

42-A-1

2008-0082

383 Commercial St.

Drive-thru Lumber Warehouse

Rufus Deering

on Spreadsheet



Development Review Application Portland, Maine

Department of Planning and Development, Planning Division and Planning Board

Address of Proposed Development: 383 COMMERCIAL STREET PORTLAND, ME		
Zone: B-5b, URBAN COMMERCIAL BUSINESS		
Project Name: RUFUS DEERING LUMBER		
Existing Building Size: 15,056 sq. ft.	Proposed Building Size: 28,921 sq. ft.	
Existing Acreage of Site: 122,323 sq. ft.	Proposed Acreage of Site: 122,323 sq. ft.	
Proposed Total Disturbed Area of the Site: 41,420 sq. ft. *		
* If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) or Chapter 500, Stormwater Management Permit with the Maine Department of Environmental Protection (DEP).		
Tax Assessor's Chart, Block & Lot: Chart # 042 Block # A Lot # 001	Property Owners Name/ Mailing address: RUFUS DEERING LUMBER 383 COMMERCIAL ST PORTLAND, ME 04101 c/o Dan Labrie	Telephone #: 772-6505 Cell Phone #:
Consultant/Agent Name, Mailing Address, Telephone #, Fax # and Cell Phone #: JAMES SEYMOUR c/o SERENO TECHNICS 1 CHABOT ST / PO BOX 1339 WESTBROOK, ME 04098 PH: 856-0277	Applicant's Name/ Mailing Address: SAME AS OWNER	Telephone #: Cell Phone #:
Fee for Service Deposit (all applications) <input checked="" type="checkbox"/> (\$200.00)		
Proposed Development (check all that apply)		
<input checked="" type="checkbox"/> New Building <input type="checkbox"/> Building Addition <input type="checkbox"/> Change of Use <input type="checkbox"/> Residential <input type="checkbox"/> Office <input checked="" type="checkbox"/> Retail <input type="checkbox"/> Manufacturing <input type="checkbox"/> Warehouse/Distribution <input type="checkbox"/> Parking lot <input type="checkbox"/> Subdivision (\$500.00) + amount of lots _____ (\$25.00 per lot) \$ _____ + major site plan fee if applicable <input type="checkbox"/> Site Location of Development (\$3,000.00) (except for residential projects which shall be \$200.00 per lot _____) <input type="checkbox"/> Traffic Movement (\$1,000.00) <input type="checkbox"/> Storm water Quality (\$250.00) <input type="checkbox"/> Section 14-403 Review (\$400.00 + \$25.00 per lot) <input type="checkbox"/> Other _____		
~ Please see next page ~		

Major Development (more than 10,000 sq. ft.)

- Under 50,000 sq. ft. (\$500.00)
- 50,000 - 100,000 sq. ft. (\$1,000.00)
- Parking Lots over 100 spaces (\$1,000.00)
- 100,000 - 200,000 sq. ft. (\$2,000.00)
- 200,000 - 300,000 sq. ft. (\$3,000.00)
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Plan Amendments

- Planning Staff Review (\$250.00)
- Planning Board Review (\$500.00)

Billing Address: (name, address and contact information)


Submittals shall include **seven (7) folded** packets containing of the following materials:

- A. Copy of the application.
- B. Cover letter stating the nature of the project.
- C. Written Submittal (Sec. 14-525 2. (c), including evidence of right, title and interest.
- D. A standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 100 feet.
- E. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
- E. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
- F. In addition to the seven (7) sets of documents listed above, one (1) set of the site plans reduced to 11 x 17 must be submitted.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov. Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for site review only; a Performance Guarantee, Inspection Fee, Building Permit Application and associated fees will be required prior to construction.

Signature of Applicant: 	Date: 6-11-08
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Site Plan Checklist Portland, Maine

Department of Planning and Development, Planning Division and Planning Board

RUFUS DEERING LUMBER - 383 COMMERCIAL ST
Project Name, Address of Project

Application Number

The form is to be completed by the Applicant or Designated Representative:

Check Submitted	Site Plan Item	Required Information	Section 14-525 (b,c)
✓	(1)	Standard boundary survey (stamped by a registered surveyor, at a scale of not less than 1 inch to 100 feet and including:	1
✓	(2)	Name and address of applicant and name of proposed development	a
✓	(3)	Scale and north points	b
✓	(4)	Boundaries of the site	c
✓	(5)	Total land area of site	d
✓	(6)	Topography - existing and proposed (2 feet intervals or less)	e
✓	(7)	Plans based on the boundary survey including:	2
NA	(8)	Existing soil conditions	a
NA	(9)	Location of water courses, wetlands, marshes, rock outcroppings and wooded areas	b
✓	(10)	Location, ground floor area and grade elevations of building and other structures existing and proposed, elevation drawings of exterior facades, and materials to be used	c
✓	(11)	Approx location of buildings or other structures on parcels abutting the site and a zoning summary of applicable dimensional standards (example page 9 of packet)	d
✓	(12)	Location of on-site waste receptacles	e
✓	(13)	Public utilities	e
✓	(14)	Water and sewer mains	e
✓	(15)	Culverts, drains, existing and proposed, showing size and directions of flows	e
✓	(16)	Location and dimensions, and ownership of easements, public or private rights-of-way, both existing and proposed	f
✓	(17)	Location and dimensions of on-site pedestrian and vehicular access ways	g
✓	(18)	Parking areas	g
✓	(19)	Loading facilities	g
✓	(20)	Design of ingress and egress of vehicles to and from the site onto public streets	g
✓	(21)	Curb and sidewalks	g
✓	(22)	Landscape plan showing:	h
✓	(23)	Location of existing vegetation and proposed vegetation	h
✓	(24)	Type of vegetation	h
✓	(25)	Quantity of plantings	h
✓	(26)	Size of proposed landscaping	h
NA	(27)	Existing areas to be preserved	h
NA	(28)	Preservation measures to be employed	h
✓	(29)	Details of planting and preservation specifications	h
✓	(30)	Location and dimensions of all fencing and screening	i
TBD	(31)	Location and intensity of outdoor lighting system	j
✓	(32)	Location of fire hydrants, existing and proposed (refer to Fire Department checklist)	k
✓	(33)	Written statements to include:	c
✓	(34)	Description of proposed uses to be located on site	cl
NA	(35)	Quantity and type of residential, if any	cl
✓	(36)	Total land area of the site	c2
✓	(37)	Total floor area, total disturbed area and ground coverage of each proposed Building and structure	c2
NA	(38)	General summary of existing and proposed easements or other burdens	c3
✓	(39)	Type, quantity and method of handling solid waste disposal	c4
✓	(40)	Applicant's evaluation or evidence of availability of off-site public facilities, including sewer, water and streets	c5
✓	(41)	Description of existing surface drainage and a proposed stormwater management plan or description of measures to control surface runoff.	c6 c6

✓ _____ NA	(42)	An estimate of the time period required for completion of the development	7
_____ NA	(43)	A list of all state and federal regulatory approvals to which the development may be subject to. Include the status of any pending applications, anticipated timeframe for obtaining such permits, or letters of non-jurisdiction.	8
_____ NA	(47)	Evidence of financial and technical capability to undertake and complete the development including a letter from a responsible financial institution stating that it has reviewed the planned development and would seriously consider financing it when approved.	h8
✓ _____ NA	(48)	Evidence of applicant's right title or interest, including deeds, leases, purchase options or other documentation.	
_____ NA	(49)	A description of any unusual natural areas, wildlife and fisheries habitats, or archaeological sites located on or near the site.	
_____ NA	(50)	A jpeg or pdf of the proposed site plan, if available.	
_____ NA	(51)	Final sets of the approved plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (*.dwg), release AutoCAD 2005 or greater.	

Note: Depending on the size and scope of the proposed development, the Planning Board or Planning Authority may request additional information, including (but not limited to):

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <ul style="list-style-type: none"> - drainage patterns and facilities - erosion and sedimentation controls to be used during construction - a parking and/or traffic study - emissions - a wind impact analysis | <ul style="list-style-type: none"> - an environmental impact study - a sun shadow study - a study of particulates and any other noxious - a noise study |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

Other comments:

June 11, 2008
07383

Ms. Barbara Barhydt, Senior Planner
City of Portland Planning Division,
City Hall 4th Floor
389 Congress St,
Portland, ME 04101

Major Site Plan Application Submittal –Site Redevelopment Plan
Rufus Deering Lumber Company's - Proposed Drive-Thru Warehouse and Retail Store
383 Commercial St. Portland, Maine

Dear Ms. Barhydt:

On behalf of Rufus Deering Lumber Company, please find nine (9) copies of the Site Plan Application and associated design plans for the Proposed Drive-Thru Warehouse and Retail Store, to be located on the street block surrounded by Commercial Street, High Street, York Street, and Maple Street in Portland, Maine. The parcel proposed for redevelopment is the existing Rufus Deering Lumber store and lumberyard site (383 Commercial St). The property consists of 2.83 acres of fully developed lumberyard, which is an allowed land use in the B-5b (Urban Commercial Business Zone).

In existence since 1854, the company has determined that to compete in their market they must become more efficient and provide customers with a more convenient method of receiving products. Hence, Rufus Deering has proposed the following application and plans, for the renovation of the existing red barn and re-development of the yard area. The renovation work associated with the proposed retail building will occur over approximately 10,650 square feet of existing building footprint and include minor parking lot improvements and reorganization of the outside storage areas for lumber and building materials/products. Yard redevelopment will include the construction of an approximately 29,000 square foot drive-through warehouse. Additionally, we will be required to reconfigure a section of their current storage shelter adjacent to Commercial Street, in order to accommodate access and internal circulation to the proposed warehouse.

Due to the limited size and scope of the project, we do not anticipate that any Maine Department of Environmental Protection (MDEP) permits will be required. The overall improvement of the site will not add any additional impervious areas, as the site is now nearly completely impervious. In fact, the proposed landscaping areas will reduce the total impervious area slightly.

Stormwater is a difficult design given the poor existing soils, limited depth for positive pipe drainage, and difficulty making connections into Commercial Street given the multiple conflicts with existing utilities. Based on the lumberyard's necessary layout, we felt the only practical measure will be to maintain existing drainage patterns and collection areas where possible (although drainage is shallow), or continue to allow sheet flow into the Commercial Street drainage system as it currently operates. We feel we have addressed drainage and that, due to the conversion of ground surface into roof area, runoff water quality is improved. We have held meetings with the City Engineer, and have determined there is limited area and grades to accomplish stormwater treatment.

As part of the application we will be improving street access, internal circulation, and providing more landscaping to improve the visual presentation of the Rufus Deering Lumber facility. The original Office building will remain, and the character of the renovated Red Barn will attempt to match the characteristics of the original store. Aside from stormwater, we do not anticipate many major upgrades to the site other than providing new utility connections for power, and upgrading the water services for fire protection for the new building/additions. The Applicant will continue to contract with a solid waste removal service.

An "Ability to Serve" letter has been received from The Portland Water District, a copy of which is included in this application. A similar letter has been requested from the Portland Sewer Division, and will be forwarded upon receipt. In planning the water main upgrades we received indications from the Fire Department for the need of a fire hydrant on the ocean side of York Street, such that in the event of fire on the site, hydrant access could be made to the site without shutting down York Street/High Street through-traffic from the bridge. To accommodate that request we are proposing a hydrant on the High Street and York Street corner, which taps into a newer 12 inch main. The section of water main in York Street is smaller and antiquated, and a hydrant installation could be hampered by the existing seawall. There are currently two accessible hydrants along the property's frontage on Commercial Street, and another at the intersection of York Street and Maple Street.

Rufus Deering Lumber Company will be holding an informational meeting and required public meetings with abutters, and we expect to be meeting again with both the Planning and Public Works Departments to review the overall plans. Given that the sidewalks are in poor condition on High Street and a section of Maple, we have assumed that as part of our plan those will be required to be reconstructed with granite curbing and brick surface. However, no sidewalk currently exists along this section of York Street and, due to the presence of the original City seawall, construction of a sidewalk in this location is impractical. The seawall creates restrictions with respect to the spacing needed to design a safe sidewalk, the constructability of a sidewalk due to the seawall materials, and the need to install a guardrail system into the existing stone wall. In lieu of a sidewalk, we are offering to landscape this section with plant vegetation/hedge. We feel that this will enhance the visual character of the street and proposed site.

Because the proposed improvements are intended to increase the efficiency and organization of the site (and not to expand the business), the owner does not expect an increase in overall customer traffic, as a main component of their business is contractor related. Therefore, we do not anticipate the need for any traffic improvements, or traffic study. We will, however, provide an analysis of entrance operations, internal delivery movements, and the need to reduce some on-street parking to improve sight safety at the site.

Rufus Deering Lumber Company is anticipating construction start date in Spring of 2009. We are looking forward to working with City staff to facilitate approvals in coordination with Portland Site Plan approval. We will be providing architectural renderings and lighting plans as details of the site become available.

In the interim, we are available at your request to hold any type of application review meeting or to meet the City's review staff to present the project on an informal basis. We feel that, given site design parameters, this would be beneficial for both the City planners and the Rufus Deering Lumber Company design team. If you have any questions please feel free to contact me at our office (856-0277). We look forward to meeting with the Planning Division and the Planning Board for a workshop meeting as soon as an agenda allows.

Sincerely,

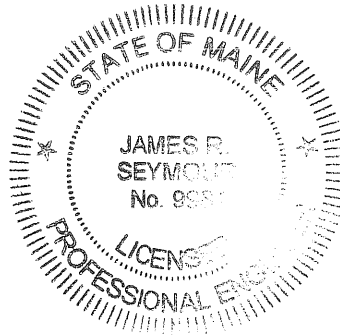
SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager

JRS:cb
Enc.

cc: Jay Breard- Rufus Deering Lumber Company





Development Review Application Portland, Maine

Department of Planning and Development, Planning Division and Planning Board

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Zone: B-5b, URBAN COMMERCIAL BUSINESS		
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Consultant/Agent Name, Mailing Address, Telephone #, Fax # and Cell Phone #:	Applicant's Name/ Mailing Address:	Telephone #: Cell Phone #:
JAMES SEYMOUR /O SERAGO TECHNICS 1 CHASE ST / PO BOX 1339 WESTBROOK, ME 04098 PH: 856-6277	SAME AS OWNER	
Fee for Service Deposit (all applications) <input checked="" type="checkbox"/> (\$200.00)		
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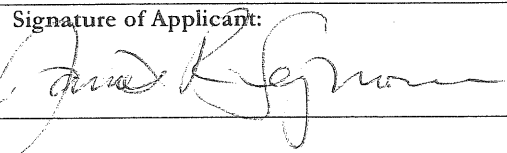
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Signature of Applicant:



Date:

6-11-08



Site Plan Checklist Portland, Maine

Department of Planning and Development, Planning Division and Planning Board

RUFUS DEERING LUMBER - 383 COMMERCIAL ST

Project Name, Address of Project

Application Number

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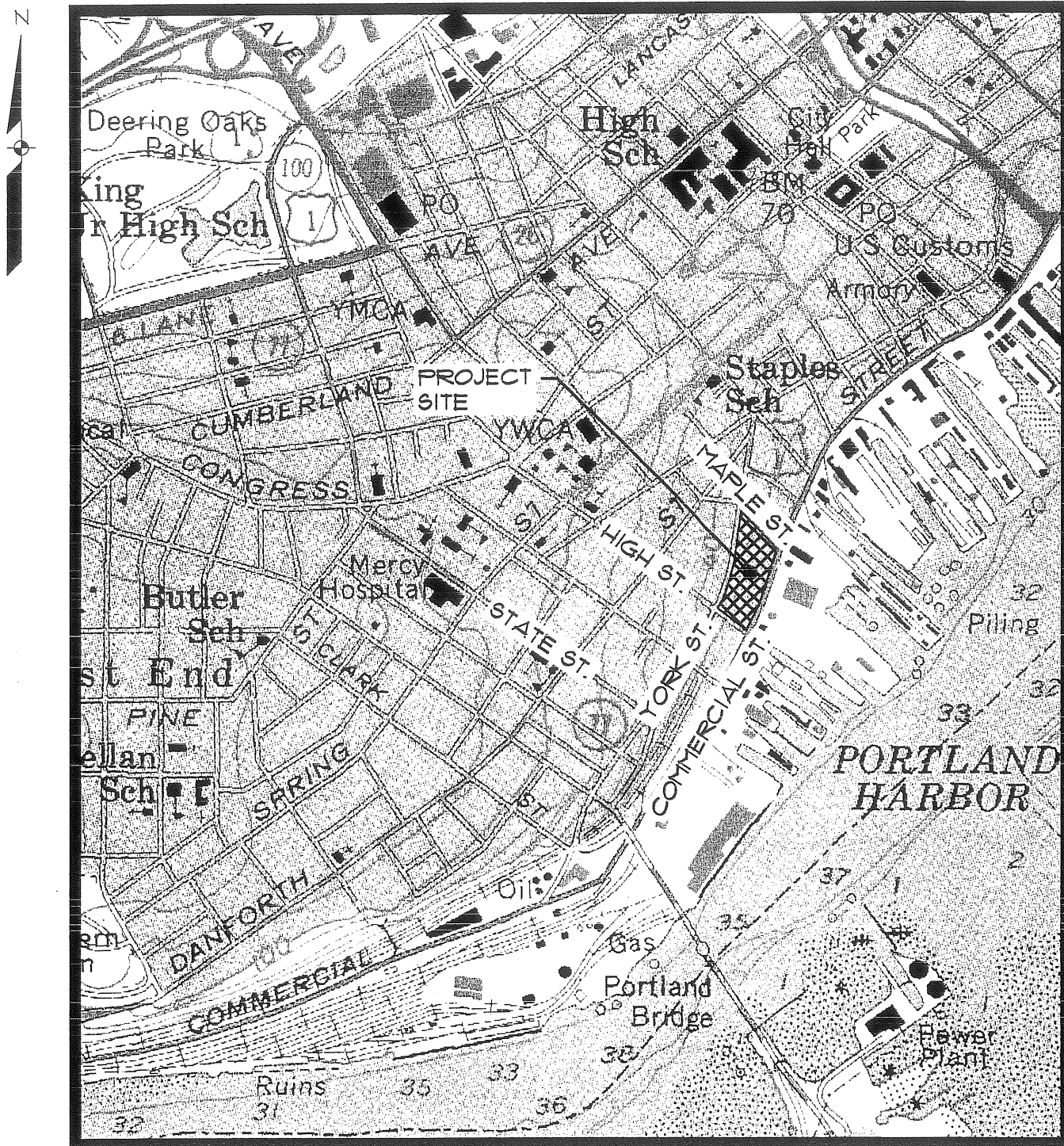
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- | | |
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|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

Other comments:

FIGURE 1



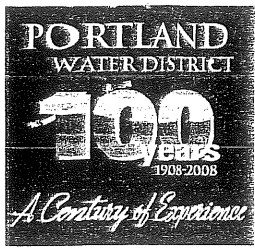
SITE LOCATION MAP
USGS TOPOGRAPHIC
7.5 MIN. QUADRANGLE
PORTLAND WEST
SCALE: 1"=1000'

Sebago Technics

Engineering Expertise You Can Build On

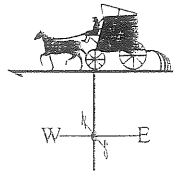
One Chabot Street
Westbrook, Me 04098-1339
Tel (207) 856-0277





225 DOUGLASS STREET
P.O. BOX 3553
PORTLAND, ME 04104-3553
P: 207.774.5961
F: 207.523.5440
WWW.PWD.ORG

PORTLAND
WATER DISTRICT



RECEIVED

JUNE 4 2008

SEBAGO TECHNICOS

June 4, 2008

Sebago Technics
One Chabot Street
Westbrook, ME 04098-1339

Attn: Patrick M. Martin, Design Engineer
Re: Rufus Deering Lumber, 383 Commercial Street, Portland
Ability to serve with PWD water

Dear Mr. Martin:

This letter is to confirm that there should be an adequate supply of clean and healthful water to serve the needs of the commercial lumber yard at 383 Commercial Street in Portland. According to District records, there is a 12-inch ductile iron water main on the southwest side of High Street as well as 4 hydrants located adjacent to the site.

The current data from a nearby hydrant indicates there should be adequate capacity of water to serve the needs of your proposed project, as stated in your letter, dated May 29, 2008.

Hydrant Location: Commercial Street, 225' north of High Street
Hydrant Number: POD-HYD00062
Static Pressure: 109 psi
Flow: 1635 gpm
Last Tested: 4/27/2004

Please notify your mechanical engineer of these results so that they can design your system to best fit the noted conditions. Because the static pressure at the main is above the limit for which the Maine State Plumbing Code recommends installing pressure reducing valves, PWD recommends consulting with your mechanical engineer to determine if they should be used on your project.

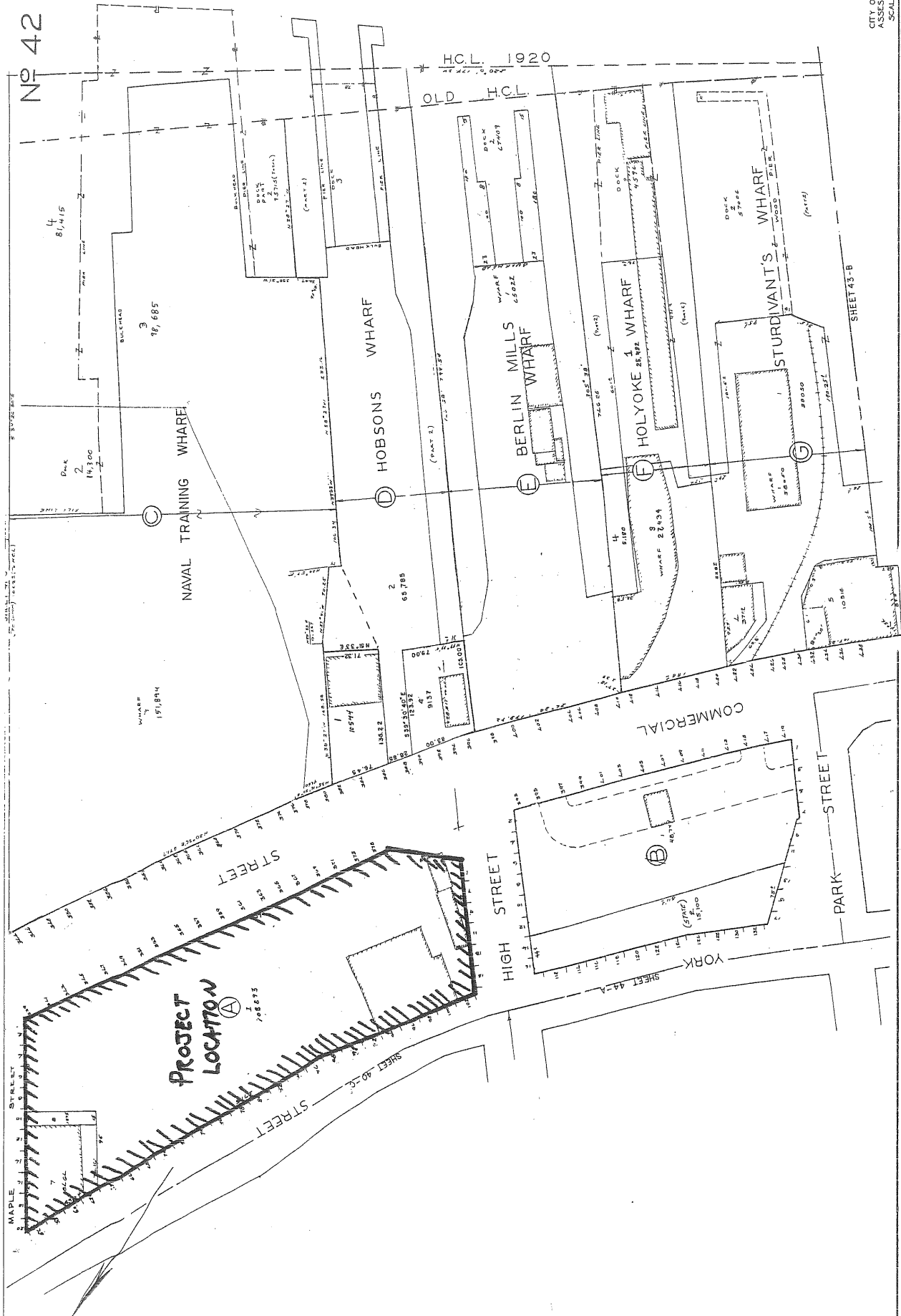
Because there is an existing hydrant on the southwest corner of High Street and York Street, it is unnecessary to install another hydrant as shown on your Grading and Utilities Plan, unless requested by the Portland Fire Department. Also, the 3/4-inch PVC domestic service is not acceptable in the public right-of-way. PWD requires a minimum of 1-inch copper to be installed to the service valve, located 6 inches from the property line in the public right-of-way. PVC pipe is an acceptable material to use on private land, from the service valve to the building. Any existing services to this property that are not going to be reused must be shut and cut at the main. If the District can be of further assistance in this matter, please let us know.

Sincerely,
Portland Water District

A handwritten signature in black ink, appearing to read 'R. Spugnardi', written in a cursive style.

Rico Spugnardi, P.E.
Business Development Engineer
rspugnardi@pwd.org

No 42



10/24

X NOTE
ETALS 8
STAIR PL298

Doc# 19840 Bk#22473 Ps: 397

QUITCLAIM DEED WITH COVENANT

KNOW ALL BY THESE PRESENTS, That I, **WILLIAM M. MOODY, JR.**, of Yarmouth, Cumberland County, Maine, for consideration paid, grant to **MILLIKEN SMITH BLOCK, LLC**, a Maine limited liability company, with a mailing address of 383 Commercial Street, Portland, Maine 04101, with Quitclaim Covenant, a one-tenth (1/10) interest in common and undivided, in and to a certain lot or parcel of land situated in Portland, Cumberland County, Maine, described more particularly as follows:

Beginning at the intersection of the northwesterly sideline of Commercial Street with the southwestery sideline of Maple Street;

Thence northwesterly in said southwestery sideline of Maple Street a distance of one hundred ten (110) feet, more or less, to the easterly corner of a strip of land fifteen (15) feet wide conveyed by the Portland Terminal Company to the Family Laundry, Inc. by deed dated May 26, 1938, recorded in the Cumberland County Registry of Deeds in Book 1555, Page 24, said strip of land to be used as a passageway in common with the Portland Terminal Company, its successors and assigns forever;

Thence southwestery in the southeasterly sideline of said strip of land a distance of eighty-five (85) feet to the southerly corner of said strip of land, said southerly corner being also the southerly corner of land conveyed by the Portland Terminal Company to said Family Laundry, Inc. by said deed dated May 26, 1938;

Thence northwesterly in said southwestery sideline of said Family Laundry land a distance of ninety-five (95) feet, more or less, to the southeasterly sideline of York Street;

Thence southwestery in said southeasterly sideline of said York Street five hundred sixteen (516) feet, more or less, to the northeasterly sideline of High Street;

Thence southeasterly in said northeasterly sideline of High Street a distance of one hundred sixty-two (162) feet, more or less, to the westerly corner of a triangle of land conveyed by Boston and Maine Railroad, predecessor in title to the Portland Terminal Company, to the City of

MAP 42
LOT 1

Portland by deed dated May 21, 1874 and recorded in said Registry of Deeds in Book 410, Page 473;

Thence northeasterly in the northwesterly sideline of said triangle of land a distance of ninety-eight (98) feet, more or less, to its intersection with said northwesterly sideline of Commercial Street;

Thence northeasterly in said northwesterly sideline of said Commercial Street four hundred seventy (470) feet, more or less, to the point of beginning.

The Grantor hereby also conveys to the Grantee, its successors and assigns, any right, title or interests she may have to lands abutting the above described parcel of land between the northeasterly sideline of High Street and the centerline of High Street and between the northwesterly sideline of Commercial Street and the centerline of Commercial Street.

ALSO conveying to the Grantee herein, all rights of the Grantor herein to use the passageway located on the strip of land as described in a deed from the Portland Terminal Company to Family Laundry, Inc., dated May 26, 1938 and recorded in the Cumberland County Registry of Deeds in Book 1555, Page 24.

Being the same premises conveyed by Rufus Deering Company to William M. Moody, Jr., et als. by deed dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 82.

This conveyance is made SUBJECT TO:

1. Mortgage and Security Agreement from William M. Moody, Jr., et als. to Rufus Deering Company dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 85.

2. Memorandum of Lease by and between William M. Moody, Jr., et als. (Lessor) and Rufus Deering Company (Lessee) dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 91.

3. Agreement by and between William M. Moody, Jr., et al. and Rufus Deering Company, et al. dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 93. Pursuant

to Paragraph 3 of said Agreement, unanimous written consent to this conveyance has been obtained from all parties to said Agreement.

Witness my hand and seal this 17th day of Decem, 2004.

Witness:

Jean M. Rolland

William M. Moody, Jr.
William M. Moody, Jr.

STATE OF MAINE
COUNTY OF CUMBERLAND, ss.

On Dec 17th, 2004, personally appeared the above-named William M. Moody, Jr. and acknowledged the foregoing instrument to be his free act and deed.

Before me,

Patricia A. Merritt

Notary Public

Printed Name:

My Commission Expires:

PATRICIA A. MERRITT

Notary Public, Maine

My Commission Expires April 1, 2010

SEAL

Received
Recorded Register of Deeds
Mar 30 2005 03:30:49P
Cumberland County
John B. O'Brien

STATE OF MAINE
NOTARY PUBLIC

BK 14115PG013

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.
By: Werner Schnetzer
Werner Schnetzer
Its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,

James S. Bueker
Notary Public/Attorney-at-Law
Print Name: James S. Bueker

122895/PSB/SKLF
91401.4926

MAP 42
LOT 7 & 8

BK 14115 PG 014

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1990 SEP -1 PM 4:48

CUMBERLAND COUNTY

John B O'Brien



June 11, 2008
07383

- 2 existing curb cuts along Commercial.
- 2 proposed
- angled parking existing along Commercial

- this will be a problem

Ms. Barbara Barhydt, Senior Planner
City of Portland Planning Division,
City Hall 4th Floor
389 Congress St,
Portland, ME 04101

Tom - close access near High St. make
Commercial 2-way

York St sidewalk - would be used but is there room?
↳ 44' wide. Could be narrowed. Would this
be their resp. or
ours? Could be
costly

Major Site Plan Application Submittal - Site Redevelopment Plan
Rufus Deering Lumber Company's - Proposed Drive-Thru Warehouse and Retail Store
383 Commercial St. Portland, Maine

- get information on seawall

- paid info?
inf. acct? or
warrant?
- will contrib to

Dear Ms. Barhydt:

On behalf of Rufus Deering Lumber Company, please find nine (9) copies of the Site Plan Application and associated design plans for the Proposed Drive-Thru Warehouse and Retail Store, to be located on the street block surrounded by Commercial Street, High Street, York Street, and Maple Street in Portland, Maine. The parcel proposed for redevelopment is the existing Rufus Deering Lumber store and lumberyard site (383 Commercial St). The property consists of 2.83 acres of fully developed lumberyard, which is an allowed land use in the B-5b (Urban Commercial Business Zone).

In existence since 1854, the company has determined that to compete in their market they must become more efficient and provide customers with a more convenient method of receiving products. Hence, Rufus Deering has proposed the following application and plans, for the renovation of the existing red barn and re-development of the yard area. The renovation work associated with the proposed retail building will occur over approximately 10,650 square feet of existing building footprint and include minor parking lot improvements and reorganization of the outside storage areas for lumber and building materials/products. Yard redevelopment will include the construction of an approximately 29,000 square foot drive-through warehouse. Additionally, we will be required to reconfigure a section of their current storage shelter adjacent to Commercial Street, in order to accommodate access and internal circulation to the proposed warehouse.

Due to the limited size and scope of the project, we do not anticipate that any Maine Department of Environmental Protection (MDEP) permits will be required. The overall improvement of the site will not add any additional impervious areas, as the site is now nearly completely impervious. In fact, the proposed landscaping areas will reduce the total impervious area slightly.

Stormwater is a difficult design given the poor existing soils, limited depth for positive pipe drainage, and difficulty making connections into Commercial Street given the multiple conflicts with existing utilities. Based on the lumberyard's necessary layout, we felt the only practical measure will be to maintain existing drainage patterns and collection areas where possible (although drainage is shallow), or continue to allow sheet flow into the Commercial Street drainage system as it currently operates. We feel we have addressed drainage and that, due to the conversion of ground surface into roof area, runoff water quality is improved. We have held meetings with the City Engineer, and have determined there is limited area and grades to accomplish stormwater treatment.

As part of the application we will be improving street access, internal circulation, and providing more landscaping to improve the visual presentation of the Rufus Deering Lumber facility. The original Office building will remain, and the character of the renovated Red Barn will attempt to match the characteristics of the original store. Aside from stormwater, we do not anticipate many major upgrades to the site other than providing new utility connections for power, and upgrading the water services for fire protection for the new building/additions. The Applicant will continue to contract with a solid waste removal service.

An "Ability to Serve" letter has been received from The Portland Water District, a copy of which is included in this application. A similar letter has been requested from the Portland Sewer Division, and will be forwarded upon receipt. In planning the water main upgrades we received indications from the Fire Department for the need of a fire hydrant on the ocean side of York Street, such that in the event of fire on the site, hydrant access could be made to the site without shutting down York Street/High Street through-traffic from the bridge. To accommodate that request we are proposing a hydrant on the High Street and York Street corner, which taps into a newer 12 inch main. The section of water main in York Street is smaller and antiquated, and a hydrant installation could be hampered by the existing seawall. There are currently two accessible hydrants along the property's frontage on Commercial Street, and another at the intersection of York Street and Maple Street.

Rufus Deering Lumber Company will be holding an informational meeting and required public meetings with abutters, and we expect to be meeting again with both the Planning and Public Works Departments to review the overall plans. Given that the sidewalks are in poor condition on High Street and a section of Maple, we have assumed that as part of our plan those will be required to be reconstructed with granite curbing and brick surface. However, no sidewalk currently exists along this section of York Street and, due to the presence of the original City seawall, construction of a sidewalk in this location is impractical. The seawall creates restrictions with respect to the spacing needed to design a safe sidewalk, the constructability of a sidewalk due to the seawall materials, and the need to install a guardrail system into the existing stone wall. In lieu of a sidewalk, we are offering to landscape this section with plant vegetation/hedge. We feel that this will enhance the visual character of the street and proposed site.

Because the proposed improvements are intended to increase the efficiency and organization of the site (and not to expand the business), the owner does not expect an increase in overall customer traffic, as a main component of their business is contractor related. Therefore, we do not anticipate the need for any traffic improvements, or traffic study. We will, however, provide an analysis of entrance operations, internal delivery movements, and the need to reduce some on-street parking to improve sight safety at the site.

Rufus Deering Lumber Company is anticipating construction start date in Spring of 2009. We are looking forward to working with City staff to facilitate approvals in coordination with Portland Site Plan approval. We will be providing architectural renderings and lighting plans as details of the site become available.

In the interim, we are available at your request to hold any type of application review meeting or to meet the City's review staff to present the project on an informal basis. We feel that, given site design parameters, this would be beneficial for both the City planners and the Rufus Deering Lumber Company design team. If you have any questions please feel free to contact me at our office (856-0277). We look forward to meeting with the Planning Division and the Planning Board for a workshop meeting as soon as an agenda allows.

Sincerely,

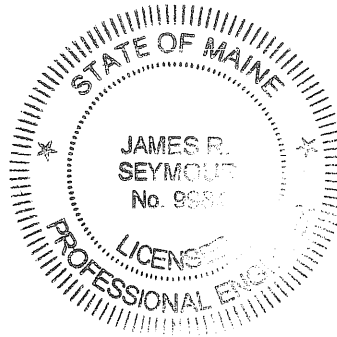
SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager

JRS:cb
Enc.

cc: Jay Breard- Rufus Deering Lumber Company





Development Review Application Portland, Maine

Department of Planning and Development, Planning Division and Planning Board

Address of Proposed Development: 383 COMMERCIAL STREET PORTLAND, ME		
Zone: B-5b, URBAN COMMERCIAL BUSINESS		
Project Name: RUFUS DEERING LUMBER		
Existing Building Size: 15,056 sq. ft.	Proposed Building Size: 28,961 sq. ft.	
Existing Acreage of Site: 122,363 sq. ft.	Proposed Acreage of Site: 122,363 sq. ft.	
Proposed Total Disturbed Area of the Site: 41,420 sq. ft. *		
* If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) or Chapter 500, Stormwater Management Permit with the Maine Department of Environmental Protection (DEP).		
Tax Assessor's Chart, Block & Lot: Chart # 042 Block # A Lot # 001	Property Owners Name/ Mailing address: RUFUS DEERING LUMBER 383 COMMERCIAL ST PORTLAND, ME 04101 C/O DAN LABRIE	Telephone #: 772-6505 Cell Phone #:
Consultant/Agent Name, Mailing Address, Telephone #, Fax # and Cell Phone #: JAMES SEYMOUR C/O SERENO TECHNICS 1 CHABOT ST / PO BOX 1359 WESTBROOK, ME 04098 PH: 856-6277	Applicant's Name/ Mailing Address: SAME AS OWNER	Telephone #: Cell Phone #:
Fee for Service Deposit (all applications) <input checked="" type="checkbox"/> (\$200.00)		
Proposed Development (check all that apply)		
<input checked="" type="checkbox"/> New Building ___ Building Addition ___ Change of Use ___ Residential ___ Office <input checked="" type="checkbox"/> Retail ___ Manufacturing ___ Warehouse/Distribution ___ Parking lot ___ Subdivision (\$500.00) + amount of lots _____ (\$25.00 per lot) \$ _____ + major site plan fee if applicable ___ Site Location of Development (\$3,000.00) (except for residential projects which shall be \$200.00 per lot _____) ___ Traffic Movement (\$1,000.00) ___ Storm water Quality (\$250.00) ___ Section 14-403 Review (\$400.00 + \$25.00 per lot) ___ Other _____		
~ Please see next page ~		

Major Development (more than 10,000 sq. ft.)

- Under 50,000 sq. ft. (\$500.00)
- 50,000 - 100,000 sq. ft. (\$1,000.00)
- Parking Lots over 100 spaces (\$1,000.00)
- 100,000 - 200,000 sq. ft. (\$2,000.00)
- 200,000 - 300,000 sq. ft. (\$3,000.00)
- Over 300,000 sq. ft. (\$5,000.00)
- After-the-fact Review (\$1,000.00 + applicable application fee)

Minor Site Plan Review

- Less than 10,000 sq. ft. (\$400.00)
- After-the-fact Review (\$1,000.00 + applicable application fee)

Plan Amendments

- Planning Staff Review (\$250.00)
- Planning Board Review (\$500.00)

Billing Address: (name, address and contact information)

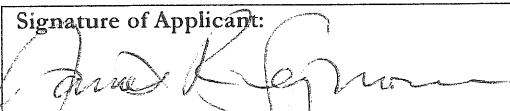
Submittals shall include seven (7) folded packets containing of the following materials:

- A. Copy of the application.
- B. Cover letter stating the nature of the project.
- C. Written Submittal (Sec. 14-525 2. (c), including evidence of right, title and interest.
- D. A standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 100 feet.
- E. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
- E. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
- F. In addition to the seven (7) sets of documents listed above, one (1) set of the site plans reduced to 11 x 17 must be submitted.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for site review only; a Performance Guarantee, Inspection Fee, Building Permit Application and associated fees will be required prior to construction.

Signature of Applicant: 	Date: 6-11-08
----------------------------------------------------------------------------------------------------------------	------------------



Site Plan Checklist Portland, Maine

Department of Planning and Development, Planning Division and Planning Board

RUFUS DEERING LUMBER - 383 COMMERCIAL ST

Project Name, Address of Project

Application Number

The form is to be completed by the Applicant or Designated Representative:

Check Submitted	Site Plan Item	Required Information	Section 14-525 (b,c)
✓	(1)	Standard boundary survey (stamped by a registered surveyor, at a scale of not less than 1 inch to 100 feet and including:	1
✓	(2)	Name and address of applicant and name of proposed development	a
✓	(3)	Scale and north points	b
✓	(4)	Boundaries of the site	c
✓	(5)	Total land area of site	d
✓	(6)	Topography - existing and proposed (2 feet intervals or less)	e
✓	(7)	Plans based on the boundary survey including:	2
NA	(8)	Existing soil conditions	a
NA	(9)	Location of water courses, wetlands, marshes, rock outcroppings and wooded areas	b
✓	(10)	Location, ground floor area and grade elevations of building and other structures existing and proposed, elevation drawings of exterior facades, and materials to be used	c
✓	(11)	Approx location of buildings or other structures on parcels abutting the site and a zoning summary of applicable dimensional standards (example page 9 of packet)	d
✓	(12)	Location of on-site waste receptacles	e
✓	(13)	Public utilities	e
✓	(14)	Water and sewer mains	e
✓	(15)	Culverts, drains, existing and proposed, showing size and directions of flows	e
✓	(16)	Location and dimensions, and ownership of easements, public or private rights-of-way, both existing and proposed	f
✓	(17)	Location and dimensions of on-site pedestrian and vehicular access ways	g
✓	(18)	Parking areas	g
✓	(19)	Loading facilities	g
✓	(20)	Design of ingress and egress of vehicles to and from the site onto public streets	g
✓	(21)	Curb and sidewalks	g
✓	(22)	Landscape plan showing:	h
✓	(23)	Location of existing vegetation and proposed vegetation	h
✓	(24)	Type of vegetation	h
✓	(25)	Quantity of plantings	h
✓	(26)	Size of proposed landscaping	h
NA	(27)	Existing areas to be preserved	h
NA	(28)	Preservation measures to be employed	h
✓	(29)	Details of planting and preservation specifications	h
✓	(30)	Location and dimensions of all fencing and screening	i
TBD	(31)	Location and intensity of outdoor lighting system	j
✓	(32)	Location of fire hydrants, existing and proposed (refer to Fire Department checklist)	k
✓	(33)	Written statements to include:	c
✓	(34)	Description of proposed uses to be located on site	cl
NA	(35)	Quantity and type of residential, if any	cl
✓	(36)	Total land area of the site	c2
✓	(37)	Total floor area, total disturbed area and ground coverage of each proposed Building and structure	c2
NA	(38)	General summary of existing and proposed easements or other burdens	c3
✓	(39)	Type, quantity and method of handling solid waste disposal	c4
✓	(40)	Applicant's evaluation or evidence of availability of off-site public facilities, including sewer, water and streets	c5
✓	(41)	Description of existing surface drainage and a proposed stormwater management plan or description of measures to control surface runoff.	c6

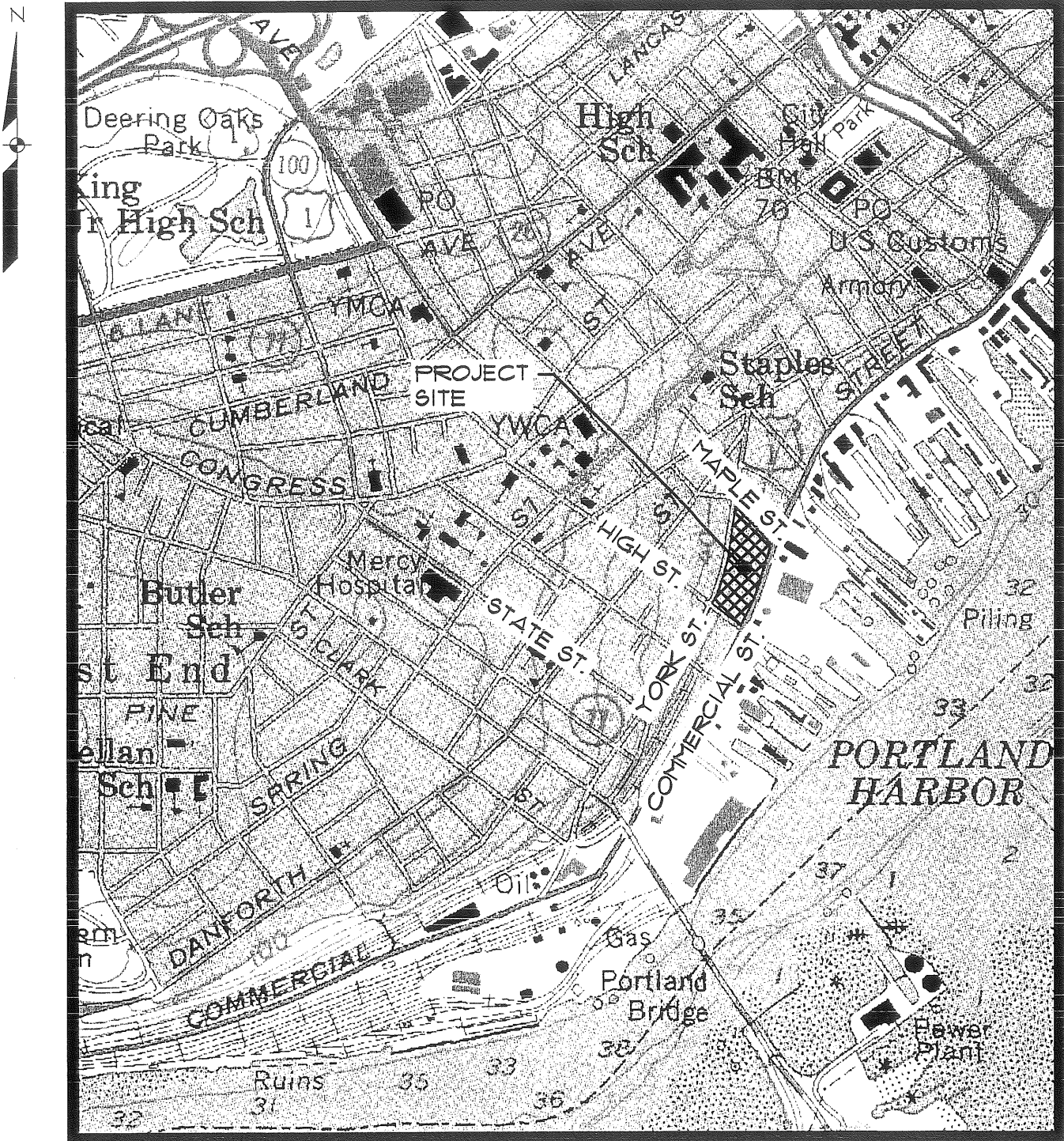
✓ _____	(42)	An estimate of the time period required for completion of the development	7
NA _____	(43)	A list of all state and federal regulatory approvals to which the development may be subject to. Include the status of any pending applications, anticipated timeframe for obtaining such permits, or letters of non-jurisdiction.	8 h8
_____	(47)	Evidence of financial and technical capability to undertake and complete the development including a letter from a responsible financial institution stating that it has reviewed the planned development and would seriously consider financing it when approved.	
✓ _____	(48)	Evidence of applicant's right title or interest, including deeds, leases, purchase options or other documentation.	
NA _____	(49)	A description of any unusual natural areas, wildlife and fisheries habitats, or archaeological sites located on or near the site.	
_____	(50)	A jpeg or pdf of the proposed site plan, if available.	
_____	(51)	Final sets of the approved plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (*.dwg), release AutoCAD 2005 or greater.	

Note: Depending on the size and scope of the proposed development, the Planning Board or Planning Authority may request additional information, including (but not limited to):

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <ul style="list-style-type: none"> - drainage patterns and facilities - erosion and sedimentation controls to be used during construction - a parking and/or traffic study - emissions - a wind impact analysis | <ul style="list-style-type: none"> - an environmental impact study - a sun shadow study - a study of particulates and any other noxious - a noise study |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

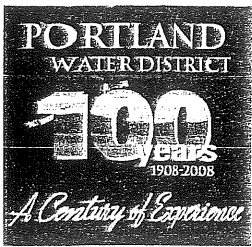
Other comments:

FIGURE 1



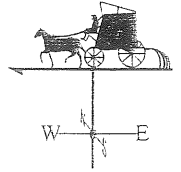
SITE LOCATION MAP
USGS TOPOGRAPHIC
7.5 MIN. QUADRANGLE
PORTLAND WEST
SCALE: 1"=1000'





225 DOUGLASS STREET
P.O. BOX 3553
PORTLAND, ME 04104-3553
P: 207.774.5961
F: 207.523.5440
WWW.PWD.ORG

0703
PORTLAND
WATER DISTRICT



RECEIVED

JUN 14 2008

SEBAGO TECHNICOS

June 4, 2008

Sebago Technics
One Chabot Street
Westbrook, ME 04098-1339

Attn: Patrick M. Martin, Design Engineer
Re: Rufus Deering Lumber, 383 Commercial Street, Portland
Ability to serve with PWD water

Dear Mr. Martin:

This letter is to confirm that there should be an adequate supply of clean and healthful water to serve the needs of the commercial lumber yard at 383 Commercial Street in Portland. According to District records, there is a 12-inch ductile iron water main on the southwest side of High Street as well as 4 hydrants located adjacent to the site.

The current data from a nearby hydrant indicates there should be adequate capacity of water to serve the needs of your proposed project, as stated in your letter, dated May 29, 2008.

Hydrant Location: Commercial Street, 225' north of High Street
Hydrant Number: POD-HYD00062
Static Pressure: 109 psi
Flow: 1635 gpm
Last Tested: 4/27/2004

Please notify your mechanical engineer of these results so that they can design your system to best fit the noted conditions. Because the static pressure at the main is above the limit for which the Maine State Plumbing Code recommends installing pressure reducing valves, PWD recommends consulting with your mechanical engineer to determine if they should be used on your project.

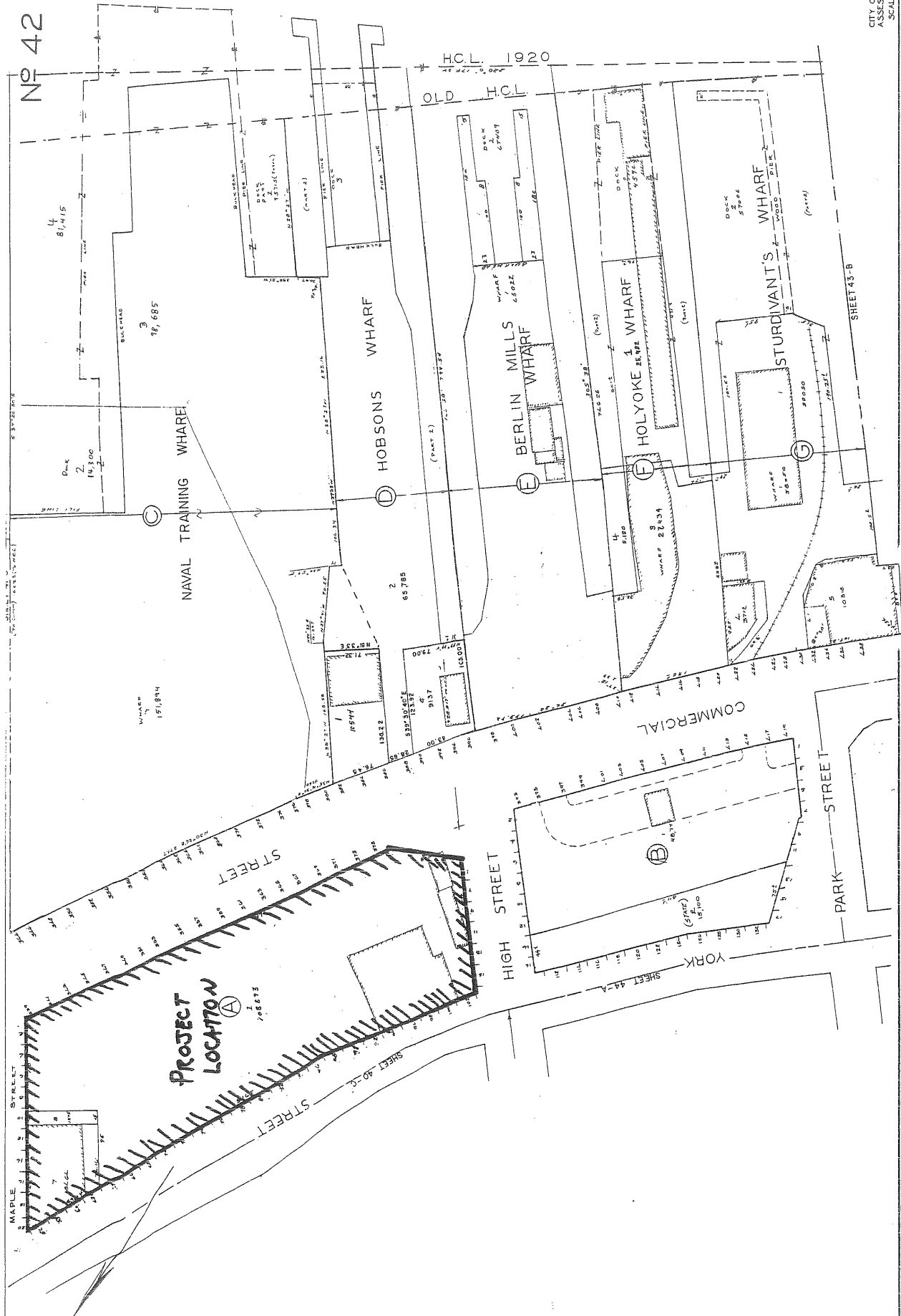
Because there is an existing hydrant on the southwest corner of High Street and York Street, it is unnecessary to install another hydrant as shown on your Grading and Utilities Plan, unless requested by the Portland Fire Department. Also, the 3/4-inch PVC domestic service is not acceptable in the public right-of-way. PWD requires a minimum of 1-inch copper to be installed to the service valve, located 6 inches from the property line in the public right-of-way. PVC pipe is an acceptable material to use on private land, from the service valve to the building. Any existing services to this property that are not going to be reused must be shut and cut at the main. If the District can be of further assistance in this matter, please let us know.

Sincerely,
Portland Water District

A handwritten signature in black ink, appearing to read 'R. Spugnardi', written in a cursive style.

Rico Spugnardi, P.E.
Business Development Engineer
rspugnardi@pwd.org

NO 42



SHEET 43-B

COMMERCIAL

HIGH STREET

PARK STREET

YORK STREET

PROJECT LOCATION

HOBSONS WHARF

BERLIN MILLS WHARF

HOLYOKE WHARF

STURDIVANT'S WHARF

NAVAL TRAINING WHARF

H.C.L. 1920

Deck 2
14,300

171,844

98,685

63,785

7,900

9,137

13,332

83,000

16,500

10,000

10,000

10,000

10,000

10,000

10,000

10,000

10,000

10,000

10,000

10,000

10,000

X NOTE
ETALS
START PLATE

QUITCLAIM DEED WITH COVENANT

KNOW ALL BY THESE PRESENTS, That I, WILLIAM M. MOODY, JR., of Yarmouth, Cumberland County, Maine, for consideration paid, grant to MILLIKEN SMITH BLOCK, LLC, a Maine limited liability company, with a mailing address of 383 Commercial Street, Portland, Maine 04101, with Quitclaim Covenant, a one-tenth (1/10) interest in common and undivided, in and to a certain lot or parcel of land situated in Portland, Cumberland County, Maine, described more particularly as follows:

Beginning at the intersection of the northwesterly sideline of Commercial Street with the southwestery sideline of Maple Street;

Thence northwesterly in said southwestery sideline of Maple Street a distance of one hundred ten (110) feet, more or less, to the easterly corner of a strip of land fifteen (15) feet wide conveyed by the Portland Terminal Company to the Family Laundry, Inc. by deed dated May 26, 1938, recorded in the Cumberland County Registry of Deeds in Book 1555, Page 24, said strip of land to be used as a passageway in common with the Portland Terminal Company, its successors and assigns forever;

Thence southwestery in the southeasterly sideline of said strip of land a distance of eighty-five (85) feet to the southerly corner of said strip of land, said southerly corner being also the southerly corner of land conveyed by the Portland Terminal Company to said Family Laundry, Inc. by said deed dated May 26, 1938;

Thence northwesterly in said southwestery sideline of said Family Laundry land a distance of ninety-five (95) feet, more or less, to the southeasterly sideline of York Street;

Thence southwestery in said southeasterly sideline of said York Street five hundred sixteen (516) feet, more or less, to the northeasterly sideline of High Street;

Thence southeasterly in said northeasterly sideline of High Street a distance of one hundred sixty-two (162) feet, more or less, to the westerly corner of a triangle of land conveyed by Boston and Maine Railroad, predecessor in title to the Portland Terminal Company, to the City of

MAP 42
LOT 1

Portland by deed dated May 21, 1874 and recorded in said Registry of Deeds in Book 410, Page 473;

Thence northeasterly in the northwesterly sideline of said triangle of land a distance of ninety-eight (98) feet, more or less, to its intersection with said northwesterly sideline of Commercial Street;

Thence northeasterly in said northwesterly sideline of said Commercial Street four hundred seventy (470) feet, more or less, to the point of beginning.

The Grantor hereby also conveys to the Grantee, its successors and assigns, any right, title or interests she may have to lands abutting the above described parcel of land between the northeasterly sideline of High Street and the centerline of High Street and between the northwesterly sideline of Commercial Street and the centerline of Commercial Street.

ALSO conveying to the Grantee herein, all rights of the Grantor herein to use the passageway located on the strip of land as described in a deed from the Portland Terminal Company to Family Laundry, Inc., dated May 26, 1938 and recorded in the Cumberland County Registry of Deeds in Book 1555, Page 24.

Being the same premises conveyed by Rufus Deering Company to William M. Moody, Jr., et als. by deed dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 82.

This conveyance is made SUBJECT TO:

1. Mortgage and Security Agreement from William M. Moody, Jr., et als. to Rufus Deering Company dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 85.

2. Memorandum of Lease by and between William M. Moody, Jr., et als. (Lessor) and Rufus Deering Company (Lessee) dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 91.

3. Agreement by and between William M. Moody, Jr., et al. and Rufus Deering Company, et al. dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 93. Pursuant

to Paragraph 3 of said Agreement, unanimous written consent to this conveyance has been obtained from all parties to said Agreement.

Witness my hand and seal this 17th day of Decem, 2004.

Witness:

Joan M. Rolland

William M. Moody, Jr.
William M. Moody, Jr.

STATE OF MAINE
COUNTY OF CUMBERLAND, ss.

On Dec 17th, 2004, personally appeared the above-named William M. Moody, Jr. and acknowledged the foregoing instrument to be his free act and deed.

Before me,

Patricia A. Merritt

Notary Public

Printed Name:

My Commission Expires:

PATRICIA A. MERRITT

Notary Public, Maine

My Commission Expires April 1, 2010

SEAL

Received
Recorded Register of Deeds
Mar 30 2005 03:20:49P
Cumberland County
John B. O'Brien

STATE OF MAINE
601-3 2005

BK 14115PG013

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.
By: [Signature]
Werner Schnetzer
Its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,

[Signature]
Notary Public/Attorney-at-Law
Print Name: Paul S. Bussard

122895/PSB/SKLF
91401.4926

MAP 42
LOT 7 & 8

BK 14115 PG 14

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

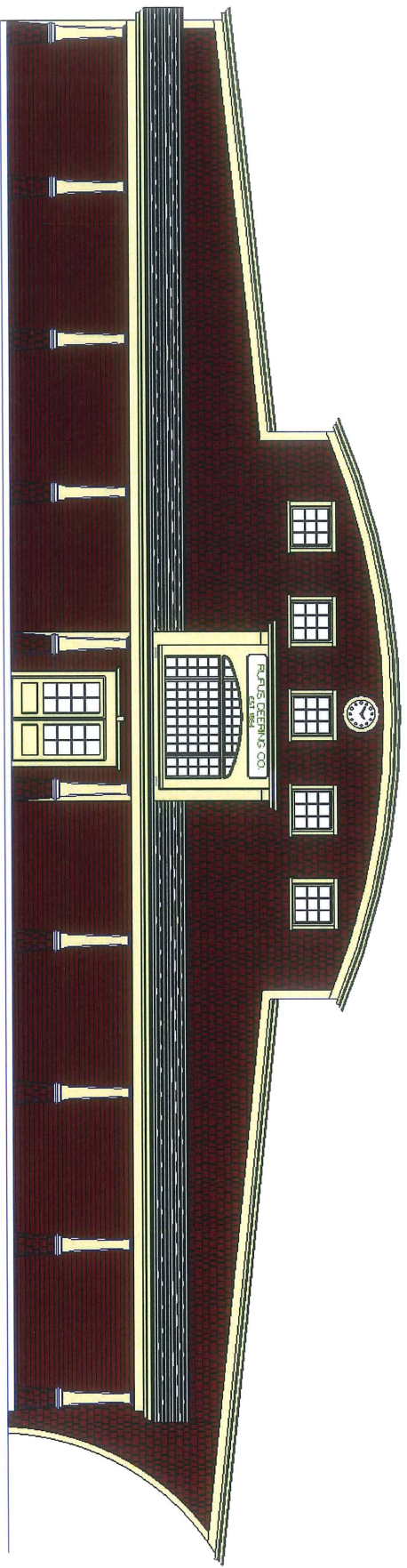
There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

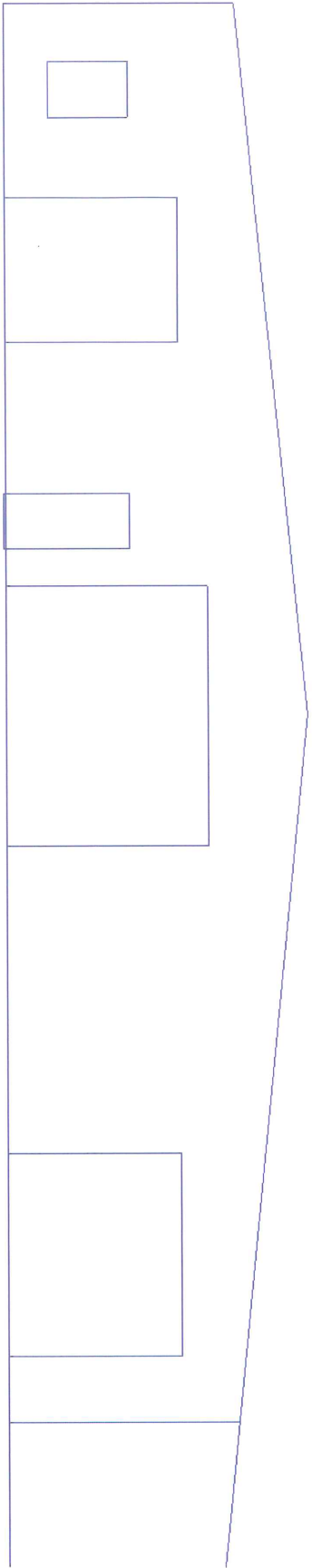
1990 SEP -1 PH 4: 48

CUMBERLAND COUNTY

John B. O'Brien



FRONT ELEVATION



CITY OF PORTLAND, MAINE

PLANNING BOARD

Janice E. Tevanian, Chair
David Silk, Vice Chair
Bill Hall
Joe Lewis
Lee Lowry, III
Shalom Odokara
Michael J. Patterson

Mr. James Seymour
Sebago Technics
1 Chabot Street
P.O. Box 1339
Westbrook, Maine 04098

Mr. Dan Labrie
Rufus Deering Lumber Company
383 Commercial Street
Portland, Maine 04101

December 9, 2008

RE: Rufus Deering Lumber Company. 383 Commercial Street
Application ID #: 2008-0082
CBL: 042-A-001

Dear Mr. Seymour and Mr. Labrie,

On November 25, 2008, the Portland Planning Board considered a major site plan application submitted by Rufus Deering Lumber Company to develop a drive-through lumber warehouse with associated site improvements. The Planning Board reviewed the proposal for conformance with the standards of the Site Plan Ordinance. The Planning Board voted unanimously (7-0) to approve the application with the following motions, waivers and conditions as presented below.

WAIVERS:

1. The Planning Board voted unanimously (7-0) to waive the requirement of Section III of the City's Technical Design Standards that the maximum driveway width for a two way commercial driveway be 30 feet, thus permitting a 36 ft driveway with an 18 ft ingress lane and an 18 ft egress lane.
2. The Planning Board voted unanimously (7-0) to waive the requirement of Section 25-96 of the City Code that the applicant install sidewalk along the section of York Street abutting the development, based on the following waiver criteria as outlined below:

Sec.14 –506 (b) Modifications.

Sidewalks-

1. *There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site.*
2. *A safe alternative-walking route is reasonably available, for example, by way of a sidewalk on the other side of the street.*

3. *Strict adherence to the sidewalk requirement would result in the loss of significant site features related to landscaping or topography that are deemed to be of a greater public value.*

SITE PLAN REVIEW:

The Planning Board voted unanimously (7-0) that the plan is in conformance with the site plan standards of the Land Use Code, subject to the following condition(s) of approval:

1. The applicant shall submit revised plans showing the curbing and bicycle lane engineered to City standards along York Street for review and approval by the Planning Authority.
2. The applicant shall submit elevation drawings for the proposed retail showroom for review and approval by the planning authority prior to the issuance of a building permit.
3. The submitted lighting and photometric information shall be reviewed and approved by the planning authority prior to the issuance of a building permit.
4. The applicant shall submit revised plans showing one additional Japanese tree lilac planted in the existing empty tree well along Commercial Street for review and approval by the City Arborist prior to the issuance of a building permit.
5. The applicant shall contribute \$20,000 to a City infrastructure account to be allocated towards improvements to the stormwater outfall on the Berlin Mill Wharf prior to the issuance of a Certificate of Occupancy. If the abovementioned improvement is not built within ten (10) years, these funds will be refunded to the applicant.
6. The applicant contribute \$3,900.00 to a City infrastructure account to be allocated towards the installation of a traffic signal at the Commercial and High Street intersection prior to the issuance of a Certificate of Occupancy. If the abovementioned improvement is not built within ten (10) years, these funds will be refunded to the applicant.

The approval is based on the submitted plans and the findings related to site plan and subdivision review standards as contained in Planning Report #62-08, which is attached.

Please note the following provisions and requirements for all site plan and subdivision approvals:

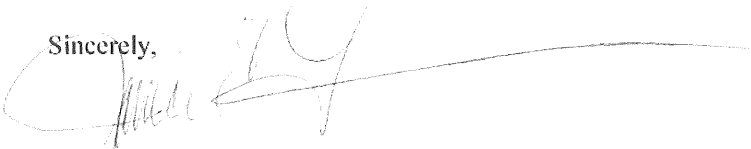
1. The site shall be developed and maintained as depicted in the site plan and the written submission of the applicant. Modification of any approved site plan or alteration of a parcel which was the subject of site plan approval after May 20, 1974, shall require the prior approval of a revised site plan by the Planning Board or the planning authority pursuant to the terms of this article. Any such parcel lawfully altered prior to the enactment date of these revisions shall not be further altered without approval as provided herein. Modification or alteration shall mean and include any deviations from the approved site plan including, but not limited to, topography, vegetation and impervious surfaces shown on the site plan. No action, other than an amendment approved by the planning authority or Planning Board, and field changes approved by the Public Services authority as provided herein, by any authority or department shall authorize any such modification or alteration.

2. The above approvals do not constitute approval of building plans, which must be reviewed and approved by the City of Portland's Inspection Division.
3. A performance guarantee covering the site improvements as well as an inspection fee payment of 2.0% of the guarantee amount and seven (7) final sets of plans must be submitted to and approved by the Planning Division and Public Services Dept. prior to the release of a building permit, street opening permit or certificate of occupancy for site plans.
4. The site plan approval will be deemed to have expired unless work in the development has commenced within one (1) year of the approval or within a time period agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the expiration date.
5. Final sets of plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (*.dwg), release AutoCAD 2005 or greater.
6. A defect guarantee, consisting of 10% of the performance guarantee, must be posted before the performance guarantee will be released.
7. Prior to construction, a pre-construction meeting shall be held at the project site with the contractor, development review coordinator, Public Service's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the pre-construction meeting.
8. If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)

Philip DiPierro, Development Review Coordinator, must be notified five (5) working days prior to date required for final site inspection. The Development Review Coordinator can be reached at 874-8632. Please make allowances for completion of site plan requirements determined to be incomplete or defective during the inspection. This is essential as all site plan requirements must be completed and approved by the Development Review Coordinator prior to issuance of a Certificate of Occupancy. Please schedule any property closing with these requirements in mind.

