter Academy

City of Portland
Code of Ordinances
Sec. 14-332

Land Use
Chapter 14
Rev. 4-19-12

- (b) Motel: One (1) parking space for each sleeping room.
- (c) Hotels: One (1) parking space for each four (4) guest rooms.

$\frac{1}{2}$ (d) Schools providing instruction for students up to and including those fifteen (15) years of age: One (1) parking space for each room used for purposes of instruction.

$\frac{1}{2}$ (e) Schools providing instruction for students sixteen (16) years of age and over: One (1) parking space for each ten (10) seats or major fraction thereof, used for purposes of instruction; if no fixed seats, one (1) parking space for each one hundred (100) square feet or major fraction thereof used for purposes of instruction.

PARKING TOTALS.

8 ROOMS PROPOSED FOR = 6805 SF

> 15yrs = 8 spaces / 2 = 4 spaces

< 15yrs = 6805/100 / 2 = 34 spaces

38 spaces *

25-50 spaces requires 2 HANDICAP PARKING

* SINCE THE PROJECT IS LOCATED IN AN HISTORIC DISTRICT
NO ON-SITE PARKING MAYBE REQUIRED.

PARKING SPACES will be provided in lease with
J.B BROWN & SONS - @ MAPLE ST. PARKING LOT.
(see attached lease doc.)

School Year 2012-2013 - Target enrollment 160 students and lease approximately 10,000 sq. ft

School Year 2013-2014 - Target enrollment 240 students and lease approximately 15,0000 sq. ft.

School Year 2014-2012 - Target Enrollment 320 students and lease approximately 20,000 sq. ft.

Ground-level York Street for occupancy September 2012

- Entryway 20' x 8'
 - Main office 16' x 13'
 - Media & student center 38' x 48' 1824
 - Robotics and engineering classroom 38' x 22' 836
 - Storage room for robotics & engineering 15' x 17'
 - Art room and Science lab 38' x 36' - room to be partitioned 1368
 - Men's bathroom 14' x 8'
 - Women's bathroom 14 x 8'
- 2 = 4028 SF

Top floor to be occupied for September 2012

- Server room 28' x 13'
 - Men's bathroom 12' x 9'
 - Women's bathroom 12' x 9'
 - Computer labs - 41' x 32' - room to be partitioned into two computer labs -- 1312
 - Teachers room 15' x 8'
 - Classroom one 13' x 28' 364
 - Classroom two 13' x 26' 338
 - Classroom three 22' x 20' 440
 - Classroom four 19' x 17' 323
 - Upstairs admin office 16' x 16'
- = 2,777 SF

**INSTRUCTIONAL
FLOOR AREA = 6805 SF**

Best regards,
John



Level II – Preliminary and Final Site Plans Development Review Application Portland, Maine

Planning and Urban Development Department
Planning Division

Portland's Planning and Urban Development Department coordinates the development review process for site plan, subdivision and other applications under the City's Land Use Code. Attached is the application form for a Level II: Preliminary or Final Site Plan. Please note that Portland has delegated review from the State of Maine for reviews under the Site Location of Development Act, Chapter 500 Stormwater Permits, and Traffic Movement Permits. General information pertaining to the thresholds of review, public noticing procedures and the fee structure is contained in the Notice to Developer's Packet.

Level II: Site Plan Development includes:

- New structures with a total floor area of less than 10,000 sf except in Industrial Zones.
- New structures with a total floor area of less than 20,000 sf in Industrial Zones.
- Any new temporary or permanent parking area, paving of an unpaved surface parking area, or creation of other impervious surface area greater than 7,500 sf.
- Building addition(s) with a total floor area of less than 10,000 sf (cumulatively within a 3 year period) except in Industrial Zones.
- Building addition(s) with a total floor area of less than 20,000 sf in Industrial Zones.
- Park improvements: New structures or buildings with a total floor area of less than 10,000 sf, facilities encompassing an area of greater than 7,500 sf and less than 20,000 sf (excludes rehabilitation or replacement of existing facilities).
- New piers, docks, wharves, bridges, retaining walls, and other structures within the Shoreland Zone.
- Land disturbance between 1 and 3 acres (includes stripping, grading, grubbing, filling or excavation).
- A change in the use of a total floor area between 10,000 and 20,000 sf in any existing building (cumulatively within a 3 year period).
- Construction of a lodging house, bed and breakfast facility, emergency shelter or special needs independent living unit.
- Signage subject to approval pursuant to Section 14-526 (d) 8.a. (IV) of the Land Use Code.
- Any new major or minor auto service station with less than 10,000 sf of building area that is outside the B-2 or B-5 zones.
- The creation of day care or home babysitting facilities to serve more than 12 children in a residential zone (not permitted as a home occupation under section 14-410) in any principal structure that has not been used as a residence within the 5 years preceding the application.

The Land Use Code (including Article V), the Technical Manual, and the Design Manual are available on the City's web site at <http://www.portlandmaine.gov/planning/default.asp> or copies may be purchased at the Planning Division Office.

Planning Division
Fourth Floor, City Hall
389 Congress Street
(207) 874-8721 or 874-8719

Office Hours
Monday thru Friday
8:00 a.m. – 4:30 p.m.

PROJECT NAME: BAXTER ACADEMY for TECHNOLOGY & SCIENCE

PROPOSED DEVELOPMENT ADDRESS:

54 YORK ST.

PROJECT DESCRIPTION:

INTERIOR CHANGE OF USE FROM OFFICES TO
CHARTER (PUBLIC) SCHOOL > 10,000 SF FLOOR SPACE.

CHART/BLOCK/LOT: MAP 42 - BLOCK A
LOTS 7 & 8

PRELIMINARY PLAN 6/5/12 (date)
FINAL PLAN _____ (date)

CONTACT INFORMATION:

Applicant's Contact for electronic plans

Name: JAMES SEYMOUR P.E.
e-mail: JSEYMOUR@SEBAGOTECHNICS.COM
work #: 200-2083

Applicant – must be owner, Lessee or Buyer Name: <u>DAN LABRIE</u> Business Name, if applicable: <u>RUFUS DEERING LUMBER Co.</u> Address: <u>383 COMMERCIAL ST.</u> City/State: <u>PORTLAND, ME</u> Zip Code: <u>04101</u>	Applicant Contact Information Work # <u>772-6505</u> Home# <u>767-1055</u> Cell # <u>838-0444</u> Fax# <u>772-6981</u> e-mail: <u>Dlabrie@rufusdeering.com</u>
Owner – (if different from Applicant) Name: Address: <u>SAME</u> City/State: Zip Code:	Owner Contact Information Work # Home# Cell # Fax# e-mail:
Agent/ Representative Name: Address: <u>N/A</u> City/State: Zip Code:	Agent/Representative Contact information Work # Cell # e-mail:
Billing Information Name: <u>APPLICANT.</u> Address: City/State: Zip Code:	Billing Information Work # Cell # Fax# e-mail:

Engineer SEBAGO TECHNICS INC Name: JAMES SEYMOUR, P.E. Address: 75 JOHN ROBERTS ROAD, SUITE 1A City/State: So. PORTLAND, ME Zip Code: 04106	Engineer Contact Information Work # 200-2083 Cell # 632-1199 Fax# 856-2256 e-mail: jseymour@sebagotechnics.com
Surveyor SEBAGO TECHNICS INC Name: WILLIAM SHIPPEN PLS Address: 75 JOHN ROBERTS RD, SUITE 1A City/State: So. PORTLAND, ME Zip Code: 04106	Surveyor Contact Information Work # 200-2084 Cell # Fax# 856-2256 e-mail: wshippen@sebagotechnics.com
Architect RICK RENNER ARCHITECTURE Name: CHARLES YOUNG Address: 35 Pleasant St. City/State: PORTLAND, ME Zip Code: 04101	Architect Contact Information Work # 773-9699 x 203 Cell # Fax# e-mail: cyoung@rrennerarchitects
Attorney PEARCE & DOW Name: MIKE PEARCE Address: 50 MONUMENT SQ. City/State: PORTLAND, ME Zip Code: 04101	Attorney Contact Information Work # 822-9900 Cell # Fax# 822-9901 e-mail:

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Cash or Check payable to the City of Portland.)

Level II Development (check applicable reviews) <input checked="" type="checkbox"/> Less than 10,000 sq. ft. (\$400.00) <input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)	Fees Paid (office use) _____ _____	Other Reviews (check applicable reviews) <input type="checkbox"/> Traffic Movement (\$1,000) <input type="checkbox"/> Stormwater Quality (\$250) <input type="checkbox"/> Section 14-403 Review (\$400 + \$25/lot) # of Lots ____ x \$25/lot = _____ <input type="checkbox"/> Other _____ <input checked="" type="checkbox"/> Change of Use <input type="checkbox"/> Flood Plain <input type="checkbox"/> Shoreland <input type="checkbox"/> Design Review <input type="checkbox"/> Housing Replacement <input type="checkbox"/> Historic Preservation	Fees Paid (office use) _____ _____
The City invoices separately for the following: <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) Third party review is assessed separately.			
Plan Amendments (check applicable reviews) <input type="checkbox"/> Planning Staff Review (\$250) <input type="checkbox"/> Planning Board Review (\$500)	Fees Paid (office use) _____ _____		

APPLICATION SUBMISSION

All site plans and written application materials must be uploaded to a website for review. At the time of application, instructions for uploading the plans will be provided to the applicant. One paper set of the plans, written materials and application fee must be submitted to the Planning Division Office to start the review process.

Application submissions shall include one (1) paper packet with folded plans containing the following materials:

Numbers below changed- can't seem to highlight

1. **One (1) full size site plans** that must be **folded.**

One (1) set of all written materials or as follows, unless otherwise noted:

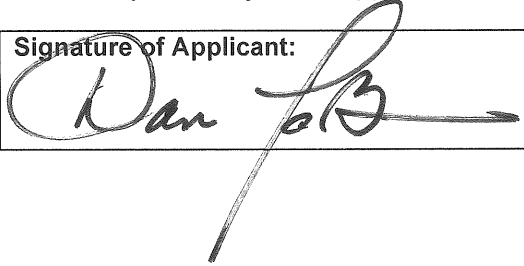

- a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
2. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet.
 3. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
 4. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
 5. One (1) set of plans reduced to 11 x 17.

Refer to the application checklist for a detailed list of submittal requirements.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level II Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: 	Date: 
--	---

PROJECT DATA

The following information is required where applicable, in order complete the application

Total Site Area	9715 sq. ft.
Proposed Total Disturbed Area of the Site	N/A sq. ft.
(If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)	
IMPERVIOUS SURFACE AREA	
• Proposed Total Paved Area	N/A sq. ft.
• Existing Total Impervious Area	9715 sq. ft.
• Proposed Total Impervious Area	N/A sq. ft.
• Proposed Impervious Net Change	0 sq. ft.
BUILDING AREA	
• Proposed Building Footprint	-NA- sq. ft.
• Proposed Building Footprint Net change	0 sq. ft.
• Existing Total Building Floor Area	sq. ft.
• Proposed Total Building Floor Area	0 sq. ft.
• Proposed Building Floor Area Net Change	0 sq. ft.
• New Building	(yes or <input checked="" type="radio"/> no)
ZONING	
• Existing	B5b
• Proposed, if applicable	SAME
LAND USE	
• Existing	MIXED USE -
• Proposed	OFFICES · RETAIL · LABS · ZUMBA CHARTER SCHOOL
RESIDENTIAL, IF APPLICABLE	
• Proposed Number of Affordable Housing Units	N/A
• Proposed Number of Residential Units to be Demolished	↓
• Existing Number of Residential Units	↓
• Proposed Number of Residential Units	↓
• Subdivision, Proposed Number of Lots	↓
PARKING SPACES	
• Existing Number of Parking Spaces	12
• Proposed Number of Parking Spaces	10 (LEASED)
• Number of Handicapped Parking Spaces	2 (YORK ST.)
• Proposed Total Parking Spaces	1
BICYCLE PARKING SPACES	
• Existing Number of Bicycle Parking Spaces	0
• Existing Number of Bicycle Parking Spaces	0
• Proposed Number of Bicycle Parking Spaces	5
• Total Bicycle Parking Spaces	5
ESTIMATED COST OF PROJECT	<u>\$185,000.00</u>

General Submittal Requirements – Preliminary Plan (Optional)

Level II Site Plan

Preliminary Plan Phase Check list (if elected by applicant)

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Completed application form
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Application fees
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written description of project
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Evidence of right, title and interest.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Copies of required State and/or Federal permits.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of proposed project's compliance with applicable zoning requirements.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written description of existing and proposed easements or other burdens.
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written requests for waivers from individual site plan and/or technical standards, where applicable.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Traffic analysis (may be preliminary, in nature, during the preliminary plan phase).
<input type="checkbox"/> NA	<input type="checkbox"/>	1	Written summary of significant natural features located on the site.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1	Written summary of project's consistency with related city master plans.
Applicant Checklist	Planner Checklist	Number of Copies	Site Plan Submittal Requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>		Boundary Survey meeting the requirements of Section 13 of the City of Portland Technical Manual.
<input type="checkbox"/>	<input type="checkbox"/>	1	Preliminary Site Plan Including the following: (*information provided may be preliminary in nature during preliminary plan phase):
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed structures with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Location of adjacent streets and intersections and approximate location of structures on abutting properties.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Proposed site access and circulation.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Proposed grading and contours.
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Preliminary landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Existing and proposed utilities (preliminary layout).
<input checked="" type="checkbox"/>	<input type="checkbox"/>		▪ Preliminary infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, transit infrastructure, roadway improvements).
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Preliminary stormwater management and erosion control plan.
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b) 1. of the Land Use Code).
<input type="checkbox"/> NA	<input type="checkbox"/>		▪ Proposed alterations to and protection measures for significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important

NA

<i>natural features listed in Section 14-526 (b)1. of the Land Use Code).</i>
▪ Existing and proposed easements or public or private rights of way.

**General Submittal Requirements – Final Plan (Required)
Level II Site Plan
Final Plan Phase Check list (including items listed above in General Requirements for Preliminary Plan, if applicant did not elect to submit for a preliminary plan review)**

Applicant Checklist	Planner Checklist	Number of Copies	Written Submittal Requirement
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of financial and technical capacity.
<input type="checkbox"/>	<input type="checkbox"/>	1	Evidence of utilities' capacity to serve the development.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of fire safety (referencing NFPA fire code and Section 3 of the City of Portland Technical Manual).
<input type="checkbox"/>	<input type="checkbox"/>	1	Construction management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Traffic Plan (if development will (1) generate 100 or more PCE or (2) generate 25 or more PCE and is located on an arterial, within 1/2 mile of a high crash location, and/or within ¼ mile of an intersection identified in a previous traffic study as a failing intersection).
<input checked="" type="checkbox"/> NA	<input type="checkbox"/>	1	Stormwater management plan.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written summary of solid waste generation and proposed management of solid waste.
<input type="checkbox"/>	<input type="checkbox"/>	1	Written assessment of conformity with applicable design standards.
<input type="checkbox"/>	<input type="checkbox"/>	1	Manufacturer's verification that HVAC and manufacturing equipment meets applicable state and federal emissions requirements.

Final Plan Phase			
<input type="checkbox"/>	<input type="checkbox"/>	1	Final Site Plan Including the following
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed structures on the site with distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Location of adjacent streets and intersections and approximate location of structures on abutting properties.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed site access and circulation.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed grading and contours.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Location and dimension of existing and proposed paved areas including all parking areas and vehicle, bicycle and pedestrian access ways. Proposed curb lines must be shown.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed loading and servicing areas, including applicable turning templates for delivery vehicles
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed snow storage areas or snow removal plan.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed trash and recycling facilities.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Landscape plan including existing vegetation to be preserved, proposed site landscaping and street trees.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Existing and proposed utilities.
<input type="checkbox"/>	<input type="checkbox"/>		▪ Location and details of proposed infrastructure improvements (e.g. - curb and sidewalk improvements, roadway intersection modifications, utility connections, public transit infrastructure, roadway improvements).
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed septic system, if not connecting to municipal sewer. (Portland Waste Water Application included in this application)
<input type="checkbox"/>	<input type="checkbox"/>		▪ Proposed finish floor elevation (FFE).

-
-
-
-
-
-
-
-
-
-
-
-
-
-

-
-
-
-
-
-
-
-
-
-
-
-
-
-

<ul style="list-style-type: none"> ▪ <i>Exterior building elevation(s) (showing all 4 sides).</i>
<ul style="list-style-type: none"> ▪ <i>Proposed stormwater management and erosion controls.</i>
<ul style="list-style-type: none"> ▪ <i>Exterior lighting plan, including street lighting improvements..</i>
<ul style="list-style-type: none"> ▪ <i>Proposed signage.</i>
<ul style="list-style-type: none"> ▪ <i>Identification of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code). Wetlands must be delineated.</i>
<ul style="list-style-type: none"> ▪ <i>Proposed alterations to and protection measures for of existing significant natural features located on the site (including wetlands, ponds, watercourses, floodplains, significant wildlife habitats and fisheries or other important natural features listed in Section 14-526 (b)1. of the Land Use Code).</i>
<ul style="list-style-type: none"> ▪ <i>Total area and limits of proposed land disturbance.</i>
<ul style="list-style-type: none"> ▪ <i>Soil type and location of test pits and borings.</i>
<ul style="list-style-type: none"> ▪ <i>Details of proposed pier rehabilitation (Shoreland areas only).</i>
<ul style="list-style-type: none"> ▪ <i>Existing and proposed easements or public or private rights of way.</i>

BEAGLE, PEARCE & RIDGE, LLC
ATTORNEYS AT LAW

C. ALAN BEAGLE
MICHAEL J. PEARCE
MARTIN J. RIDGE
JACQUELYN M. KURZ
PAULA M. CRAIGHEAD

24 CITY CENTER
P.O. BOX 7044
PORTLAND, MAINE 04112
(207) 773-1751
FAX (207) 775-3382

November 30, 1998

Mr. William M. Moody, Jr., President
Rufus Deering Company
383 Commercial Street
P.O. Box 880
Portland, ME 04112-0880

Sally Deering
Box

Re: Rufus Deering Company/Peoples Heritage Bank/York
Street, Inc.

Dear Bill:

Enclosed please find the following original documents:

1. Warranty Deed from York Street, Inc. to Rufus Deering Company. This Warranty Deed has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 13.
2. Assignment of Leases and Security Deposits from York Street, Inc. to Rufus Deering Company. This Assignment has been recorded in the Cumberland County Registry of Deeds in Book 14115, Page 36.

If you have any questions, please feel free to contact me.

Very truly yours,

Robyn A. Pearce
Robyn A. Pearce

Assistant to Michael J. Pearce

enclosure

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.
By: Werner Schnetzer
Werner Schnetzer
Its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,
Paul S. Bunker
Notary Public/Attorney-at-Law
Print Name: Paul S. Bunker

122895/PSB/SKLF
91401.4926

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4: 48

CUMBERLAND COUNTY

John B. O'Brien

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PM 4: 52

CUMBERLAND COUNTY

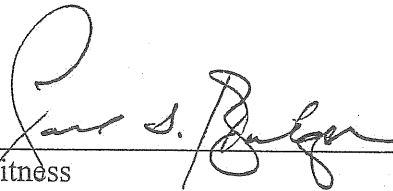
John B O'Brien

61274


ASSIGNMENT OF LEASES AND SECURITY DEPOSITS

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04106, for consideration, does hereby ASSIGN to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, its successors and assigns, all its right, title and interest in and to any and all leases, subleases and/or tenancies of the premises located on or about 54-62 York Street in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto, including, without limitation, any and all tenant security deposits relating thereto.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by Werner Schnetzer, its duly authorized President, on this 1st day of September, 1998.



Witness

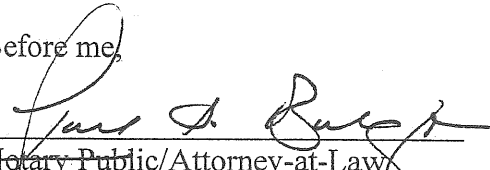
YORK STREET, INC.
By: 

Werner Schnetzer
Its: President

STATE OF MAINE
CUMBERLAND, SS.

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in said capacity and the free act and deed of said YORK STREET, INC.

Before me,


Notary Public/Attorney-at-Law
Print Name: Paul S. Buzzer

122937/PSB/SKLF
91401.4926

Review Staff
City of Portland
Portland, Me 04101

Dear Staff Members:

Baxter Academy for Technology and Science
54 York Street

HISTORY

The building is located on the corner of York and Maple (see attached map) in Portland. Fifty-four York Street has had numerous uses over the years. Most recently approximately 20,000 sq. ft. was leased to Media Power. In April of 2010 Media Power needed more space and moved to a building on Foden Road in South Portland.

In mid-2011 John Jaques, future Executive Director of Baxter Academy, and Daniel LaBrie, Senior Vice President of Rufus Deering Lumber Company, began discussing a possible partnership by leasing 54 York Street to them for a charter school.

OBJECTIVE

The school aims to link students with local businesses, scientists, researchers and other professionals working in fields of science, technology, engineering and mathematics. The Portland location offers easy access to the Gulf of Maine Research Institute, Maine Medical Center, the University of Southern Maine and Southern Maine Community College.

The school hopes to get a charter from the state by July 1st and open in September. It hopes to be the first of its kind in Portland.

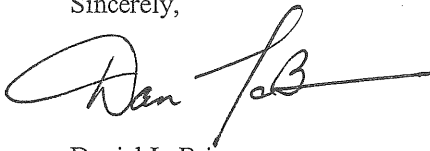
PROPOSAL

The fit up to prepare the building for a school consists of upgrading HVAC, Security system, Handicap Accessibility, new carpets and paint. The building as it stands today does not have an elevator. An exterior elevator will be installed on the southwest side of the building on a 15' easement.

CLOSING

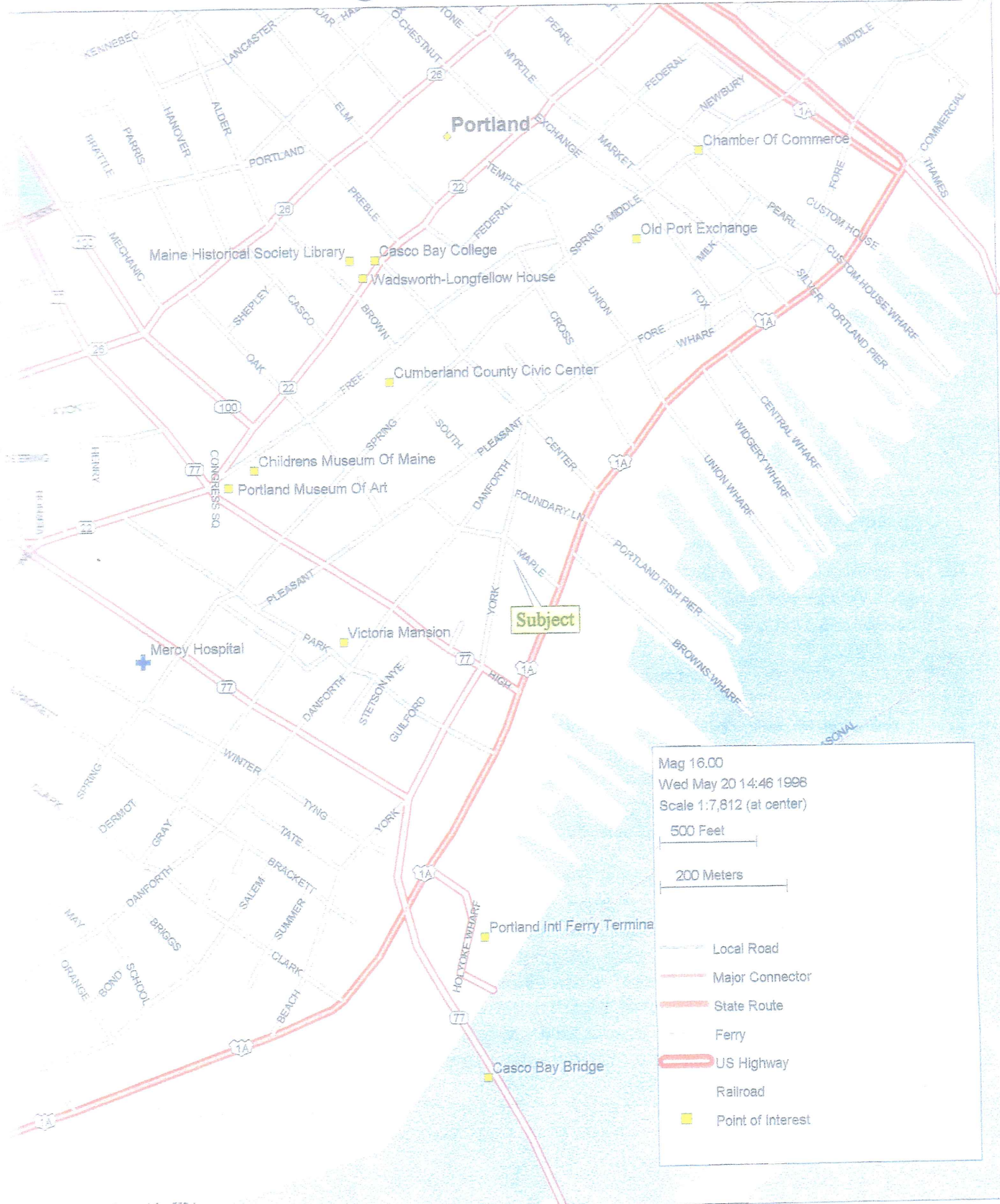
I believe this is a perfect location for a charter school. It is wonderful to be able to offer our youth an alternative education to the usual public school system. These charter schools have been very successful all over the country. Time is of the essence, I would ask that you move this process along and grant the permit in a timely manner.

Sincerely,



Daniel LaBrie
Senior Vice President

Neighborhood Map



Mag 16.00
Wed May 20 14:46 1998
Scale 1:7,812 (at center)

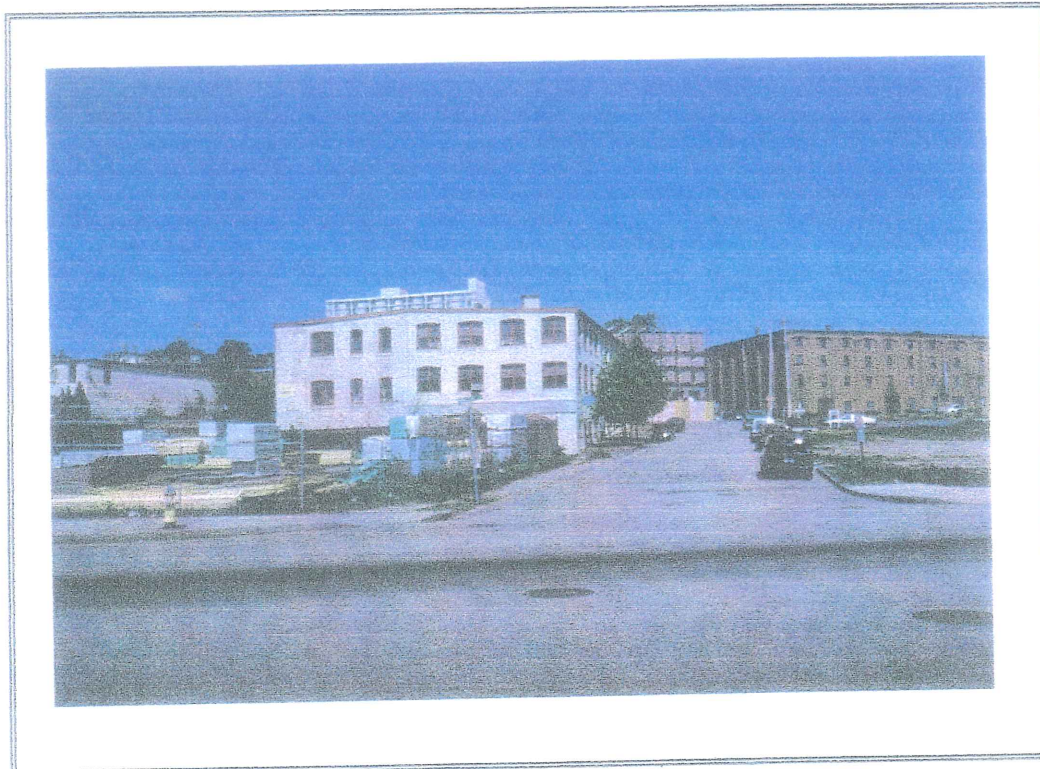


- Local Road
- Major Connector
- State Route
- Ferry
- US Highway
- Railroad
- Point of Interest

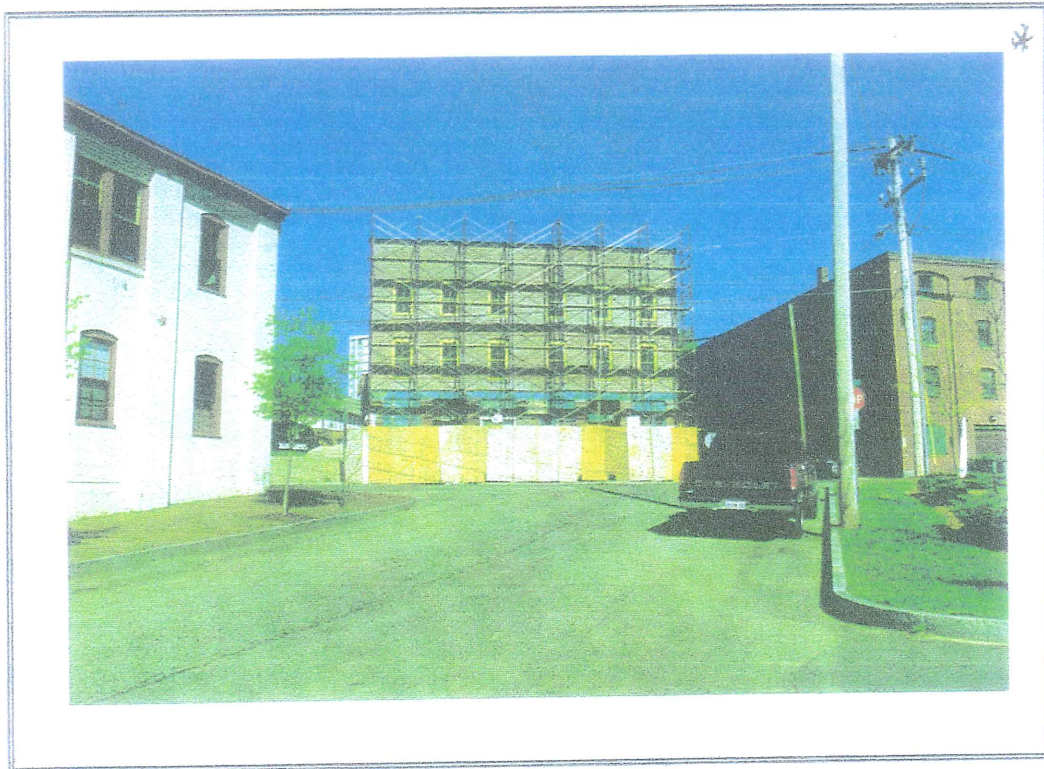
SUBJECT PHOTOGRAPHS



1) Easterly view of subject from York Street



2) Westerly view of subject from Commercial Street, looking up Maple Street



N2) Ongoing rehabilitation/revitalization of the subject neighborhood (subject at left)

SUBJECT SITE DESCRIPTION

Location

The subject site enjoys a corner lot location at the apex of York and Maple Streets in Portland Maine. This location receives heavy traffic flow along York Street as this is the major connector route from the Portland CBD to the Casco Bay Bridge providing access to the City of South Portland. The site is also within close proximity to the Portland Waterfront District with marine, transit, and tourist related land uses prevalent.

Land Area

The subject site, according to the legal description provided, contains an estimated land area of 8,712±sf in fee simple, plus an additional passageway area measuring 15' x 85' (1,275±sf) for a total of 9,987± sf. The survey plan also provided to the appraiser, showed an area of 8,447±sf, although it is not clear that this is not the footprint of the building rather than the site. For purposes of this appraisal, this is not a large issue as the site is considered fully built out and the leased fee interest has been appraised. The client is urged to consult with a surveyor and/or title attorney to clarify such matters further prior to making decisions which may be based upon land area estimates.

Shape/Frontage

The shape of the site is characterized as irregular. The parcel is a corner lot with an almost trapezoidal shape due to its frontage on two streets. According to the legal description provided, the site has 125'± of frontage along Maple Street plus another 100'± of frontage along York Street.

Access/Visibility

The subject site is considered to have good access and visibility characteristics. The site enjoys high traffic counts approximating 12,000 cars per day according to MDOT traffic volumes 1996. York Street feeds directly into/from the new Casco Bay Bridge (replaced the Million Dollar Bridge last year) and is a direct link to the Portland CBD and Old Port Districts. Commercial Street to the south is also a major commuter travel artery and serves the working waterfront section of Portland. Maple Street is a small connector road which traverses the two.

Topography/Soils

The topography of the site is characterized as sloping in an easterly direction down from its York Street frontage towards Commercial Street and the Portland Waterfront. No on site soils investigation was performed by the appraiser. The Cumberland County Soils Survey classifies this area as Urban, Cut and Fill land. It was reported that a portion of the subject (original construction) dates back to 1820 and survived the Great Fire of 1936 which leveled much of downtown Portland and gave way to the filling in along the waterfront and creation of Commercial Street in 1950. Given the age and varied uses of the site over time, the client is urged to conduct due diligence in regards to environmental issues to their own satisfaction. The appraised value herein assumes no adverse conditions related to environmental issues which may or may not affect the subject site. Reference is made to the Statement of Assumptions and Limiting Conditions in this regard.

Utilities

The subject site is serviced with all public utilities including water, sewer, natural gas, electricity, telephone and cable TV. Municipal services such as fire and police protection and sanitation are deemed to be adequate.

Easements

Reference is made to the attached legal description and title report contained in the Addenda section of this report for an overview of the various limitations of title to the subject property. As discussed, part of the site is encumbered as a passageway in common with others. There were no other adverse easements or encroachments noted on the date of inspection/value herein which would limit the use and/or value of the subject property under its current land use program.

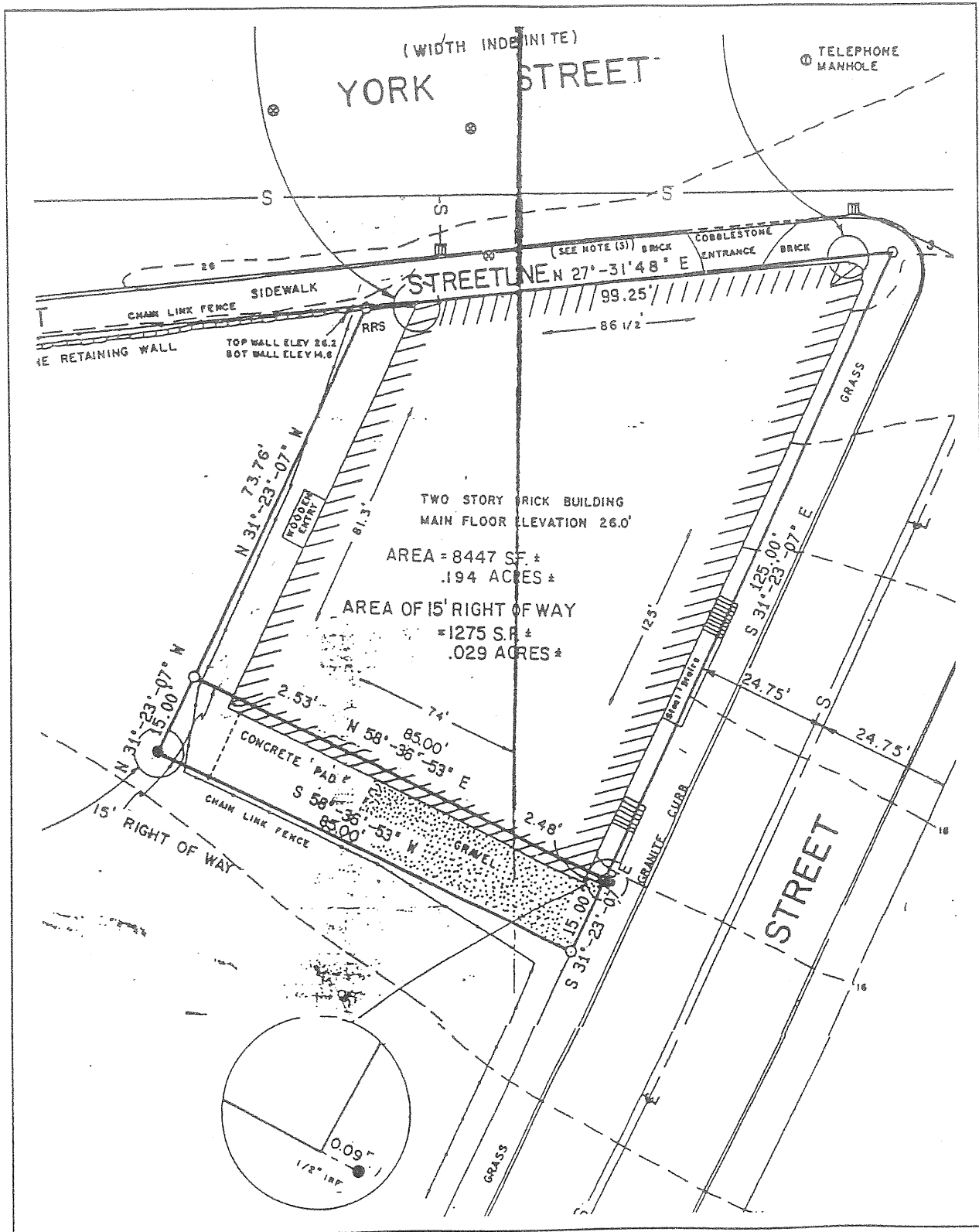
Functional Utility

The subject site is considered to offer average functional utility for development purposes. If vacant and available, it would likely be assembled with abutting lands for a larger scale development. The site does offer good exposure for commercial purposes and adequate frontage for access. It is zoned for business use and is located in an area of revitalization. An interim use for surface parking would be an immediate use of the site.

Marketability/Appeal

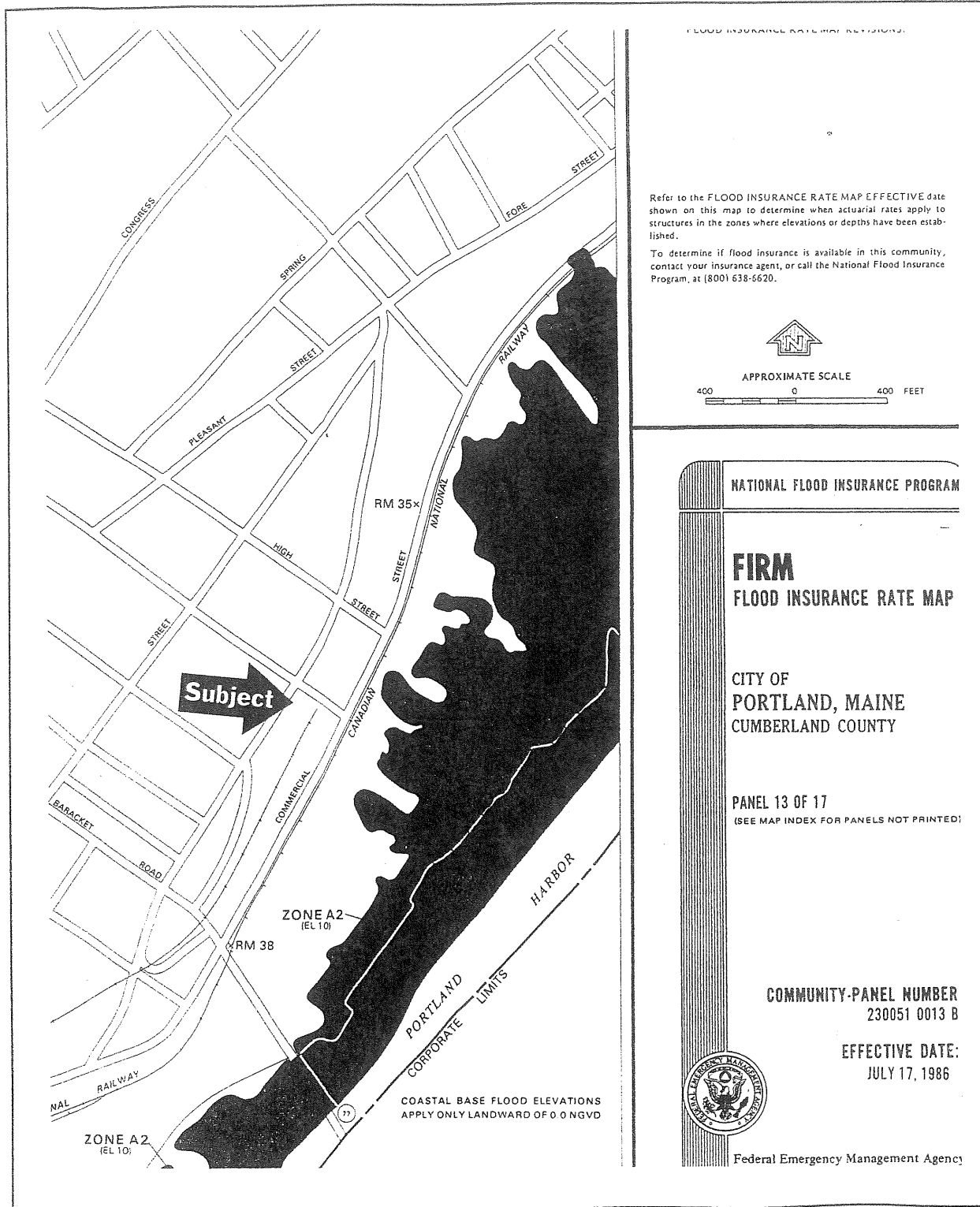
For the reasons cited above, the subject land parcel is considered very marketable and would be anticipated to sell quickly if vacant and available.

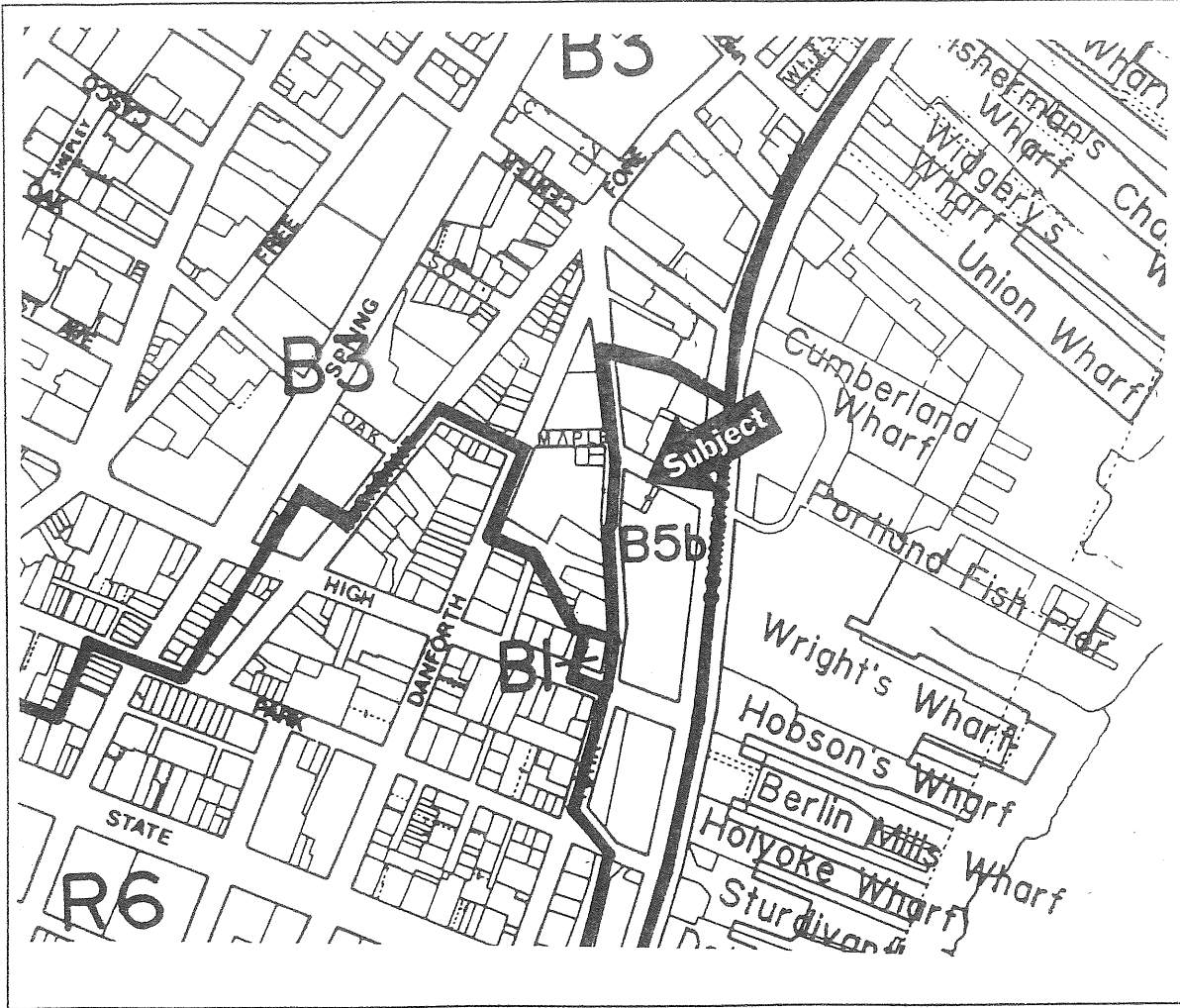
SUBJECT SITE PLAN
(not to scale)



FEDERAL EMERGENCY MANAGEMENT FLOOD HAZARD ZONE

The subject property is not located in a federally designated 100-year flood hazard zone, according to the FEMA Map presented below:





PICK

SUBJECT IMPROVEMENTS DESCRIPTION

Year Built

The subject building improvements were built in the late 1800's, and subsequent to a fire, was rebuilt and expanded around the 1920's.

Gross Building Area

The building is estimated to contain approximately 21,297±sf of gross building area, all of which has been considered net rentable area as per existing leases in place (common areas prorated).

Foundation

The foundation walls are of mortared brick and appear to be in sound condition.

Frame

The frame of the building is heavy timber columns and wooden floors, some supported with brick columns and steel girders.

Exterior Walls

The exterior walls are of painted brick.

Floors

The top two floors in the building were generally of wood underlayment with a concrete slab floor at the daylight basement level.

Floor Covering

Floor coverings ranged from low cost to medium grade commercial carpeting in the office areas, some composition tile and/or vinyl floor finishes in the bathroom areas. The lower level was mostly painted concrete with some finished areas carpeted.

Partitions

Interior partitions were mostly of wood stud walls and sheetrocked panels. Some paneling and matchboard wainscoting were also noted.

Layout

The layout is fairly heavily partitioned into numerous offices and suites for multi-tenant occupancy.

Ceilings

The ceilings were primarily suspended acoustic panel tiles and in good condition, with recessed fluorescent lighting throughout.

Roof Structure & Cover

The roof structure is of a flat, shed style with a covering of rubber membrane dating to 1989 according to the present owner.

Electrical

The electrical entrance to the building consists of two primary services of four-wire, three phase, 240 volt capacity with a 400-amp rating. Distribution is via six 100 amp and 1 200-amp panels with circuit breaker protection. Electricity is separately metered to each tenant excepting unit 102 which is on the house meter.

Plumbing

The building is serviced with municipal water and sewer. The main water supply enters in the basement sprinkler room and has a backflow preventer. Where visible, plumbing distribution piping is primarily copper with some PVC and cast iron waste lines noted. There is also a recent, gas fired domestic hot water heater in the basement.

H.V.A.C.

The building is serviced with a natural gas heater with individual tenant meters, excepting unit 102 which is on the house. Heat is provided through the building with a combination of ceiling and wall hung gas heaters. There is a central, gas-fired boiler with five zones noted on the first floor as well as some electric baseboard heat interspersed.

Elevators

There are no elevators currently in the building.

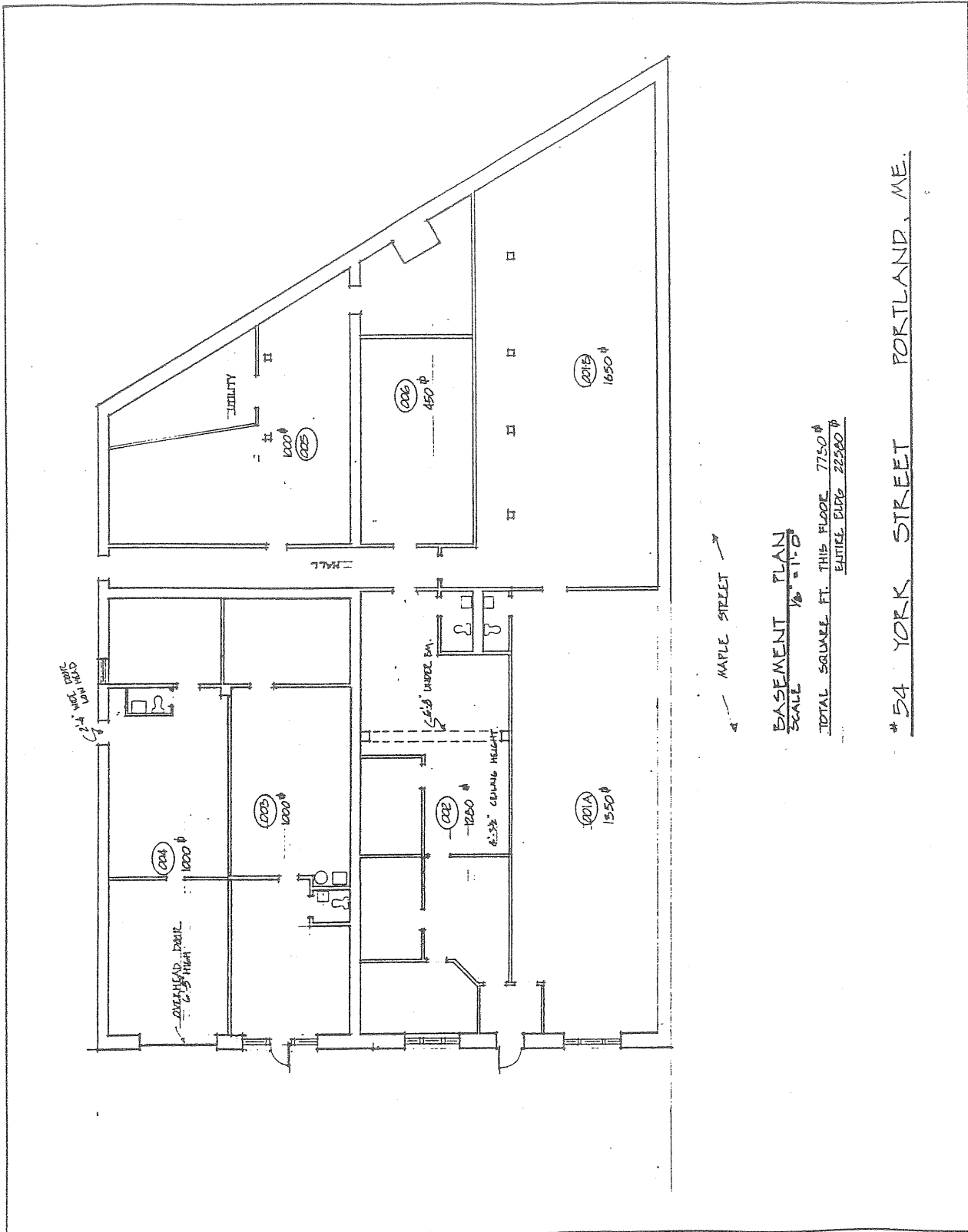
Sprinkler System

The building is provided with a dry type sprinkler system.

Overall Condition

The overall condition of the improvements is rated as average to good.

SUBJECT IMPROVEMENTS SKETCH - Basement Level
(not to scale)

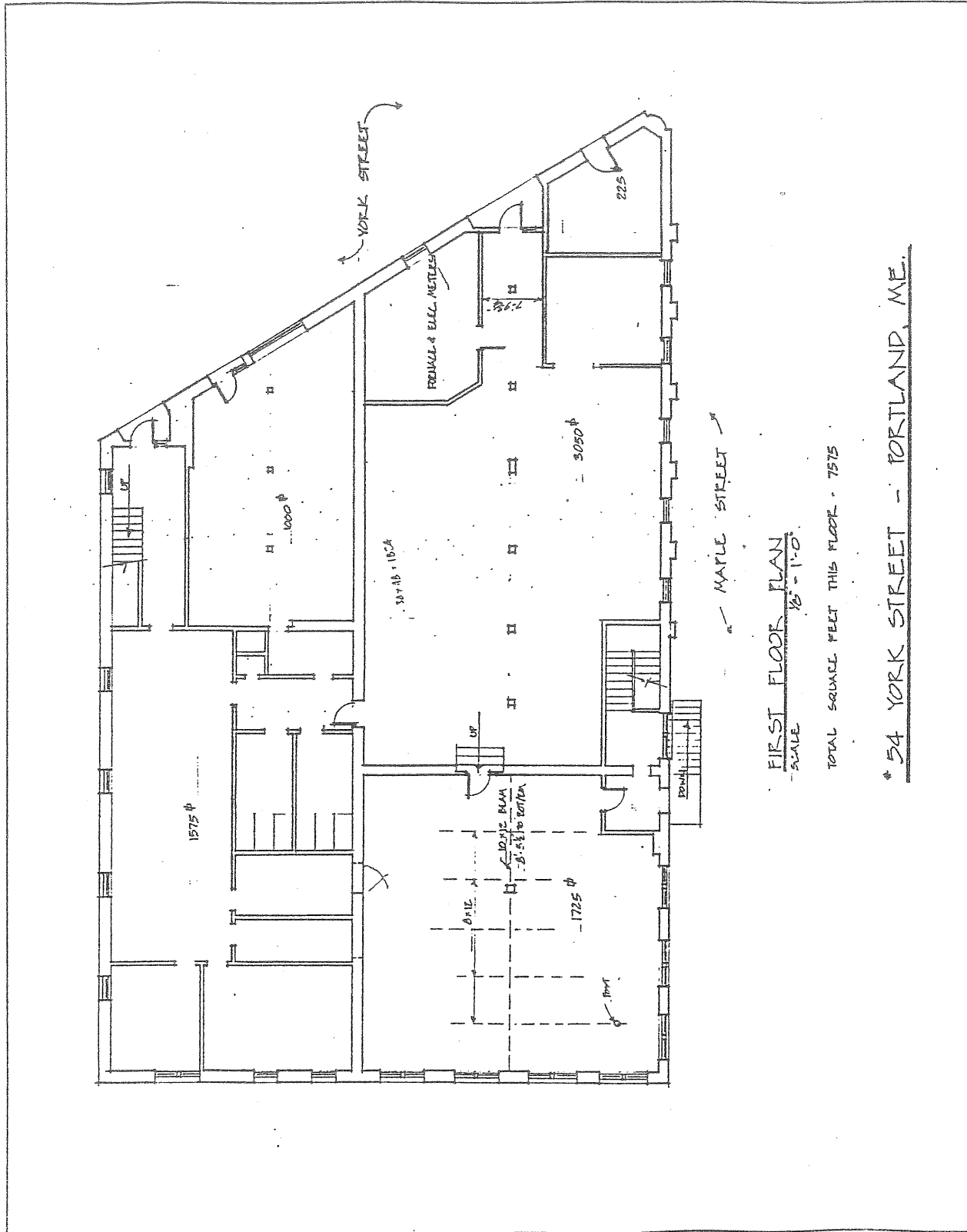


BASEMENT PLAN
SCALE 1/8" = 1'-0"

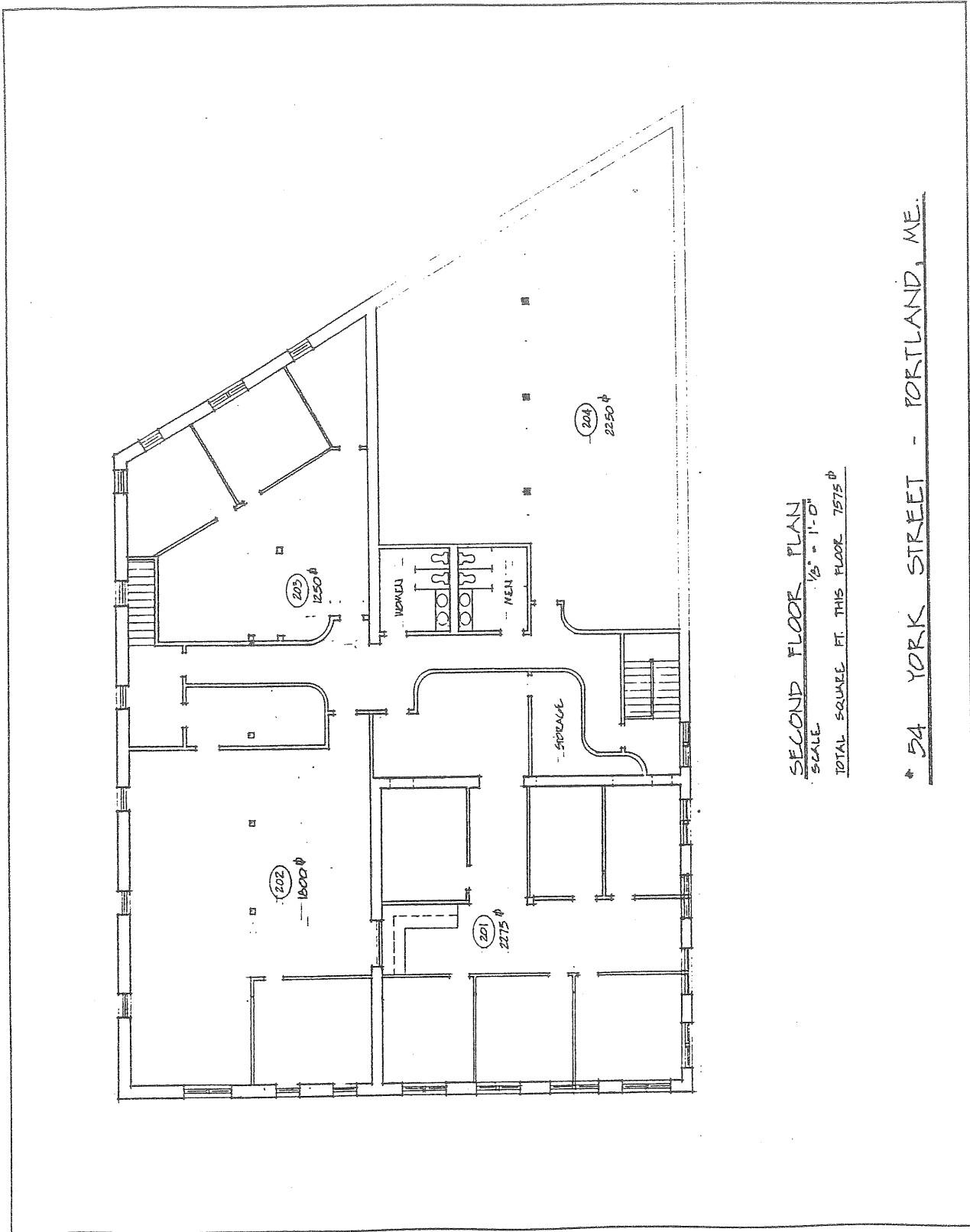
TOTAL SQUARE FT. THIS FLOOR 7750 #
ENTIRE BLDG 22280 #

54 YORK STREET PORTLAND, ME.

SUBJECT IMPROVEMENTS SKETCH - First Floor Level
(not to scale)



SUBJECT IMPROVEMENTS SKETCH - Top Floor Level
(not to scale)



SECOND FLOOR PLAN
SCALE 1/8" = 1'-0"
TOTAL SQUARE FT. THIS FLOOR 7575 sq ft

54 YORK STREET - PORTLAND, ME.

SURVEYOR'S REPORT

Fill out and sign with all Surveys in accordance with the Instructions on the reverse side.

To: LAWYERS TITLE INSURANCE CORPORATION

Richmond, Virginia.

THIS IS TO CERTIFY, that on December 3, 19 87, I made an accurate survey of the premises standing in the name of William J. Dowd

situated at Portland Cumberland Maine City County State

briefly described as: Commercial Structure

and shown on the accompanying survey entitled: Standard Boundary Survey for William J. Dowd

I made a careful inspection of said premises and of the buildings located thereon at the time of making such survey, and again on December 17, 19 87, and at the time of such latter inspection I found William J. Dowd to be in possession of said premises as owner

(tenant) or (owner)

I further certify as to the existence or non-existence of the following at the time of my last inspection:

1. Rights of way, old highways, or abandoned roads, lanes or driveways, drains, sewer, water, gas or oil pipe lines across said premises (Include any such matters shown on the recorded plat of subdivision):

15 foot Right of Way to land now or formerly of Rufus Deering Location of sewer connection uncertain

2. Springs, streams, rivers, ponds, or lakes located, bordering on or running through said premises: None observed

3. Cemeteries or family burying grounds located on said premises. (Show location on plat): None observed

4. Telephone, telegraph or electric power poles, wires or lines located on, under, overhanging or crossing said premises and serving other property or properties: None observed above ground below grade lines uncertain

5. Joint driveways or walkways; party walls or rights of support; porches, steps or roofs used in common or joint garages: 15 foot strip above mentioned apparently owned by Dowd subject to use by abutter.

6. Disputed boundaries, encroachments or overhanging projections. (If the buildings, projections or cornices thereof, or signs affixed thereto, fences or other indications of occupancy encroach upon or overhang adjoining properties or easement areas, or the like encroach upon or overhang surveyed premises, specify all such): Encroachment of building onto 15 foot Right of Way, Sign overhangs York Street, Fire Escapes overhang Maple Street, Fence of Abutter encroaches on said property.

7. Physical evidence of boundary lines on all sides. (Be specific): Iron pins found at two corners (see plan)

8. Is the property improved? Yes (a) Building is: Brick (X); Clapboard (); other ()

(b) Building is: One story (); Two story (X); split-level (); other () Specify Specify

9. Indications of building construction, alterations or repairs within recent months: Exterior brick cleaned, repainted, new doors

(a) If new improvements under construction, how far have they progressed? 50 per cent completed more or less

City in progress of determin-

10. Changes in street lines either completed or officially proposed: ing York Streetline

(a) Are there indications of recent street or sidewalk construction or repairs? None

11. Are all abutting streets or roads maintained by public authorities? Yes

(a) Is access to such streets or roads limited? NO

12. If the surveyed premises are subject to restrictive covenants, do the improvements, use and occupancy comply with such? (If the premises are subject to restrictive covenants, have the examining attorney furnish you verbatim copy of them) Note: Origination Deed 1555/24 requires Grantee to build and maintain fence. Existing fence appears to have been constructed by abutter.

William A. Campbell Jr. Civil Engineer or Surveyor

NOTE: In all cases where there are encroachments, easements, party walls, etc., they should also be denoted upon the map of your survey. Also, be certain map complies with Instructions on reverse side. Particular attention is directed to Paragraph 3 of these instructions.