If there are any questions, please contact Molly Cato, Senior Planner at (207) 874- 8901

Sincerely,



Janice Tevanian, Chair
Portland Planning Board

Attachments:

2. Planning Board Report #62-08
3. Performance Guarantee Packet

Electronic Distribution:

Penny St. Louis Littell, Director of Planning and Urban Development

Alexander Jaegerman, Planning Division Director

Barbara Barhydt, Development Review Services Manager

Molly Casto, Senior Planner

Philip DiPierro, Development Review Coordinator

Marge Schmuckal, Zoning Administrator

Tammy Munson, Inspections Division Director

Lisa Danforth, Administrative Assistant

Michael Bobinsky, Public Services Director

Kathi Earley, Public Works

Bill Clark, Public Works

Michael Farmer, Public Works

Jim Carmody, City Transportation Engineer

Jane Ward, Public Works

Captain Greg Cass, Fire Prevention

Jeff Tarling, City Arborist

Tom Errico, Wilbur Smith Consulting Engineers

Dan Goyette, Woodard & Curran

Assessor's Office

Approval Letter File

Hard Copy: Project File

PLANNING BOARD REPORT #62-08

RUFUS DEERING LUMBER COMPANY
VICINITY OF 383 COMMERCIAL STREET
SITE PLAN REVIEW
RUFUS DEERING LUMBER COMPANY, APPLICANT

Submitted to:

Portland Planning Board
Portland, Maine
November 14, 2008

I. INTRODUCTION

Rufus Deering Lumber Company has requested Planning Board review and approval of their proposal to redevelop the existing Rufus Deering Lumber store and lumber yard site at 383 Commercial Street. The parcel is located on the block bounded by Commercial Street, High Street, York Street and Maple Street near the Commercial Street waterfront. The development proposal includes renovating the existing "red barn" warehouse building into a retail store, constructing an approximately 29,000 sf drive-through warehouse, associated parking lot improvements and reconfiguring existing on-site lumber and material storage areas. The project is being reviewed as a major site plan in the B5-b zone. The applicant has submitted a partial waiver request to the Planning Board from the installation of curb and sidewalk along York Street as part of their application.

Representatives for the applicant include Sebago Technics and Johnson Design Resources, Architects.

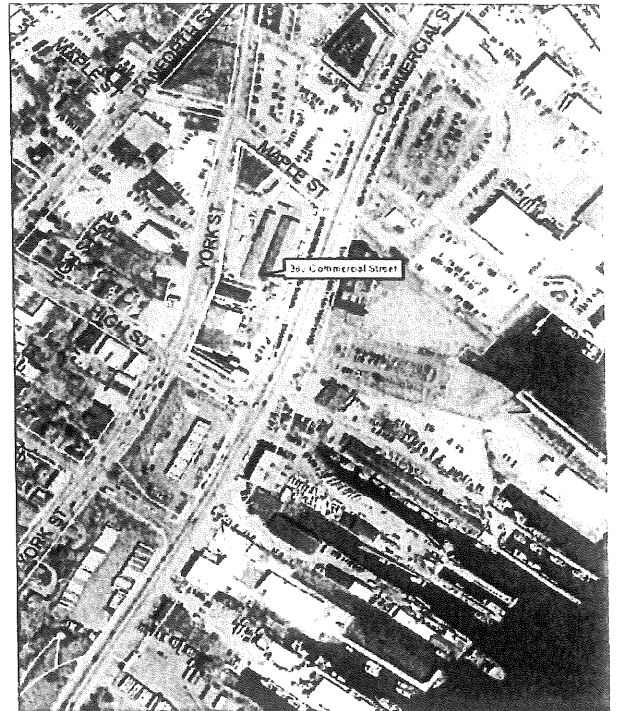


Image 1- Site Location

106 notices were sent to area residents. A notice also appeared in the November 17th and November 18th editions of the *Portland Press Herald*.

II. SUMMARY OF FINDINGS

Zoning:	B5-b Urban Commercial Business Zone
Parcel Size:	122,363 SF (2.8 acres)
Required Vehicle/Bicycle Parking:	None
Proposed Parking:	30 vehicle spaces
Uses:	Commercial lumber store and lumber yard
Street frontage:	567 feet (Commercial Street) 262.5 feet (Maple Street) 620 feet (York Street) 172.5 feet (High Street)
Sq. footage- retail bldg:	10,650 sq. ft. footprint
Sq. footage- drive-thru warehouse:	28961 sq. ft. footprint

III. PROPOSED DEVELOPMENT

Rufus Deering proposes to update and expand their existing commercial lumber yard and retail operation. According to the applicant, the primary rationale behind the redevelopment is to improve efficiency and safety. In their correspondence with the planning authority, the applicant notes that the yard has become increasingly antiquated, creating excessive losses due to cull

lumber, boards that become weathered, split and unusable. The goal of the project is to combat this by becoming increasingly efficient and to get more of their product stored under cover (Attachments 2 and 14). The business has been located on or in the vicinity of this site along the northerly side of Commercial Street since 1854. The parcel occupies the entire block bounded by Commercial, High, York and Maple Streets. York Street runs the length of the westerly side (rear) of the property, approximately 25 feet above the parcel on top of the existing stone sea wall. The site includes a lumber yard with three (3) open-sided storage warehouses totaling 18,376 sq. ft., two (2) partially to fully enclosed storage warehouses totaling 17,726 sq. ft. and two (2) office buildings at the north and south corners of the block. These office buildings are 2,998 sq. ft and 7,540.2 sq. ft ground floor area, respectively. The 7,540.2 sq. ft office building at the northern corner of the block is owned by Rufus Deering but is not part of the Rufus Deering lumber yard operation and is not under review as part of this development proposal.

The proposed development includes the following: Two of the existing open sided warehouses, the existing Quonset hut and a portion of the third open-sided warehouse will be demolished to allow for the development of a 27.5 ft tall, 28,961 sq. ft drive-through warehouse. A portion of the third open-sided warehouse will be rebuilt and re-oriented to accommodate revised access and internal circulation for the site. A small existing storage shed along the Commercial Street frontage will also be relocated on the site. The 11,404 sq. ft. "red barn" warehouse at the westerly corner of the site, near York and High streets will be remodeled into a 10,648 sq. ft. retail store. There are no changes proposed for the existing 3,000 sq. ft office building at the south corner of the parcel near the intersection of Commercial and High Streets. The applicant proposes to add 30 on-site parking spaces... The applicant proposes to consolidate the two existing curb cuts along Commercial Street into a single 36 ft wide access drive. The existing curb cut along Maple Street would remain open but would be restricted to deliveries only and would be secured with a ten (10) ft chain link sliding gate.

IV. THRESHOLD QUESTIONS FOR THE BOARD'S CONSIDERATION

A. Request for a waiver from the requirements of Section 25-96 that curb and sidewalk be installed along the York Street frontage: Pursuant to Section 25-96 of the City Code, the development is required to install sidewalks and granite curbing on all abutting streets. This site encompasses an entire city block, thus this provision applies to all four frontages. The parcel has approximately 620 feet of frontage along York Street along the rear of the site. The rear of the parcel consists of an existing granite sea wall, approximately 25 feet tall, with York Street running along the top, thus there is no access to the site from York Street. York Street has a sidewalk on the west (opposite) side of the street only and the existing granite curbing on the east side along the Rufus Deering property is in poor condition with low reveal.

The applicant has requested a waiver from the requirements to install brick sidewalk along their York Street frontage (see Attachment 2). As an alternative, they propose to install new granite curbing and to mark out a five (5) foot wide bike lane along that frontage, providing bicycle infrastructure to supplement the existing pedestrian infrastructure on the opposite side of the street. Section 14-506 of the zoning ordinance pertains to waivers from curb and sidewalk installation. Section V (B) of this report provides additional discussion of this issue.

V. STAFF REVIEW

The proposed development has been reviewed by staff for conformance with the relevant review standards of site plan ordinance and zoning requirements of the B5-b zone. Staff comments are highlighted in this report.

A. Zoning Review:

Lumber and building materials dealers in existence before the passage of the ordinance are permitted in the B5-b zone. Rufus Deering Lumber Company has been in existence at this site for approximately 150 years, making it a permitted use. Marge Schmuckal, Zoning Administrator has reviewed the proposal and her comments are included as Attachment 3a and 3b.

The proposed modifications to the site would bring it into greater conformance with current ordinance requirements by lessening existing nonconformities, namely that structures exceed the maximum ten (10) ft front yard setback requirement for the B-5b zone. The existing site includes a front yard setback of approximately 30 feet. This is being reduced under the current proposal to approximately 16 feet. The Zoning Ordinance encourages such lessening of nonconformity on already developed sites.

Section 14-230.5 lists performance standards for the B5-b zone. According to this section, storage of new materials, finished products, or related equipment shall be suitably screened from the public way and from abutting properties by a solid fence at least five (5) feet in height or by a solid evergreen planting strip. The applicant has addressed conformance with this provision in their letter, dated August 22, 2008 (Attachment 2). The applicant comments that day to day operations of a lumberyard make it necessary to store lumber and building materials outdoors. Typically, these materials are stacked upwards of sixteen (16) feet high. This existing condition would not change as a result of their development proposal. The performance standard requiring screening of such materials with a five (5) foot fence would be inadequate for screening of this particular type of outdoor storage. While the applicant would be willing to provide opaque 5 ft fencing along the Commercial Street frontage if necessary to comply with Zoning, Rufus Deering buildings and buildings in the vicinity have been vandalized in the past and there is concern that opaque fencing would become a regular target of vandalism. Marge Schmuckal comments in her review that, because of the existing and historic use of this site, she does not think the full force of this provision applies. Marge has determined that the existing nonconformities have been lessened and the property has come into better compliance with zoning as a result of this proposal.

Elevation drawings of the proposed drive-thru warehouse have been included as Attachment 13. The maximum building height in the B5-b zone is 65 feet. According to the applicant, the proposed drive-through warehouse will not exceed 27.5 feet in height, well below that permitted by zoning. Elevation drawings have not yet been submitted for the proposed renovations to the red barn warehouse into a retail showroom. The applicant proposes to design the façade to match that of the abutting office building at the corner of Commercial and High Street.

Proposed Condition of Approval: Planning staff proposes a condition of approval that the applicant submits elevation drawings for the proposed retail showroom for review and approval by the planning authority prior to the issuance of a building permit.

No proposals for new signage have been included as part of this application. Separate permits shall be required for any new signage.

B. Site Plan Review:

(1)(2)(24) Traffic, Parking and Circulation:

As stated above, the applicant is asking for a curb and sidewalk waiver for this project. Please see the applicant's statement included as Attachment 2. Pursuant to Section 25-96 of the City Code, this development is required to install sidewalks and granite curbing on all abutting streets. This site encompasses a city block thus, this provision applies to all four frontages. Currently,

Commercial Street is constructed with granite curbing and concrete sidewalk. Maple Street is partially constructed with granite curbing and brick sidewalk with no sidewalk on the opposite side of the street. York Street has no sidewalk and granite curbing that is in poor condition with low reveal. There is sidewalk along the opposite side of York Street of various materials and conditions. High Street has bituminous sidewalk and curbing along both sides of the street. The applicant proposes to install new brick sidewalk and curbing along High Street, along the remaining portions of Maple Street and on Commercial Street where the existing driveway will be closed.

The applicant has requested a waiver from the requirements to install brick sidewalk along the York Street frontage. Instead, they offer to install new granite curbing and to mark out a five (5) foot wide bike lane along that frontage. Section 14-506 of the zoning ordinance pertains to waivers from curb and sidewalk installation and states the following:

Sec.14 –506 (b) Modifications.

(b) Where the Planning Board or planning authority finds that, for each of the requirements listed below, two or more of the conditions exist with respect to compliance with the requirements set forth in sections 14-498 and 14-499 pertaining to the provision and construction of curbs and/or sidewalks, it may vary the regulations so that substantial justice may be done and the public interest secured:

Sidewalks-

1. There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site.
2. There is no sidewalk in existence or expected within 1000 feet and the construction of sidewalks does not contribute to the development of a pedestrian oriented infrastructure.
3. A safe alternative-walking route is reasonably available, for example, by way of a sidewalk on the other side of the street.
4. The reconstruction of the street is specifically identified in the first or second year of the current Capital Improvement Program.
5. The street has been constructed or reconstructed without sidewalks within the last 24 months.
6. Strict adherence to the sidewalk requirement would result in the loss of significant site features related to landscaping or topography that are deemed to be of a greater public value.

Please see the attached letter from Sebago Technics, Inc where they present how the applicant may meet the requirements for waivers (Attachment 2). The applicant proposes that they meet criteria (3) and (6) for a sidewalk waiver. Points raised by Sebago Technics are summarized as follows:

1. A safe alternative walking route is available by way of a sidewalk on the opposite side of York Street.
2. Strict adherence to the sidewalk requirement would result in the loss of significant site features related to topography that are deemed to be of greater public value. The seawall is a historic and permanent feature of the site. It's presence at the edge of the right of way makes it unsafe to install sidewalk closer to the wall's edge and could jeopardize stability of pole supports and guardrails.

3. If the sidewalk were, as an alternative, to be installed within the current paved section of roadway (thus narrowing the existing travel lanes), the sidewalk could not be constructed with an esplanade and would only be four (4) feet wide with a five (5) foot bike lane.
4. The necessity to relocate an existing catch basin would further tighten the separation of underground infrastructure along York Street.
5. Improvements could necessitate upgrades to the current guardrail system to bring it up to a safe elevation for the protection of City snowplowing equipment, adding additional significant cost to the development.
6. Constricting the geometry of York Street could create increase traffic queuing at the intersection of York and High Streets and would create a "pinch point" of approximately 30 ft for three lanes.

As previously stated, the applicant proposes to replace the existing curbing with new granite curbing and to paint a five (5) foot bike lane. They note that the street experiences a significant amount of bicycle traffic that is not currently accommodated. The elimination of the sidewalk on this side of York Street would permit safer lane widths, eliminate the need to re-signal the pedestrian crossing or modify existing drainage infrastructure and would provide a necessary safe link for bicycles navigating from the Casco Bay Bridge or the West End to the Old Port. The applicant has submitted color aerial plans depicting their proposal versus the requirement that a brick sidewalk and bike lane be constructed (Attachment 12). The aerial is intended to illustrate the applicant's point that the geometry of York Street could be constricted and might result in a "pinch point" at the intersection of York and High Streets.

Dan Goyette, Consulting Engineer for the Department of Public Services has reviewed the applicant's request and submitted a memorandum stating:

The applicant has requested a waiver on the sidewalk requirement for York Street. They are partly basing their request on waiver criteria #6. The applicant states that installing the sidewalk as Public Services has requested inboard of the existing curb line, so there by reducing the street width, creates an unsafe condition. Both Tom Errico, Wilbur Smith Associate's traffic consultant to the City, and Jim Carmody, City Traffic Engineer, have reviewed the proposed 4' sidewalk and 5' shoulder/bike lane configuration and are comfortable with the proposal.

Jim Carmody and Tom Errico, City Transportation Engineers conducted a field review of existing roadway characteristics on York Street as they relate to the provision of a sidewalk. It is their professional opinion that the proposed layout as illustrated on the plan is acceptable from a public safety perspective. Provision of a 5-foot bicycle lane would serve bicyclists well and the 4-foot sidewalk would provide an enhanced pedestrian environment as is expected in an urban environment. The alignment of travel lanes would be adequate and the overall roadway cross-section would provide a good balance of multi-modal accommodations (See Tom Errico's review comments- Attachment 8).

The applicant identifies in their submittals that City standards require a significant level of infrastructure contribution and/or installation in the right of way for this development due to its location in the City and multiple frontages. During the review process, the planning authority explored the possibility of including this area of York Street in an upcoming CIP project, however this area is not a priority under the Capital Improvement program at this time. At the request of the planning authority, the applicant submitted estimates of the cost of constructing the York Street sidewalk improvements, as required along with the estimated project cost for constructing the sidewalk improvements along High Street and Maple Street, as proposed (Attachment 9).

According to the applicant's calculations, the base estimated project cost for the York Street sidewalk improvements would be \$88,625.00. Public Services reviewed this estimate and felt that the cost would be somewhat less, with an estimated total of \$73,000.00. According to the applicant's calculations, the total adjusted estimated project cost for all sidewalk improvements, including York Street would be \$147,174.50. As the applicant notes, Rufus Deering has existed at this location on Commercial Street for well over a century (since 1854) and seeks to redevelop, not to expand their business, but in order to remain competitive in their industry at this location. The applicant has determined that the projected cost of constructing a sidewalk along York Street could render their project financially unfeasible. It is not expected that pedestrians coming from or going to the Rufus Deering site will utilize the York Street sidewalk, because there is no possibility of pedestrian access from this frontage. There is some pedestrian traffic on York Street within this vicinity.

Due to its proximity to the Commercial and High Street intersection, the applicant proposes to close the existing driveway along Commercial Street located nearest to High Street, and establish a two way primary entrance closer to the middle of the Commercial Street property frontage. On-street parking conditions on Commercial Street may have to be modified to accommodate this primary driveway. In addition, during the review process it was determined that the public right of way currently encroaches onto Rufus Deering's property. To remedy this, the applicant proposes to rebuild the corner at High Street and Commercial, reducing the northeastern radius on the corner of Commercial and High Street and to move the sidewalk back into the public right of way. Jim Carmody, City Transportation Engineer and Tom Errico, Consulting Transportation Engineer have reviewed this proposal and determined that correcting this will result in an overall improved condition at that corner (see staff review comments, Attachment 8). Commercial Street is a moratorium street, however, Public Services and Maine DOT have concluded that due to the nature of the improvements, the proposed scope of work can proceed prior to expiration of the moratorium.

Waiver Request and Proposed Condition of Approval: The applicant has requested a waiver for the driveway width of the new primary access along Commercial Street. The proposed new driveway is 36 feet wide. Section III of the City's Technical Design Standards states that the maximum driveway width for a two way commercial driveway is 24 feet. Under special conditions, this width may be extended to 30 feet by the City Traffic Engineer if conditions warrant. Public Services has reviewed the applicant's request and supports the driveway width waiver in order to accommodate access and egress for larger vehicles. It is recommended, however, that the driveway be marked for one wide entry and one wide exit lane only, rather than the three (3) lanes, as shown on the plans (see Tom Errico's review comments, Attachment 8). Planning staff recommends a condition of approval that the final plans be revised to include two (2) lanes only at the primary Commercial Street driveway, consisting of an 18 ft ingress lane and an 18 ft egress lane.

The City of Portland has been collecting monetary contributions for the future installation of a traffic signal at the Commercial Street/High Street intersection. Contributions are calculated based on trip generation from proposed developments. According to the November 5, 2008 analysis prepared by Sebago Technics, Inc (Attachment 13), the proposed project is expected to generate an additional 24 driveway trips during the PM peak hour. Based upon prior development contribution levels, the proposed project should contribute \$3,900.00 towards the future installation of a traffic signal at the Commercial Street/High Street intersection.

Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant contribute \$3,900.00 into a City infrastructure account to be allocated towards the

installation of a traffic signal at the Commercial and High Street intersection prior to the issuance of a Certificate of Occupancy.

The applicant proposes to add 30 parking spaces to the site. The spaces are 10 ft x 20 ft. Tom Errico has reviewed the proposal and supports parking stall sizes that exceed City of Portland Technical Standards due to the type of business activity occurring on the site. No bicycle parking is required for this development proposal due to the fact that there is no minimum parking requirement.

(3)(4) The bulk, location or height of proposed buildings and structures

The proposed drive-thru warehouse is 27.5 feet tall and would not appear to create an adverse impact to abutting uses.

(5)(8) Utilities, stormwater, drainage and erosion control

The applicant submitted confirmation from Portland Water District that there is adequate capacity to serve the development. A letter from the Portland Sewer Division has been requested by the applicant and is pending.

Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant submit evidence of adequate sewer capacity prior to the issuance of a building permit.

The applicant has submitted a stormwater analysis with flow calculations (Attachment 11) along with responses to Public Services review comments concerning stormwater management (Attachment 2-b). The proposal does not add additional impervious area to the site, as the site is currently almost entirely impervious. Proposed landscaping will incrementally reduce the amount of impervious surface at the site however it will remain near 100%. The applicant proposes to maintain existing drainage patterns, allowing sheet flow towards Commercial Street. The existing inverts in Commercial Street limit the proposed drainage improvements to too shallow a depth to install effective treatments. In lieu of onsite treatment, the City has asked that the applicant contribute \$20,000 to an infrastructure account to be allocated towards improvements to the stormwater outfall on the Berlin Mill Wharf, which has begun to fail and needs to be rebuilt. Dan Goyette, Consulting Engineer has provided review comments (Attachment 5). The amount of the contribution was calculated based on the size of the proposed parking area.

Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant contribute \$20,000 to a City infrastructure account to be allocated towards improvements to the stormwater outfall on the Berlin Mill Wharf prior to the issuance of a Certificate of Occupancy.

The applicant proposes to remove snow rather than storing it on site. Note 26 on sheet 4 of the site plan indicates that the applicant will contract with a local firm for removal of snow.

(6)(7) Landscaping

The project proposes to add additional landscaping (daylily) and street trees (Japanese tree lilac) along the Commercial Street frontage. The applicant proposes to add three street trees (ash) along High Street and a planting bed at the corner of High and York Streets. Along Maple Street, the applicant proposes two new street trees (ash) installed with tree grates and landscaping (daylily). Interior to the site, the front (east) façade of the proposed retail building will be landscaped with reed grass and bonica shrubrose. Jeff Tarling, City Arborist has reviewed the latest landscape plan and finds it acceptable but would like the applicant to plant a new street tree in the empty tree well along the Commercial Street frontage (Attachment 6-b). He agrees with

the applicant that street trees along York Street should be avoided if this frontage is to be improved and may not be feasible due to the potential for conflict between the sea wall and tree root systems.

Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant amend the final plans to include one additional Japanese tree lilac planted in the existing empty tree well along Commercial Street prior to the issuance of a building permit.

(9) Lighting

The applicant proposes to update the existing lighting at the site as part of their proposal. They have submitted catalogue cuts or photometric information but these were received subsequent to this report and have not been fully reviewed. Existing lighting on the site includes building mounted open lights that illuminate the parking area.

Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant submits lighting information, including a photometric plan showing that all proposed lighting meets City Technical and Design Standards. These materials shall be reviewed and approved by the planning authority prior to the issuance of a building permit.

(10) Fire Safety

Captain Greg Cass of the Portland Fire Department has reviewed and approved the proposal (Attachment 4). The applicant had originally proposed to add an additional hydrant; however Captain Cass determined that the four (4) existing hydrants adequately serve the site.

(20)(21) Environmental Impact:

The proposal is for the development of an existing developed site. The use will not be changed and the corresponding impact will remain unchanged. The applicant submitted a stormwater analysis for review and proposes to pay into an infrastructure account to offset the stormwater impact from the site. Please refer to Section V (B) - *Utilities, stormwater, drainage and erosion control* for additional discussion.

VI. MOTIONS FOR THE BOARD TO CONSIDER

WAIVERS:

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #62-08 relevant to the Portland's Technical and Design Standards and other regulations, and the testimony presented at the Planning Board hearing:

1. The Planning Board (waives/does not waive) the requirement of Section III of the City's Technical Design Standards that the maximum driveway width for a two way commercial driveway be 30 feet, thus permitting a 36 ft driveway with an 18 ft ingress lane and an 18 ft egress lane.
2. The Planning Board (waives/does not waive) Section 25-96 of the City Code, requiring that the applicant install sidewalk along the section of York Street abutting the development, based on the following waiver criteria as outlined below:

Sec.14 –506 (b) Modifications.

(b) Where the planning board or planning authority finds that, for each of the requirements listed below, two or more of the conditions exist with respect to compliance with the

requirements set forth in sections 14-498 and 14-499 pertaining to the provision and construction of curbs and/or sidewalks, it may vary the regulations so that substantial justice may be done and the public interest secured:

Sidewalks-

- 1. There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site.*
- 2. There is no sidewalk in existence or expected within 1000 feet and the construction of sidewalks does not contribute to the development of a pedestrian oriented infrastructure.*
- 3. A safe alternative-walking route is reasonably available, for example, by way of a sidewalk on the other side of the street.*
- 4. The reconstruction of the street is specifically identified in the first or second year of the current Capital Improvement Program.*
- 5. The street has been constructed or reconstructed without sidewalks within the last 24 months.*
- 6. Strict adherence to the sidewalk requirement would result in the loss of significant site features related to landscaping or topography that are deemed to be of a greater public value.*

SITE PLAN

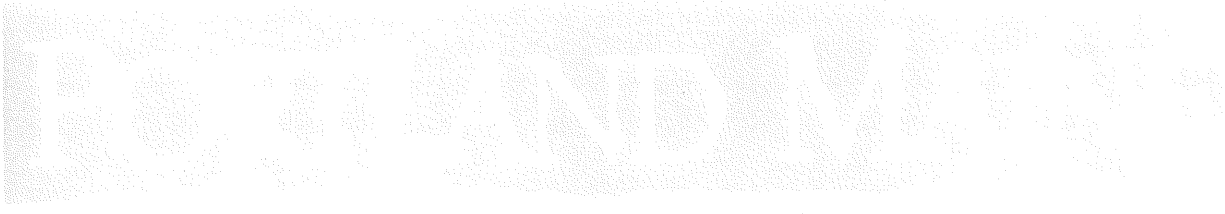
On the basis of plans and materials submitted by the applicant and on the basis of information contained in Planning Report #62-08 relevant to standards for site plan regulations, and other findings, the Planning Board finds that the plan **is / is not** in conformance with the site plan standards of the land use code, subject to the following conditions:

Potential Conditions of Approval:

- 1. If the planning Board requires the construction of sidewalk and a bicycle lane along York Street, the applicant shall submit revised plans showing the sidewalk engineered to City standards along York Street for review and approval by the Planning Authority.*
- 2. The applicant shall submit elevation drawings for the proposed retail showroom for review and approval by the planning authority prior to the issuance of a building permit.*
- 3. The applicant submitted lighting information, including a photometric plan showing that proposed lighting meets City Technical and Design Standards shall be reviewed and approved by the planning authority prior to the issuance of a building permit.*
- 4. The applicant shall submit revised plans showing one additional Japanese tree lilac planted in the existing empty tree well along Commercial Street for review and approval by the City Arborist prior to the issuance of a building permit.*
- 5. The applicant shall contribute \$20,000 to a City infrastructure account to be allocated towards improvements to the stormwater outfall on the Berlin Mill Wharf prior to the issuance of a Certificate of Occupancy.*
- 6. The applicant contribute \$3,900.00 to a City infrastructure account to be allocated towards the installation of a traffic signal at the Commercial and High Street intersection prior to the issuance of a Certificate of Occupancy.*

ATTACHMENTS:

1. Application Materials:
 - a. Letter, dated July 11, 20008
 - b. Development review application
 - c. Site plan checklist
 - d. Site location map
 - e. Capacity letter- Portland Water District, dated June 4, 2008
 - f. Deed
 - g. Neighborhood Meeting documentation
2. Correspondence from the applicant
 - a. Updated written statement and waiver request, dated August 22, 2008
 - b. Letter from the applicant, dated October 15, 2008
3. Zoning Review
 - a. Memorandum from Marge Schmuckal, Zoning Administrator, dated August 27, 2008
 - b. Final comments, dated November 14, 2008
4. Memorandum from Captain Greg Cass, dated August 13, 2008
5. Engineering Review
 - a. Memorandum from Dan Goyette, Consulting Engineer to Public Services, dated July 9, 2008
 - b. Memorandum from Dan Goyette, Consulting Engineer to Public Services, dated September 3, 2008
 - c. Final comments, dated November 3, 2008
6. Landscaping Review
 - a. Memorandum from Jeff Tarling, City Arborist, dated September 5, 2008
 - b. Final comments, dated November 14, 2008
7. Memorandum from David Margolis-Pineo, dated September 5, 2008
8. Transportation Review
 - a. Memorandum from Tom Errico, Consulting Transportation Engineer to Public Services, dated September 4, 2008
 - b. Final comments, dated November 14, 2008
9. Cost estimate spreadsheets submitted by applicant
10. Email concerning cost estimates from David Margolis Pineo, Public Services, dated October 1, 2008
11. Stormwater analysis, submitted by Sebago Technics
12. Aerial plans, showing York Street scenarios, submitted by applicant.
13. Trip generation calculations submitted by Sebago Technics, dated November 5, 2008
14. Letter to Molly Casto from Dan LaBrie, Rufus Deering Lumber, dated November 7, 2008
15. Site plans and elevation drawings
16. Lighting Photometric Plan and Specification Sheet



Strengthening a Remarkable City. Building a Community for Life www.portlandmaine.gov

Planning & Urban Development Department
Penny St. Louis Littell, Director

February 2, 2009

Mr. Daniel LaBrie
Vice President and General Manager
Rufus Deering Lumber Company
383 Commercial Street
Portland, Maine 04104

RE: Extension of Site Plan Approval
Rufus Deering Lumber Company
Application ID #: 2008-0082
CBL: 042-A-001

Dear Mr. LaBrie:

Thank you for submitting a request for an extension of site plan approval for Rufus Deering Lumber Company located at 383 Commercial Street.

Section 14-525 (f) (2) of the Land Use Code states:

Expiration of Approval: A Site plan approved under this article shall expire twelve (12) months from the date of approval unless:

(2) (a) For Industrial, office and retail developments: prior to the expiration of the site plan, such other time period is agreed upon, in writing, by the Planning Authority and the applicant, not to exceed three (3) years from the date of approval;

In my capacity as Planning Division Director for the City of Portland, I am granting your request to extend your site plan approval for three years from the date of approval, to expire on November 15, 2011. If there are any questions, please contact Molly Casto, Senior Planner at (207) 874-8901 or by email at mpe@portlandmaine.gov.

Sincerely,

Alexander Jaegerman, Planning Division Director

Electronic Distribution:

Danielle West-Chuta, Associate Corporation Counsel
Tammy Munson, Inspections Division Director
Barbara Barhydt, Development Review Services Manager
Marge Schmuckal, Zoning Administrator
Phil DiPierro, Development Review Coordinator

July 7, 2008

James Seymour
Sebago Technics
1 Chabot Street
P.O. Box 1339
Westbrook, Maine 04098

RE: Rufus Deering Lumber Company. 383 Commercial Street
Application ID #: 2008-0082
CBL: 042-A-001

Dear Jim:

Thank you for submitting a major site plan application to the City of Portland Planning Division for a proposed development at Rufus Deering Lumber Company at 383 Commercial Street. It is my understanding that the project includes the construction of a drive-thru warehouse and retail store and the reconstruction of an existing warehouse along with associated infrastructure and landscaping improvements. This proposal shall be reviewed according to the standards for major site plan and the requirements of the B5-b zone. Staff review of your submittals has generated the following preliminary review comments:

Zoning:

1. The site is located within a B5-b Urban Commercial Mixed Use Zone. Marge Schmuckal, Zoning Administrator is reviewing the proposal for conformance with applicable zoning provisions. Preliminary review comments suggest that the development proposal is lessening the existing nonconformity of the site. Marge will be submitting individual review comments which, upon receipt, I will forward to you.
2. Section 14-230.5 of the Land Use Code contains performance standards for the B-5 and B5-b zones. Standard (a) states that any storage of new materials, finished products or related equipment must be suitable screened from the public way and form abutting properties by a solid fence at least five (50 feet tall, or by a solid evergreen planting strip. This provision appears to apply to the proposed outside storage areas on the submitted plan, which appears to be visible from Commercial and Maple Streets. Please review subsections (a) through (n) of this section to ensure compliance with these provisions. These include standards pertaining to noise, vibration, odor, storage and lighting.

Site Plan:

1. Please provide ground floor area and grade elevations of all existing and proposed structures.
2. Please provide elevation drawings of all exterior facades, including information on proposed building materials. According to your submitted narrative and per our phone discussion, my understanding is that this information is forthcoming.
3. Please provide a lighting plan including catalogue cuts for any proposed fixtures and a photometric plan.
4. Please identify the location for on-site storage of recyclable materials.

Traffic:

5. Section III of the Technical and Design Standards and Guidelines limits developments to a single 24 ft wide driveway for ingress and egress. Under special circumstances, a driveway width of up to 30 ft will be permitted if the City Transportation Engineer determines that site conditions or vehicle characteristics warrant a wider access. The site plan shows three proposed driveways. In order to have more than one driveway at the site, you'll need to submit a request for a waiver from the Technical and Design Standards to the Planning Board for their consideration. Preliminary recommendations from the City Transportation Engineer include maintaining the driveway along Maple Street for delivery access but closing the driveway on Commercial Street, nearest High Street and making the more northerly Commercial Street driveway a two-way driveway. I would be happy to arrange a meeting with the City Transportation Engineer to discuss the proposal. The City Transportation Engineer will also be submitting individual written review comments which I will forward to you for your review.

Curb and Sidewalks:

6. Section 25-96 of the City Code requires all development requiring site plan approval to construct sidewalk and granite curbing on all abutting accepted streets unless sidewalk and curbing is already in place. The Planning Board has the authority to waive or modify this requirement if the proposal meets two or more of the conditions set forth in Section 14-506(b) of the land use code. Because York Street abuts the property and does not currently have a sidewalk abutting the parcel, this provision does apply. If you intend to pursue a waiver, I recommend reviewing Section 14-506(b) and submitting a detailed description of why the proposal would meet two or more of the conditions, as listed. If you have any questions about this, I'd be happy to discuss it with you further.

Additional Information (written submittals):

7. Please provide the estimated cost of development.
8. Please provide evidence of sewer capacity.
9. Please provide evidence of financial capacity.

As noted, the proposal is undergoing review by the City Zoning Administrator, the City Arborist and the Department of Public Services. I will forward any individual review comments directly to you as they

become available. Please note that the Planning Authority may request additional information during our continued review of the proposal according to applicable laws, ordinances and regulations. If you have any questions, feel free to contact me at 874-8901 or by email at mpc@portlandmaine.gov. I look forward to working with you.

Sincerely,

Molly Casto, Planner

cc: Barbara Barhydt, Development Review Services Manager

Molly Casto - Re: District 1 Sidewalk rehab CIP funds status - York St.

From: Katherine Earley
To: Molly Casto
Date: 10/1/2008 8:45 AM
Subject: Re: District 1 Sidewalk rehab CIP funds status - York St.
CC: Steven Earley

Thanks - glad to try and help.

Know that Steve Earley MAY be engaging in an exercise to combine back all available Sidewalk CIP cash rather than have it split out into the 5 Districts. If he's successful....then there may end up being an opportunity to add some CIP cash to the DPC grant and maybe get to a 50/50 split with Rufus Deering. But that's Steve's battle to wage....

(Steve - RD estimates the cost to be \$95,000; I'll forward you Molly's e-mail with that estimate.)

Kathi

>>> Molly Casto 10/01 8:42 AM >>>

That's ok! When I saw the (-) symbol I figured I'd need to consider other options.

Thanks anyway- it was certainly an idea worth exploring and I appreciate you following up.

Molly

>>> Katherine Earley 10/01 8:39 AM >>>

Hi Molly:

So...at this stage it looks like we don't have the option to pursue getting Councilor Donoghue to agree to offset/share in the cost of the York St. sidewalk requirement for Rufus Deering. Sorry!

Kathi

>>> Michael Bobinsky 09/30 9:53 PM >>>

Curious how that is possible, I would have anticipated a savings from charging only materials over the past 2 years to this account. do you have a summary accounting of the expenses you have assessed against District 1 allocation?

>>> Betsy Beety 9/30/2008 3:57 PM >>>

I was just finalizing those reports today. District #1 has a balance of (\$4,539.57) - yes a negative. I'm thinking that Steve will need to scale back the \$50K estimate for Downtown Improvements to offset the overage. So sorry - there is no money to spend from the sidewalk CIP. The reports will be in your box today.

Betsy

>>> Katherine Earley 9/30/2008 3:47:44 PM >>>

Hi:

Can you clarify what amount of funds are 'leftover' (if any) in the District 1 CIP account(s)?

A hallway brainstorming session on the topic of the York St. sidewalk obligation on Rufus Deering took place late last week, and Planning would like to explore the possibility that Councilor Donoghue MIGHT consider offering some of 'his' CIP funds towards that sidewalk.

So - can you please reply to all re: status so Molly stays in the loop? thanks

Kathi

From: Alex Jaegerman
To: Barbara Barhydt; Katherine Earley; Michael Bobinsky; Molly Casto; Nelle Hanig; Pat Finnigan; Penny Littell
Date: 10/1/2008 8:27:30 AM
Subject: Rufus Deering York St. Sidewalk

Rufus Deering is undergoing site plan review, and will be required to put curb and sidewalk on York Street from Maple to High Street, along with improvements to the other three sides of their block. They have requested a waiver of this requirement, because they have no access from this side, due to the fact that York Street is built above the seawall, about roof level to their project.

They do not meet the waiver criteria, and it is unlikely that the Planning Board will grant a waiver, and Public Services is not supportive of a waiver. I don't know the exact cost of the curb and side walk, but have seen estimates of about \$70,000. They may be eligible for a Development Action Grant from DPC of \$25,000.

Mayor Suslovic was contacted by Rufus Deering and has asked that we consider some kind of compromise. I have previously inquired from Joe if he would consider this sidewalk in the upcoming CIP, but Joe stated that it is not considered a priority for CIP funding. Ed suggested that the district councilor, Kevin Donoghue, might have some CIP funds available, but I am told that he does not.

Is there any further compromise or cost sharing idea that anyone has? The applicant has agreed to all other requirements, but is concerned that this will make the project non-economic.

This will be going to the Planning Board in November.

Alex.

Alexander Jaegerman, AICP
Planning Division Director
389 Congress Street, Suite 400
Portland, ME 04101

Phone: (207)874-8724

Att 8-b

Molly Casto - Rufus Deering - Commercial Street

From: "Errico, Thomas A" <TERRICO@wilbursmith.com>
To: "Molly Casto" <MPC@portlandmaine.gov>
Date: 11/14/2008 10:57 AM
Subject: Rufus Deering - Commercial Street
CC: "James Carmody" <JPC@portlandmaine.gov>, "Katherine Earley" <KAS@portlandmaine.gov>

Molly –

The following summarizes the status of my final comments for the project.

1. Based upon discussions at the Department of Public Services, it is our recommendation that a sidewalk be provided on York Street between High Street and Maple Street (on the south side). Based upon a field investigation, it is suggested that the sidewalk be located within a portion of the existing York Street roadway pavement. The applicant shall submit plans for review and comment. I would note that there may be sections of the sidewalk where a 4-foot width will be necessary to ensure reasonable on-road bicycle provisions can be provided.

Status 9/4/08: Both Jim Carmody and I conducted a field review of existing roadway characteristics on York Street as it relates to the provision of a sidewalk on the abutting frontage of the Rufus Deering property. It is our professional opinion that the proposed layout as illustrated on the August 22, 2008 plan is acceptable from a public safety perspective. Provision of a 5-foot bicycle lane will serve bicyclist well and the 4-foot sidewalk will provide an enhanced pedestrian environment that would be expected in an urban environment. The alignment of travel lanes will be adequate and the overall roadway cross-section will provide a good balance of multi-modal accommodations.

Current Status: The above response continues to remain valid.

2. It is recommended that the Commercial Street driveway located nearest High Street be eliminated and a two-way primary entrance be located along the middle of the property frontage. The applicant shall submit a revised plan for review and comment. On-street parking conditions on Commercial Street may have to be modified to accommodate this primary driveway. I'll coordinate with Jim Carmody on the parking issue after receiving the revised plans.

Status 9/4/08: The plans illustrate this change and therefore I find the plans to be acceptable. I would note that because Commercial Street is a moratorium street, temporary treatments will be required until the moratorium has expired. I believe Dan Goyette's letter includes comments on this issue.

Current Status: MaineDOT and the Department of Public Services have concluded that the proposed scope of work can proceed.

3. The northeastern radius on the corner of the Commercial Street/High Street intersection should be reduced in size such that the sidewalk is located in the public right-of-way. This change will also greatly benefit pedestrians by reducing the intersection crossing distance and will slow vehicle speeds. Implementation of the suggested changes cannot occur until the Commercial Street pavement moratorium expires or City policy changes. I would suggest that the applicant prepare a plan that illustrates the recommended change and the applicant be responsible implementation of the changes at a time when restrictions have expired.

Status 9/4/08: The plans illustrate this change and therefore I find the plans to be acceptable. I would note that because Commercial Street is a moratorium street, we suggest that this work not take place until the moratorium expires. I believe Dan Goyette's letter includes comments on this issue.

Current Status: MaineDOT and the Department of Public Services have concluded that the proposed scope of work can proceed.

4. The city has been collecting monetary contributions for the future installation of a traffic signal at the

Commercial Street/High Street intersection. The applicant may be required to contribute towards the signal installation. Because the project is not changing substantially, from a traffic generation perspective, I would expect the contribution to be minimal. I'll work with City staff to determine an appropriate contribution level and provide that information in the future.

Status 9/4/08: It is my understanding that the applicant does not expect any additional traffic associated with the proposed project. The applicant should provide documentation that supports this case for review and comment.

Current Status: The proposed project is expected to generate an additional 24 driveway trips during the PM peak hour according to the November 5, 2008 analysis prepared by Sebago Technics, Inc. Based upon prior development contribution levels, the proposed project should contribute \$3,900.00 towards the future installation of a traffic signal at the Commercial Street/High Street intersection.

Additional Comments

- The proposed main driveway is being proposed as a three-lane (1 entry lane and 2 exit lanes) curb cut with a total width of 36 feet. As noted in the applicant's submittal, this width exceeds the City standard and therefore requires a design waiver. I would ask that the applicant provide supporting information on the need for this width. This should consist of an Auto-Turn vehicle turning template graphic and traffic volume projections, if available.

Current Status: We support a driveway width waiver to accommodate large vehicle access and egress maneuvers. However, it is recommended that the driveway be marked for one wide entry lane and one wide exit lane.

- The proposed project will provide parking stall sizes that exceed City of Portland standards. I support these larger spaces due to the customer/business activity occurring on site.
- A handicapped ramp should be provided at the southeast corner of York Street and High Street intersection.

Please contact me if you have any questions.
Best regards

Thomas A. Errico, P.E.
Senior Transportation Engineer
Wilbur Smith Associates
59 Middle Street
Portland, Maine 04101
w: 207.871.1785 f: 207.871.5825
TErrico@WilburSmith.com
www.WilburSmith.com

Rufus Deering Off Site Improvements

Portland, Maine

STI Job# 07383

6-Oct-08

Att. 9

York Street Sidewalk Improvements (620 LF +/-)

	Unit Cost	Subtotal(\$)	Total Costs
Sidewalk Breakdown			
Brick surfacw/base prep	\$85/ SY	\$29,325.00	
Granite Curbing	\$35/LF	\$21,700.00	
Trench pavement and base prep.	LS	\$2,250.00	
Drainage improvements	\$3000/ea	\$3,000.00	
Piping replaced	\$40/LF w 20lf	\$800.00	
Contractor/Equipment Mobil. W traffic control	6,000	\$6,000.00	
Erosion Control Measures	200	\$200.00	
Landscaping	Not Included		
Pavement markings/ lane striping	1,500	\$1,500.00	
Pedestrian Signals	2800/ea	\$11,200.00	
Construction Cost			\$75,975.00
Street Opening fees.	9,650	\$9,650.00	9,650.00
Sub Total			85,625.00
DESIGN/CONSTR ADMIN FEES		\$3,000.00	3,000.00
Base Estimated Project Cost for Applicant			88,625.00

Estimate does not account for relocation of any CMP poles, or traffic arms if necessary.

Estimate does not include transfer of materials excavated in St. which may be deemed hazardous and require

testing or hauling to approved site.

Area of Sidewalk = 620ft(length) x 5 ft(width)= 3100SF=345 SY

Design includes field survey of street/ and engineering design to match street profile

Street opening permit fees are approximate

Rufus Deering Company

383 Commercial St.
Portland, Me. 04101-4625
207/772-6505

Established 1854
Lumber/Building Materials
Email: lumber@rufusdeering.com

Molly P. Casto,
2009
Senior Planner,
City of Portland

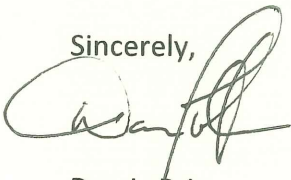
January 30,

Molly,

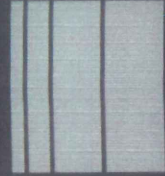
Rufus Deering would like to request an extension on the approval we received on November 15th, 2008 for our drive-thru lumber yard project. Taking into account the current economic conditions, and fulfilling all the requirements set forth by the planning board before starting the project, we are requesting additional time to prepare relative to the existing economy. Please get back to me with your input.

Thanks you for the consideration on this subject and I hope to hear from you soon.

Sincerely,



Dan LaBrie
V.P. and General Manager
Rufus Deering Lumber Co.
383 Commercial St.
Portland, Maine 04104



Att. 2a

August 22, 2008
07383

sebagotechnics.com

One Chabot Street
P.O. Box 1339
Westbrook, Maine
04098-1339
Ph. 207-856-0277
Fax 856-2206

Ms. Molly Casto, Planner
City of Portland Planning Division
City Hall 4th Floor
389 Congress St,
Portland, ME 04101

Revised Site Plan Planning Board Workshop Submittal
Rufus Deering Lumber Company's - Proposed Drive-Thru Warehouse and Retail Store
383 Commercial St. Portland, Maine

Dear Ms. Casto:

Please find 9 copies of the revised Site Plan set that we have prepared for Rufus Deering Lumber Company - Proposed Drive-Thru Warehouse and Retail Store to be located on the street block surrounded by Commercial Street, High Street, York Street, and Maple Street in Portland Maine. This letter and the plans are in response to your review letter dated July 7, 2008, and per our meeting with you back in mid July. We understand that there could be other issues that could be brought forward, as we have not received all the comments from the various city departments. Our letter responds to the review items in the same numerical order as those in your July 7 letter.

Zoning

1. We are waiting for Marge Schmukal, Zoning Administrator's comments discussing the lessening of the existing non-conformity with our proposed site project. We expect that there are no substantial zoning issues with our proposed site project.
2. After much discussion about screening the materials, we had hoped to get a determination by the Zoning Administrator with regards to the need for site screening. The use of the property as a lumberyard has been in existence since 1854, and the general operations of a lumberyard make it absolutely necessary to have outdoor storage of lumber or building materials. Often these materials, are stacked upwards of 16 feet high, and have traditionally not required screening on the current site. Given the height, and front yard location of stored outdoor lumber, we feel screening with a 5 to 6 foot high fence does not accomplish the original intent of the ordinance to hide on site materials, but as discussed, if the City wishes us to screen the site from a pedestrian street level view, we would offer to screen the property with vinyl strips within the existing chain-link fencing. Our biggest objections to any screening, is that this has not been historically required on the site as a lumberyard, and that the area has been subject to graffiti and destruction of property by means of spray painting.

Site Plan

1. We have added floor areas and grade elevations of the existing and proposed structures as noted on the Grading and Utility Plan Sheet 5 of 9.
2. The owner is still working on supplying the architectural elevation drawings which will show building fronts, materials, and roof lines. Once those are complete they will be forwarded to the staff for their review.
3. A lighting plan will be prepared upon the Planning Board accepting the general layout of the proposed site. We are fully aware that the project will be required to use fully shielded cut-off fixtures per City requirements.
4. We have identified that all rubbish and recyclables will be stored in an enclosed dumpster area north of the proposed loading dock, or will be conducted within the confines of the drive-thru warehouse.

Traffic

5. We have not formerly received comments to date from the traffic engineer, but have a general sense of his concerns. We have eliminated the entrance closest to High Street and created more parking. Based on the conversations at our July meeting, we have widened the Commercial Street entrance to align with the drive-thru warehouse building entrance, and kept the Maple St. entrance for the purposes of material deliveries to the site. Based on your comments we respectfully request a waiver of the technical design standards to allow for two entrances, and to make one entrance 36 feet wide for the purposes of three lanes (one in, two out) to accommodate truck traffic.

Additionally, we have moved the corner of High and Commercial Street further away from the existing store, with the insertion of a 40 foot radius. This allows both the sidewalk, and street pavement to fully fall within the limits of the right-of-way and not encroach over the Rufus Deering property. Our concern is that the section of Commercial Street was just reconstructed, and that to construct the new corner radius could mean that the applicant could be forced to open a moratorium street. We are hoping the City will waive those fees to correct this situation and improve traffic safety near the intersection.

Curb and Sidewalk

6. Pursuant to Section 25-96, the project is required to install sidewalks and granite curbing on abutting streets. The project is unique in that it is an existing block surrounded by 4 streets. Commercial Street is constructed with granite curbing and concrete sidewalk, Maple Street is partially constructed with granite curbing and brick sidewalk, High Street is constructed with poor quality asphalt curbing and sidewalk in very good condition, and York Street has no sidewalks and poor low reveal granite curbing. We are proposing new curbing and sidewalk on High Street, and completing Maple Street where curbing

and sidewalk are missing, Commercial Street will not require either, with the exception of where the old site exit was, and where the new corner radius will be installed. Brick sidewalk and new granite curbing will be constructed over that corner area.

The sidewalk on York Street has presented many challenges due to the presence of an existing stone sea wall along the street and property line, and due to the unique geometry of York Street. Therefore, we submit a request to waive the sidewalk requirement under meeting two of the conditions under Section 14-506(b) of the Land Use Ordinance. Based on discussions with the staff, we offer to install granite curbing, and mark out a 5 foot wide bike lane.

We feel that we meet two of the waiver conditions:

(3) A safe alternative-walking route is reasonably available, for example, by way of a sidewalk on the other side of the street. There is an existing sidewalk on the opposing side of York Street which is currently designed into the signalization and crossings of High Street. Placement of sidewalk on the Rufus Deering frontage on York Street would require upgrades to signalization, lane markings, and doesn't pose the safest sidewalk scenario, which is discussed in our second waiver condition.

(6) Strict adherence to the sidewalk requirement would result in the loss of significant site features related to landscaping, or topography that are deemed to be of greater public value. The presence of a seawall along the edge of the right-of-way makes it unsafe to install a sidewalk closer to that wall and could jeopardize stability of pole supports and guardrails necessary for vehicular and property protection. Moving the sidewalk into the current paved section of York Street also has implications into public safety. The sidewalk could not be constructed with an esplanade, and could only be 4 feet wide, and even that diminishes the lane widths available to cars on York Street and could require re-striping of the travel lanes. Furthermore, this would require relocation of an existing catch basin, further tightening the separation of underground infrastructure in York Street. This could require upgrades to the current guardrail system to bring it up to a safe elevation to protect City snow plowing equipment and operators from a potential accident over the wall, all of which add substantial cost to the sidewalk installation. Due to constricting geometry of York Street, the outward traffic flow from Portland to High Street or South Portland, could increase the queuing at the intersection because there would not be the opportunity to turn with a separate right at the light after two or three cars stopped. A connecting cross walk would push the stop line back about 6 feet and there would be about 60 feet to a pinch point which leaves only 30 feet for three lanes, which is not ideal traffic engineering practice for a busy street subject to commercial traffic. Therefore, we feel the sidewalk would result in substantial loss of site features and public safety.

We have offered a proposal to replace the poor curbing with new granite curbing on York Street and paint a 5 foot wide bike lane. The street is subject to a lot of bike traffic and is a link for bike uses between South Portland, West End, and the Old Port. The elimination of the sidewalk and keeping of the bike lane would allow for safer lane widths, eliminating the need to re-signal the pedestrian crossing, adjust/relocate drainage infrastructure, while providing a safe link for bicyclists to get from the Casco Bay Bridge to the Old Port.

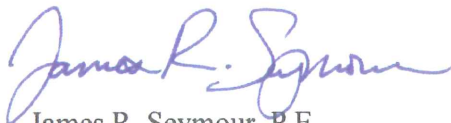
7. Prior to the Public Hearing, the owner will provide estimated costs for the project. We are trying to get a handle on the additional sidewalk or other construction costs which may be requested by the Board.
8. We have filed a letter with Public Works requesting a letter of sewer capacity. No response has been returned to date.
9. The owner will provide evidence of financial capacity once we have a known scope of site work required.
10. We expect that we will receive formal review comments from various departments or staff at or following the work shop. We have attempted to revise as many verbal comments as possible. We also understand, and will attempt to be prepared to answer questions about the warehouse operation, on-site traffic flow, parking, landscaping and building appearance.

Rufus Deering Lumber Company is looking to possibly start construction in 2009. We will be providing architectural renderings, and lighting plans as details of the site become available. In the interim, we are available at your request to meet the City's review staff. We feel given site design parameters that there is currently adequate information for the purposes of a workshop with the Planning Board.

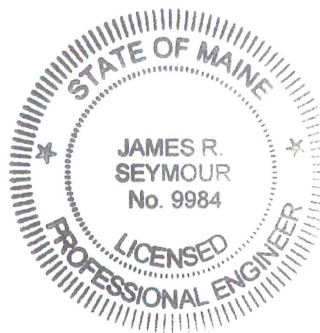
If you have any questions, please feel free to contact me at our office (856-0277). We look forward to meeting with the Planning Board for a workshop meeting as soon as an agenda allows, hopefully on September 9th.

Sincerely,

SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager



JRS:jrs/cb
Enc.

cc: Dan Labrie, Vice President- Rufus Deering Lumber Company
Jay Breard- Rufus Deering Lumber Company

Att-10

Molly Casto - Rufus Deering - Brick Sidewalk Price

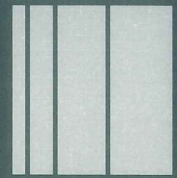
From: David Margolis-Pineo
To: Alex Jaegerman ; Dan Goyette; Jean Fraser; Marge Schmuckal; Molly Casto; Rick Knowland ; William Needelman
Date: 10/1/2008 5:18 PM
Subject: Rufus Deering - Brick Sidewalk Price
CC: Gretel Varney; Todd Merkle

Just spoke with Scott LaBrecque who does sidewalks for a living.

The following prices should build the 620 L.F. of brick sidewalk on York St.

Move catch basin w/ curb inlet.	\$5,000	
Granite Curbing @\$35/ft	\$21,700	same as estimate by applicant
Brick sidewalk @ \$120/sy	\$46,335	Applicant used \$85/sy
	about	\$73,035

Note: the newly specified city bricks, cost about \$30/sy more than the what LaChance's bricks cost. So the cost per sq yd of brick sidewalk increased from about \$90/sy to \$120/sy.



Attachment 2b

October 15, 2008
07383

Molly Casto, Planner
City of Portland
389 Congress Street
Portland, ME 04101

sebagotechnics.com
One Chabot Street
P.O. Box 1339
Westbrook, Maine
04098-1339
Ph. 207-856-0277
Fax 856-2206

Rufus Deering Lumber Company – 383 Commercial Street
Preliminary Site Plan Review

Dear Ms. Casto:

We have received peer review comments prepared by Dan Goyette, P.E. and Megan LaPierre E.I.T. of Woodard & Curran, dated July 9, 2008, for the subject project. Review materials included the Site Plan application and Engineering Plan set (Sheets 1-9), both dated June 11, 2008. After reviewing the comments provided, we offer the following responses.

Responses to Comments:

1. *Stormwater Management Plan and flow calculations have not been submitted for review.*

A Stormwater Management Plan and flow calculations were not submitted because we do not anticipate a significant change in peak flows from pre-development to post-development conditions. In both cases the site is completely developed with nearly 100% impervious surface. In fact, the amount of landscaped area increases slightly in the post-development site, but not enough to produce a significant change in runoff.

A peak flow analysis of the post-development site is provided with this response, in order to evaluate the performance of the proposed drainage improvements. Because the outlet of this system (SP-2) is an existing 12-inch storm drain in Commercial Street, only 12-inch pipes are proposed on-site. The model only includes an evaluation of the site. Runoff from the adjacent streets is not modeled.

Regarding the requirement for on-site treatment of runoff, this issue is currently being reviewed by the City staff, and will be resolved to their satisfaction prior to approval. The existing inverts in Commercial Street (proposed outlet) limit the proposed drainage improvements to a very shallow depth. The most likely solution, per an on-site meeting held in September, is that the applicant will provide a contribution to the City for a future improvement project. The amount of the contribution and the future project will both be determined by the City.

2. *The Note 11 on Sheet 4 indicates that there are 22 proposed parking spaces. The site plan shows 27 parking spaces, 22 adjacent to the proposed/remodeled retail building and 5 adjacent to Maple Street. According to the City of Portland Technical Design*

Standards, if a parking lot proposes 25 or more parking spaces, then on-site stormwater treatment is required.

A total of 30 spaces are now proposed. Note 11 on Sheet 4 has been updated. See comment 1 above regarding proposed stormwater treatment.

3. *Sheet 5 shows pipes from the proposed drive-through warehouse and retail building connecting into Pipe SD-3. What are these pipes? How do the pipes connect to SD-3, wye or tee? Why do the pipes tie into the storm drain pipe and not catch basin CB-1? Please provide information and pipe material, size, etc.*

These pipes represent roof drain connections. We have increased the diameter of CB-1 to 6 feet, and reconnected the roof drains so that they discharge directly into the catch basin. As the building design progresses and actual roof drain sizes and locations are determined, the plans will be revised to show any new information or changes.

4. *A note should be provided on the drawings stating all new catch basins shall include a Casco Trap.*

Note 27 has been added to Sheet 5 of 9 requiring the use of Casco Traps in all new catch basins.

5. *The line work and labels on the Typical Sidewalk ADA Ramp elevation detail are unclear. The top of the granite curb and top of roadway pavement are shown at the same elevation. Also, the granite curb is labeled as 6". The curbing should have a 7" reveal, as shown in Typical Installation of Curb detail.*

The detail has been revised for clarification. All details show a curb reveal of 7".

6. *Details for the dumpster enclosure, tree grate, retaining wall and seat wall with rail should be provided.*

Details for the dumpster enclosure and tree grate have been added to the detail sheets. Retaining wall details will be submitted for approval prior to a performance bond being established. Materials and actual wall systems have not been determined, and we request that this be either a condition of approval or made part of the building permit process.

7. *The dumpster should be placed on a concrete pad.*

The concrete pad for the dumpster is shown in the detail on Sheet 8 of 9.

8. *Vehicular access to the dumpster is limited to an 11-foot travel width due to the adjacent walls. It will be difficult for a waste management vehicle, traveling in reverse, to access the dumpster, as the typical roadway width is a minimum of 12-feet. It is suggested that the access to the dumpster be widened, or the dumpster be relocated.*

We feel that 11 feet is an adequate width for truck access to the dumpster location. We have eliminated the short wall and will slope the gravel to allow more room. The 11-foot width is equal to a street lane and should be adequate.

9. *Location of the pedestrian ramps should be shown on the drawings.*

Pedestrian ramps have been added as requested. However, based on off-site improvements, the locations may change as the design evolves. We have included a note on the Site Plan that recommends coordination with the City Traffic Engineer prior to construction.

10. *Location of erosion control measures (i.e. filter barrier, silt sacks) should be shown on the drawings.*

Temporary inlet protection is shown on all catch basins, and filter barrier is shown where appropriate.

11. *Location of snow storage should be indicated on the plans.*

Note 26 has been added to Sheet 4 of 9 indicating that the applicant will contract with a local firm for snow removal and that no snow storage area is provided.

12. *The installation of sidewalk along the York Street frontage needs to be addressed.*

This issue is currently being reviewed by City Planners and Public Works. The plans show the alignment of curb, brick sidewalk, and bicycle lane. The applicant is petitioning for a sidewalk waiver and a request has been previously submitted. The plans show a layout for a sidewalk, but may be revised if the Board agrees to waive the requirement in part or in whole.

Also, we have attached for Planning Dept. review a cost estimate for construction of all sidewalk sections required to be installed per site plan standards on High, York, and Maple Streets for your use. A separate letter will be attached for the sidewalk on York Street arguments.

This completes our response to the peer review comments submitted by Woodard & Curran. If you have any additional comments or require further information, please contact us at 856-0277. Thank you for your time.

Sincerely,

SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager

JRS:kn

Att. 4

MEMORANDUM

To: FILE

From: Greg Cass

Dept: Fire

Subject: Application ID: 2008-0082

Date: 8/13/2008

They do not need the new hydrant, The 4 existing are fine.
Most issues for me will come during the building permit process.
The PFD approves this project.

Att. 8 -a

Molly Casto - Rufus Deering

From: "Errico, Thomas A" <TERRICO@wilbursmith.com>
To: "Molly Casto" <MPC@portlandmaine.gov>
Date: 9/4/2008 4:12 PM
Subject: Rufus Deering
CC: "James Carmody" <JPC@portlandmaine.gov>, "Katherine Earley" <KAS@portlandmaine.gov>

Molly –

The following summarizes the status of my August 22, 2008 comments and additional comments associated with a review of the August 22, 2008 submittal from Sebago Technics, Inc.

1. Based upon discussions at the Department of Public Services, it is our recommendation that a sidewalk be provided on York Street between High Street and Maple Street (on the south side). Based upon a field investigation, it is suggested that the sidewalk be located within a portion of the existing York Street roadway pavement. The applicant shall submit plans for review and comment. I would note that there may be sections of the sidewalk where a 4-foot width will be necessary to ensure reasonable on-road bicycle provisions can be provided.
Status: Both Jim Carmody and I conducted a field review of existing roadway characteristics on York Street as it relates to the provision of a sidewalk on the abutting frontage of the Rufus Deering property. It is our professional opinion that the proposed layout as illustrated on the August 22, 2008 plan is acceptable from a public safety perspective. Provision of a 5-foot bicycle lane will serve bicyclist well and the 4-foot sidewalk will provide an enhanced pedestrian environment that would be expected in an urban environment. The alignment of travel lanes will be adequate and the overall roadway cross-section will provide a good balance of multi-modal accommodations.
2. It is recommended that the Commercial Street driveway located nearest High Street be eliminated and a two-way primary entrance be located along the middle of the property frontage. The applicant shall submit a revised plan for review and comment. On-street parking conditions on Commercial Street may have to be modified to accommodate this primary driveway. I'll coordinate with Jim Carmody on the parking issue after receiving the revised plans.
Status: The plans illustrate this change and therefore I find the plans to be acceptable. I would note that because Commercial Street is a moratorium street, temporary treatments will be required until the moratorium has expired. I believe Dan Goyette's letter includes comments on this issue.
3. The northeastern radius on the corner of the Commercial Street/High Street intersection should be reduced in size such that the sidewalk is located in the public right-of-way. This change will also greatly benefit pedestrians by reducing the intersection crossing distance and will slow vehicle speeds. Implementation of the suggested changes cannot occur until the Commercial Street pavement moratorium expires or City policy changes. I would suggest that the applicant prepare a plan that illustrates the recommended change and the applicant be responsible implementation of the changes at a time when restrictions have expired.
Status: The plans illustrate this change and therefore I find the plans to be acceptable. I would note that because Commercial Street is a moratorium street, we suggest that this work not take place until the moratorium expires. I believe Dan Goyette's letter includes comments on this issue.
4. The city has been collecting monetary contributions for the future installation of a traffic signal at the Commercial Street/High Street intersection. The applicant may be required to contribute towards the signal installation. Because the project is not changing substantially, from a traffic generation perspective, I would expect the contribution to minimal. I'll work with City staff to determine an appropriate contribution

level and provide that information in the future.

Status: It is my understanding that the applicant does not expect any additional traffic associated with the proposed project. The applicant should provide documentation that supports this case for review and comment.

Additional Comments

- **The proposed main driveway is being proposed as a three-lane (1 entry lane and 2 exit lanes) curb cut with a total width of 36 feet. As noted in the applicant's submittal, this width exceeds the City standard and therefore requires a design waiver. I would ask that the applicant provide supporting information on the need for this width. This should consist of an Auto-Turn vehicle turning template graphic and traffic volume projections, if available.**

Please contact me if you have any questions.
Best regards

Thomas A. Errico, P.E.
Senior Transportation Engineer
Wilbur Smith Associates
59 Middle Street
Portland, Maine 04101
w: 207.871.1785 f: 207.871.5825
TErrico@WilburSmith.com
www.WilburSmith.com

**COMMITMENT & INTEGRITY
DRIVE RESULTS**

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T 800.426.4262
T 207.774.2112
F 207.774.6635

MEMORANDUM

TO: Molly Casto
FROM: Dan Goyette, PE and Megan LaPierre, EIT
DATE: July 9, 2008
RE: Rufus Deering Lumber Company, 383 Commercial Street

Woodard & Curran has reviewed the Major Site Plan Application for the proposed site redevelopment at 383 Commercial Street. The proposed project involves the construction of a 29,000 square foot drive-through warehouse and reconfiguration of current storage shelter to accommodate access and internal circulation. The project also includes remodeling the existing retail building and minor parking lot improvements.

Documents Reviewed

- Site Plan Application and attachments, addressed to Barbara Barhydt, City of Portland Senior Planner, prepared by James R. Seymour, PE, Sebago Technics, Inc., dated June 11, 2008.
- Engineering Plan Sheets 1-9, prepared for Rufus Deering Lumber, prepared by Sebago Technics, Inc., dated June 11, 2008.

Comments

- Stormwater Management Plan and flow calculations have not been submitted for review.
- The Note 11 on Sheet 4 indicate there are 22 proposed parking spaces. The site plan shows 27 parking spaces, 22 adjacent to the proposed/remodeled retail building and 5 adjacent to Maple Street. According to the City of Portland Technical and Design Standards, if a parking lot proposes 25 or more parking spaces then on-site stormwater treatment is required.
- Sheet 5 shows pipes from the proposed drive-through warehouse and retail building connecting into Pipe SD-3. What are these pipes? How do the pipes connect to SD-3, wye or tee? Why do the pipes tie into the storm drain pipe and not catch basin CB-1? Please provide information and pipe material, size, etc.
- A note should be provided on the drawings stating all new catch basins shall include a Casco trap.
- The line work and labels on the Typical Sidewalk ADA Ramp elevation detail are unclear. The top of the granite curb and top of roadway pavement are shown at the same elevation. Also, the granite curb is labeled as 6". The curbing should have a 7" reveal, as shown in Typical Installation of Curb detail.
- Details for the dumpster enclosure, tree grate, retaining wall and seat wall with rail should be provided.
- The dumpster should be placed on a concrete pad.
- Vehicular access to the dumpster is limited to an 11-foot travel width due to the adjacent walls. It will be difficult for a waste management vehicle, traveling in reverse, to access the dumpster, as the typical travel roadway width is a minimum of 12-feet. It is suggest the access to the dumpster be widen or the dumpster be relocated.
- Location of pedestrian ramps should be shown on the drawings.
- Location of erosion control measures (i.e. filter barrier and silt sacks) should be shown on the drawings.
- Location of snow storage should be indicated on the plans.
- The installation of a sidewalk along the York Street frontage needs to be addressed.

Please contact our office if you have any questions.

DRG/MDL
203943.96

MEMORANDUM



TO: Molly Casto
FROM: Dan Goyette, PE
DATE: September 3, 2008
RE: Rufus Deering Lumber Company, 383 Commercial Street

Woodard & Curran has reviewed the Major Site Plan Application for the proposed site redevelopment at 383 Commercial Street. The proposed project involves the construction of a 29,000 square foot drive-through warehouse and reconfiguration of current storage shelter to accommodate access and internal circulation. The project also includes remodeling the existing retail building and minor parking lot improvements.

Documents Reviewed

- Revised Site Plan Workshop Submittal, addressed to Molly Casto, City of Portland Senior Planner, prepared by James R. Seymour, PE, Sebago Technics, Inc., dated August 22, 2008.
- Engineering Plan Sheets 1-9, prepared for Rufus Deering Lumber, prepared by Sebago Technics, Inc., dated August 22, 2008.

Comments

- The comments in our July 9 memo have not been addressed.
- The applicant has requested a waiver on the sidewalk requirement for York Street. They are partly basing their request on waiver criteria #6. The applicant states that installing the sidewalk as Public Services has requested inboard of the existing curb line, so there by reducing the street width, creates an unsafe condition. Both Tom Errico, Wilbur Smith Associates traffic consultant to the City, and Jim Carmody, City Traffic Engineer, have reviewed the proposed 4' sidewalk and 5' shoulder/bike lane configuration and are comfortable with the proposal.
- The applicant is proposing to close the driveway closest to the intersection of High and Commercial Street. To install the new curb and sidewalk would impact a moratorium street. We suggest the installation of a temporary bituminous curb and sidewalk. The installation of the granite curb and brick sidewalk can then take place at the same time as the work required to reconfigure that corner of the street after the moratorium has expired.

Please contact our office if you have any questions.

DRG
203943.96

Att. 5-c

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www.woodardcurran.com

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T 207.774.2112
F 207.774.6635

MEMORANDUM



TO: Molly Casto
FROM: Dan Goyette, PE
DATE: November 3, 2008
RE: Rufus Deering Lumber Company, 383 Commercial Street

Woodard & Curran has reviewed the Major Site Plan Application for the proposed site redevelopment at 383 Commercial Street. The proposed project involves the construction of a 29,000 square foot drive-through warehouse and reconfiguration of current storage shelter to accommodate access and internal circulation. The project also includes remodeling the existing retail building and minor parking lot improvements.

Documents Reviewed

- Response to Comments Submittal, addressed to Molly Casto, City of Portland Senior Planner, prepared by James R. Seymour, PE, Sebago Technics, Inc., dated October 15, 2008.

Comments

- The applicant has agreed to provide a contribution in lieu of implementing on-site stormwater treatment measures. The site does not allow for the implementation of these measures due to the shallow existing inverts in Commercial Street. The contribution amount is based upon the size of the parking area and will be used for the improvements to the stormwater outfall on the Berlin Mill Wharf (by Gowen Marine) across from High St. The amount of the contribution has been determined to be \$20,000.

Please contact our office if you have any questions.

DRG
203943.96

Molly Casto - Rufus Deering

From: Jeff Tarling
To: Molly Casto
Date: 9/5/2008 10:26 AM
Subject: Rufus Deering

Hi Molly -

I reviewed the latest landscape plan for the proposed Rufus Deering project and find it acceptable. The plan adds trees along High Street from Commercial to York Street. I would agree with the plan that street trees along York Street might not be feasible due to the potential root system / wall conflict.

Overall the tree placement & selection should improve the area.

Thanks,

Jeff Tarling
City Arborist

Att. 6-b

Molly Casto - Re: Rufus Deering

From: Jeff Tarling
To: Molly Casto
Date: 11/14/2008 9:18 AM
Subject: Re: Rufus Deering
CC: Barbara Barhydt

Hi Molly -

The Rufus Deering landscape plan is acceptable as shown. I would make a condition that the empty tree well along Commercial Street be planted with a new street tree. Limitations to the site along York Street to plant trees close to the retaining wall due to potential problem with tree roots interfering with the wall stability.

Jeff Tarling
City Arborist

Att 7

Molly Casto - Rufus Deering Lumber Company, 383 Commercial Street

From: David Margolis-Pineo
To: Molly Casto
Date: 9/5/2008 10:18 AM
Subject: Rufus Deering Lumber Company, 383 Commercial Street
CC: Dan Goyette; James Carmody; Tom Errico

September 5, 2008

To: Molly Casto
From: David Margolis-Pineo
Re: Rufus Deering Lumber Company, 383 Commercial Street

Section III of the City's Technical Design Standards states:

A. Commercial, Industrial and Multi-Family (3 units or more)

- (a) **Minimum driveway width (one-way): Any site with driveway access to a street shall have a minimum 20 foot wide driveway for one-way ingress or egress. Driveways shall permit traffic to enter and leave the site simultaneously without conflict in aisles, parking or maneuvering areas. Both the entrance and exit drives shall be marked with appropriate signage.**
- (b) **Minimum/maximum driveway width (two-way): Any site with driveway access to a street shall have a 24 foot wide driveway for two-way ingress and egress. Under special conditions a driveway width of up to 30 feet will be permitted if the City's Transportation Engineer determines that site conditions or vehicle characteristics warrant a wider access, e.g., dedicated turn lanes at exits.**

It is my understanding that the City's Transportation Engineer is agreeable with permitting a 30 foot wide driveway at this location.

To be allowed to install a 36 foot drive, Rufus Deering must submit a written request to the Planning Board for a waiver.

Due to the fact that Rufus Deering is proposing to consolidate their driveway cuts, Public Services has no objection if the Planning Board should grant a waiver request to widen the proposed driveway cut to 36 feet.

The applicant does show an additional curb cut onto Commercial St. which is currently not being used. The City has painted parking spaces in front of this cut. If the applicant is not intending to use this driveway cut in the future, it would be desirable to eliminate it.

MEMORANDUM

To: FILE
From: Marge Schmuckal **Dept:** Zoning
Subject: Application ID: 2008-0082
Date: 8/27/2008

I have reviewed the most current submittal which I received on 8/27/08. This property is located within the B-5b Urban Commercial Mixed Use Zone. Lumber and building materials dealers that are in existence are permitted. It has been submitted that Rufus Deering Lumber Company has been in existence since 1854 (154 years old).

The alterations to the existing developed site brings the site into greater conformance with todays ordinances and lessens the existing nonconformities especially in regard to the maximum front yard setback requirement of the B-5b zone. The Zoning Ordinance encourages the lessening of nonconformity on developed sites.

Under Performance Standards section 14-230.5, there is a standard that any storage of new materials, finished products, or related equipment be suitably screened from the public way and from abutting properties by a solid fence at least finve (5) feet in height or by a solid evergreen planting strip. Again because of the existing development of this site, I do not think the full force of this provision applies. I would hope that the City and the applicant can reach a point where existing nonconformities have been lessened and the project comes more into compliance.

To confirm building heights, I will need scalable drawing of the new structures. I am not anticipating that the proposal will be in violation of the maximum 65 foot building height.

Separate permits shall be required for any new signage.

Marge Schmuckal
Zoning Administrator

MEMORANDUM

To: FILE

From: Marge Schmuckal

Dept: Zoning

Subject: Application ID: 2008-0082

Date: 11/14/2008

I have reviewed the most current site plan that has been submitted. As stated previously, under section 14-230.5 the storage of any new materials, finished products, or related equipment be suitable screened from the public way and from abutting properties by a solid fence at least five (5) feet in height or by a solid evergreen planting strip. I believe that the applicant has made an effort to clean up the front storage area of the property. There is a line of existing product storage adjacent to the front property line that is being removed. It is noted that this business is the entire block. There is no direct abutting neighbor who needs to be screened and protected from the visual impacts of this business. I have determined that existing nonconformities have been lessened and the property has come into better compliance.

Marge Schmuckal
Zoning Administrator



Memorandum
Department of Planning and Development
Planning Division

To: Chair Tevanian and Members of the Portland Planning Board

From: Molly Casto, Senior Planner

Date: September 5, 2008

Re: September 9, 2008 Planning Board Workshop
383 Commercial Street. Rufus Deering Lumber Company

I. INTRODUCTION

Rufus Deering Lumber Company has requested a Planning Board workshop to review a proposal to redevelop the existing Rufus Deering Lumber store and lumber yard site at 383 Commercial Street. The 2.83 acre site is located on the block bounded by Commercial Street, High Street, York Street and Maple Street. The proposal includes the renovation of the existing red barn into a retail store, construction of an approximately 29,000 s.f. drive-through warehouse, parking lot improvements, and reorganization of the outside storage areas for lumber and other building products and reconfiguration of the existing storage shelter along Commercial Street.

The project is being reviewed as a major site plan in the B5-b zone. The applicants also are requesting a curb and sidewalk waiver as part of their proposal. Notice of the workshop was sent to 109 area property owners and was advertised in the Portland Press Herald and on the City website.

Representatives for the applicant include Sebago Technics and Johnson Design Resources, Architects.

II. FINDINGS:

Parcel size:	2.83 acres (123,274.78 s.f.)
Street frontage:	567 feet (Commercial Street) 262.5 feet (Maple Street) 616.5 feet (York Street) 172.5 feet (High Street)
Total renovation area:	10,650 sq. ft. footprint
Total new construction:	29,000 sq. ft. footprint
Zone:	B5-b Urban Commercial Business
Current use:	Commercial lumber store and lumber yard
Proposed use:	Same as above
Existing curb cuts:	Three (3) curb cuts along Commercial and Maple Streets (and 1 inactive curb cut on Commercial)
Proposed curb cuts:	Two (2) curb cuts along Commercial and Maple Streets (and 1 inactive curb cut on Commercial)
Required parking:	none

Proposed parking: 30 vehicle spaces
Existing parking: none

III. SITE DESCRIPTION:

The project site is located along the northerly side of the Commercial Street. Rufus Deering Lumber has occupied this location since 1854. The parcel occupies the entire block bounded by Commercial, High, York and Maple Streets. The York Street (westerly) side of the parcel abuts the existing stone seawall. The site includes a lumber yard with three (3) open-sided storage warehouses totaling 18,376 sq. ft., two (2) partially to fully enclosed storage warehouses totaling 17,726 sq. ft. and two (2) office buildings at the north and south corners of the block. These office buildings are 2,998 sq. ft and 7, 540.2 sq. ft ground floor area, respectively. The 7,540.2 sq. ft office building at the northern corner of the block is owned by Rufus Deering but is not part of the commercial lumber company and lumber yard operation. It is on its own lot and is separated from the parcel containing the lumber company by a stockade fence. It is not being reviewed as part of this proposal.

IV. DEVELOPMENT PROPOSAL:

The proposal includes renovation of the existing red barn and redevelopment of the lumber yard. Two of the open sided warehouses, one of the enclosed warehouses (the Quonset Hut warehouse) and a portion of the third open-sided warehouse will be demolished to allow for the development of a 28,961 sq. ft drive-through warehouse at the rear (west) of the parcel. The portion of the third open-sided warehouse will be rebuilt and re-oriented to accommodate access and internal circulation to the proposed warehouse. An existing storage shed along the Commercial Street frontage will also be relocated on the site. The second 11,404 sq. ft. enclosed warehouse (the “red barn”) at the west corner of the lot will be remodeled into a 10,648 sq. ft. retail building. There are no changes proposed for the existing 3,000 sq. ft office building at the south corner of the parcel near the intersection of Commercial and High Streets. According to the submitted elevation drawings, building heights would not exceed 27.5 feet (Attachment 9). There are 30 parking spaces proposed. The applicant proposes to consolidate the two existing active curb cuts along Commercial Street into a single 36 ft wide access drive. The existing curb cut along Maple Street would remain open but would be restricted to deliveries only and would be secured with a 10 ft chainlink sliding gate.

The applicant proposes to replace the existing bituminous sidewalk along High Street and a portion of Commercial Street with brick sidewalk and granite curbing. The applicant also proposes brick sidewalk along the easterly portion of Maple Street near the intersection of Commercial Street. The applicant has requested a waiver from City requirements to construct brick sidewalk with granite curbing along the length of York Street, above the existing stone seawall (Attachment 2).

V. ZONING:

The property is located within the B-5b Urban Commercial Mixed Use Zone. Lumber and building materials dealers that are in existence are permitted. It has been submitted that Rufus Deering Lumber Company has been in existence at this site since 1854. Marge Schmuckal, Zoning Administrator has reviewed the proposal and her review comments are included under Attachment 3.

The alterations to the existing developed site bring it into greater conformance with current ordinance requirements and lessen existing nonconformities, namely in terms of the maximum ten (10) ft front yard setback requirement for the B-5b zone. The existing site includes a front yard setback of approximately 30 feet. This is being reduced under the current proposal to

approximately 16 feet. The Zoning Ordinance encourages such lessening of nonconformity on developed sites.

Section 14-230.5 - Performance Standards, includes a standard that any storage of new materials, finished products, or related equipment be suitably screened from the public way and from abutting properties by a solid fence at least five (5) feet in height or by a solid evergreen planting strip. The applicant has been alerted to this provision and has submitted a response in their letter, dated August 22, 2008 (Attachment 2). The applicant notes that the general operations of a lumberyard make it necessary to store lumber and building materials outdoors. Often these materials are stacked upwards of sixteen (16) feet high. Given the height and front yard location of stored outdoor lumber, the requirement to screen with a five (5) foot fence would not provide adequate screening of this particular type of outdoor storage. The applicant would be willing, if the Planning Board wishes, to provide screening of the site at the pedestrian level using vinyl strips within the existing chain link fence. They are concerned, however, about the fencing becoming a target for graffiti vandalism. Marge Schmuckal notes in her review that, because of the existing and historic use of this site, she does not think the full force of this provision applies. She hopes that “the City and the applicant can reach a point where existing nonconformities have been lessened and the project comes more into compliance” (Attachment 3).

As previously discussed, the applicant has provided elevation drawings, as requested by Marge (Attachment 3). These were submitted on September 5th and have not yet been reviewed by staff. The maximum height in the B5-b zone is 65 feet. According to the applicant, the proposed buildings will not exceed 27.5 feet in height, well below that permitted by zoning.

No proposals for new signage have been included as part of this application. Separate permits shall be required for any new signage.

VI. WAIVER REQUESTS- SIDEWALK AND CURBING:

As stated above, the applicant is asking for a curb and sidewalk waiver for this project. Please see the applicant’s statement included as Attachment 2. Pursuant to Section 25-96 of the City Code, this development is required to install sidewalks and granite curbing on all abutting streets. This site encompasses a city block thus, this provision applies to all four frontages. Currently, Commercial Street is constructed with granite curbing and concrete sidewalk. Maple Street is partially constructed with granite curbing and brick sidewalk with no sidewalk on the opposite side of the street. York Street has no sidewalk and granite curbing that is in poor condition with low reveal. Interestingly, a portion of the York Street curbing is constructed of historic cobblestone turned on-end. There is sidewalk along the opposite side of York Street of various materials and conditions. High Street has bituminous sidewalk and curbing along both sides of the street. The applicant proposes to install new brick sidewalk and curbing along High Street, along the remaining portions of Maple Street and on Commercial Street where the existing driveway will be closed.

The applicant has requested a waiver from the requirements to install brick sidewalk along the York Street frontage. Instead, they offer to install new granite curbing and to mark out a five (5) foot wide bike lane along that frontage. Section 14-506 of the zoning ordinance pertains to waivers from curb and sidewalk installation and states the following:

Sec.14 –506 (b) Modifications.

(b) Where the planning board or planning authority finds that, for each of the requirements listed below, two or more of the conditions exist with respect to compliance with the requirements set forth in sections 14-498 and 14-499 pertaining to the provision

and construction of curbs and/or sidewalks, it may vary the regulations so that substantial justice may be done and the public interest secured:

Sidewalks-

1. There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site.
2. There is no sidewalk in existence or expected within 1000 feet and the construction of sidewalks does not contribute to the development of a pedestrian oriented infrastructure.
3. A safe alternative-walking route is reasonably available, for example, by way of a sidewalk on the other side of the street.
4. The reconstruction of the street is specifically identified in the first or second year of the current Capital Improvement Program.
5. The street has been constructed or reconstructed without sidewalks within the last 24 months.
6. Strict adherence to the sidewalk requirement would result in the loss of significant site features related to landscaping or topography that are deemed to be of a greater public value.

Please see the attached letter from Sebago Technics, Inc where they present how the applicant may meet the requirements for waivers ([Attachment 2](#)). The applicant proposes that they meet criteria (3) and (6) for a sidewalk waiver. Points raised by Sebago Technics are summarized as follows:

1. A safe alternative walking route is available by way of a sidewalk on the opposite side of York Street.
2. Strict adherence to the sidewalk requirement would result in the loss of significant site features related to topography that are deemed to be of greater public value. The seawall is a historic and permanent feature of the site. It's presence at the edge of the right of way makes it unsafe to install sidewalk closer to the wall's edge and could jeopardize stability of pole supports and guardrails.
3. If the sidewalk were, as an alternative, to be installed within the current paved section of roadway (thus narrowing the existing travel lanes), the sidewalk could not be constructed with an esplanade and would only be four (4) feet wide with a five (5) foot bike lane.
4. The necessity to relocate an existing catch basin would further tighten the separation of underground infrastructure along York Street.
5. Improvements could necessitate upgrades to the current guardrail system to bring it up to a safe elevation for the protection of City snowplowing equipment, adding additional significant cost to the development.
6. Constricting the geometry of York Street could create increase traffic queuing at the intersection of York and High Streets and would create a "pinch point" of approximately 30 ft for three lanes.

As previously stated, the applicant proposes to replace the existing curbing with new granite curbing and to paint a five (5) foot bike lane. They note that the street experiences a significant amount of bicycle traffic that is not currently accommodated. The elimination of the sidewalk on this side of York Street would permit safer lane widths, eliminate the need to re-signal the pedestrian crossing or modify existing drainage infrastructure and would provide a necessary safe link for bicycles navigating from the Casco Bay Bridge or the West End to the Old Port.

Dan Goyette, Consulting Engineer for the Department of Public Services has reviewed the applicant's request and submitted a memorandum stating:

The applicant has requested a waiver on the sidewalk requirement for York Street. They are partly basing their request on waiver criteria #6. The applicant states that installing the sidewalk as Public Services has requested inboard of the existing curb line, so there by reducing the street width, creates an unsafe condition. Both Tom Errico, Wilbur Smith Associate's traffic consultant to the City, and Jim Carmody, City Traffic Engineer, have reviewed the proposed 4' sidewalk and 5' shoulder/bike lane configuration and are comfortable with the proposal.

Jim Carmody and Tom Errico, City Transportation Engineers conducted a field review of existing roadway characteristics on York Street as they relate to the provision of a sidewalk. It is their professional opinion that the proposed layout as illustrated on the plan is acceptable from a public safety perspective. Provision of a 5-foot bicycle lane would serve bicyclists well and the 4-foot sidewalk would provide an enhanced pedestrian environment as is expected in an urban environment. The alignment of travel lanes would be adequate and the overall roadway cross-section would provide a good balance of multi-modal accommodations (See Tom Errico's review comments- [Attachment 8](#)).

The applicant identifies in their submittals that City standards require a significant level of infrastructure contribution and/or installation in the right of way for this development due to its location in the City and multiple frontages. As the applicant notes, Rufus Deering has existed at this location on Commercial Street for well over a century (since 1854) and seeks to redevelop, not to expand their business, but in order to remain competitive in their industry at this location.

SITE PLAN REVIEW (14-526 (a)) :

(1)(2)(24) Traffic, Parking and Circulation:

As previously noted, the proposal includes closing the Commercial Street driveway located nearest High Street. Commercial Street is a moratorium street, therefore temporary treatments such as bituminous curbing will be required at these and similar locations until the moratorium have expired. The remaining driveway has been revised as a two-way primary entrance. The proposed new driveway is 36 feet wide. Section III of the City's Technical Design Standards states that the maximum driveway width for a two way commercial driveway is 24 feet. Under special conditions, this width may be extended to 30 feet by the City Traffic Engineer if conditions warrant (e.g. – dedicated turn lanes). To be allowed to install a 36 foot drive, Rufus Deering must submit a written request to the Planning Board for a waiver along with additional information for review to the City Transportation Engineer, consisting of an Auto-Turn vehicle turning template graphic and traffic volume projections, if available. This information is pending. Due to the fact that Rufus Deering is proposing to consolidate their driveway cuts and if submitted information indicates the need, Public Services has no objection if the Planning Board should grant a waiver request to widen the proposed driveway cut to 36 feet (see memorandums from Tom Errico and from David Margolis- Pineo- [Attachments 7 and 8](#)).

The sidewalk at the corner of Commercial and High Street currently encroaches into the Rufus Deering property. The applicant proposes to insert at 40 foot radius so that the sidewalk is located in the public right-of-way. This change will also greatly benefit pedestrians by reducing the intersection crossing distance and will slow vehicle speeds. Again, because Commercial Street is a moratorium street, Public Services recommends that this work not take place until the moratorium expires.

The applicant may be required to contribute towards the signal installation at the Commercial Street/ High Street intersection. Contributions are calculated based on trip generation from proposed developments. Because the project is not changing substantially from a traffic generation perspective, this contribution will likely be minimal. It is the understanding of Planning staff that the applicant does not expect any additional traffic associated with the proposed project and that the development is intended to increase the efficiency and organization of the site, rather than to expand the business. If this is the case, the applicant has been asked to provide documentation that supports this for review and comment by the City Transportation Engineer. This information is pending.

The applicant proposes to add 30 parking spaces to the site. The spaces are 10 ft x 20 ft. Section III of the Technical and Design Standards states that parking spaces should be 9 ft x 19 ft. 10 ft x 20 ft spaces would require a written request to the Planning Board for a waiver from this standard.

Bicycle Parking:

The City Code requires two (2) bicycle parking spaces for every ten (10) vehicle spaces. This proposal would, therefore, require six (6) bicycle parking spaces located proximate to building entrances. No bicycle parking has been included as part of the development at this time. The Planning Board may reduce the required number of bicycle parking spaces upon review of evidence that the demand or usage would be below that required due to unique characteristics.

(3)(4) The bulk, location or height of proposed buildings and structures

The structure is not negatively impacting abutting uses.

(5)(8) Utilities, stormwater, drainage and erosion control

The applicant submitted confirmation from Portland Water District that there is adequate capacity to serve the development. A letter from the Portland Sewer Division has been requested by the applicant as is pending.

The applicant has not yet submitted a stormwater management plan for this proposal. The proposal does not add additional impervious areas, as the site is currently almost entirely impervious. Proposed landscaping will incrementally reduce the amount of impervious surface at the site. The applicant proposes to maintain existing drainage patterns, allowing sheet flow towards Commercial Street or, if recommended, to install collection areas where possible. This would be difficult given poor existing soils and limited depth (see applicant's narrative- Attachment 1). Dan Goyette, Consulting Engineer has provided review comment concerning this proposal (Attachment 5). Dan has asked the applicant to submit a Stormwater Management Plan and flow calculations.

The proposal includes the addition of 30 parking spaces. According to the City of Portland Technical and Design Standards, if a parking lot proposes 25 or more parking spaces then on-site stormwater treatment is required. Location of erosion control measures (i.e. filter barrier and silt sacks) should be shown on the drawings and snow storage should be addressed with snow storage areas indicated on the plans.

(6)(7) Landscaping

The project proposes to add additional landscaping (daylily) and street trees (Japanese tree lilac) along the Commercial Street frontage. The applicant proposes to add three street trees (ash) along High Street and a planting bed at the corner of High and York Streets. Along Maple Street, the applicant proposes two new street trees (ash) installed with tree grates and landscaping

(daylily). Interior to the site, the front (east) façade of the proposed retail building will be landscaped with reed grass and bonica shrubrose. Jeff Tarling, City Arborist has reviewed the latest landscape plan and finds it acceptable (Attachment 6). He agrees with the applicant that street trees along York Street should be avoided and may not be feasible due to the potential for conflict between the sea wall and tree root systems.

(9) Lighting

Information on existing and proposed lighting is pending.

(10) Fire Safety

Captain Greg Cass of the Portland Fire Department has reviewed and approved the proposal (Attachment 4). The applicant had originally proposed to add an additional hydrant; however Captain Cass determined that the four (4) existing hydrants adequately serve the site.

(11) Off-premises infrastructure, existing or planned by the city

As previously stated, the section of Commercial Street abutting the site is a moratorium street. Public Services suggests the installation of a temporary bituminous curb and sidewalk at the location of the driveway to be closed. The installation of the granite curb and brick sidewalk can then take place at the same time as the work required to reconfigure that corner of the street to move the sidewalk outside the property line after the moratorium has expired.

(20)(21)Environmental Impact:

The proposal is for the development of an existing developed site. The use will not be changed. The applicant is required to submit a stormwater management plan for review to determine that the proposed develop will not have adverse environmental effects due to the quality or quantity of stormwater discharge.

NEXT STEPS

- Provide further information re Stormwater
- Address other development review comments
- Submit necessary waiver requests
- Provide details of proposed lighting
- Host a Neighborhood Meeting

ATTACHMENTS:

1. Application Materials:
 - a. Letter, dated July 11, 2008
 - b. Development review application
 - c. Site plan checklist
 - d. Site location map
 - e. Capacity letter- Portland Water District, dated June 4, 2008
 - f. Deed
2. Updated written statement and waiver request, dated August 22, 2008
3. Memorandum from Marge Schmuckal, Zoning Administrator, dated August 27, 2008
4. Memorandum from Captain Greg Cass, dated August 13, 2008
5. Memorandum from Dan Goyette, Consulting Engineer to Public Services
 - a. Dated July 9, 2008
 - b. Dated September 3, 2008
6. Memorandum from Jeff Tarling, City Arborist, dated September 5, 2008

7. Memorandum from David Margolis-Pineo, dated September 5, 2008
8. Memorandum from Tom Errico, Consulting Transportation Engineer to Public Services, dated September 4, 2008
9. Existing conditions and site plans

PLANNING BOARD REPORT #62-08

**RUFUS DEERING LUMBER COMPANY
VICINITY OF 383 COMMERCIAL STREET
SITE PLAN REVIEW
RUFUS DEERING LUMBER COMPANY, APPLICANT**

Submitted to:

Portland Planning Board
Portland, Maine
November 14, 2008

I. INTRODUCTION

Rufus Deering Lumber Company has requested Planning Board review and approval of their proposal to redevelop the existing Rufus Deering Lumber store and lumber yard site at 383 Commercial Street. The parcel is located on the block bounded by Commercial Street, High Street, York Street and Maple Street near the Commercial Street waterfront. The development proposal includes renovating the existing "red barn" warehouse building into a retail store, constructing an approximately 29,000 sf drive-through warehouse, associated parking lot improvements and reconfiguring existing on-site lumber and material storage areas. The project is being reviewed as a major site plan in the B5-b zone. The applicant has submitted a partial waiver request to the Planning Board from the installation of curb and sidewalk along York Street as part of their application.

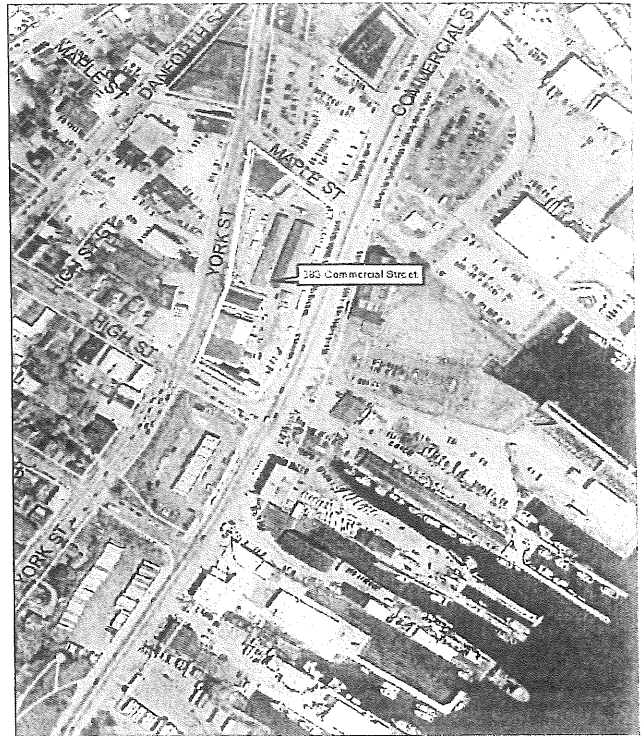


Image 1- Site Location

Representatives for the applicant include Sebago Technics and Johnson Design Resources, Architects.

Jim SEVADUR

106 notices were sent to area residents. A notice also appeared in the November 17th and November 18th editions of the *Portland Press Herald*.

II. SUMMARY OF FINDINGS

Zoning:	B5-b Urban Commercial Business Zone
Parcel Size:	122,363 SF (2.8 acres)
Required Vehicle/Bicycle Parking:	None
Proposed Parking:	30 vehicle spaces
Uses:	Commercial lumber store and lumber yard
Street frontage:	567 feet (Commercial Street) 262.5 feet (Maple Street) 620 feet (York Street) 172.5 feet (High Street)
Sq. footage- retail bldg:	10,650 sq. ft. footprint
Sq. footage- drive-thru warehouse:	28961 sq. ft. footprint

III. PROPOSED DEVELOPMENT

Rufus Deering proposes to update and expand their existing commercial lumber yard and retail operation. According to the applicant, the primary rationale behind the redevelopment is to improve efficiency and safety. In their correspondence with the planning authority, the applicant notes that the yard has become increasingly antiquated, creating excessive losses due to cull

lumber, boards that become weathered, split and unusable. The goal of the project is to combat this by becoming increasingly efficient and to get more of their product stored under cover (Attachments 2 and 14). The business has been located on or in the vicinity of this site along the northerly side of Commercial Street since 1854. The parcel occupies the entire block bounded by Commercial, High, York and Maple Streets. York Street runs the length of the westerly side (rear) of the property, approximately 25 feet above the parcel on top of the existing stone sea wall. The site includes a lumber yard with three (3) open-sided storage warehouses totaling 18,376 sq. ft., two (2) partially to fully enclosed storage warehouses totaling 17,726 sq. ft, and two (2) office buildings at the north and south corners of the block. These office buildings are 2,998 sq. ft and 7,540.2 sq. ft ground floor area, respectively. The 7,540.2 sq. ft office building at the northern corner of the block is owned by Rufus Deering but is not part of the Rufus Deering lumber yard operation and is not under review as part of this development proposal.

The proposed development includes the following: Two of the existing open sided warehouses, the existing Quonset hut and a portion of the third open-sided warehouse will be demolished to allow for the development of a 27.5 ft tall, 28,961 sq. ft drive-through warehouse. A portion of the third open-sided warehouse will be rebuilt and re-oriented to accommodate revised access and internal circulation for the site. A small existing storage shed along the Commercial Street frontage will also be relocated on the site. The 11,404 sq. ft. "red barn" warehouse at the westerly corner of the site, near York and High streets will be remodeled into a 10,648 sq. ft. retail store. There are no changes proposed for the existing 3,000 sq. ft office building at the south corner of the parcel near the intersection of Commercial and High Streets. The applicant proposes to add 30 on-site parking spaces... The applicant proposes to consolidate the two existing curb cuts along Commercial Street into a single 36 ft wide access drive. The existing curb cut along Maple Street would remain open but would be restricted to deliveries only and would be secured with a ten (10) ft chain link sliding gate.

IV. THRESHOLD QUESTIONS FOR THE BOARD'S CONSIDERATION

A. Request for a waiver from the requirements of Section 25-96 that curb and sidewalk be installed along the York Street frontage: Pursuant to Section 25-96 of the City Code, the development is required to install sidewalks and granite curbing on all abutting streets. This site encompasses an entire city block, thus this provision applies to all four frontages. The parcel has approximately 620 feet of frontage along York Street along the rear of the site. The rear of the parcel consists of an existing granite sea wall, approximately 25 feet tall, with York Street running along the top, thus there is no access to the site from York Street. York Street has a sidewalk on the west (opposite) side of the street only and the existing granite curbing on the east side along the Rufus Deering property is in poor condition with low reveal.

The applicant has requested a waiver from the requirements to install brick sidewalk along their York Street frontage (see Attachment 2). As an alternative, they propose to install new granite curbing and to mark out a five (5) foot wide bike lane along that frontage, providing bicycle infrastructure to supplement the existing pedestrian infrastructure on the opposite side of the street. Section 14-506 of the zoning ordinance pertains to waivers from curb and sidewalk installation. Section V (B) of this report provides additional discussion of this issue.

V. STAFF REVIEW

The proposed development has been reviewed by staff for conformance with the relevant review standards of site plan ordinance and zoning requirements of the B5-b zone. Staff comments are highlighted in this report.

A. Zoning Review:

Lumber and building materials dealers in existence before the passage of the ordinance are permitted in the B5-b zone. Rufus Deering Lumber Company has been in existence at this site for approximately 150 years, making it a permitted use. Marge Schmuckal, Zoning Administrator has reviewed the proposal and her comments are included as Attachment 3a and 3b.

The proposed modifications to the site would bring it into greater conformance with current ordinance requirements by lessening existing nonconformities, namely that structures exceed the maximum ten (10) ft front yard setback requirement for the B-5b zone. The existing site includes a front yard setback of approximately 30 feet. This is being reduced under the current proposal to approximately 16 feet. The Zoning Ordinance encourages such lessening of nonconformity on already developed sites.

Section 14-230.5 lists performance standards for the B5-b zone. According to this section, storage of new materials, finished products, or related equipment shall be suitably screened from the public way and from abutting properties by a solid fence at least five (5) feet in height or by a solid evergreen planting strip. The applicant has addressed conformance with this provision in their letter, dated August 22, 2008 (Attachment 2). The applicant comments that day to day operations of a lumberyard make it necessary to store lumber and building materials outdoors. Typically, these materials are stacked upwards of sixteen (16) feet high. This existing condition would not change as a result of their development proposal. The performance standard requiring screening of such materials with a five (5) foot fence would be inadequate for screening of this particular type of outdoor storage. While the applicant would be willing to provide opaque 5 ft fencing along the Commercial Street frontage if necessary to comply with Zoning, Rufus Deering buildings and buildings in the vicinity have been vandalized in the past and there is concern that opaque fencing would become a regular target of vandalism. Marge Schmuckal comments in her review that, because of the existing and historic use of this site, she does not think the full force of this provision applies. Marge has determined that the existing nonconformities have been lessened and the property has come into better compliance with zoning as a result of this proposal.

Elevation drawings of the proposed drive-thru warehouse have been included as Attachment 13. The maximum building height in the B5-b zone is 65 feet. According to the applicant, the proposed drive-through warehouse will not exceed 27.5 feet in height, well below that permitted by zoning. Elevation drawings have not yet been submitted for the proposed renovations to the red barn warehouse into a retail showroom. The applicant proposes to design the façade to match that of the abutting office building at the corner of Commercial and High Street.

Proposed Condition of Approval: Planning staff proposes a condition of approval that the applicant submits elevation drawings for the proposed retail showroom for review and approval by the planning authority prior to the issuance of a building permit.

No proposals for new signage have been included as part of this application. Separate permits shall be required for any new signage.

B. Site Plan Review:

(1)(2)(24) Traffic, Parking and Circulation:

As stated above, the applicant is asking for a curb and sidewalk waiver for this project. Please see the applicant's statement included as Attachment 2. Pursuant to Section 25-96 of the City Code, this development is required to install sidewalks and granite curbing on all abutting streets. This site encompasses a city block thus, this provision applies to all four frontages. Currently,

Commercial Street is constructed with granite curbing and concrete sidewalk. Maple Street is partially constructed with granite curbing and brick sidewalk with no sidewalk on the opposite side of the street. York Street has no sidewalk and granite curbing that is in poor condition with low reveal. There is sidewalk along the opposite side of York Street of various materials and conditions. High Street has bituminous sidewalk and curbing along both sides of the street. The applicant proposes to install new brick sidewalk and curbing along High Street, along the remaining portions of Maple Street and on Commercial Street where the existing driveway will be closed.

The applicant has requested a waiver from the requirements to install brick sidewalk along the York Street frontage. Instead, they offer to install new granite curbing and to mark out a five (5) foot wide bike lane along that frontage. Section 14-506 of the zoning ordinance pertains to waivers from curb and sidewalk installation and states the following:

Sec.14 -506 (b) Modifications.

(b) Where the Planning Board or planning authority finds that, for each of the requirements listed below, two or more of the conditions exist with respect to compliance with the requirements set forth in sections 14-498 and 14-499 pertaining to the provision and construction of curbs and/or sidewalks, it may vary the regulations so that substantial justice may be done and the public interest secured:

Sidewalks-

- 1. Partial There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site. ✓
- 2. no There is no sidewalk in existence or expected within 1000 feet and the construction of sidewalks does not contribute to the development of a pedestrian oriented infrastructure. ✓
- 3. yes A safe alternative-walking route is reasonably available, for example, by way of a sidewalk on the other side of the street.
- 4. no The reconstruction of the street is specifically identified in the first or second year of the current Capital Improvement Program.
- 5. no The street has been constructed or reconstructed without sidewalks within the last 24 months.
- 6. Partial Strict adherence to the sidewalk requirement would result in the loss of significant site features related to landscaping or topography that are deemed to be of a greater public value. |

Please see the attached letter from Sebago Technics, Inc where they present how the applicant may meet the requirements for waivers (Attachment 2). The applicant proposes that they meet criteria (3) and (6) for a sidewalk waiver. Points raised by Sebago Technics are summarized as follows:

- 1. A safe alternative walking route is available by way of a sidewalk on the opposite side of York Street.
- 2. Strict adherence to the sidewalk requirement would result in the loss of significant site features related to topography that are deemed to be of greater public value. The seawall is a historic and permanent feature of the site. It's presence at the edge of the right of way makes it unsafe to install sidewalk closer to the wall's edge and could jeopardize stability of pole supports and guardrails.

3. If the sidewalk were, as an alternative, to be installed within the current paved section of roadway (thus narrowing the existing travel lanes), the sidewalk could not be constructed with an esplanade and would only be four (4) feet wide with a five (5) foot bike lane.
4. The necessity to relocate an existing catch basin would further tighten the separation of underground infrastructure along York Street.
5. Improvements could necessitate upgrades to the current guardrail system to bring it up to a safe elevation for the protection of City snowplowing equipment, adding additional significant cost to the development.
6. Constricting the geometry of York Street could create increase traffic queuing at the intersection of York and High Streets and would create a "pinch point" of approximately 30 ft for three lanes.

As previously stated, the applicant proposes to replace the existing curbing with new granite curbing and to paint a five (5) foot bike lane. They note that the street experiences a significant amount of bicycle traffic that is not currently accommodated. The elimination of the sidewalk on this side of York Street would permit safer lane widths, eliminate the need to re-signal the pedestrian crossing or modify existing drainage infrastructure and would provide a necessary safe link for bicycles navigating from the Casco Bay Bridge or the West End to the Old Port. The applicant has submitted color aerial plans depicting their proposal versus the requirement that a brick sidewalk and bike lane be constructed (Attachment 12). The aerial is intended to illustrate the applicant's point that the geometry of York Street could be constricted and might result in a "pinch point" at the intersection of York and High Streets.

Dan Goyette, Consulting Engineer for the Department of Public Services has reviewed the applicant's request and submitted a memorandum stating:

The applicant has requested a waiver on the sidewalk requirement for York Street. They are partly basing their request on waiver criteria #6. The applicant states that installing the sidewalk as Public Services has requested inboard of the existing curb line, so there by reducing the street width, creates an unsafe condition. Both Tom Errico, Wilbur Smith Associate's traffic consultant to the City, and Jim Carmody, City Traffic Engineer, have reviewed the proposed 4' sidewalk and 5' shoulder/bike lane configuration and are comfortable with the proposal.

Jim Carmody and Tom Errico, City Transportation Engineers conducted a field review of existing roadway characteristics on York Street as they relate to the provision of a sidewalk. It is their professional opinion that the proposed layout as illustrated on the plan is acceptable from a public safety perspective. Provision of a 5-foot bicycle lane would serve bicyclists well and the 4-foot sidewalk would provide an enhanced pedestrian environment as is expected in an urban environment. The alignment of travel lanes would be adequate and the overall roadway cross-section would provide a good balance of multi-modal accommodations (See Tom Errico's review comments- Attachment 8).

The applicant identifies in their submittals that City standards require a significant level of infrastructure contribution and/or installation in the right of way for this development due to its location in the City and multiple frontages. During the review process, the planning authority explored the possibility of including this area of York Street in an upcoming CIP project, however this area is not a priority under the Capital Improvement program at this time. At the request of the planning authority, the applicant submitted estimates of the cost of constructing the York Street sidewalk improvements, as required along with the estimated project cost for constructing the sidewalk improvements along High Street and Maple Street, as proposed (Attachment 9).

According to the applicant's calculations, the base estimated project cost for the York Street sidewalk improvements would be \$88,625.00. Public Services reviewed this estimate and felt that the cost would be somewhat less, with an estimated total of \$73,000.00. According to the applicant's calculations, the total adjusted estimated project cost for all sidewalk improvements, including York Street would be \$147,174.50. As the applicant notes, Rufus Deering has existed at this location on Commercial Street for well over a century (since 1854) and seeks to redevelop, not to expand their business, but in order to remain competitive in their industry at this location. The applicant has determined that the projected cost of constructing a sidewalk along York Street could render their project financially unfeasible. It is not expected that pedestrians coming from or going to the Rufus Deering site will utilize the York Street sidewalk, because there is no possibility of pedestrian access from this frontage. There is some pedestrian traffic on York Street within this vicinity.

Due to its proximity to the Commercial and High Street intersection, the applicant proposes to close the existing driveway along Commercial Street located nearest to High Street, and establish a two way primary entrance closer to the middle of the Commercial Street property frontage. On-street parking conditions on Commercial Street may have to be modified to accommodate this primary driveway. In addition, during the review process it was determined that the public right of way currently encroaches onto Rufus Deering's property. To remedy this, the applicant proposes to rebuild the corner at High Street and Commercial, reducing the northeastern radius on the corner of Commercial and High Street and to move the sidewalk back into the public right of way. Jim Carmody, City Transportation Engineer and Tom Errico, Consulting Transportation Engineer have reviewed this proposal and determined that correcting this will result in an overall improved condition at that corner (see staff review comments, Attachment 8). Commercial Street is a moratorium street, however, Public Services and Maine DOT have concluded that due to the nature of the improvements, the proposed scope of work can proceed prior to expiration of the moratorium.

Waiver Request and Proposed Condition of Approval: The applicant has requested a waiver for the driveway width of the new primary access along Commercial Street. The proposed new driveway is 36 feet wide. Section III of the City's Technical Design Standards states that the maximum driveway width for a two way commercial driveway is 24 feet. Under special conditions, this width may be extended to 30 feet by the City Traffic Engineer if conditions warrant. Public Services has reviewed the applicant's request and supports the driveway width waiver in order to accommodate access and egress for larger vehicles. It is recommended, however, that the driveway be marked for one wide entry and one wide exit lane only, rather than the three (3) lanes, as shown on the plans (see Tom Errico's review comments, Attachment 8). Planning staff recommends a condition of approval that the final plans be revised to include two (2) lanes only at the primary Commercial Street driveway, consisting of an 18 ft ingress lane and an 18 ft egress lane.

The City of Portland has been collecting monetary contributions for the future installation of a traffic signal at the Commercial Street/High Street intersection. Contributions are calculated based on trip generation from proposed developments. According to the November 5, 2008 analysis prepared by Sebago Technics, Inc (Attachment 13), the proposed project is expected to generate an additional 24 driveway trips during the PM peak hour. Based upon prior development contribution levels, the proposed project should contribute \$3,900.00 towards the future installation of a traffic signal at the Commercial Street/High Street intersection.



Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant contribute \$3,900.00 into a City infrastructure account to be allocated towards the

installation of a traffic signal at the Commercial and High Street intersection prior to the issuance of a Certificate of Occupancy.

The applicant proposes to add 30 parking spaces to the site. The spaces are 10 ft x 20 ft. Tom Errico has reviewed the proposal and supports parking stall sizes that exceed City of Portland Technical Standards due to the type of business activity occurring on the site. No bicycle parking is required for this development proposal due to the fact that there is no minimum parking requirement.

(3)(4) The bulk, location or height of proposed buildings and structures

The proposed drive-thru warehouse is 27.5 feet tall and would not appear to create an adverse impact to abutting uses.

(5)(8) Utilities, stormwater, drainage and erosion control

The applicant submitted confirmation from Portland Water District that there is adequate capacity to serve the development. A letter from the Portland Sewer Division has been requested by the applicant and is pending.

Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant submit evidence of adequate sewer capacity prior to the issuance of a building permit.

The applicant has submitted a stormwater analysis with flow calculations (Attachment 11) along with responses to Public Services review comments concerning stormwater management (Attachment 2-b). The proposal does not add additional impervious area to the site, as the site is currently almost entirely impervious. Proposed landscaping will incrementally reduce the amount of impervious surface at the site however it will remain near 100%. The applicant proposes to maintain existing drainage patterns, allowing sheet flow towards Commercial Street. The existing inverts in Commercial Street limit the proposed drainage improvements to too shallow a depth to install effective treatments. In lieu of onsite treatment, the City has asked that the applicant contribute \$20,000 to an infrastructure account to be allocated towards improvements to the stormwater outfall on the Berlin Mill Wharf, which has begun to fail and needs to be rebuilt. Dan Goyette, Consulting Engineer has provided review comments (Attachment 5). The amount of the contribution was calculated based on the size of the proposed parking area.

Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant contribute \$20,000 to a City infrastructure account to be allocated towards improvements to the stormwater outfall on the Berlin Mill Wharf prior to the issuance of a Certificate of Occupancy.

The applicant proposes to remove snow rather than storing it on site. Note 26 on sheet 4 of the site plan indicates that the applicant will contract with a local firm for removal of snow.

(6)(7) Landscaping

The project proposes to add additional landscaping (daylily) and street trees (Japanese tree lilac) along the Commercial Street frontage. The applicant proposes to add three street trees (ash) along High Street and a planting bed at the corner of High and York Streets. Along Maple Street, the applicant proposes two new street trees (ash) installed with tree grates and landscaping (daylily). Interior to the site, the front (east) façade of the proposed retail building will be landscaped with reed grass and bonica shrubrose. Jeff Tarling, City Arborist has reviewed the latest landscape plan and finds it acceptable but would like the applicant to plant a new street tree in the empty tree well along the Commercial Street frontage (Attachment 6-b). He agrees with

the applicant that street trees along York Street should be avoided if this frontage is to be improved and may not be feasible due to the potential for conflict between the sea wall and tree root systems.

Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant amend the final plans to include one additional Japanese tree lilac planted in the existing empty tree well along Commercial Street prior to the issuance of a building permit.

(9) Lighting

The applicant proposes to update the existing lighting at the site as part of their proposal. They have submitted catalogue cuts or photometric information but these were received subsequent to this report and have not been fully reviewed. Existing lighting on the site includes building mounted open lights that illuminate the parking area.

Proposed Condition of Approval: Planning staff recommends a condition of approval that the applicant submits lighting information, including a photometric plan showing that all proposed lighting meets City Technical and Design Standards. These materials shall be reviewed and approved by the planning authority prior to the issuance of a building permit.

(10) Fire Safety

Captain Greg Cass of the Portland Fire Department has reviewed and approved the proposal (Attachment 4). The applicant had originally proposed to add an additional hydrant; however Captain Cass determined that the four (4) existing hydrants adequately serve the site.

(20)(21) Environmental Impact:

The proposal is for the development of an existing developed site. The use will not be changed and the corresponding impact will remain unchanged. The applicant submitted a stormwater analysis for review and proposes to pay into an infrastructure account to offset the stormwater impact from the site. Please refer to Section V (B) - *Utilities, stormwater, drainage and erosion control* for additional discussion.

VI. MOTIONS FOR THE BOARD TO CONSIDER

WAIVERS:

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in Planning Board Report #62-08 relevant to the Portland's Technical and Design Standards and other regulations, and the testimony presented at the Planning Board hearing:

1. The Planning Board (waives/does not waive) the requirement of Section III of the City's Technical Design Standards that the maximum driveway width for a two way commercial driveway be 30 feet, thus permitting a 36 ft driveway with an 18 ft ingress lane and an 18 ft egress lane.
2. The Planning Board (waives/does not waive) Section 25-96 of the City Code, requiring that the applicant install sidewalk along the section of York Street abutting the development, based on the following waiver criteria as outlined below:

Sec.14 -506 (b) Modifications.

(b) Where the planning board or planning authority finds that, for each of the requirements listed below, two or more of the conditions exist with respect to compliance with the

requirements set forth in sections 14-498 and 14-499 pertaining to the provision and construction of curbs and/or sidewalks, it may vary the regulations so that substantial justice may be done and the public interest secured:

Sidewalks-

1. There is no reasonable expectation for pedestrian usage coming from, going to and traversing the site.
2. There is no sidewalk in existence or expected within 1000 feet and the construction of sidewalks does not contribute to the development of a pedestrian oriented infrastructure.
3. A safe alternative-walking route is reasonably available, for example, by way of a sidewalk on the other side of the street.
4. The reconstruction of the street is specifically identified in the first or second year of the current Capital Improvement Program.
5. The street has been constructed or reconstructed without sidewalks within the last 24 months.
6. Strict adherence to the sidewalk requirement would result in the loss of significant site features related to landscaping or topography that are deemed to be of a greater public value.

SITE PLAN

On the basis of plans and materials submitted by the applicant and on the basis of information contained in Planning Report #62-08 relevant to standards for site plan regulations, and other findings, the Planning Board finds that the plan is / is not in conformance with the site plan standards of the land use code, subject to the following conditions:

Potential Conditions of Approval:

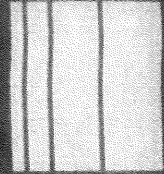
1. ~~If the planning Board requires the construction of sidewalk and a bicycle lane along York Street, the applicant shall submit revised plans showing the sidewalk engineered to City standards along York Street for review and approval by the Planning Authority.~~
2. The applicant shall submit elevation drawings for the proposed retail showroom for review and approval by the planning authority prior to the issuance of a building permit.
3. ^{reality} The applicant submitted lighting information, including a photometric plan showing that proposed lighting meets City Technical and Design Standards shall be reviewed and approved by the planning authority prior to the issuance of a building permit.
4. The applicant shall submit revised plans showing one additional Japanese tree lilac planted in the existing empty tree well along Commercial Street for review and approval by the City Arborist prior to the issuance of a building permit.
5. The applicant shall contribute \$20,000 to a City infrastructure account to be allocated towards improvements to the stormwater outfall on the Berlin Mill Wharf prior to the issuance of a Certificate of Occupancy.
6. The applicant contribute \$3,900.00 to a City infrastructure account to be allocated towards the installation of a traffic signal at the Commercial and High Street intersection prior to the issuance of a Certificate of Occupancy.

*crimson and
bicycle lane
city*

*16' x 16' built within
10 years
with a replacement
10 years*

ATTACHMENTS:

1. Application Materials:
 - a. Letter, dated July 11, 20008
 - b. Development review application
 - c. Site plan checklist
 - d. Site location map
 - e. Capacity letter- Portland Water District, dated June 4, 2008
 - f. Deed
 - g. Neighborhood Meeting documentation
2. Correspondence from the applicant
 - a. Updated written statement and waiver request, dated August 22, 2008
 - b. Letter from the applicant, dated October 15, 2008
3. Zoning Review
 - a. Memorandum from Marge Schmuckal, Zoning Administrator, dated August 27, 2008
 - b. Final comments, dated November 14, 2008
4. Memorandum from Captain Greg Cass, dated August 13, 2008
5. Engineering Review
 - a. Memorandum from Dan Goyette, Consulting Engineer to Public Services, dated July 9, 2008
 - b. Memorandum from Dan Goyette, Consulting Engineer to Public Services, dated September 3, 2008
 - c. Final comments, dated November 3, 2008
6. Landscaping Review
 - a. Memorandum from Jeff Tarling, City Arborist, dated September 5, 2008
 - b. Final comments, dated November 14, 2008
7. Memorandum from David Margolis-Pineo, dated September 5, 2008
8. Transportation Review
 - a. Memorandum from Tom Errico, Consulting Transportation Engineer to Public Services, dated September 4, 2008
 - b. Final comments, dated November 14, 2008
9. Cost estimate spreadsheets submitted by applicant
10. Email concerning cost estimates from David Margolis Pineo, Public Services, dated October 1, 2008
11. Stormwater analysis, submitted by Sebago Technics
12. Aerial plans, showing York Street scenarios, submitted by applicant.
13. Trip generation calculations submitted by Sebago Technics, dated November 5, 2008
14. Letter to Molly Casto from Dan LaBrie, Rufus Deering Lumber, dated November 7, 2008
15. Site plans and elevation drawings
16. Lighting Photometric Plan and Specification Sheet



Att. 1-a

sebagotechnics.com

One Chabot Street
P.O. Box 1339
Westbrook, Maine
04098-1339
Ph. 207-856-0277
Fax 856-2200

June 11, 2008
07383

Ms. Barbara Barhydt, Senior Planner
City of Portland Planning Division,
City Hall 4th Floor
389 Congress St,
Portland, ME 04101

Major Site Plan Application Submittal - Site Redevelopment Plan
Rufus Deering Lumber Company's - Proposed Drive-Thru Warehouse and Retail Store
383 Commercial St. Portland, Maine

Dear Ms. Barhydt:

On behalf of Rufus Deering Lumber Company, please find nine (9) copies of the Site Plan Application and associated design plans for the Proposed Drive-Thru Warehouse and Retail Store, to be located on the street block surrounded by Commercial Street, High Street, York Street, and Maple Street in Portland, Maine. The parcel proposed for redevelopment is the existing Rufus Deering Lumber store and lumberyard site (383 Commercial St). The property consists of 2.83 acres of fully developed lumberyard, which is an allowed land use in the B-5b (Urban Commercial Business Zone).

In existence since 1854, the company has determined that to compete in their market they must become more efficient and provide customers with a more convenient method of receiving products. Hence, Rufus Deering has proposed the following application and plans, for the renovation of the existing red barn and re-development of the yard area. The renovation work associated with the proposed retail building will occur over approximately 10,650 square feet of existing building footprint and include minor parking lot improvements and reorganization of the outside storage areas for lumber and building materials/products. Yard redevelopment will include the construction of an approximately 29,000 square foot drive-through warehouse. Additionally, we will be required to reconfigure a section of their current storage shelter adjacent to Commercial Street, in order to accommodate access and internal circulation to the proposed warehouse.

Due to the limited size and scope of the project, we do not anticipate that any Maine Department of Environmental Protection (MDEP) permits will be required. The overall improvement of the site will not add any additional impervious areas, as the site is now nearly completely impervious. In fact, the proposed landscaping areas will reduce the total impervious area slightly.

Stormwater is a difficult design given the poor existing soils, limited depth for positive pipe drainage, and difficulty making connections into Commercial Street given the multiple conflicts with existing utilities. Based on the lumberyard's necessary layout, we felt the only practical measure will be to maintain existing drainage patterns and collection areas where possible (although drainage is shallow), or continue to allow sheet flow into the Commercial Street drainage system as it currently operates. We feel we have addressed drainage and that, due to the conversion of ground surface into roof area, runoff water quality is improved. We have held meetings with the City Engineer, and have determined there is limited area and grades to accomplish stormwater treatment.

As part of the application we will be improving street access, internal circulation, and providing more landscaping to improve the visual presentation of the Rufus Deering Lumber facility. The original Office building will remain, and the character of the renovated Red Barn will attempt to match the characteristics of the original store. Aside from stormwater, we do not anticipate many major upgrades to the site other than providing new utility connections for power, and upgrading the water services for fire protection for the new building/additions. The Applicant will continue to contract with a solid waste removal service.

An "Ability to Serve" letter has been received from The Portland Water District, a copy of which is included in this application. A similar letter has been requested from the Portland Sewer Division, and will be forwarded upon receipt. In planning the water main upgrades we received indications from the Fire Department for the need of a fire hydrant on the ocean side of York Street, such that in the event of fire on the site, hydrant access could be made to the site without shutting down York Street/High Street through-traffic from the bridge. To accommodate that request we are proposing a hydrant on the High Street and York Street corner, which taps into a newer 12 inch main. The section of water main in York Street is smaller and antiquated, and a hydrant installation could be hampered by the existing seawall. There are currently two accessible hydrants along the property's frontage on Commercial Street, and another at the intersection of York Street and Maple Street.

Rufus Deering Lumber Company will be holding an informational meeting and required public meetings with abutters, and we expect to be meeting again with both the Planning and Public Works Departments to review the overall plans. Given that the sidewalks are in poor condition on High Street and a section of Maple, we have assumed that as part of our plan those will be required to be reconstructed with granite curbing and brick surface. However, no sidewalk currently exists along this section of York Street and, due to the presence of the original City seawall, construction of a sidewalk in this location is impractical. The seawall creates restrictions with respect to the spacing needed to design a safe sidewalk, the constructability of a sidewalk due to the seawall materials, and the need to install a guardrail system into the existing stone wall. In lieu of a sidewalk, we are offering to landscape this section with plant vegetation/hedge. We feel that this will enhance the visual character of the street and proposed site.

Because the proposed improvements are intended to increase the efficiency and organization of the site (and not to expand the business), the owner does not expect an increase in overall customer traffic, as a main component of their business is contractor related. Therefore, we do not anticipate the need for any traffic improvements, or traffic study. We will, however, provide an analysis of entrance operations, internal delivery movements, and the need to reduce some on-street parking to improve sight safety at the site.

Rufus Deering Lumber Company is anticipating construction start date in Spring of 2009. We are looking forward to working with City staff to facilitate approvals in coordination with Portland Site Plan approval. We will be providing architectural renderings and lighting plans as details of the site become available.

In the interim, we are available at your request to hold any type of application review meeting or to meet the City's review staff to present the project on an informal basis. We feel that, given site design parameters, this would be beneficial for both the City planners and the Rufus Deering Lumber Company design team. If you have any questions please feel free to contact me at our office (856-0277). We look forward to meeting with the Planning Division and the Planning Board for a workshop meeting as soon as an agenda allows.

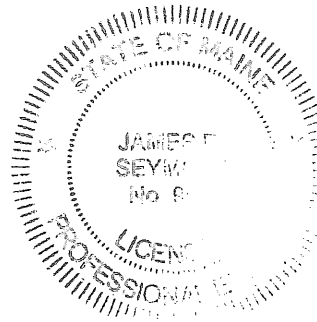
Sincerely,

SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager

JRS:cb
Enc.



cc: Jay Breard- Rufus Deering Lumber Company



**Development Review Application
Portland, Maine**

Department of Planning and Development, Planning Division and Planning Board

Address of Proposed Development: 383 COMMERCIAL STREET PORTLAND, ME		
Zone: B-5b, URBAN COMMERCIAL BUSINESS		
Project Name: RUFUS DEERING LUMBER		
Existing Building Size: 15,056 sq. ft.	Proposed Building Size: 22,911 sq. ft.	
Existing Acreage of Site: 122,343 sq. ft.	Proposed Acreage of Site: 122,343 sq. ft.	
Proposed Total Disturbed Area of the Site: 41,420 sq. ft. *		
* If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) or Chapter 500, Stormwater Management Permit with the Maine Department of Environmental Protection (DEP).		
Tax Assessor's Chart, Block & Lot: Chart # 042 Block # A Lot # 001	Property Owners Name/ Mailing address: RUFUS DEERING LUMBER 383 COMMERCIAL ST PORTLAND, ME 04101 C/O DON LABRIC	Telephone #: 772-6505 Cell Phone #:
Consultant/Agent Name, Mailing Address, Telephone #, Fax # and Cell Phone # : JAMES SEYMOUR 46 SERENO TECHNICS 1 CHABOT ST / PO BOX 1339 WESTBROOK, ME 04098 PH: 856-6277	Applicant's Name/ Mailing Address: SAME AS OWNER	Telephone #: Cell Phone #:
Fee for Service Deposit (all applications) <input checked="" type="checkbox"/> (\$200.00)		
Proposed Development (check all that apply)		
<input checked="" type="checkbox"/> New Building ___ Building Addition ___ Change of Use ___ Residential ___ Office <input checked="" type="checkbox"/> Retail ___ Manufacturing ___ Warehouse/Distribution ___ Parking lot ___ Subdivision (\$500.00) + amount of lots _____ (\$25.00 per lot) \$ _____ + major site plan fee if applicable ___ Site Location of Development (\$3,000.00) (except for residential projects which shall be \$200.00 per lot _____) ___ Traffic Movement (\$1,000.00) ___ Storm water Quality (\$250.00) ___ Section 14-403 Review (\$400.00 + \$25.00 per lot) ___ Other _____		
~ Please see next page ~		

Major Development (more than 10,000 sq. ft.)

- Under 50,000 sq. ft. (\$500.00)
- 50,000 - 100,000 sq. ft. (\$1,000.00)
- Parking Lots over 100 spaces (\$1,000.00)
- 100,000 - 200,000 sq. ft. (\$2,000.00)
- 200,000 - 300,000 sq. ft. (\$3,000.00)
- Over 300,000 sq. ft. (\$5,000.00)
- After-the-fact Review (\$1,000.00 + applicable application fee)

Minor Site Plan Review

- Less than 10,000 sq. ft. (\$400.00)
- After-the-fact Review (\$1,000.00 + applicable application fee)

Plan Amendments

- Planning Staff Review (\$250.00)
- Planning Board Review (\$500.00)

Billing Address: (name, address and contact information)

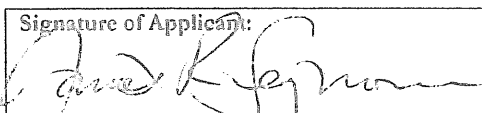
Submittals shall include seven (7) folded packets containing of the following materials:

- A. Copy of the application.
- B. Cover letter stating the nature of the project.
- C. Written Submittal (Sec. 14-525 2. (c), including evidence of right, title and interest.
- D. A standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 100 feet.
- E. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
- F. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
- F. In addition to the seven (7) sets of documents listed above, one (1) set of the site plans reduced to 11 x 17 must be submitted.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov. Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for site review only; a Performance Guarantee, Inspection Fee, Building Permit Application and associated fees will be required prior to construction.

Signature of Applicant: 	Date: 6-11-08
----------------------------------------------------------------------------------------------------------------	------------------



Site Plan Checklist Portland, Maine

Department of Planning and Development, Planning Division and Planning Board

RUFUS DEERING LUMBER - 383 COMMERCIAL ST

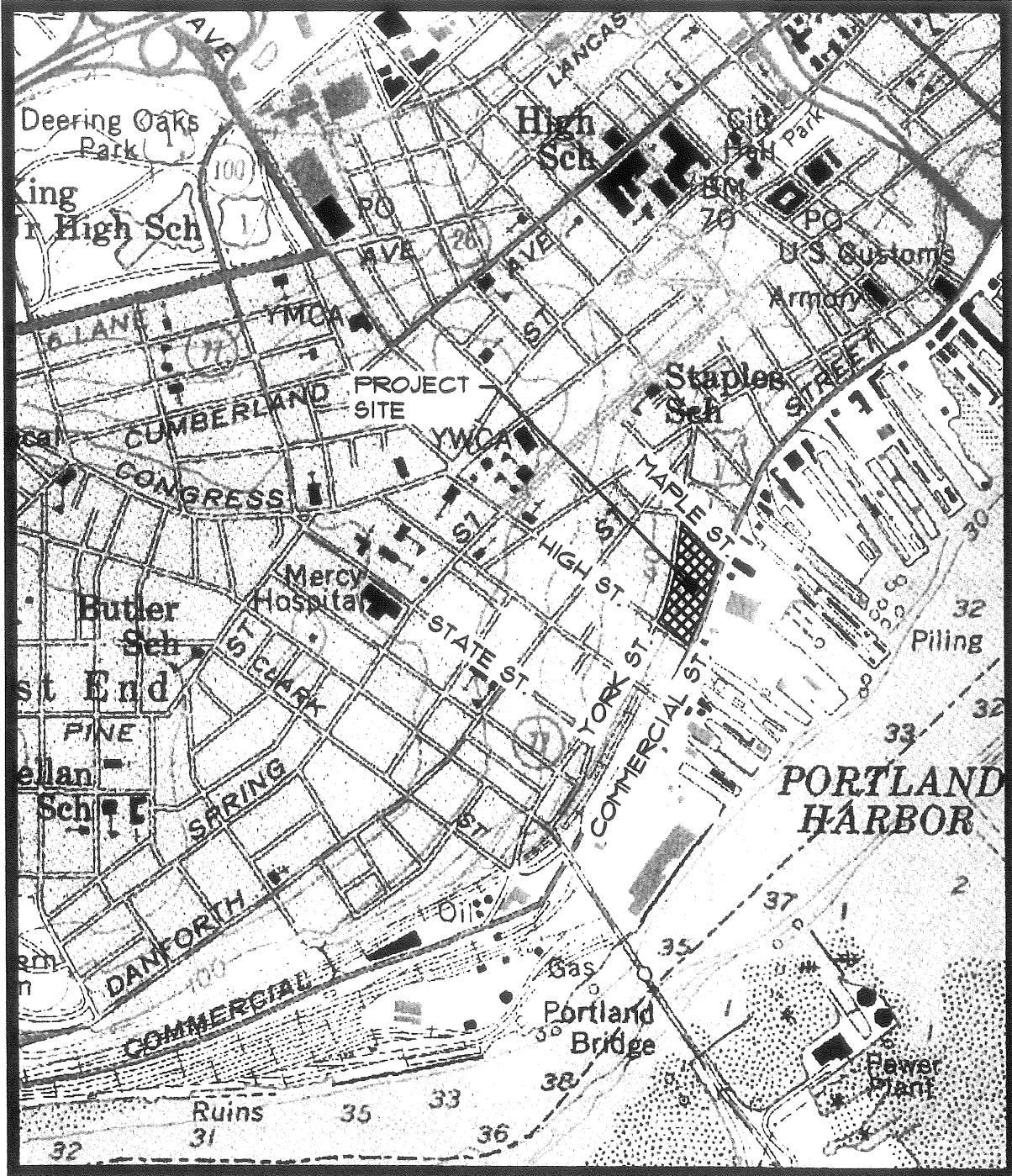
Project Name, Address of Project

Application Number

The form is to be completed by the Applicant or Designated Representative:

Check Submitted	Site Plan Item	Required Information	Section 14-525 (b,c)
✓	(1)	Standard boundary survey (stamped by a registered surveyor, at a scale of not less than 1 inch to 100 feet and including:	1
✓	(2)	Name and address of applicant and name of proposed development	a
✓	(3)	Scale and north points	b
✓	(4)	Boundaries of the site	c
✓	(5)	Total land area of site	d
✓	(6)	Topography - existing and proposed (2 feet intervals or less)	e
✓	(7)	Plans based on the boundary survey including:	2
NA	(8)	Existing soil conditions	a
NA	(9)	Location of water courses, wetlands, marshes, rock outcroppings and wooded areas	b
✓	(10)	Location, ground floor area and grade elevations of building and other structures existing and proposed, elevation drawings of exterior facades, and materials to be used	c
✓	(11)	Approx location of buildings or other structures on parcels abutting the site and a zoning summary of applicable dimensional standards (example page 9 of packet)	d
✓	(12)	Location of on-site waste receptacles	e
✓	(13)	Public utilities	e
✓	(14)	Water and sewer mains	e
✓	(15)	Culverts, drains, existing and proposed, showing size and directions of flows	e
✓	(16)	Location and dimensions, and ownership of easements, public or private rights-of-way, both existing and proposed	f
✓	(17)	Location and dimensions of on-site pedestrian and vehicular access ways	g
✓	(18)	Parking areas	g
✓	(19)	Loading facilities	g
✓	(20)	Design of ingress and egress of vehicles to and from the site onto public streets	g
✓	(21)	Curb and sidewalks	g
✓	(22)	Landscape plan showing:	h
✓	(23)	Location of existing vegetation and proposed vegetation	h
✓	(24)	Type of vegetation	h
✓	(25)	Quantity of plantings	h
✓	(26)	Size of proposed landscaping	h
NA	(27)	Existing areas to be preserved	h
NA	(28)	Preservation measures to be employed	h
✓	(29)	Details of planting and preservation specifications	h
✓	(30)	Location and dimensions of all fencing and screening	i
TBD	(31)	Location and intensity of outdoor lighting system	j
✓	(32)	Location of fire hydrants, existing and proposed (refer to Fire Department checklist)	k
✓	(33)	Written statements to include:	e
✓	(34)	Description of proposed uses to be located on site	cl
NA	(35)	Quantity and type of residential, if any	cl
✓	(36)	Total land area of the site	c2
✓	(37)	Total floor area, total disturbed area and ground coverage of each proposed Building and structure	c2
NA	(38)	General summary of existing and proposed easements or other burdens	c3
✓	(39)	Type, quantity and method of handling solid waste disposal	c4
✓	(40)	Applicant's evaluation or evidence of availability of off-site public facilities, including sewer, water and streets	c5
✓	(41)	Description of existing surface drainage and a proposed stormwater management plan or description of measures to control surface runoff.	c6

FIGURE 1



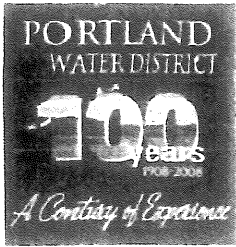
SITE LOCATION MAP
USGS TOPOGRAPHIC
7.5 MIN. QUADRANGLE
PORTLAND WEST
SCALE: 1"=1000'

Sebago Technics

Engineering Expertise You Can Build On

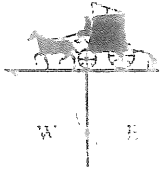
One Chabel Street
Westbrook, Me 04098-1339
Te (207) 856-0277





225 DOUGLASS STREET
P.O. BOX 5553
PORTLAND, ME 04104-3553
P: 207.774.8941
F: 207.528.5440
WWW.PWD.ORG

Att. 1-e



PORTLAND
WATER DISTRICT

RECEIVED

June 4, 2008

Sebago Technics
One Chabot Street
Westbrook, ME 04098-1339

Attn: Patrick M. Martin, Design Engineer
Re: Rufus Deering Lumber, 383 Commercial Street, Portland
Ability to serve with PWD water

Dear Mr. Martin:

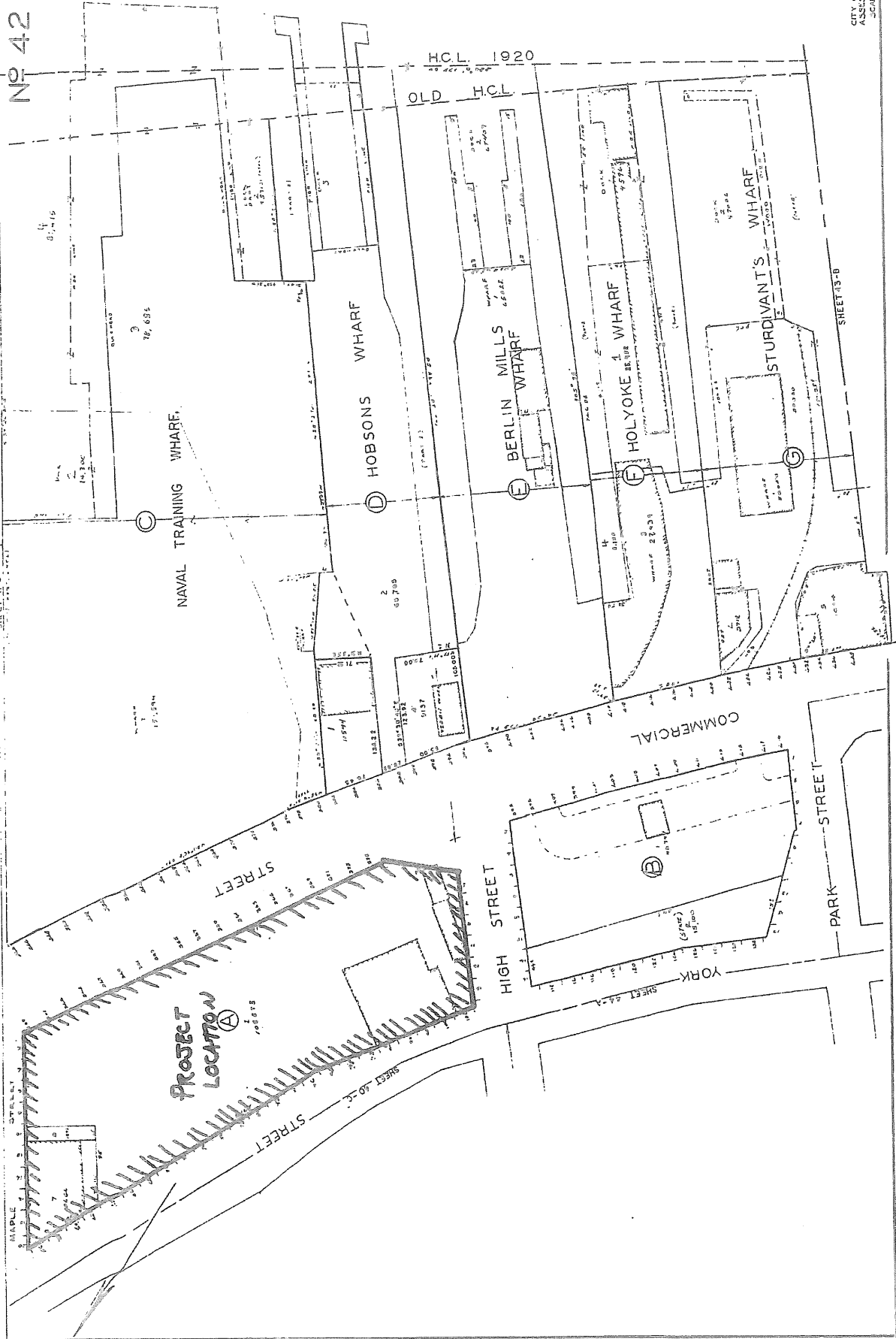
This letter is to confirm that there should be an adequate supply of clean and healthful water to serve the needs of the commercial lumber yard at 383 Commercial Street in Portland. According to District records, there is a 12-inch ductile iron water main on the southwest side of High Street as well as 4 hydrants located adjacent to the site.