(Be sure to answer each item. If the property is not subject to any such of items 1 to 6 inclusive, insert the word "none" following the item.)



June 5, 2012
07383

Barbara Barhydt
City of Portland Planning Division
City Hall 4th Floor
389 Congress St.
Portland, ME 04101

**Minor Site Plan Level II Application Submittal – Change of Use Permit
Proposed Baxter Academy for Technology and Science –Charter School
54 York Street, Portland, ME**

Dear Ms. Barhydt:

Enclosed please find a copy of the Level II Site Plan Application and associated site drawings that we have prepared for the Baxter Academy for Technology and Science on property owned by Rufus Deering Lumber Company at 54 York Street, Portland, Maine (located on the corner of York Street and Maple Street). The parcel proposed for development is the existing Rufus Deering Lumber Milling display store with other mixed uses for offices, labs, and dance studios. The property consists of a 0.22 acre lot fully developed with an existing 7,540 square foot (SF) footprint for a three story building zoned in the B-5b (Urban Commercial Business Zone). The proposed use will entail a floor area conversion of over 10,000 SF change of use and will require a Level II Site Plan review by the Planning Staff. It is also within the Old Port Historical District.

The overall improvement of the site will not add any additional impervious areas, as the site now is completely impervious. The Baxter Academy is a proposed Public Charter High School focusing in the areas of mathematics, technology, and science. Their vision is to convert the entire building into a full 4 year high school over the next 3 to 4 years. Due to the Baxter Academy's plan to convert the existing structure into a school, all the retrofitting will occur with the interior spacing, and no building additions are anticipated to the exterior. Therefore, items such as stormwater and existing utilities services will not be altered as a result of the change of use. However, the need for Americans with Disabilities Act (ADA) access into the building will require a handicap ramp access to be extended into the City's right-of-way (ROW) at their York Street entrance, along with street space to be dedicated for handicap parking and access. This work will require rebuilding a small portion of the sidewalk and curbing along a section of York Street.

Rufus Deering Lumber Company will provide the lease for Baxter Academy and will be incorporating all necessary interior building improvements for the Charter School. We anticipate that the Baxter Academy will be a long-term lease as the Charter School envisions an enrollment of 320 students by 2014. However, in the first year the expectation is an enrollment of nearly 160 students. Our primary issue on the site has to deal with the transportation of students and parking arrangements for faculty and administrators.

Based on the City's parking requirements, school parking requirements are based on age. One regulation is based on parking for schools with students 15 years of age and under and one deal with students 16 years and older. This facility will be handling students in each grouping equally given the high school ages. We calculated that 42 spaces would be required for the leased space in year one, and that the

requirements will increase slightly with complete full building lease and build-out by 2014. Baxter Academy has a contract for parking spaces with JB Brown for their parking lot located opposite of the school off Maple Street and would expand that as the school expands in the future.

In addition, the Director, John Jacques, has indicated that the Charter School intends to contract for bus transportation for three full-size buses for student transportation in year one, which is the equivalent of upwards of 162 students. Other students could take other modes of public transportation, or be dropped off by parents. Student parking will not be encouraged or provide by the school. Maple Street has been designated as a location for bus delivery and pick-up of students. Busses are intended to be turning down Maple Street off of York Street such that students will be loaded/unloaded from the existing sidewalk adjacent to the proposed school building. Although the school has a requirement for several spaces, the school does not feel that they will have a need for the required spaces, but could retain JB Brown's lot on an event basis for special activities or conferences which may be held at the school above its base lease for faculty and administrators.

We are proposing that the City allow us to reconstruct a portion of the sidewalk along York Street, as well as dedicate two on-street parking spaces to be allowed for ADA compliant parking to meet our current demand. Both will be constructed to allow van access and direct ramps to the sidewalk and access into the York Street main entrance. In the final year of building renovation, when the bottom floor of the existing building (currently the Deering Lumber Milling Store) is occupied by the school, a second location off Maple Street could be dedicated for additional handicap spaces.

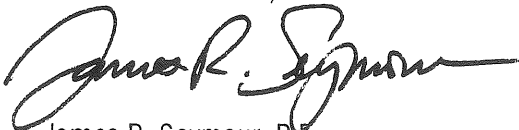
Rufus Deering Lumber Company is very anxious to start construction in early July. We are fully aware of the allocated review time, but would like to be proactive with your staff and be responsive to facilitate approvals in coordination with Portland Site Plan approval. We will be providing Architectural Plans and Site Improvements Plans to address ADA compliance issues for ramps and handicap parking. However, based on the ownerships desire to schedule for a September opening, we feel the Site Plan process must begin immediately.

In the interim, we are available at your request to hold any type of application review meeting or to meet the City's review staff to present the project on an informal basis. We feel given site design parameters that this would be beneficial for both the City Planners and the Baxter Academy design team, especially with parking and transportation needs.

If you have any questions please feel free to contact me at our office (207-200-2083). We look to working with the Planning Division Staff and City Engineers to facilitate the opening of this new opportunity for a charter school in the Downtown Portland area.

Sincerely,

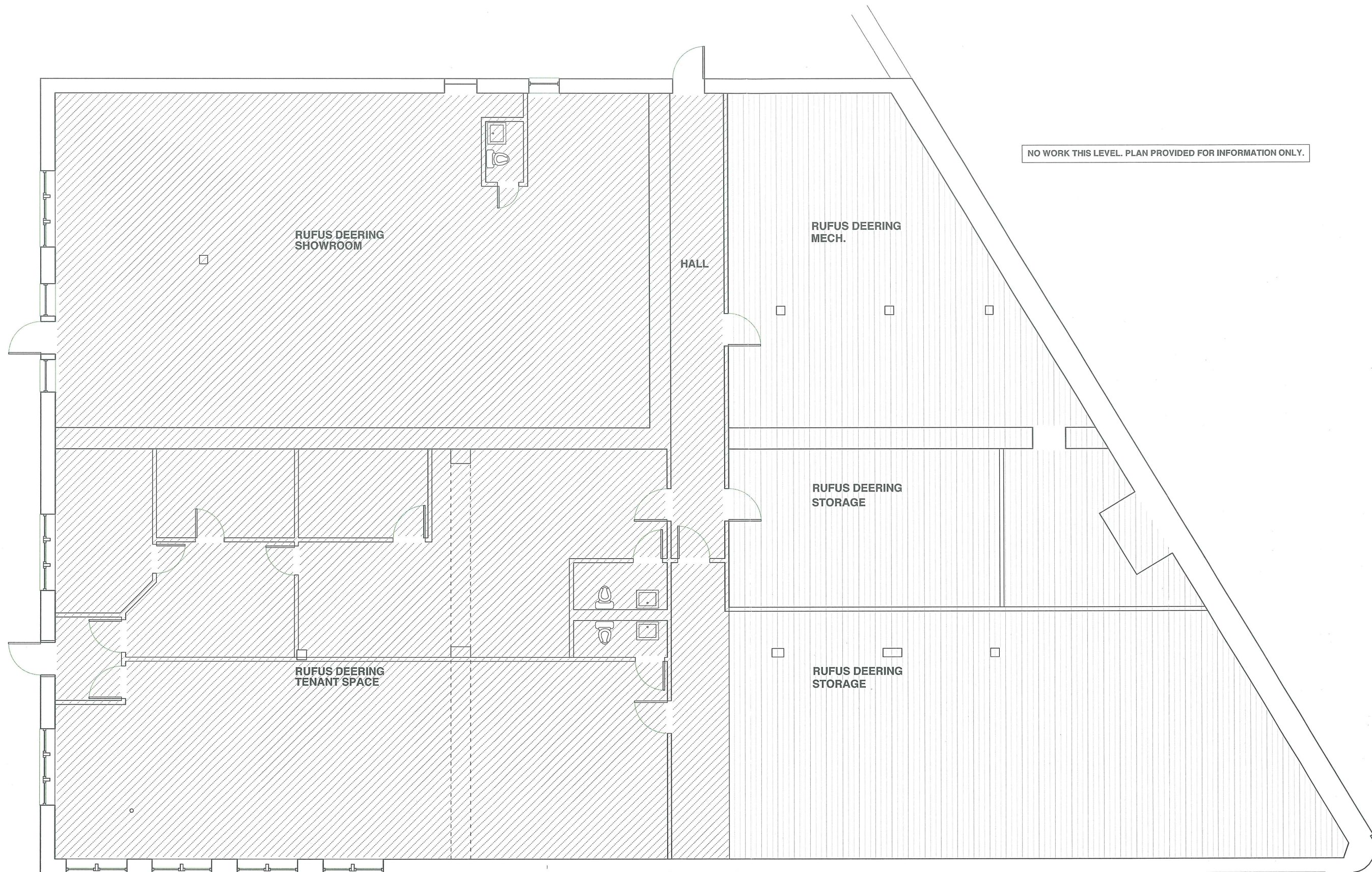
SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager

JRS:jrs/kn
Enc.

cc: Dan Labrie, President, Rufus Deering Lumber Company
John Jacques, Executive Director, Baxter Academy for Technology & Science



NO WORK THIS LEVEL. PLAN PROVIDED FOR INFORMATION ONLY.

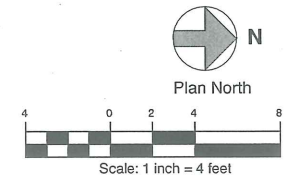
MAPLE STREET

 Unrenovated Area This Floor to Remain
 With Rufus Deering after Phase III
 Baxter Academy Renovated Area This Contract = 0 SF

 Phase III Future
 School Expansion, 4,406 Total Sf
 NIC

 Total GSF this Floor = 7,067

1 Existing Lower Level Plan
 SCALE: 1/4" = 1'-0"



Copyright 2012 Richard J Renner Architects

RRA
 Richard Renner | Architects
 Portland, ME 207.773.9699
 Sherborn, MA 508.651.2365

REGISTERED ARCHITECT
 RICHARD K. RENNER
 No. 1056
 STATE OF MAINE

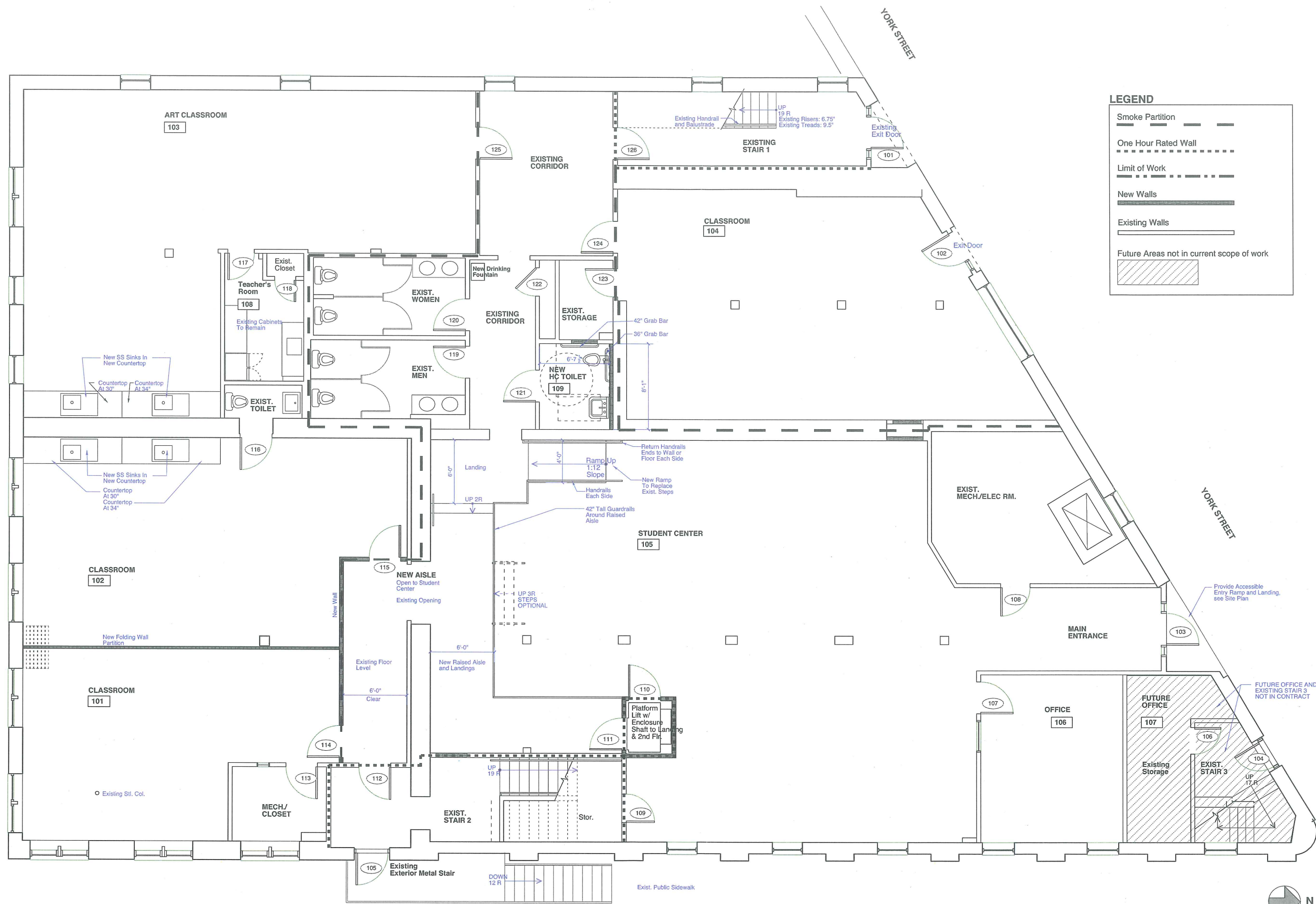
Consultants:
 Structural Engineer
 (company) Structural Engineers
 Mechanical Engineer
 (company) Energy
 Lighting Design
 company

 Landscape Architect
 company
 Energy Consultant
 company

Baxter Academy
 54 York Street
 Portland, Maine

Existing Lower Level Plan
 Drawn by: RRA
 Scale: 1/4" = 1'-0"
 Date: 06-21-2012
 File Name:
 Project No. 2012-10-00
 Revised:

Baxter Academy
A1.0



LEGEND

- Smoke Partition
- One Hour Rated Wall
- Limit of Work
- New Walls
- Existing Walls
- Future Areas not in current scope of work

1 First Floor Plan
SCALE: 1/4" = 1'-0"



Copyright 2012 Richard J. Renner Architects

RRA
Richard Renner | Architects
Portland, ME 04101
207.779.9699
Sherborn, MA 01866
508.651.2365

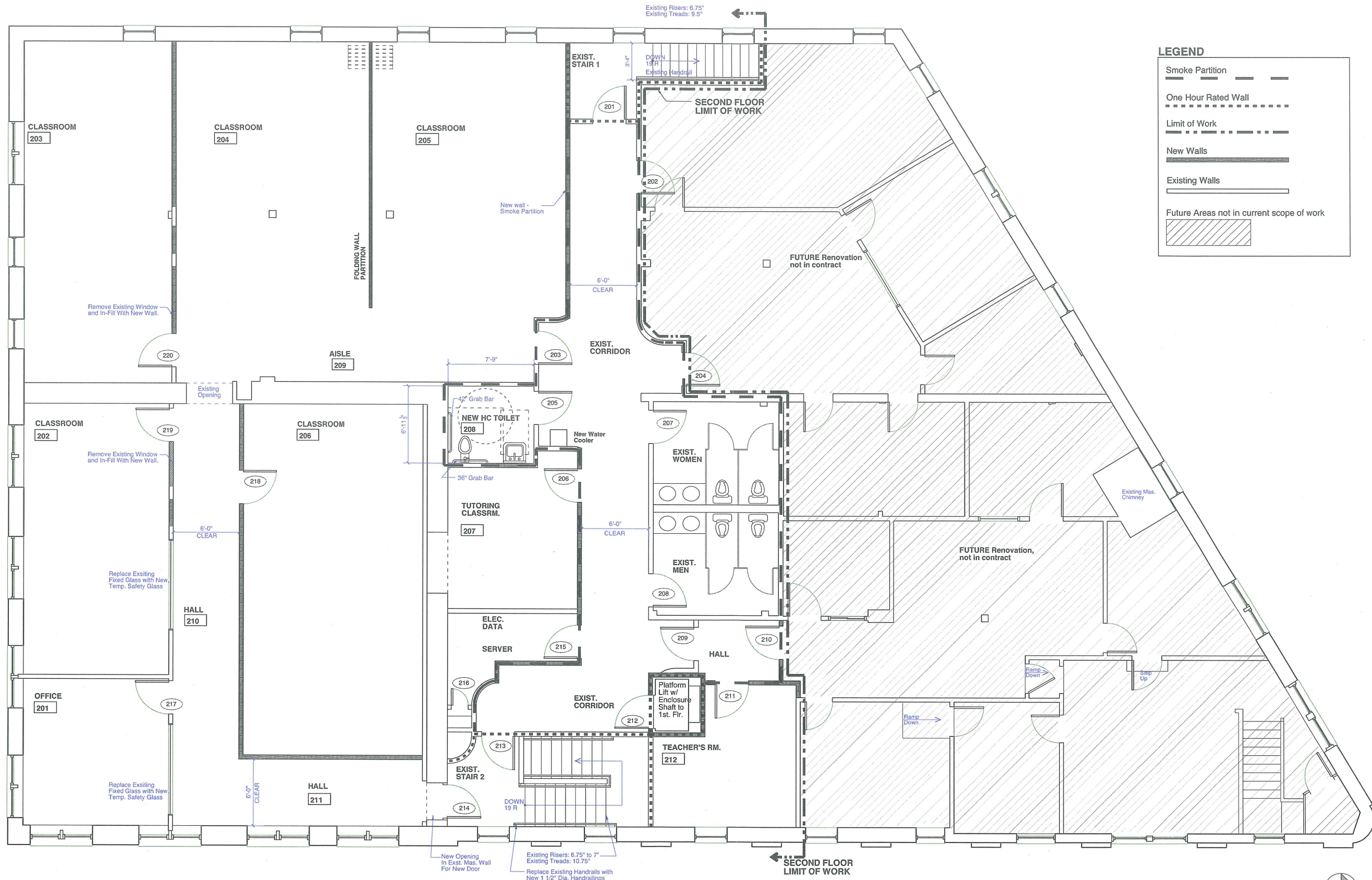
REGISTERED ARCHITECT
RICHARD K. RENNER
No. 1056
STATE OF MAINE

Baxter Academy
54 York Street
Portland, Maine

**Existing First Floor Plan
With Proposed Changes**

Drawn by: RRA File Name:
Scale: 1/4"=1'-0" Project No. 2012-10-00
Date: 06-21-2012 Revised:

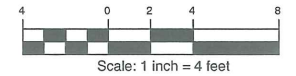
Baxter Academy
A1.1



LEGEND

- Smoke Partition
- One Hour Rated Wall
- Limit of Work
- New Walls
- Existing Walls
- Future Areas not in current scope of work

1 Second Floor Plan
SCALE: 1/4"=1'-0"



Copyright 2012 Richard I Renner Architects

RRRA
REGISTERED ARCHITECT

Richard Renner | Architects
Portland, ME 207.773.9689
Sherborn, MA 508.651.2365

RICHARD K. RENNER
No. 1056
STATE OF MAINE

Baxter Academy
54 York Street
Portland, Maine

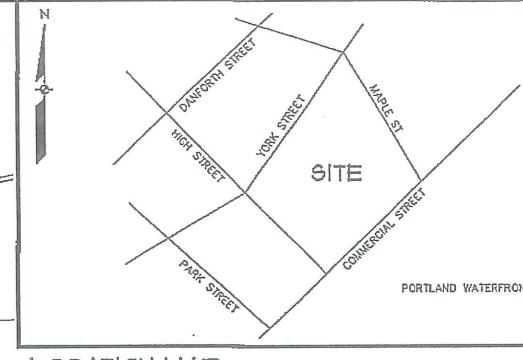
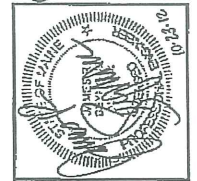
**Existing Second Floor Plan
With Proposed Changes**

Drawn by: RRA
File Name:
Scale: 1/4"=1'-0"
Project No. 2012-10-00
Date: 06-21-2012
Revised:

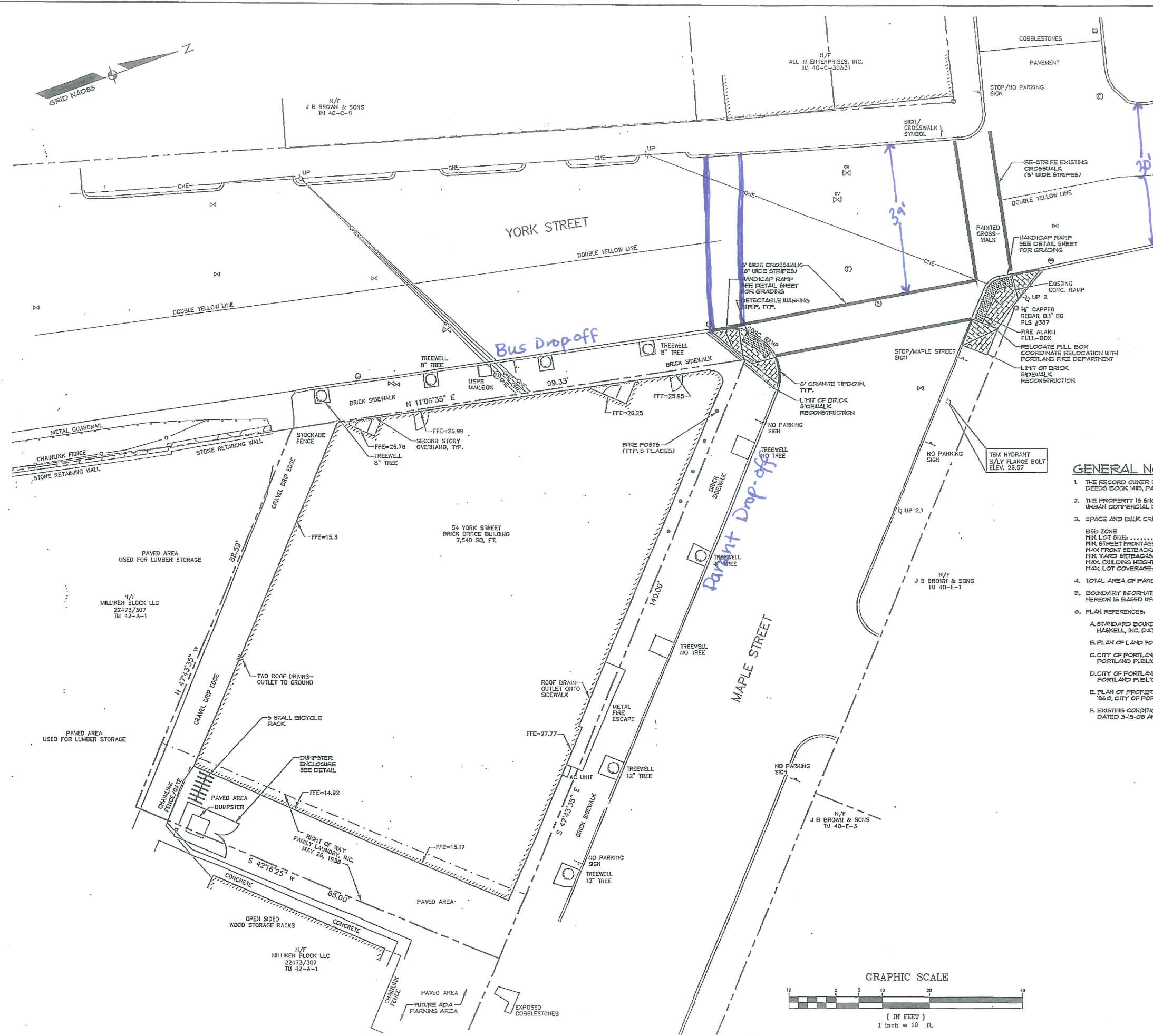
Baxter Academy

A1.2

NOT FOR CONSTRUCTION



LOCATION MAP N.T.S.



Bus Dropoff

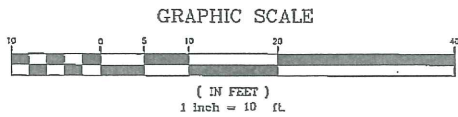
Paint Drop-off

GENERAL NOTES:

- THE RECORD OWNER IS RUFUS DEERING COMPANY AS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 1415, PAGE 13.
- THE PROPERTY IS SHOWN AS LOTS 1 AND 2 ON THE CITY OF PORTLAND TAX MAP 42, BLOCK A AND IS IN THE URBAN COMMERCIAL MIXED USE (1956) ZONE.
- SPACE AND BULK CRITERIA:
 B50 ZONE
 MIN. LOT SIZE: NONE
 MIN. STREET FRONTAGE: NONE
 MAX. FRONT SETBACK: 10 FEET
 MIN. YARD SETBACKS: NONE
 MAX. BUILDING HEIGHT: 65 FEET
 MAX. LOT COVERAGE: 100%
- TOTAL AREA OF PARCEL: 9,715 SF. (222 AC.)
- BOUNDARY INFORMATION SHOWN HEREON IS BASED ON PLAN REFERENCE 6A. TOPOGRAPHIC INFORMATION SHOWN HEREON IS BASED UPON A FIELD SURVEY COMPLETED BY SEBAGO TECHINCS, INC. DURING APRIL OF 2012.
- PLAN REFERENCES:
 A. STANDARD BOUNDARY AND EXISTING CONDITIONS SURVEY A.G.A. REALTY TRUST, PREPARED BY OWEN HASKELL, INC. DATED OCTOBER 2 1925, RECORDED IN SAID REGISTRY PLAN BOOK 155 PAGE 380.
 B. PLAN OF LAND FOR RUFUS DEERING COMPANY, PREPARED BY OWEN HASKELL, INC. DATED JANUARY 9, 1929.
 C. CITY OF PORTLAND PLAN OF CHANGE OF LINE CORNER OF HIGH STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 366/A.
 D. CITY OF PORTLAND PLAN OF HIGH STREET BETWEEN YORK STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 668/B DATED 1962.
 E. PLAN OF PROPERTY FOR CITIES SERVICE OIL CO. PREPARED BY HJ. AND E.C. JORDAN DATED FEBRUARY 9, 1960, CITY OF PORTLAND PUBLIC WORKS RECORDS 129/A.
 F. EXISTING CONDITIONS SURVEY OF RUFUS DEERING LUMBER IMPROVEMENTS FOR: RUFUS DEERING LUMBER CO. DATED 3-19-09 AND REVISED THROUGH 8-22-09 BY SEBAGO TECHINCS, INC.

LEGEND

EXISTING	DESCRIPTION	PROPOSED
---	BOUNDARY LINE/ROW	---
---	ABUTTER LINE/ROW	---
---	SETBACK	---
---	EASEMENT	---
---	MONUMENT	---
---	IRON PIPE/POD	---
---	DRILLHOLE	---
---	CURVE/LINE NO.	---
---	BENCHMARK	---
---	BUILDING	---
---	SIGN	---
---	EDGE PAVEMENT	---
---	EDGE CONCRETE	---
---	PAVEMENT PAINT	---
---	CURBLINE	---
---	CHAIN LINK FENCE	---
---	RETAINING WALL	---
---	GUARDRAIL	---
---	BOLLARD	---
---	GAS GATE VALVE	---
---	WATER GATE VALVE	---
---	HYDRANT	---
---	SEWER MH	---
---	CATCH BASIN	---
---	DRAINAGE MH	---
---	OVERHEAD UTILITY	---
---	ELECTRICAL MANHOLE	---
---	LIGHT POLE/WALL	---
---	UTILITY POLE	---
---	GUY	---



REV.	DATE	BY	STATUS
10-25-12		JRS	ISSUED FOR TRAFFIC MOVEMENT PERMIT REVIEW
07-05-12		JRS	REVISED HANDICAP PARKING LOCATION
05-05-12		JRS	SUBMITTED TO CITY FOR REVIEW

SEBAGO TECHNICAL SERVICES
 70 North Bazaar Rd., Suite 1A
 Portland, ME 04106
 TEL: 207.775.5659
 FAX: 207.775.5658
 WWW.SEBAGOTECHNICAL.COM

PROJECT NO. FIELD BOOK DESIGN CHWD DRAWN
 07893 ELEC JRS JRS JRS JRS

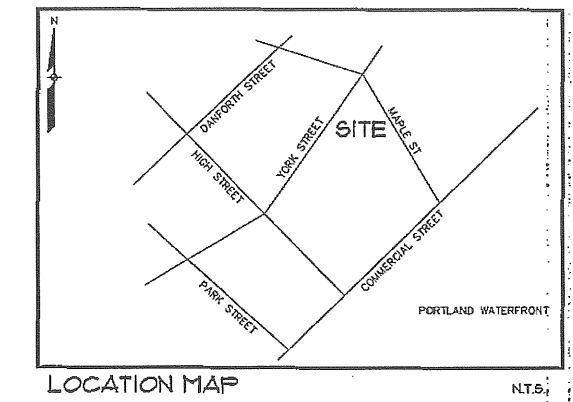
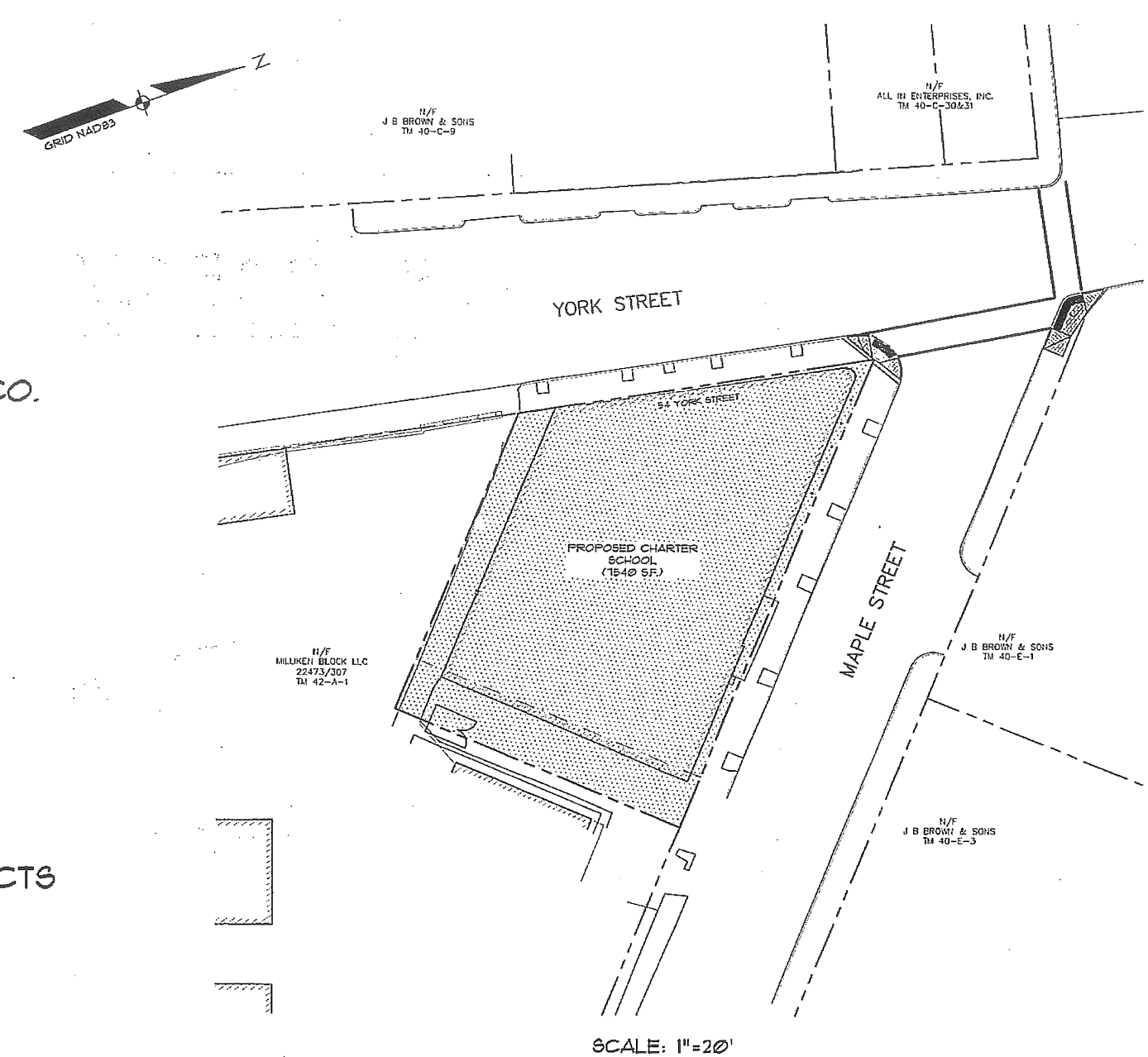
SITE PLAN
 OF:
BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE
 54 YORK STREET
 PORTLAND, MAINE
 FOR:
RUFUS DEERING LUMBER CO.
 54 YORK STREET
 PORTLAND, MAINE 04101