The current data from a nearby hydrant indicates there should be adequate capacity of water to serve the needs of your proposed project, as stated in your letter, dated May 29, 2008.

Hydrant Location: Commercial Street, 225' north of High Street
Hydrant Number: POD-HYD00062
Static Pressure: 109 psi
Flow: 1635 gpm
Last Tested: 4/27/2004

Please notify your mechanical engineer of these results so that they can design your system to best fit the noted conditions. Because the static pressure at the main is above the limit for which the Maine State Plumbing Code recommends installing pressure reducing valves, PWD recommends consulting with your mechanical engineer to determine if they should be used on your project.

No 42



CITY OF PORTLAND
 ASSESSORS PLAN
 SCALE 1" = 50'

Att. 1-f
w/NOTE
ET AL
START PAGE 298

QUITCLAIM DEED WITH COVENANT

KNOW ALL BY THESE PRESENTS, That I, **WILLIAM M. MOODY, JR.** of Yarmouth, Cumberland County, Maine, for consideration paid, grant to **MILLIKEN SMITH BLOCK, LLC**, a Maine limited liability company, with a mailing address of 383 Commercial Street, Portland, Maine 04101, with Quitclaim Covenant, a one-tenth (1/10) interest in common and undivided, in and to a certain lot or parcel of land situated in Portland, Cumberland County, Maine, described more particularly as follows:

Beginning at the intersection of the northwesterly sideline of Commercial Street with the southwesterly sideline of Maple Street;

Thence northwesterly in said southwesterly sideline of Maple Street a distance of one hundred ten (110) feet, more or less, to the easterly corner of a strip of land fifteen (15) feet wide conveyed by the Portland Terminal Company to the Family Laundry, Inc. by deed dated May 26, 1938, recorded in the Cumberland County Registry of Deeds in Book 1555, Page 24, said strip of land to be used as a passageway in common with the Portland Terminal Company, its successors and assigns forever;

Thence southwesterly in the southeasterly sideline of said strip of land a distance of eighty-five (85) feet to the southerly corner of said strip of land, said southerly corner being also the southerly corner of land conveyed by the Portland Terminal Company to said Family Laundry, Inc. by said deed dated May 26, 1938;

Thence northwesterly in said southwesterly sideline of said Family Laundry land a distance of ninety-five (95) feet, more or less, to the southeasterly sideline of York Street;

Thence southwesterly in said southeasterly sideline of said York Street five hundred sixteen (516) feet, more or less, to the northeasterly sideline of High Street;

Thence southeasterly in said northeasterly sideline of High Street a distance of one hundred sixty-two (162) feet, more or less, to the westerly corner of a triangle of land conveyed by Boston and Maine Railroad, predecessor in title to the Portland Terminal Company, to the City of

MAP 42
LOT 1

Portland by deed dated May 21, 1874 and recorded in said Registry of Deeds in Book 410, Page 473;

Thence northeasterly in the northwesterly sideline of said triangle of land a distance of ninety-eight (98) feet, more or less, to its intersection with said northwesterly sideline of Commercial Street;

Thence northeasterly in said northwesterly sideline of said Commercial Street four hundred seventy (470) feet, more or less, to the point of beginning.

The Grantor hereby also conveys to the Grantee, its successors and assigns, any right, title or interests she may have to lands abutting the above described parcel of land between the northeasterly sideline of High Street and the centerline of High Street and between the northwesterly sideline of Commercial Street and the centerline of Commercial Street.

ALSO conveying to the Grantee herein, all rights of the Grantor herein to use the passageway located on the strip of land as described in a deed from the Portland Terminal Company to Family Laundry, Inc., dated May 26, 1938 and recorded in the Cumberland County Registry of Deeds in Book 1555, Page 24.

Being the same premises conveyed by Rufus Deering Company to William M. Moody, Jr., et als. by deed dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 82.

This conveyance is made SUBJECT TO:

1. Mortgage and Security Agreement from William M. Moody, Jr., et als. to Rufus Deering Company dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 85.
2. Memorandum of Lease by and between William M. Moody, Jr., et als. (Lessor) and Rufus Deering Company (Lessee) dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 91.
3. Agreement by and between William M. Moody, Jr., et al. and Rufus Deering Company, et al. dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 93. Pursuant

to Paragraph 3 of said Agreement, unanimous written consent to this conveyance has been obtained from all parties to said Agreement.

Witness my hand and seal this 17th day of Decem, 2004.

Witness:

Jean M. Rolland

William M. Moody, Jr.
William M. Moody, Jr.

STATE OF MAINE
COUNTY OF CUMBERLAND, ss.

On Dec 12th, 2004, personally appeared the above-named William M. Moody, Jr. and acknowledged the foregoing instrument to be his free act and deed.

Before me,

Patricia A. Merritt

Notary Public

Printed Name:

My Commission Expires:

PATRICIA A. MERRITT

Notary Public, Maine

My Commission Expires April 1, 2010

SEAL

Received
Recorded Register of Deeds
Nov 30 2005 03:30:49P
Cumberland County
John B O'Brien

BK 14115PG013

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.
By: Werner Schnetzer
Werner Schnetzer
its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,
James S. Bussard
Notary Public/Attorney-at-Law
Print Name: James S. Bussard

122895/PSB/SKLF
91401.4926

1998 9/1
121 1/2

BK 14115PG014

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1990 SEP -1 PM 4: 48

CUMBERLAND COUNTY

John B. O'Brien

Att. 1-g

07383

**Public Notice
Neighborhood Meeting**

Please join us for a neighborhood meeting to discuss our development plans for:

Rufus Deering Company Proposed warehouse facility (29,000 square feet) remodeling of the existing storage barn for office, showroom and retail, located at 383 Commercial Street, Portland, Maine.

Date: October 6, 2008

Time: 5:30 PM

Location: Becky's Diner
390 Commercial Street, 2nd floor
Portland, Maine 04101

If you have questions, please call James Seymour, Sebago Technics, Inc., (856-0277), or Jay Breard, Rufus Deering Lumber Company (772-6505).

Sincerely,

Daniel LaBrie, Vice President
Rufus Deering Lumber Company
383 Commercial Street
Portland, ME 04101

NOTE: Under Section 14-32© of the City Code of Ordinances, an applicant for a major development, subdivision of over five lots/units, or zone change is required to hold a neighborhood meeting at least seven days prior to the Planning Board public hearing on the proposal.

07383

Neighborhood Meeting Certificate

I, James Seymour, hereby certify that a neighborhood meeting was held on October 6, 2008 at the 2nd floor meeting room at Becky's Diner located at 390 Commercial Street, Portland, Maine, at 5:30 PM.

I also certify that on September 29, 2008 invitations were mailed to all addresses on the mailing list provided by the Planning Division, including property owners within 500 feet of the proposed development and all residents on the "interested parties" list.

Signed:



This the 7th day of October, 2008

Attached to this certification are:

- 1) Copy of the Invitation Sent
- 2) Sign-in Sheet
- 3) Meeting Minutes

07383

**Public Notice
Neighborhood Meeting**

Please join us for a neighborhood meeting to discuss our development plans for:

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remodeling of the existing storage barn for office,
showroom and retail, located at 383 Commercial Street,
Portland, Maine.

Date: October 6, 2008

Time: 5:30 PM

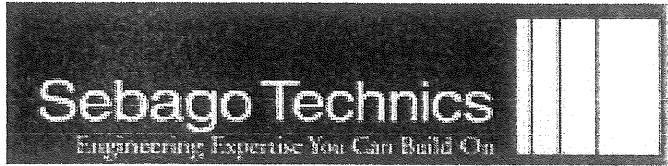
Location: Becky's Diner
390 Commercial Street, 2nd floor
Portland, Maine 04101

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Sincerely,

Daniel LaBrie, Vice President
Rufus Deering Lumber Company
383 Commercial Street
Portland, ME 04101

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07383

TO: Molly Castor, Planner
FROM: Jim Seymour P.E., Sebago Technics, Inc.
RE: Rufus Deering Lumber Co., 383 Commercial St., Neighborhood Meeting Minutes
DATE: October 7, 2008

Neighborhood Meeting Minutes:

On October 6, 2008 a Neighborhood Meeting discussing the site improvements proposed by Rufus Deering Lumber Co. at 383 Commercial Street. The meeting was held at Becky's Diner's meeting room at 390 Commercial Street

The following were in attendance representing the project for Rufus Deering Lumber Co.:

Dan LaBrie	--	Vice President	Rufus Deering Lumber Co.
Jay Breard	--	Controller	Rufus Deering Lumber Co.
James Seymour, PE	--	Civil Engineer	Sebago Technics Inc

The following were in attendance from the general public:

Mr. & Mrs. Anthony Mezoian	85 York Street
Paul Becker	75 York Street

Mr. Labrie described the basic function of the site and need to expand the facilities and how the warehouse is a necessary part of the Company's need to survive in a competitive marketplace. The abutters were told that the proposed warehouse would displace the current Quonset hut and older shelter. Mr. Seymour explained that the access will be gated and will be locked each night. No one objected regarding circulation or business hours.

Mr. Seymour discussed the sites drainage, sidewalk and traffic improvements. All in attendance were aware of Rufus Deering's historical presence and had only concerns of proposed height. Most felt that the improvements on York Street were excessive, and agreed that the warehouse is an improved enhancement for the property.

Mr. & Mrs. Anthony Mezoian and Mr. Becker raised questions regarding the proposed building height. Mr. Labrie and Mr. Seymour both stated that the height could exceed the height of the existing shelters but that the height would not be much higher than a one story, structure on York Street. All felt that this would not be a burden, and with the preserving of trees along the street edge it would be a good buffer, and the height would not impact their view of the harbor from their upper floors of their properties' structures.

They offered support and wished Rufus Deering success.
The meeting adjourned at 6:30 PM.

JRS:jrs/kn

Neighborhood Meeting

List Of Attendees

Rufus Deering Lumber Co. Warehouse project
October 6, 2008

Abutter(s) Name

Address

PAUL BECKER 75 YORK STREET, PORTLAND

Anthony P. Mezoian 85 York, Portland,

COOPER LIGHTING—LUMARK®

DESCRIPTION

The IMPACT Quarter Sphere cutoff wall luminaire makes an ideal complement to site design. U.L. Listed and CSA Certified for wet locations in down mount applications and damp locations in up mounted applications.

Rugged construction and full cutoff classified optics provide facade and security lighting for light restricted zones surrounding schools, office complexes, apartments, and recreational facilities.

Catalog #		Type	
Project			
Comments		Date	
Prepared by			

SPECIFICATION FEATURES

A—Housing

The housing is a two-piece design of die-cast aluminum for precise control of tolerances and repeatability.

B—Mounting

Gasketed and zinc plated rigid steel mounting attachment fits directly to 4" J-Box or wall with "Hook-N-Lock" mechanism for quick installation. Secured with two (2) captive corrosion resistant black oxide coated allen head set screws concealed but accessible from bottom.

C—Optical Modules

All optical modules utilize high performance 95% reflective sheet. Strong Type II optical module is standard.

D—Ballast

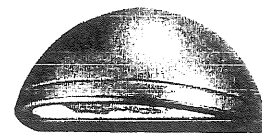
HID luminaires supplied with high power factor ballast with Class H insulation. Minimum starting temperatures are -40°C (-40°F) for HPS and -30°C (-22°F) for MH. Compact Fluorescent luminaires feature electronic universal 120-277V high efficient 50/60Hz ballast with -18°C (0°F) minimum starting.

E—Door

Die-cast door features, 1/8" heat- and impact-resistant clear tempered glass lens mounted with internal plated steel clips and sealed with EPDM gasketing. Hinged door secured in place via two (2) captive fasteners.

F—Finish

Durable polyester powder coat finish. Standard color is bronze. Optional white and black colors available. Other finish colors available. Consult your Cooper Lighting Representative concerning special color requirements.



IP IMPACT QUARTER SPHERE

50 - 175 W

High Pressure Sodium
Metal Halide

26 - 52 W

Compact Fluorescent

FULL CUTOFF
WALL MOUNT LUMINAIRE

TECHNICAL DATA

25°C Maximum Ambient Temperature
External Supply Wiring 90°C Minimum
Down Mounted—Wet Location
Up Mounted—Damp Location

ENERGY DATA

High Reactance Ballast Input Watts

50W HPS HPF (66 Watts)
50W MH HPF (72 Watts)
70W HPS HPF (91 Watts)
70W MH HPF (90 Watts)
100W HPS HPF (130 Watts)
100W MH HPF (129 Watts)
150W HPS HPF (190 Watts)
150W MH HPF (185 Watts)

CWA Ballast Input Watts

175W MH HPF (210 Watts)

Electronic Ballast Input Watts

26W PL HPF (29 Watts)
32W PL HPF (36 Watts)
42W PL HPF (46 Watts)
52W PL HPF (55 Watts)

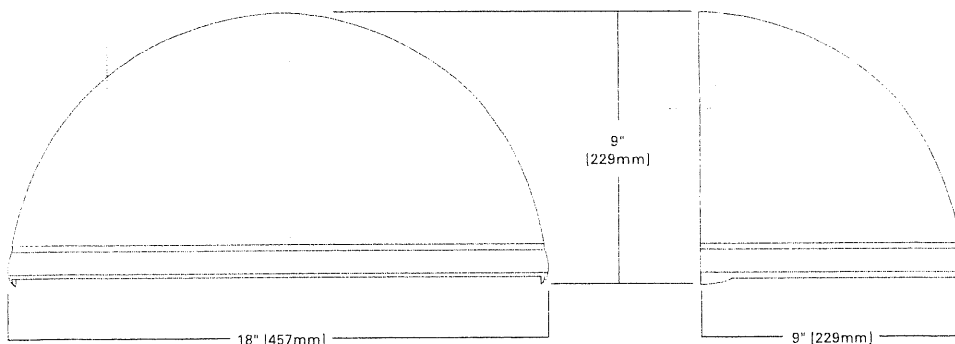
SHIPPING DATA

Approximate Net Weight:
18 lbs. (8 kgs.)



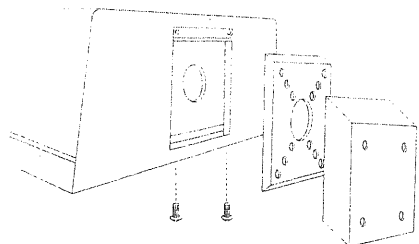
A B C

D E F



In downlight applications only.

HOOK-N-LOCK MOUNTING (Mounting attachment included, J-Box not included.)

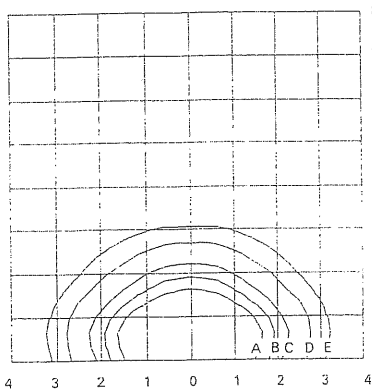


COOPER LIGHTING

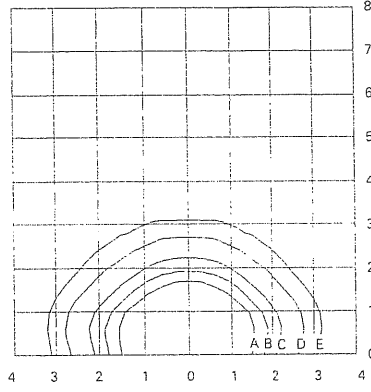
ADH060605



PHOTOMETRICS (Complete IES files available at www.cooperlighting.com)



MHIP-S-175-MT-LL
175-Watt MH
14,000-Lumen Clear Lamp



HPIP-S-150-MT-LL
150-Watt HPS
16,000-Lumen Clear Lamp

Footcandle Table

Select mounting height and read across for footcandle values of each isofootcandle line. Distance in units of mounting height.

Mounting Height	Footcandle Values for Isofootcandle Lines				
	A	B	C	D	E
10'	4.50	2.25	1.13	0.45	0.23
15'	2.00	1.00	0.50	0.20	0.10
20'	1.12	0.56	0.28	0.11	0.06

ORDERING INFORMATION

SAMPLE NUMBER: MHIP-S-150-MT-LL

	IP	S			
Lamp Type MH=Metal Halide HP=High Pressure Sodium PL=Compact Fluorescent	Fixture Type IP=IMPACT	Fixture Shape S=Quarter Sphere	Lamp Wattage HID ¹ 50=50W ² 70=70W ³ 100=100W 150=150W 175=175W ⁴ Compact Fluorescent 26/32/42=26, 32, or 42W 52=52W ⁵	Voltage ⁶ 120V 208V 240V 277V 347V 480V DT=Dual-Tap ⁷ MT=Multi-Tap ⁸ TT=Triple-Tap ⁹ E=Electronic Ballast ¹⁰	Options (add as suffix / replace XXX with voltage) ¹¹ F1=Single Fuse - 120, 277 or 347V (Must Specify Voltage) F2=Double Fused - 208 or 240V (Must Specify Voltage) TR=Tamper Resistant Screw (Door and Mounting Plate) PE=Button Photocontrol (Must Specify Voltage) LL=Lamp Included (Must Specify Wattage on PL) ¹² BK=Black WH=White Q=Quartz Restrike T4 Lamp ^{13, 14, 15} EM=Emergency Quartz Restrike T4 Lamp With Time Delay Relay ^{13, 14, 15} EMI40-XXX=CFL Cold Weather Emergency Battery Pack (Must Specify 120 or 277V) ^{13, 14} CF/EM-XXX=Emergency Battery Pack (Must Specify 120 or 277V) ^{13, 14} QMR=Quartz Restrike MR16 Lamp ^{15, 16, 20} EMMR=Emergency Quartz Restrike MR16 Lamp With Time Delay Relay ^{15, 16, 20} EM/SC=Emergency Separate Circuit T4 Lamp ^{14, 16, 21} EM/SC/MR=Emergency Separate Circuit MR16 Lamp ^{20, 21} EM/SC/12V=Emergency Separate Circuit (12V) ²²

STOCK SAMPLE NUMBER (Lamp Included)

SAMPLE NUMBER: MHIS17

	IS	
Lamp Type HP=High Pressure Sodium MH=Metal Halide	Fixture Type ²³ IS=IMPACT Quarter Sphere	Lamp Wattage 10=100W 15=150W 17=175W

NOTES: Options not available with stock products.
Order Accessories as separate items for field installation. Refer to standard ordering information to add options and accessories

NOTES: 1 All HID lamps are medium-base. 2 Available only in 120 or 277V and Dual-Tap. 3 Not available in 480V. 4 Metal Halide construction only. 5 (2) 26W quad tube lamps only. 6 HID products also available in non-US voltages and 50Hz for international markets. Consult your Cooper Lighting Representative for availability and ordering information. 7 Dual-Tap ballast are 120/277V wired 277V. 8 Multi-Tap ballast are 120/208/240/277V wired 277V. 9 Triple-Tap ballast are 120/277/347V wired 347V. 10 Supplied with 120V through 277V 50/60Hz for Compact Fluorescent. 11 Must be listed in the order shown and separated by a dash. 12 Lamp is Cooper Lighting designated product based on luminaire requirements. Specified lamps must be ordered as a separate line item. 13 The power might need to cycle and allow HID lamp to cool in warm climates. Available up to 175W HID. 14 For use with T4 double contact bayonet base, 100W maximum, 120V halogen lamp by others. 15 Lamps wired to quartz restrike relay. 16 Not available with CFL. 17 Battery pack will operate up to 42W CFL lamp for 90 minutes. 18 CF lamps only, rated for minimum temperature of 0°F (-18°C). (1) 42W CFL maximum, heater rated for 300W with thermostat control. 19 CF lamps only, rated for minimum temperature of 32°F (0°C). (1) 42W CFL maximum. 20 For use with MR16, GU10 base, 50W maximum, 120V halogen lamp by others. 21 Leads run out back for connection to auxiliary 120V circuit. 22 Supplied with 12V Bi-pin socket for connection to emergency battery pack (supplied by others). For use with MR16, GU5.3 base, 35W maximum, 12V halogen lamp by others. 23 Painted bronze. Supplied with lamp and Multi-Tap HPF ballast wired 277V. 24 Specifications and dimensions subject to change without notice. 25 Products also available in non-US voltages and frequencies for international markets. 26 Consult your Cooper Lighting Representative for availability and ordering information.

NOTE: Specifications and dimensions subject to change without notice.

DESCRIPTION

The classic lines and sophisticated construction of the Vision Wall luminaire make it an ideal complement to architectural site design. IP65 ingress Rating standard. U.L. listed and CSA Certified for wet locations in up or down mounting applications with no necessary modifications to the door or housing.

Catalog #	Type
Project	
Comments	
Prepared by	Date

SPECIFICATION FEATURES

A...Housing

One piece die-cast aluminum construction for precise tolerance control and repeatability in manufacturing.

B...Door

One piece die-cast aluminum with continuous silicone gasket accommodates either up or down mounting configurations. Door frame is hinged and secured to the housing via four (4) captive stainless steel Allen head fasteners. Lens is impact-resistant 1/8" thick tempered clear or optional frosted flat glass, sealed to the door with a one-piece silicone gasket.

C...Optical System

Choice of five (5) high efficiency optical systems. Type II, III, IV, and FX optical systems constructed of premium 95% reflective anodized aluminum sheet. Optical segments are rigidly mounted inside a heavy-wall aluminum housing for superior protection. All segment faces are clean of rivet heads, tabs, or other means of attachment which may cause streaking in the light distribution. TS optic constructed of spun and polished specular anodized aluminum. All reflector modules feature toolless removal, quick disconnect wiring plugs, and are field rotatable in 90° increments. HID lamp sources in VWM medium housing optics feature mogul-base lampholders

D...Optical Assembly

Optical systems are secured to an internal rotating assembly that allows up to 10° of outward adjustment. A concealed stainless steel adjustment screw is provided on the exterior surface of the housing to allow for tilt adjustment of the optical module while under full power without accessing internals of the fixture or affecting the outward appearance of the luminaire.

E...Mounting

Standard zinc plated attachment plate fits directly to 4" J-Box. Two (2) threaded studs with locking nuts allow for fixture mounting via keyhole slots on backside of housing. Mounting plate features one-piece EPDM gasket on either side of plate to firmly seal fixture to wall surface, forbidding entry of moisture and particulates. Optional mounting arrangements include an embedded mount bracket, or cast aluminum surface conduit adapter, each available as accessories.

G...Finish

Housing and door finished in a 5 stage premium TGIC polyester

powder coat paint, 2.5 mil nominal thickness for superior protection against fade and wear. Standard colors include black, bronze, grey, white, dark platinum, and graphite metallic. RAL and custom color matches available. Consult your INVUE Lighting Systems Representative for more information.



VWM VISION WALL MEDIUM

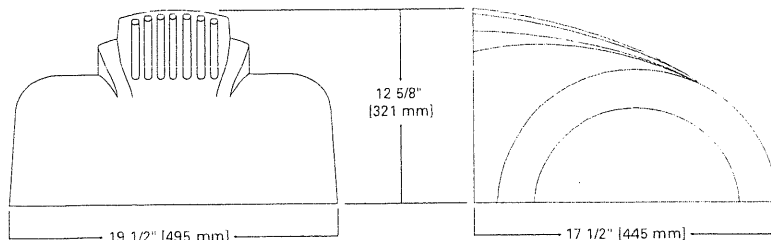
1 1 4 - 4 0 0 W

- Metal Halide
 - Pulse Start Metal Halide
 - High Pressure Sodium
 - Compact Fluorescent
- ARCHITECTURAL WALL LUMINAIRE



Wattage Table	
	VWM
Metal Halide	175, 250, 400W
Pulse Start Metal Halide	250, 320, 350, 400W
High Pressure Sodium	150, 250, 400W
Compact Fluorescent	(2) 57W, (2) 70W

DIMENSIONS



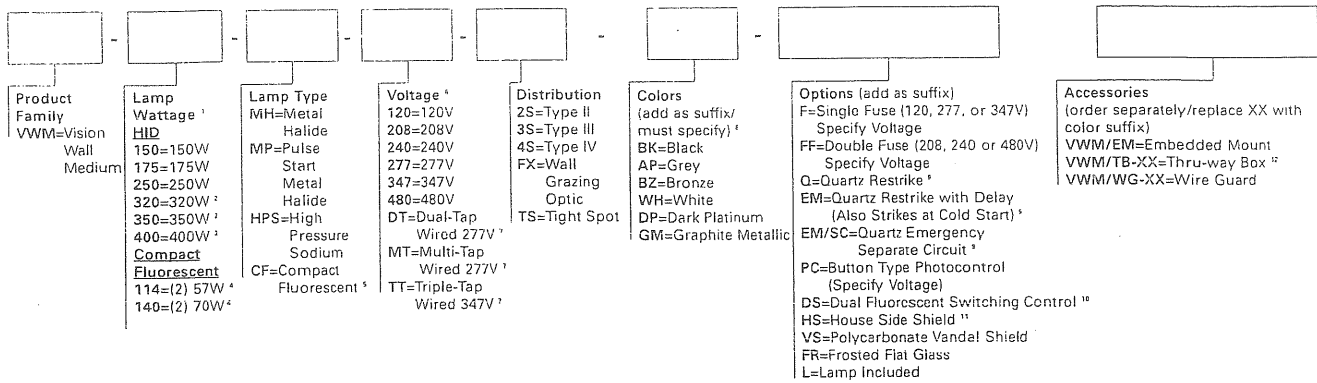
Certifications

IP65 Rated	U.L. 1598		FCO Full Cutoff
CSA Listed	25°C Ambient	ISO 9001	

SHIPPING DATA (approx.)
 Net Weight (lbs.): 45
 Volume (cu. ft): 13

ORDERING INFORMATION

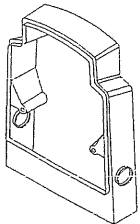
SAMPLE NUMBER: VWM-400-MH-MT-3S-BK-L



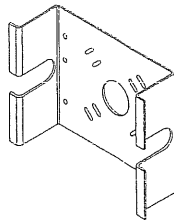
- NOTES: 1 All HID lamps are mogul-base.
2 320 and 350W Pulse Start Metal Halide lamps only.
3 400W Metal Halide requires reduced envelope ED28 lamp, 400W rated for 25°C ambient environments.
4 Dual 57W and dual 70W Compact Fluorescent lamp options available in Type 4S distribution only.
5 Compact Fluorescent ballasts contain internal fusing. No supplemental fusing is necessary. CF ballasts are 120 through 277V.
6 Products also available in non-US voltages and 50Hz for international markets. Consult factory for availability and ordering information.
7 Dual-tap is 120/277V wired 277V. Multi-tap is 120/208/240/277V wired 277V. Triple-tap ballast is 120/217/347V wired 347V.
8 Custom and RAL color matching available upon request. Consult your INVUE Lighting Systems Representative for further information.
9 Quartz options not available with FX or TS distributions.
10 Dual switching requires dual 57W or dual 70W compact fluorescent lamps, and allows independent switching control of each lamp through use of two electronic ballasts. Allows 50% power reduction when dual ballasts are independently wired and controlled.
11 House side shield not available on FX and TS optics.
12 For use in down mounting applications only.

ACCESSORIES (order separately)

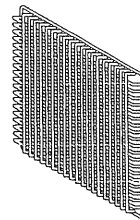
Thru-way Mounting Box



Embedded Mount

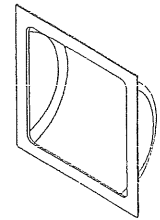


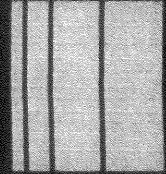
Wire Guard



OPTIONS (add as suffix)

Polycarbonate Vandal Shield





August 22, 2008
07383

Att. 2a

sebagotechnics.com
One Chabot Street
P.O. Box 1339
Westbrook, Maine
04098-1339
Ph: 207-856-0277
Fax: 856-2206

Ms. Molly Casto, Planner
City of Portland Planning Division
City Hall 4th Floor
389 Congress St.
Portland, ME 04101

Revised Site Plan Planning Board Workshop Submittal
Rufus Deering Lumber Company's - Proposed Drive-Thru Warehouse and Retail Store
383 Commercial St. Portland, Maine

Dear Ms. Casto:

Please find 9 copies of the revised Site Plan set that we have prepared for Rufus Deering Lumber Company - Proposed Drive-Thru Warehouse and Retail Store to be located on the street block surrounded by Commercial Street, High Street, York Street, and Maple Street in Portland Maine. This letter and the plans are in response to your review letter dated July 7, 2008, and per our meeting with you back in mid July. We understand that there could be other issues that could be brought forward, as we have not received all the comments from the various city departments. Our letter responds to the review items in the same numerical order as those in your July 7 letter.

Zoning

1. We are waiting for Marge Schmukal, Zoning Administrator's comments discussing the lessening of the existing non-conformity with our proposed site project. We expect that there are no substantial zoning issues with our proposed site project.
2. After much discussion about screening the materials, we had hoped to get a determination by the Zoning Administrator with regards to the need for site screening. The use of the property as a lumberyard has been in existence since 1854, and the general operations of a lumberyard make it absolutely necessary to have outdoor storage of lumber or building materials. Often these materials, are stacked upwards of 16 feet high, and have traditionally not required screening on the current site. Given the height, and front yard location of stored outdoor lumber, we feel screening with a 5 to 6 foot high fence does not accomplish the original intent of the ordinance to hide on site materials, but as discussed, if the City wishes us to screen the site from a pedestrian street level view, we would offer to screen the property with vinyl strips within the existing chain-link fencing. Our biggest objections to any screening, is that this has not been historically required on the site as a lumberyard, and that the area has been subject to graffiti and destruction of property by means of spray painting.

Site Plan

1. We have added floor areas and grade elevations of the existing and proposed structures as noted on the Grading and Utility Plan Sheet 5 of 9.
2. The owner is still working on supplying the architectural elevation drawings which will show building fronts, materials, and roof lines. Once those are complete they will be forwarded to the staff for their review.
3. A lighting plan will be prepared upon the Planning Board accepting the general layout of the proposed site. We are fully aware that the project will be required to use fully shielded cut-off fixtures per City requirements.
4. We have identified that all rubbish and recyclables will be stored in an enclosed dumpster area north of the proposed loading dock, or will be conducted within the confines of the drive-thru warehouse.

Traffic

5. We have not formerly received comments to date from the traffic engineer, but have a general sense of his concerns. We have eliminated the entrance closest to High Street and created more parking. Based on the conversations at our July meeting, we have widened the Commercial Street entrance to align with the drive-thru warehouse building entrance, and kept the Maple St. entrance for the purposes of material deliveries to the site. Based on your comments we respectfully request a waiver of the technical design standards to allow for two entrances, and to make one entrance 36 feet wide for the purposes of three lanes (one in, two out) to accommodate truck traffic.

Additionally, we have moved the corner of High and Commercial Street further away from the existing store, with the insertion of a 40 foot radius. This allows both the sidewalk, and street pavement to fully fall within the limits of the right-of-way and not encroach over the Rufus Deering property. Our concern is that the section of Commercial Street was just reconstructed, and that to construct the new corner radius could mean that the applicant could be forced to open a moratorium street. We are hoping the City will waive those fees to correct this situation and improve traffic safety near the intersection.

Curb and Sidewalk

6. Pursuant to Section 25-96, the project is required to install sidewalks and granite curbing on abutting streets. The project is unique in that it is an existing block surrounded by 4 streets. Commercial Street is constructed with granite curbing and concrete sidewalk, Maple Street is partially constructed with granite curbing and brick sidewalk, High Street is constructed with poor quality asphalt curbing and sidewalk in very good condition, and York Street has no sidewalks and poor low reveal granite curbing. We are proposing new curbing and sidewalk on High Street, and completing Maple Street where curbing

and sidewalk are missing. Commercial Street will not require either, with the exception of where the old site exit was, and where the new corner radius will be installed. Brick sidewalk and new granite curbing will be constructed over that corner area.

The sidewalk on York Street has presented many challenges due to the presence of an existing stone sea wall along the street and property line, and due to the unique geometry of York Street. Therefore, we submit a request to waive the sidewalk requirement under meeting two of the conditions under Section 14-506(b) of the Land Use Ordinance. Based on discussions with the staff, we offer to install granite curbing, and mark out a 5 foot wide bike lane.

We feel that we meet two of the waiver conditions:

(3) A safe alternative-walking route is reasonably available, for example, by way of a sidewalk on the other side of the street. There is an existing sidewalk on the opposing side of York Street which is currently designed into the signalization and crossings of High Street. Placement of sidewalk on the Rufus Deering frontage on York Street would require upgrades to signalization, lane markings, and doesn't pose the safest sidewalk scenario, which is discussed in our second waiver condition.

(6) Strict adherence to the sidewalk requirement would result in the loss of significant site features related to landscaping, or topography that are deemed to be of greater public value. The presence of a seawall along the edge of the right-of-way makes it unsafe to install a sidewalk closer to that wall and could jeopardize stability of pole supports and guardrails necessary for vehicular and property protection. Moving the sidewalk into the current paved section of York Street also has implications into public safety. The sidewalk could not be constructed with an esplanade, and could only be 4 feet wide, and even that diminishes the lane widths available to cars on York Street and could require re-striping of the travel lanes. Furthermore, this would require relocation of an existing catch basin, further tightening the separation of underground infrastructure in York Street. This could require upgrades to the current guardrail system to bring it up to a safe elevation to protect City snow plowing equipment and operators from a potential accident over the wall, all of which add substantial cost to the sidewalk installation. Due to constricting geometry of York Street, the outward traffic flow from Portland to High Street or South Portland, could increase the queuing at the intersection because there would not be the opportunity to turn with a separate right at the light after two or three cars stopped. A connecting cross walk would push the stop line back about 6 feet and there would be about 60 feet to a pinch point which leaves only 30 feet for three lanes, which is not ideal traffic engineering practice for a busy street subject to commercial traffic. Therefore, we feel the sidewalk would result in substantial loss of site features and public safety.

We have offered a proposal to replace the poor curbing with new granite curbing on York Street and paint a 5 foot wide bike lane. The street is subject to a lot of bike traffic and is a link for bike uses between South Portland, West End, and the Old Port. The elimination of the sidewalk and keeping of the bike lane would allow for safer lane widths, eliminating the need to re-signal the pedestrian crossing, adjust/relocate drainage infrastructure, while providing a safe link for bicyclists to get from the Casco Bay Bridge to the Old Port.

7. Prior to the Public Hearing, the owner will provide estimated costs for the project. We are trying to get a handle on the additional sidewalk or other construction costs which may be requested by the Board.
8. We have filed a letter with Public Works requesting a letter of sewer capacity. No response has been returned to date.
9. The owner will provide evidence of financial capacity once we have a known scope of site work required.
10. We expect that we will receive formal review comments from various departments or staff at or following the work shop. We have attempted to revise as many verbal comments as possible. We also understand, and will attempt to be prepared to answer questions about the warehouse operation, on-site traffic flow, parking, landscaping and building appearance.

Rufus Deering Lumber Company is looking to possibly start construction in 2009. We will be providing architectural renderings, and lighting plans as details of the site become available. In the interim, we are available at your request to meet the City's review staff. We feel given site design parameters that there is currently adequate information for the purposes of a workshop with the Planning Board.

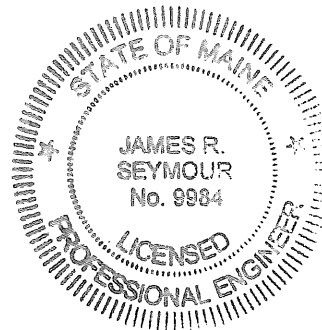
If you have any questions, please feel free to contact me at our office (856-0277). We look forward to meeting with the Planning Board for a workshop meeting as soon as an agenda allows, hopefully on September 9th.

Sincerely,

SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager



JRS:jrs/cb
Enc.

cc: Dan Labrie, Vice President- Rufus Deering Lumber Company
Jay Breard- Rufus Deering Lumber Company

Attachment 2b

October 15, 2008
07383

Molly Casto, Planner
City of Portland
389 Congress Street
Portland, ME 04101

Rufus Deering Lumber Company – 383 Commercial Street
Preliminary Site Plan Review

Dear Ms. Casto:

We have received peer review comments prepared by Dan Goyette, P.E. and Megan LaPierre E.I.T. of Woodard & Curran, dated July 9, 2008, for the subject project. Review materials included the Site Plan application and Engineering Plan set (Sheets 1-9), both dated June 11, 2008. After reviewing the comments provided, we offer the following responses.

Responses to Comments:

1. *Stormwater Management Plan and flow calculations have not been submitted for review.*

A Stormwater Management Plan and flow calculations were not submitted because we do not anticipate a significant change in peak flows from pre-development to post-development conditions. In both cases the site is completely developed with nearly 100% impervious surface. In fact, the amount of landscaped area increases slightly in the post-development site, but not enough to produce a significant change in runoff.

A peak flow analysis of the post-development site is provided with this response, in order to evaluate the performance of the proposed drainage improvements. Because the outlet of this system (SP-2) is an existing 12-inch storm drain in Commercial Street, only 12-inch pipes are proposed on-site. The model only includes an evaluation of the site. Runoff from the adjacent streets is not modeled.

Regarding the requirement for on-site treatment of runoff, this issue is currently being reviewed by the City staff, and will be resolved to their satisfaction prior to approval. The existing inverts in Commercial Street (proposed outlet) limit the proposed drainage improvements to a very shallow depth. The most likely solution, per an on-site meeting held in September, is that the applicant will provide a contribution to the City for a future improvement project. The amount of the contribution and the future project will both be determined by the City.

2. *The Note 11 on Sheet 4 indicates that there are 22 proposed parking spaces. The site plan shows 27 parking spaces, 22 adjacent to the proposed/remodeled retail building and 5 adjacent to Maple Street. According to the City of Portland Technical Design*

Standards, if a parking lot proposes 25 or more parking spaces, then on-site stormwater treatment is required.

A total of 30 spaces are now proposed. Note 11 on Sheet 4 has been updated. See comment 1 above regarding proposed stormwater treatment.

3. *Sheet 5 shows pipes from the proposed drive-through warehouse and retail building connecting into Pipe SD-3. What are these pipes? How do the pipes connect to SD-3, wye or tee? Why do the pipes tie into the storm drain pipe and not catch basin CB-1? Please provide information and pipe material, size, etc.*

These pipes represent roof drain connections. We have increased the diameter of CB-1 to 6 feet, and reconnected the roof drains so that they discharge directly into the catch basin. As the building design progresses and actual roof drain sizes and locations are determined, the plans will be revised to show any new information or changes.

4. *A note should be provided on the drawings stating all new catch basins shall include a Casco Trap.*

Note 27 has been added to Sheet 5 of 9 requiring the use of Casco Traps in all new catch basins.

5. *The line work and labels on the Typical Sidewalk ADA Ramp elevation detail are unclear. The top of the granite curb and top of roadway pavement are shown at the same elevation. Also, the granite curb is labeled as 6". The curbing should have a 7" reveal, as shown in Typical Installation of Curb detail.*

The detail has been revised for clarification. All details show a curb reveal of 7".

6. *Details for the dumpster enclosure, tree grate, retaining wall and seat wall with rail should be provided.*

Details for the dumpster enclosure and tree grate have been added to the detail sheets. Retaining wall details will be submitted for approval prior to a performance bond being established. Materials and actual wall systems have not been determined, and we request that this be either a condition of approval or made part of the building permit process.

7. *The dumpster should be placed on a concrete pad.*

The concrete pad for the dumpster is shown in the detail on Sheet 8 of 9.

8. *Vehicular access to the dumpster is limited to an 11-foot travel width due to the adjacent walls. It will be difficult for a waste management vehicle, traveling in reverse, to access the dumpster, as the typical roadway width is a minimum of 12-feet. It is suggested that the access to the dumpster be widened, or the dumpster be relocated.*

We feel that 11 feet is an adequate width for truck access to the dumpster location. We have eliminated the short wall and will slope the gravel to allow more room. The 11-foot width is equal to a street lane and should be adequate.

9. *Location of the pedestrian ramps should be shown on the drawings.*

Pedestrian ramps have been added as requested. However, based on off-site improvements, the locations may change as the design evolves. We have included a note on the Site Plan that recommends coordination with the City Traffic Engineer prior to construction.

10. *Location of erosion control measures (i.e. filter barrier, silt sacks) should be shown on the drawings.*

Temporary inlet protection is shown on all catch basins, and filter barrier is shown where appropriate.

11. *Location of snow storage should be indicated on the plans.*

Note 26 has been added to Sheet 4 of 9 indicating that the applicant will contract with a local firm for snow removal and that no snow storage area is provided.

12. *The installation of sidewalk along the York Street frontage needs to be addressed.*

This issue is currently being reviewed by City Planners and Public Works. The plans show the alignment of curb, brick sidewalk, and bicycle lane. The applicant is petitioning for a sidewalk waiver and a request has been previously submitted. The plans show a layout for a sidewalk, but may be revised if the Board agrees to waive the requirement in part or in whole.

Also, we have attached for Planning Dept. review a cost estimate for construction of all sidewalk sections required to be installed per site plan standards on High, York, and Maple Streets for your use. A separate letter will be attached for the sidewalk on York Street arguments.

This completes our response to the peer review comments submitted by Woodard & Curran. If you have any additional comments or require further information, please contact us at 856-0277. Thank you for your time.

Sincerely,

SEBAGO TECHNICS, INC.



James R. Seymour, P.E.
Project Manager

JRS:kn

MEMORANDUM

To: FILE
From: Marge Schmuckal Dept: Zoning
Subject: Application ID: 2008-0082
Date: 8/27/2008

I have reviewed the most current submittal which I received on 8/27/08. This property is located within the B-5b Urban Commercial Mixed Use Zone. Lumber and building materials dealers that are in existence are permitted. It has been submitted that Rufus Deering Lumber Company has been in existence since 1854 (154 years old).

The alterations to the existing developed site brings the site into greater conformance with todays ordinances and lessens the existing nonconformities especially in regard to the maximum front yard setback requirement of the B-5b zone. The Zoning Ordinance encourages the lessening of nonconformity on developed sites.

Under Performance Standards section 14-230.5, there is a standard that any storage of new materials, finished products, or related equipment be suitably screened from the public way and from abutting properties by a solid fence at least five (5) feet in height or by a solid evergreen planting strip. Again because of the existing development of this site, I do not think the full force of this provision applies. I would hope that the City and the applicant can reach a point where existing nonconformities have been lessened and the project comes more into compliance.

To confirm building heights, I will need scalable drawing of the new structures. I am not anticipating that the proposal will be in violation of the maximum 65 foot building height.

Separate permits shall be required for any new signage.

Marge Schmuckal
Zoning Administrator

MEMORANDUM

To: FILE
From: Marge Schmuckal **Dept:** Zoning
Subject: Application ID: 2008-0082
Date: 11/14/2008

I have reviewed the most current site plan that has been submitted. As stated previously, under section 14-230.5 the storage of any new materials, finished products, or related equipment be suitable screened from the public way and from abutting properties by a solid fence at least five (5) feet in height or by a solid evergreen planting strip. I believe that the applicant has made an effort to clean up the front storage area of the property. There is a line of existing product storage adjacent to the front property line that is being removed. It is noted that this business is the entire block. There is no direct abutting neighbor who needs to be screened and protected from the visual impacts of this business. I have determined that existing nonconformities have been lessened and the property has come into better compliance.

Marge Schmuckal
Zoning Administrator

Att. 4

MEMORANDUM

To: FILE

From: Greg Cass

Dept: Fire

Subject: Application ID: 2008-0082

Date: 8/13/2008

They do not need the new hydrant, The 4 existing are fine.
Most issues for me will come during the building permit process.
The PFD approves this project.

Alt - 5 - a

**COMMITMENT & INTEGRITY
DRIVE RESULTS**

41 Hutchins Drive
Portland, Maine 04102
www.woodardcurran.com

T 800.426.4232
T 207.774.2112
F 207.774.6635

MEMORANDUM



TO: Molly Casto
FROM: Dan Goyette, PE and Megan LaPierre, EIT
DATE: July 9, 2008
RE: Rufus Deering Lumber Company, 383 Commercial Street

Woodard & Curran has reviewed the Major Site Plan Application for the proposed site redevelopment at 383 Commercial Street. The proposed project involves the construction of a 29,000 square foot drive-through warehouse and reconfiguration of current storage shelter to accommodate access and internal circulation. The project also includes remodeling the existing retail building and minor parking lot improvements.

Documents Reviewed

- Site Plan Application and attachments, addressed to Barbara Barhydt, City of Portland Senior Planner, prepared by James R. Seymour, PE, Sebago Technics, Inc., dated June 11, 2008.
- Engineering Plan Sheets 1-9, prepared for Rufus Deering Lumber, prepared by Sebago Technics, Inc., dated June 11, 2008.

Comments

- Stormwater Management Plan and flow calculations have not been submitted for review.
- The Note 11 on Sheet 4 indicate there are 22 proposed parking spaces. The site plan shows 27 parking spaces, 22 adjacent to the proposed/remodeled retail building and 5 adjacent to Maple Street. According to the City of Portland Technical and Design Standards, if a parking lot proposes 25 or more parking spaces then on-site stormwater treatment is required.
- Sheet 5 shows pipes from the proposed drive-through warehouse and retail building connecting into Pipe SD-3. What are these pipes? How do the pipes connect to SD-3, wye or tee? Why do the pipes tie into the storm drain pipe and not catch basin CB-1? Please provide information and pipe material, size, etc.
- A note should be provided on the drawings stating all new catch basins shall include a Casco trap.
- The line work and labels on the Typical Sidewalk ADA Ramp elevation detail are unclear. The top of the granite curb and top of roadway pavement are shown at the same elevation. Also, the granite curb is labeled as 6". The curbing should have a 7" reveal, as shown in Typical Installation of Curb detail.
- Details for the dumpster enclosure, tree grate, retaining wall and seat wall with rail should be provided.
- The dumpster should be placed on a concrete pad.
- Vehicular access to the dumpster is limited to an 11-foot travel width due to the adjacent walls. It will be difficult for a waste management vehicle, traveling in reverse, to access the dumpster, as the typical travel roadway width is a minimum of 12-feet. It is suggest the access to the dumpster be widen or the dumpster be relocated.
- Location of pedestrian ramps should be shown on the drawings.
- Location of erosion control measures (i.e. filter barrier and silt sacks) should be shown on the drawings.
- Location of snow storage should be indicated on the plans.
- The installation of a sidewalk along the York Street frontage needs to be addressed.

Please contact our office if you have any questions.

DRG/MDL
203943.96

**COMMITMENT & INTEGRITY
DRIVE RESULTS**

41 Hutchins Drive
Portland, Maine 04102
www.woodardcurran.com

T 800.426.4232
T 207.774.2112
F 207.774.6836

Att. 5-b

MEMORANDUM



TO: Molly Casto
FROM: Dan Goyette, PE
DATE: September 3, 2008
RE: Rufus Deering Lumber Company, 383 Commercial Street

Woodard & Curran has reviewed the Major Site Plan Application for the proposed site redevelopment at 383 Commercial Street. The proposed project involves the construction of a 29,000 square foot drive-through warehouse and reconfiguration of current storage shelter to accommodate access and internal circulation. The project also includes remodeling the existing retail building and minor parking lot improvements.

Documents Reviewed

- Revised Site Plan Workshop Submittal, addressed to Molly Casto, City of Portland Senior Planner, prepared by James R. Seymour, PE, Sebago Technics, Inc., dated August 22, 2008.
- Engineering Plan Sheets 1-9, prepared for Rufus Deering Lumber, prepared by Sebago Technics, Inc., dated August 22, 2008.

Comments

- The comments in our July 9 memo have not been addressed.
- The applicant has requested a waiver on the sidewalk requirement for York Street. They are partly basing their request on waiver criteria #6. The applicant states that installing the sidewalk as Public Services has requested inboard of the existing curb line, so there by reducing the street width, creates an unsafe condition. Both Tom Errico, Wilbur Smith Associates traffic consultant to the City, and Jim Carmody, City Traffic Engineer, have reviewed the proposed 4' sidewalk and 5' shoulder/bike lane configuration and are comfortable with the proposal.
- The applicant is proposing to close the driveway closest to the intersection of High and Commercial Street. To install the new curb and sidewalk would impact a moratorium street. We suggest the installation of a temporary bituminous curb and sidewalk. The installation of the granite curb and brick sidewalk can then take place at the same time as the work required to reconfigure that corner of the street after the moratorium has expired.

Please contact our office if you have any questions.

DRG
203943.96

Att- D-C

**COMMITMENT & INTEGRITY
DRIVE RESULTS**

41 Hutchins Drive
Portland, Maine 04102
www.woodardcurran.com

T 800.426.4262
T 207.774.2112
F 207.774.6635

MEMORANDUM



TO: Molly Casto
FROM: Dan Goyette, PE
DATE: November 3, 2008
RE: Rufus Deering Lumber Company, 383 Commercial Street

Woodard & Curran has reviewed the Major Site Plan Application for the proposed site redevelopment at 383 Commercial Street. The proposed project involves the construction of a 29,000 square foot drive-through warehouse and reconfiguration of current storage shelter to accommodate access and internal circulation. The project also includes remodeling the existing retail building and minor parking lot improvements.

Documents Reviewed

- Response to Comments Submittal, addressed to Molly Casto, City of Portland Senior Planner, prepared by James R. Seymour, PE, Sebago Technics, Inc., dated October 15, 2008.

Comments

- The applicant has agreed to provide a contribution in lieu of implementing on-site stormwater treatment measures. The site does not allow for the implementation of these measures due to the shallow existing inverts in Commercial Street. The contribution amount is based upon the size of the parking area and will be used for the improvements to the stormwater outfall on the Berlin Mill Wharf (by Gowen Marine) across from High St. The amount of the contribution has been determined to be \$20,000.

Please contact our office if you have any questions.

DRG
203943.96

Molly Casto - Rufus Deering

From: Jeff Tarling
To: Molly Casto
Date: 9/5/2008 10:26 AM
Subject: Rufus Deering

Hi Molly -

I reviewed the latest landscape plan for the proposed Rufus Deering project and find it acceptable. The plan adds trees along High Street from Commercial to York Street. I would agree with the plan that street trees along York Street might not be feasible due to the potential root system / wall conflict.

Overall the tree placement & selection should improve the area.

Thanks,

Jeff Tarling
City Arborist

Att. 6-b

Molly Casto - Re: Rufus Deering

From: Jeff Tarling
To: Molly Casto
Date: 11/14/2008 9:18 AM
Subject: Re: Rufus Deering
CC: Barbara Barhydt

Hi Molly -

The Rufus Deering landscape plan is acceptable as shown. I would make a condition that the empty tree well along Commercial Street be planted with a new street tree. Limitations to the site along York Street to plant trees close to the retaining wall due to potential problem with tree roots interfering with the wall stability.

Jeff Tarling
City Arborist

A667

Molly Casto - Rufus Deering Lumber Company, 383 Commercial Street

From: David Margolis-Pineo
To: Molly Casto
Date: 9/5/2008 10:18 AM
Subject: Rufus Deering Lumber Company, 383 Commercial Street
CC: Dan Goyette; James Carmody; Tom Errico

September 5, 2008

To: Molly Casto
From: David Margolis-Pineo
Re: Rufus Deering Lumber Company, 383 Commercial Street

Section III of the City's Technical Design Standards states:

A. Commercial, Industrial and Multi-Family (3 units or more)

- (a) **Minimum driveway width (one-way):** Any site with driveway access to a street shall have a minimum 20 foot wide driveway for one-way ingress or egress. Driveways shall permit traffic to enter and leave the site simultaneously without conflict in aisles, parking or maneuvering areas. Both the entrance and exit drives shall be marked with appropriate signage.
- (b) **Minimum/maximum driveway width (two-way):** Any site with driveway access to a street shall have a 24 foot wide driveway for two-way ingress and egress. Under special conditions a driveway width of up to 30 feet will be permitted if the City's Transportation Engineer determines that site conditions or vehicle characteristics warrant a wider access, e.g., dedicated turn lanes at exits.

It is my understanding that the City's Transportation Engineer is agreeable with permitting a 30 foot wide driveway at this location.

To be allowed to install a 36 foot drive, Rufus Deering must submit a written request to the Planning Board for a waiver.

Due to the fact that Rufus Deering is proposing to consolidate their driveway cuts, Public Services has no objection if the Planning Board should grant a waiver request to widen the proposed driveway cut to 36 feet.

The applicant does show an additional curb cut onto Commercial St. which is currently not being used. The City has painted parking spaces in front of this cut. If the applicant is not intending to use this driveway cut in the future, it would be desirable to eliminate it.

Att. 8 -a

Molly Casto - Rufus Deering

From: "Errico, Thomas A" <FERRICO@wilbursmith.com>
To: "Molly Casto" <MPC@portlandmaine.gov>
Date: 9/4/2008 4:12 PM
Subject: Rufus Deering
CC: "James Carmody" <JPC@portlandmaine.gov>, "Katherine Farley" <KAS@portlandmaine.gov>

Molly –

The following summarizes the status of my August 22, 2008 comments and additional comments associated with a review of the August 22, 2008 submittal from Sebago Technics, Inc.

1. Based upon discussions at the Department of Public Services, it is our recommendation that a sidewalk be provided on York Street between High Street and Maple Street (on the south side). Based upon a field investigation, it is suggested that the sidewalk be located within a portion of the existing York Street roadway pavement. The applicant shall submit plans for review and comment. I would note that there may be sections of the sidewalk where a 4-foot width will be necessary to ensure reasonable on-road bicycle provisions can be provided.
Status: Both Jim Carmody and I conducted a field review of existing roadway characteristics on York Street as it relates to the provision of a sidewalk on the abutting frontage of the Rufus Deering property. It is our professional opinion that the proposed layout as illustrated on the August 22, 2008 plan is acceptable from a public safety perspective. Provision of a 5-foot bicycle lane will serve bicyclist well and the 4-foot sidewalk will provide an enhanced pedestrian environment that would be expected in an urban environment. The alignment of travel lanes will be adequate and the overall roadway cross-section will provide a good balance of multi-modal accommodations.
2. It is recommended that the Commercial Street driveway located nearest High Street be eliminated and a two-way primary entrance be located along the middle of the property frontage. The applicant shall submit a revised plan for review and comment. On-street parking conditions on Commercial Street may have to be modified to accommodate this primary driveway. I'll coordinate with Jim Carmody on the parking issue after receiving the revised plans.
Status: The plans illustrate this change and therefore I find the plans to be acceptable. I would note that because Commercial Street is a moratorium street, temporary treatments will be required until the moratorium has expired. I believe Dan Goyette's letter includes comments on this issue.
3. The northeastern radius on the corner of the Commercial Street/High Street intersection should be reduced in size such that the sidewalk is located in the public right-of-way. This change will also greatly benefit pedestrians by reducing the intersection crossing distance and will slow vehicle speeds. Implementation of the suggested changes cannot occur until the Commercial Street pavement moratorium expires or City policy changes. I would suggest that the applicant prepare a plan that illustrates the recommended change and the applicant be responsible implementation of the changes at a time when restrictions have expired.
Status: The plans illustrate this change and therefore I find the plans to be acceptable. I would note that because Commercial Street is a moratorium street, we suggest that this work not take place until the moratorium expires. I believe Dan Goyette's letter includes comments on this issue.
4. The city has been collecting monetary contributions for the future installation of a traffic signal at the Commercial Street/High Street intersection. The applicant may be required to contribute towards the signal installation. Because the project is not changing substantially, from a traffic generation perspective, I would expect the contribution to minimal. I'll work with City staff to determine an appropriate contribution

level and provide that information in the future.

Status: It is my understanding that the applicant does not expect any additional traffic associated with the proposed project. The applicant should provide documentation that supports this case for review and comment.

Additional Comments

- The proposed main driveway is being proposed as a three-lane (1 entry lane and 2 exit lanes) curb cut with a total width of 36 feet. As noted in the applicant's submittal, this width exceeds the City standard and therefore requires a design waiver. I would ask that the applicant provide supporting information on the need for this width. This should consist of an Auto-Turn vehicle turning template graphic and traffic volume projections, if available.

Please contact me if you have any questions.

Best regards

Thomas A. Errico, P.E.
Senior Transportation Engineer
Wilbur Smith Associates
59 Middle Street
Portland, Maine 04101
w: 207.871.1785 f: 207.871.5825
TErrico@WilburSmith.com
www.WilburSmith.com

Molly Casto - Rufus Deering - Commercial Street

Att 8-b

From: "Errico, Thomas A" <TERRICO@wilbursmith.com>
To: "Molly Casto" <MPC@portlandmaine.gov>
Date: 11/14/2008 10:57 AM
Subject: Rufus Deering - Commercial Street
CC: "James Carmody" <JPC@portlandmaine.gov>, "Katherine Earley" <KAS@portlandmaine.gov>

Molly –

The following summarizes the status of my final comments for the project.

1. Based upon discussions at the Department of Public Services, it is our recommendation that a sidewalk be provided on York Street between High Street and Maple Street (on the south side). Based upon a field investigation, it is suggested that the sidewalk be located within a portion of the existing York Street roadway pavement. The applicant shall submit plans for review and comment. I would note that there may be sections of the sidewalk where a 4-foot width will be necessary to ensure reasonable on-road bicycle provisions can be provided.

Status 9/4/08: Both Jim Carmody and I conducted a field review of existing roadway characteristics on York Street as it relates to the provision of a sidewalk on the abutting frontage of the Rufus Deering property. It is our professional opinion that the proposed layout as illustrated on the August 22, 2008 plan is acceptable from a public safety perspective. Provision of a 5-foot bicycle lane will serve bicyclist well and the 4-foot sidewalk will provide an enhanced pedestrian environment that would be expected in an urban environment. The alignment of travel lanes will be adequate and the overall roadway cross-section will provide a good balance of multi-modal accommodations.

Current Status: The above response continues to remain valid.
2. It is recommended that the Commercial Street driveway located nearest High Street be eliminated and a two-way primary entrance be located along the middle of the property frontage. The applicant shall submit a revised plan for review and comment. On-street parking conditions on Commercial Street may have to be modified to accommodate this primary driveway. I'll coordinate with Jim Carmody on the parking issue after receiving the revised plans.

Status 9/4/08: The plans illustrate this change and therefore I find the plans to be acceptable. I would note that because Commercial Street is a moratorium street, temporary treatments will be required until the moratorium has expired. I believe Dan Goyette's letter includes comments on this issue.

Current Status: MaineDOT and the Department of Public Services have concluded that the proposed scope of work can proceed.
3. The northeastern radius on the corner of the Commercial Street/High Street intersection should be reduced in size such that the sidewalk is located in the public right-of-way. This change will also greatly benefit pedestrians by reducing the intersection crossing distance and will slow vehicle speeds. Implementation of the suggested changes cannot occur until the Commercial Street pavement moratorium expires or City policy changes. I would suggest that the applicant prepare a plan that illustrates the recommended change and the applicant be responsible implementation of the changes at a time when restrictions have expired.

Status 9/4/08: The plans illustrate this change and therefore I find the plans to be acceptable. I would note that because Commercial Street is a moratorium street, we suggest that this work not take place until the moratorium expires. I believe Dan Goyette's letter includes comments on this issue.

Current Status: MaineDOT and the Department of Public Services have concluded that the proposed scope of work can proceed.
4. The city has been collecting monetary contributions for the future installation of a traffic signal at the

Commercial Street/High Street intersection. The applicant may be required to contribute towards the signal installation. Because the project is not changing substantially, from a traffic generation perspective, I would expect the contribution to be minimal. I'll work with City staff to determine an appropriate contribution level and provide that information in the future.

Status 9/4/08: It is my understanding that the applicant does not expect any additional traffic associated with the proposed project. The applicant should provide documentation that supports this case for review and comment.

Current Status: The proposed project is expected to generate an additional 24 driveway trips during the PM peak hour according to the November 5, 2008 analysis prepared by Sebago Technics, Inc. Based upon prior development contribution levels, the proposed project should contribute \$3,900.00 towards the future installation of a traffic signal at the Commercial Street/High Street intersection.

Additional Comments

- The proposed main driveway is being proposed as a three-lane (1 entry lane and 2 exit lanes) curb cut with a total width of 36 feet. As noted in the applicant's submittal, this width exceeds the City standard and therefore requires a design waiver. I would ask that the applicant provide supporting information on the need for this width. This should consist of an Auto-Turn vehicle turning template graphic and traffic volume projections, if available.

Current Status: We support a driveway width waiver to accommodate large vehicle access and egress maneuvers. However, it is recommended that the driveway be marked for one wide entry lane and one wide exit lane.

- The proposed project will provide parking stall sizes that exceed City of Portland standards. I support these larger spaces due to the customer/business activity occurring on site.
- A handicapped ramp should be provided at the southeast corner of York Street and High Street intersection.

Please contact me if you have any questions.
Best regards

Thomas A. Errico, P.E.
Senior Transportation Engineer
Wilbur Smith Associates
59 Middle Street
Portland, Maine 04101
w: 207.871.1785 f: 207.871.5825
TErrico@WilburSmith.com
www.WilburSmith.com

Rufus Deering Off Site Improvements
 Portland, Maine

STI Job# 07383
 6-Oct-08

Att. 9

York Street Sidewalk Improvements (620 LF +/-)

Sidewalk Breakdown	Unit Cost	Subtotal(\$)	Total Costs
Brick surface/base prep	\$85/ SY	\$29,325.00	
Granite Curbing	\$35/LF	\$21,700.00	
Trench pavement and base prep.	LS	\$2,250.00	
Drainage Improvements	\$3000/ea	\$3,000.00	
Piping replaced	\$40/LF w 20lf	\$800.00	
Contractor/Equipment Mobil. W traffic control	6,000	\$6,000.00	
Erosion Control Measures	200	\$200.00	
Landscaping	Not Included		
Pavement markings/ lane striping	1,500	\$1,500.00	
Pedestrian Signals	2800/ea	\$11,200.00	
Construction Cost			\$75,975.00
Street Opening fees.	9,650	\$9,650.00	9,650.00
Sub Total			85,625.00
DESIGN/CONSTR ADMIN FEES		\$3,000.00	3,000.00
Base Estimated Project Cost for Applicant			88,625.00

Estimate does not account for relocation of any CMP poles, or traffic arms if necessary.
 Estimate does not include tranfer of materials excavated in St. which may be deemed hazardous and require testing or hauling to approved site.

Area of Sidewalk = 620ft(length) x 5 ft(width)= 3100SF=345 SY
 Design includes field survey of street/ and engineering design to match street profile
 Street opening permit fees are approximate

High Street Sidewalk Improvements (280 LF +/-)

	Unit Cost	Subtotal(\$)	Total Costs
Sidewalk Breakdown			
Brick surfacew/base prep	\$85/ SY	\$17,000.00	
Granite Curbing	\$35/LF	\$9,800.00	
Trench pavement and base prep.	LS	\$1,200.00	
Drainage improvements	\$3000/ea	\$0.00	
Piping replaced	\$40/LF w 20lf	\$0.00	
Contractor/Equipment Mobil. W traffic control	Lump	\$2,500.00	
Erosion Control Measures	LS	\$200.00	
Landscaping	Not Included		
Pavement markings/ lane striping	Lump	\$500.00	
Pedestrian Signals	2800/ea	\$0.00	
Construction Cost			\$31,200.00
Street Opening fees.	Lump	\$4,000.00	4,000.00
Sub Total			\$35,200.00
DESIGN/CONSTR ADMIN FEES		\$800.00	\$800.00
Base Estimated Project Cost for Applicant			\$36,000.00

Maple Street Sidewalk Improvements (110LF +/-)			
	Unit Cost	Subtotal(\$)	Total Costs
Sidewalk Breakdown			
Brick surfacew/base prep	\$85/ SY	\$5,270.00	
Granite Curbing	\$35/LF	\$350.00	
Trench pavement and base prep.	LS	\$500.00	
Drainage improvements	\$3000/ea	\$0.00	
Piping replaced	\$40/LF w 20lf	\$0.00	
Contractor/Equipment Mobil. W traffic control	Lump	\$500.00	
Erosion Control Measures	LS	\$200.00	
Landscaping/Esplande	110LF	\$550.00	
Pavement markings/ lane striping	Lump	\$0.00	
Pedestrian Signals	2800/ea	\$0.00	
Construction Cost			\$7,370.00
Street Opening fees.	Lump	\$1,600.00	1,600.00
Sub Total			\$8,970.00
DESIGN/CONSTR ADMIN FEES		\$200.00	\$200.00
Base Estimated Project Cost for Applicant			\$9,170.00

York St. Base Estimate
High Street Base Estimate
Maple Street Base Estimate

\$88,625.00
\$36,000.00
\$9,170.00

Total Base Estimated Project Cost for Applicant
10% Contingency

\$133,795.00
\$13,379.50

Total Adjusted Estimated Project Cost for All Sidewalks

\$147,174.50

Estimated Project Total = \$1,200,000.00
% Project cost towards curb and sidewalks = 12.3%

Molly Casto - Rufus Deering - Brick Sidewalk Price

From: David Margolis-Pineo
To: Alex Jaegerman ; Dan Goyette; Jean Fraser; Marge Schmuckal; Molly Casto; Rick Knowland ; William Needelman
Date: 10/1/2008 5:18 PM
Subject: Rufus Deering - Brick Sidewalk Price
CC: Gretel Varney; Todd Merkle

Just spoke with Scott LaBrecque who does sidewalks for a living.

The following prices should build the 620 L.F. of brick sidewalk on York-St.

Move catch basin w/ curb inlet.	\$5,000	
Granite Curbing @\$35/ft	\$21,700	same as estimate by applicant
Brick sidewalk @ \$120/sy	<u>\$46,335</u>	Applicant used \$85/sy
	about \$73,035	

Note: the newly specified city bricks, cost about \$30/sy more than the what LaChance's bricks cost. So the cost per sq yd of brick sidewalk increased from about \$90/sy to \$120/sy.

Att - 10

Molly Casto - Rufus Deering - Brick Sidewalk Price

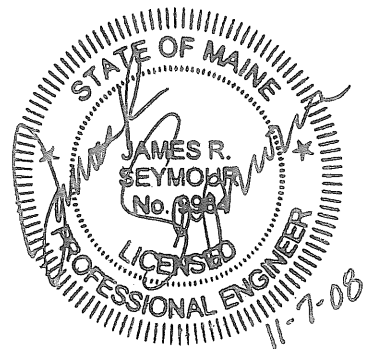
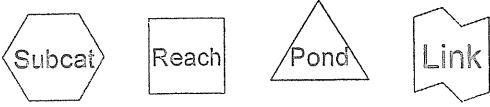
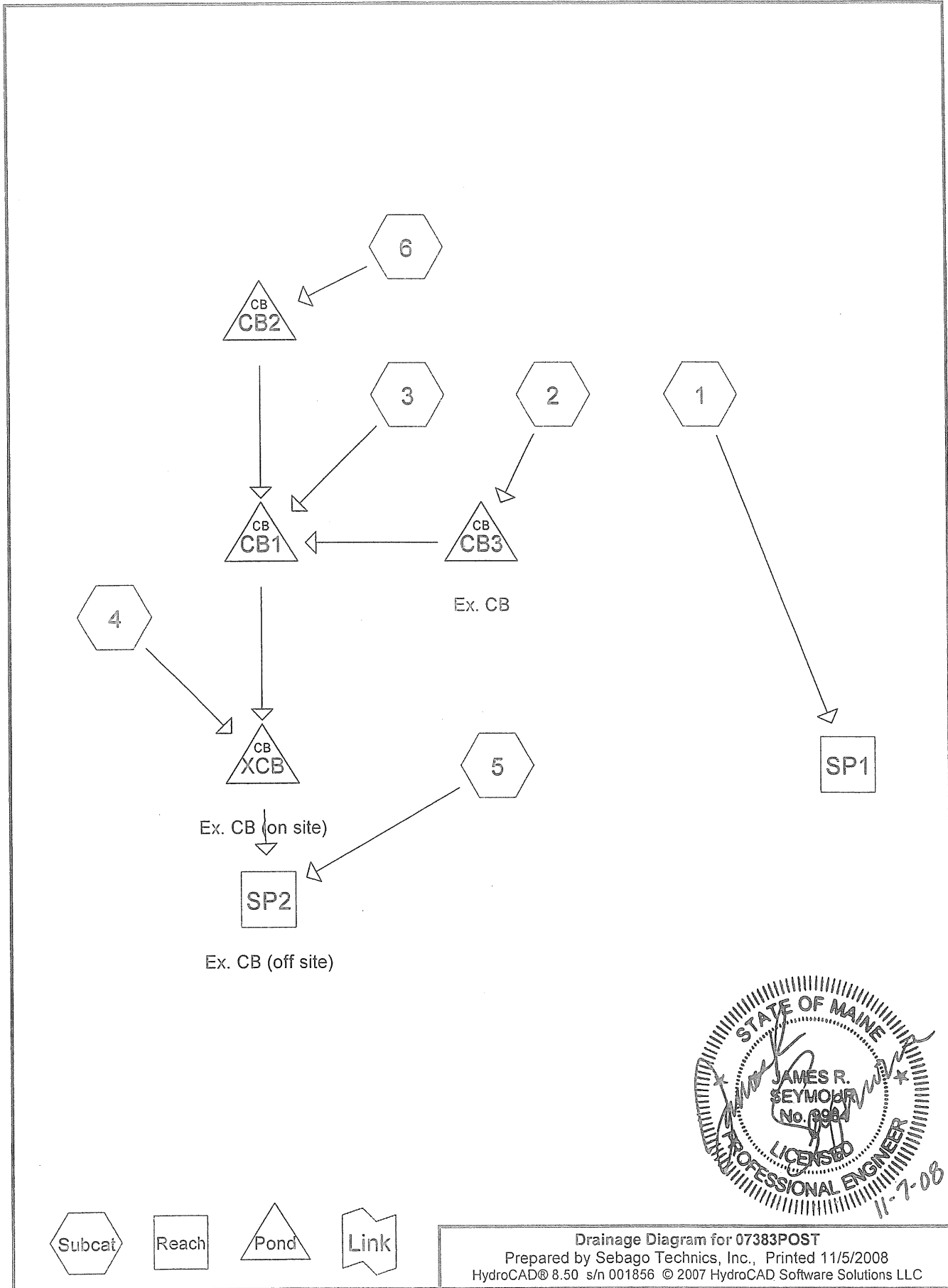
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	about	\$73,035

Note: the newly specified city bricks, cost about \$30/sy more than the what LaChance's bricks cost. So the cost per sq yd of brick sidewalk increased from about \$90/sy to \$120/sy.



Drainage Diagram for 07383POST
 Prepared by Sebago Technics, Inc., Printed 11/5/2008
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THE ENGINEERING SEAL ON THIS COVER PAGE ONLY APPLIES TO ALL 40 PAGES OF CALCULATIONS ATTACHED.

07383POST

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Rufus Deering Lumber
Type III 24-hr 2-Year Rainfall=3.00"

Printed 11/5/2008

Page 2

Time span=0.00-36.00 hrs, dt=0.01 hrs, 3601 points

Runoff by SCS TR-20 method, UH=SCS

Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment 1: Runoff Area=0.616 ac 100.00% Impervious Runoff Depth=2.77"
Tc=5.0 min CN=98 Runoff=1.86 cfs 0.142 af

Subcatchment 2: Runoff Area=0.610 ac 100.00% Impervious Runoff Depth=2.77"
Tc=5.0 min CN=98 Runoff=1.84 cfs 0.141 af

Subcatchment 3: Runoff Area=0.458 ac 100.00% Impervious Runoff Depth=2.77"
Tc=5.0 min CN=98 Runoff=1.38 cfs 0.106 af

Subcatchment 4: Runoff Area=0.497 ac 94.57% Impervious Runoff Depth=2.66"
Tc=5.0 min CN=97 Runoff=1.47 cfs 0.110 af

Subcatchment 5: Runoff Area=0.301 ac 100.00% Impervious Runoff Depth=2.77"
Tc=5.0 min CN=98 Runoff=0.91 cfs 0.069 af

Subcatchment 6: Runoff Area=0.085 ac 0.00% Impervious Runoff Depth=2.45"
Tc=5.0 min CN=95 Runoff=0.24 cfs 0.017 af

Reach SP1: Inflow=1.86 cfs 0.142 af
Outflow=1.86 cfs 0.142 af

Reach SP2: Ex. CB (off site) Avg. Depth=1.00' Max Vel=5.37 fps Inflow=5.83 cfs 0.443 af
D=12.0" n=0.011 L=84.0' S=0.0077 '/' Capacity=3.70 cfs Outflow=3.74 cfs 0.443 af

Pond CB1: Peak Elev=10.14' Inflow=3.46 cfs 0.264 af
12.0" x 71.5' Culvert Outflow=3.46 cfs 0.264 af

Pond CB2: Peak Elev=9.95' Inflow=0.24 cfs 0.017 af
12.0" x 120.5' Culvert Outflow=0.24 cfs 0.017 af

Pond CB3: Ex. CB Peak Elev=10.34' Inflow=1.84 cfs 0.141 af
12.0" x 101.0' Culvert Outflow=1.84 cfs 0.141 af

Pond XCB: Ex. CB (on site) Peak Elev=10.12' Inflow=4.93 cfs 0.374 af
12.0" x 6.0' Culvert Outflow=4.93 cfs 0.374 af

Total Runoff Area = 2.567 ac Runoff Volume = 0.585 af Average Runoff Depth = 2.74"
4.36% Pervious = 0.112 ac 95.64% Impervious = 2.455 ac

Summary for Subcatchment 1:

Runoff = 1.86 cfs @ 12.07 hrs, Volume= 0.142 af, Depth= 2.77"

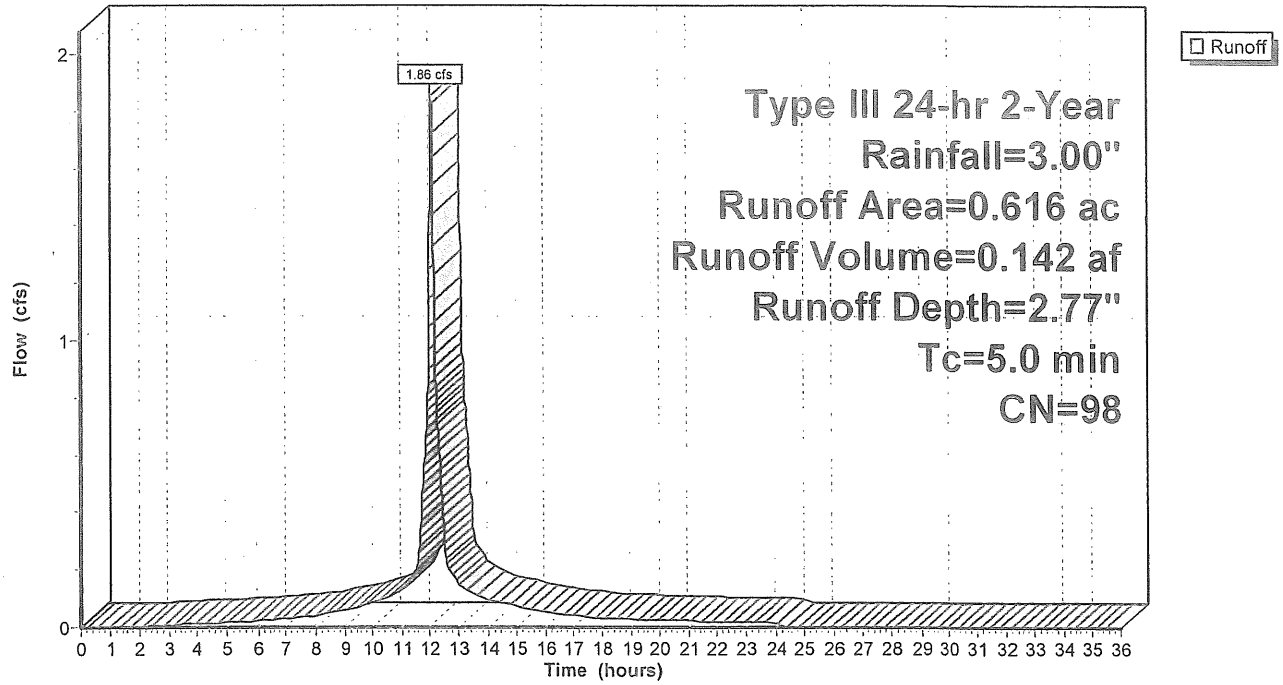
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.616	98	Paved parking & roofs
0.616		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 1:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 2-Year Rainfall=3.00"
Printed 11/5/2008
Page 4

Summary for Subcatchment 2:

Runoff = 1.84 cfs @ 12.07 hrs, Volume= 0.141 af, Depth= 2.77"

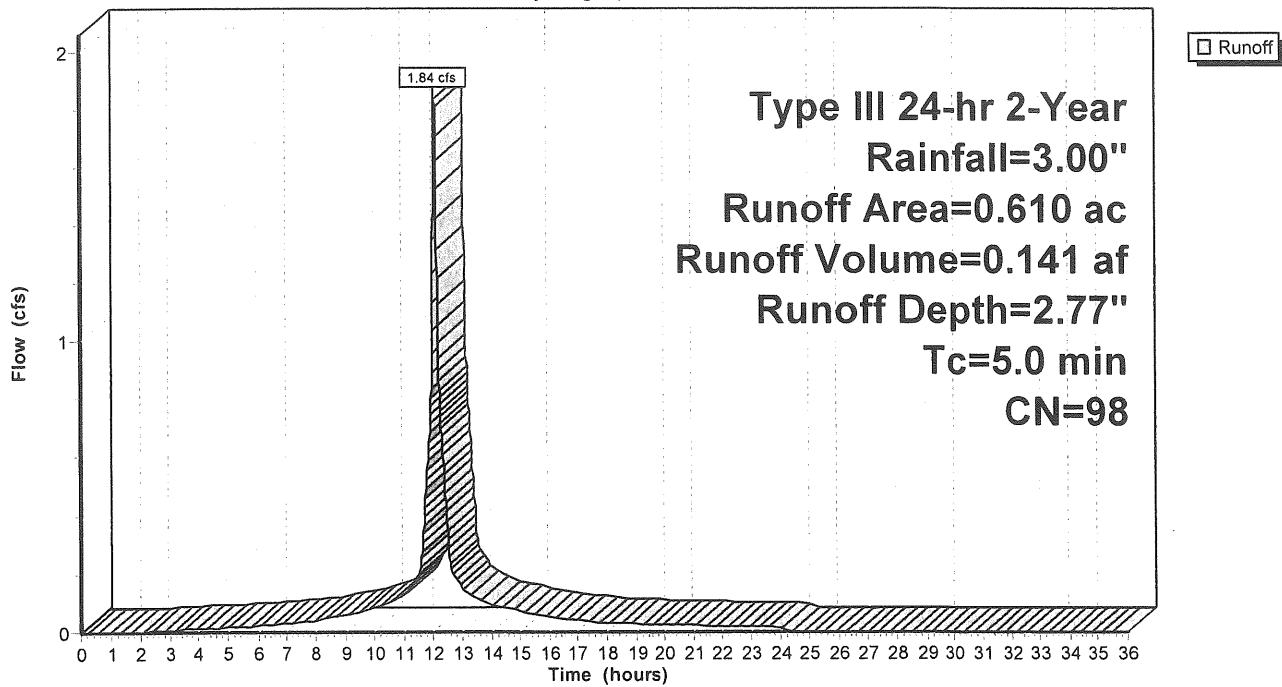
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.610	98	Paved parking & roofs
0.610		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 2:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 2-Year Rainfall=3.00"

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Summary for Subcatchment 3:

Runoff = 1.38 cfs @ 12.07 hrs, Volume= 0.106 af, Depth= 2.77"

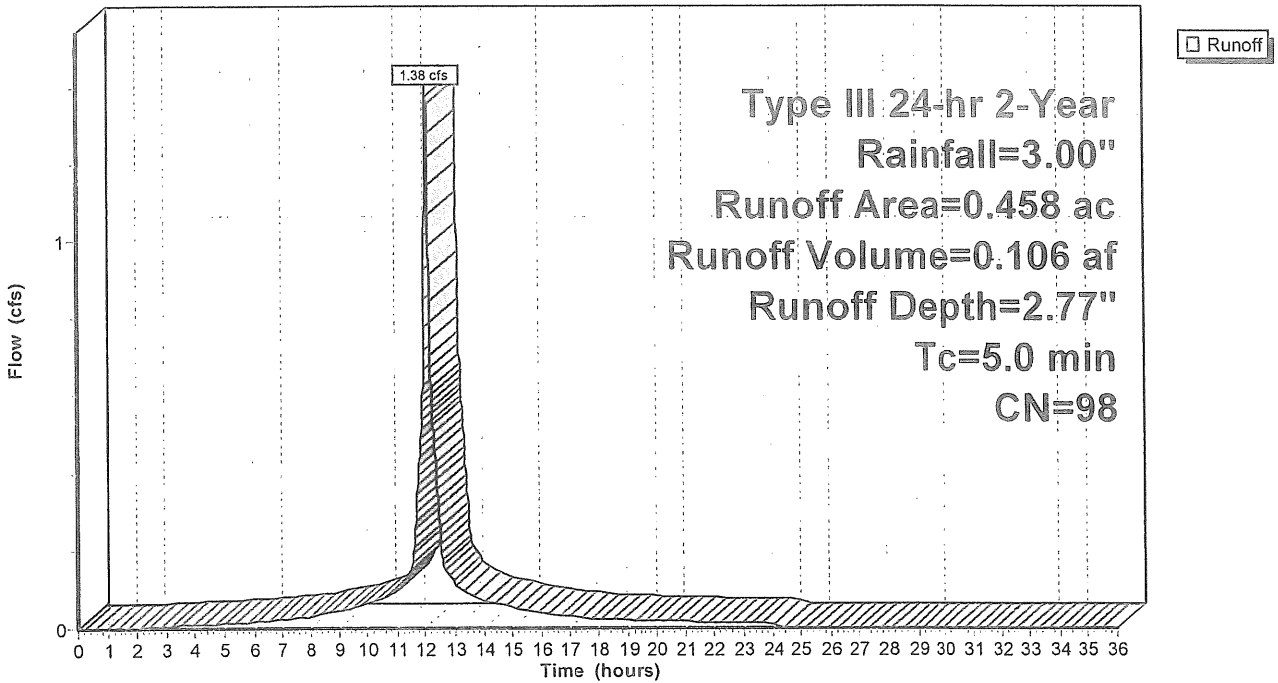
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.458	98	Paved parking & roofs
0.458		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 3:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 2-Year Rainfall=3.00"

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Summary for Subcatchment 4:

Runoff = 1.47 cfs @ 12.07 hrs, Volume= 0.110 af, Depth= 2.66"

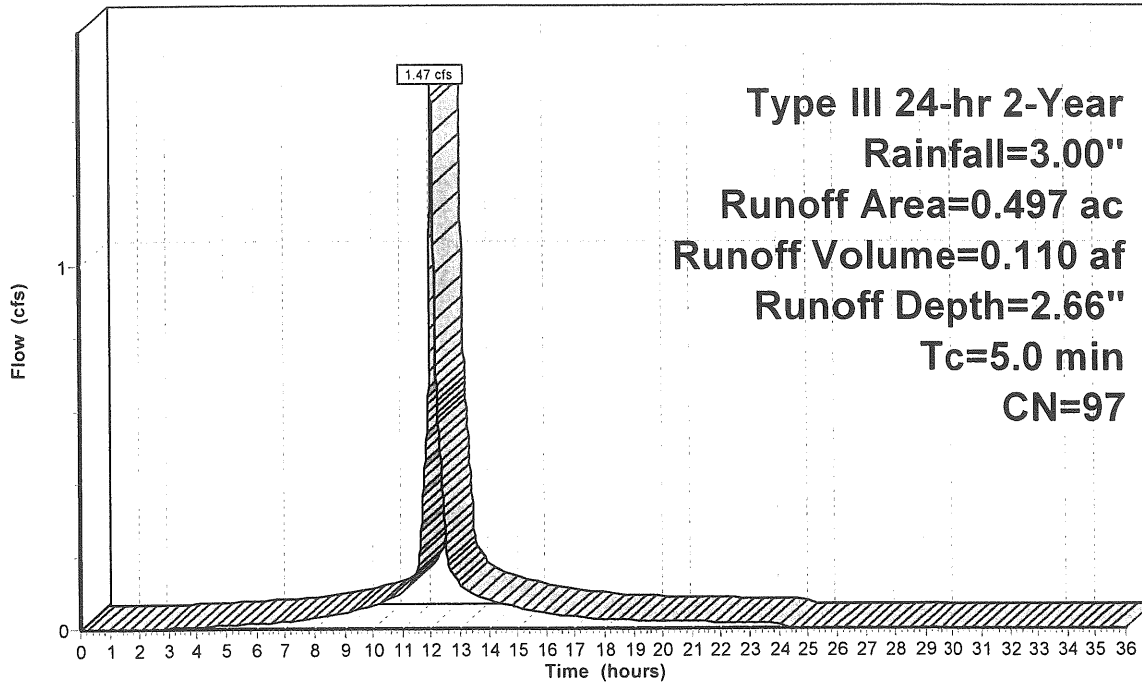
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.470	98	Paved parking & roofs
0.027	80	>75% Grass cover, Good, HSG D
0.497	97	Weighted Average
0.027		Pervious Area
0.470		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 4:

Hydrograph



Summary for Subcatchment 5:

Runoff = 0.91 cfs @ 12.07 hrs, Volume= 0.069 af, Depth= 2.77"

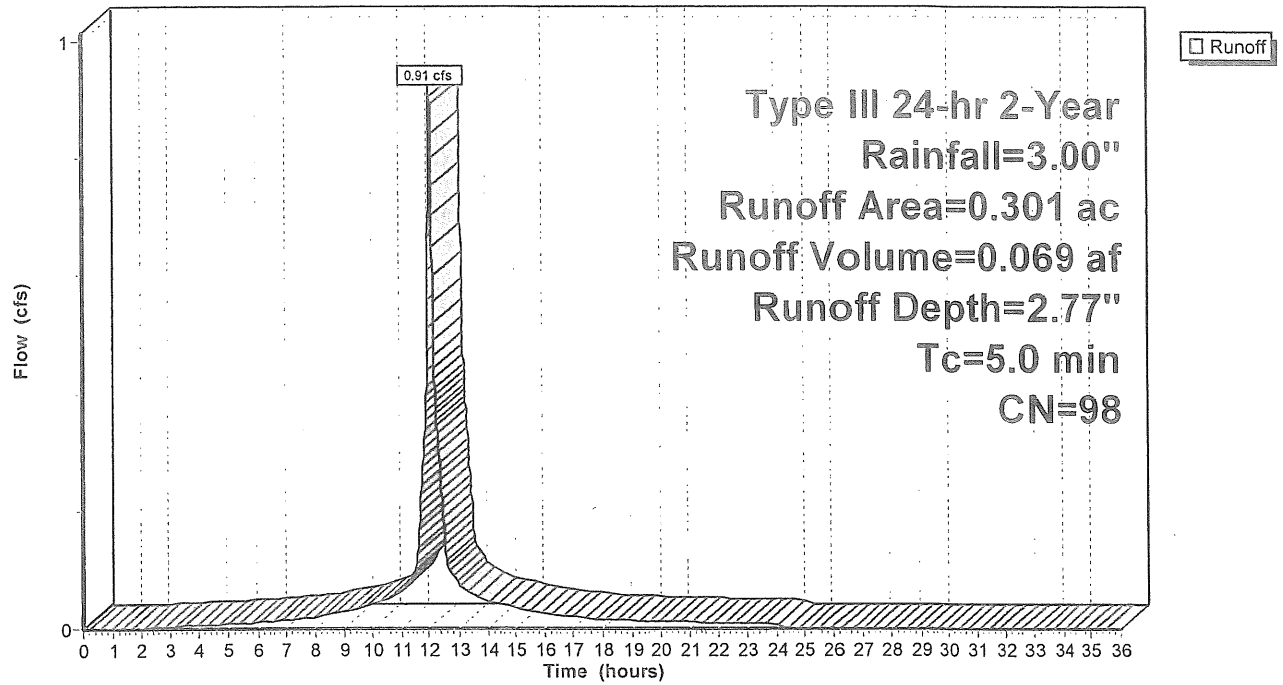
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.301	98	Paved parking & roofs
0.301		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 5:

Hydrograph



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Type III 24-hr 2-Year Rainfall=3.00"
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Summary for Subcatchment 6:

Runoff = 0.24 cfs @ 12.07 hrs, Volume= 0.017 af, Depth= 2.45"

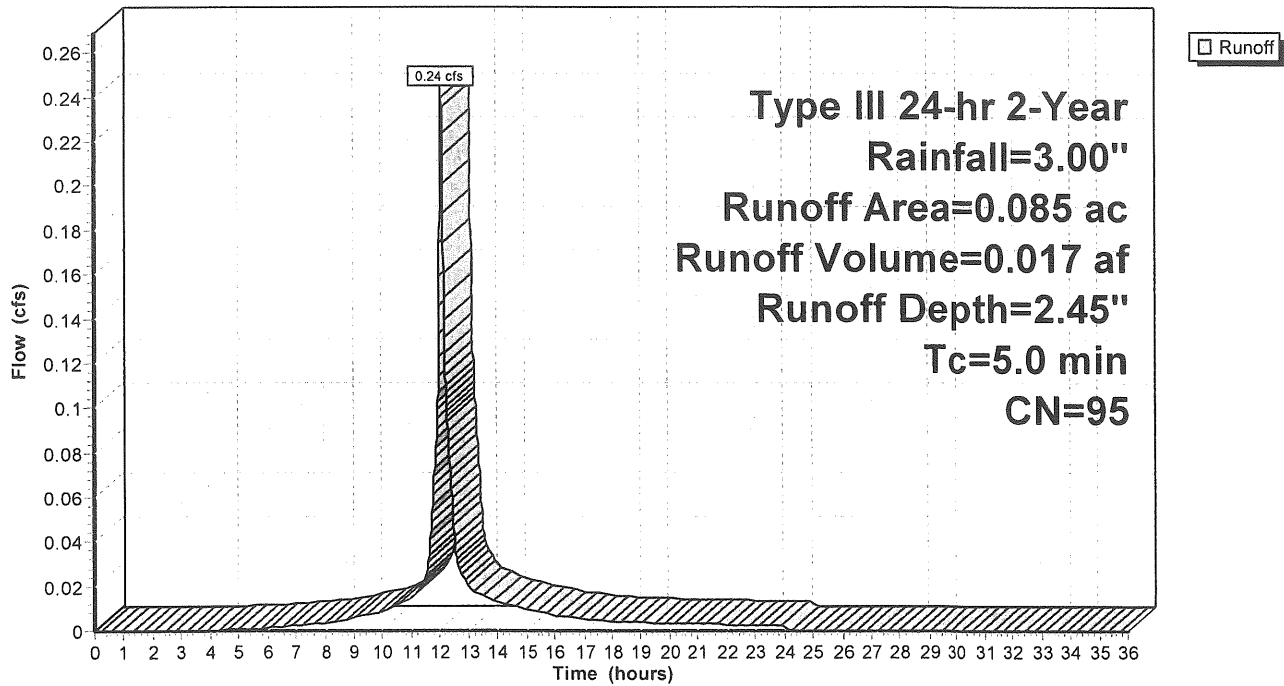
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
* 0.085	95	Gravel
0.085		Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 6:

Hydrograph



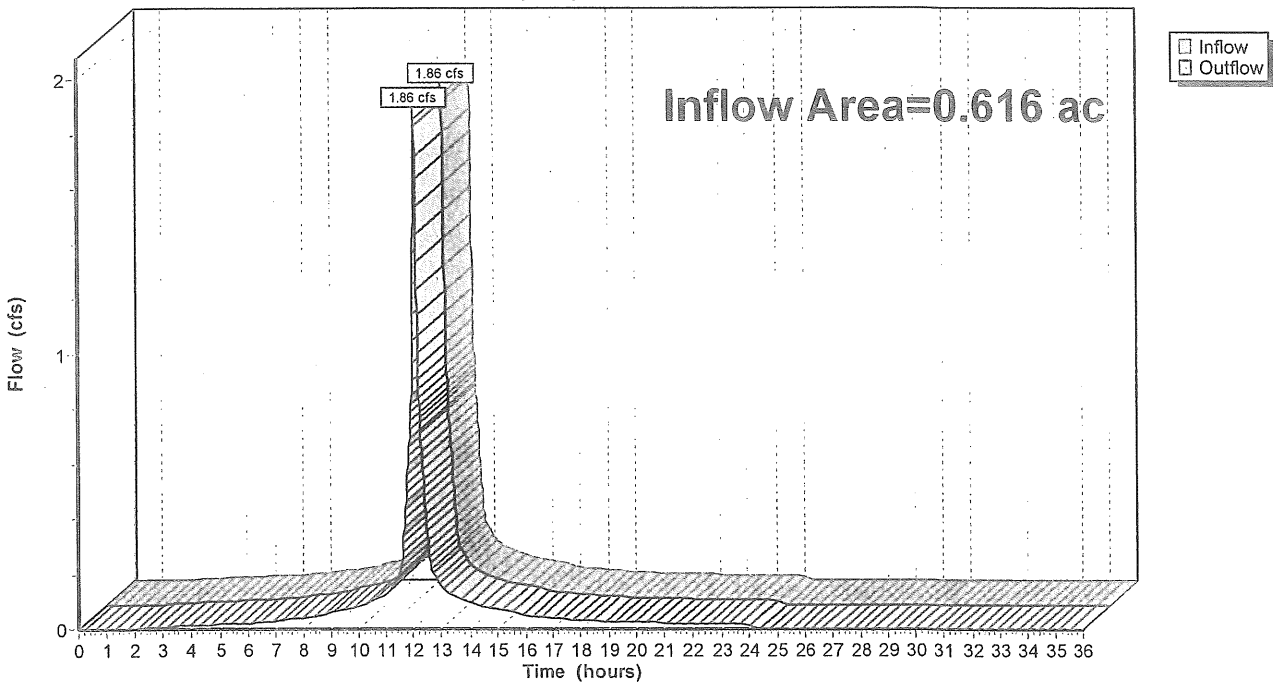
Summary for Reach SP1:

Inflow Area = 0.616 ac, 100.00% Impervious, Inflow Depth = 2.77" for 2-Year event
Inflow = 1.86 cfs @ 12.07 hrs, Volume= 0.142 af
Outflow = 1.86 cfs @ 12.07 hrs, Volume= 0.142 af, Atten= 0%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs

Reach SP1:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 2-Year Rainfall=3.00"

Printed 11/5/2008

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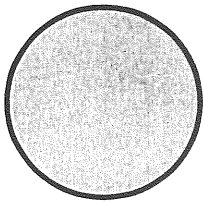
Summary for Reach SP2: Ex. CB (off site)

Inflow Area = 1.951 ac, 94.26% Impervious, Inflow Depth = 2.73" for 2-Year event
Inflow = 5.83 cfs @ 12.07 hrs, Volume= 0.443 af
Outflow = 3.74 cfs @ 12.01 hrs, Volume= 0.443 af, Atten= 36%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Max. Velocity= 5.37 fps, Min. Travel Time= 0.3 min
Avg. Velocity = 2.16 fps, Avg. Travel Time= 0.6 min

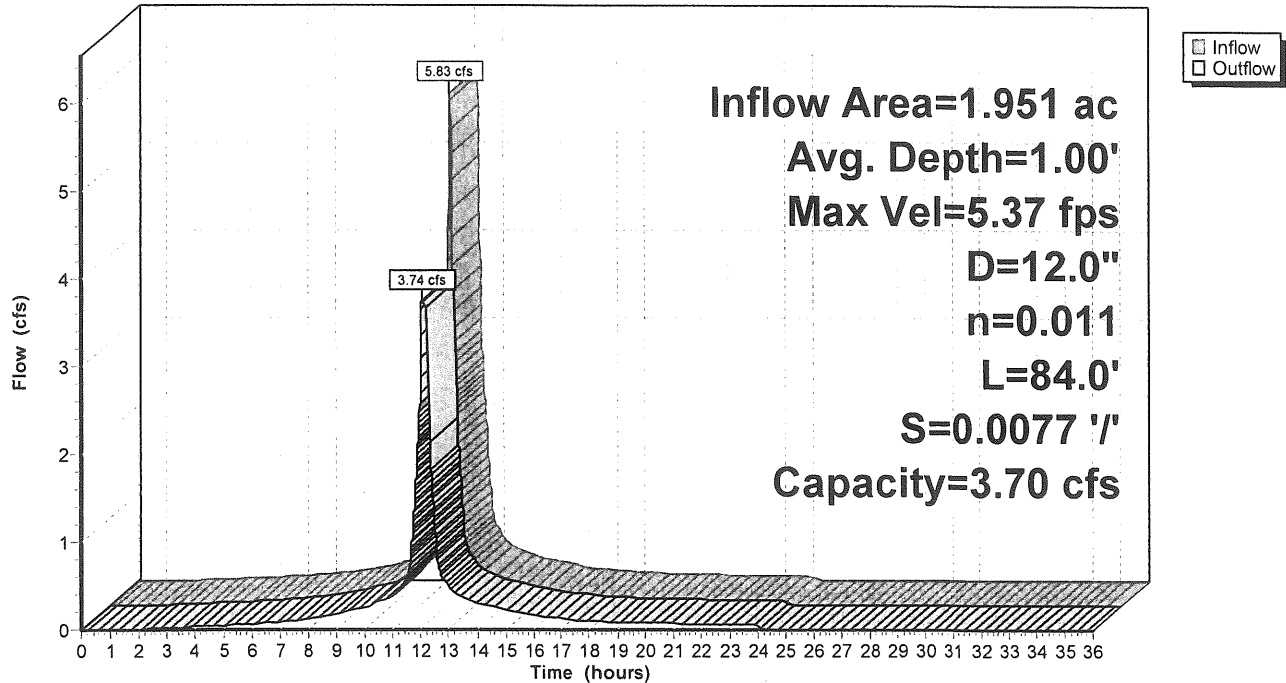
Peak Storage= 66 cf @ 12.01 hrs, Average Depth at Peak Storage= 1.00'
Bank-Full Depth= 1.00', Capacity at Bank-Full= 3.70 cfs

12.0" Diameter Pipe, n= 0.011 Concrete pipe, straight & clean
Length= 84.0' Slope= 0.0077 '/'
Inlet Invert= 8.00', Outlet Invert= 7.35'



Reach SP2: Ex. CB (off site)

Hydrograph



Summary for Pond CB1:

Inflow Area = 1.153 ac, 92.63% Impervious, Inflow Depth = 2.74" for 2-Year event
 Inflow = 3.46 cfs @ 12.07 hrs, Volume= 0.264 af
 Outflow = 3.46 cfs @ 12.07 hrs, Volume= 0.264 af, Atten= 0%, Lag= 0.0 min
 Primary = 3.46 cfs @ 12.07 hrs, Volume= 0.264 af

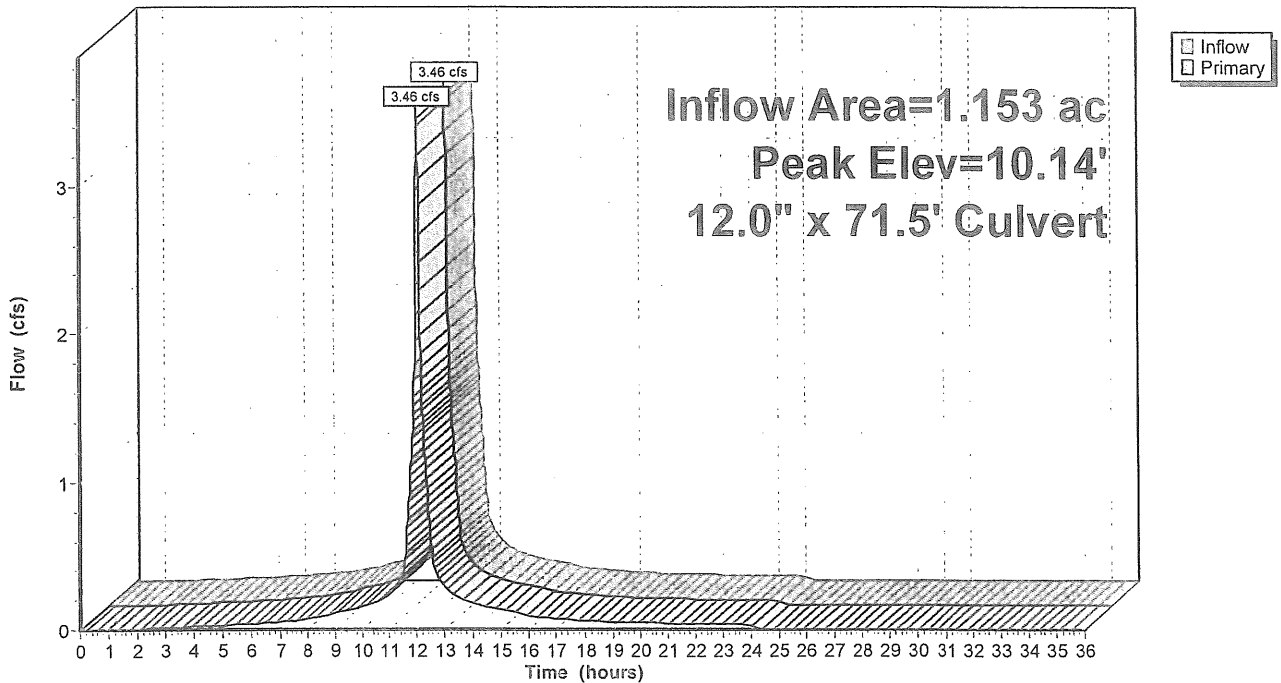
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3
 Peak Elev= 10.14' @ 12.07 hrs
 Flood Elev= 12.25'

Device	Routing	Invert	Outlet Devices
#1	Primary	9.00'	12.0" x 71.5' long SD-1 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.47' S= 0.0074 '/' Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=3.46 cfs @ 12.07 hrs HW=10.14' (Free Discharge)
 ↖ 1=SD-1 (Barrel Controls 3.46 cfs @ 4.85 fps)

Pond CB1:

Hydrograph



Summary for Pond CB2:

Inflow Area = 0.085 ac, 0.00% Impervious, Inflow Depth = 2.45" for 2-Year event
 Inflow = 0.24 cfs @ 12.07 hrs, Volume= 0.017 af
 Outflow = 0.24 cfs @ 12.07 hrs, Volume= 0.017 af, Atten= 0%, Lag= 0.0 min
 Primary = 0.24 cfs @ 12.07 hrs, Volume= 0.017 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs

Peak Elev= 9.95' @ 12.07 hrs

Flood Elev= 13.25'

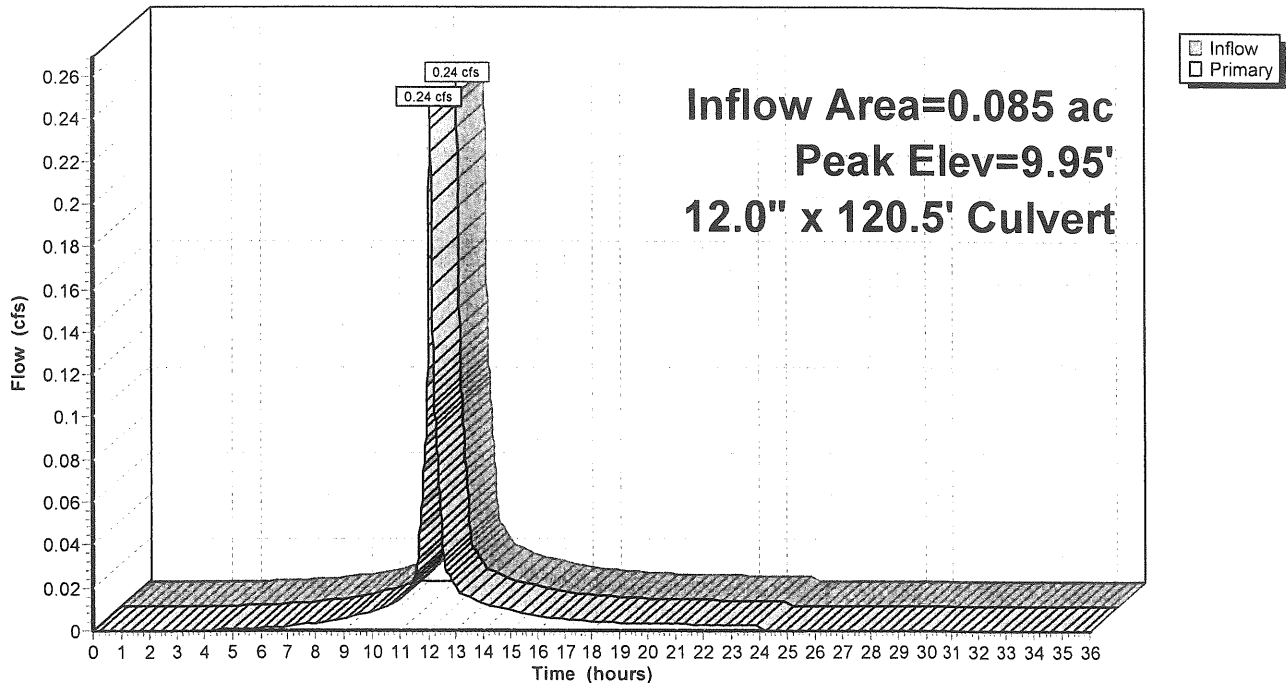
Device	Routing	Invert	Outlet Devices
#1	Primary	9.70'	12.0" x 120.5' long SD-3 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0050 '/' Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=0.24 cfs @ 12.07 hrs HW=9.95' (Free Discharge)

↳1=SD-3 (Barrel Controls 0.24 cfs @ 2.37 fps)

Pond CB2:

Hydrograph



Summary for Pond CB3: Ex. CB

Inflow Area = 0.610 ac, 100.00% Impervious, Inflow Depth = 2.77" for 2-Year event
 Inflow = 1.84 cfs @ 12.07 hrs, Volume= 0.141 af
 Outflow = 1.84 cfs @ 12.07 hrs, Volume= 0.141 af, Atten= 0%, Lag= 0.0 min
 Primary = 1.84 cfs @ 12.07 hrs, Volume= 0.141 af

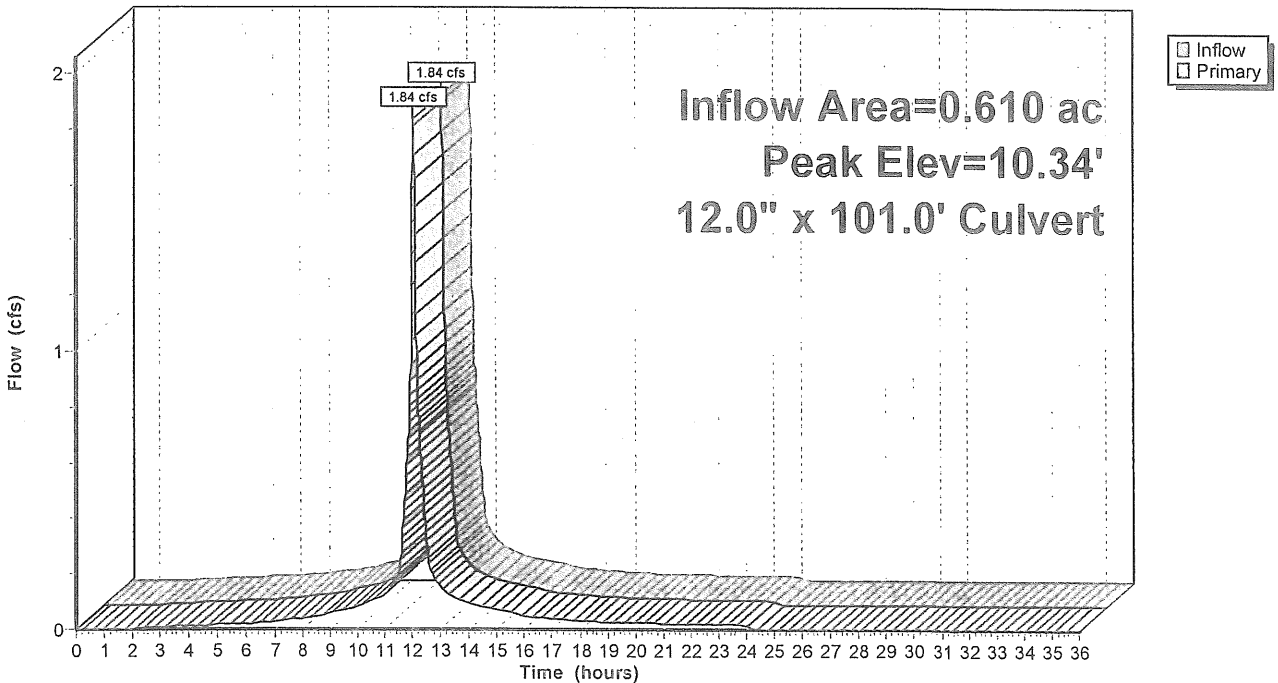
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3
 Peak Elev= 10.34' @ 12.07 hrs
 Flood Elev= 12.47'

Device	Routing	Invert	Outlet Devices
#1	Primary	9.56'	12.0" x 101.0' long SD-2 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0046 '/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=1.84 cfs @ 12.07 hrs HW=10.34' (Free Discharge)
 1=SD-2 (Barrel Controls 1.84 cfs @ 3.84 fps)

Pond CB3: Ex. CB

Hydrograph



Summary for Pond XCB: Ex. CB (on site)

Inflow Area = 1.650 ac, 93.21% Impervious, Inflow Depth = 2.72" for 2-Year event
 Inflow = 4.93 cfs @ 12.07 hrs, Volume= 0.374 af
 Outflow = 4.93 cfs @ 12.07 hrs, Volume= 0.374 af, Atten= 0%, Lag= 0.0 min
 Primary = 4.93 cfs @ 12.07 hrs, Volume= 0.374 af

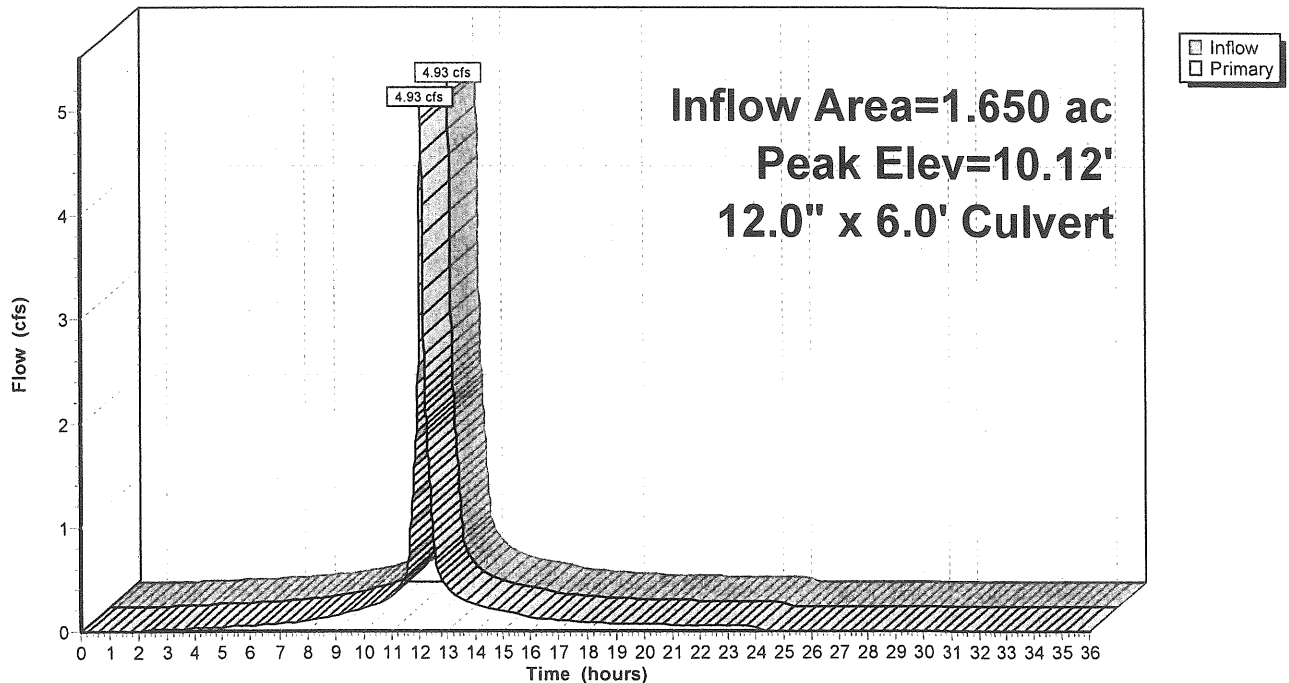
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3
 Peak Elev= 10.12' @ 12.07 hrs
 Flood Elev= 11.47'

Device	Routing	Invert	Outlet Devices
#1	Primary	8.47'	12.0" x 6.0' long Ex. SD RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.30' S= 0.0283 '/ Cc= 0.900 n= 0.011 Concrete pipe, straight & clean

Primary OutFlow Max=4.93 cfs @ 12.07 hrs HW=10.12' (Free Discharge)
 ←1=Ex. SD (Barrel Controls 4.93 cfs @ 6.27 fps)

Pond XCB: Ex. CB (on site)

Hydrograph



Time span=0.00-36.00 hrs, dt=0.01 hrs, 3601 points
Runoff by SCS TR-20 method, UH=SCS
Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment 1:	Runoff Area=0.616 ac 100.00% Impervious Runoff Depth=4.46" Tc=5.0 min CN=98 Runoff=2.93 cfs 0.229 af
Subcatchment 2:	Runoff Area=0.610 ac 100.00% Impervious Runoff Depth=4.46" Tc=5.0 min CN=98 Runoff=2.90 cfs 0.227 af
Subcatchment 3:	Runoff Area=0.458 ac 100.00% Impervious Runoff Depth=4.46" Tc=5.0 min CN=98 Runoff=2.18 cfs 0.170 af
Subcatchment 4:	Runoff Area=0.497 ac 94.57% Impervious Runoff Depth=4.35" Tc=5.0 min CN=97 Runoff=2.35 cfs 0.180 af
Subcatchment 5:	Runoff Area=0.301 ac 100.00% Impervious Runoff Depth=4.46" Tc=5.0 min CN=98 Runoff=1.43 cfs 0.112 af
Subcatchment 6:	Runoff Area=0.085 ac 0.00% Impervious Runoff Depth=4.12" Tc=5.0 min CN=95 Runoff=0.39 cfs 0.029 af
Reach SP1:	Inflow=2.93 cfs 0.229 af Outflow=2.93 cfs 0.229 af
Reach SP2: Ex. CB (off site)	Avg. Depth=1.00' Max Vel=5.38 fps Inflow=9.26 cfs 0.719 af D=12.0" n=0.011 L=84.0' S=0.0077 ' Capacity=3.70 cfs Outflow=3.93 cfs 0.719 af
Pond CB1:	Peak Elev=11.38' Inflow=5.48 cfs 0.426 af 12.0" x 71.5' Culvert Outflow=5.48 cfs 0.426 af
Pond CB2:	Peak Elev=10.02' Inflow=0.39 cfs 0.029 af 12.0" x 120.5' Culvert Outflow=0.39 cfs 0.029 af
Pond CB3: Ex. CB	Peak Elev=10.65' Inflow=2.90 cfs 0.227 af 12.0" x 101.0' Culvert Outflow=2.90 cfs 0.227 af
Pond XCB: Ex. CB (on site)	Peak Elev=11.71' Inflow=7.82 cfs 0.607 af 12.0" x 6.0' Culvert Outflow=7.82 cfs 0.607 af

Total Runoff Area = 2.567 ac Runoff Volume = 0.948 af Average Runoff Depth = 4.43"
4.36% Pervious = 0.112 ac 95.64% Impervious = 2.455 ac

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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

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Summary for Subcatchment 1:

Runoff = 2.93 cfs @ 12.07 hrs, Volume= 0.229 af, Depth= 4.46"

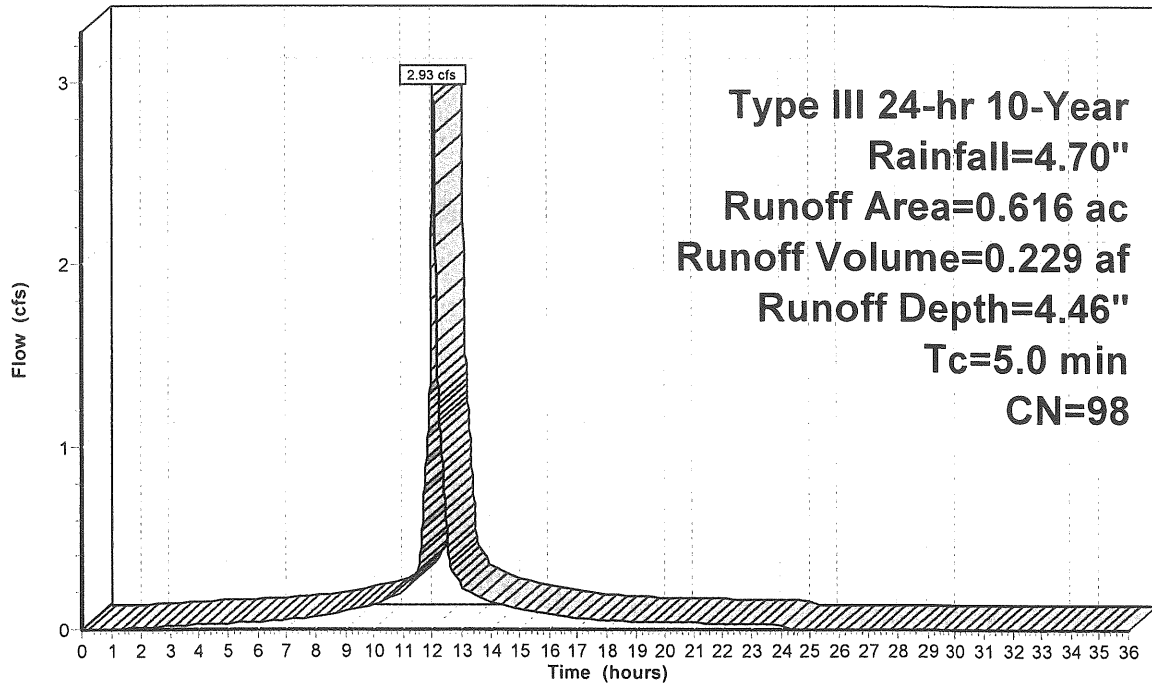
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.616	98	Paved parking & roofs
0.616		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 1:

Hydrograph



Runoff

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Summary for Subcatchment 2:

Runoff = 2.90 cfs @ 12.07 hrs, Volume= 0.227 af, Depth= 4.46"

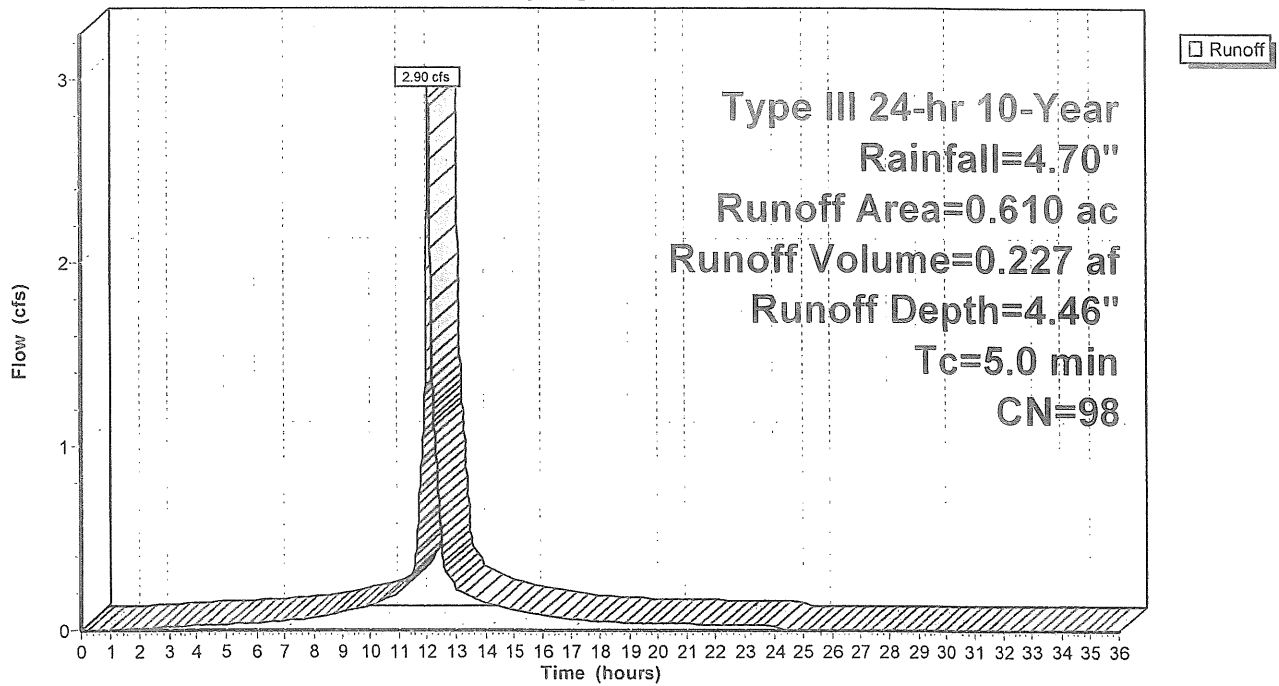
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.610	98	Paved parking & roofs
0.610		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 2:

Hydrograph



Summary for Subcatchment 3:

Runoff = 2.18 cfs @ 12.07 hrs, Volume= 0.170 af, Depth= 4.46"

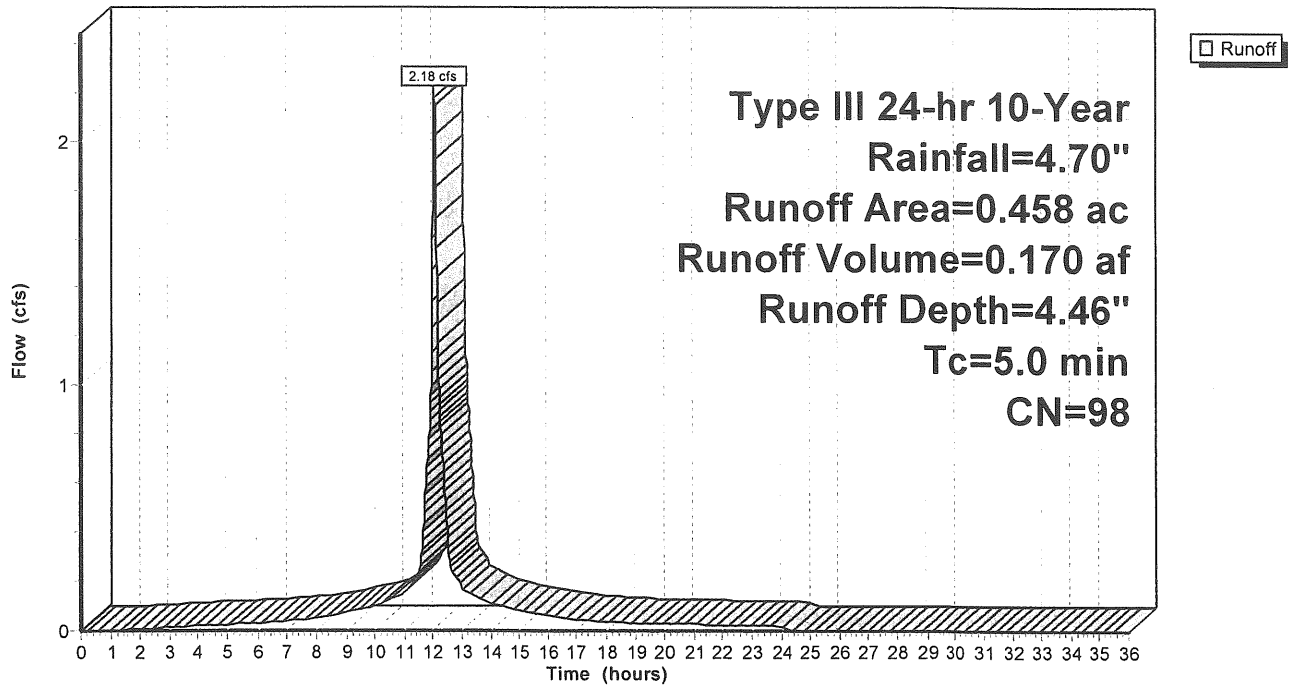
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.458	98	Paved parking & roofs
0.458		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 3:

Hydrograph



Summary for Subcatchment 4:

Runoff = 2.35 cfs @ 12.07 hrs, Volume= 0.180 af, Depth= 4.35"

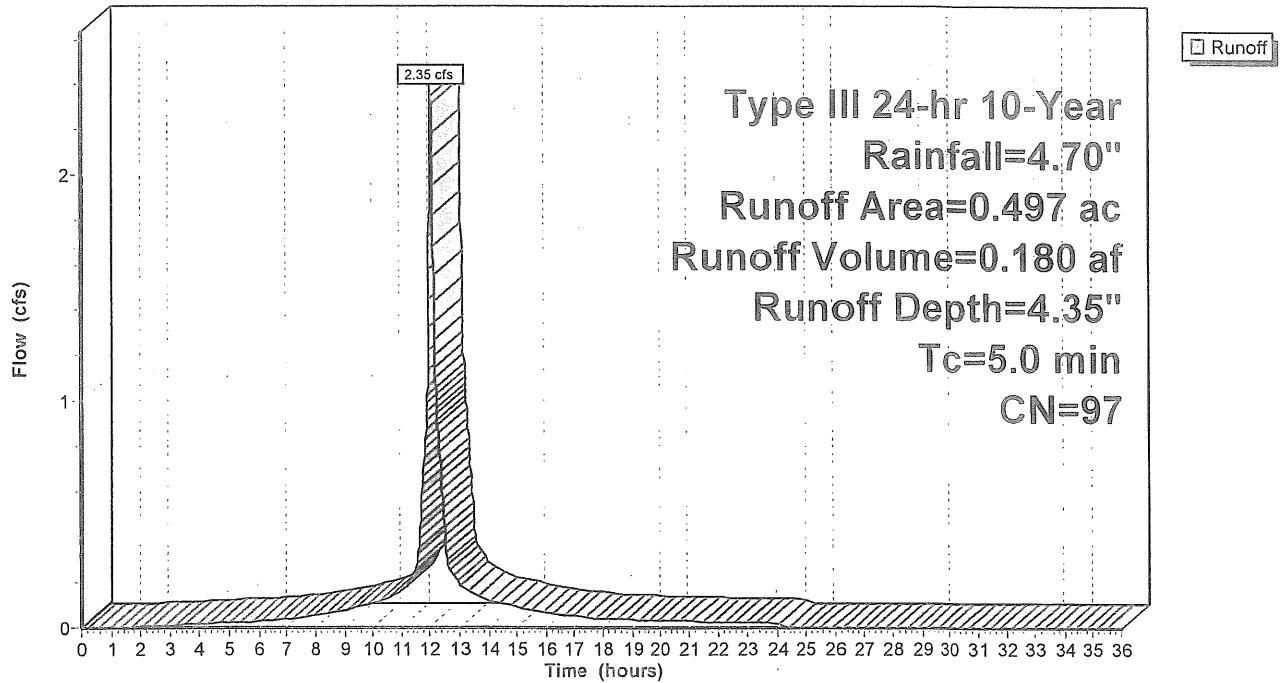
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.470	98	Paved parking & roofs
0.027	80	>75% Grass cover, Good, HSG D
0.497	97	Weighted Average
0.027		Pervious Area
0.470		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 4:

Hydrograph



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Type III 24-hr 10-Year Rainfall=4.70"

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Summary for Subcatchment 5:

Runoff = 1.43 cfs @ 12.07 hrs, Volume= 0.112 af, Depth= 4.46"

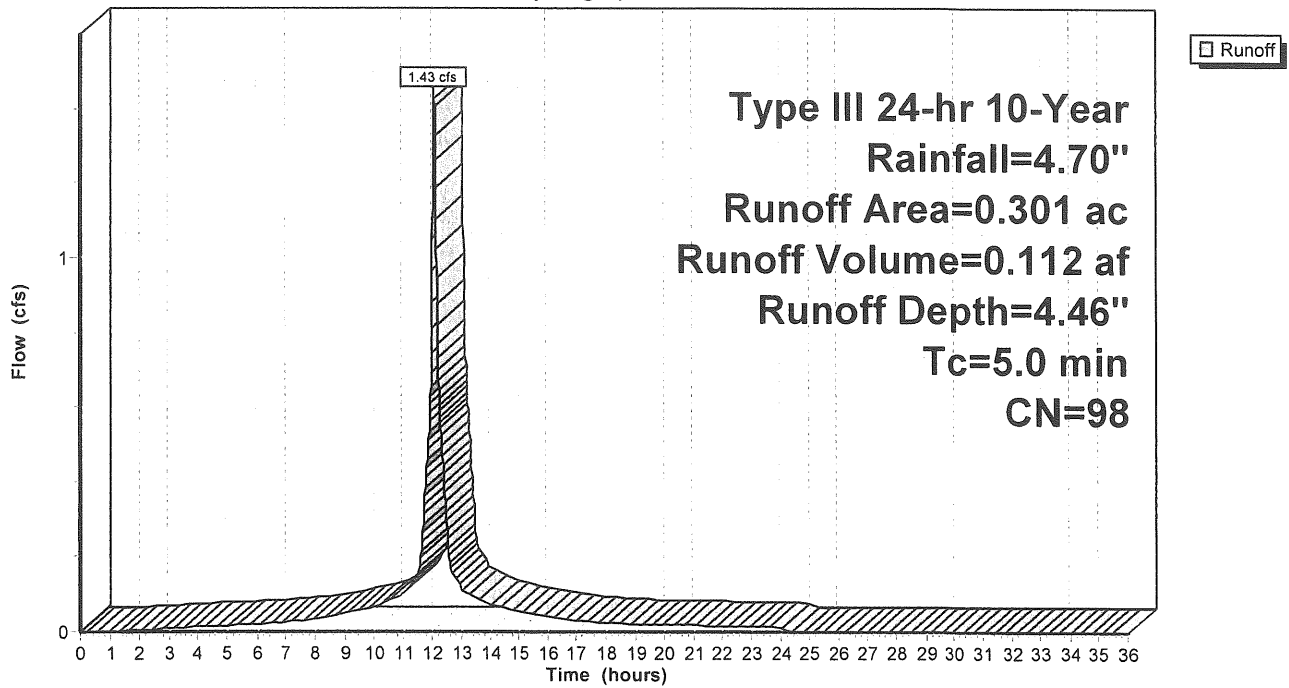
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.301	98	Paved parking & roofs
0.301		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 5:

Hydrograph



Summary for Subcatchment 6:

Runoff = 0.39 cfs @ 12.07 hrs, Volume= 0.029 af, Depth= 4.12"