DATE: 05-22-12 SCALE: 1"=10'

SHEET 3 OF 4

BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE

54 YORK STREET
PORTLAND, MAINE



APPLICANT:

RUFUS DEERING LUMBER, CO.
54 YORK STREET
PORTLAND, MAINE 04101

ENGINEER / SURVEYOR:

SEBAGO
TECHNICS
CIVIL ENGINEERING - SURVEYING - LANDSCAPE ARCHITECTURE
WWW.SEBAGOTECHNICS.COM

75 John Roberts Rd. - Suite 1A South Portland, ME 04108 Tel. 207-200-2100

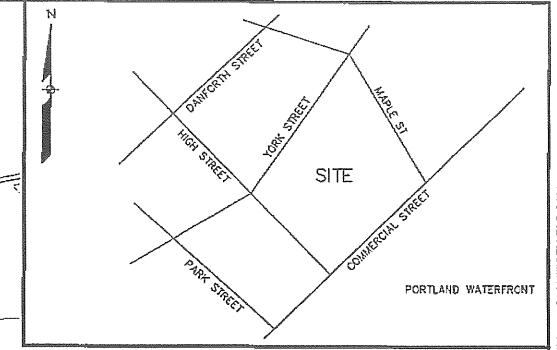
250 Goddard Rd. - Suite B Lewiston, ME 04240 Tel. 207-783-5858

ARCHITECT:

RICHARD RENNER ARCHITECTS
35 PLEASANT STREET
PORTLAND, MAINE 04101

SHEET INDEX:

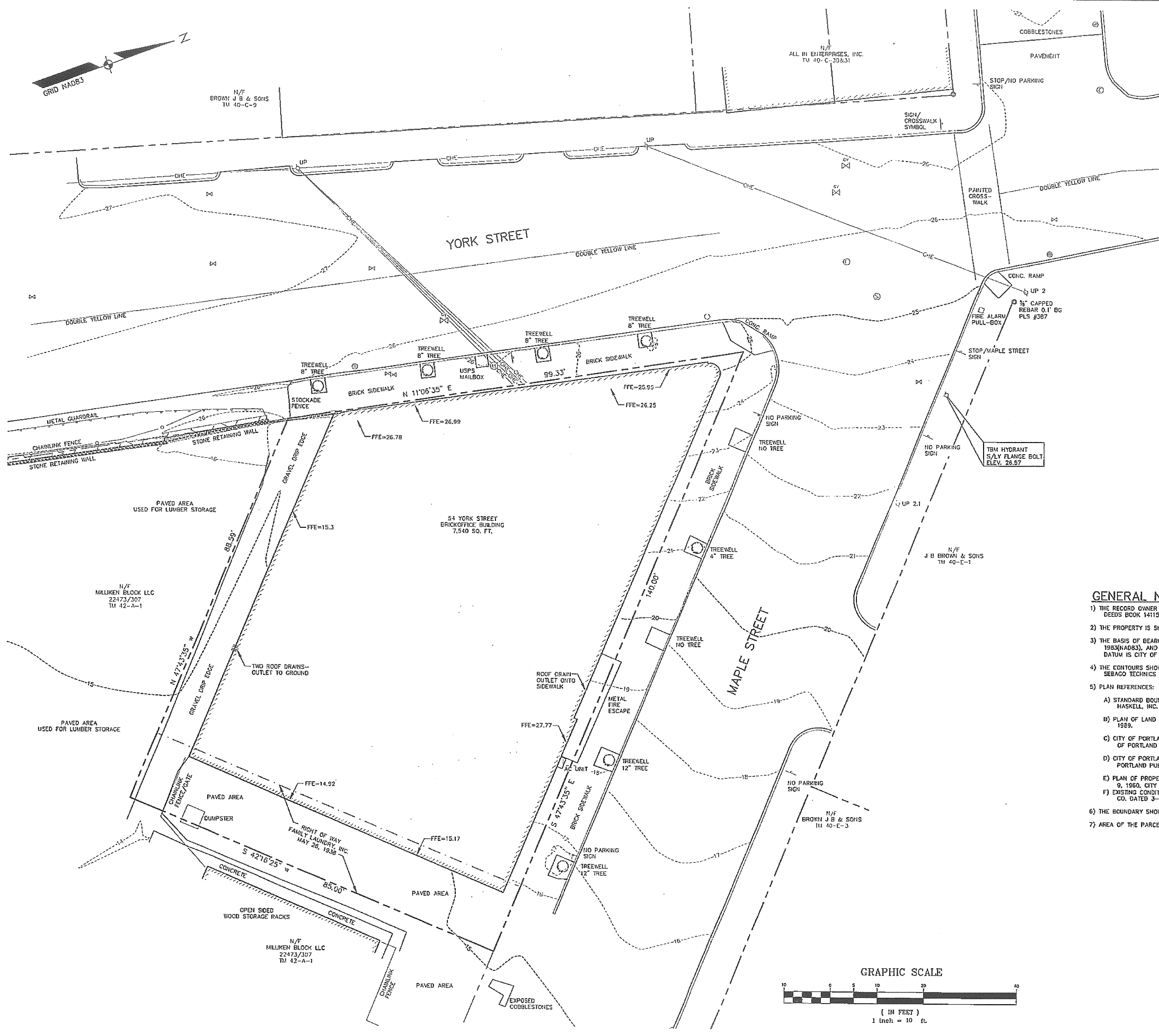
SHEET	DESCRIPTION
1	COVER SHEET
2	EXISTING CONDITIONS PLAN
3	SITE PLAN
4	DETAILS



LOCATION MAP N.T.S.

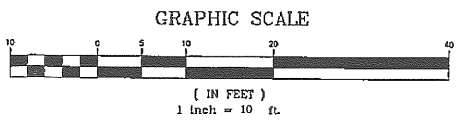
LEGEND

EXISTING	DESCRIPTION
---	BOUNDARY LINE/R.O.W.
- - -	ADJUTER LINE/R.O.W.
□	MONUMENT
○	IRON PIPE/ROD
▭	BENCHMARK
▭	BUILDING
—	SIGN
—	EDGE PAVEMENT
—	CURBLINE
-12-	CONTOURS
—	CHAIN LINK FENCE
—	STOCKADE FENCE
—	RETAINING WALL
—	GUARDRAIL
W	WATER
⊕	GATE VALVE
⊕	HYDRANT
⊕	SEWER MH
⊕	STORM DRAIN
⊕	CATCH BASIN
⊕	DRAINAGE MH
⊕	OVERHEAD ELEC. & TEL
⊕	UNDERGROUND ELEC. & TEL
⊕	ELECTRICAL METER
⊕	ELECTRICAL MANHOLE
⊕	ELECTRICAL BOX
⊕	UTILITY POLE
⊕	GUY
⊕	TELEPHONE MANHOLE
⊕	C.O.P.



GENERAL NOTES

- THE RECORD OWNER IS RUFUS DEERING COMPANY AS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 14115, PAGE 13.
- THE PROPERTY IS SHOWN AS LOTS 7 AND B ON THE CITY OF PORTLAND TAX MAP 42, BLOCK A.
- THE BASIS OF BEARINGS IS MAINE STATE PLANE GRID, MAINE WEST ZONE, NORTH AMERICAN DATUM 1983(NAD83), AND REFERENCED BY REAL TIME GLOBAL POSITIONING SYSTEM RECEIVERS. THE VERTICAL DATUM IS CITY OF PORTLAND DATUM REFERENCED TO A PK NAIL IN UTILITY POLE #CMP 32/NET 36.
- THE CONTOURS SHOWN ARE ONE FOOT CONTOUR INTERVAL, BASED UPON A FIELD SURVEY COMPLETED BY SEBAGO TECHNICS DURING APRIL OF 2012.
- PLAN REFERENCES:
 - STANDARD BOUNDARY AND EXISTING CONDITIONS SURVEY A.G.A. REALTY TRUST, PREPARED BY OWEN HASKELL, INC. DATED OCTOBER 2 1995, RECORDED IN SAID REGISTRY PLAN BOOK 195 PAGE 360.
 - PLAN OF LAND FOR RUFUS DEERING COMPANY, PREPARED BY OWEN HASKELL, INC. DATED JANUARY 9, 1989.
 - CITY OF PORTLAND PLAN OF CHANGE OF LINE CORNER OF HIGH STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 366/9.
 - CITY OF PORTLAND PLAN OF HIGH STREET BETWEEN YORK STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 668/S DATED 1969.
 - PLAN OF PROPERTY FOR CITIES SERVICE OIL CO., PREPARED BY H.L. AND E.C. JORDAN DATED FEBRUARY 9, 1960, CITY OF PORTLAND PUBLIC WORKS RECORDS 755/3.
 - EXISTING CONDITIONS SURVEY OF: RUFUS DEERING LUMBER IMPROVEMENTS FOR: RUFUS DEERING LUMBER CO. DATED 3-19-08 AND REVISED THROUGH 8-22-08 BY SEBAGO TECHNICS.
- THE BOUNDARY SHOWN IS BASED ON THE PLAN REFERENCED IN NOTE 5-A.
- AREA OF THE PARCEL IS APPROXIMATELY 9,715 SQUARE FEET.



WILLIAM C. SHIFFER PLS 2118

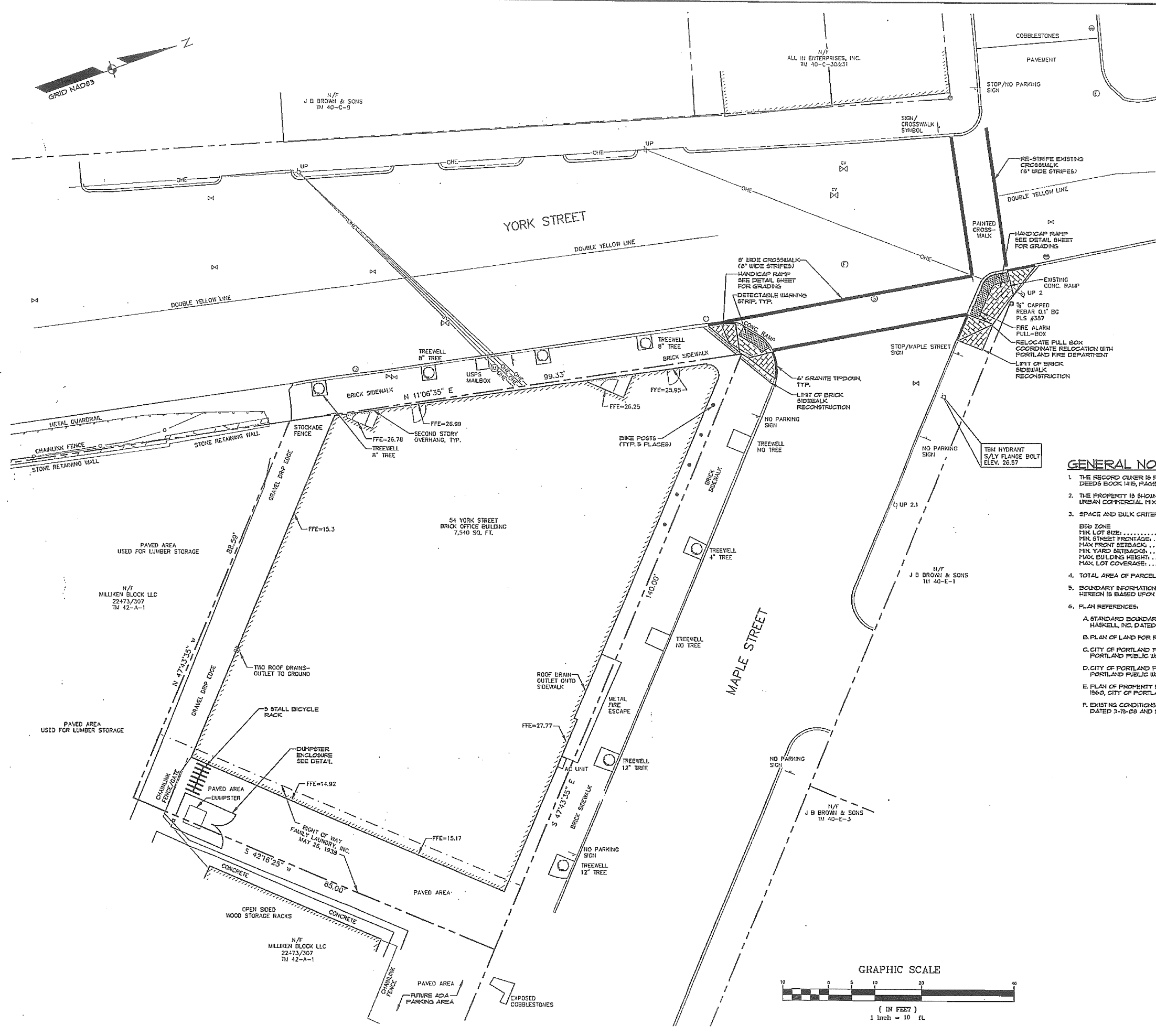
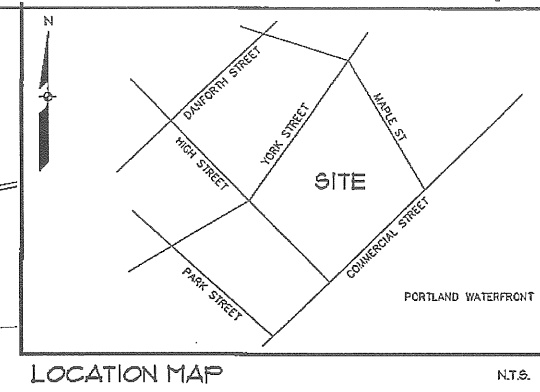
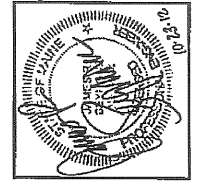
DATE	05-07-12	STATUS	PRELIMINARY
BY	WCS	DATE	05-08-12
REV	A	DATE	05-08-12
REV	B	DATE	05-08-12

THIS PLAN SHALL NOT BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT PERMISSION FROM SEBAGO TECHNICS, INC. ANY ALTERATIONS, AUTHORIZED OR UNAUTHORIZED, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC.

SEBAGO TECHNICS
 15 BAY ROAD, PORTLAND, ME 04106
 TEL: 207-753-8600 FAX: 207-753-8600
 WWW.SEBAGOTECHNICS.COM
 PROJECT NO. 07383 FIELD BOOK DESIGN CHKD DRAWN
 07383 ELEC CDMP-CLB WCS

EXISTING CONDITIONS SURVEY
 OF: BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE
 54 YORK STREET
 PORTLAND, MAINE
 FOR: RUFUS DEERING LUMBER CO.
 383 COMMERCIAL STREET
 PORTLAND, MAINE 04101

NOT FOR CONSTRUCTION

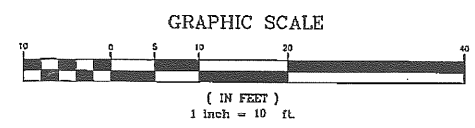


GENERAL NOTES:

- THE RECORD QUATER IS RUFUS DEERING COMPANY AS RECORDED IN THE CLIMBERLAND COUNTY REGISTRY OF DEEDS BOOK 1418, PAGE 13.
- THE PROPERTY IS SHOWN AS LOTS T AND B ON THE CITY OF PORTLAND TAX MAP 42, BLOCK A AND IS IN THE URBAN COMMERCIAL MIXED USE (B5b) ZONE.
- SPACE AND BULK CRITERIA:
B5b ZONE
MIN LOT SIZE: NONE
MIN STREET FRONTAGE: NONE
MAX FRONT SETBACK: 10 FEET
MIN YARD SETBACKS: NONE
MAX BUILDING HEIGHT: 65 FEET
MAX LOT COVERAGE: 100%
- TOTAL AREA OF PARCEL: 9,715 SF. (0.22 AC)
- BOUNDARY INFORMATION SHOWN HEREON IS BASED ON PLAN REFERENCE 6A. TOPOGRAPHIC INFORMATION SHOWN HEREON IS BASED UPON A FIELD SURVEY COMPLETED BY SEBAGO TECHINCS, INC. DURING APRIL OF 2002.
- PLAN REFERENCES:
A. STANDARD BOUNDARY AND EXISTING CONDITIONS SURVEY A&A, REALTY TRUST, PREPARED BY OWEN HASKELL, INC. DATED OCTOBER 3 1989, RECORDED IN SAID REGISTRY PLAN BOOK 155 PAGE 380.
B. PLAN OF LAND FOR RUFUS DEERING COMPANY, PREPARED BY OWEN HASKELL, INC. DATED JANUARY 9, 1989.
C. CITY OF PORTLAND PLAN OF CHANGE OF LINE CORNER OF HIGH STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 366/3.
D. CITY OF PORTLAND PLAN OF HIGH STREET BETWEEN YORK STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 660/3 DATED 1992.
E. PLAN OF PROPERTY FOR CITIES SERVICE OIL CO. PREPARED BY HJ. AND EC. JORDAN DATED FEBRUARY 9, 1960, CITY OF PORTLAND PUBLIC WORKS RECORDS 139/2.
F. EXISTING CONDITIONS SURVEY OF RUFUS DEERING LUMBER IMPROVEMENTS FOR RUFUS DEERING LUMBER CO. DATED 3-19-08 AND REVISED THROUGH 8-22-09 BY SEBAGO TECHINCS.

LEGEND

EXISTING	DESCRIPTION	PROPOSED
---	BOUNDARY LINE/ROLL	---
---	ABUTTER LINE/ROLL	---
---	SETBACK	---
---	EASEMENT	---
□	MONUMENT	---
○	IRON PIPE/ROD	---
⊙	DRILLHOLE	---
C/A	CURVE/LINE NO.	---
▬	BENCHMARK	---
▬	BUILDING	▬
▬	SIGN	▬
▬	EDGE PAVEMENT	▬
▬	EDGE CONCRETE	▬
▬	PAVEMENT PAINT	▬
▬	CURLINE	▬
▬	CHAIN LINK FENCE	▬
▬	RETAINING WALL	▬
▬	GUARDRAIL	▬
○	BOLLARD	○
⊕	GAS GATE VALVE	⊕
⊕	WATER GATE VALVE	⊕
⊕	HYDRANT	⊕
⊕	SEWER MH	⊕
⊕	CATCH BASIN	⊕
⊕	DRAINAGE MH	⊕
⊕	OVERHEAD UTILITY	⊕
⊕	ELECTRICAL MANNHOLE	⊕
⊕	LIGHT POLE/WALL	⊕
⊕	UTILITY POLE	⊕
⊕	GUY	⊕



REV.	DATE	BY	STATUS
10-23-12			ISSUED FOR TRAFFIC MOVEMENT PERMIT REVIEW
07-05-12			REVISED HANDICAP PARKING LOCATION
05-05-12			SUBMITTED TO CITY FOR REVIEW

SEBAGO TECHNICS
 WWW.SEBAGOTECHNICS.COM
 15 Park Plaza, Portland, ME 04103
 Tel: 207-783-5656
 Fax: 207-783-5657

PROJECT NO.	FIELD BOOK	DESIGN	CHKD	DRAWN
07393		JRS	JRS	JRH

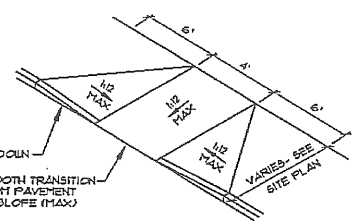
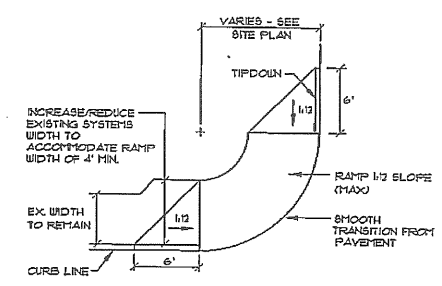
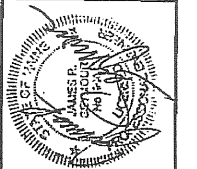
SITE PLAN
 OF:
 BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE
 54 YORK STREET
 PORTLAND, MAINE
 FOR:
 RUFUS DEERING LUMBER CO.
 54 YORK STREET
 PORTLAND, MAINE 04101

DATE	SCALE
05-22-12	1"=10'

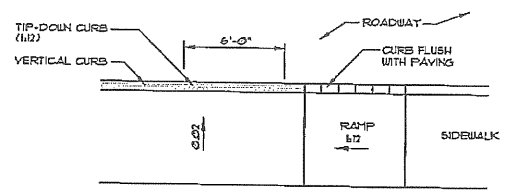
SHEET 3 OF 4

THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM SEBAGO TECHINCS, INC. ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHINCS, INC.

NOT FOR CONSTRUCTION

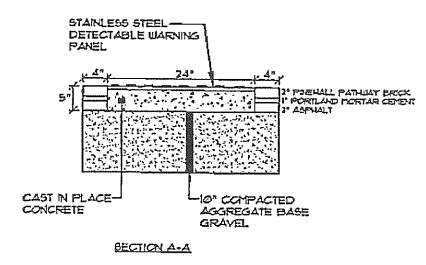
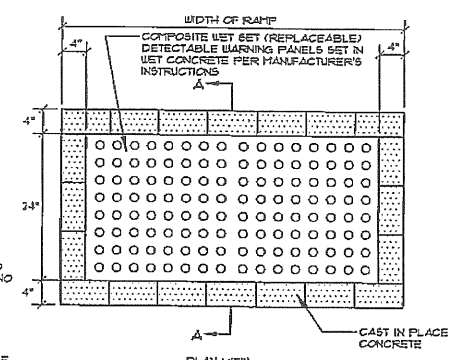


HANDICAP RAMP
NOT TO SCALE

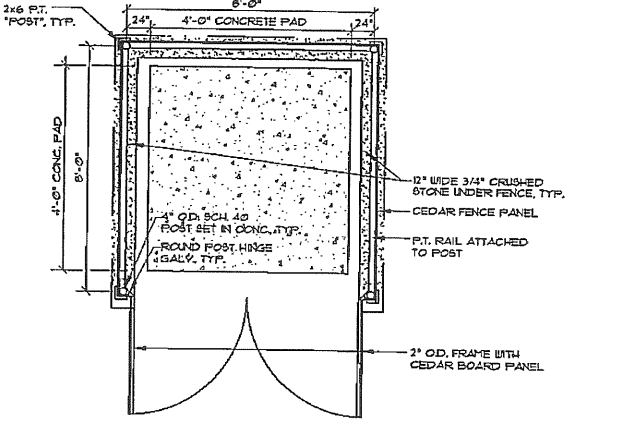
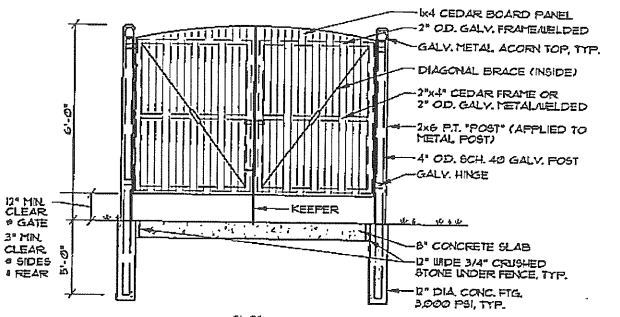


HANDICAP RAMP
NOT TO SCALE

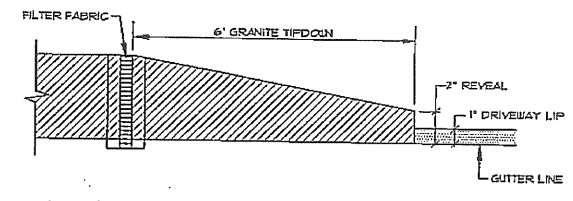
- NOTES:**
1. COMPOSITE WET SET (REPLACEABLE) DETECTABLE WARNING PANELS SHALL BE AS MANUFACTURED BY ADA SOLUTIONS, INC. (WWW.ADATILE.COM), OR APPROVED EQUAL.
 2. CAST IN PLACE CONCRETE SHALL MEET SPECIFICATIONS FOR MDOT CLASS A STRUCTURAL CONCRETE, MINIMUM COMPRESSIVE STRENGTH 4000 PSI.
 3. TRUNCATED DOMES SHALL BE ALIGNED IN ROWS, PARALLEL AND PERPENDICULAR TO THE PREDOMINANT DIRECTION OF TRAVEL. NO OTHER DETECTABLE WARNING DESIGN OR CONFIGURATION IS ALLOWED.
 4. FOR ALL DETECTABLE WARNING PANELS, WITHIN OR ADJACENT HISTORIC DISTRICTS AND HISTORIC LANDSCAPES, "DARK GRAY" COLORED (F5618) PANELS SHALL BE USED. FOLLOW MANUFACTURER'S INSTRUCTIONS FOR INSTALLATION.
 5. THE DETECTABLE WARNING PANEL SHALL HAVE ONE FULL COURSE OF PINEHALL PATHWAY PAVERS (THE CURRENT BRICK STANDARD) AROUND THE FULL PERIMETER OF THE PANEL. THE PERIMETER COURSE SHALL BE SET USING PORTLAND MORTAR CEMENT TO CREATE A FLUSH SURFACE BETWEEN THE BRICK AND THE PANEL.
 6. SIZE THE DETECTABLE WARNING PANEL(S) SHALL EXTEND 24 INCHES MINIMUM IN THE DIRECTION OF TRAVEL AND THE FULL WIDTH OF THE CURB RAMP, LANDING, OR BLENDED TRANSITION TO THE STREET.
 7. ORIENTATION: THE DETECTABLE WARNING PANEL SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6 INCHES MINIMUM AND 8 INCHES MAXIMUM FROM THE CURB LINE. THE PANEL SHALL BE ORIENTED TO THE DIRECTION OF TRAVEL AS IDENTIFIED BY THE POINT OF EGRESS.



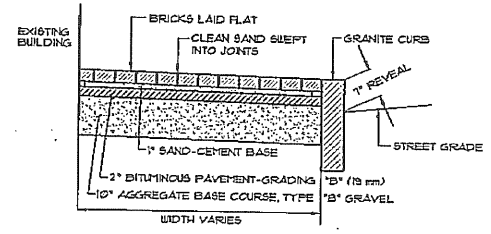
SIDEWALK RAMP DETECTABLE WARNING PANEL
NOT TO SCALE



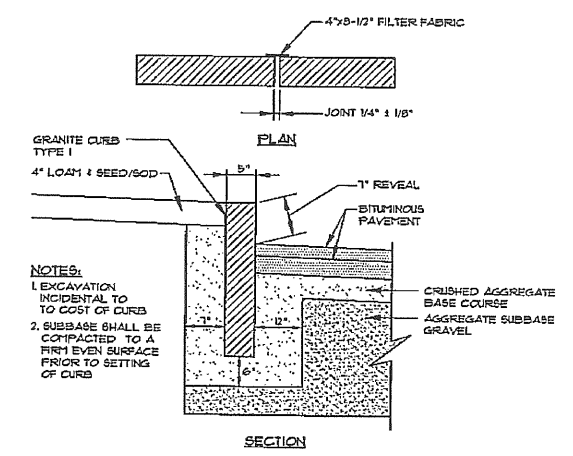
TYPICAL DUMPSTER ENCLOSURE
NOT TO SCALE



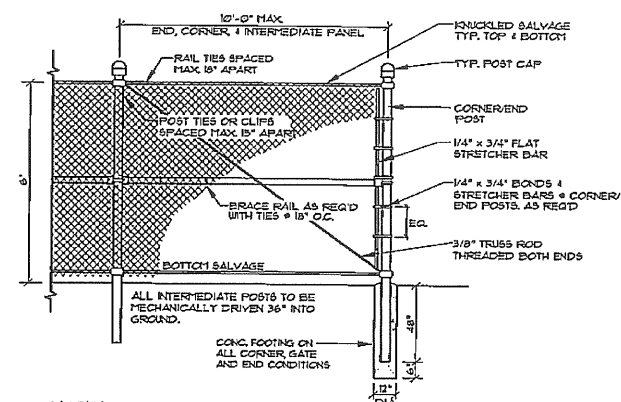
TYPICAL TIPDOWN CURB INSTALLATION
NOT TO SCALE



BRICK SIDEWALK DETAIL
NOT TO SCALE



VERTICAL GRANITE CURB
NOT TO SCALE



- NOTES:**
1. CORNER OR END POST: NOMINAL 3" O.D. GALVANIZED STEEL PIPE, MIN. 3/8" DIA. A.F.
 2. INTERMEDIATE POST: NOMINAL 2-1/2" O.D. GALVANIZED STEEL PIPE, MIN. 3/8" DIA. A.F.
 3. BRACES (TOP & BOTTOM): NOMINAL 1-5/8" O.D. GALVANIZED STEEL PIPE, MIN. 27" DIA. A.F.
 4. THE OUTSIDE OF THE FENCE FABRIC SHALL BE 3" INSIDE THE EDGE OF PAVEMENT.
 5. BRACE RAIL AND DIAGONAL BRACE ROD SHALL BE INSTALLED AT EACH 10' CORNER SECTION OF ENCLOSURE. CONCRETE SHALL HAVE MINIMUM COMPRESSIVE STRENGTH (f'c) OF 3000 psi WITH 6#4 AIR ENTRAINMENT.
 6. SUEBIT SHOP DRAWINGS FOR OWNER'S/ENGINEER'S APPROVAL. FENCE FABRIC, POST, RAILS AND APPURTENANCES SHALL BE VERT. CLAD COLOR: BLACK.

TYPICAL CHAIN LINK FENCE
NOT TO SCALE

REV:	A	DATE:	05-25-12	STATUS:	SUBMITTED TO CITY FOR REVIEW
BY:	JRS	DATE:		STATUS:	

THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM SEBAGO TECHNICALS, INC. ANY ALTERATIONS, AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICALS, INC.