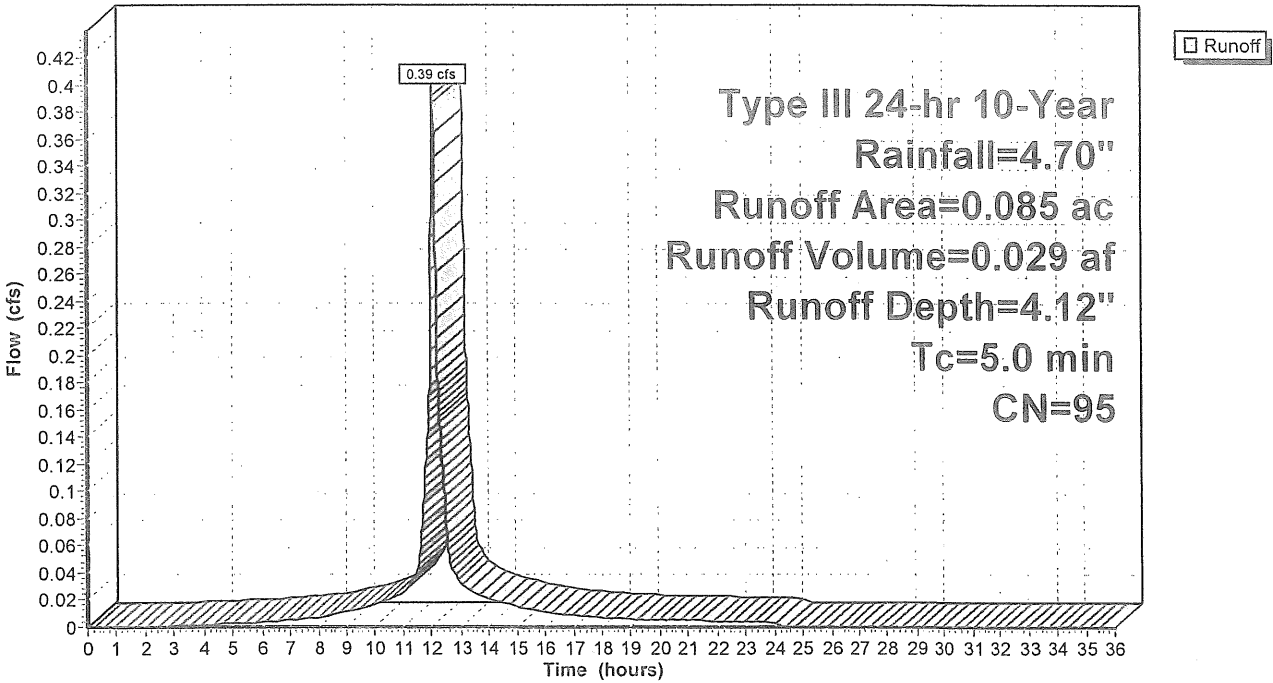
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
* 0.085	95	Gravel
0.085		Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 6:

Hydrograph



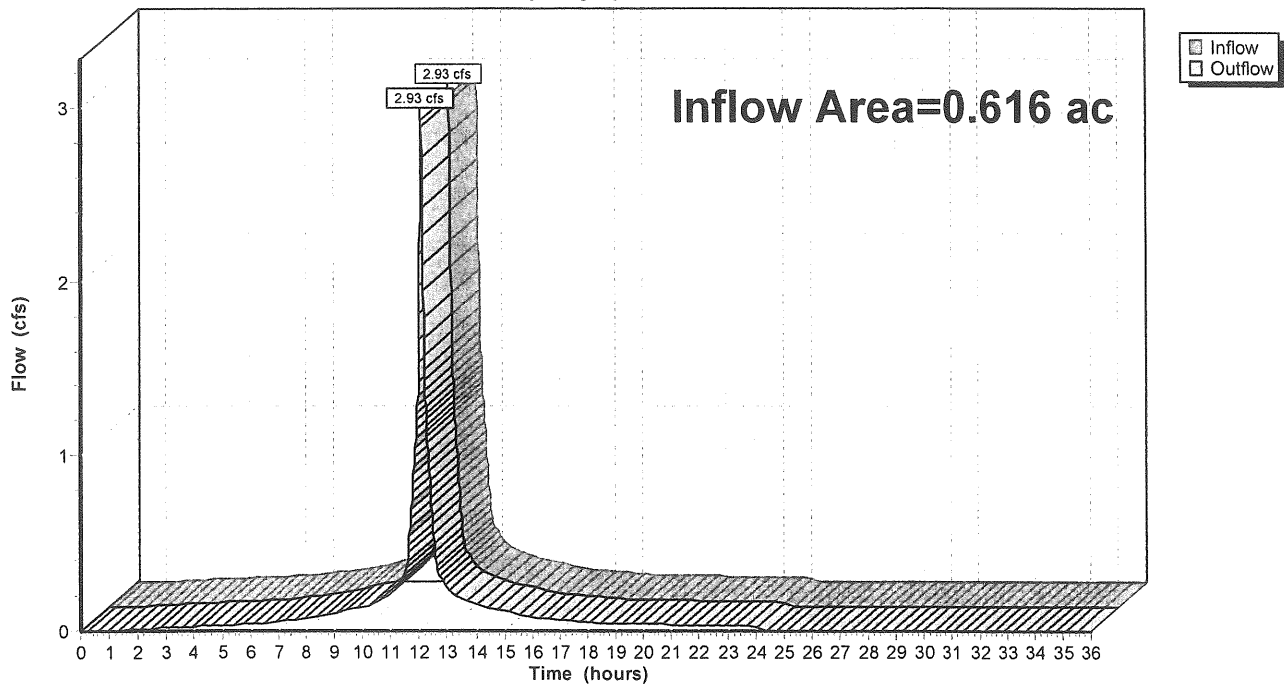
Summary for Reach SP1:

Inflow Area = 0.616 ac, 100.00% Impervious, Inflow Depth = 4.46" for 10-Year event
Inflow = 2.93 cfs @ 12.07 hrs, Volume= 0.229 af
Outflow = 2.93 cfs @ 12.07 hrs, Volume= 0.229 af, Atten= 0%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs

Reach SP1:

Hydrograph



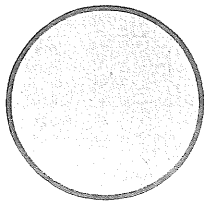
Summary for Reach SP2: Ex. CB (off site)

Inflow Area = 1.951 ac, 94.26% Impervious, Inflow Depth = 4.42" for 10-Year event
Inflow = 9.26 cfs @ 12.07 hrs, Volume= 0.719 af
Outflow = 3.93 cfs @ 11.94 hrs, Volume= 0.719 af, Atten= 57%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Max. Velocity= 5.38 fps, Min. Travel Time= 0.3 min
Avg. Velocity = 2.49 fps, Avg. Travel Time= 0.6 min

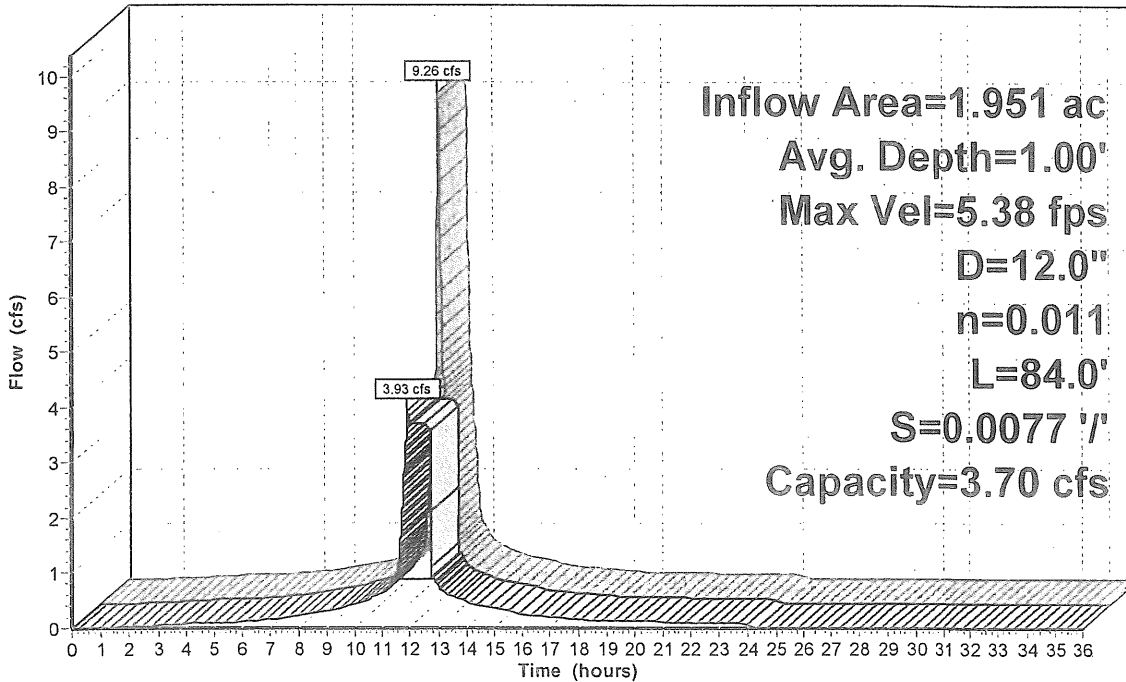
Peak Storage= 66 cf @ 11.95 hrs, Average Depth at Peak Storage= 1.00'
Bank-Full Depth= 1.00', Capacity at Bank-Full= 3.70 cfs

12.0" Diameter Pipe, n= 0.011 Concrete pipe, straight & clean
Length= 84.0' Slope= 0.0077 '/'
Inlet Invert= 8.00', Outlet Invert= 7.35'



Reach SP2: Ex. CB (off site)

Hydrograph



Legend:
Inflow (shaded)
Outflow (unshaded)

Summary for Pond CB2:

Inflow Area = 0.085 ac, 0.00% Impervious, Inflow Depth = 4.12" for 10-Year event
 Inflow = 0.39 cfs @ 12.07 hrs, Volume= 0.029 af
 Outflow = 0.39 cfs @ 12.07 hrs, Volume= 0.029 af, Atten= 0%, Lag= 0.0 min
 Primary = 0.39 cfs @ 12.07 hrs, Volume= 0.029 af

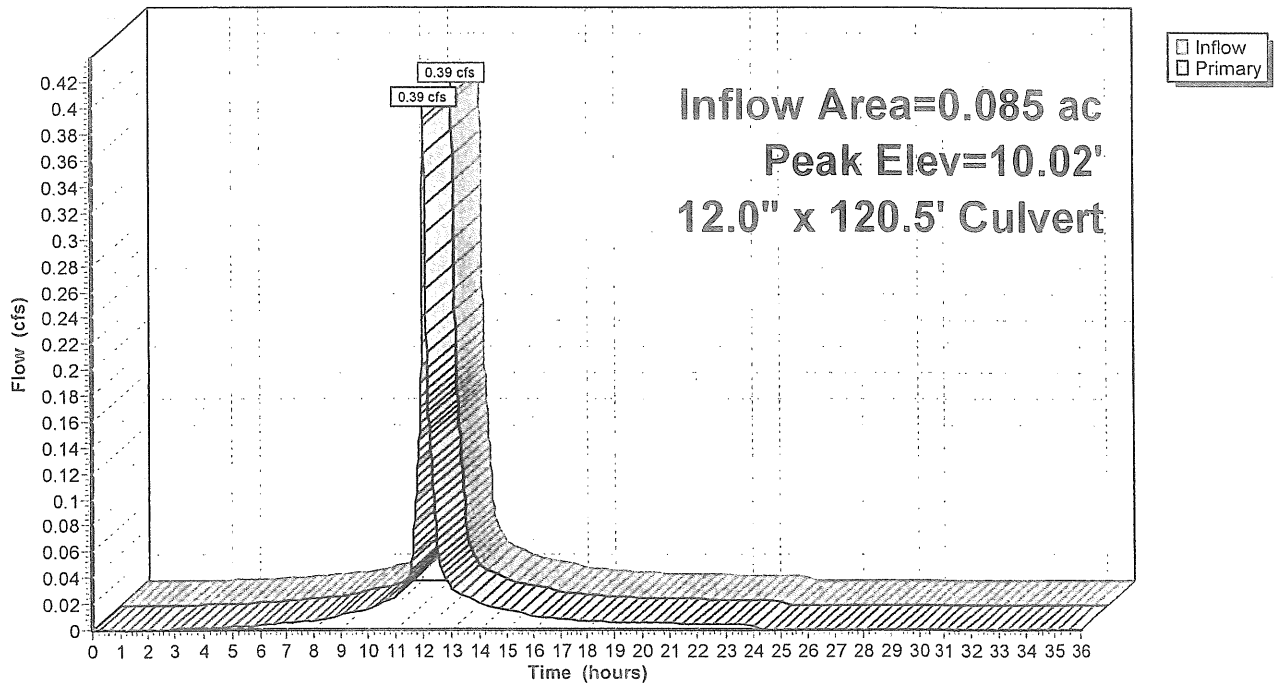
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Peak Elev= 10.02' @ 12.07 hrs
 Flood Elev= 13.25'

Device	Routing	Invert	Outlet Devices
#1	Primary	9.70'	12.0" x 120.5' long SD-3 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0050 '/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=0.39 cfs @ 12.07 hrs HW=10.02' (Free Discharge)
 ↳1=SD-3 (Barrel Controls 0.39 cfs @ 2.71 fps)

Pond CB2:

Hydrograph



Summary for Pond CB3: Ex. CB

Inflow Area = 0.610 ac, 100.00% Impervious, Inflow Depth = 4.46" for 10-Year event
 Inflow = 2.90 cfs @ 12.07 hrs, Volume= 0.227 af
 Outflow = 2.90 cfs @ 12.07 hrs, Volume= 0.227 af, Atten= 0%, Lag= 0.0 min
 Primary = 2.90 cfs @ 12.07 hrs, Volume= 0.227 af

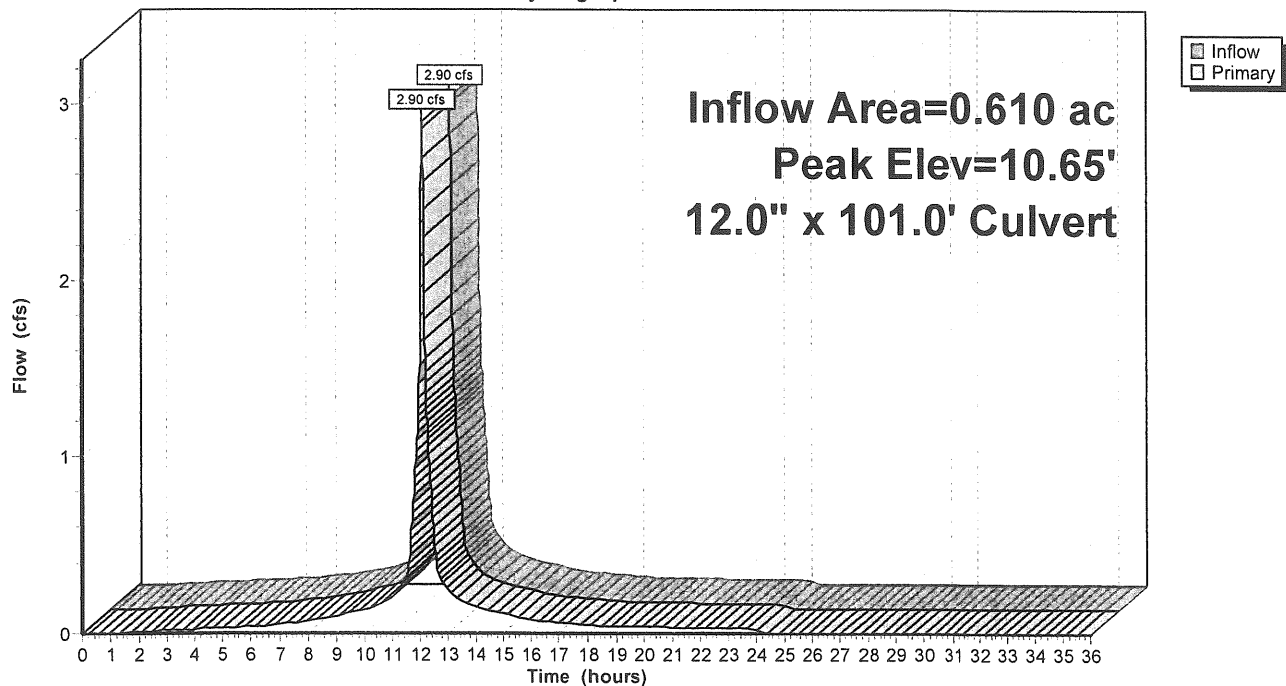
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3
 Peak Elev= 10.65' @ 12.07 hrs
 Flood Elev= 12.47'

Device	Routing	Invert	Outlet Devices
#1	Primary	9.56'	12.0" x 101.0' long SD-2 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0046 1/1' Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=2.90 cfs @ 12.07 hrs HW=10.65' (Free Discharge)
 ↳1=SD-2 (Barrel Controls 2.90 cfs @ 4.22 fps)

Pond CB3: Ex. CB

Hydrograph



Summary for Pond XCB: Ex. CB (on site)

Inflow Area = 1.650 ac, 93.21% Impervious, Inflow Depth = 4.41" for 10-Year event
 Inflow = 7.82 cfs @ 12.07 hrs, Volume= 0.607 af
 Outflow = 7.82 cfs @ 12.07 hrs, Volume= 0.607 af, Atten= 0%, Lag= 0.0 min
 Primary = 7.82 cfs @ 12.07 hrs, Volume= 0.607 af

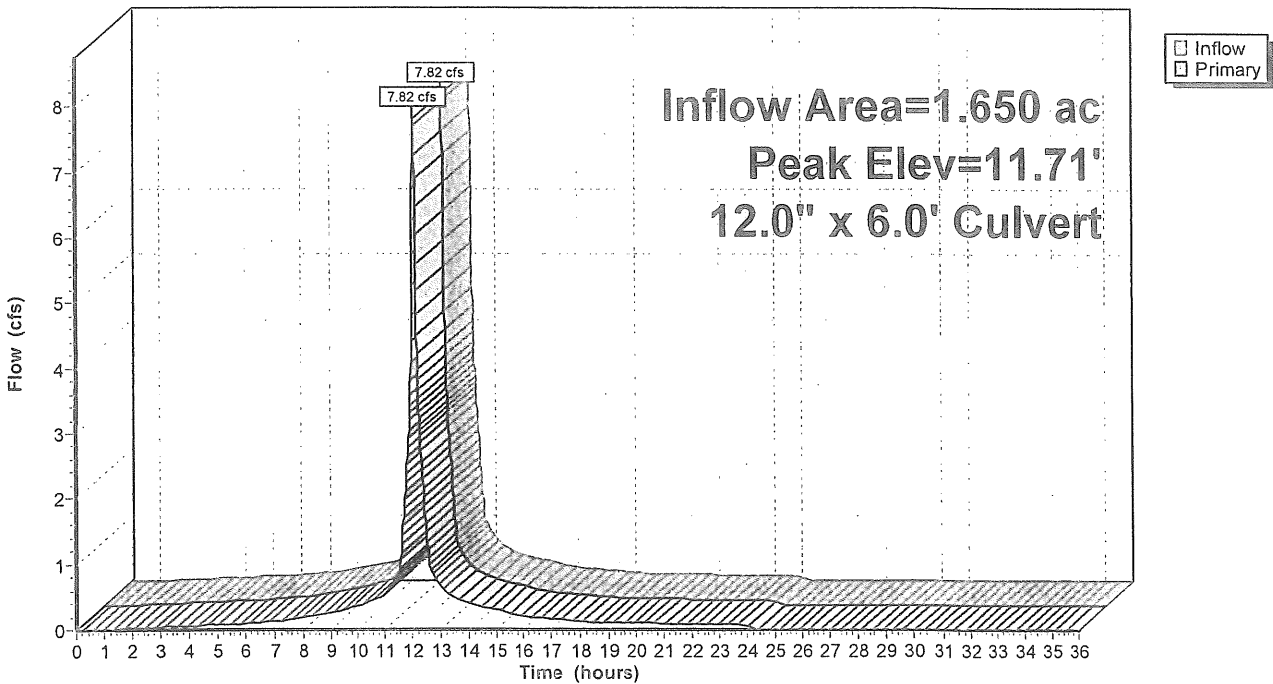
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3
 Peak Elev= 11.71' @ 12.07 hrs
 Flood Elev= 11.47'

Device	Routing	Invert	Outlet Devices
#1	Primary	8.47'	12.0" x 6.0' long Ex. SD RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.30' S= 0.0283 '/' Cc= 0.900 n= 0.011 Concrete pipe, straight & clean

Primary OutFlow Max=7.82 cfs @ 12.07 hrs HW=11.71' (Free Discharge)
 ↳1=Ex. SD (Inlet Controls 7.82 cfs @ 9.96 fps)

Pond XCB: Ex. CB (on site)

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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Time span=0.00-36.00 hrs, dt=0.01 hrs, 3601 points

Runoff by SCS TR-20 method, UH=SCS

Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment 1:	Runoff Area=0.616 ac 100.00% Impervious Runoff Depth=5.26" Tc=5.0 min CN=98 Runoff=3.44 cfs 0.270 af
Subcatchment 2:	Runoff Area=0.610 ac 100.00% Impervious Runoff Depth=5.26" Tc=5.0 min CN=98 Runoff=3.40 cfs 0.268 af
Subcatchment 3:	Runoff Area=0.458 ac 100.00% Impervious Runoff Depth=5.26" Tc=5.0 min CN=98 Runoff=2.56 cfs 0.201 af
Subcatchment 4:	Runoff Area=0.497 ac 94.57% Impervious Runoff Depth=5.15" Tc=5.0 min CN=97 Runoff=2.76 cfs 0.213 af
Subcatchment 5:	Runoff Area=0.301 ac 100.00% Impervious Runoff Depth=5.26" Tc=5.0 min CN=98 Runoff=1.68 cfs 0.132 af
Subcatchment 6:	Runoff Area=0.085 ac 0.00% Impervious Runoff Depth=4.92" Tc=5.0 min CN=95 Runoff=0.46 cfs 0.035 af
Reach SP1:	Inflow=3.44 cfs 0.270 af Outflow=3.44 cfs 0.270 af
Reach SP2: Ex. CB (off site)	Avg. Depth=1.00' Max Vel=5.38 fps Inflow=10.86 cfs 0.848 af D=12.0" n=0.011 L=84.0' S=0.0077 '/' Capacity=3.70 cfs Outflow=3.93 cfs 0.848 af
Pond CB1:	Peak Elev=12.10' Inflow=6.42 cfs 0.503 af 12.0" x 71.5' Culvert Outflow=6.42 cfs 0.503 af
Pond CB2:	Peak Elev=10.05' Inflow=0.46 cfs 0.035 af 12.0" x 120.5' Culvert Outflow=0.46 cfs 0.035 af
Pond CB3: Ex. CB	Peak Elev=11.00' Inflow=3.40 cfs 0.268 af 12.0" x 101.0' Culvert Outflow=3.40 cfs 0.268 af
Pond XCB: Ex. CB (on site)	Peak Elev=12.74' Inflow=9.18 cfs 0.716 af 12.0" x 6.0' Culvert Outflow=9.18 cfs 0.716 af

Total Runoff Area = 2.567 ac Runoff Volume = 1.118 af Average Runoff Depth = 5.23"
4.36% Pervious = 0.112 ac 95.64% Impervious = 2.455 ac

Summary for Subcatchment 1:

Runoff = 3.44 cfs @ 12.07 hrs, Volume= 0.270 af, Depth= 5.26"

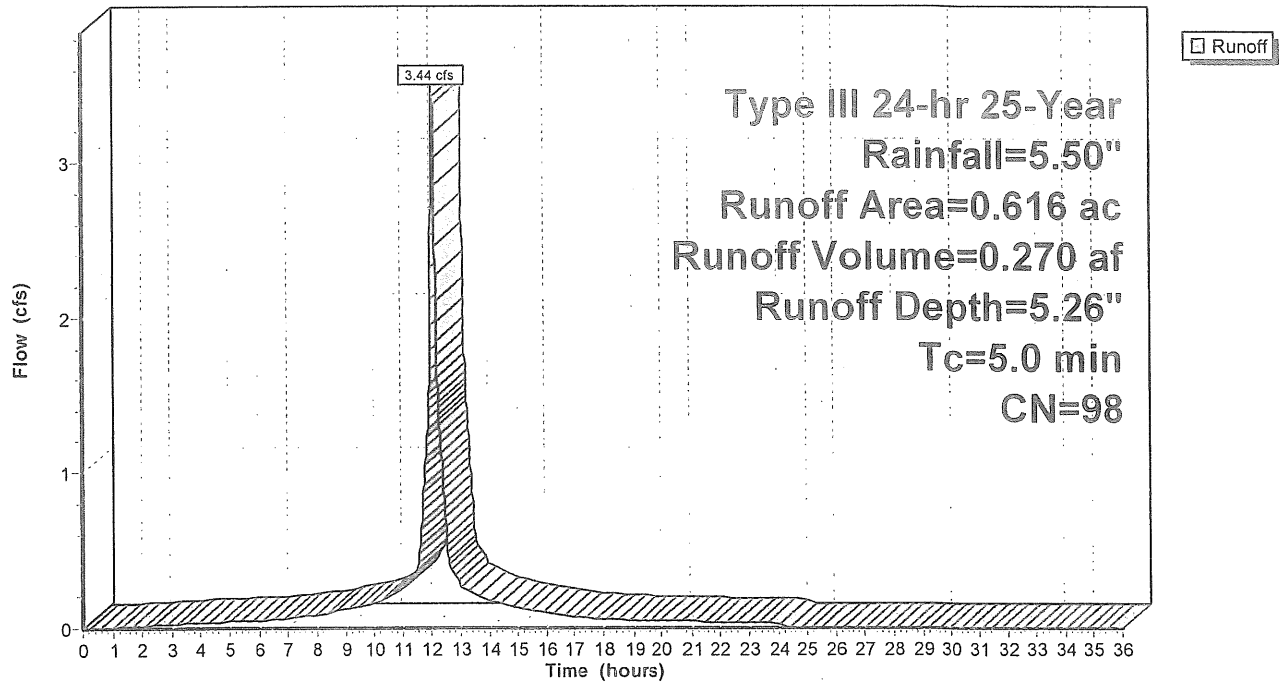
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.616	98	Paved parking & roofs
0.616		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 1:

Hydrograph



Summary for Subcatchment 2:

Runoff = 3.40 cfs @ 12.07 hrs, Volume= 0.268 af, Depth= 5.26"

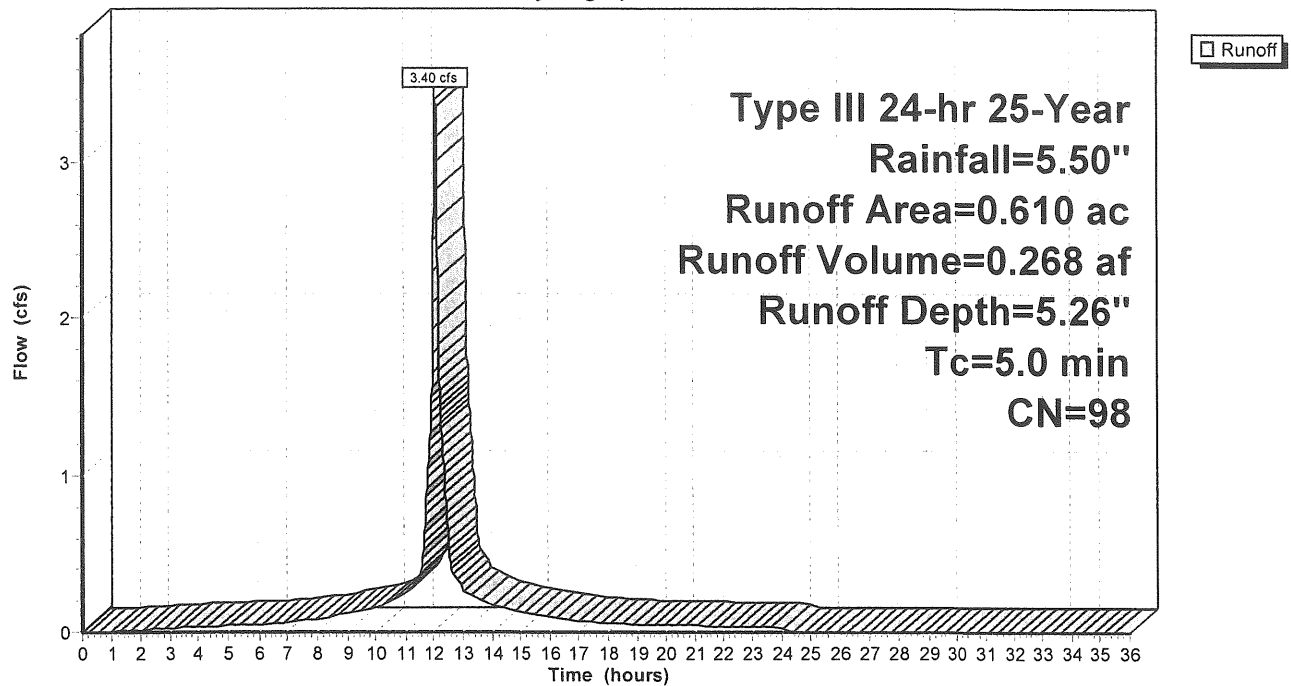
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.610	98	Paved parking & roofs
0.610		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 2:

Hydrograph



Summary for Subcatchment 3:

Runoff = 2.56 cfs @ 12.07 hrs, Volume= 0.201 af, Depth= 5.26"

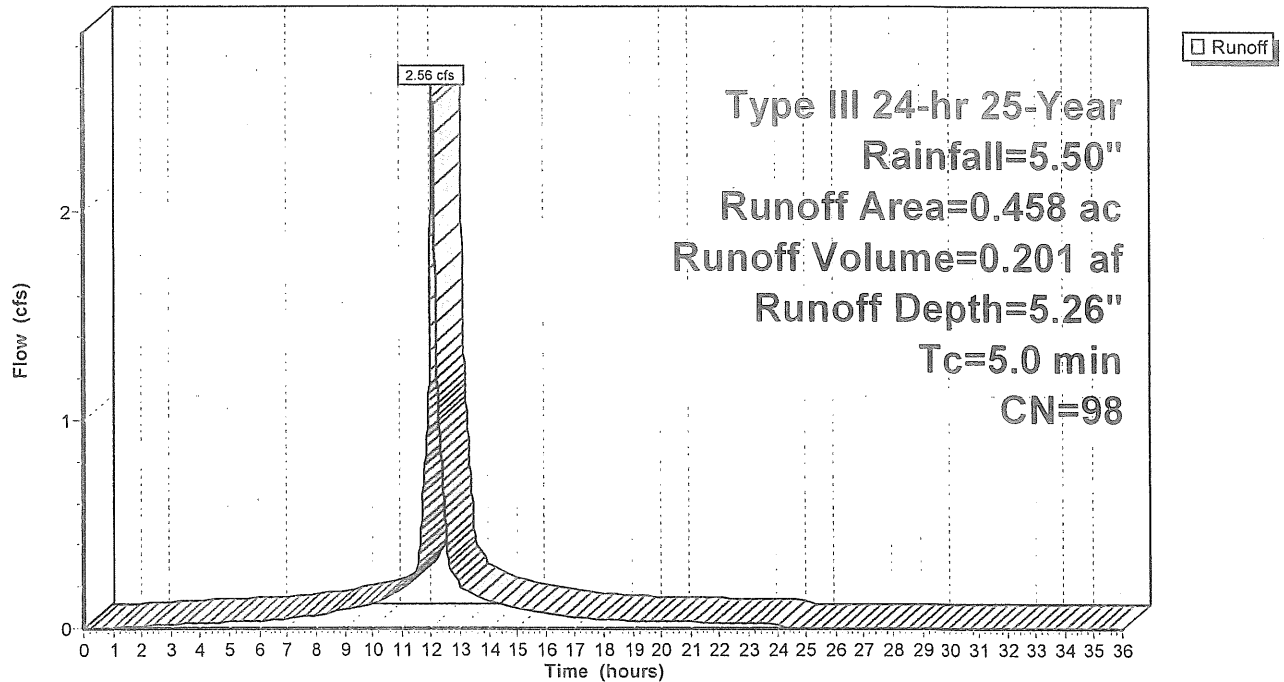
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.458	98	Paved parking & roofs
0.458		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 3:

Hydrograph



Summary for Subcatchment 4:

Runoff = 2.76 cfs @ 12.07 hrs, Volume= 0.213 af, Depth= 5.15"

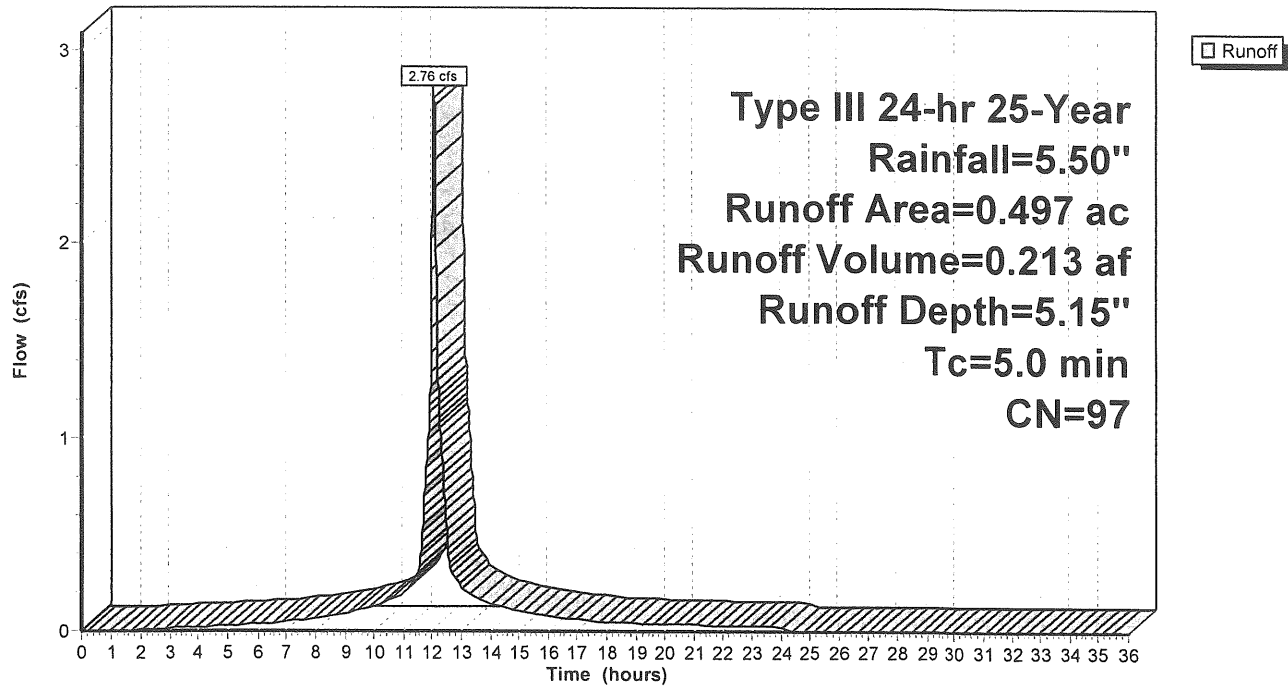
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.470	98	Paved parking & roofs
0.027	80	>75% Grass cover, Good, HSG D
0.497	97	Weighted Average
0.027		Pervious Area
0.470		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 4:

Hydrograph



Summary for Subcatchment 5:

Runoff = 1.68 cfs @ 12.07 hrs, Volume= 0.132 af, Depth= 5.26"

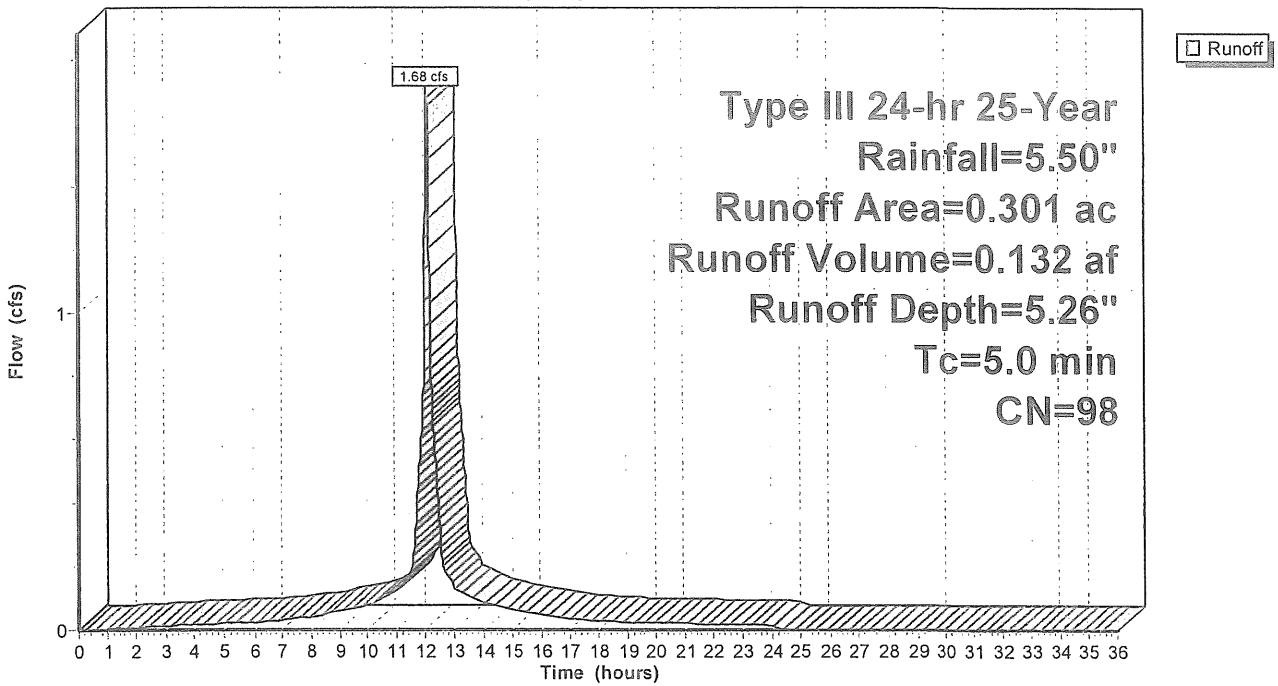
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.301	98	Paved parking & roofs
0.301		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 5:

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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Summary for Subcatchment 6:

Runoff = 0.46 cfs @ 12.07 hrs, Volume= 0.035 af, Depth= 4.92"

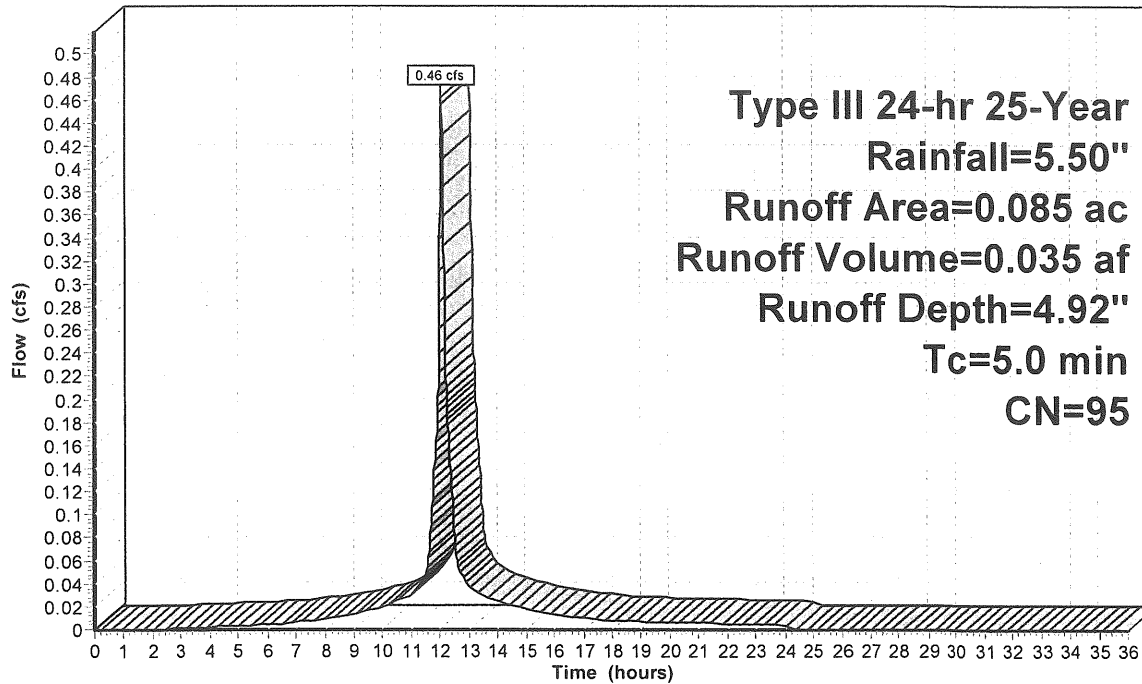
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
* 0.085	95	Gravel
0.085		Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 6:

Hydrograph



Runoff

**Type III 24-hr 25-Year
Rainfall=5.50"
Runoff Area=0.085 ac
Runoff Volume=0.035 af
Runoff Depth=4.92"
Tc=5.0 min
CN=95**

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Type III 24-hr 25-Year Rainfall=5.50"

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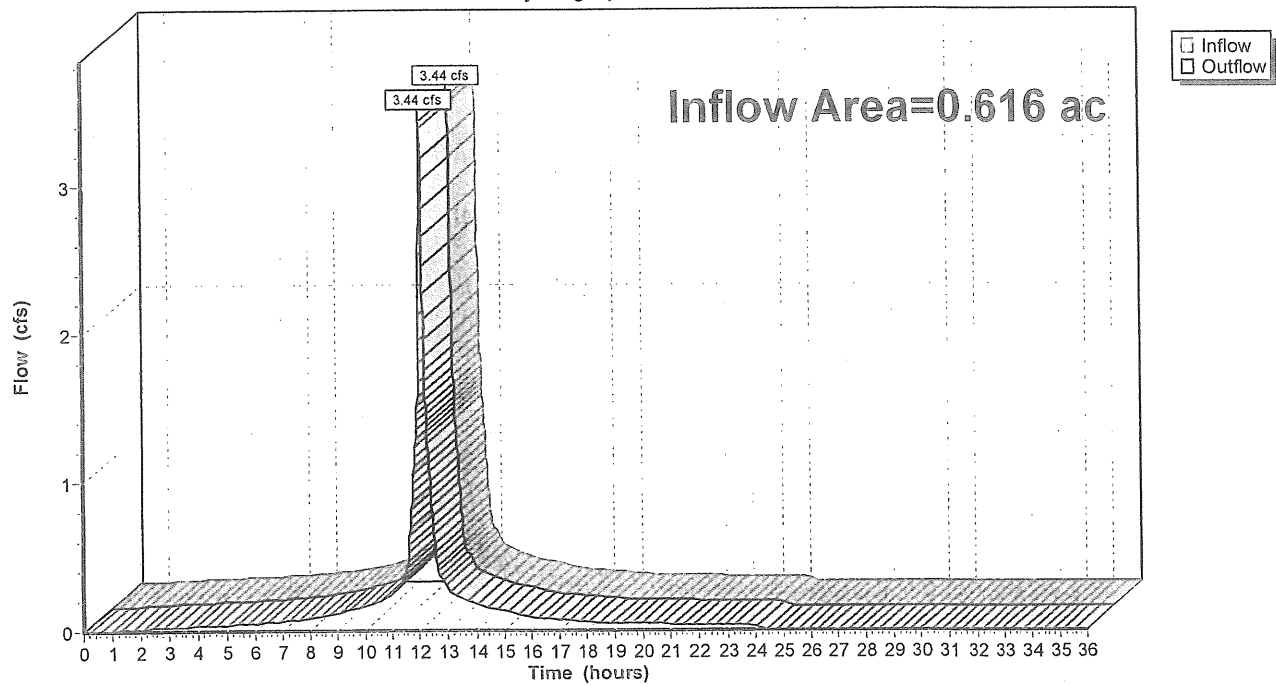
Summary for Reach SP1:

Inflow Area = 0.616 ac, 100.00% Impervious, Inflow Depth = 5.26" for 25-Year event
Inflow = 3.44 cfs @ 12.07 hrs, Volume= 0.270 af
Outflow = 3.44 cfs @ 12.07 hrs, Volume= 0.270 af, Atten= 0%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs

Reach SP1:

Hydrograph



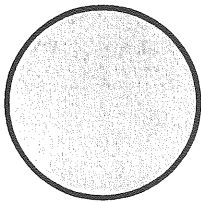
Summary for Reach SP2: Ex. CB (off site)

Inflow Area = 1.951 ac, 94.26% Impervious, Inflow Depth = 5.22" for 25-Year event
 Inflow = 10.86 cfs @ 12.07 hrs, Volume= 0.848 af
 Outflow = 3.93 cfs @ 11.87 hrs, Volume= 0.848 af, Atten= 64%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Max. Velocity= 5.38 fps, Min. Travel Time= 0.3 min
 Avg. Velocity = 2.61 fps, Avg. Travel Time= 0.5 min

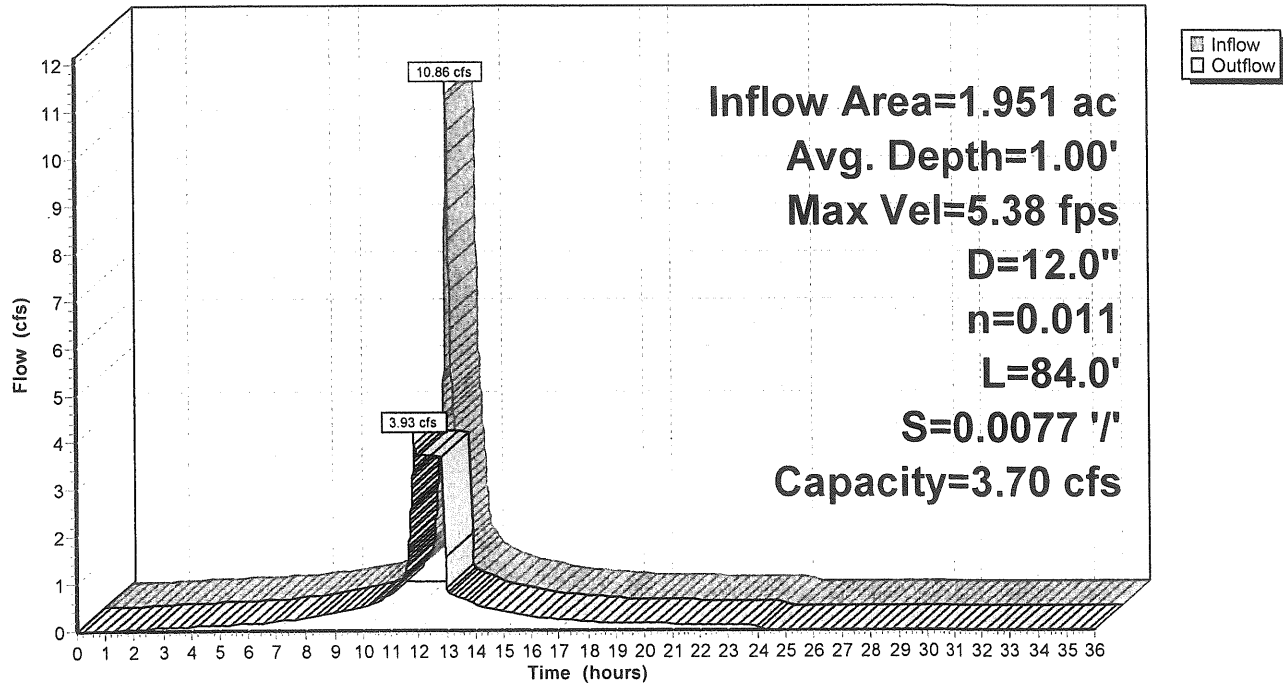
Peak Storage= 66 cf @ 11.88 hrs, Average Depth at Peak Storage= 1.00'
 Bank-Full Depth= 1.00', Capacity at Bank-Full= 3.70 cfs

12.0" Diameter Pipe, n= 0.011 Concrete pipe, straight & clean
 Length= 84.0' Slope= 0.0077 '/'
 Inlet Invert= 8.00', Outlet Invert= 7.35'



Reach SP2: Ex. CB (off site)

Hydrograph



Summary for Pond CB1:

Inflow Area = 1.153 ac, 92.63% Impervious, Inflow Depth = 5.24" for 25-Year event
 Inflow = 6.42 cfs @ 12.07 hrs, Volume= 0.503 af
 Outflow = 6.42 cfs @ 12.07 hrs, Volume= 0.503 af, Atten= 0%, Lag= 0.0 min
 Primary = 6.42 cfs @ 12.07 hrs, Volume= 0.503 af

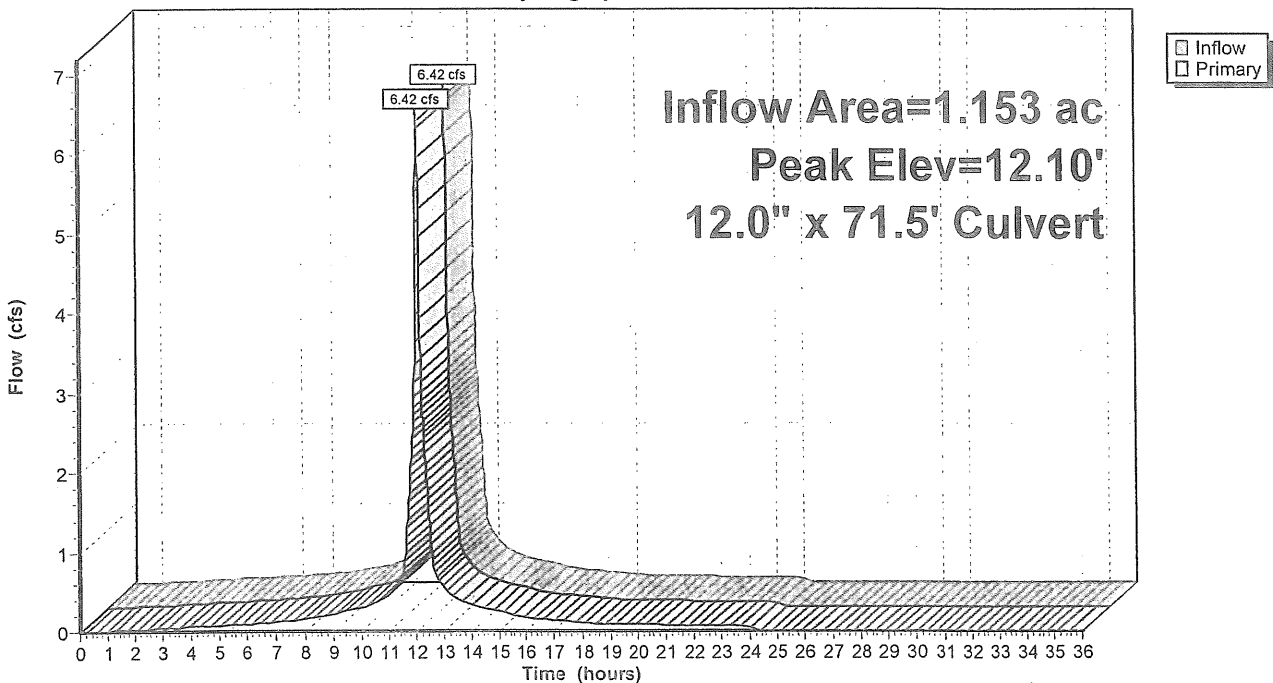
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3
 Peak Elev= 12.10' @ 12.07 hrs
 Flood Elev= 12.25'

Device	Routing	Invert	Outlet Devices
#1	Primary	9.00'	12.0" x 71.5' long SD-1 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.47' S= 0.0074 '/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=6.42 cfs @ 12.07 hrs HW=12.10' (Free Discharge)
 ↳1=SD-1 (Barrel Controls 6.42 cfs @ 8.18 fps)

Pond CB1:

Hydrograph



Summary for Pond CB2:

Inflow Area = 0.085 ac, 0.00% Impervious, Inflow Depth = 4.92" for 25-Year event
 Inflow = 0.46 cfs @ 12.07 hrs, Volume= 0.035 af
 Outflow = 0.46 cfs @ 12.07 hrs, Volume= 0.035 af, Atten= 0%, Lag= 0.0 min
 Primary = 0.46 cfs @ 12.07 hrs, Volume= 0.035 af

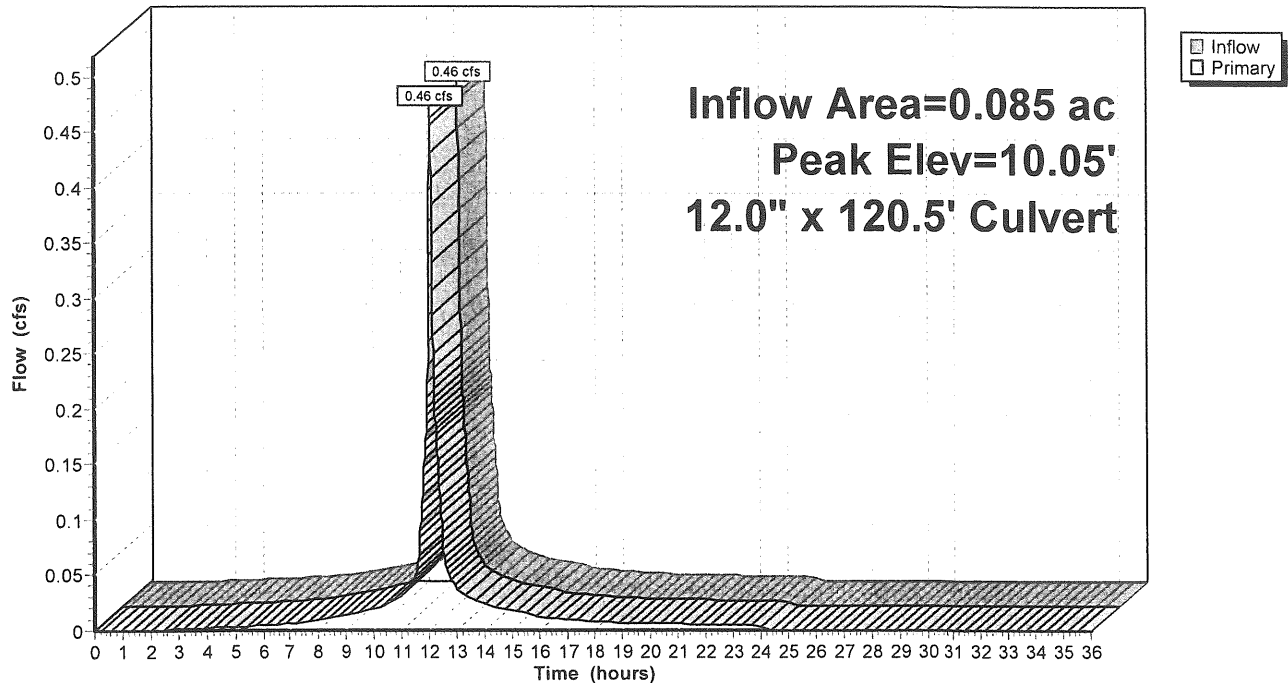
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Peak Elev= 10.05' @ 12.07 hrs
 Flood Elev= 13.25'

Device	Routing	Invert	Outlet Devices
#1	Primary	9.70'	12.0" x 120.5' long SD-3 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0050 '/' Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=0.46 cfs @ 12.07 hrs HW=10.05' (Free Discharge)
 ↳1=SD-3 (Barrel Controls 0.46 cfs @ 2.83 fps)

Pond CB2:

Hydrograph



Summary for Pond CB3: Ex. CB

Inflow Area = 0.610 ac, 100.00% Impervious, Inflow Depth = 5.26" for 25-Year event
 Inflow = 3.40 cfs @ 12.07 hrs, Volume= 0.268 af
 Outflow = 3.40 cfs @ 12.07 hrs, Volume= 0.268 af, Atten= 0%, Lag= 0.0 min
 Primary = 3.40 cfs @ 12.07 hrs, Volume= 0.268 af

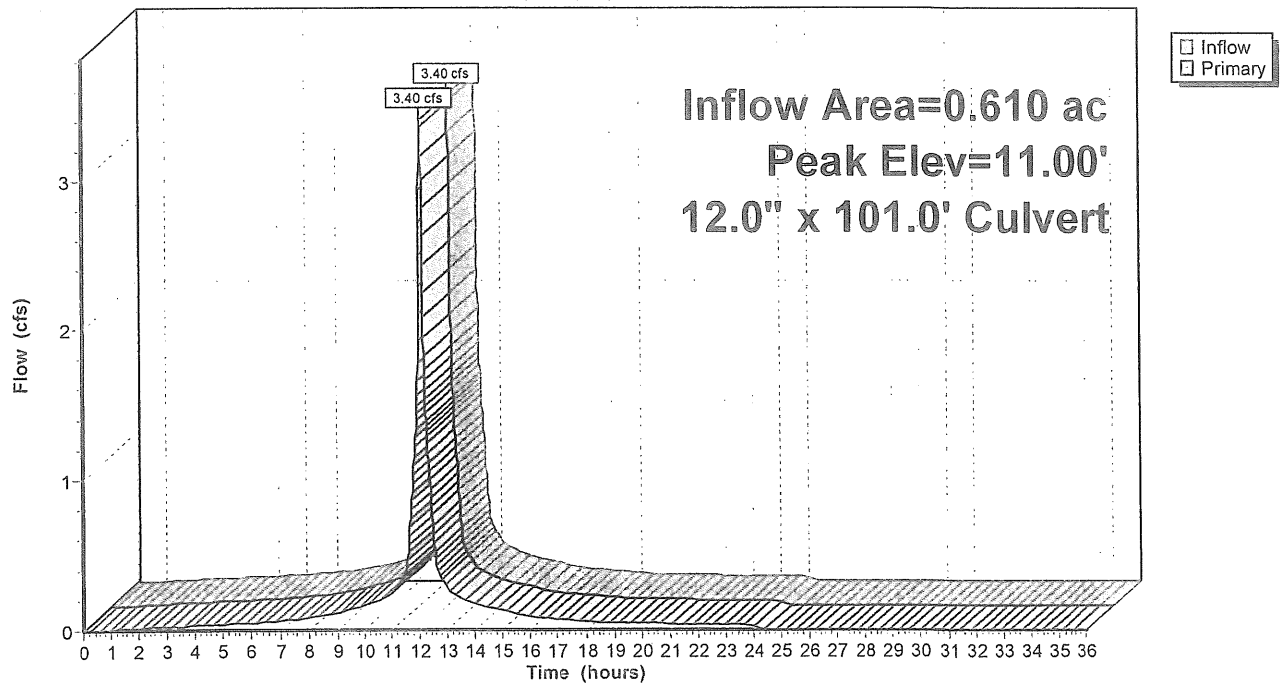
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3
 Peak Elev= 11.00' @ 12.07 hrs
 Flood Elev= 12.47'

Device	Routing	Invert	Outlet Devices
#1	Primary	9.56'	12.0" x 101.0' long SD-2 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0046 '/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=3.40 cfs @ 12.07 hrs HW=11.00' (Free Discharge)
 ↳ SD-2 (Barrel Controls 3.40 cfs @ 4.33 fps)

Pond CB3: Ex. CB

Hydrograph



Summary for Pond XCB: Ex. CB (on site)

Inflow Area = 1.650 ac, 93.21% Impervious, Inflow Depth = 5.21" for 25-Year event
 Inflow = 9.18 cfs @ 12.07 hrs, Volume= 0.716 af
 Outflow = 9.18 cfs @ 12.07 hrs, Volume= 0.716 af, Atten= 0%, Lag= 0.0 min
 Primary = 9.18 cfs @ 12.07 hrs, Volume= 0.716 af

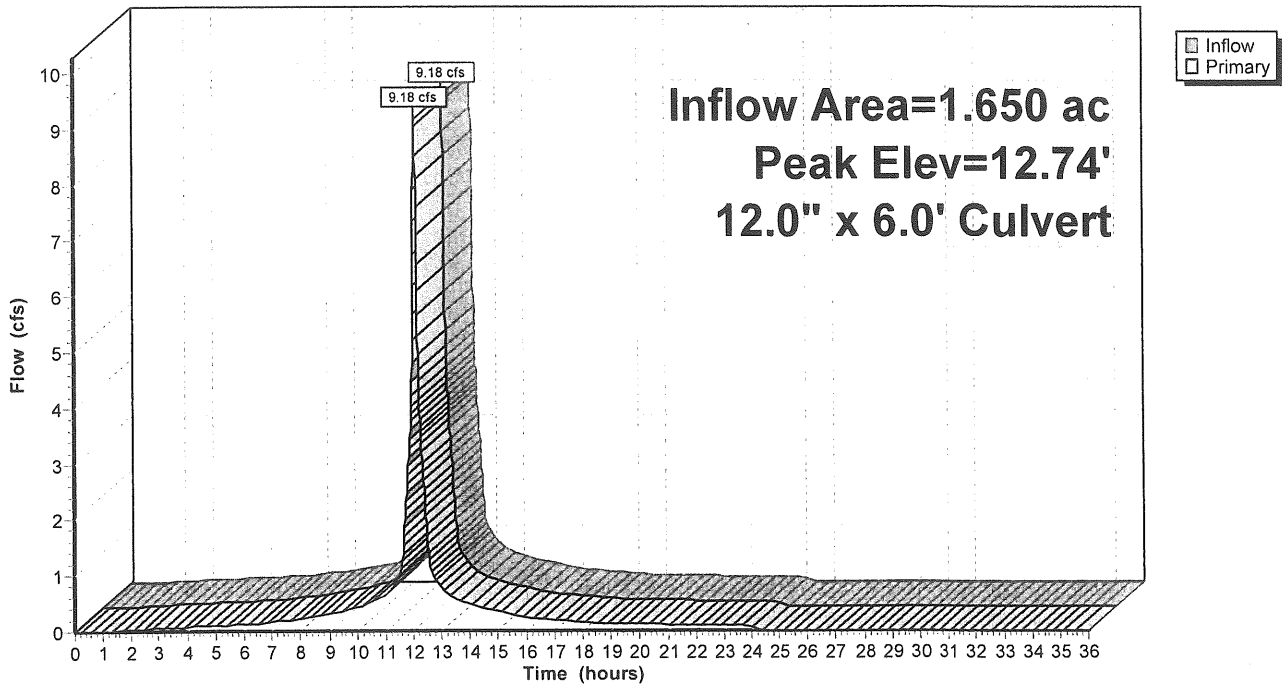
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3
 Peak Elev= 12.74' @ 12.07 hrs
 Flood Elev= 11.47'

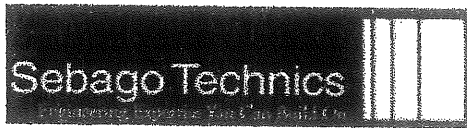
Device	Routing	Invert	Outlet Devices
#1	Primary	8.47'	12.0" x 6.0' long Ex. SD RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.30' S= 0.0283 '/ Cc= 0.900 n= 0.011 Concrete pipe, straight & clean

Primary OutFlow Max=9.18 cfs @ 12.07 hrs HW=12.74' (Free Discharge)
 ↑1=Ex. SD (Inlet Controls 9.18 cfs @ 11.69 fps)

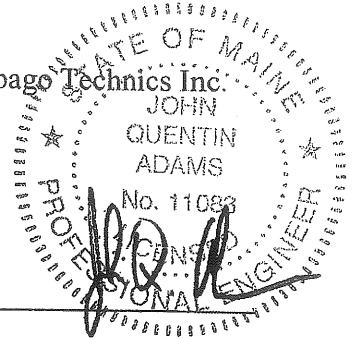
Pond XCB: Ex. CB (on site)

Hydrograph





Project: 07383
To: Molly P. Casto, Senior Planner, City of Portland
From: John Q. Adams, P.E., Senior Transportation Engineer, Sebago Technics Inc.
Date: November 5, 2008
Subject: Trip Generation Calculations
Proposed Rufus Deering Redevelopment
Commercial Street, Portland



The purpose of this memo is to establish the net new trip-ends that will be generated by the proposed Rufus Deering redevelopment project. It is important to note the primary reasons for the redevelopment, which are to modernize program space and increase the efficiency of how the limited available site space is utilized. The existing site is limited in usable space without any practical alternatives to increase the size by acquiring more property. The intent of this redevelopment is not to significantly increase site generated trips, rather, to remain a viable option in the highly competitive retail lumber industry. As such, there is no increase in current employment levels planned as part of this redevelopment. In fact, with the current economic climate there will likely be a slight reduction in current employment levels. A letter from Rufus Deering Lumber is enclosed which states their intention to not increase employment levels as part of the redevelopment.

With the primary reasons for the project as stated above, the difficulty is in accurately estimating the expected increase in trip generation associated with the redevelopment. The Institute of Transportation Engineer's (ITE) Trip Generation Manual, calculates trip generation for Building Materials and Lumber store, Land Use Code 812, by both building square footage and number of employees. A reasonable procedure to estimate net new trips would be to average the two ITE methods. Calculating by new building square footage would account for an increase in trips which could result from a modernized and more efficient site layout, while calculating trips by the number of employees would account for the fact that employees levels are not changing or may be reduced. As part of the redevelopment, existing warehouse space will be converted to 10,648 s.f. of retail store and showroom use. The new retail space was used to calculate new trips based on building square footage. There will be no new employees as part of this redevelopment.

Table 1, on Page 2, summarizes net new trip generation based on averaging the ITE two methods.

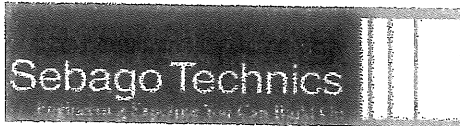


Table 1
Net New Trip Generation
Rufus Deering Lumber Redevelopment

Time Period	By Building SF			By Employees			Average
	Expansion SF	Rate	Subtotal	# New Employees.	Rate	Subtotal	
Weekday	10,648	45.16	481	0	32.12	0	240
AM Peak Site	10,648	4.16	44	0	3.94	0	22
PM Peak Site	10,648	5.56	59	0	3.83	0	30
AM Peak 7-9 am	10,648	2.60	28	0	2.42	0	14
PM Peak 4-6 pm	10,648	4.49	48	0	2.77	0	24
Saturday	10,648	51.60	549	0	36.69	0	275
Saturday Peak	10,648	9.58	102	0	5.23	0	51

As can be seen from the above Table 1, the redevelopment will not produce a significant amount of net new trips and will not need a MDOT Traffic Movement Permit (TMP). Projects that generate a minimum of 100 new trips in their peak hours are required to receive a MDOT TMP.

In summary the site will produce 14 and 24 new trips during the AM and PM peak hours of the adjacent roadways, respectively.

JQA:jqa/df

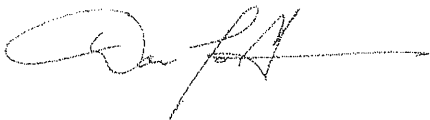
2008

November 7,

Molly P. Casto, Senior Planner, city of Portland

The primarily reasons for the redevelopment of the Rufus Deering Lumber Co. site is to improve efficiency and safety by the development of the proposed drive-thru lumber yard. It has become very difficult to compete and be profitable working out of this very antiquated yard. Our yearly losses due to cull lumber, boards that become weathered, split and unusable, is much higher than the national average. The only way to combat this is to become much more efficient and get all of our product undercover. With the newly designed yard this will reduce the number of employees and also improve the safety of our employees and customers.

Sincerely,

A handwritten signature in black ink, appearing to read "Dan LaBrie", with a long horizontal stroke extending to the right.