SEBAGO TECHNICALS
WWW.SEBAGOTECHNICALS.COM
25 John Road, Box 84, Unit 8
South Portland, ME 04106
Tel: 207-763-2400

PROJECT NO. 07353D
FIELD BOOK DESIGN CHKO DRAWN JRS
ELEC JRS

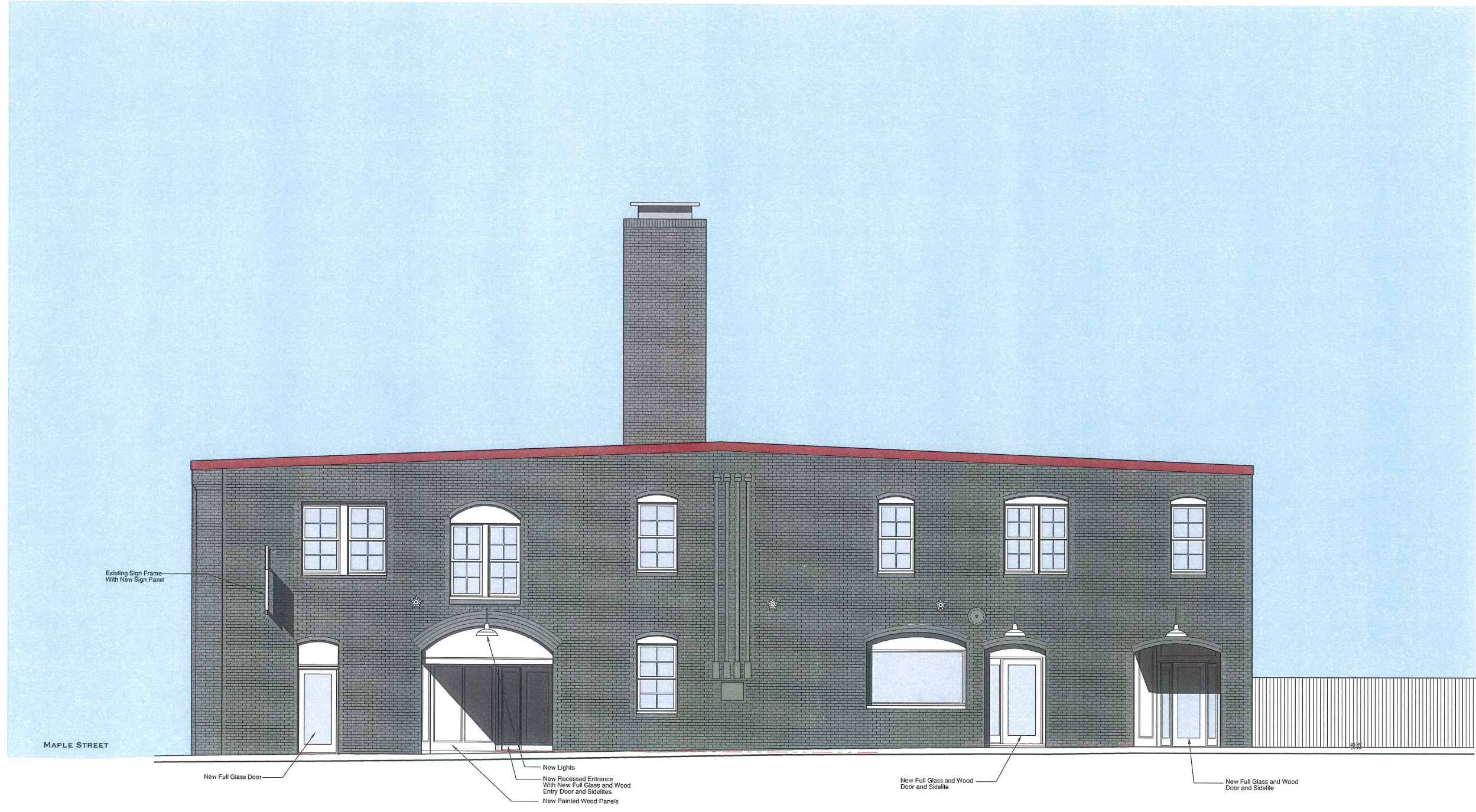
DETAILS OF:
BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE
54 YORK STREET
PORTLAND, MAINE

FOR:
RUFUS DEERING LUMBER CO.
54 YORK STREET
PORTLAND, MAINE 04101

TAB: D

DATE	SCALE
05-22-12	AS NOTED

Copyright 2012 Richard J Renner Architects
RR/A
Richard Renner | Architects
Portland, ME
207.773.9699
Sherborn, MA
508.651.2385

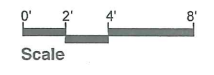


Baxter Academy
54 York Street
Portland, Maine

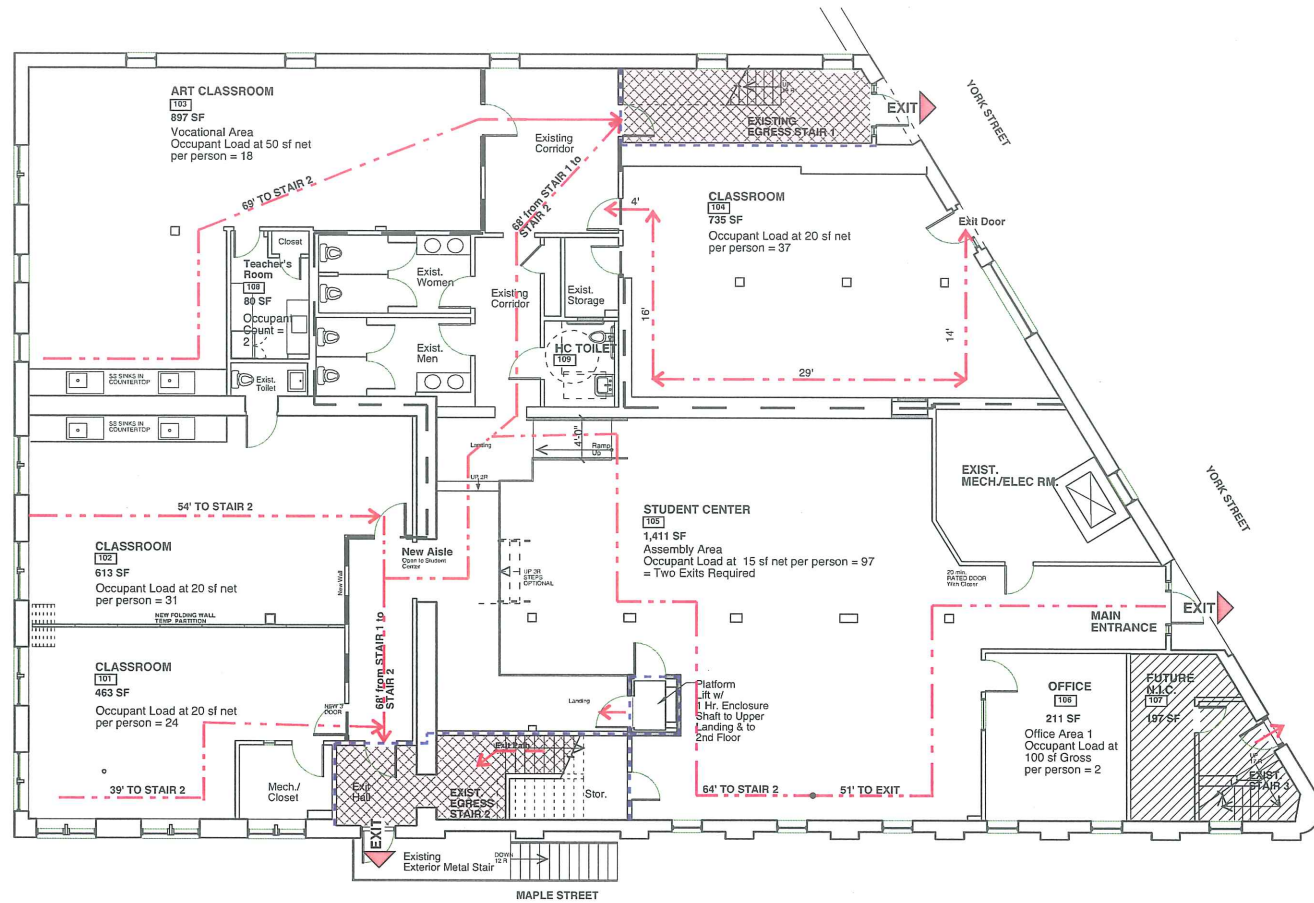
York Street Elevation
Drawn by: RRA
File Name:
Scale: 1/4"=1'-0"
Project No. 2012-10.00
Date: 11-12-2012
Revised:

54 YORK STREET ELEVATION

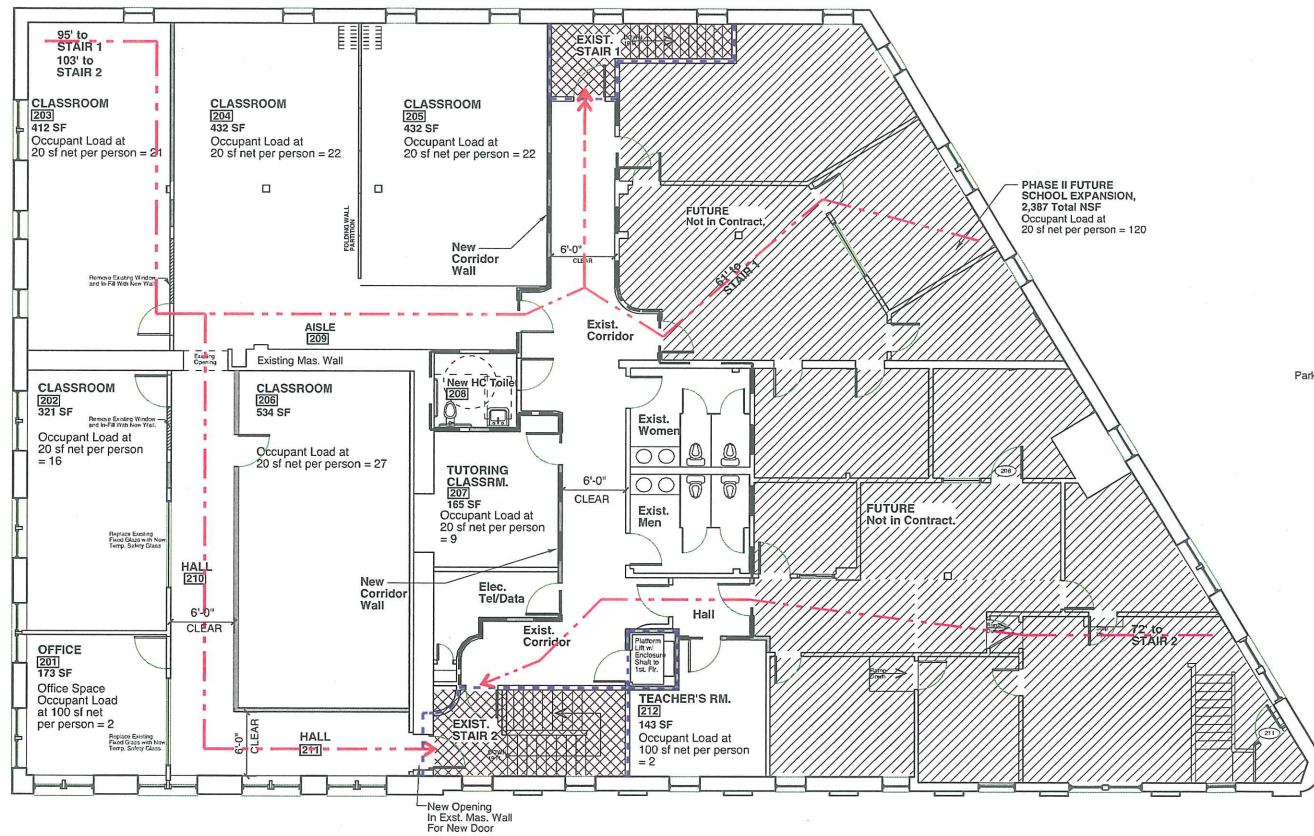
Baxter Academy
Portland, Maine



Baxter Academy

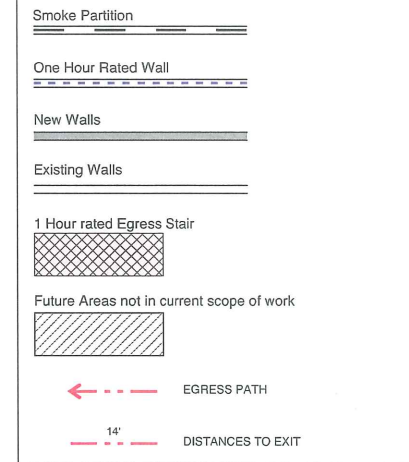


1 First Floor Plan
SCALE: 1/8" = 1'-0"

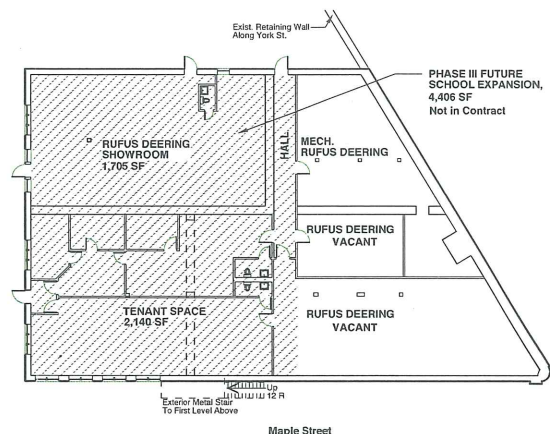


2 Second Floor Plan
SCALE: 1/8" = 1'-0"

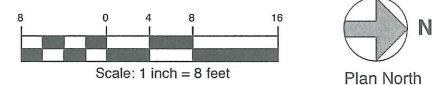
LEGEND



- TYPICAL DOOR AND HARDWARE NOTES:
- All Interior Doors Into Egress Stairs To Be Min. (1) Hr Rated With Panic Type Exit Devices and Closers.
 - All Doors from Egress Stairs to Exterior to Have Panic Type Exit Devices and Closers.
 - All Doors To Be 3'-0" Wide, Unless Otherwise Noted.
 - All Door Glazing and Sidelight Glazing to be 1/4" Tempered Safety Glass.
 - Door to Existing Mech. room To Be Min. 20 Min Rated Door With Closer.
 - All Door Locksets And Passages Sets To Be Lever Handle Type.



1 Basement Floor Plan No Work This Level
SCALE: 1" = 2'-0"



Note: All Sq. Ft. Numbers shown are NET unless otherwise noted.

Building Code Analysis (IBC 2009 and NFPA 101 2009)						
Current Use and Occupancy Classification	Business Group B (304.1), and (6.1.11)					
Proposed Use and Occupancy Classification	Educational Group E (NFPA 6.1.3)					
Existing Building Rehabilitation:	NFPA 43.1.2.2, and 43.7.1					
Construction Type	Type III B Exterior walls are of non-combustible material and interior building elements are of any material permitted by code					
Automatic Sprinkler System	The building is currently fully sprinklered (monitored system)					
1. Existing Structure Information						
Lower Level	Building Area: 7,067 sq ft; Renovation Area: 0 sq ft					
First Floor	7,067 sq ft; 7,067 sq ft					
Second Floor	7,067 sq ft; 4,032 sq ft					
Totals	21,201 sq ft; 11,099 sq ft					
Number of Stories above Grade	3					
Building height	approx. 35 ft					
2. Allowable Area (Table 503)						
Allowable area per floor	23,500 sq ft					
Allowable height	65'					
Allowable number of stories	2					
3. Height and Area Modification with Automatic Sprinkler System (504.2)						
Allowable height increase	1 Story, 20 ft; 2+1 = 3 Stories					
Total allowable height with modifications	3 Stories, 85 ft					
THE BUILDING HEIGHT AND AREA ARE WITHIN THE PERMITTED LIMITS						
4. Occupant Load (NFPA 101 Table 7.3.1.2, & IBC Table 1004.1.1)						
Floor	Occupied Spaces, Area NET Sq. Ft., Occupancy Load, Actual Design Occupancy, Use Classification					
Phase I Not Including Future Areas	Classroom 101	463	24	21	Educational Classroom space = 20 net/person	
	Classroom 102	613	31	21	-	
	Classroom 103	897	45	21	-	
	Art Classroom 104	735	18	16	Educational vocational space = 50 net/person	
	Student Center 105	1,457	97	-	Assembly space = 15 net/person	
	Office 106	211	2	2	Business Office space = 100 net/person	
	Future Office 107	197	2	2	Business Office space = 100 net/person	
	Teacher's Room 108	80	2	-	Occupant Load = 2	
	TOTAL First Floor	4,653	213	83	-	
	Phase II	Office 201	173	2	2	Business Office space = 100 net/person
		Classroom 202	321	16	19	Educational Classroom space = 20 net/person
		Classroom 203	412	21	21	-
Classroom 204		432	22	21	-	
Classroom 205		432	22	21	-	
Classroom 206		534	27	21	-	
Tutoring Room 208		165	9	2	-	
Office 212		168	2	2	Business Office space = 100 net/person	
TOTAL Phase I & II - First & Second Flr.	7,598	334	192	-		
TOTAL Phase I & II - First & Second	9,985	458	337	-		
5. Minimum Fire Resistance Requirements of Building Elements (IBC Tables 601 & 602)						
Structural Frame	0 hour	Table 601				
Load bearing exterior walls	0 hours	Table 602				
Load bearing interior walls	0 hour	Table 601				
Non-load bearing exterior walls (fire separation Greater than 30 ft.)	0 hour	Table 602				
Non-load bearing exterior walls (fire separation distance greater than 30 ft.)	0 hours	Table 602				
Floor construction	0 hour	Table 601				
Roof construction	0 hour	Table 601				
Shaft enclosures	1 hour	Section 708.4				
Exit enclosures	1 hour	Section 1022.1				
Corridors	0 hours	Table 1018.1 with automatic sprinkler system				
Corridors per NFPA 101 15.3.6	0 hours	IF Smoke Partitions provided Per NFPA 101, 8.4.3.5				
Mixed Occupancy - No Separation	0 hour	NFPA 101-6.1.14.2.2				
Accessory Occup. - Main Occup.	0 hour	IBC 508.2.4				
6. Means of Egress Components						
Exit access Travel Distance with automatic sprinkler system	200 ft. per NFPA 101, 15.2.6.3					
Minimum Number of exits required per floor	(2) (Table 1021.1)					
Number of exits provided per floor	(2)					
Spaces with (1) means of Egress allowed when Occ. load less than 50 in Group E	IBC Table 1015.1, NFPA 15.2.5.4					
Common path of travel in Group E fully sprinklered buildings.	75 ft (IBC1014.3), NFPA 15.2.5.3.1					
Dead End Limit in Group E fully sprinklered buildings.	50 ft (IBC 1018), NFPA 15.2.5.2					
Required Corridor Width = 8'-0"	Educational Occupancy NFPA 101, 15.2.3.2					
Min. Corridor Width Provided = 6'-0"						
Accessible means of egress are Not required in alterations to EXISTING buildings	IBC Section 1007.1 and NFPA 3411.6					
7. Minimum Plumbing Fixtures						
REQUIRED FIXTURES	WC fixtures/person, Urinal fixtures/male, Lav fixtures/person, DF/person					
Educational Occupancy	1 per 50, 67% max., 1 per 50, 1 per 100					
Occupancy Count Based On Actual Number Of Students And Staff Planned For Final Capacity.						
required fixtures	Male: 169, Female: 169, Occupancy Count: 338					
provided fixtures includes all floors	Male: 4, Female: 4, Urinal: 5, Lav: 5, DF: 2					

Copyright 2012 Richard J. Renner Architects

RRA
Richard Renner | Architects
Portland, ME
207.773.9689
508.651.2885

REGISTERED ARCHITECT
RICHARD K. RENNER
No. 1056
STATE OF MAINE

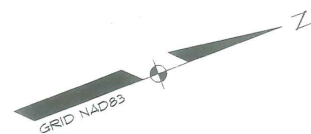
Baxter Academy
54 York Street
Portland, Maine

Code Summary
File Name: Project No. 2012-10-00
Drawn by: RRA
Scale: As Shown
Date: 06-21-2012
Revised:

CS

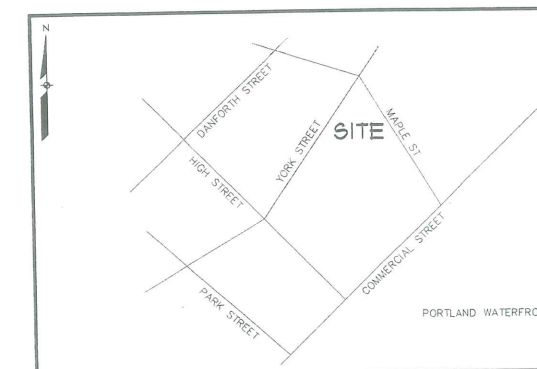
BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE

54 YORK STREET
PORTLAND, MAINE



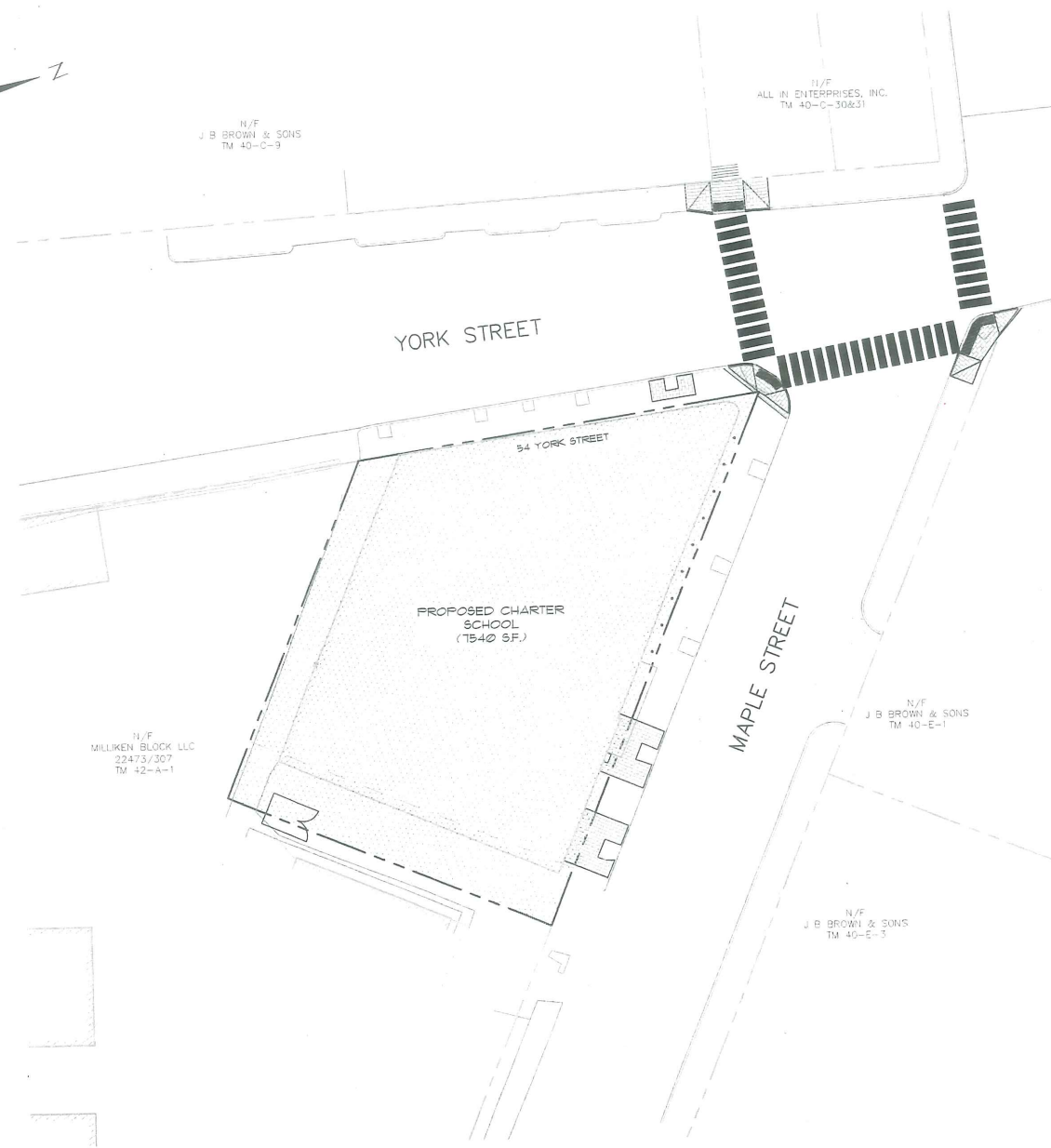
N/F
J B BROWN & SONS
TM 40-C-9

N/F
ALL IN ENTERPRISES, INC.
TM 40-C-30&31



LOCATION MAP

N.T.S.



SCALE: 1"=20'

APPLICANT:

RUFUS DEERING LUMBER, CO.
54 YORK STREET
PORTLAND, MAINE 04101

ENGINEER / SURVEYOR:

SEBAGO
TECHNICS

CIVIL ENGINEERING - SURVEYING - LANDSCAPE ARCHITECTURE

WWW.SEBAGOTECHNICS.COM

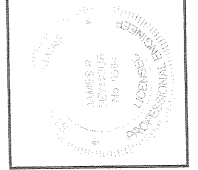
75 John Roberts Rd. - Suite 1A 250 Goddard Rd. - Suite B
South Portland, ME 04106 Lewiston, ME 04240
Tel. 207-200-2100 Tel. 207-783-5656

ARCHITECT:

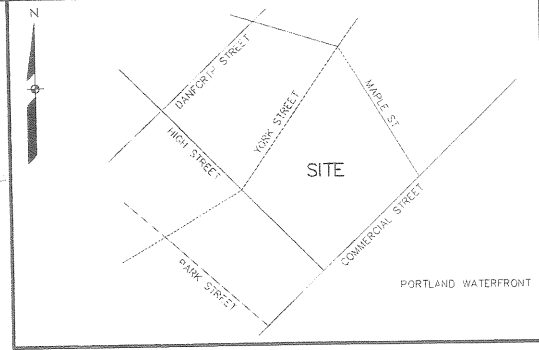
RICHARD RENNER ARCHITECTS
35 PLEASANT STREET
PORTLAND, MAINE 04101

SHEET INDEX:

SHEET	DESCRIPTION
1	COVER SHEET
2	EXISTING CONDITIONS PLAN
3	SITE PLAN
4	DETAILS



WILLIAM C. SHIPPEN PLS 2118
N.T.S.



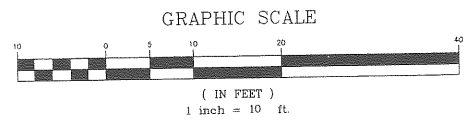
LOCATION MAP N.T.S.

LEGEND

EXISTING	DESCRIPTION
---	BOUNDARY LINE/R.O.W.
- - - -	ABUTTER LINE/R.O.W.
⊕	MONUMENT
⊙	IRON PIPE/ROD
□	BENCHMARK
▭	BUILDING
—	SIGN
—	EDGE PAVEMENT
—	CURBLINE
—	CONTOURS
—	CHAIN LINK FENCE
—	STOCKADE FENCE
—	RETAINING WALL
—	GUARDRAIL
—	WATER
—	GATE VALVE
—	HYDRANT
—	SEWER MH
—	STORM DRAIN
—	CATCH BASIN
—	DRAINAGE MH
—	OVERHEAD ELEC. & TEL
—	UNDERGROUND ELEC. & TEL
—	ELECTRICAL METER
—	ELECTRICAL MANHOLE
—	ELECTRICAL BOX
—	UTILITY POLE
—	GUY
—	TELEPHONE MANHOLE
—	C.O.P.

GENERAL NOTES

- THE RECORD OWNER IS RUFUS DEERING COMPANY AS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 1415, PAGE 13.
- THE PROPERTY IS SHOWN AS LOTS 7 AND 8 ON THE CITY OF PORTLAND TAX MAP 42, BLOCK A.
- THE BASIS OF BEARINGS IS MAINE STATE PLANE GRID, MAINE WEST ZONE, NORTH AMERICAN DATUM 1983 (NAD83), AND REFERENCED BY REAL TIME GLOBAL POSITIONING SYSTEM RECEIVERS. THE VERTICAL DATUM IS CITY OF PORTLAND DATUM REFERENCED TO A PK NAIL IN UTILITY POLE #CMP 32/WET 36.
- THE CONTOURS SHOWN ARE ONE FOOT CONTOUR INTERVAL, BASED UPON A FIELD SURVEY COMPLETED BY SEBAGO TECHNICS DURING APRIL OF 2012.
- PLAN REFERENCES:
 - STANDARD BOUNDARY AND EXISTING CONDITIONS SURVEY A.C.A. REALTY TRUST, PREPARED BY OWEN HASKELL, INC. DATED OCTOBER 2 1995, RECORDED IN SAID REGISTRY PLAN BOOK 195 PAGE 380.
 - PLAN OF LAND FOR RUFUS DEERING COMPANY, PREPARED BY OWEN HASKELL, INC. DATED JANUARY 9, 1989.
 - CITY OF PORTLAND PLAN OF CHANGE OF LINE CORNER OF HIGH STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 366/9.
 - CITY OF PORTLAND PLAN OF HIGH STREET BETWEEN YORK STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 668/5 DATED 1989.
 - PLAN OF PROPERTY FOR CITIES SERVICE OIL CO., PREPARED BY H.I. AND E.C. JORDAN DATED FEBRUARY 8, 1969, CITY OF PORTLAND PUBLIC WORKS RECORDS 755/3.
 - EXISTING CONDITIONS SURVEY OF RUFUS DEERING LUMBER IMPROVEMENTS FOR: RUFUS DEERING LUMBER CO. DATED 3-19-08 AND REVISED THROUGH 8-22-08 BY SEBAGO TECHNICS
 - THE BOUNDARY SHOWN IS BASED ON THE PLAN REFERENCED IN NOTE 5-A.
 - AREA OF THE PARCEL IS APPROXIMATELY 9,715 SQUARE FEET.

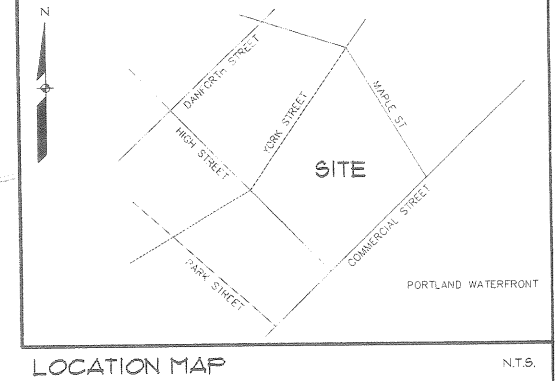
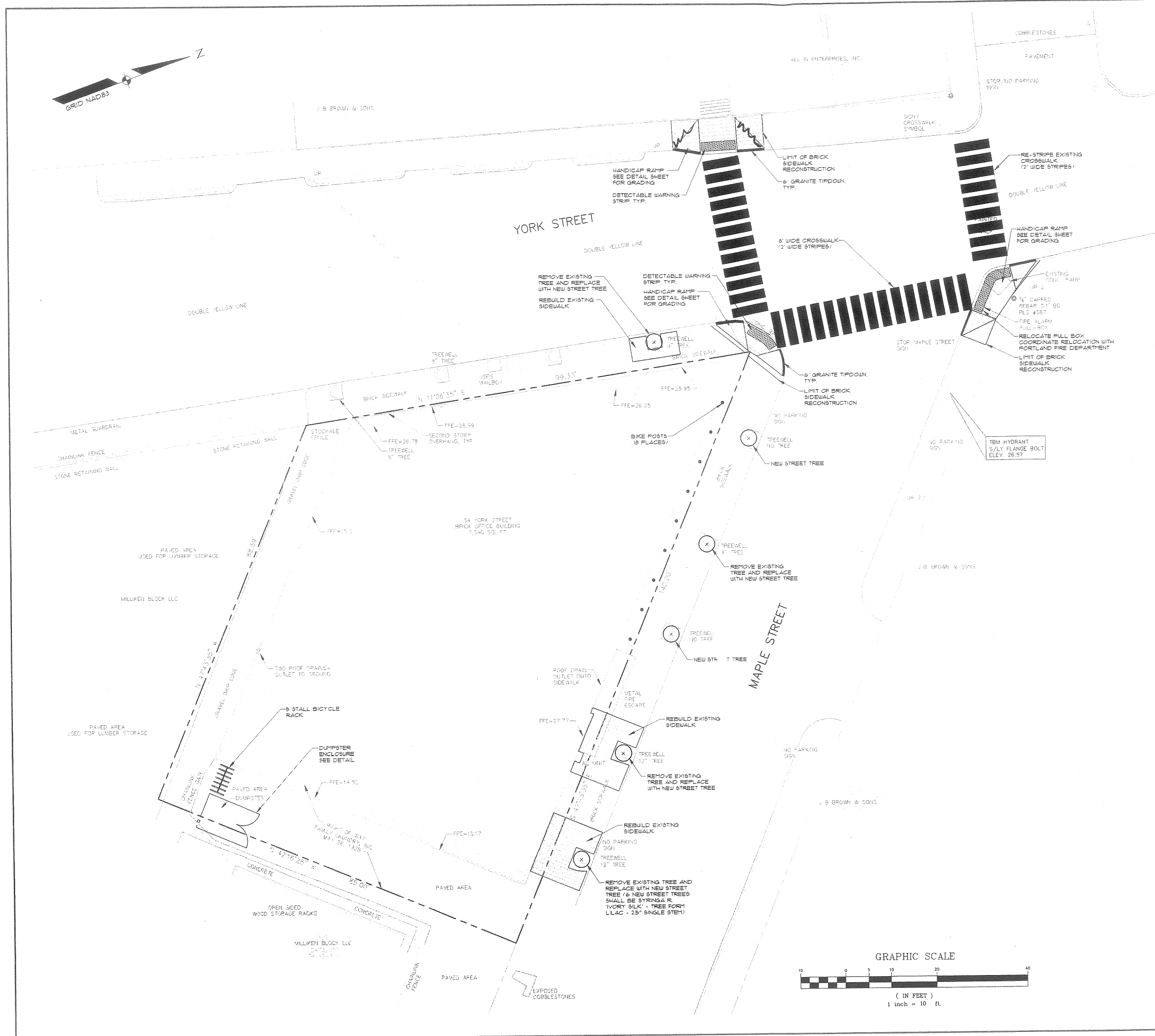


SEBAGO
TECHNICS
WWW.SEBAGOTECHNICS.COM
75 John Paul Road, Suite B
South Portland, ME 04106
Tel: 207-763-5656
Fax: 207-763-5658

PROJECT NO.	07383
FIELD BOOK	ELEC
DESIGN	CHKD
CDM-CLB	WCS

EXISTING CONDITIONS SURVEY
OF
BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE
54 YORK STREET
PORTLAND, MAINE
FOR:
RUFUS DEERING LUMBER CO.
383 COMMERCIAL STREET
PORTLAND, MAINE 04101

DATE	SCALE
05-07-12	1"=10'

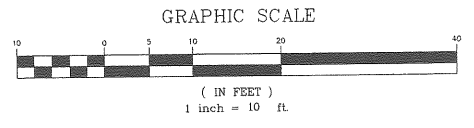


GENERAL NOTES:

- THE RECORD OWNER IS RUFUS DEERING COMPANY AS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 1415, PAGE 13.
- THE PROPERTY IS SHOWN AS LOTS 1 AND 8 ON THE CITY OF PORTLAND TAX MAP 42, BLOCK A AND 8 IN THE URBAN COMMERCIAL MIXED USE (B8b) ZONE.
- SPACE AND BULK CRITERIA:
 B8b ZONE
 MIN. LOT SIZE: NONE
 MIN. STREET FRONTAGE: NONE
 MAX. FRONT SETBACK: 10 FEET
 MIN. YARD SETBACKS: NONE
 MAX. BUILDING HEIGHT: 65 FEET
 MAX. LOT COVERAGE: 100%
- TOTAL AREA OF PARCEL: 9,715 SF. (0.22 AC.)
- BOUNDARY INFORMATION SHOWN HEREON IS BASED ON PLAN REFERENCE 6A TOPOGRAPHIC INFORMATION SHOWN HEREON IS BASED UPON A FIELD SURVEY COMPLETED BY SEBAGO TECHNICS, INC. DURING APRIL OF 2012.
- PLAN REFERENCES:
 A. STANDARD BOUNDARY AND EXISTING CONDITIONS SURVEY A.G.A. REALTY TRUST, PREPARED BY OWEN HASKELL, INC. DATED OCTOBER 7, 1999. RECORDED IN SAID REGISTRY PLAN BOOK 185 PAGE 390.
 B. PLAN OF LAND FOR RUFUS DEERING COMPANY, PREPARED BY OWEN HASKELL, INC. DATED JANUARY 9, 1999.
 C. CITY OF PORTLAND PLAN OF CHANGE OF LINE CORNER OF HIGH STREET AND COMMERCIAL STREET CITY OF PORTLAND PUBLIC WORKS RECORDS 366/3.
 D. CITY OF PORTLAND PLAN OF HIGH STREET BETWEEN YORK STREET AND COMMERCIAL STREET CITY OF PORTLAND PUBLIC WORKS RECORDS 668/5 DATED 1989.
 E. PLAN OF PROPERTY FOR CITIES SERVICE OIL CO. PREPARED BY H.I. AND E.C. JORDAN DATED FEBRUARY 9, 1960. CITY OF PORTLAND PUBLIC WORKS RECORDS 189/3.
 F. EXISTING CONDITIONS SURVEY OF RUFUS DEERING LUMBER IMPROVEMENTS FOR RUFUS DEERING LUMBER CO. DATED 3-19-09 AND REVISED THROUGH 9-22-09 BY SEBAGO TECHNICS.
 G. THE OWNER WILL BE RESPONSIBLE FOR REMOVAL OF SNOW IMMEDIATELY FOLLOWING A SNOW EVENT, BY MEANS OF CONTRACTED REMOVAL AS THERE IS NO AVAILABLE SNOW STORAGE ON SITE.
 H. ALL STREET TREES PROPOSED OR FOR REPLACEMENT, SHALL BE COORDINATED WITH THE CITY ARBORIST FOR SPECIE SELECTION.

LEGEND

EXISTING	DESCRIPTION	PROPOSED
---	BOUNDARY LINE/ROW	---
---	ABUTTER LINE/ROW	---
---	SETBACK	---
---	EASEMENT	---
---	MONUMENT	---
---	IRON PIPE/ROD	---
---	DRILLHOLE	---
---	CURVE/LINE NO.	---
---	BENCHMARK	---
---	BUILDING	---
---	SIGN	---
---	EDGE PAVEMENT	---
---	EDGE CONCRETE	---
---	PAVEMENT PAINT	---
---	CURELINE	---
---	CHAIN LINK FENCE	---
---	RETAINING WALL	---
---	GUARDRAIL	---
---	BOLLARD	---
---	GAS GATE VALVE	---
---	WATER GATE VALVE	---
---	HYDRANT	---
---	SEWER MH	---
---	CATCH BASIN	---
---	DRAINAGE MH	---
---	OVERHEAD UTILITY	---
---	ELECTRICAL MANHOLE	---
---	LIGHT POLE/WALL	---
---	UTILITY POLE	---
---	GUY	---



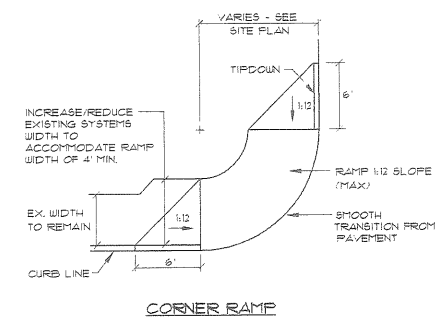
DATE	BY	STATUS
01-10-13	JRS	FINAL PLAN SUBMISSION
12-04-12	JRS	FINAL SITE PLAN REVIEW
10-23-12	JRS	ISSUED FOR TRAFFIC MOVEMENT PERMIT REVIEW
07-03-12	JRS	REVISED HANDICAP PARKING LOCATION
06-05-12	JRS	SUBMITTED TO CITY FOR REVIEW

SEBAGO
 TECHNICAL SERVICES
 75 John Bowers Rd., Suite B
 Lewiston, ME 04240
 Tel: 207-200-2100
 Fax: 207-783-8656

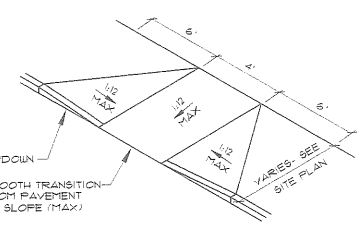
PROJECT NO. 07383
 FIELD BOOK
 DESIGN CHKD
 DRAWN
 JRS
 JRS
 JRS
 JRS

SITE PLAN OF
 BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE
 54 YORK STREET
 PORTLAND, MAINE
 FOR:
 RUFUS DEERING LUMBER CO.
 54 YORK STREET
 PORTLAND, MAINE 04101

DATE	SCALE
05-22-12	1"=10'

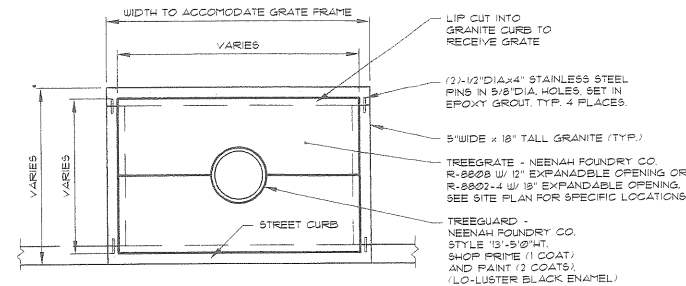


CORNER RAMP



STRAIGHT RAMP

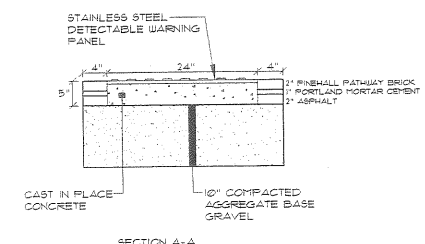
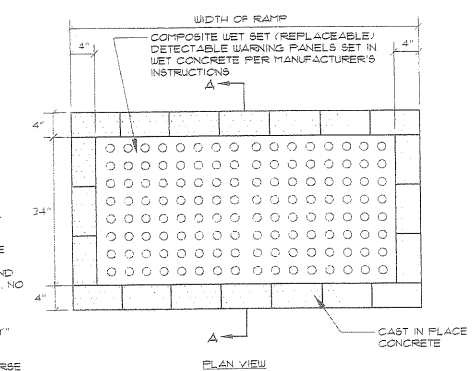
HANDICAP RAMP
NOT TO SCALE



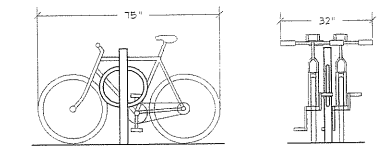
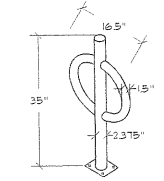
TREE GRATE DETAIL
NOT TO SCALE

NOTES:

1. COMPOSITE SET SET (REPLACEABLE/ DETECTABLE WARNING PANELS SHALL BE AS MANUFACTURED BY ADA SOLUTIONS, INC. (WWW.ADATILE.COM) OR APPROVED EQUAL.
2. CAST IN PLACE CONCRETE SHALL MEET SPECIFICATIONS FOR "DOT CLASS A STRUCTURAL CONCRETE, MINIMUM COMPRESSIVE STRENGTH 4000 PSI.
3. TRUNCATED CONES SHALL BE ALIGNED IN ROWS, PARALLEL AND PERPENDICULAR TO THE PREDOMINANT DIRECTION OF TRAVEL. NO OTHER DETECTABLE WARNING DESIGN OR CONFIGURATION IS ALLOWED.
4. FOR ALL DETECTABLE WARNING PANELS WITHIN OR ADJUTING HISTORIC DISTRICTS AND HISTORIC LANDSCAPES, "DARK GRAY" COLORED (9619) PANELS SHALL BE USED. FOLLOW MANUFACTURER'S INSTRUCTIONS FOR INSTALLATION.
5. THE DETECTABLE WARNING PANEL SHALL HAVE ONE FULL COURSE OF FINISHALL PATHWAY PAVERS (THE CURRENT BRICK STANDARD) AROUND THE FULL PERIMETER OF THE PANEL. THE PERIMETER COURSE SHALL BE SET USING PORTLAND MORTAR CEMENT TO CREATE A FLUSH SURFACE BETWEEN THE BRICK AND THE PANEL.
6. SIZE: THE DETECTABLE WARNING PANEL(S) SHALL EXTEND 24 INCHES MINIMUM IN THE DIRECTION OF TRAVEL AND THE FULL WIDTH OF THE CURB RAMP, LANDING, OR BLENDED TRANSITION TO THE STREET.
7. ORIENTATION: THE DETECTABLE WARNING PANEL SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6 INCHES MINIMUM AND 8 INCHES MAXIMUM FROM THE CURB LINE. THE PANEL SHALL BE ORIENTED TO THE DIRECTION OF TRAVEL AS IDENTIFIED BY THE POINT OF EGRESS.

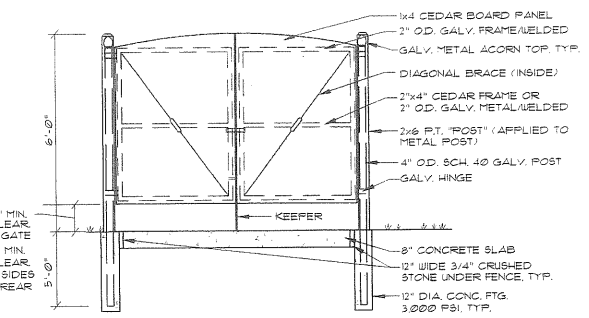


SIDEWALK RAMP DETECTABLE WARNING PANEL
NOT TO SCALE

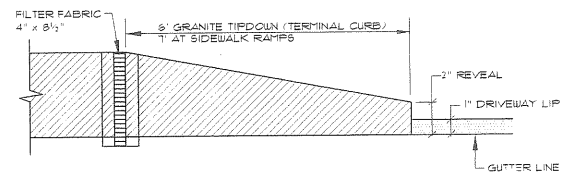


BIKE HITCH BICYCLE RACK
NOT TO SCALE

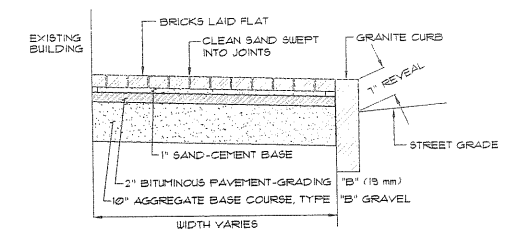
- NOTES:
1. BICYCLE RACK AS MANUFACTURED BY DERO BIKE RACKS OR CITY OF PORTLAND APPROVED EQUAL.



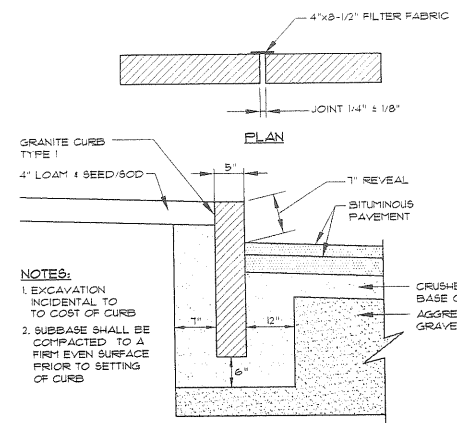
TYPICAL DUMPSTER ENCLOSURE
NOT TO SCALE



TYPICAL TIPDOWN CURB INSTALLATION
NOT TO SCALE



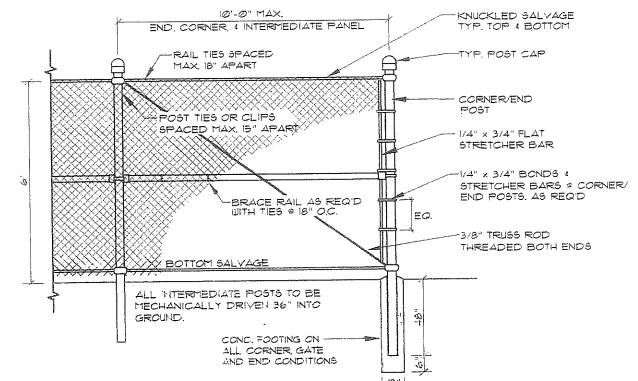
BRICK SIDEWALK DETAIL
NOT TO SCALE



VERTICAL GRANITE CURB
NOT TO SCALE

NOTES:

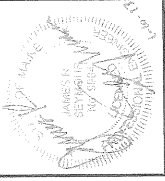
1. EXCAVATION INCIDENTAL TO COST OF CURB
2. SUBBASE SHALL BE COMPACTED TO A FIRM EVEN SURFACE PRIOR TO SETTING OF CURB



NOTES:

1. CORNER OR END POST: NOMINAL 3\"/>
- 2. INTERMEDIATE POST: NOMINAL 2-1/2\"/>
- 3. BRACES (TOP & BOTTOM): NOMINAL 1-5/8\"/>
- 4. THE OUTSIDE OF THE FENCE FABRIC SHALL BE 3\"/>
- 5. BRACE RAIL AND DIAGONAL BRACE ROD SHALL BE INSTALLED AT EACH 10\"/>
- 6. CONCRETE SHALL HAVE MINIMUM COMPRESSIVE STRENGTH (f'c) OF 3000 PSI WITH 5% AIR ENTRAINMENT.
- 7. SUBMIT SHOP DRAWINGS FOR OWNER'S/ENGINEER'S APPROVAL.
- 8. FENCE FABRIC, POST, RAILS AND APPURTENANCES SHALL BE VINYL CLAD; COLOR: BLACK.

TYPICAL CHAIN LINK FENCE
NOT TO SCALE



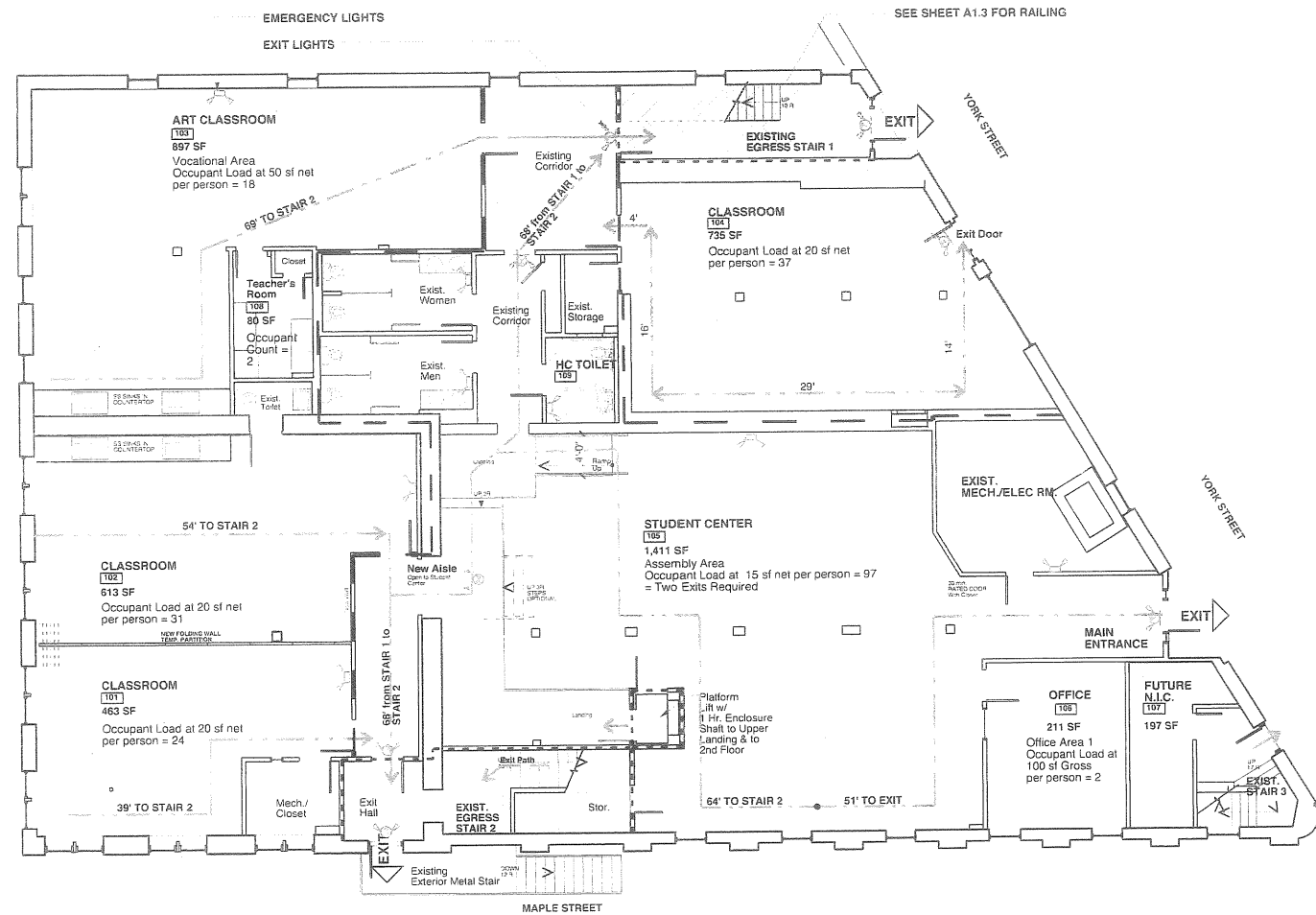
DATE	SCALE
05-22-12	AS NOTED

REV.	BY	DATE	STATUS
C	JRS	01-10-13	FINAL PLAN SUBMISSION
B	JRS	12-04-12	FINAL SITE PLAN REVIEW
A	JRS	06-05-12	SUBMITTED TO CITY FOR REVIEW

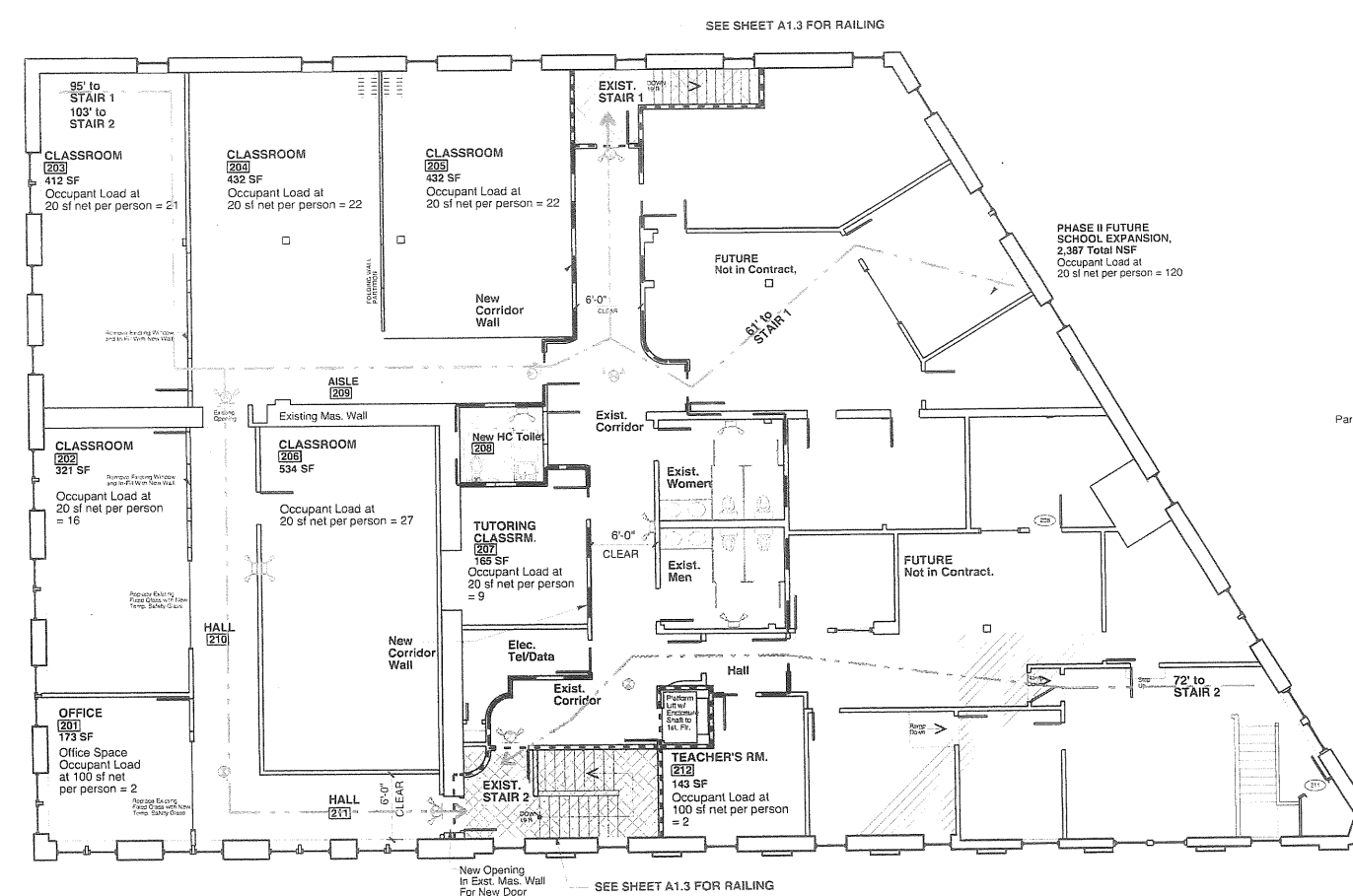
SEBAGO
TECHNICAL
WWW.SEAGOTECHNICALS.COM
25 Judd Street, Suite B
Portland, ME 04108
Tel: 207-783-9595

DETAILS OF:
BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE
64 YORK STREET
PORTLAND, MAINE

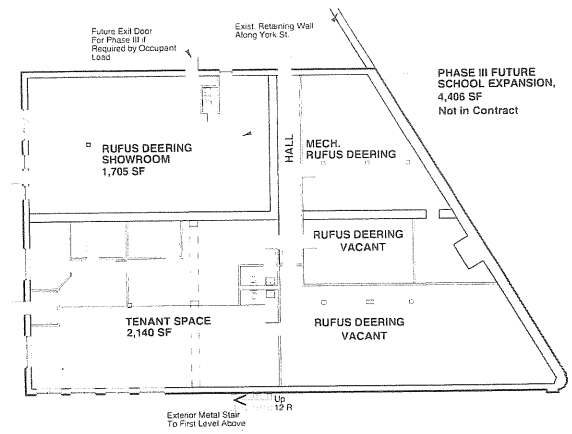
FOR:
RUFUS DEERING LUMBER CO.
64 YORK STREET
PORTLAND, MAINE 04101



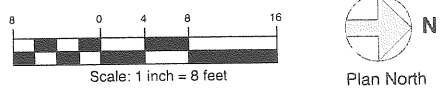
1 First Floor Plan
SCALE: 1/8" = 1'-0"



2 Second Floor Plan
SCALE: 1/8" = 1'-0"



1 Basement Floor Plan No Work This Level
SCALE: 1" = 20'



Note: All Sq. Ft. Numbers shown are NET unless otherwise noted.

LEGEND

Smoke Partition

One Hour Rated Wall

New Walls

Existing Walls

1 Hour rated Egress Stair

Future Areas not in current scope of work

EGRESS PATH

DISTANCES TO EXIT

EXIT SIGNS

EMERGENCY LIGHTS

EXIT/EMERGENCY COMBINATION LIGHTS

TYPICAL DOOR AND HARDWARE NOTES:

- All Interior Doors into Egress Stairs To Be Min. 1 Hr Rated With Panic Type Exit Devices and Closers.
- All Doors from Egress Stairs to Exterior to Have 1/2 Hr Type Exit Devices and Closers.
- All Doors To Be 3'-0" Wide, Unless Otherwise Noted.
- All Door Glazing and Sidelight Glazing to be 1/4" Tempered Safety Glass.
- Door To Existing Mech. room To Be Min. 20 Min Rated Door With Closer.
- All Door Locksets And Passages Sets To Be Lever Handle Type.

SEE SHEET A1.3 FOR COMPLETE DOOR SCHEDULE AND HARDWARE.

Building Code Analysis (IBC 2009 and NFPA 101 2009)

Current Use and Occupancy Classification: Business Group B (304.1), and (6.1.15)

Proposed Use and Occupancy Classification: MIXED OCCUPANCY - Educational Group E (NFPA 6.1.14.3) / Business, NON-separated

Existing Building Rehabilitation: NFPA 43.1.2.2, and 43.7.1

Construction Type: Type III B Exterior walls are of non-combustible material and interior building elements are of any material permitted by code

Automatic Sprinkler System: The building is currently fully sprinklered (monitored system)

1. Existing Structure Information

	Building Area	Renovation Area
Lower Level	7,067 sq ft	0 sq ft
First Floor	7,067 sq ft	7,067 sq ft
Second Floor	7,067 sq ft	4,032 sq ft
Totals	21,201 sq ft	11,099 sq ft

Number of Stories above Grade: 3
Building height: approx. 35 ft

2. Allowable Area (Table 503)

Allowable area per floor	23,500 sq ft
Allowable height	85'
Allowable number of stories	2

3. Height and Area Modification with Automatic Sprinkler System (504.2)

Allowable height increase	1 Story, 20 ft	2+1 = 3 Stories
Total allowable height with modifications	3 Stories, 85 ft	

THE BUILDING HEIGHT AND AREA ARE WITHIN THE PERMITTED LIMITS

4. Occupant Load (NFPA 101 Table 7.2.1.2, & IBC Table 1004.1.1)

Floor	Occupied Spaces	Area NET Sq. Ft.	Occupancy Load	Actual Design Occupancy	Use Classification
Phase I (Not including Future Areas)	Classroom 101	463	24	21	Educational Classroom space = 20/person
	Classroom 102	613	31	21	
	Classroom 103	897	45	21	
	An Classroom 104	735	18	16	Educational vocational space = 50/person
	Student Center 105	1,457	97	---	Assembly space = 15/person
	Office 106	211	2	2	Business Office space = 100/person
	Future Office 107	197	2	2	Business Office space = 100/person
	Teacher's Room 108	80	2	---	Occupant Load = 2
	TOTAL First Floor	4,653	213	83	
	Phase II	Office 201	173	2	2
Classroom 202		321	16	19	Educational Classroom space = 20/person
Classroom 203		412	21	21	
Classroom 204		432	22	21	
Classroom 205		432	22	21	
Classroom 206		534	27	21	
Tutoring Room 208		165	9	2	Business Office space = 100/person
Office 212	168	2	2		
TOTAL Phase I Second Floor	2,637	121	109		
Phase II Future Areas	2,367	120	145	Educational Classroom space = 20/person	
TOTAL Second Floor Phase I & II	5,024	241	254		
TOTAL Phase I Only - First & Second Flr.	7,598	334	192		
TOTAL Phase I & II - First & Second	9,985	458	337		

5. Minimum Fire Resistance Requirements of Building Elements (IBC Tables 601 & 602)

Structural Frame	0 hour	Table 601
Load bearing exterior walls	0 hours	Table 602
Load bearing interior walls	0 hour	Table 601
Non-load bearing exterior walls (fire separation Greater than 30 ft.)	0 hour	Table 602
Non-load bearing exterior walls (fire separation distance greater than 30 ft.)	0 hours	Table 602
Floor construction	0 hour	Table 601
Roof construction	0 hour	Table 601
Shaft enclosures	1 hours	Section 708.4
Exit enclosures	1 hours	Section 1022.1
Corridors	0 hours	Table 1018.1 with automatic sprinkler system
Corridors per NFPA 101 15.3.6	1/2 hours	
	0 hours	IF Smoke Partitions provided Per NFPA 101, 8.4.3.5

Mixed Occupancy - No Separation
Accessory Occup. - Main Occup. 0 hour
NFPA 101-6.1.14.3
IBC 509.2.4

6. Means of Egress Components

Exit access Travel Distance with automatic sprinkler system	200 ft. per NFPA 101, 15.2.6.3
Minimum Number of exits required per floor	(2) (Table 1021.1)
Number of exits provided per floor	(2)
Spaces with (1) means of Egress allowed when Occ. load less than 50 in Group E	IBC Table 1015.1, NFPA 15.2.5.4
Common path of travel in Group E fully sprinklered buildings.	75 ft. (IBC1014.3), NFPA 15.2.5.3.1
Dead End Limit in Group E fully sprinklered buildings.	50 ft. (IBC 1018), NFPA 15.2.5.2
Required Corridor Width = 6'-0"	Educational Occupancy NFPA 101, 15.2.3.2
Mn. Corridor Width Provided = 6'-0"	
Accessible means of egress are Not required in alterations to EXISTING buildings	IBC Section 1007.1 and NFPA 3411.6

Area Served	Occupant Load	Width Required	Width Provided
1/2 Total	(min. req'd.)	(min. req'd.)	(min. req'd.)
Stair 1	3,533 gross sq ft	133 X 3' = 39.9' (*36')	40'
*Existing Risers: 6.75" to 7" *Existing Treads: 9.5"			
Stair 2	3,533 gross sq ft	133 X 3' = 39.9' (*36')	46'
*Existing Risers: 6.75" to 7" *Existing Treads: 10.75"			
Egress Doors	133 x 2' =	26.6' (*36')	36'

7. Minimum Plumbing Fixtures

REQUIRED FIXTURES	WC fixtures/person	Urinal fixtures/male	Lav fixtures/person	DF/person
Educational Occupancy	1 per 50	67% max.	1 per 50	1 per 100

	Male	Female	Occupancy Count Based On Actual Number Of Students And Staff Planned For Final Capacity.
required fixtures	4	4	None
provided fixtures includes all floors	5	5	None

Copyright 2012 Richard Renner Architects

RRR

Richard Renner | Architects
Shelburne, MA
Portland, ME
207.773.9699

REGISTERED ARCHITECT
RICHARD K. RENNER
EX. 1019
STATE OF MAINE

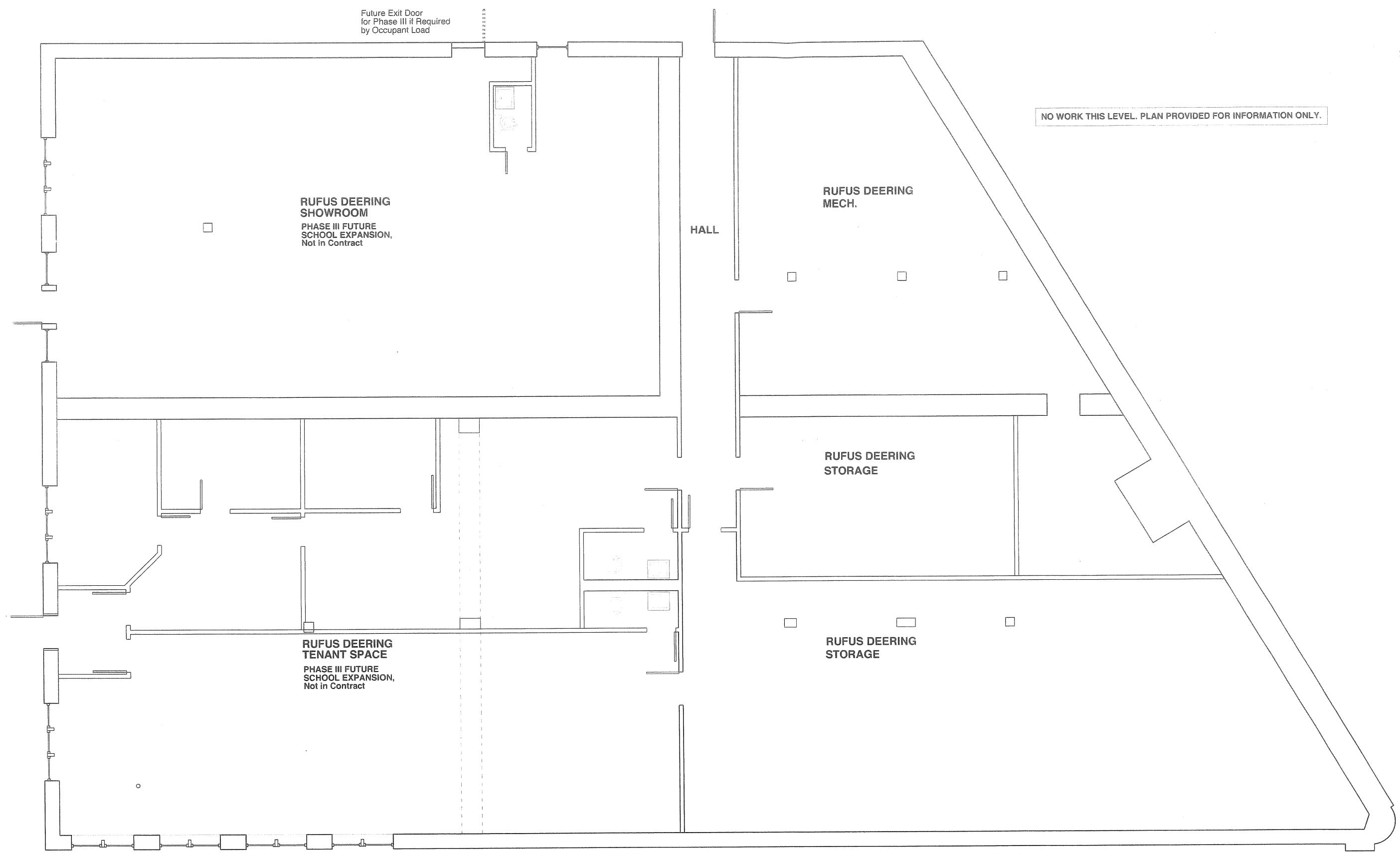
Baxter Academy
54 York Street
Portland, Maine




Code Summary

Drawn by: RRA
Scale: As Shown
Date: 12-10-2012

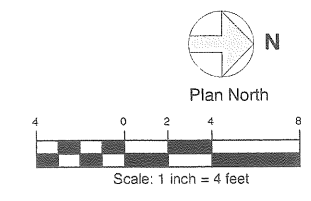
File Name:
Project No.: 2012-10-00
Revised:

Baxter Academy
CS



	Unrenovated Area This Floor to Remain With Rufus Deering after Phase III	= 2,661 SF
	Baxter Academy Renovated Area This Contract	= 0 SF
Total GSF this Floor		= 7,067
	Phase III Future School Expansion, 4,406 Total Sf NIC	

1 Existing Lower Level Plan
SCALE: 1/4" = 1'-0"



Copyright 2012 Richard J Renner Architects

RRRA



Richard Renner | Architects
Portland, ME 207.773.9699
Sherborn, MA 508.651.2365

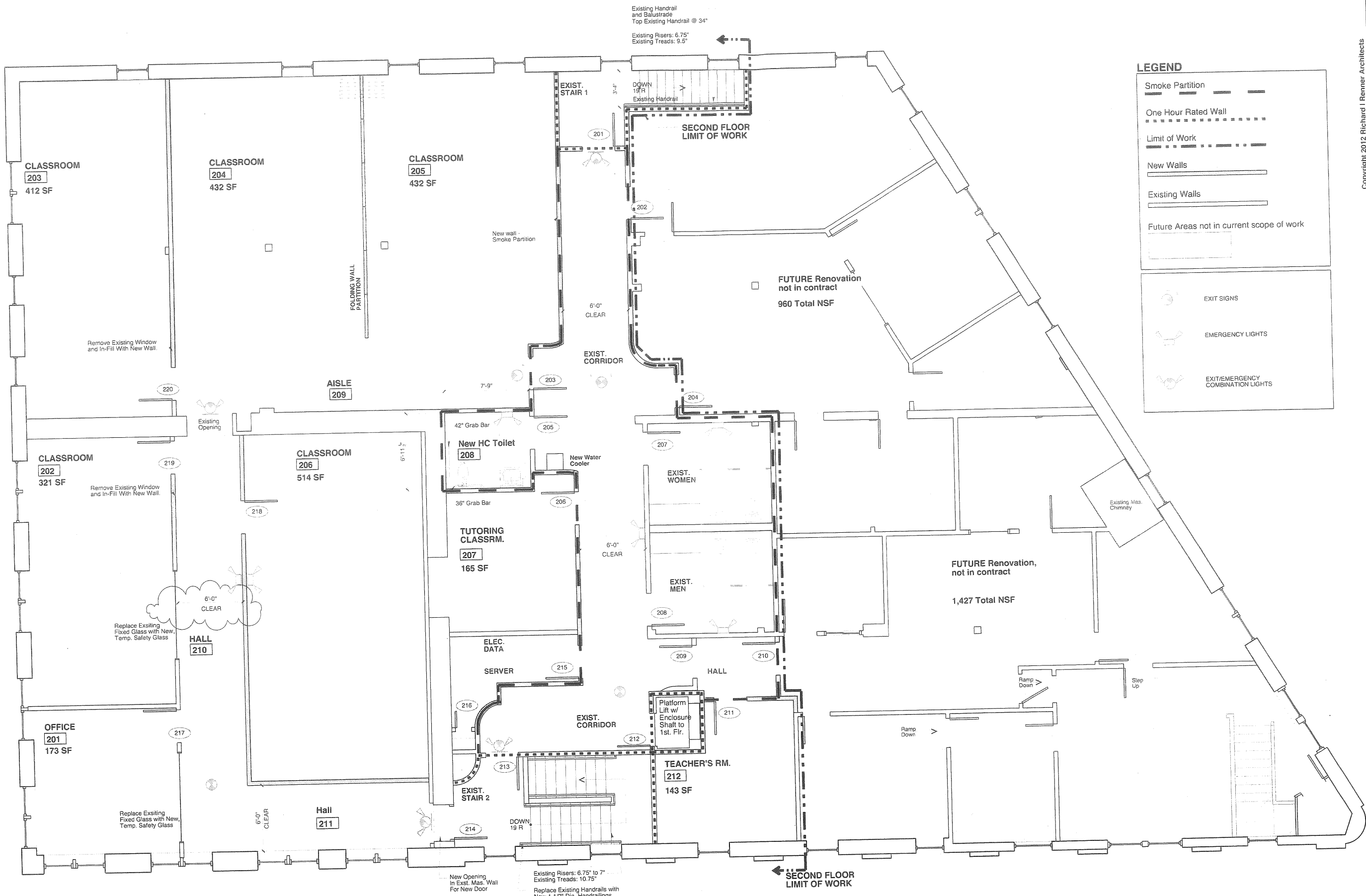
Consultants:
Structural Engineer (company) Structural Engineers
Mechanical Engineer (company) Energy
Landscape Architect company
Energy Consultant company
Lighting Design company

Baxter Academy
54 York Street
Portland, Maine

Existing Lower Level Plan
Drawn by: RRA
Scale: 1/4" = 1'-0"
Date: 12-10-2012

File Name:
Project No. 2012-10.00
Revised:

Baxter Academy
A1.0

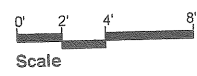


LEGEND

- Smoke Partition
- One Hour Rated Wall
- Limit of Work
- New Walls
- Existing Walls
- Future Areas not in current scope of work

- EXIT SIGNS
- EMERGENCY LIGHTS
- EXIT/EMERGENCY COMBINATION LIGHTS

1 Second Floor Plan
SCALE: 1/4"=1'-0"



Copyright 2012 Richard Renner Architects

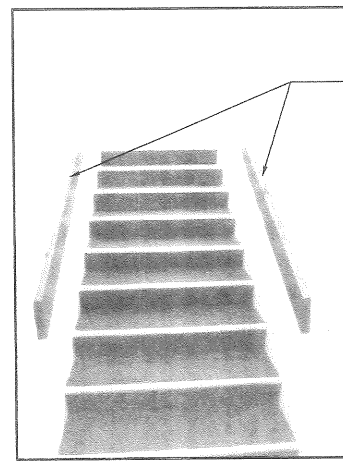
RRRA
Richard Renner | Architects
Portland, ME
207.773.9699
Sherborn, MA
508.651.2385



Baxter Academy
54 York Street
Portland, Maine

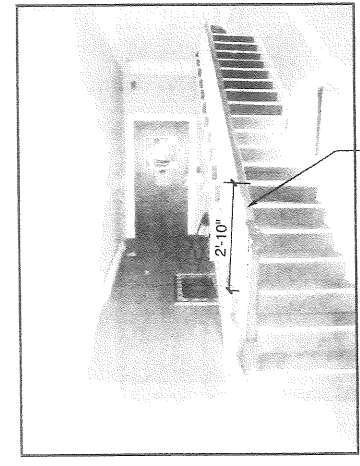
**Existing Second Floor Plan
With Proposed Changes**
Drawn by: RRA
Scale: 1/4"=1'-0"
Date: 12-10-2012
File Name: Project No. 2012-10-00
Revised: 11-21-2012

Baxter Academy
A1.2



Remove Existing Railings and Replace With New 1 1/2" dia Wood or Std. Metal Handrail. Top of Handrail to Be at 2'-10" Min. Above Tread Nosing.

EXISTING EAST STAIR



Existing Railing To Remain

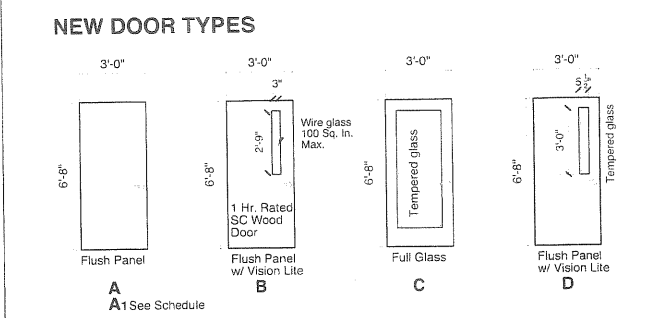
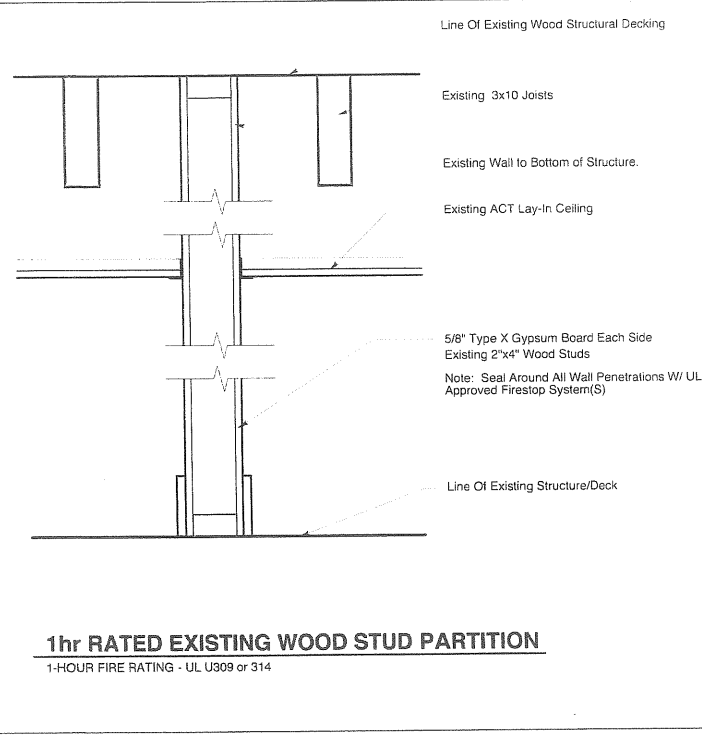
EXISTING WEST STAIR

HARDWARE SETS

HW1 Exterior Exit Doors, 101, 103, 105 (Out-swing) Exit Device Closer Weatherstripping Door Bottom Sweep Kickplate
HW2 Exterior Classroom Door 102 (In-swing) Lever Lockset "Classroom Intruder Function" Closer Weatherstripping Door Bottom Sweep
HW3 Interior Egress Stair and Corridor Doors, 110, 111, 112, 126, 201, 203, 209, 210, 212, 213, 214 New Or Exist. Hinges Fire Rated Exit Device Closer Wall Stop
HW4 Classroom Doors 114, 115, 124, 125, 206, 218, 219, 220 New Or Exist. Hinges Lever Lockset "Classroom Intruder Function" Closer Door Stop
HW5 Toilet Room Doors, (Gang Toilets), 119, 120, 207, 208 Closet Doors 123, 118 Lever Passage Lockset Closer Door Stop
HW6 Office Doors 107, 117, 211, 217 New or Exist. Hinges Lever Lockset - "Office Function F109" Closer Door Stop Silencers
HW7 Storage or Utility Room Doors 108, 109, 113, 122, 215, Lever Lockset "Closet Function F86" Closer
HW8 UniSex Bathroom Doors 116, 121, 122, 205, New or Exist. Hinges Lever Lockset "Privacy Function F76" Closer Door Stop

DOOR SCHEDULE

Door No.	LOCATION		DOOR				FRAME		HARDWARE	COMMENTS
	From	To	Size	Type	Material	Finish	Type	Material	Group	
LOWER FLOOR LEVEL - NO WORK										
FIRST FLOOR LEVEL										
101	Existing Stair 1	York Street	3'-0" x 6'-8"	Exist.						HW1
102	Classroom 104	York Street	3'-0" x 6'-8" x 1 3/4"	C	SC Wd/Glass					HW2
103	Main Entrance	York Street	3'-0" x 6'-8" x 1 3/4"	C	SC Wd/Glass			HM		HW1
104	Existing Stair 3	York Street	3'-0" x 6'-8" x 1 3/4"	C	SC Wd/Glass					HW1
105	Existing Stair 2	Maple Street	3'-0" x 6'-8"	Exist.						HW1
106	Existing Storage Future Office 107	Existing Stair 3	Not In Contract	Exist.						Exist.
107	Student Center 105	Office 106	3'-0" x 6'-8"	Exist.						HW6
108	Exist. Mech./Elec Rm.	Student Center 105	3'-0" x 6'-8"	Exist.				HM		Exist Lever Lockset
109	Student Center 105	Storage	2'-8" x 6'-8"	Exist.						HW7
110	Platform Lift	Student Center 105	3'-0" x 6'-8" x 1 3/4"	A1	SC Wood				HM	HW3
111	Platform Lift	New Aisle	3'-0" x 6'-8" x 1 3/4"	A1	SC Wood				HM	HW3
112	New Aisle	Exist Hall	3'-0" x 6'-8" x 1 3/4"	B	SC Wood				HM	HW3
113	Classroom 101	Mech. Closet	3'-0" x 6'-8"	Exist.						HW7
114	New Aisle	Classroom 101	3'-0" x 6'-8" x 1 3/4"	D	SC Wood					HW4
115	New Aisle	Classroom 102	3'-0" x 6'-8" x 1 3/4"	D	SC Wood					HW4
116	Exist. Toilet	Classroom 102	3'-0" x 6'-8"	Exist.						HW8
117	Art Classroom 103	Teacher's Room 108	2'-6" x 6'-8"	Exist.						HW6
118	Closet	Teacher's Room 108	2'-0" x 6'-8"	Exist.						HW5
119	Existing Corridor	Existing Men	3'-0" x 6'-8"	Exist.						HW5
120	Existing Corridor	Existing Woman	3'-0" x 6'-8"	Exist.						HW5
121	HC Toilet 109	Existing Corridor	3'-0" x 6'-8" x 1 3/4"	A	SC Wood				HM	HW8
122	Existing Storage	Existing Corridor	2'-8" x 6'-8"	Exist.						HW7
123	Classroom	Existing Storage	2'-8" x 6'-8"	Exist.						HW5
124	Classroom 104	Existing Corridor	3'-0" x 6'-8" x 1 3/4"	D	SC Wood				HM	HW4
125	Art Classroom 103	Existing Corridor	3'-0" x 6'-8"	Exist.						HW4
126	Existing Corridor	Existing Stair 1	3'-0" x 6'-8" x 1 3/4"	B	SC Wood				HM	HW3
SECOND FLOOR LEVEL										
201	Existing Corridor	Existing Stair 1	3'-0" x 6'-8"	Exist.					HM	Exist.
202	Existing Corridor	Future	3'-0" x 6'-8"	NIC	Exist.					HW3
203	Aisle 209	Existing Corridor	3'-0" x 6'-8"	Exist.						HW3
204	Existing Corridor	Future	3'-0" x 6'-8"	NIC	Exist.					Exist.
205	New HC Toilet 208	Existing Corridor	3'-0" x 6'-8" x 1 3/4"	A	SC Wood				HM	HW8
206	Existing Corridor	Tutoring Classrm. 207	3'-0" x 6'-8" x 1 3/4"	D	SC Wood				HM	HW4
207	Existing Corridor	Existing Women	3'-0" x 6'-8"	Exist.						HW5
208	Existing Corridor	Existing Men	3'-0" x 6'-8"	Exist.						HW5
209	Hall	Existing Corridor	3'-0" x 6'-8"	Exist.						HW3
210	Future	Hall	3'-0" x 6'-8" x 1 3/4"	D	SC Wood				HM	HW3
211	Hall	Teacher's Room 212	3'-0" x 6'-8" x 1 3/4"	D	SC Wood				HM	HW6
212	Platform Lift	Existing Corridor	3'-0" x 6'-8" x 1 3/4"	A1	SC Wood				HM	HW3
213	Existing Corridor	Existing Stair 2	3'-0" x 6'-8"	Exist.					HM	Exist.
214	Hall 211	Existing Stair 2	3'-0" x 6'-8" x 1 3/4"	B	SC Wood				HM	HW3
215	Existing Corridor	Elect. Data Server	3'-0" x 6'-8" x 1 3/4"	A	SC Wood				HM	HW7
216	Closet	Elect. Data Server	1'-6" x 6'-8"	Exist.						Exist.
217	Hall 210	Office 201	3'-0" x 6'-8" x 1 3/4"	A	SC Wood				HM	HW6
218	Hall 210	Classroom 206	3'-0" x 6'-8" x 1 3/4"	D	SC Wood				HM	HW4
219	Hall 210	Classroom 202	3'-0" x 6'-8" x 1 3/4"	D	SC Wood				HM	HW4
220	Aisle 209	Classroom 203	3'-0" x 6'-8" x 1 3/4"	D	SC Wood				HM	HW4



TYPICAL DOOR AND HARDWARE NOTES:

- All interior doors into egress stairs to be min. (1) Hr Rated With Panic Type Exit Devices and Closers.
- All doors from egress stairs to exterior to have panic type exit devices and closers.
- All doors to be 3'-0" wide, unless otherwise noted.
- All door glazing and sidelight glazing to be 1/4" tempered safety glass.
- All door locksets and passages sets to be lever handle type.

EXIT DEVICES

- Manufacturing: Subject to compliance with requirements, provide products by one of the following:
 - Precision Hardware, Inc. (PHI)
 - Sargent Manufacturing Company; Div. of ESSEX Industries, Inc. (SGT)
 - Von Duprin; an ingersoll-Rand Company (VDC)
- Products: All exit devices for this project shall be one of the following:
 - Freedom, Olympian Series
 - The 80 Series exit device by Sargent & Co.
 - 88 Series by Von Duprin Division
- Standard: BHMA A156.8
 - BHMA Grade, Grade 1
- Certified Products: Provide exit devices listed in BHMA's "Directory of Certified Exit Devices"
- Panic Exit Devices: Listed and labeled by a testing and inspecting agency acceptable to authorities having jurisdiction, for panic protection, based on testing according to UL 305
- Fire Exit Devices: Complying with NFPA 80 that are listed and labeled by a testing and inspecting agency acceptable to authorities having jurisdiction, for fire and panic protection, based on testing according to UL 306 and NFPA 262. Only fire exit devices may be used for fire doors.

RRR Architects

Richard Renner | Architects
Portland, ME 207.773.9699
Sherborn, MA 508.651.2365

Copyright 2012, Richard I. Renner Architects

Consultants:

Baxter Academy
54 York Street
Portland, Maine

Door & Hardware Schedules
Wall Details & Stair Railings

Drawn by: RRA File Name:
Scale: 1/4"=1'-0" Project No. 2012-10-00
Date: 12-10-2012 Revised:

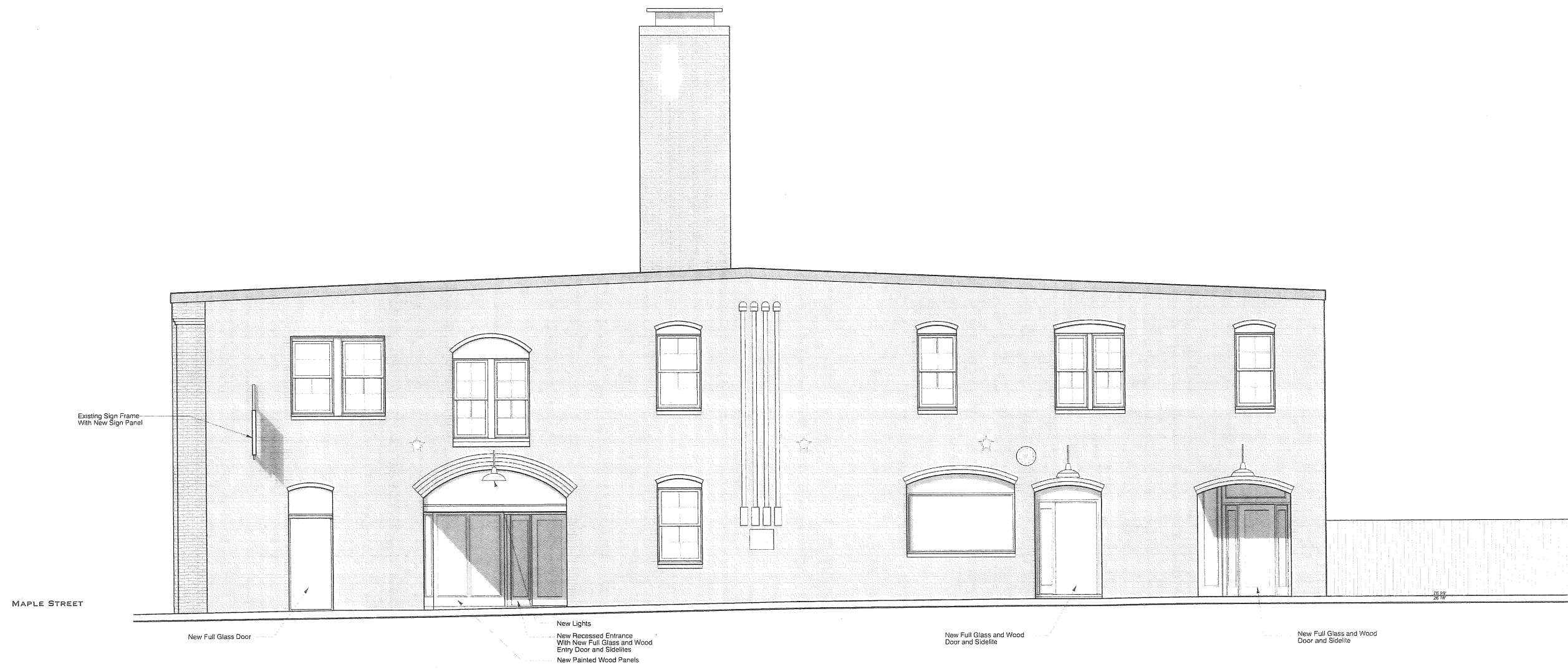
Baxter Academy

A1.3

Copyright 2012 Richard I Renner Architects



Richard Renner | Architects
Portland, ME
207.773.9699
Sherborn, MA
508.651.2385



54 YORK STREET ELEVATION

Baxter Academy
Portland, Maine

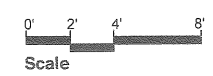
Baxter Academy
54 York Street
Portland, Maine

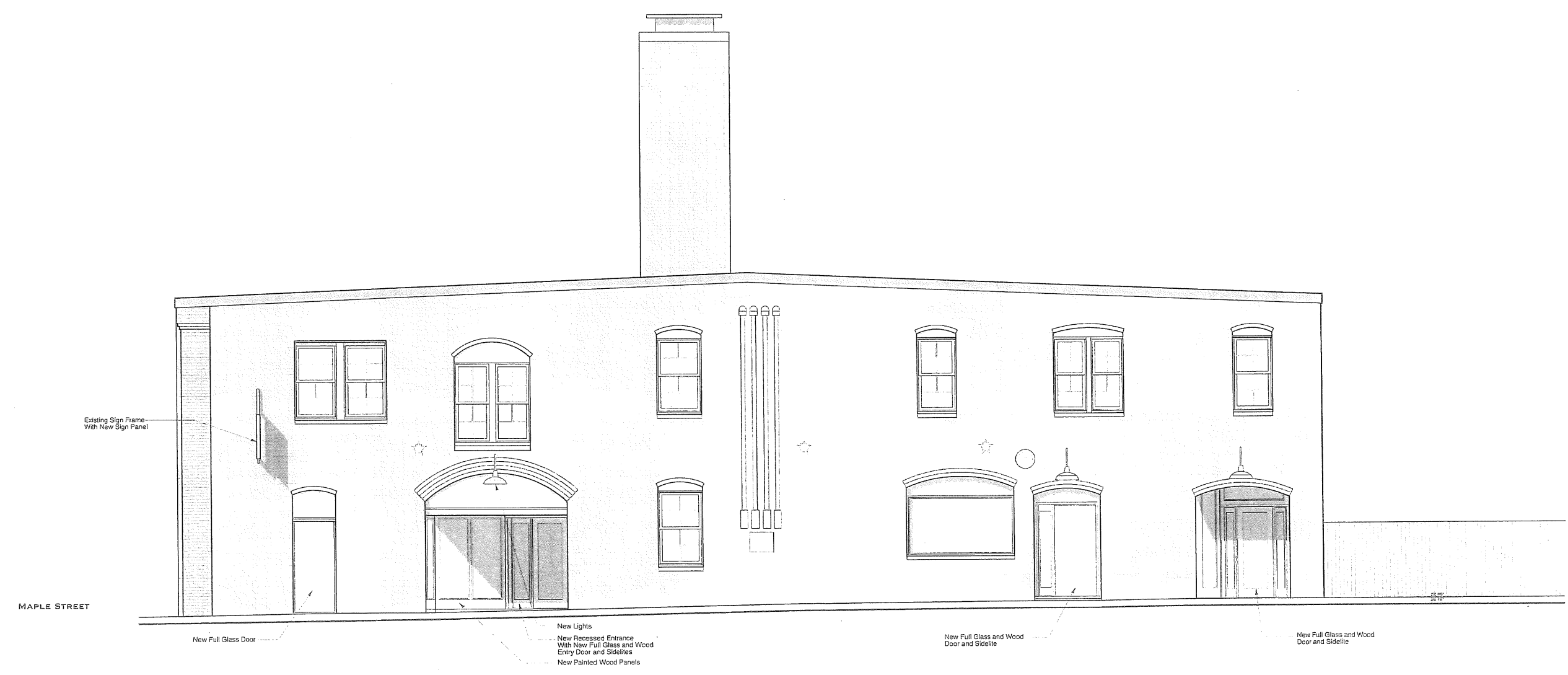
York Street Elevation

Drawn by: RRA
Scale: 1/4"=1'-0"
Date: 11-12-2012
File Name:
Project No.: 2012-10.00
Revised:

Baxter Academy

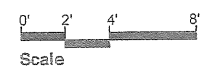
A2.1





54 YORK STREET ELEVATION

Baxter Academy
Portland, Maine



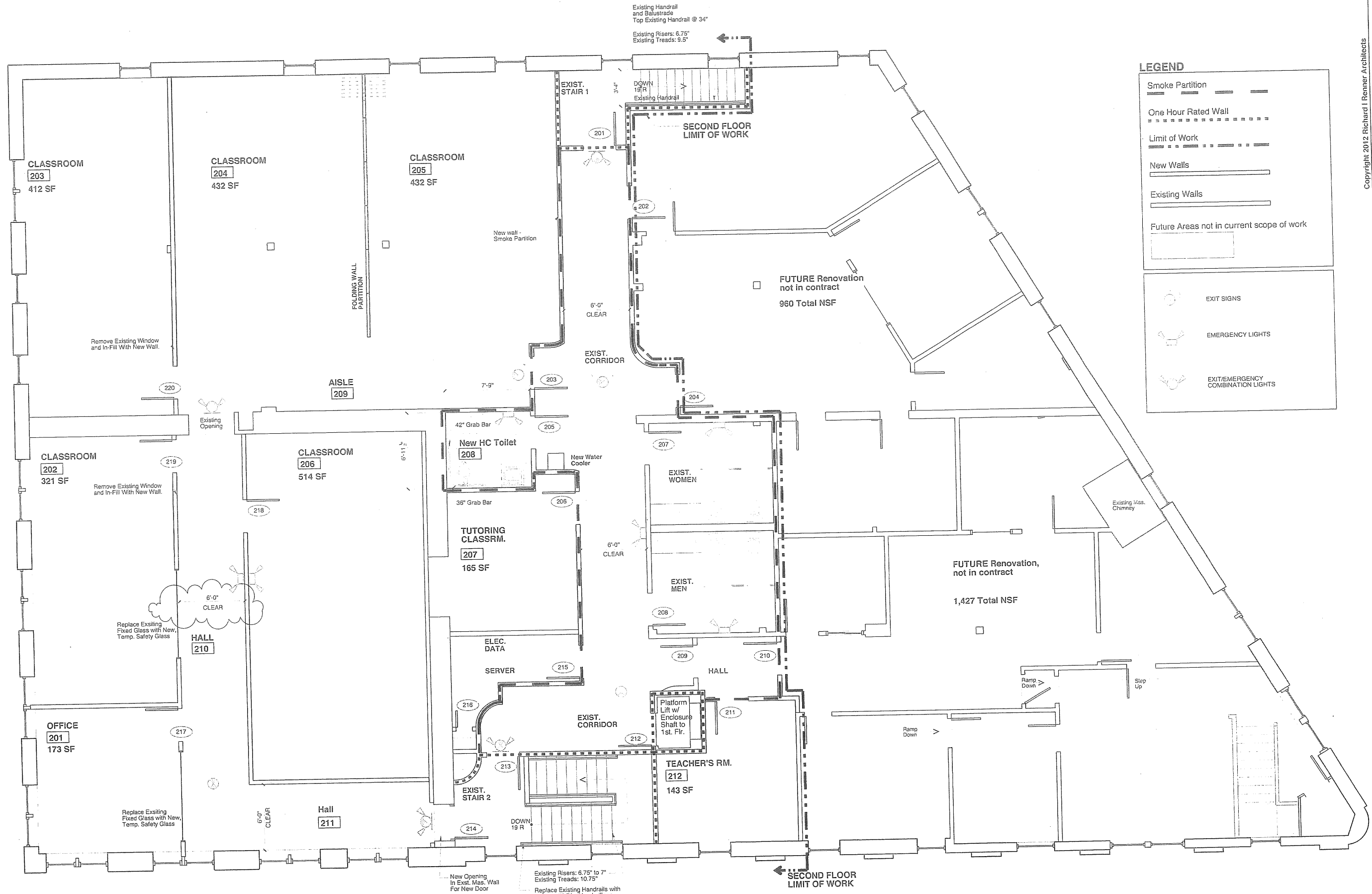
Baxter Academy
54 York Street

Portland, Maine

York Street Elevation

Drawn by: RRA
Scale: 1/4"=1'-0"
Date: 11-12-2012
File Name:
Project No. 2012-10.00
Revised:

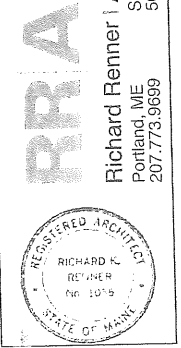
Baxter Academy
A2.1



LEGEND

- Smoke Partition
- One Hour Rated Wall
- Limit of Work
- New Walls
- Existing Walls
- Future Areas not in current scope of work
- EXIT SIGNS
- EMERGENCY LIGHTS
- EXIT/EMERGENCY COMBINATION LIGHTS

Copyright 2012 Richard Renner Architects



Richard Renner | Architects
 Portland, ME 207.773.9699
 Sherborn, MA 508.651.2385

Baxter Academy
 54 York Street
 Portland, Maine

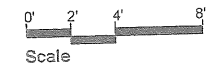
**Existing Second Floor Plan
 With Proposed Changes**

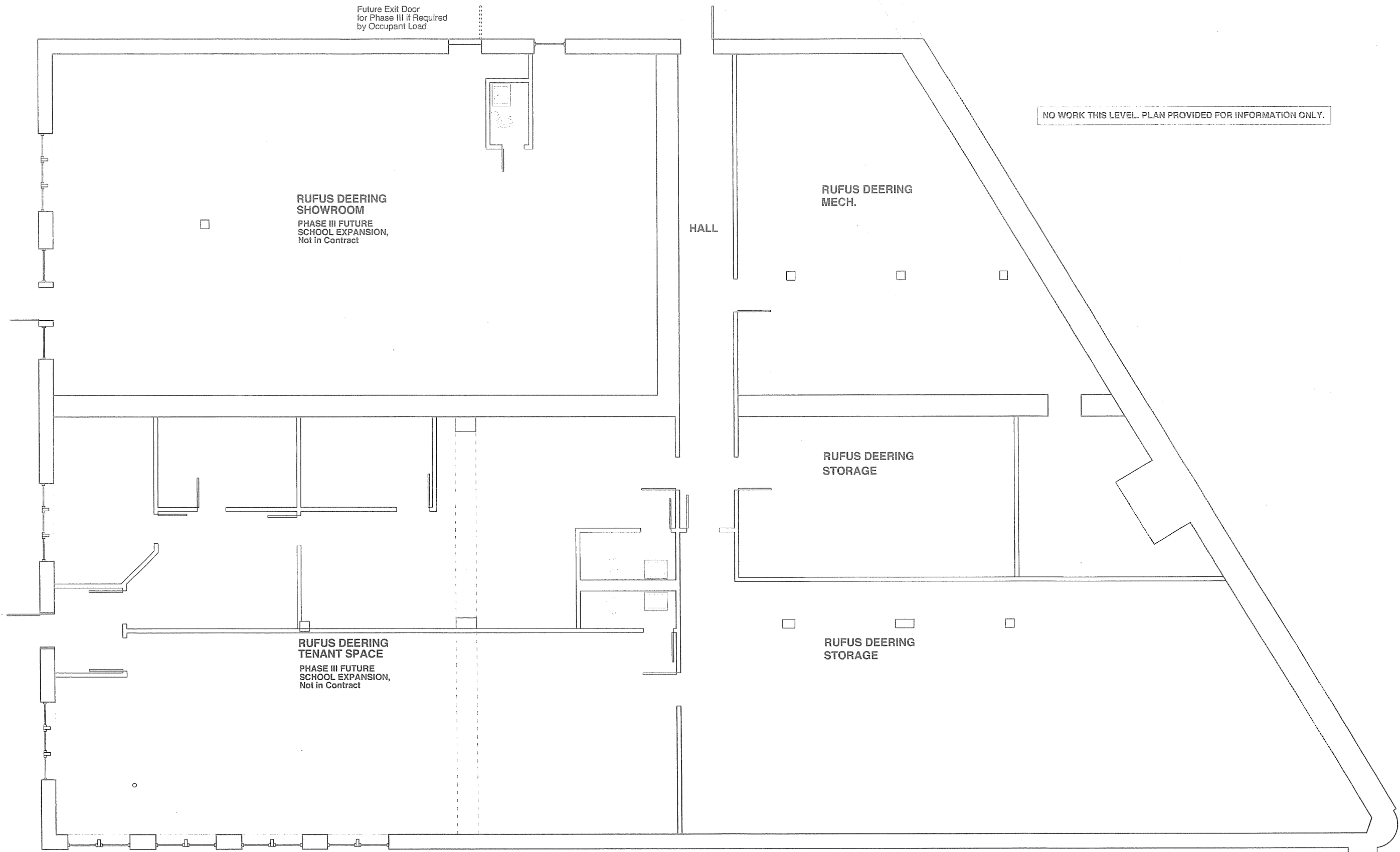
Drawn by: RRA
 File Name: Project No. 2012-10.00
 Scale: 1/4"=1'-0"
 Date: 12-10-2012
 Revised: 11-21-2012

Baxter Academy

A1.2

1 Second Floor Plan
 SCALE: 1/4"=1'-0"





Future Exit Door for Phase III if Required by Occupant Load

RUFUS DEERING SHOWROOM
PHASE III FUTURE SCHOOL EXPANSION, Not in Contract

RUFUS DEERING MECH.

HALL

NO WORK THIS LEVEL. PLAN PROVIDED FOR INFORMATION ONLY.

RUFUS DEERING STORAGE

RUFUS DEERING STORAGE

RUFUS DEERING TENANT SPACE
PHASE III FUTURE SCHOOL EXPANSION, Not in Contract

Exterior Metal Stair DOWN 12 R

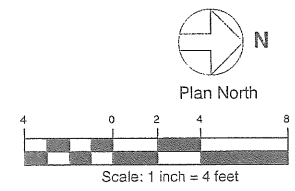
	Unrenovated Area This Floor to Remain With Rufus Deering after Phase III	= 2,661 SF
	Baxter Academy Renovated Area This Contract	= 0 SF

Total GSF this Floor = 7,067

	Phase III Future School Expansion, 4,406 Total Sf
--	---

MAPLE STREET

1 Existing Lower Level Plan
SCALE: 1/4" = 1'-0"



Copyright 2012 Richard I Renner Architects

RRRA

Richard Renner | Architects
Sherborn, MA
508.651.2385

Richard K. Renner
REGISTERED ARCHITECT
No. 1005
STATE OF MAINE

Consultants:

- Structural Engineer (company) Structural Engineers
- Landscape Architect company
- Mechanical Engineer (company) Energy
- Energy Consultant company
- Lighting Design company

Baxter Academy
54 York Street
Portland, Maine

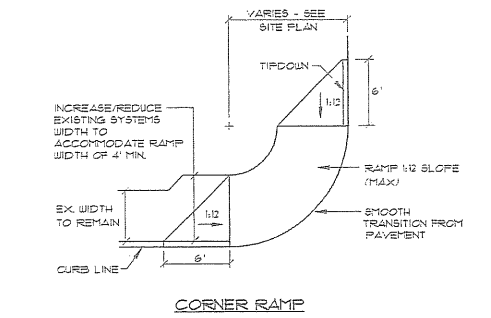
Existing Lower Level Plan

Drawn by: RRA
Scale: 1/4"=1'-0"
Date: 12-10-2012

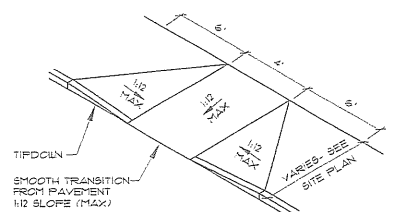
File Name:
Project No.: 2012-10.00
Revised:

Baxter Academy

A1.0

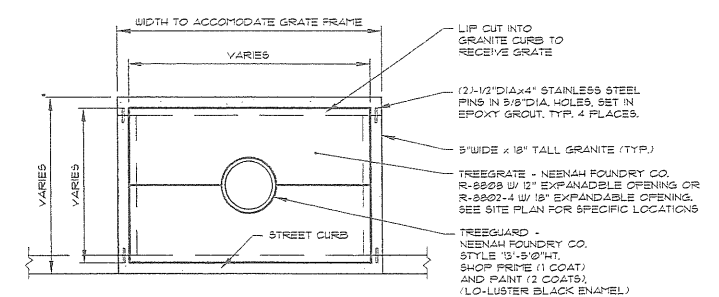


CORNER RAMP



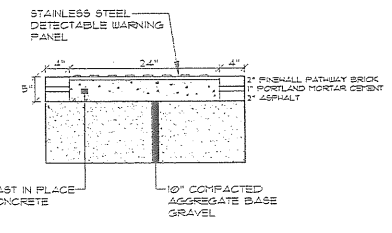
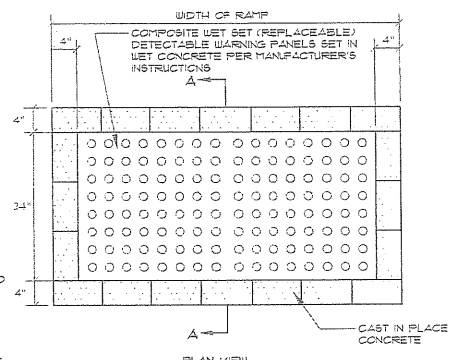
STRAIGHT RAMP

HANDICAP RAMP
NOT TO SCALE

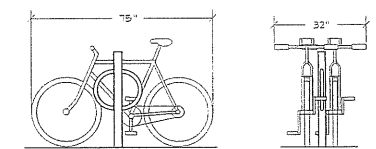
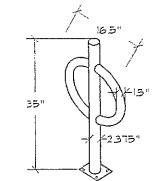


TREE GRATE DETAIL
NOT TO SCALE

- NOTES:**
- COMPOSITE LIFT SET (REPLACEABLE) DETECTABLE WARNING PANELS SHALL BE AS MANUFACTURED BY ADA SOLUTIONS, INC. (WWW.ADATILE.COM) OR APPROVED EQUAL.
 - CAST IN PLACE CONCRETE SHALL MEET SPECIFICATIONS FOR MDOT CLASS A STRUCTURAL CONCRETE, MINIMUM COMPRESSIVE STRENGTH 4000 PSI.
 - TRUNCATED CONES SHALL BE ALIGNED IN ROWS, PARALLEL AND PERPENDICULAR TO THE PREDOMINANT DIRECTION OF TRAVEL. NO OTHER DETECTABLE WARNING DESIGN OR CONFIGURATION IS ALLOWED.
 - FOR ALL DETECTABLE WARNING PANELS, WITHIN OR ABUTTING HISTORIC DISTRICTS AND HISTORIC LANDSCAPES, "DARK GRAY" COLORED (79618) PANELS SHALL BE USED. FOLLOW MANUFACTURER'S INSTRUCTIONS FOR INSTALLATION.
 - THE DETECTABLE WARNING PANEL SHALL HAVE ONE FULL COURSE OF FINISHALL PATHWAY PAVERS (THE CURRENT BRICK STANDARD) AROUND THE FULL PERIMETER OF THE PANEL. THE PERIMETER COURSE SHALL BE SET USING PORTLAND MORTAR CEMENT TO CREATE A FLUSH SURFACE BETWEEN THE BRICK AND THE PANEL.
 - THE DETECTABLE WARNING PANEL(S) SHALL EXTEND 34 INCHES MINIMUM IN THE DIRECTION OF TRAVEL AND THE FULL WIDTH OF THE CURB RAMP, LANDING, OR BLENDED TRANSITION TO THE STREET.
 - ORIENTATION: THE DETECTABLE WARNING PANEL SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 6 INCHES MINIMUM AND 8 INCHES MAXIMUM FROM THE CURB LINE. THE PANEL SHALL BE ORIENTED TO THE DIRECTION OF TRAVEL AS IDENTIFIED BY THE POINT OF EGRESS.

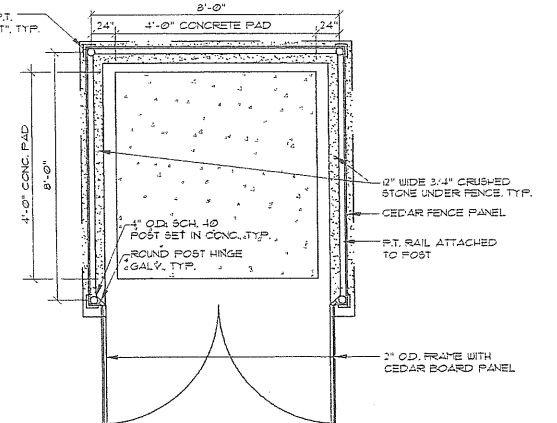
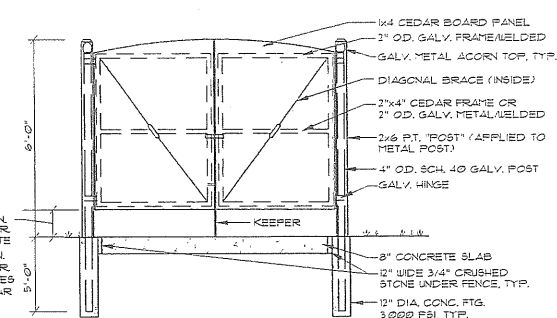


SIDEWALK RAMP DETECTABLE WARNING PANEL
NOT TO SCALE

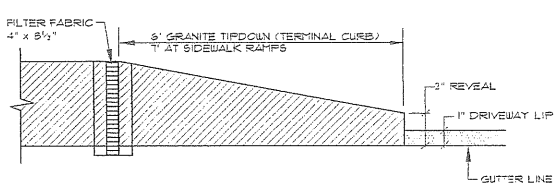


BIKE HITCH BICYCLE RACK
NOT TO SCALE

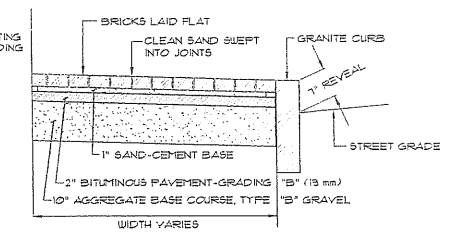
- NOTES:**
- BICYCLE RACK AS MANUFACTURED BY DERO BIKE RACKS OR CITY OF PORTLAND APPROVED EQUAL.



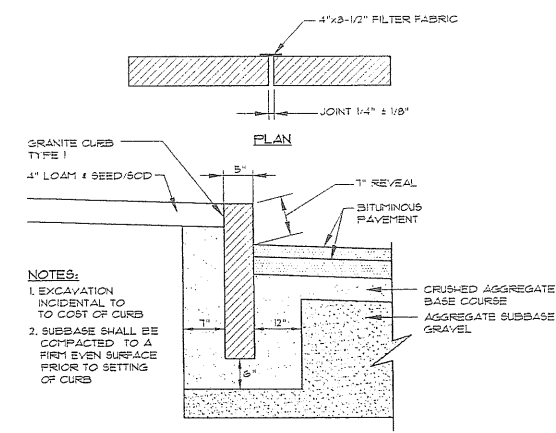
TYPICAL DUMPSTER ENCLOSURE
NOT TO SCALE



TYPICAL TIPDOWN CURB INSTALLATION
NOT TO SCALE

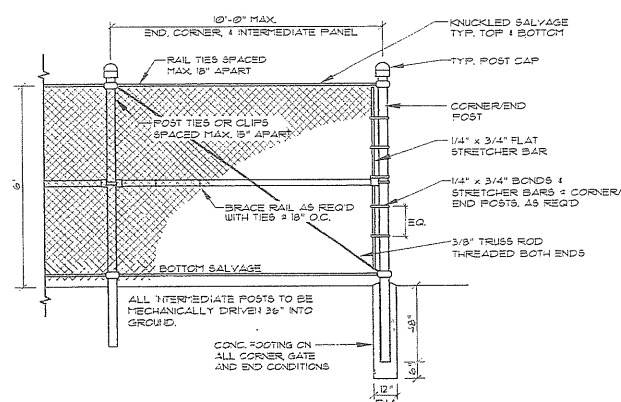


BRICK SIDEWALK DETAIL
NOT TO SCALE



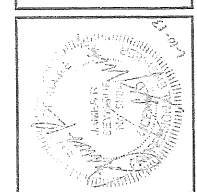
- NOTES:**
- EXCAVATION INCIDENTAL TO COST OF CURB.
 - SUBBASE SHALL BE COMPACTED TO A FIRM EVEN SURFACE PRIOR TO SETTING OF CURB.

VERTICAL GRANITE CURB
NOT TO SCALE



- NOTES:**
- CORNER OR END POST, NOMINAL 3" O.D. GALVANIZED STEEL PIPE, MIN. 5.75 IBS/LF.
 - INTERMEDIATE POST, NOMINAL 2-1/2" O.D. GALVANIZED STEEL PIPE, MIN. 3.85 IBS/LF.
 - BRACES (TOP & BOTTOM), NOMINAL 1-5/8" O.D. GALVANIZED STEEL PIPE, MIN. 2.75 IBS/LF.
 - THE OUTSIDE OF THE FENCE FABRIC SHALL BE 3" INSIDE THE EDGE OF PAVEMENT.
 - BRACE RAIL AND DIAGONAL BRACE ROD SHALL BE INSTALLED AT EACH 10' CORNER SECTION OF ENCLOSURE. CONCRETE SHALL HAVE MINIMUM COMPRESSIVE STRENGTH (f'c) OF 3,000 PSI WITH 6% AIR ENTRAINED.
 - SUBMIT SHOP DRAWINGS FOR OWNER'S/ENGINEER'S APPROVAL.
 - FENCE FABRIC, POST, RAILS AND APPURTENANCES SHALL BE VINYL CLAD COLOR: BLACK.

TYPICAL CHAIN LINK FENCE
NOT TO SCALE

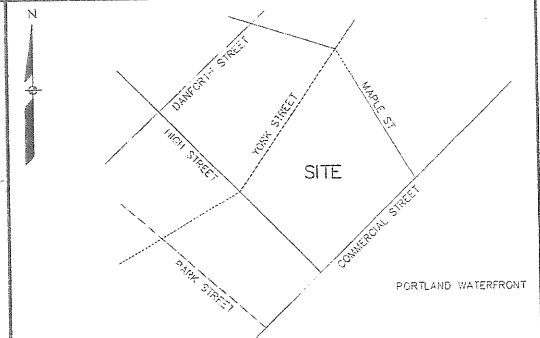
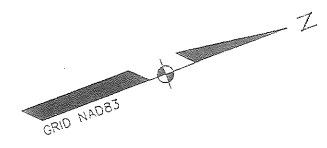


FINAL PLAN SUBMISSION	01-10-13	JRS	BY:	STATUS:
FINAL SITE PLAN REVIEW	12-04-12	JRS	BY:	STATUS:
SUBMITTED TO CITY FOR REVIEW	05-05-12	JRS	BY:	STATUS:

SEBAGO
75 John Roberts Rd., Suite 1A
South Portland, ME 04106
Tel: 207-762-3859

DETAILS OF:
BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE
54 YORK STREET
PORTLAND, MAINE
FOR:
RUFUS DEERING LUMBER CO.
54 YORK STREET
PORTLAND, MAINE 04101

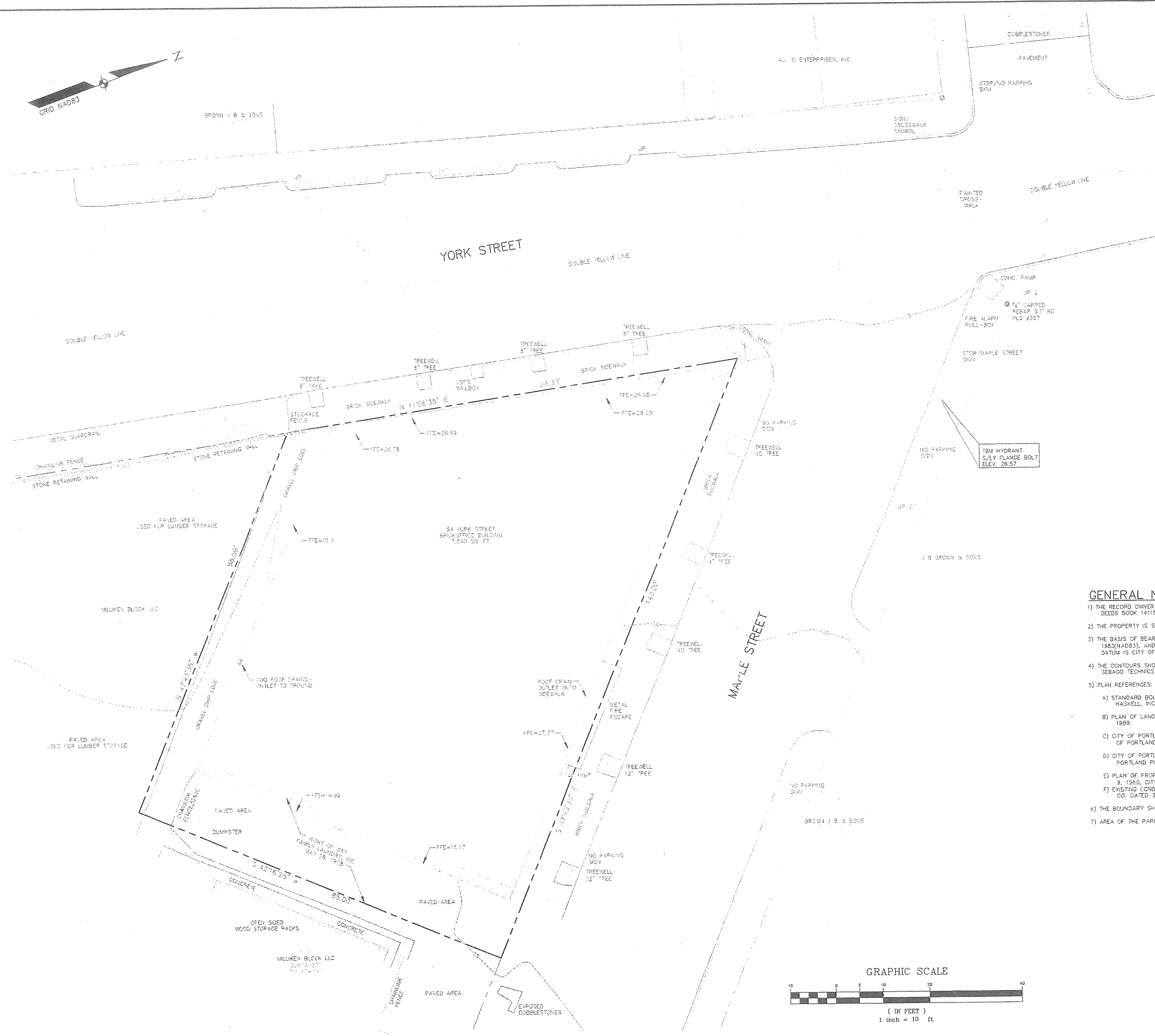
DATE	SCALE
05-22-12	AS NOTED



LOCATION MAP

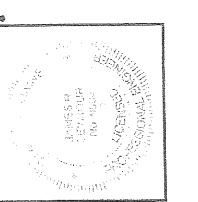
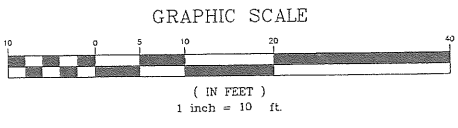
LEGEND

EXISTING	DESCRIPTION
---	BOUNDARY LINE/R.O.W.
---	ABUTTER LINE/R.O.W.
○	MONUMENT
⊗	IRON PIPE/RCD
□	BENCHMARK
---	BUILDING
---	SIGN
---	EDGE PAVEMENT
---	CURBLINE
---	CONTOURS
---	CHAIN LINK FENCE
---	STOCKADE FENCE
---	RETAINING WALL
---	GUARDRAIL
---	WATER
---	GATE VALVE
---	HYDRANT
---	SEWER MH
---	STORM DRAIN
---	CATCH BASIN
---	DRAINAGE MH
---	OVERHEAD ELEC. & TEL
---	UNDERGROUND ELEC. & TEL
---	ELECTRICAL METER
---	ELECTRICAL MANHOLE
---	ELECTRICAL BOX
---	UTILITY POLE
---	GUY
---	TELEPHONE MANHOLE
C.O.P.	CITY OF PORTLAND



GENERAL NOTES

- 1) THE RECORD OWNER IS RUFUS DEERING COMPANY AS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 14115, PAGE 13.
- 2) THE PROPERTY IS SHOWN AS LOTS 7 AND 8 ON THE CITY OF PORTLAND TAX MAP 42, BLOCK A.
- 3) THE BASIS OF BEARINGS IS MAINE STATE PLANE GRID, MAINE WEST ZONE, NORTH AMERICAN DATUM 1983(NAD83), AND REFERENCED BY REAL TIME GLOBAL POSITIONING SYSTEM RECEIVERS. THE VERTICAL DATUM IS CITY OF PORTLAND DATUM REFERENCED TO A PK NAIL IN UTILITY POLE #CMP 32/NET 35.
- 4) THE CONTOURS SHOWN ARE ONE FOOT CONTOUR INTERVAL, BASED UPON A FIELD SURVEY COMPLETED BY SEBAGO TECHNICS DURING APRIL OF 2012.
- 5) PLAN REFERENCES:
 - A) STANDARD BOUNDARY AND EXISTING CONDITIONS SURVEY A.G.A. REALTY TRUST, PREPARED BY OWEN HASKELL, INC. DATED OCTOBER 2 1993, RECORDED IN SAID REGISTRY PLAN BOOK 195 PAGE 330.
 - B) PLAN OF LAND FOR RUFUS DEERING COMPANY, PREPARED BY OWEN HASKELL, INC. DATED JANUARY 9, 1989.
 - C) CITY OF PORTLAND PLAN OF CHANGE OF LINE CORNER OF HIGH STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 366/9.
 - D) CITY OF PORTLAND PLAN OF HIGH STREET BETWEEN YORK STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 668/5 DATED 1959.
 - E) PLAN OF PROPERTY FOR CITIES SERVICE OIL CO., PREPARED BY H.I. AND E.G. JORDAN DATED FEBRUARY 9, 1950, CITY OF PORTLAND PUBLIC WORKS RECORDS 705/3.
 - F) EXISTING CONDITIONS SURVEY OF RUFUS DEERING LUMBER IMPROVEMENTS FOR: RUFUS DEERING LUMBER CO. DATED 3-19-08 AND REVISED THROUGH 8-22-08 BY SEBAGO TECHNICS.
 - G) THE BOUNDARY SHOWN IS BASED ON THE PLAN REFERENCED IN NOTE 5-A.
 - H) AREA OF THE PARCEL IS APPROXIMATELY 9,715 SQUARE FEET.



WILLIAM C. SHIPPEN PLS 2118

REV.	DATE	DESCRIPTION
D	01-10-13	FINAL PLAN SUBMISSION
C	12-04-12	FINAL SITE PLAN REVIEW
B	08-05-12	SUBMITTED TO CITY FOR REVIEW
A	05-08-12	PRELIMINARY

STATUS: PRELIMINARY

THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM SEBAGO TECHNICS, INC. ANY ALTERATIONS AUTHORIZED OR OTHERWISE, SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC.

SEBAGO
 1 2 3 4 5 6 7 8 9 10
 WWW.SEAGOTECHNICS.COM
 76 John Robards Rd. - Suite 1A
 South Portland, ME 04106
 Tel: 207-262-5100

PROJECT NO.	07383
FIELD BOOK	ELEC
DESIGN	CHKD
CON-CLB	WCS

EXISTING CONDITIONS SURVEY
 OF:
BAXTER ACADEMY FOR TECHNOLOGY AND SCIENCE
 54 YORK STREET
 PORTLAND, MAINE
 FOR:
RUFUS DEERING LUMBER CO.
 383 COMMERCIAL STREET
 PORTLAND, MAINE 04101

DATE	SCALE
05-07-12	1"=10'

SHEET 2 OF 4