Dan LaBrie VP General Manager

Summary for Pond CB2:

Inflow Area = 0.085 ac, 0.00% Impervious, Inflow Depth = 4.12" for 10-Year event
 Inflow = 0.39 cfs @ 12.07 hrs, Volume= 0.029 af
 Outflow = 0.39 cfs @ 12.07 hrs, Volume= 0.029 af, Atten= 0%, Lag= 0.0 min
 Primary = 0.39 cfs @ 12.07 hrs, Volume= 0.029 af

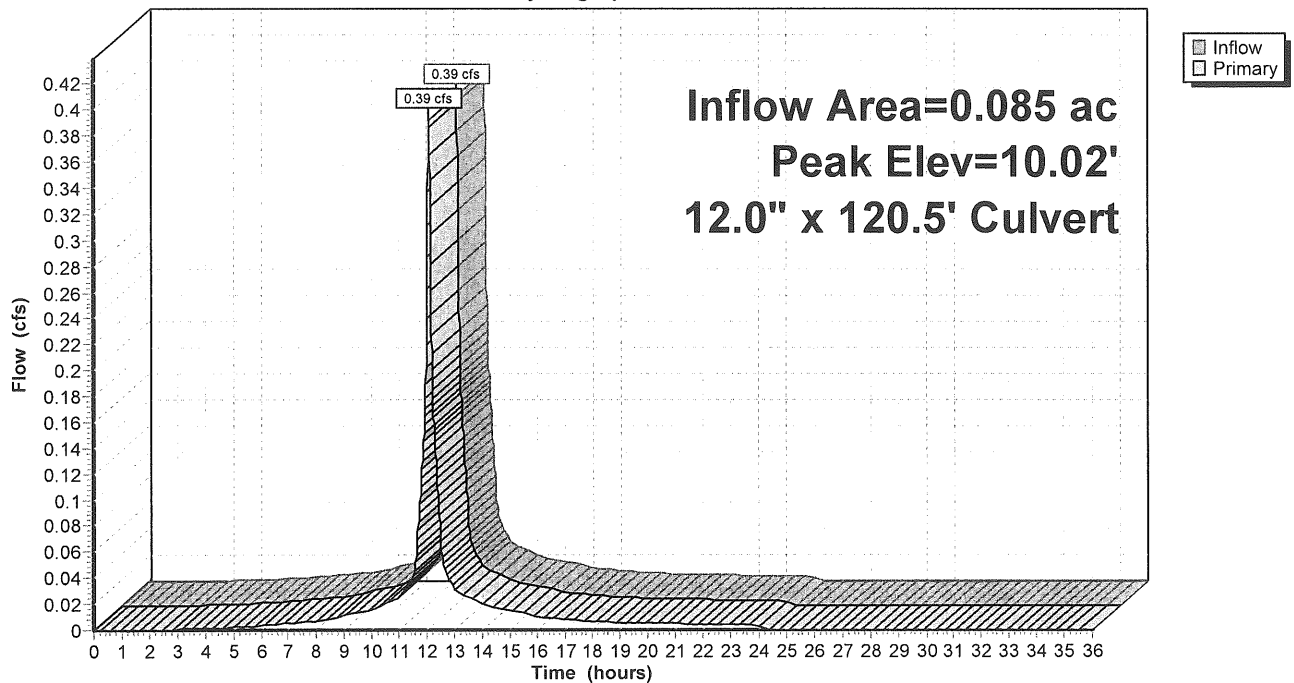
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
 Peak Elev= 10.02' @ 12.07 hrs
 Flood Elev= 13.25'

Device	Routing	Invert	Outlet Devices
#1	Primary	9.70'	12.0" x 120.5' long SD-3 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0050 '/' Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=0.39 cfs @ 12.07 hrs HW=10.02' (Free Discharge)
 ↳1=SD-3 (Barrel Controls 0.39 cfs @ 2.71 fps)

Pond CB2:

Hydrograph



Summary for Pond CB3: Ex. CB

Inflow Area = 0.610 ac, 100.00% Impervious, Inflow Depth = 4.46" for 10-Year event
 Inflow = 2.90 cfs @ 12.07 hrs, Volume= 0.227 af
 Outflow = 2.90 cfs @ 12.07 hrs, Volume= 0.227 af, Atten= 0%, Lag= 0.0 min
 Primary = 2.90 cfs @ 12.07 hrs, Volume= 0.227 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 10.65' @ 12.07 hrs

Flood Elev= 12.47'

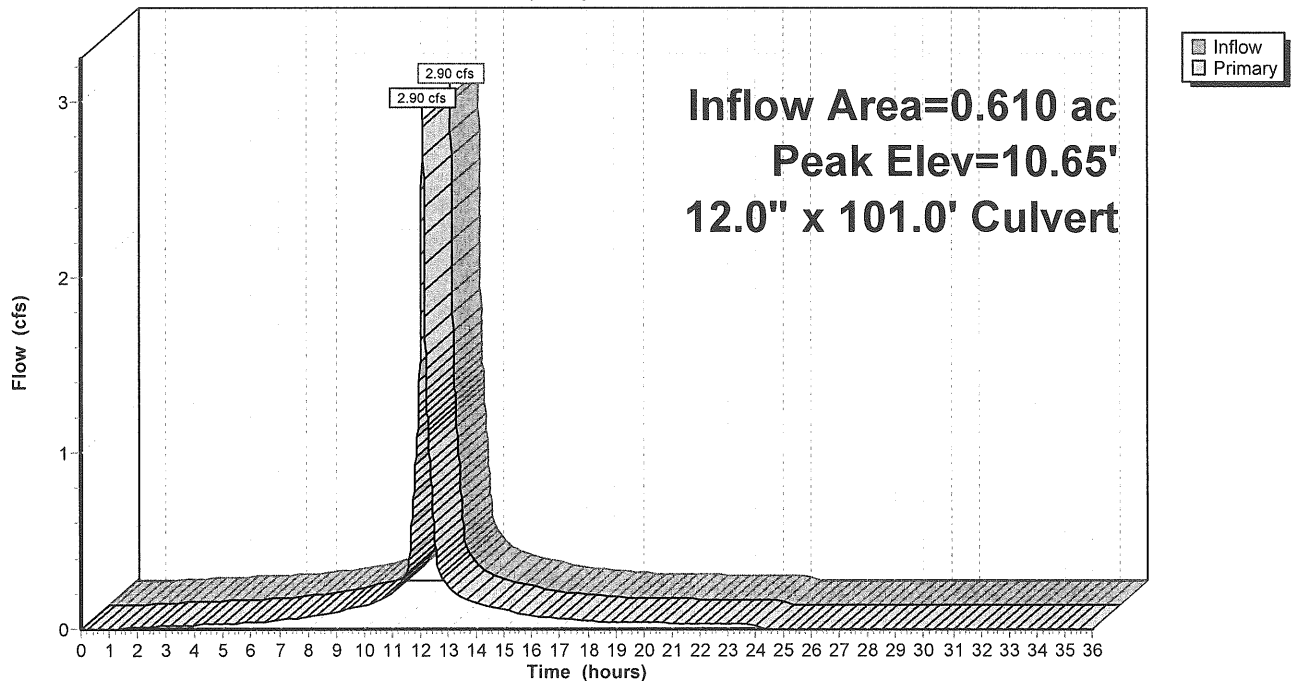
Device	Routing	Invert	Outlet Devices
#1	Primary	9.56'	12.0" x 101.0' long SD-2 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0046 '/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=2.90 cfs @ 12.07 hrs HW=10.65' (Free Discharge)

1=SD-2 (Barrel Controls 2.90 cfs @ 4.22 fps)

Pond CB3: Ex. CB

Hydrograph



Summary for Pond XCB: Ex. CB (on site)

Inflow Area = 1.650 ac, 93.21% Impervious, Inflow Depth = 4.41" for 10-Year event
 Inflow = 7.82 cfs @ 12.07 hrs, Volume= 0.607 af
 Outflow = 7.82 cfs @ 12.07 hrs, Volume= 0.607 af, Atten= 0%, Lag= 0.0 min
 Primary = 7.82 cfs @ 12.07 hrs, Volume= 0.607 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 11.71' @ 12.07 hrs

Flood Elev= 11.47'

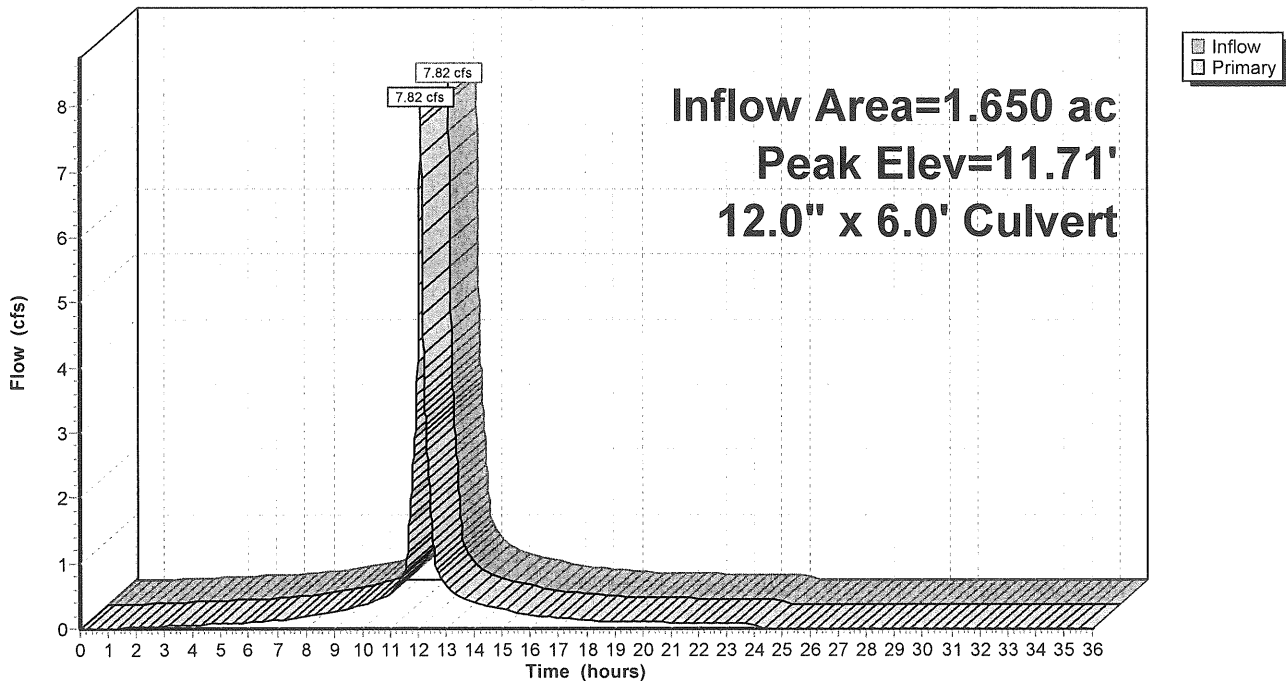
Device	Routing	Invert	Outlet Devices
#1	Primary	8.47'	12.0" x 6.0' long Ex. SD RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.30' S= 0.0283 '/ Cc= 0.900 n= 0.011 Concrete pipe, straight & clean

Primary OutFlow Max=7.82 cfs @ 12.07 hrs HW=11.71' (Free Discharge)

↑1=Ex. SD (Inlet Controls 7.82 cfs @ 9.96 fps)

Pond XCB: Ex. CB (on site)

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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Time span=0.00-36.00 hrs, dt=0.01 hrs, 3601 points

Runoff by SCS TR-20 method, UH=SCS

Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment 1:	Runoff Area=0.616 ac 100.00% Impervious Runoff Depth=5.26" Tc=5.0 min CN=98 Runoff=3.44 cfs 0.270 af
Subcatchment 2:	Runoff Area=0.610 ac 100.00% Impervious Runoff Depth=5.26" Tc=5.0 min CN=98 Runoff=3.40 cfs 0.268 af
Subcatchment 3:	Runoff Area=0.458 ac 100.00% Impervious Runoff Depth=5.26" Tc=5.0 min CN=98 Runoff=2.56 cfs 0.201 af
Subcatchment 4:	Runoff Area=0.497 ac 94.57% Impervious Runoff Depth=5.15" Tc=5.0 min CN=97 Runoff=2.76 cfs 0.213 af
Subcatchment 5:	Runoff Area=0.301 ac 100.00% Impervious Runoff Depth=5.26" Tc=5.0 min CN=98 Runoff=1.68 cfs 0.132 af
Subcatchment 6:	Runoff Area=0.085 ac 0.00% Impervious Runoff Depth=4.92" Tc=5.0 min CN=95 Runoff=0.46 cfs 0.035 af
Reach SP1:	Inflow=3.44 cfs 0.270 af Outflow=3.44 cfs 0.270 af
Reach SP2: Ex. CB (off site)	Avg. Depth=1.00' Max Vel=5.38 fps Inflow=10.86 cfs 0.848 af D=12.0" n=0.011 L=84.0' S=0.0077 '/' Capacity=3.70 cfs Outflow=3.93 cfs 0.848 af
Pond CB1:	Peak Elev=12.10' Inflow=6.42 cfs 0.503 af 12.0" x 71.5' Culvert Outflow=6.42 cfs 0.503 af
Pond CB2:	Peak Elev=10.05' Inflow=0.46 cfs 0.035 af 12.0" x 120.5' Culvert Outflow=0.46 cfs 0.035 af
Pond CB3: Ex. CB	Peak Elev=11.00' Inflow=3.40 cfs 0.268 af 12.0" x 101.0' Culvert Outflow=3.40 cfs 0.268 af
Pond XCB: Ex. CB (on site)	Peak Elev=12.74' Inflow=9.18 cfs 0.716 af 12.0" x 6.0' Culvert Outflow=9.18 cfs 0.716 af

Total Runoff Area = 2.567 ac Runoff Volume = 1.118 af Average Runoff Depth = 5.23"
4.36% Pervious = 0.112 ac 95.64% Impervious = 2.455 ac

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Type III 24-hr 25-Year Rainfall=5.50"

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Summary for Subcatchment 1:

Runoff = 3.44 cfs @ 12.07 hrs, Volume= 0.270 af, Depth= 5.26"

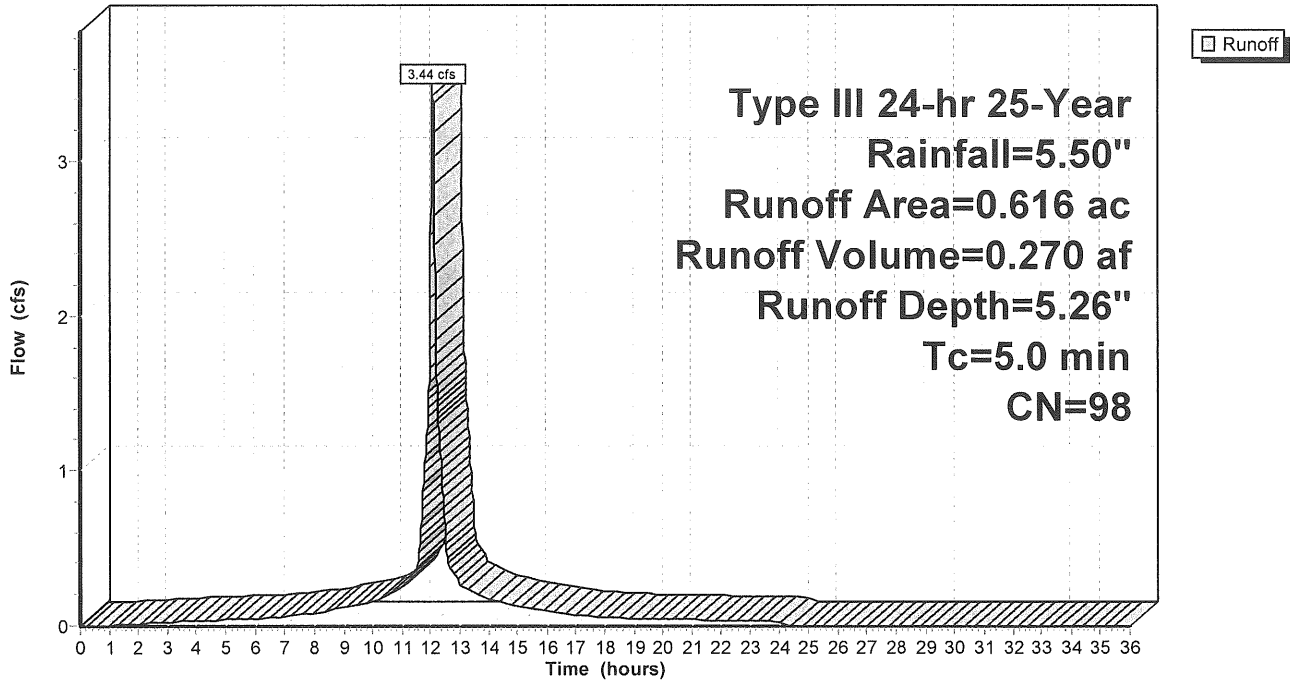
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.616	98	Paved parking & roofs
0.616		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 1:

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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Summary for Subcatchment 2:

Runoff = 3.40 cfs @ 12.07 hrs, Volume= 0.268 af, Depth= 5.26"

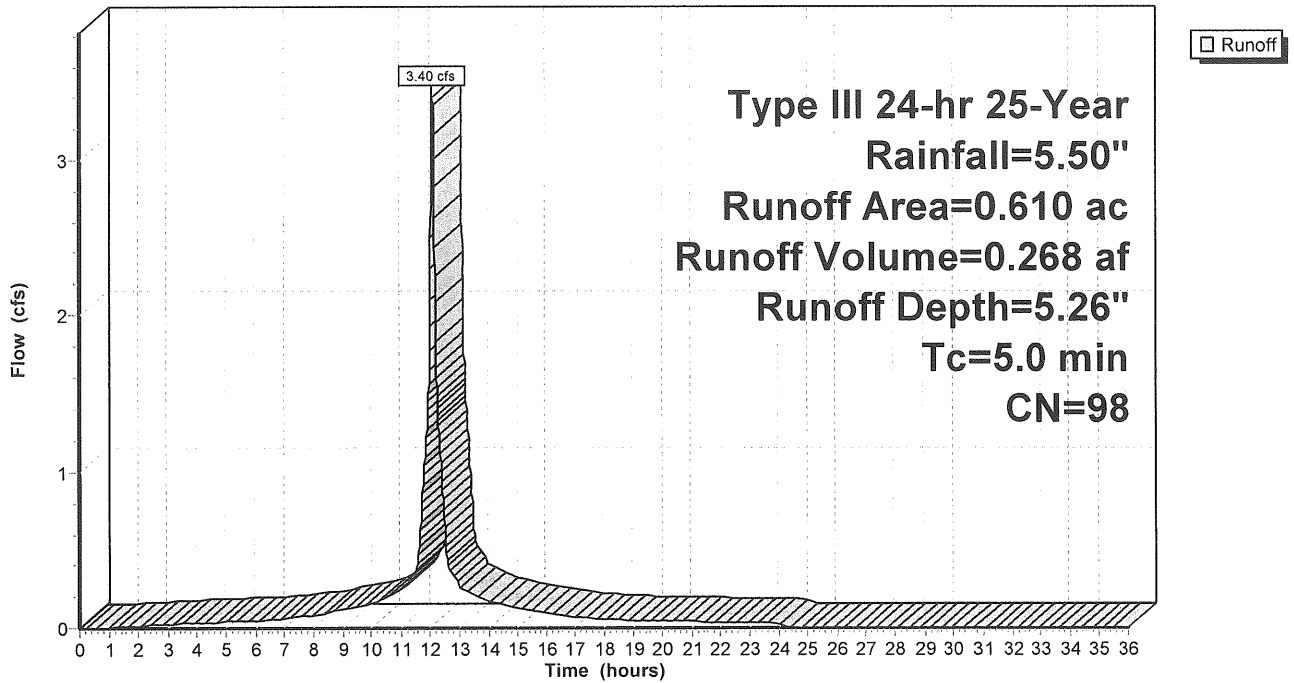
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.610	98	Paved parking & roofs
0.610		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 2:

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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Summary for Subcatchment 3:

Runoff = 2.56 cfs @ 12.07 hrs, Volume= 0.201 af, Depth= 5.26"

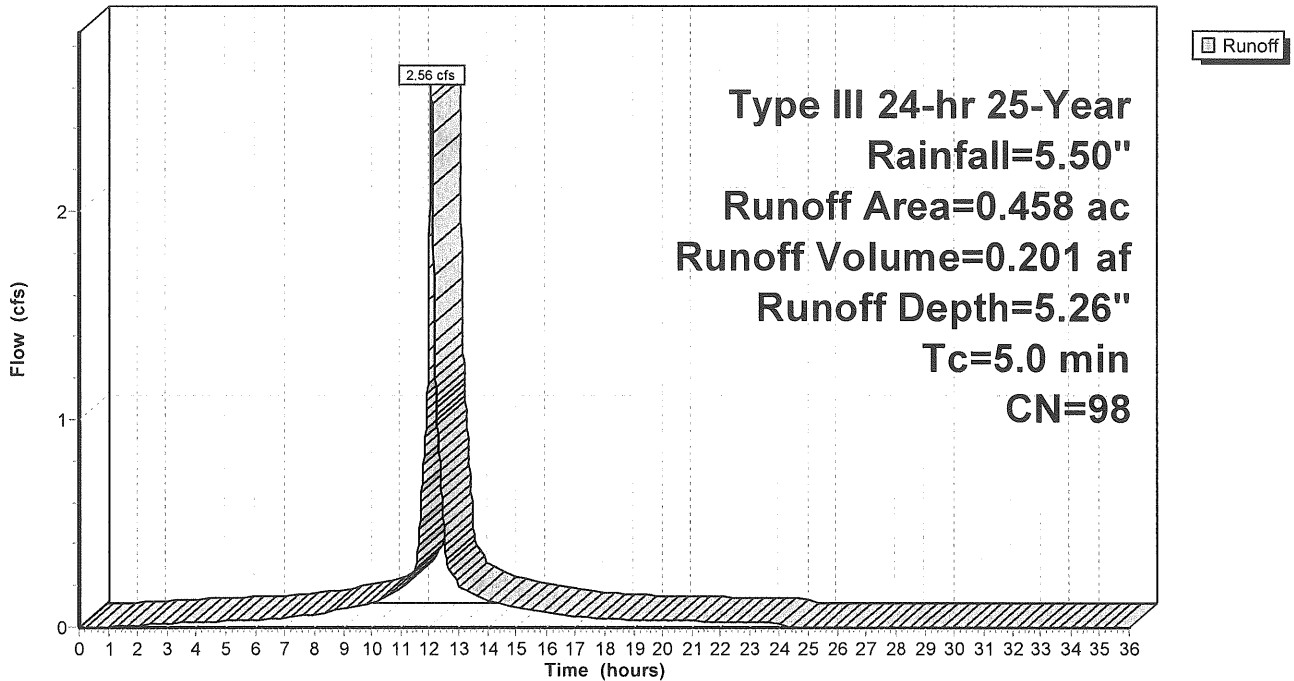
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.458	98	Paved parking & roofs
0.458		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 3:

Hydrograph



Summary for Subcatchment 4:

Runoff = 2.76 cfs @ 12.07 hrs, Volume= 0.213 af, Depth= 5.15"

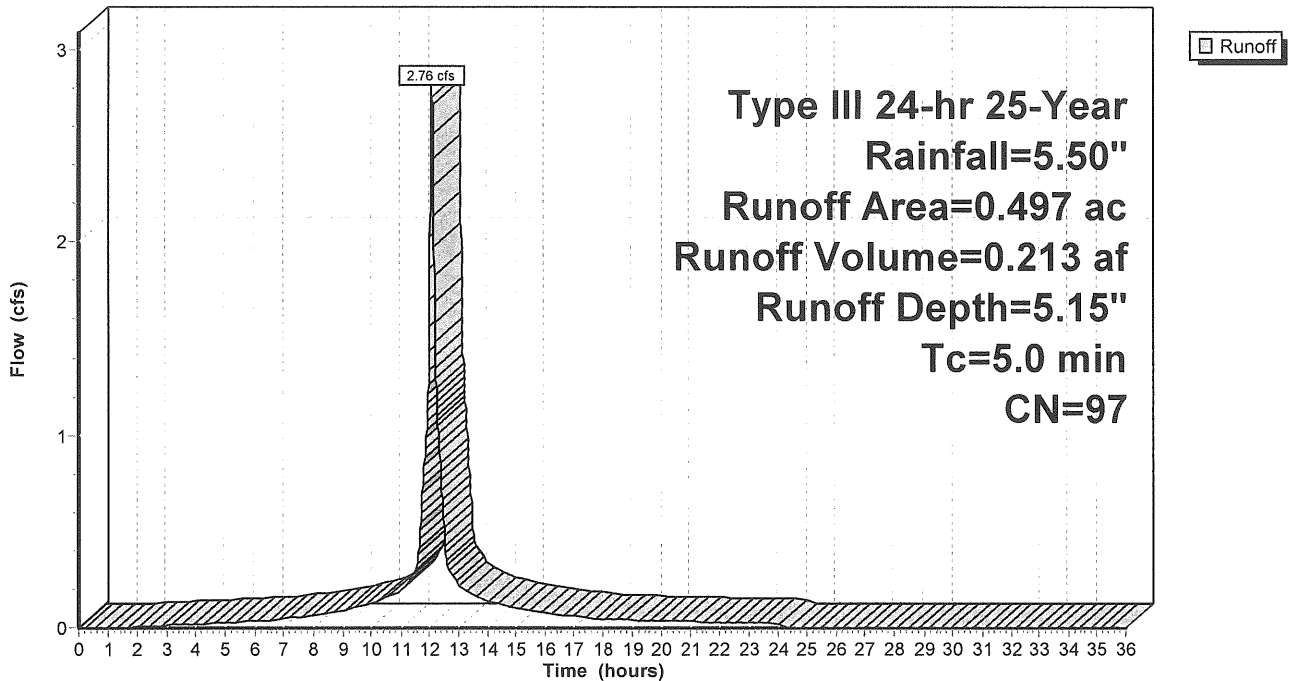
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.470	98	Paved parking & roofs
0.027	80	>75% Grass cover, Good, HSG D
0.497	97	Weighted Average
0.027		Pervious Area
0.470		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 4:

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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Summary for Subcatchment 5:

Runoff = 1.68 cfs @ 12.07 hrs, Volume= 0.132 af, Depth= 5.26"

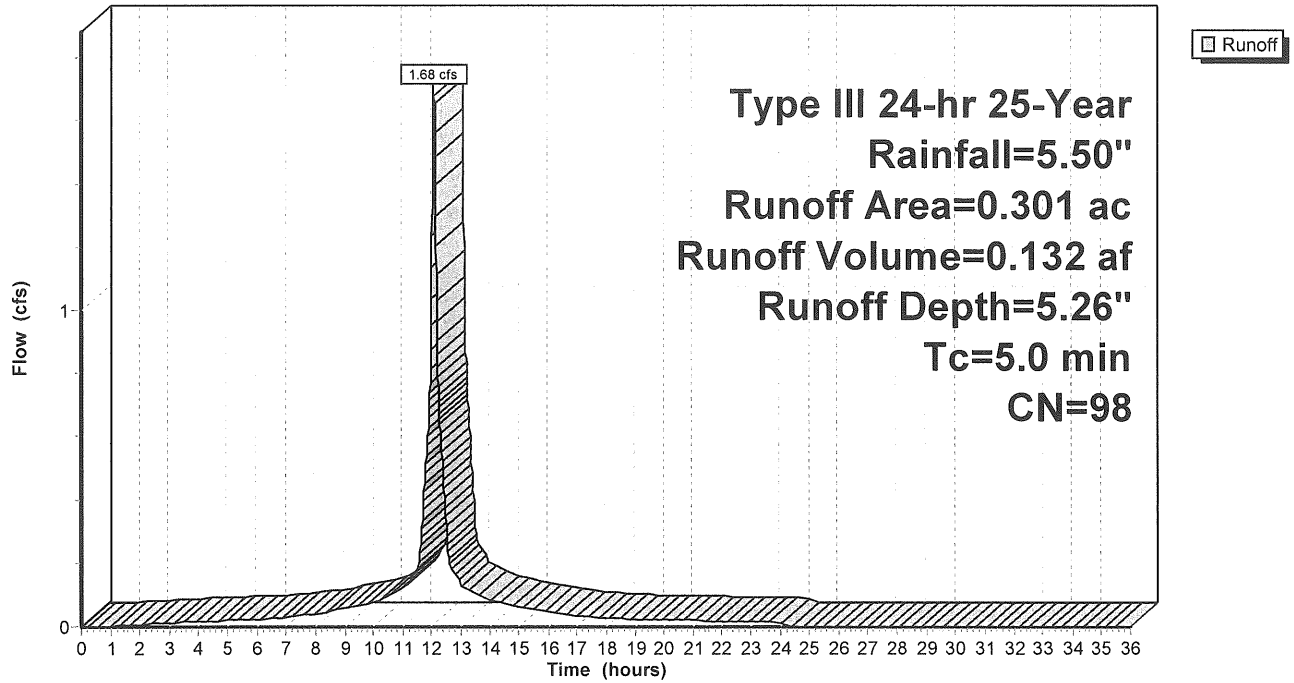
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
0.301	98	Paved parking & roofs
0.301		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 5:

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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Summary for Subcatchment 6:

Runoff = 0.46 cfs @ 12.07 hrs, Volume= 0.035 af, Depth= 4.92"

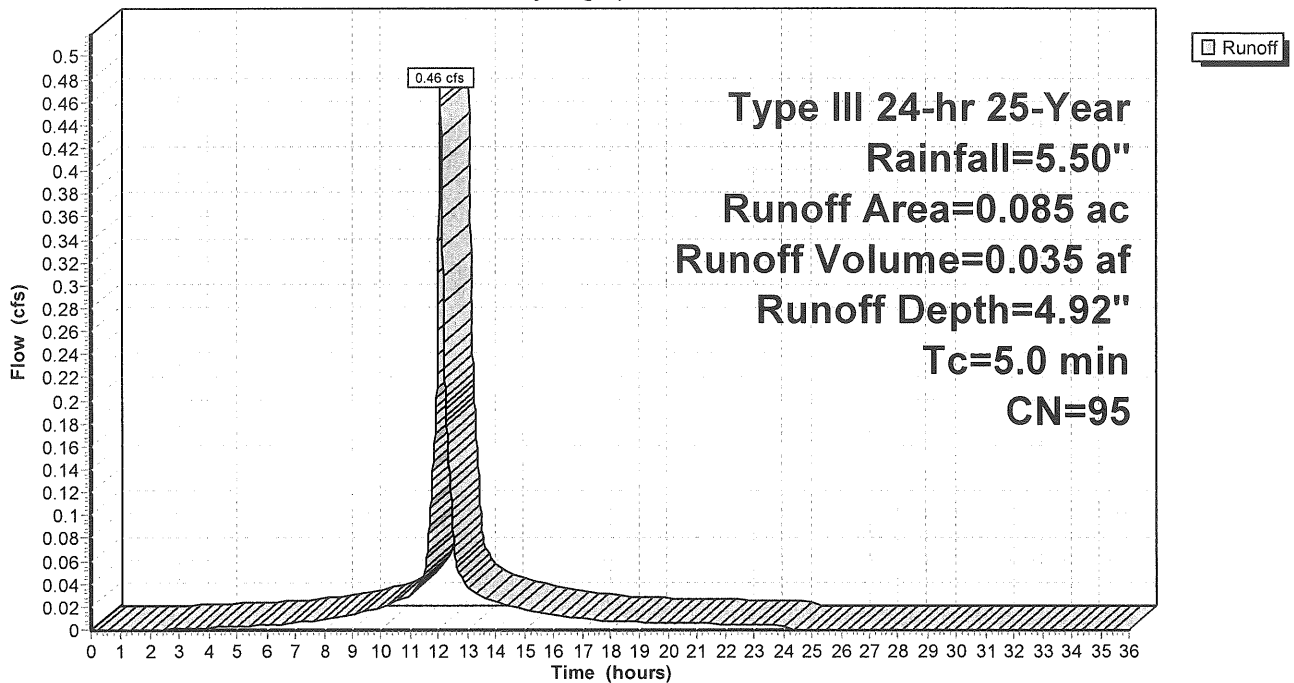
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 25-Year Rainfall=5.50"

Area (ac)	CN	Description
* 0.085	95	Gravel
0.085		Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 6:

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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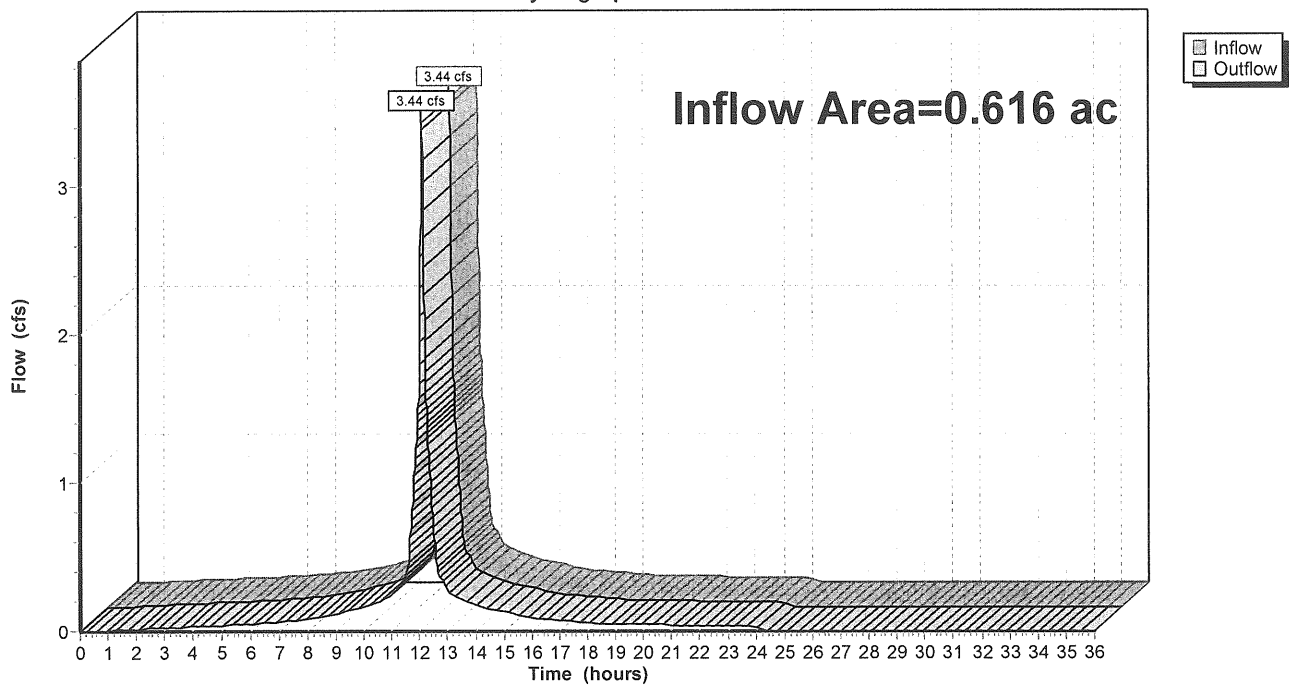
Summary for Reach SP1:

Inflow Area = 0.616 ac, 100.00% Impervious, Inflow Depth = 5.26" for 25-Year event
Inflow = 3.44 cfs @ 12.07 hrs, Volume= 0.270 af
Outflow = 3.44 cfs @ 12.07 hrs, Volume= 0.270 af, Atten= 0%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs

Reach SP1:

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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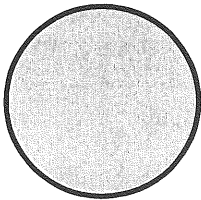
Summary for Reach SP2: Ex. CB (off site)

Inflow Area = 1.951 ac, 94.26% Impervious, Inflow Depth = 5.22" for 25-Year event
Inflow = 10.86 cfs @ 12.07 hrs, Volume= 0.848 af
Outflow = 3.93 cfs @ 11.87 hrs, Volume= 0.848 af, Atten= 64%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Max. Velocity= 5.38 fps, Min. Travel Time= 0.3 min
Avg. Velocity = 2.61 fps, Avg. Travel Time= 0.5 min

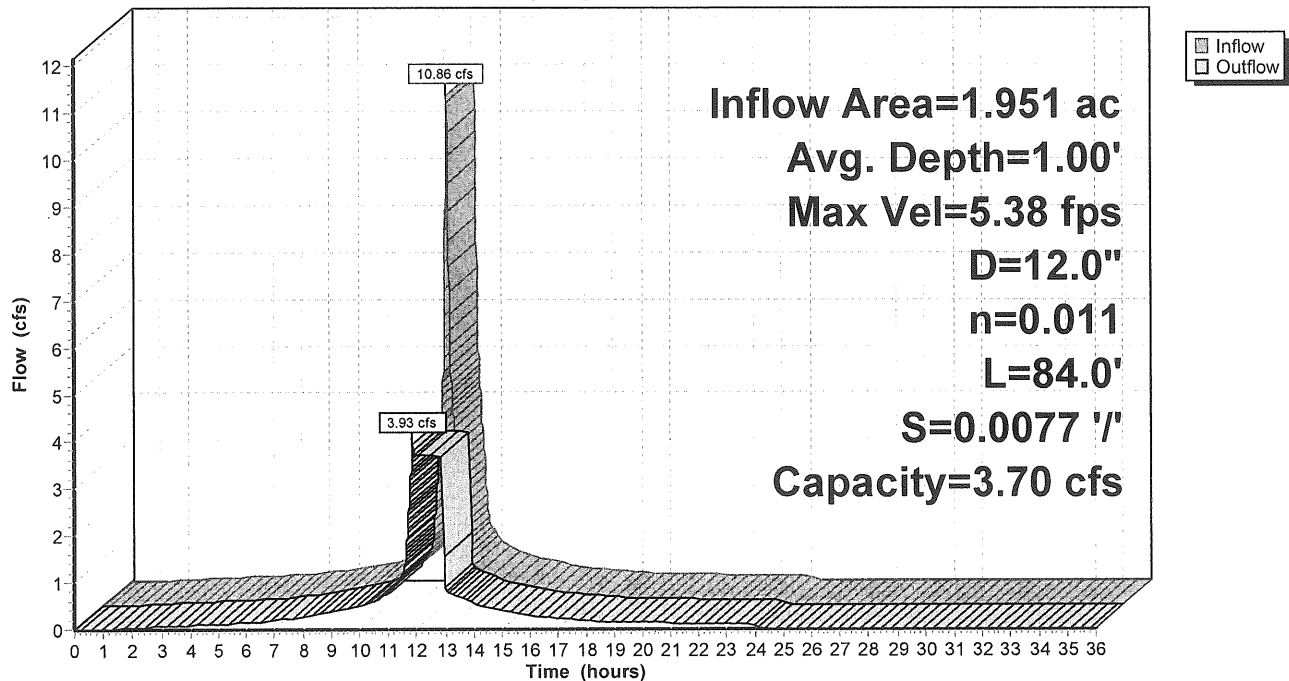
Peak Storage= 66 cf @ 11.88 hrs, Average Depth at Peak Storage= 1.00'
Bank-Full Depth= 1.00', Capacity at Bank-Full= 3.70 cfs

12.0" Diameter Pipe, n= 0.011 Concrete pipe, straight & clean
Length= 84.0' Slope= 0.0077 '/'
Inlet Invert= 8.00', Outlet Invert= 7.35'



Reach SP2: Ex. CB (off site)

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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Summary for Pond CB1:

Inflow Area = 1.153 ac, 92.63% Impervious, Inflow Depth = 5.24" for 25-Year event
 Inflow = 6.42 cfs @ 12.07 hrs, Volume= 0.503 af
 Outflow = 6.42 cfs @ 12.07 hrs, Volume= 0.503 af, Atten= 0%, Lag= 0.0 min
 Primary = 6.42 cfs @ 12.07 hrs, Volume= 0.503 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 12.10' @ 12.07 hrs

Flood Elev= 12.25'

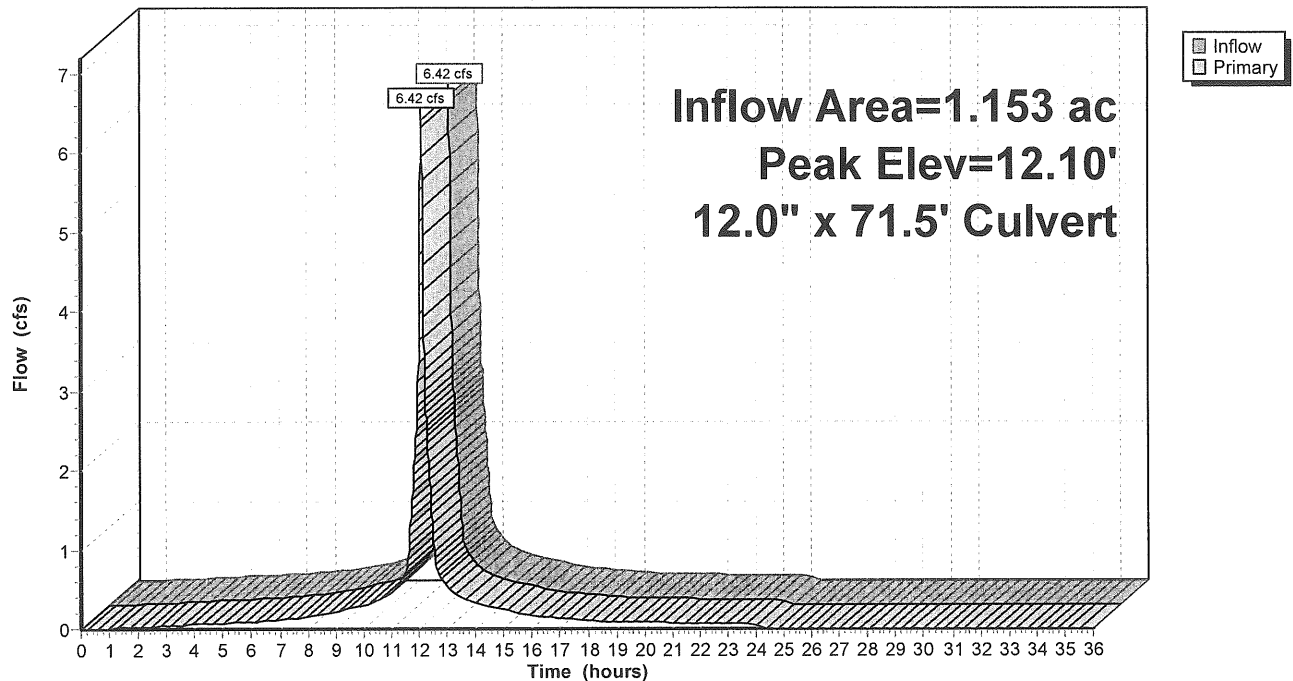
Device	Routing	Invert	Outlet Devices
#1	Primary	9.00'	12.0" x 71.5' long SD-1 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.47' S= 0.0074 '/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=6.42 cfs @ 12.07 hrs HW=12.10' (Free Discharge)

←1=SD-1 (Barrel Controls 6.42 cfs @ 8.18 fps)

Pond CB1:

Hydrograph



Summary for Pond CB2:

Inflow Area = 0.085 ac, 0.00% Impervious, Inflow Depth = 4.92" for 25-Year event
 Inflow = 0.46 cfs @ 12.07 hrs, Volume= 0.035 af
 Outflow = 0.46 cfs @ 12.07 hrs, Volume= 0.035 af, Atten= 0%, Lag= 0.0 min
 Primary = 0.46 cfs @ 12.07 hrs, Volume= 0.035 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs

Peak Elev= 10.05' @ 12.07 hrs

Flood Elev= 13.25'

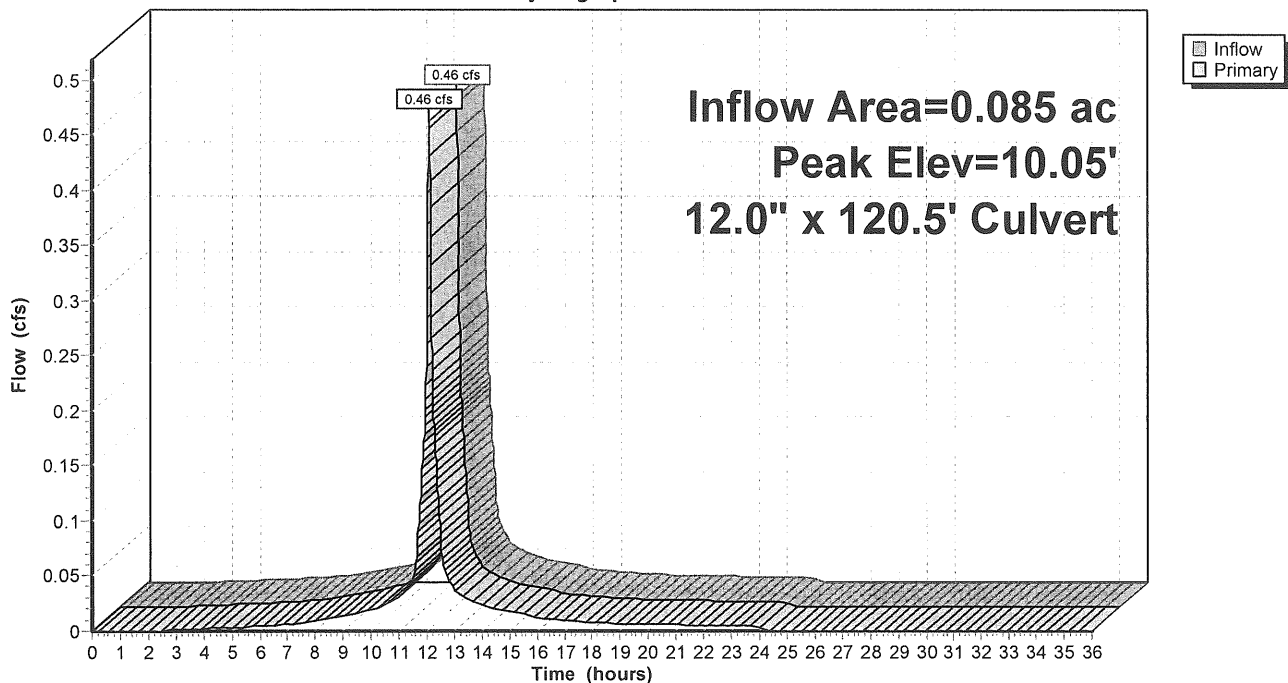
Device #	Routing	Invert	Outlet Devices
#1	Primary	9.70'	12.0" x 120.5' long SD-3 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0050 '/' Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=0.46 cfs @ 12.07 hrs HW=10.05' (Free Discharge)

↑1=SD-3 (Barrel Controls 0.46 cfs @ 2.83 fps)

Pond CB2:

Hydrograph



Summary for Pond CB3: Ex. CB

Inflow Area = 0.610 ac, 100.00% Impervious, Inflow Depth = 5.26" for 25-Year event
 Inflow = 3.40 cfs @ 12.07 hrs, Volume= 0.268 af
 Outflow = 3.40 cfs @ 12.07 hrs, Volume= 0.268 af, Atten= 0%, Lag= 0.0 min
 Primary = 3.40 cfs @ 12.07 hrs, Volume= 0.268 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 11.00' @ 12.07 hrs

Flood Elev= 12.47'

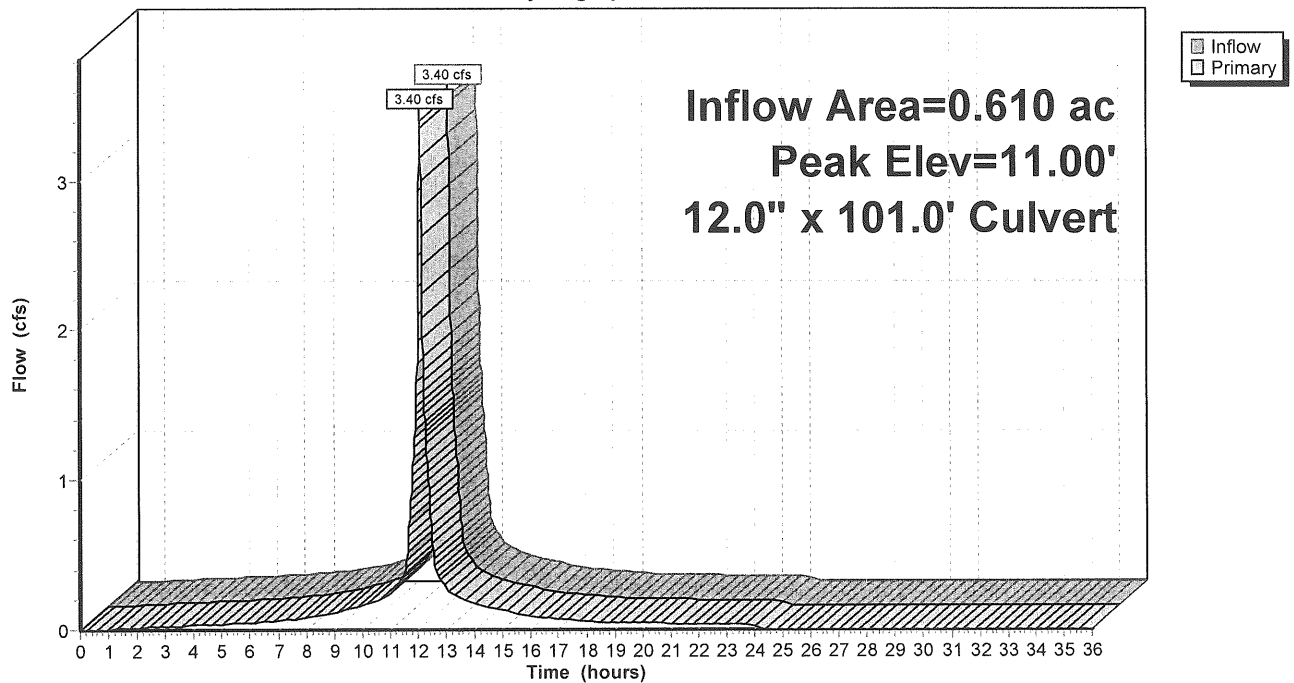
Device	Routing	Invert	Outlet Devices
#1	Primary	9.56'	12.0" x 101.0' long SD-2 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0046 1/ S= 0.0046 1/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=3.40 cfs @ 12.07 hrs HW=11.00' (Free Discharge)

1=SD-2 (Barrel Controls 3.40 cfs @ 4.33 fps)

Pond CB3: Ex. CB

Hydrograph



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Type III 24-hr 25-Year Rainfall=5.50"

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Summary for Pond XCB: Ex. CB (on site)

Inflow Area = 1.650 ac, 93.21% Impervious, Inflow Depth = 5.21" for 25-Year event
 Inflow = 9.18 cfs @ 12.07 hrs, Volume= 0.716 af
 Outflow = 9.18 cfs @ 12.07 hrs, Volume= 0.716 af, Atten= 0%, Lag= 0.0 min
 Primary = 9.18 cfs @ 12.07 hrs, Volume= 0.716 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 12.74' @ 12.07 hrs

Flood Elev= 11.47'

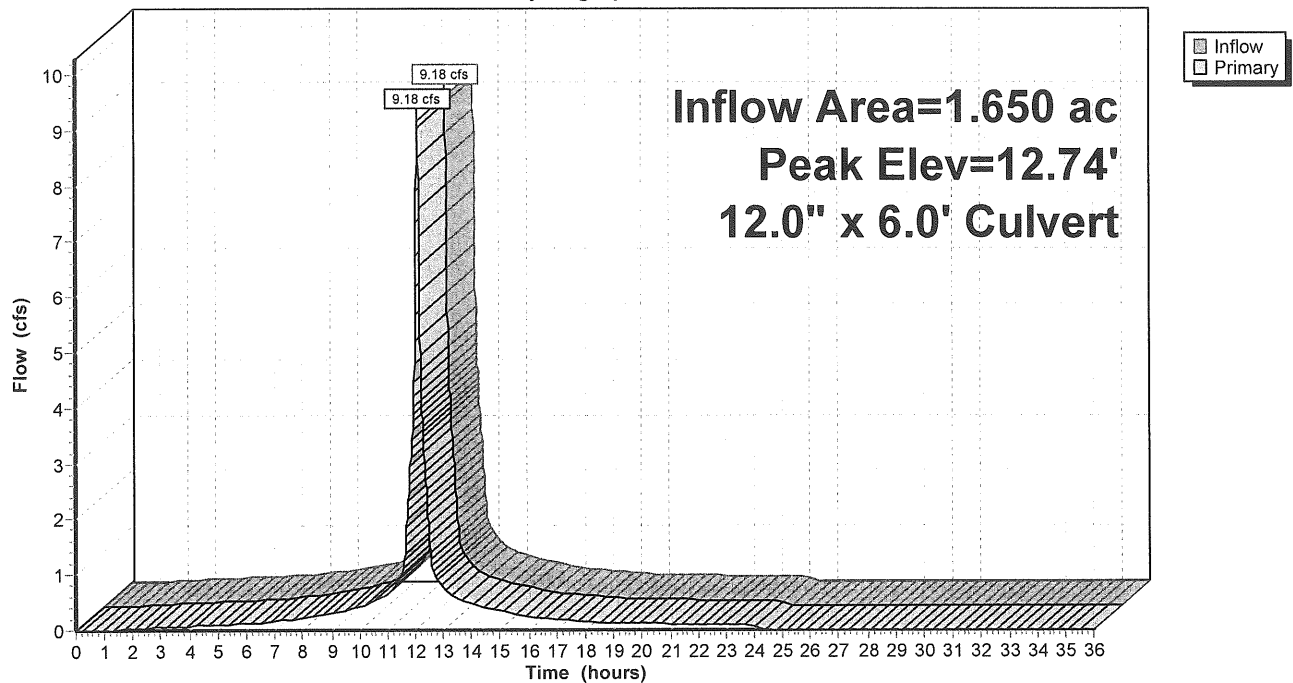
Device	Routing	Invert	Outlet Devices
#1	Primary	8.47'	12.0" x 6.0' long Ex. SD RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.30' S= 0.0283 '/ Cc= 0.900 n= 0.011 Concrete pipe, straight & clean

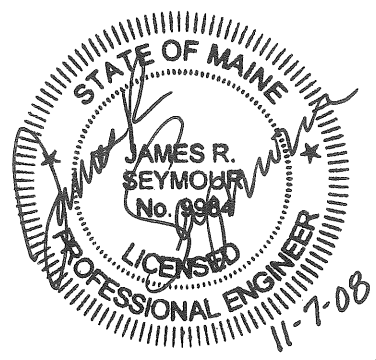
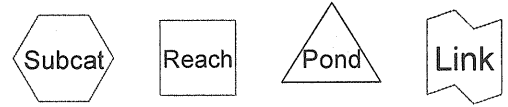
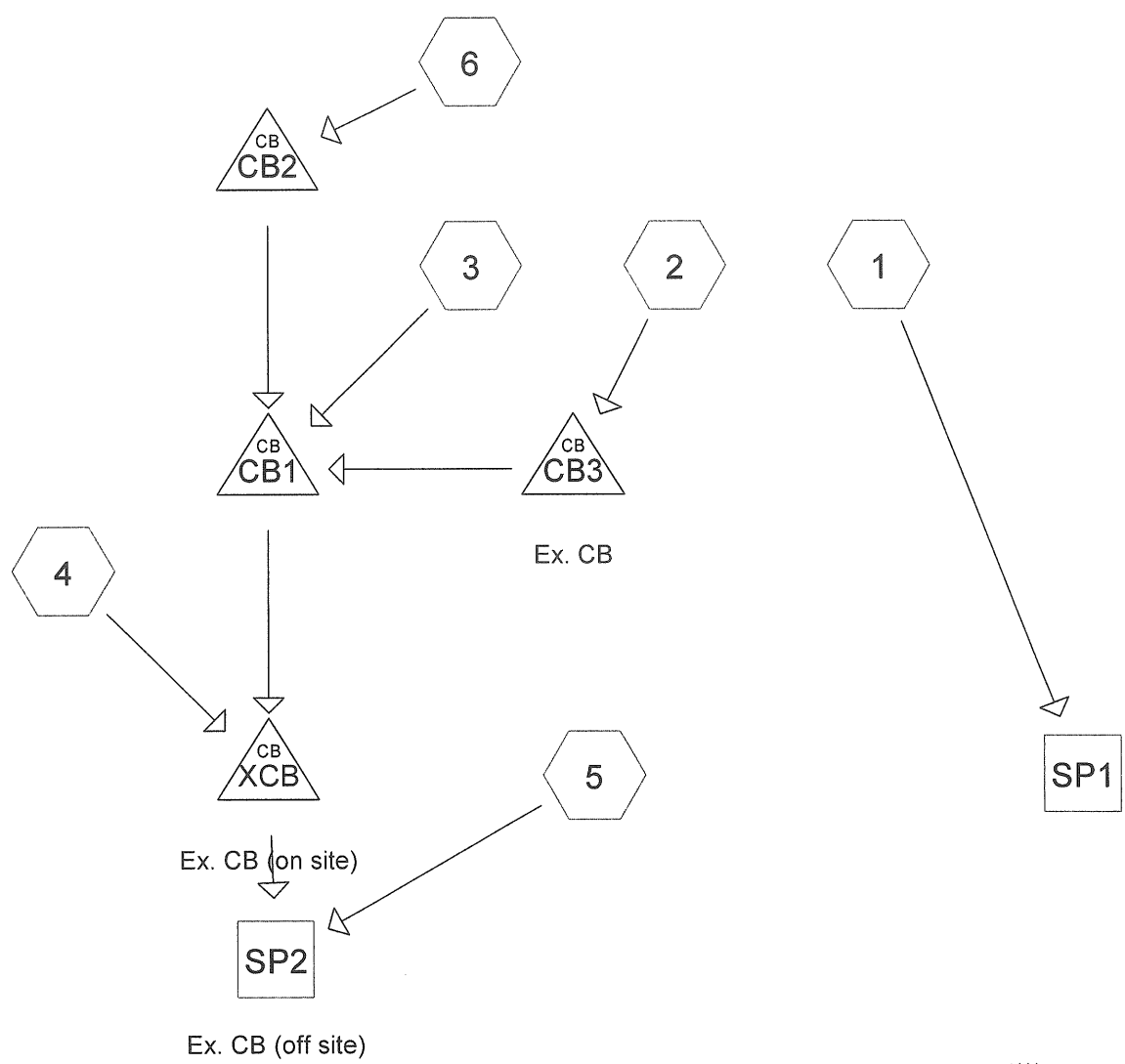
Primary OutFlow Max=9.18 cfs @ 12.07 hrs HW=12.74' (Free Discharge)

↑1=Ex. SD (Inlet Controls 9.18 cfs @ 11.69 fps)

Pond XCB: Ex. CB (on site)

Hydrograph





Drainage Diagram for 07383POST
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Rufus Deering Lumber

Type III 24-hr 2-Year Rainfall=3.00"

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Time span=0.00-36.00 hrs, dt=0.01 hrs, 3601 points

Runoff by SCS TR-20 method, UH=SCS

Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment 1:	Runoff Area=0.616 ac 100.00% Impervious Runoff Depth=2.77" Tc=5.0 min CN=98 Runoff=1.86 cfs 0.142 af
Subcatchment 2:	Runoff Area=0.610 ac 100.00% Impervious Runoff Depth=2.77" Tc=5.0 min CN=98 Runoff=1.84 cfs 0.141 af
Subcatchment 3:	Runoff Area=0.458 ac 100.00% Impervious Runoff Depth=2.77" Tc=5.0 min CN=98 Runoff=1.38 cfs 0.106 af
Subcatchment 4:	Runoff Area=0.497 ac 94.57% Impervious Runoff Depth=2.66" Tc=5.0 min CN=97 Runoff=1.47 cfs 0.110 af
Subcatchment 5:	Runoff Area=0.301 ac 100.00% Impervious Runoff Depth=2.77" Tc=5.0 min CN=98 Runoff=0.91 cfs 0.069 af
Subcatchment 6:	Runoff Area=0.085 ac 0.00% Impervious Runoff Depth=2.45" Tc=5.0 min CN=95 Runoff=0.24 cfs 0.017 af
Reach SP1:	Inflow=1.86 cfs 0.142 af Outflow=1.86 cfs 0.142 af
Reach SP2: Ex. CB (off site)	Avg. Depth=1.00' Max Vel=5.37 fps Inflow=5.83 cfs 0.443 af D=12.0" n=0.011 L=84.0' S=0.0077 '/' Capacity=3.70 cfs Outflow=3.74 cfs 0.443 af
Pond CB1:	Peak Elev=10.14' Inflow=3.46 cfs 0.264 af 12.0" x 71.5' Culvert Outflow=3.46 cfs 0.264 af
Pond CB2:	Peak Elev=9.95' Inflow=0.24 cfs 0.017 af 12.0" x 120.5' Culvert Outflow=0.24 cfs 0.017 af
Pond CB3: Ex. CB	Peak Elev=10.34' Inflow=1.84 cfs 0.141 af 12.0" x 101.0' Culvert Outflow=1.84 cfs 0.141 af
Pond XCB: Ex. CB (on site)	Peak Elev=10.12' Inflow=4.93 cfs 0.374 af 12.0" x 6.0' Culvert Outflow=4.93 cfs 0.374 af

Total Runoff Area = 2.567 ac Runoff Volume = 0.585 af Average Runoff Depth = 2.74"
4.36% Pervious = 0.112 ac 95.64% Impervious = 2.455 ac

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Type III 24-hr 2-Year Rainfall=3.00"

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Summary for Subcatchment 1:

Runoff = 1.86 cfs @ 12.07 hrs, Volume= 0.142 af, Depth= 2.77"

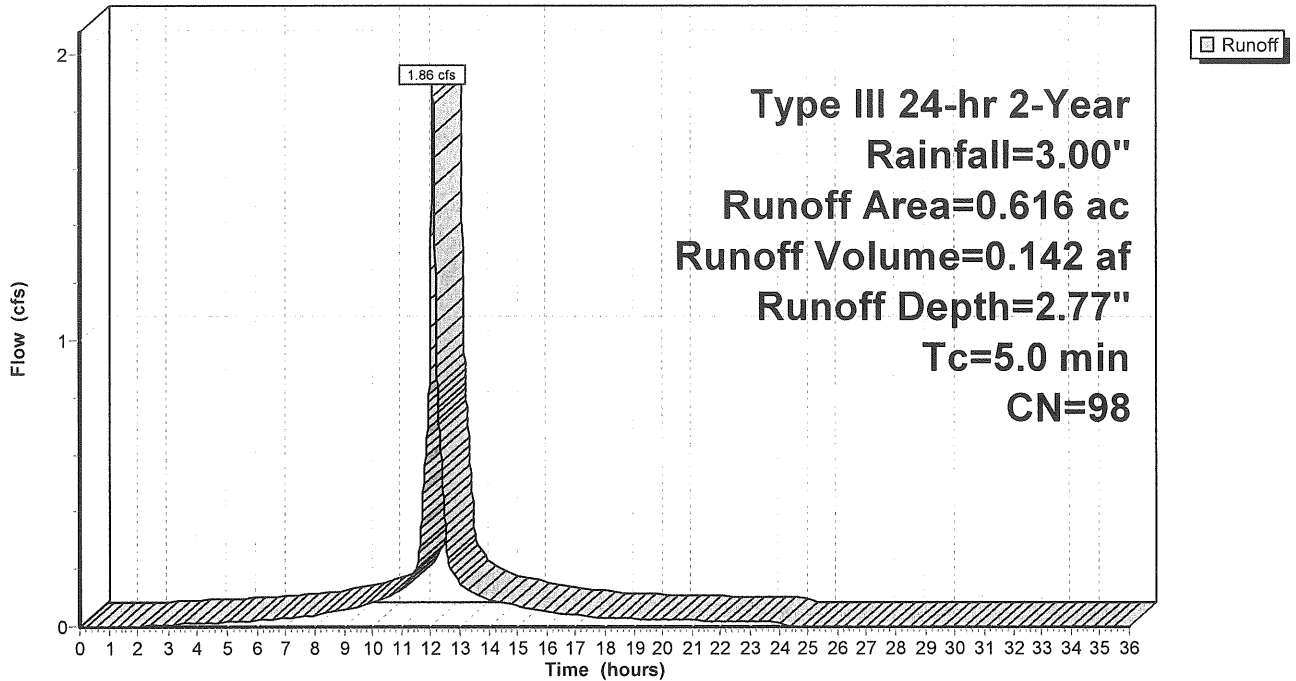
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.616	98	Paved parking & roofs
0.616		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 1:

Hydrograph



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Type III 24-hr 2-Year Rainfall=3.00"

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Summary for Subcatchment 2:

Runoff = 1.84 cfs @ 12.07 hrs, Volume= 0.141 af, Depth= 2.77"

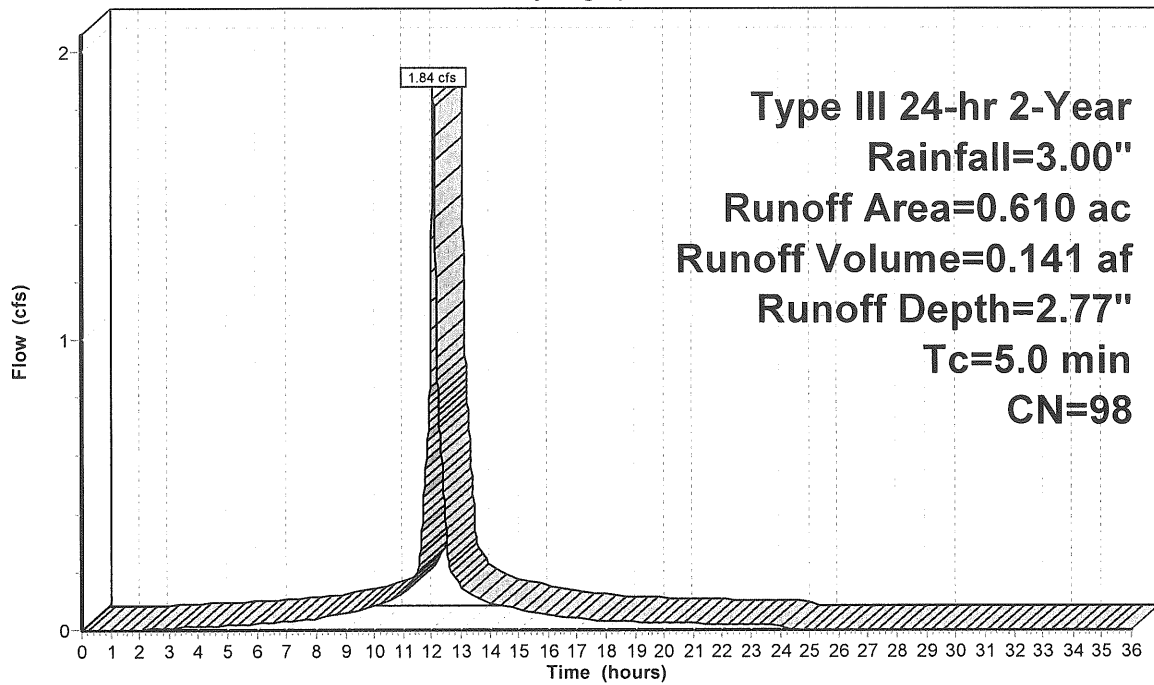
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.610	98	Paved parking & roofs
0.610		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 2:

Hydrograph



Runoff

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Type III 24-hr 2-Year Rainfall=3.00"

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Summary for Subcatchment 3:

Runoff = 1.38 cfs @ 12.07 hrs, Volume= 0.106 af, Depth= 2.77"

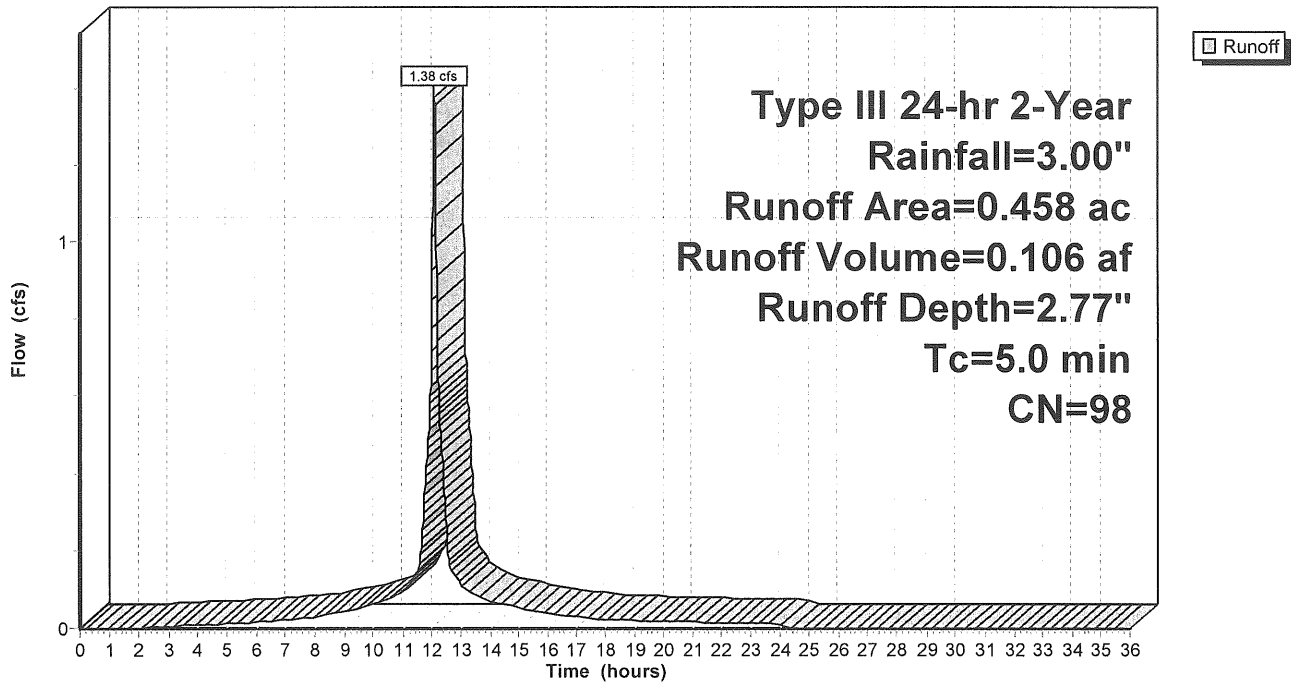
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.458	98	Paved parking & roofs
0.458		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 3:

Hydrograph



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Type III 24-hr 2-Year Rainfall=3.00"

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Summary for Subcatchment 4:

Runoff = 1.47 cfs @ 12.07 hrs, Volume= 0.110 af, Depth= 2.66"

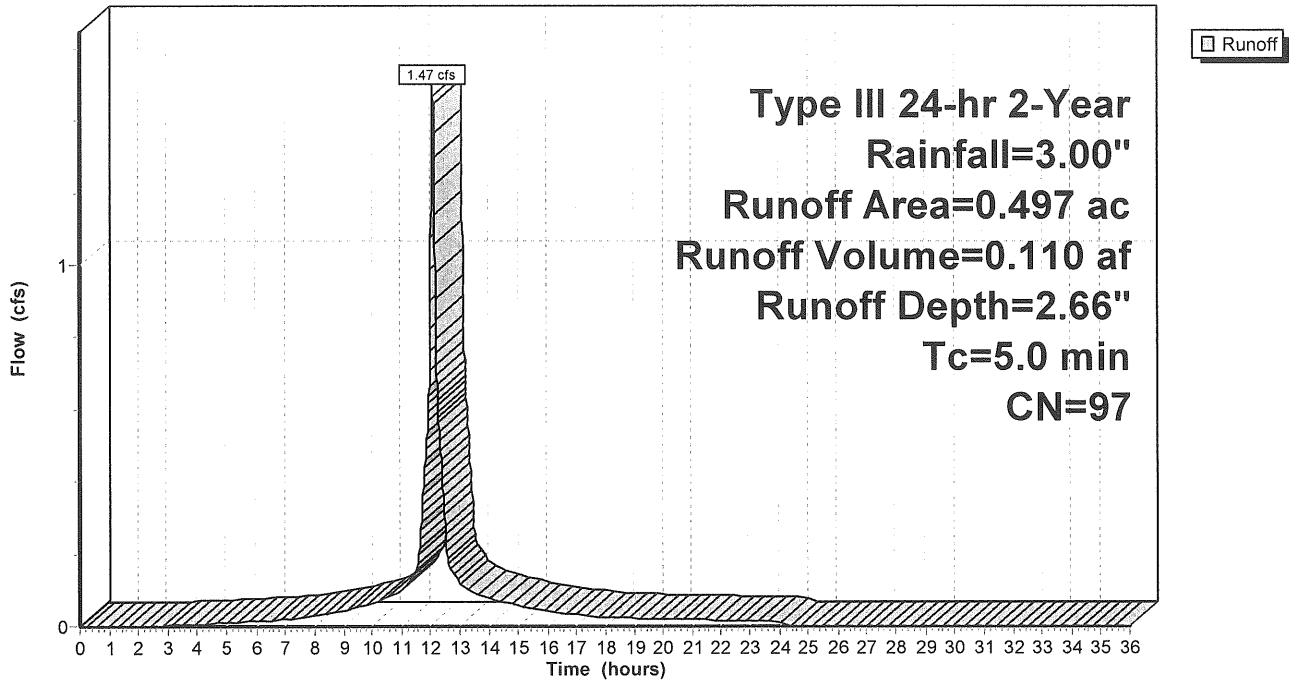
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.470	98	Paved parking & roofs
0.027	80	>75% Grass cover, Good, HSG D
0.497	97	Weighted Average
0.027		Pervious Area
0.470		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 4:

Hydrograph



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Type III 24-hr 2-Year Rainfall=3.00"

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Summary for Subcatchment 5:

Runoff = 0.91 cfs @ 12.07 hrs, Volume= 0.069 af, Depth= 2.77"

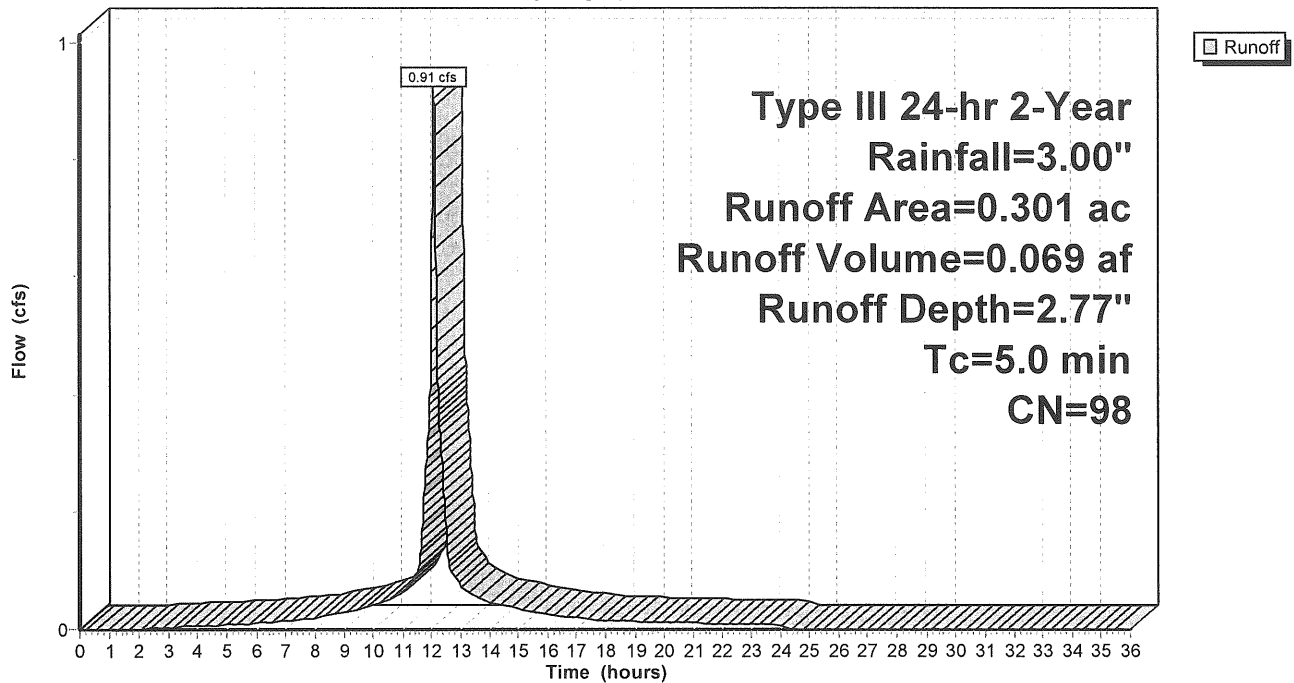
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
0.301	98	Paved parking & roofs
0.301		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 5:

Hydrograph



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Type III 24-hr 2-Year Rainfall=3.00"

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Summary for Subcatchment 6:

Runoff = 0.24 cfs @ 12.07 hrs, Volume= 0.017 af, Depth= 2.45"

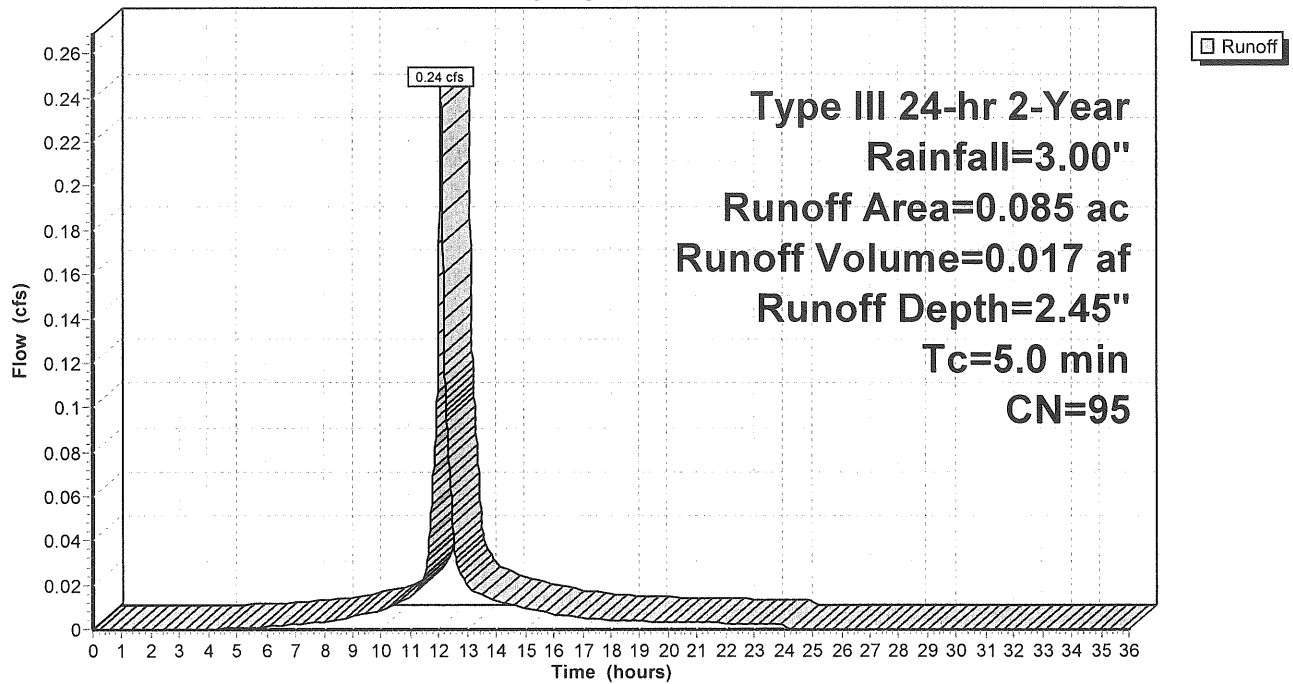
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.00"

Area (ac)	CN	Description
* 0.085	95	Gravel
0.085		Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 6:

Hydrograph



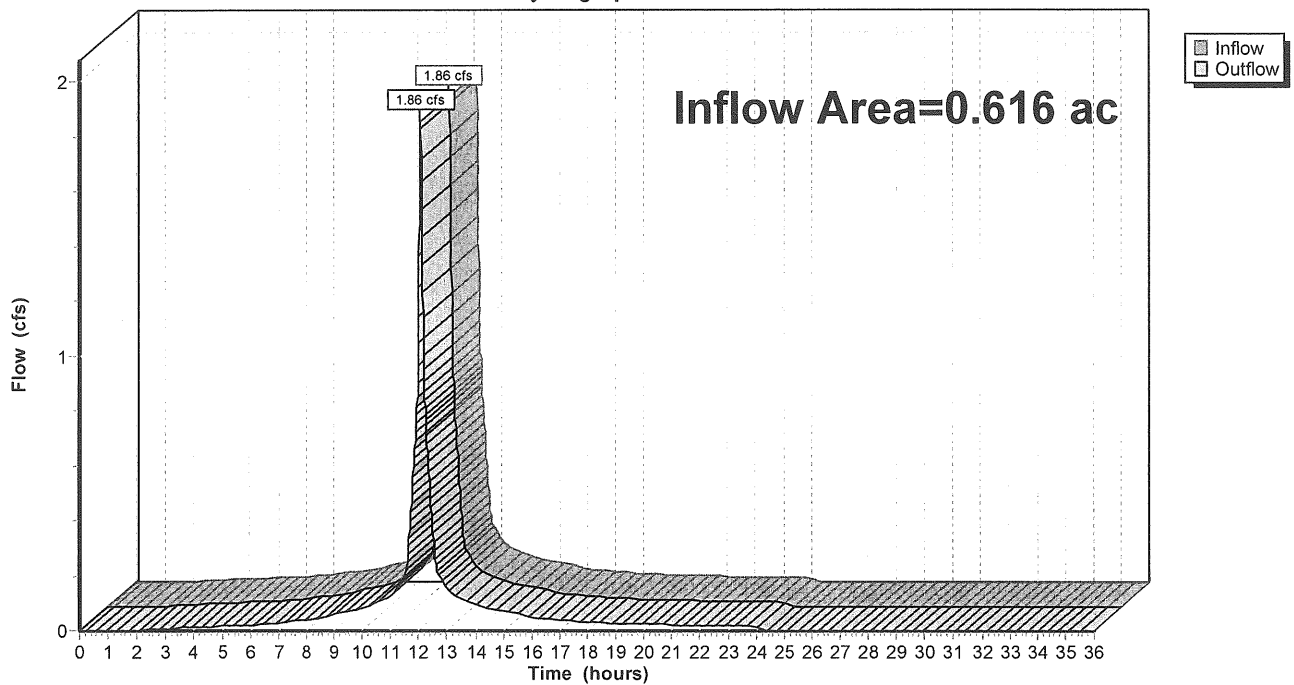
Summary for Reach SP1:

Inflow Area = 0.616 ac, 100.00% Impervious, Inflow Depth = 2.77" for 2-Year event
Inflow = 1.86 cfs @ 12.07 hrs, Volume= 0.142 af
Outflow = 1.86 cfs @ 12.07 hrs, Volume= 0.142 af, Atten= 0%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs

Reach SP1:

Hydrograph



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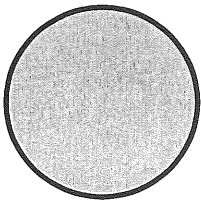
Summary for Reach SP2: Ex. CB (off site)

Inflow Area = 1.951 ac, 94.26% Impervious, Inflow Depth = 2.73" for 2-Year event
Inflow = 5.83 cfs @ 12.07 hrs, Volume= 0.443 af
Outflow = 3.74 cfs @ 12.01 hrs, Volume= 0.443 af, Atten= 36%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Max. Velocity= 5.37 fps, Min. Travel Time= 0.3 min
Avg. Velocity = 2.16 fps, Avg. Travel Time= 0.6 min

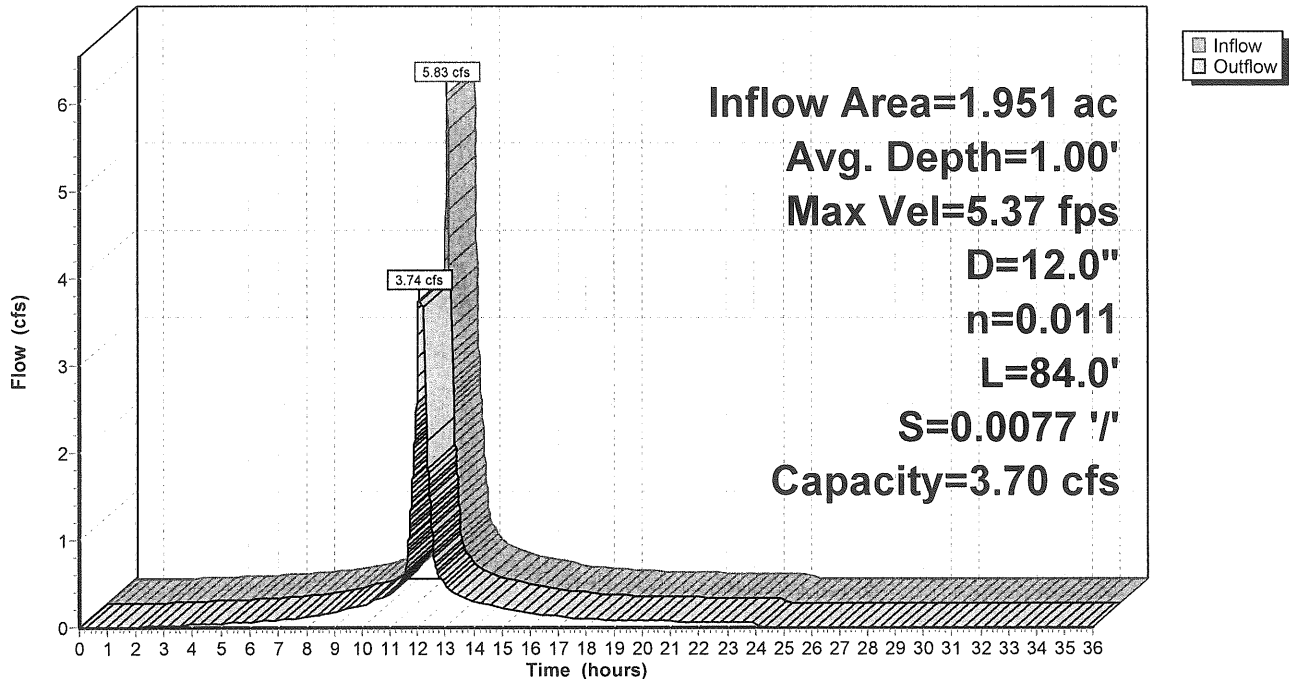
Peak Storage= 66 cf @ 12.01 hrs, Average Depth at Peak Storage= 1.00'
Bank-Full Depth= 1.00', Capacity at Bank-Full= 3.70 cfs

12.0" Diameter Pipe, n= 0.011 Concrete pipe, straight & clean
Length= 84.0' Slope= 0.0077 '/'
Inlet Invert= 8.00', Outlet Invert= 7.35'



Reach SP2: Ex. CB (off site)

Hydrograph



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Type III 24-hr 2-Year Rainfall=3.00"

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Summary for Pond CB1:

Inflow Area = 1.153 ac, 92.63% Impervious, Inflow Depth = 2.74" for 2-Year event
Inflow = 3.46 cfs @ 12.07 hrs, Volume= 0.264 af
Outflow = 3.46 cfs @ 12.07 hrs, Volume= 0.264 af, Atten= 0%, Lag= 0.0 min
Primary = 3.46 cfs @ 12.07 hrs, Volume= 0.264 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 10.14' @ 12.07 hrs

Flood Elev= 12.25'

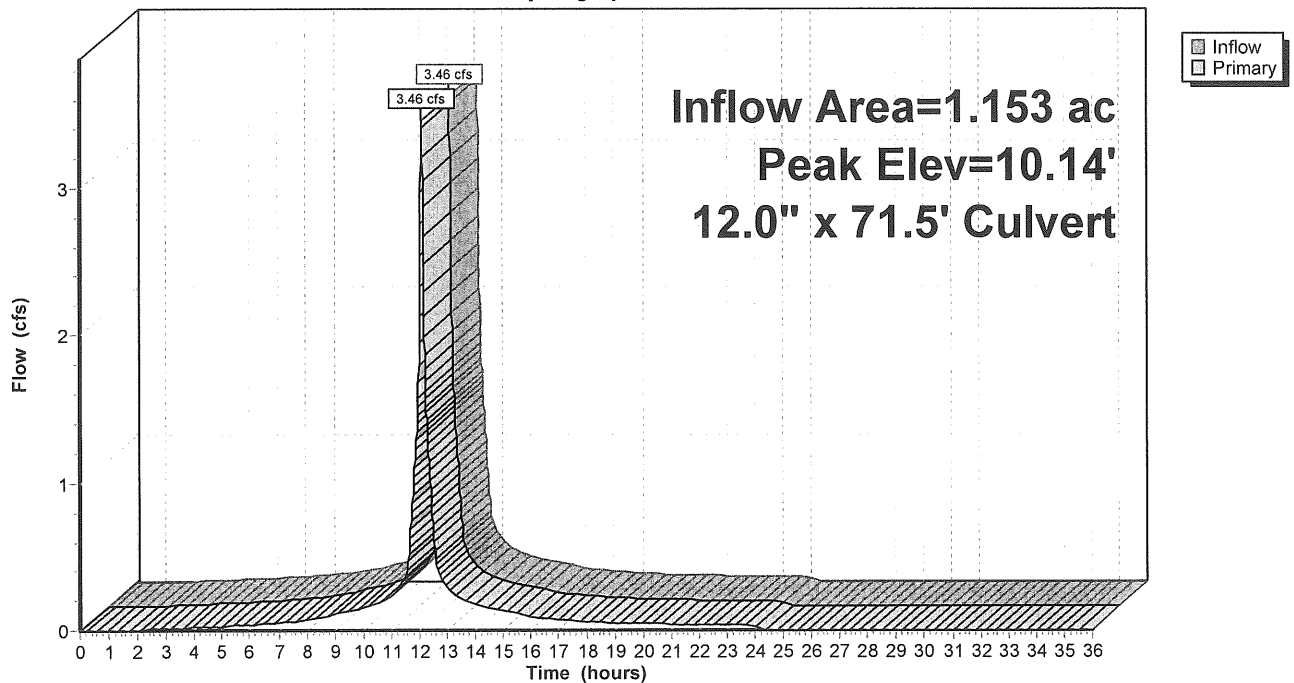
Device	Routing	Invert	Outlet Devices
#1	Primary	9.00'	12.0" x 71.5' long SD-1 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.47' S= 0.0074 '/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=3.46 cfs @ 12.07 hrs HW=10.14' (Free Discharge)

←1=SD-1 (Barrel Controls 3.46 cfs @ 4.85 fps)

Pond CB1:

Hydrograph



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Summary for Pond CB2:

Inflow Area = 0.085 ac, 0.00% Impervious, Inflow Depth = 2.45" for 2-Year event
Inflow = 0.24 cfs @ 12.07 hrs, Volume= 0.017 af
Outflow = 0.24 cfs @ 12.07 hrs, Volume= 0.017 af, Atten= 0%, Lag= 0.0 min
Primary = 0.24 cfs @ 12.07 hrs, Volume= 0.017 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs

Peak Elev= 9.95' @ 12.07 hrs

Flood Elev= 13.25'

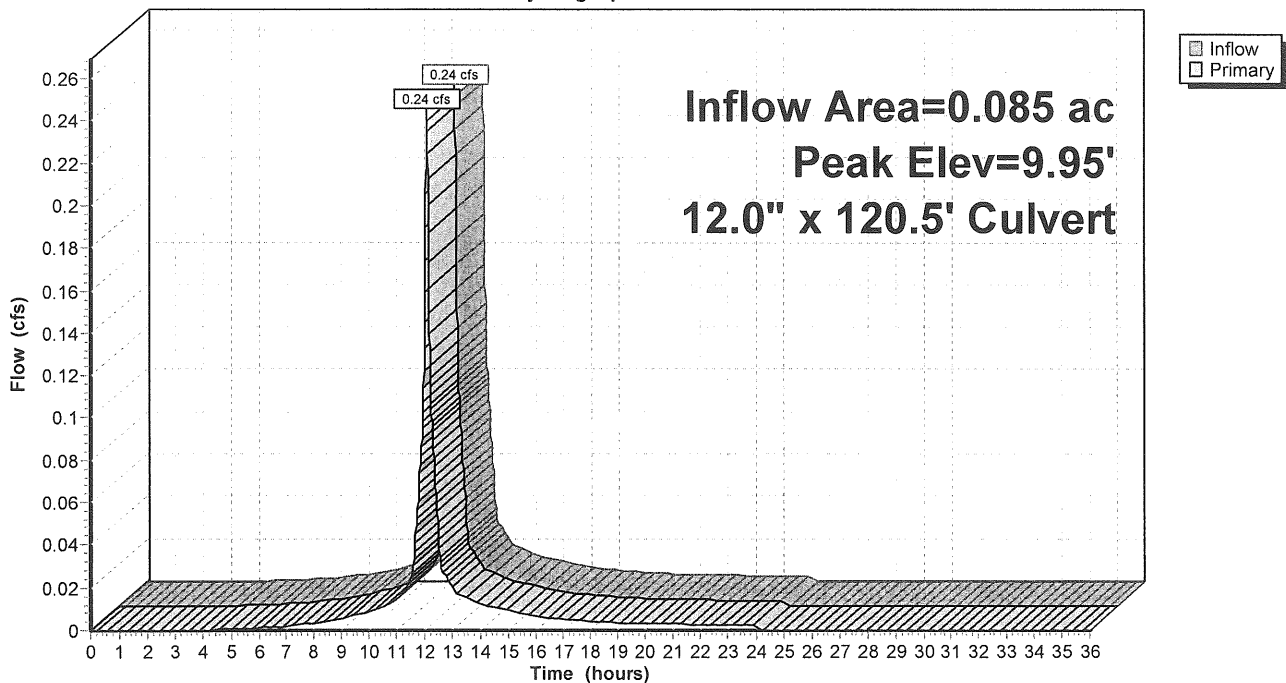
Device	Routing	Invert	Outlet Devices
#1	Primary	9.70'	12.0" x 120.5' long SD-3 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0050 '/' Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=0.24 cfs @ 12.07 hrs HW=9.95' (Free Discharge)

↑1=SD-3 (Barrel Controls 0.24 cfs @ 2.37 fps)

Pond CB2:

Hydrograph



Summary for Pond CB3: Ex. CB

Inflow Area = 0.610 ac, 100.00% Impervious, Inflow Depth = 2.77" for 2-Year event
 Inflow = 1.84 cfs @ 12.07 hrs, Volume= 0.141 af
 Outflow = 1.84 cfs @ 12.07 hrs, Volume= 0.141 af, Atten= 0%, Lag= 0.0 min
 Primary = 1.84 cfs @ 12.07 hrs, Volume= 0.141 af

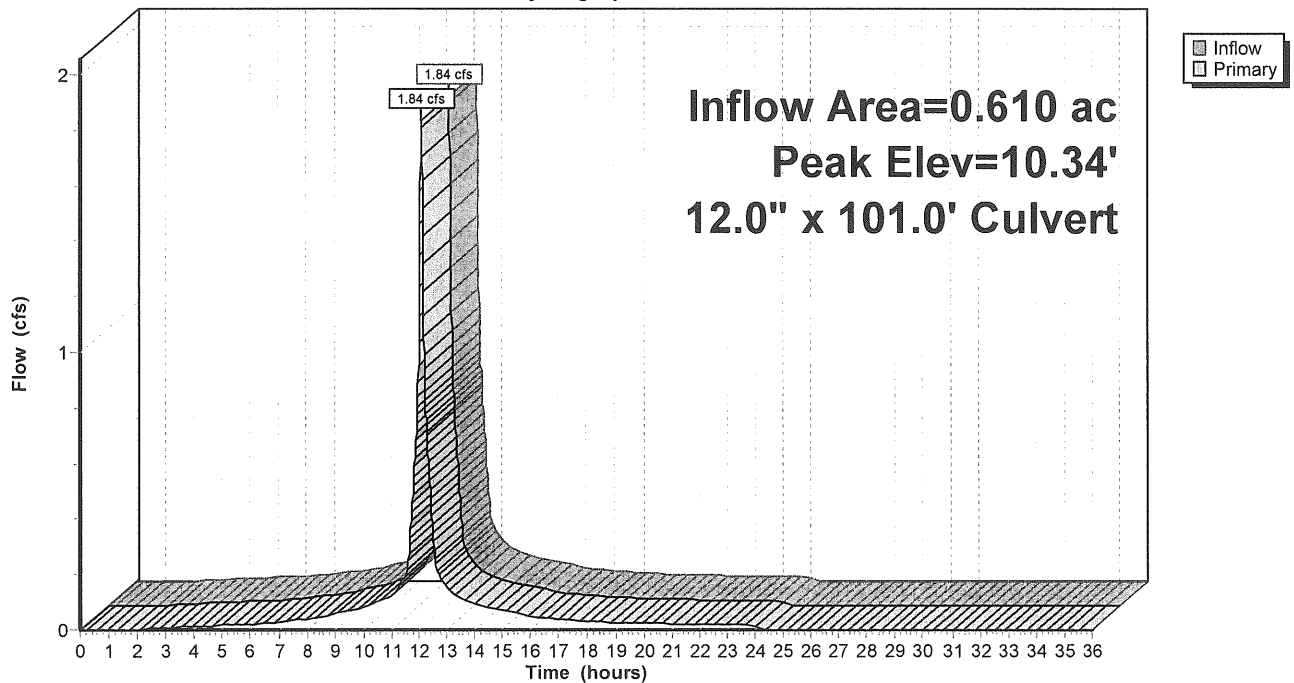
Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3
 Peak Elev= 10.34' @ 12.07 hrs
 Flood Elev= 12.47'

Device	Routing	Invert	Outlet Devices
#1	Primary	9.56'	12.0" x 101.0' long SD-2 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 9.10' S= 0.0046 '/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=1.84 cfs @ 12.07 hrs HW=10.34' (Free Discharge)
 ↳1=SD-2 (Barrel Controls 1.84 cfs @ 3.84 fps)

Pond CB3: Ex. CB

Hydrograph



Summary for Pond XCB: Ex. CB (on site)

Inflow Area = 1.650 ac, 93.21% Impervious, Inflow Depth = 2.72" for 2-Year event
 Inflow = 4.93 cfs @ 12.07 hrs, Volume= 0.374 af
 Outflow = 4.93 cfs @ 12.07 hrs, Volume= 0.374 af, Atten= 0%, Lag= 0.0 min
 Primary = 4.93 cfs @ 12.07 hrs, Volume= 0.374 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 10.12' @ 12.07 hrs

Flood Elev= 11.47'

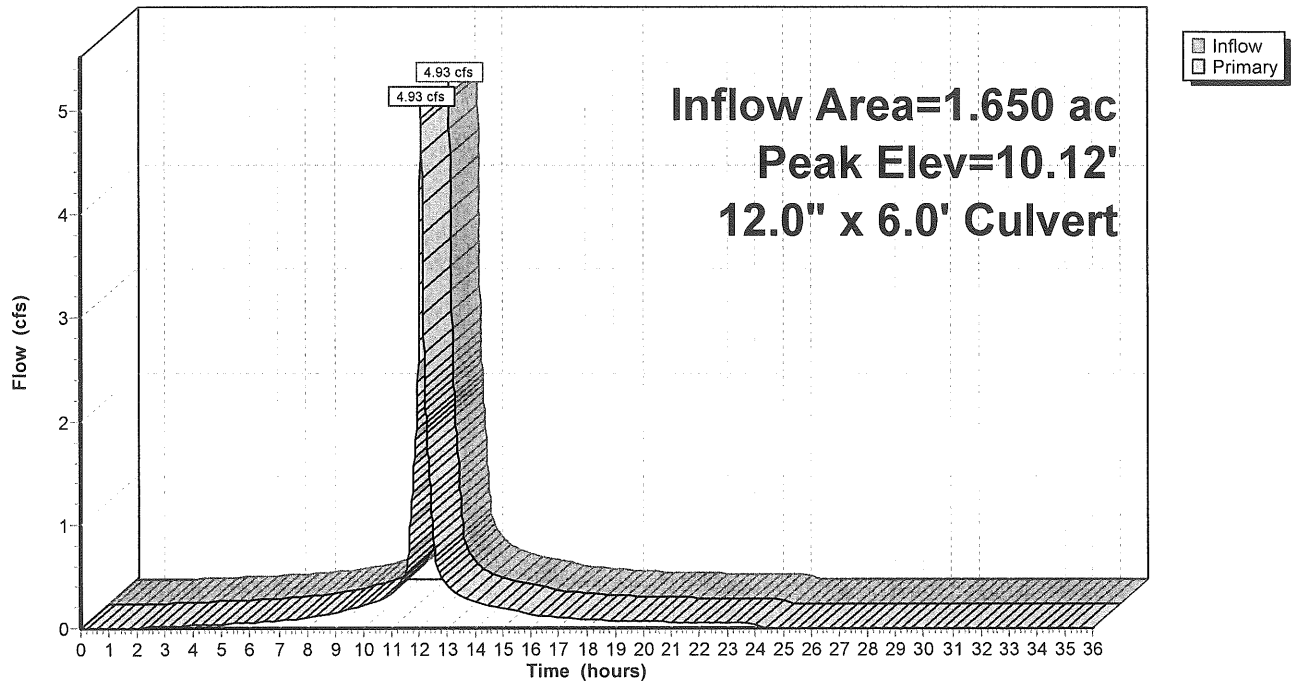
Device	Routing	Invert	Outlet Devices
#1	Primary	8.47'	12.0" x 6.0' long Ex. SD RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.30' S= 0.0283 '/ Cc= 0.900 n= 0.011 Concrete pipe, straight & clean

Primary OutFlow Max=4.93 cfs @ 12.07 hrs HW=10.12' (Free Discharge)

←1=Ex. SD (Barrel Controls 4.93 cfs @ 6.27 fps)

Pond XCB: Ex. CB (on site)

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

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Time span=0.00-36.00 hrs, dt=0.01 hrs, 3601 points

Runoff by SCS TR-20 method, UH=SCS

Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment 1:	Runoff Area=0.616 ac 100.00% Impervious Runoff Depth=4.46" Tc=5.0 min CN=98 Runoff=2.93 cfs 0.229 af
Subcatchment 2:	Runoff Area=0.610 ac 100.00% Impervious Runoff Depth=4.46" Tc=5.0 min CN=98 Runoff=2.90 cfs 0.227 af
Subcatchment 3:	Runoff Area=0.458 ac 100.00% Impervious Runoff Depth=4.46" Tc=5.0 min CN=98 Runoff=2.18 cfs 0.170 af
Subcatchment 4:	Runoff Area=0.497 ac 94.57% Impervious Runoff Depth=4.35" Tc=5.0 min CN=97 Runoff=2.35 cfs 0.180 af
Subcatchment 5:	Runoff Area=0.301 ac 100.00% Impervious Runoff Depth=4.46" Tc=5.0 min CN=98 Runoff=1.43 cfs 0.112 af
Subcatchment 6:	Runoff Area=0.085 ac 0.00% Impervious Runoff Depth=4.12" Tc=5.0 min CN=95 Runoff=0.39 cfs 0.029 af
Reach SP1:	Inflow=2.93 cfs 0.229 af Outflow=2.93 cfs 0.229 af
Reach SP2: Ex. CB (off site)	Avg. Depth=1.00' Max Vel=5.38 fps Inflow=9.26 cfs 0.719 af D=12.0" n=0.011 L=84.0' S=0.0077 '/' Capacity=3.70 cfs Outflow=3.93 cfs 0.719 af
Pond CB1:	Peak Elev=11.38' Inflow=5.48 cfs 0.426 af 12.0" x 71.5' Culvert Outflow=5.48 cfs 0.426 af
Pond CB2:	Peak Elev=10.02' Inflow=0.39 cfs 0.029 af 12.0" x 120.5' Culvert Outflow=0.39 cfs 0.029 af
Pond CB3: Ex. CB	Peak Elev=10.65' Inflow=2.90 cfs 0.227 af 12.0" x 101.0' Culvert Outflow=2.90 cfs 0.227 af
Pond XCB: Ex. CB (on site)	Peak Elev=11.71' Inflow=7.82 cfs 0.607 af 12.0" x 6.0' Culvert Outflow=7.82 cfs 0.607 af

Total Runoff Area = 2.567 ac Runoff Volume = 0.948 af Average Runoff Depth = 4.43"
4.36% Pervious = 0.112 ac 95.64% Impervious = 2.455 ac

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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

Printed 11/5/2008

Page 16

Summary for Subcatchment 1:

Runoff = 2.93 cfs @ 12.07 hrs, Volume= 0.229 af, Depth= 4.46"

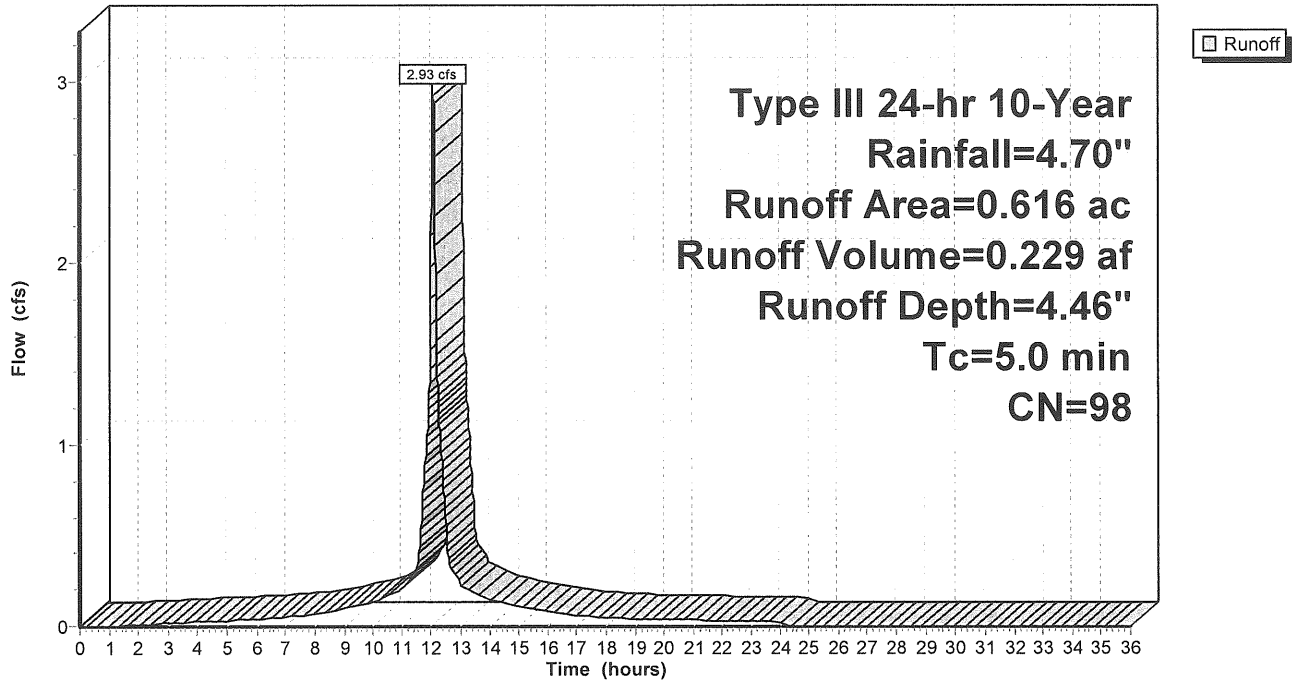
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.616	98	Paved parking & roofs
0.616		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 1:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

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Summary for Subcatchment 2:

Runoff = 2.90 cfs @ 12.07 hrs, Volume= 0.227 af, Depth= 4.46"

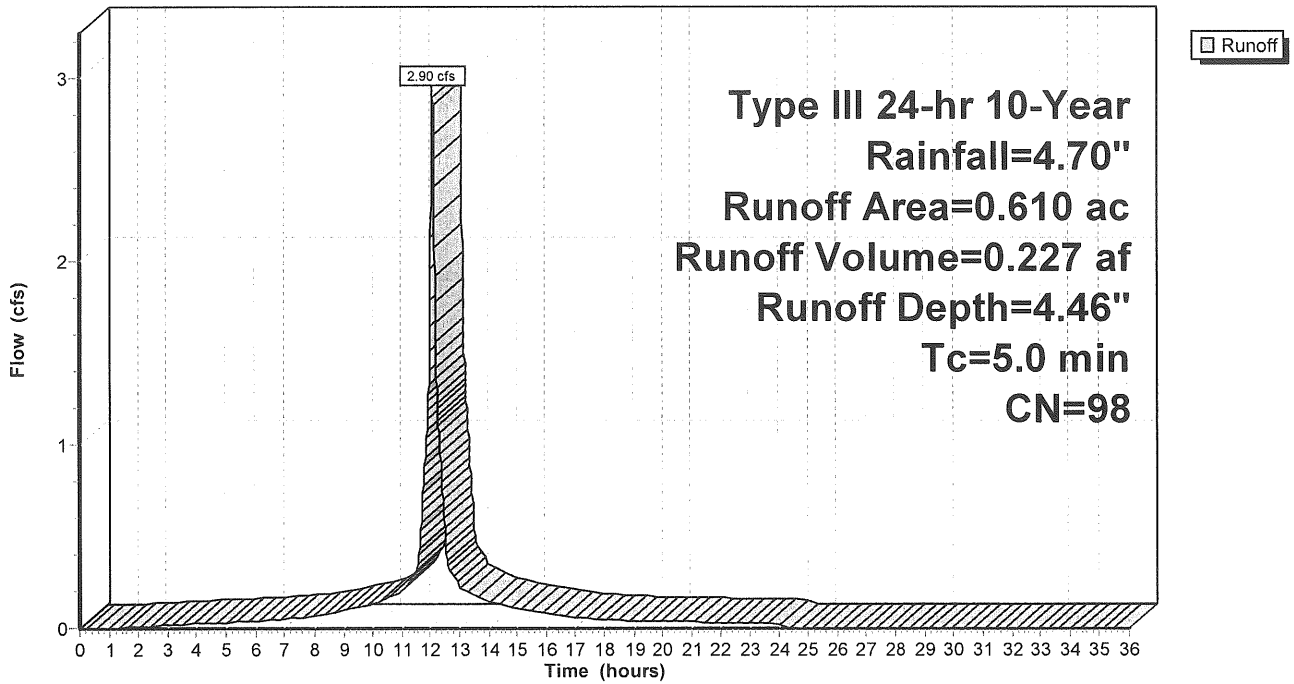
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.610	98	Paved parking & roofs
0.610		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 2:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

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Summary for Subcatchment 3:

Runoff = 2.18 cfs @ 12.07 hrs, Volume= 0.170 af, Depth= 4.46"

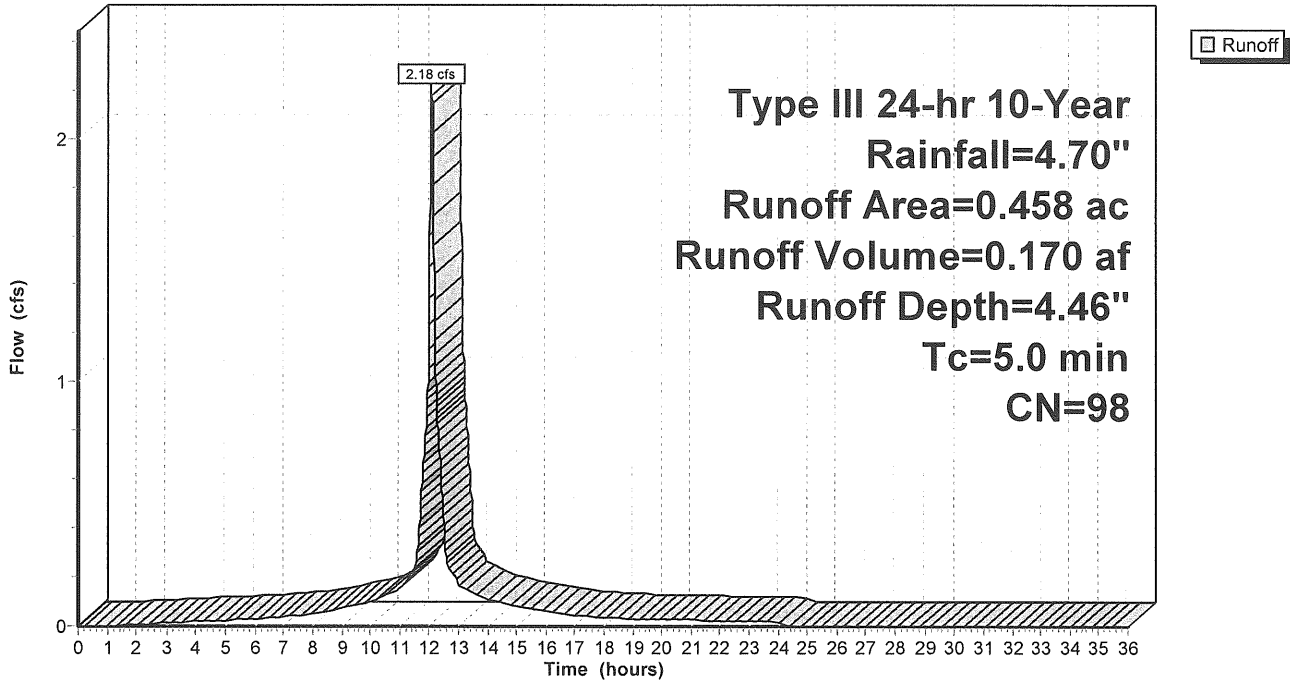
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.458	98	Paved parking & roofs
0.458		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 3:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

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Summary for Subcatchment 4:

Runoff = 2.35 cfs @ 12.07 hrs, Volume= 0.180 af, Depth= 4.35"

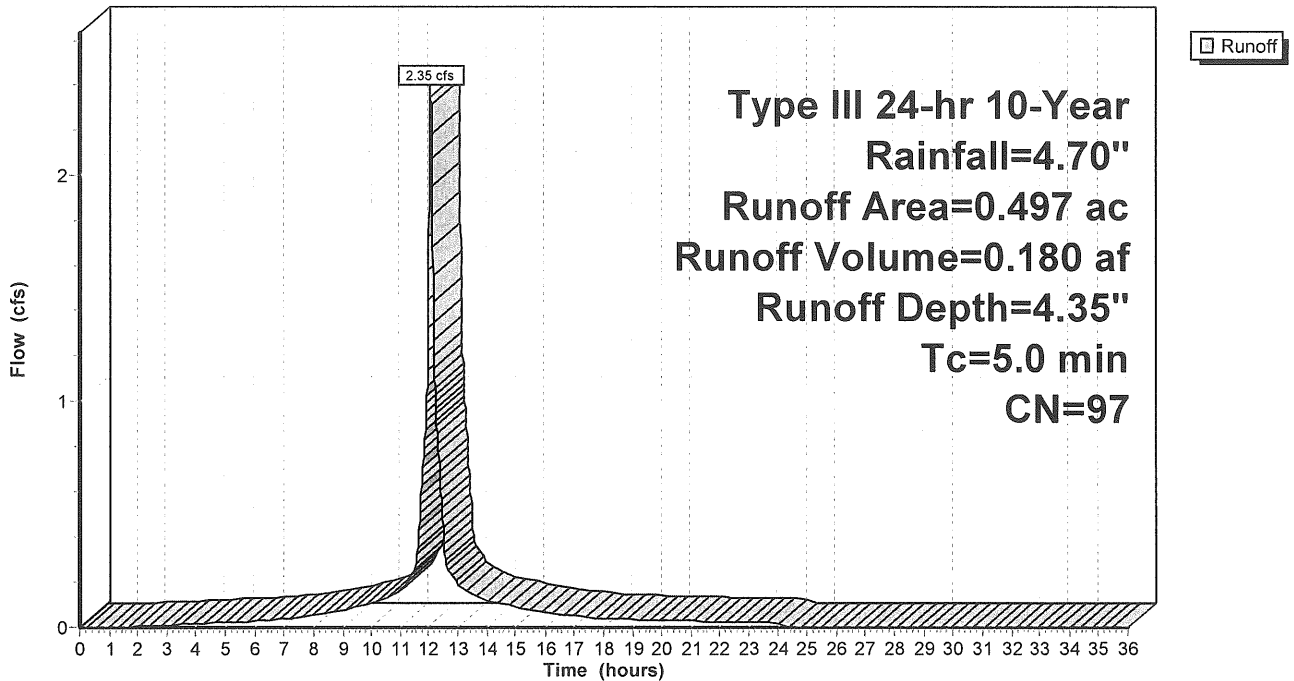
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.470	98	Paved parking & roofs
0.027	80	>75% Grass cover, Good, HSG D
0.497	97	Weighted Average
0.027		Pervious Area
0.470		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 4:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

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Summary for Subcatchment 5:

Runoff = 1.43 cfs @ 12.07 hrs, Volume= 0.112 af, Depth= 4.46"

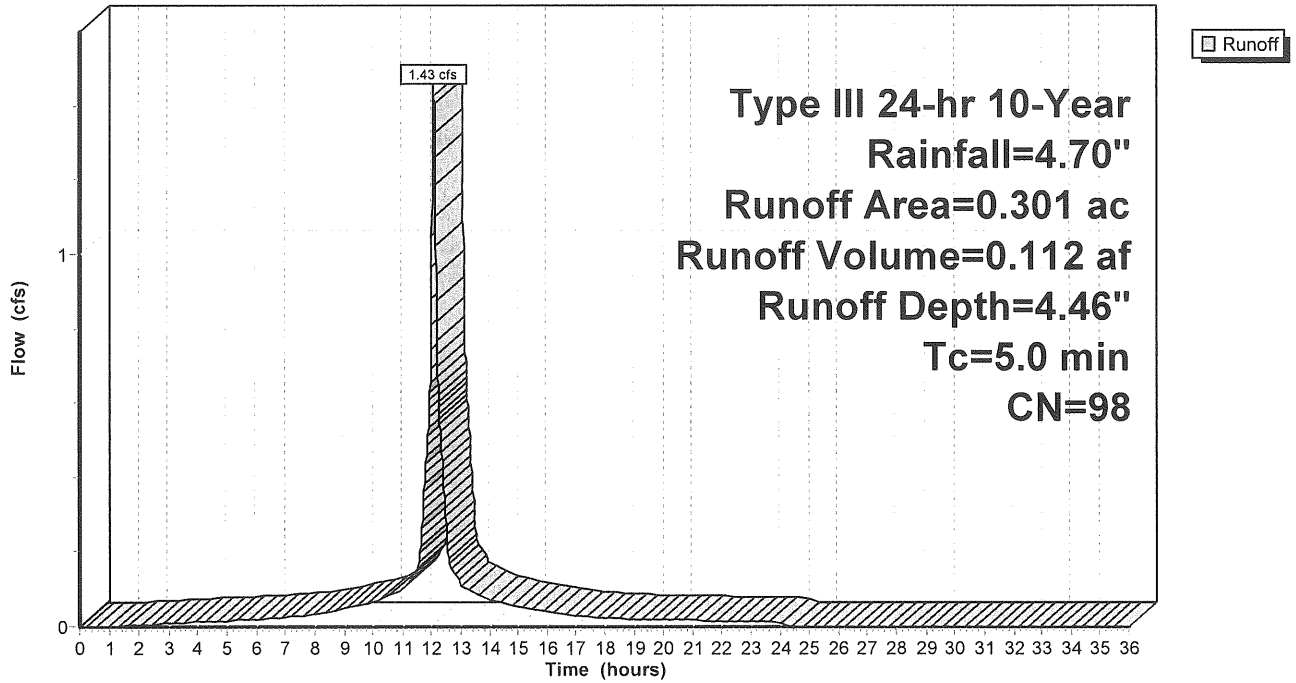
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
0.301	98	Paved parking & roofs
0.301		Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 5:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

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Summary for Subcatchment 6:

Runoff = 0.39 cfs @ 12.07 hrs, Volume= 0.029 af, Depth= 4.12"

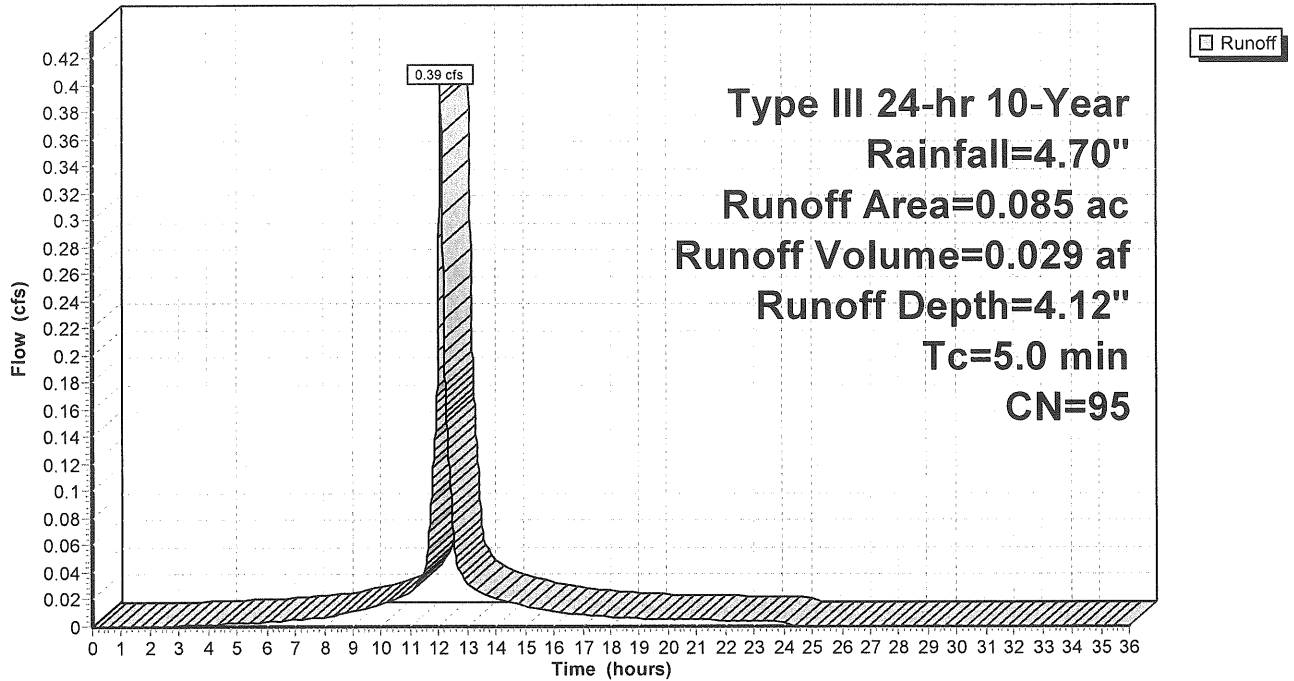
Runoff by SCS TR-20 method, UH=SCS, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.70"

Area (ac)	CN	Description
* 0.085	95	Gravel
0.085		Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, Minimum

Subcatchment 6:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

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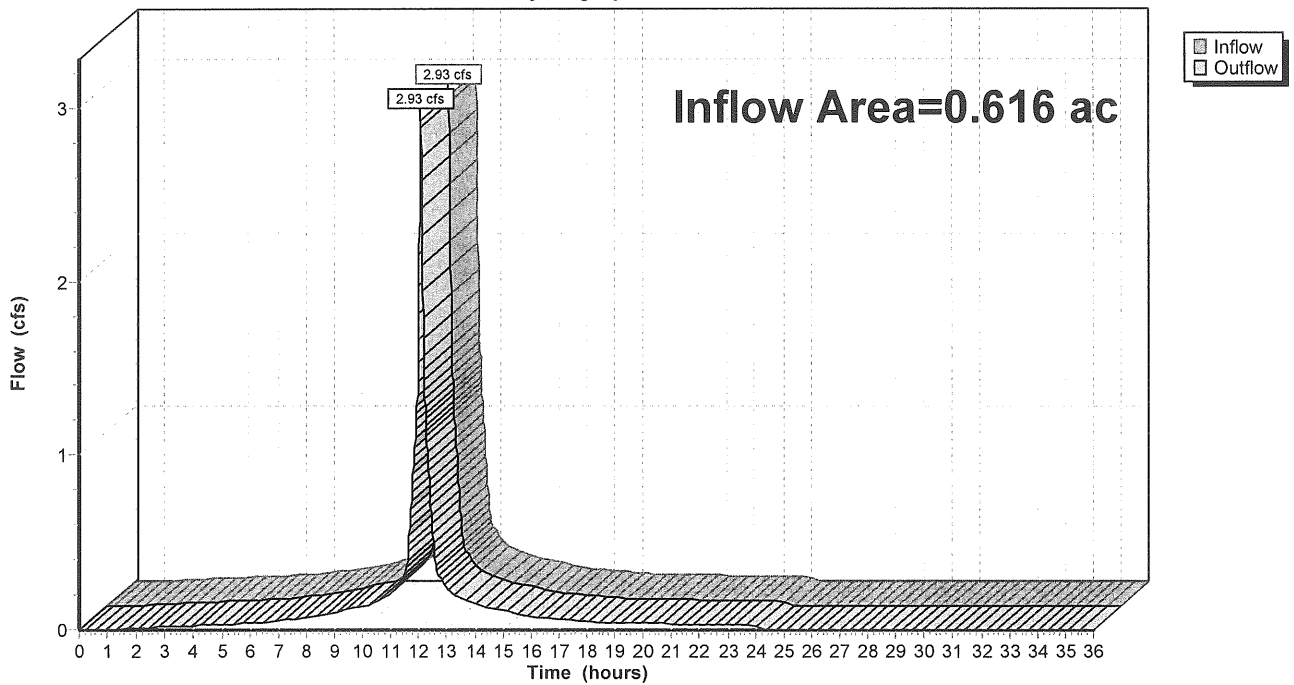
Summary for Reach SP1:

Inflow Area = 0.616 ac, 100.00% Impervious, Inflow Depth = 4.46" for 10-Year event
Inflow = 2.93 cfs @ 12.07 hrs, Volume= 0.229 af
Outflow = 2.93 cfs @ 12.07 hrs, Volume= 0.229 af, Atten= 0%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs

Reach SP1:

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

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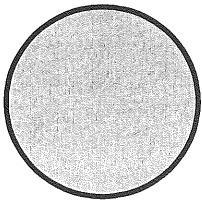
Summary for Reach SP2: Ex. CB (off site)

Inflow Area = 1.951 ac, 94.26% Impervious, Inflow Depth = 4.42" for 10-Year event
Inflow = 9.26 cfs @ 12.07 hrs, Volume= 0.719 af
Outflow = 3.93 cfs @ 11.94 hrs, Volume= 0.719 af, Atten= 57%, Lag= 0.0 min

Routing by Stor-Ind+Trans method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs
Max. Velocity= 5.38 fps, Min. Travel Time= 0.3 min
Avg. Velocity = 2.49 fps, Avg. Travel Time= 0.6 min

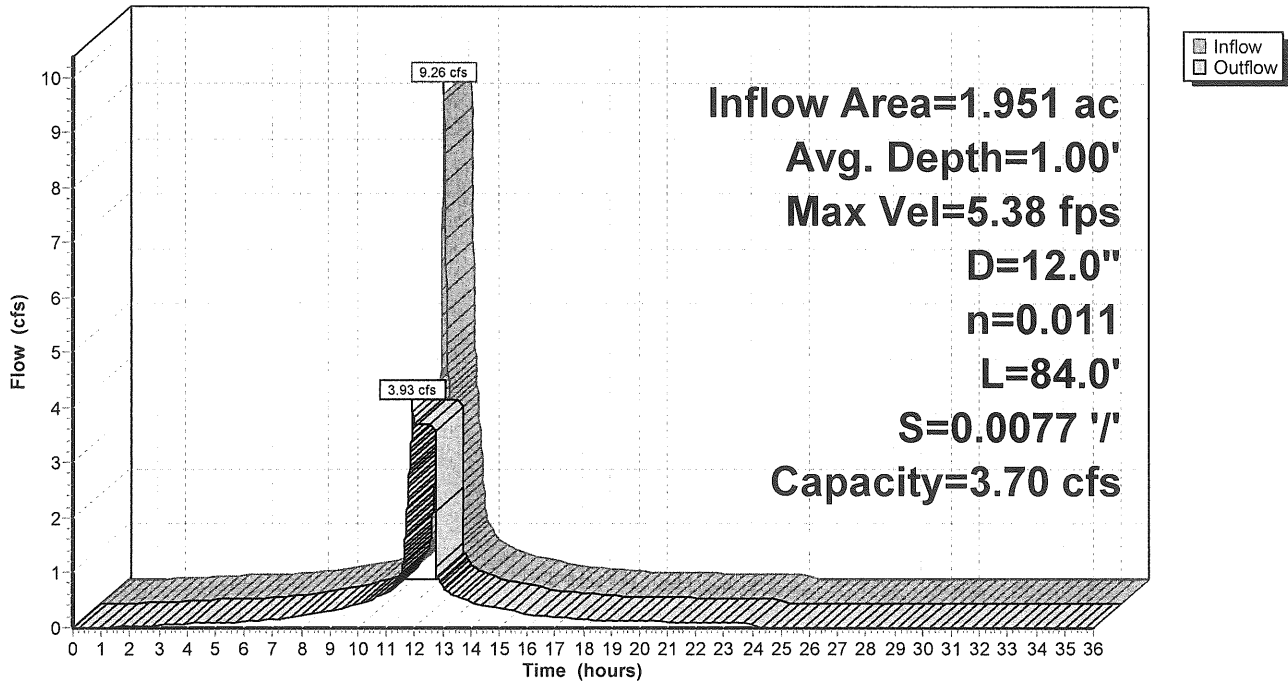
Peak Storage= 66 cf @ 11.95 hrs, Average Depth at Peak Storage= 1.00'
Bank-Full Depth= 1.00', Capacity at Bank-Full= 3.70 cfs

12.0" Diameter Pipe, n= 0.011 Concrete pipe, straight & clean
Length= 84.0' Slope= 0.0077 '/'
Inlet Invert= 8.00', Outlet Invert= 7.35'



Reach SP2: Ex. CB (off site)

Hydrograph



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Rufus Deering Lumber
Type III 24-hr 10-Year Rainfall=4.70"

Printed 11/5/2008

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Summary for Pond CB1:

Inflow Area = 1.153 ac, 92.63% Impervious, Inflow Depth = 4.44" for 10-Year event
Inflow = 5.48 cfs @ 12.07 hrs, Volume= 0.426 af
Outflow = 5.48 cfs @ 12.07 hrs, Volume= 0.426 af, Atten= 0%, Lag= 0.0 min
Primary = 5.48 cfs @ 12.07 hrs, Volume= 0.426 af

Routing by Stor-Ind method, Time Span= 0.00-36.00 hrs, dt= 0.01 hrs / 3

Peak Elev= 11.38' @ 12.07 hrs

Flood Elev= 12.25'

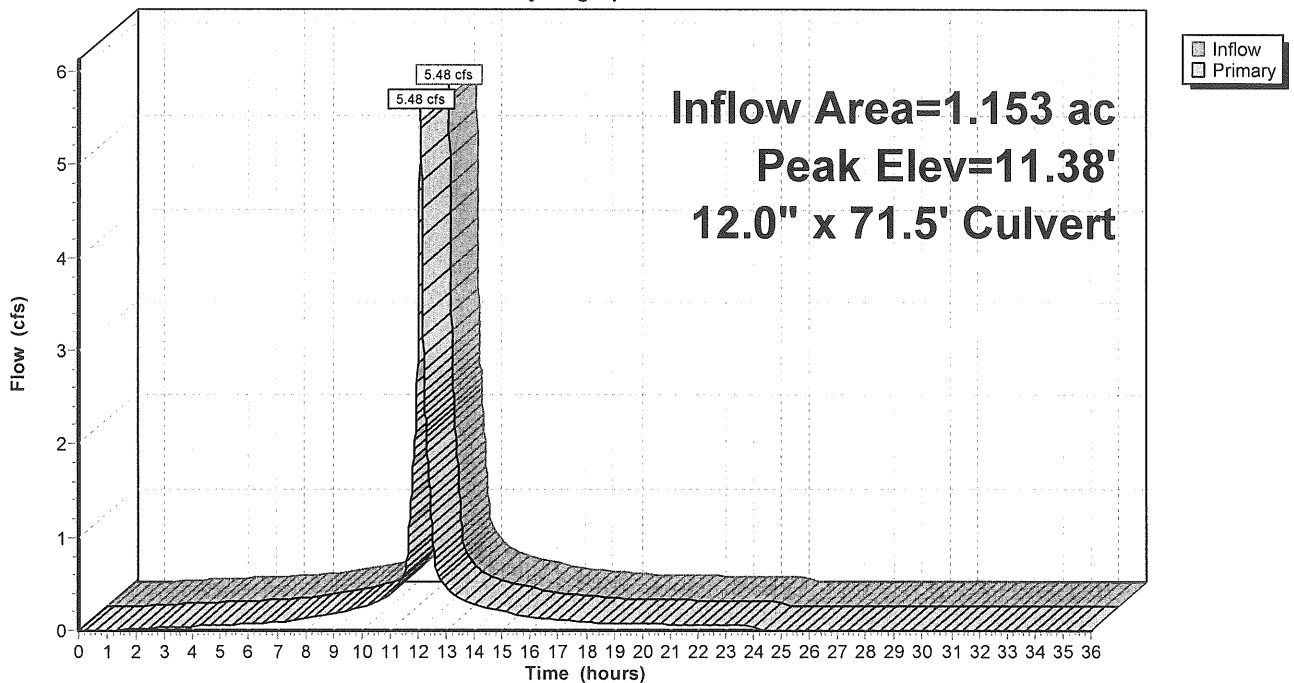
Device	Routing	Invert	Outlet Devices
#1	Primary	9.00'	12.0" x 71.5' long SD-1 RCP, groove end projecting, Ke= 0.200 Outlet Invert= 8.47' S= 0.0074 '/ Cc= 0.900 n= 0.010 PVC, smooth interior

Primary OutFlow Max=5.48 cfs @ 12.07 hrs HW=11.38' (Free Discharge)

↳1=SD-1 (Barrel Controls 5.48 cfs @ 6.97 fps)

Pond CB1:

Hydrograph



Rufus Deering Off Site Improvements
York Street Sidewalk Improvements (620 LF +/-)
Portland, Maine

STI Job# 07383
 September 19,2008

	Unit Cost	Subtotal(\$)	Total Costs
Sidewalk Breakdown			
Brick surface/base prep	\$85/ SY	\$29,325.00	
Granite Curbing	\$35/LF	\$21,700.00	
Trench pavement and base prep.	LS	\$2,250.00	
Drainage improvements	\$3000/ea	\$3,000.00	
Piping replaced	\$40/LF w 20lf	\$800.00	
Contractor/Equipment Mobil. W traffic control	6,000	\$6,000.00	
Erosion Control Measures	200	\$200.00	
Landscaping	Not Included		
Pavement markings/ lane striping	1,500	\$1,500.00	
Pedestrian Signals	2800/ea	\$11,200.00	
			\$75,975.00
Street Opening fees.	9,650	\$9,650.00	9,650.00
Sub Total			85,625.00
DESIGN/CONSTR ADMIN FEES		\$3,000.00	3,000.00
Base Estimated Project Cost for Applicant			88,625.00
10 % Contingency			\$8,862.50
Total Project Adjusted Budget Estimate			\$97,487.50

Estimate does not account for relocation of any CMP poles, or traffic arms if necessary.

Estimate does not include tranfer of materials excavated in St. which may be deemed hazardous and require testing or hauling to approved site.

Area of Sidewalk = 620ft(length) x 5 ft(width)= 3100SF=345 SY

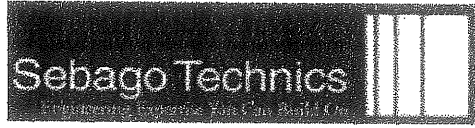
Design includes field survey of street/ and engineering design to match street profile

Street opening permit fees are approximate

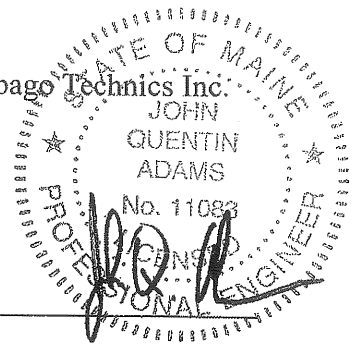
Proposed Policy (New Private Sector Construction)

Market Segment	Step 1 - January 1, 2009*	Step 2 - 2012*
Commercial/Industrial - Tier 1	<p>< 25,000 square feet = LEED NC Checklist</p>	<p>< 10,000 square feet = LEED NC Checklist</p>
Commercial/Industrial - Tier 2	<p>≥25,000 square feet = LEED Silver</p>	<p>≥10,000 square feet = LEED Silver</p>
Residential < 10 units Tier 1	<p>< 10 units = GreenPoint or LEED Checklist</p>	<p>< 10 units = GreenPoint or LEED Checklist</p>
Residential ≥ 10 units Tier 2	<p>≥10 units = GreenPoint Rated 50 Points or LEED Certified</p>	<p>≥ 10 units = GreenPoint Rated 50 points or LEED Certified</p>
High Rise Residential (75' or higher)	<p>LEED Certified</p>	<p>LEED Silver</p>

*Applies to New Planning Applications



Project: 07383
To: Molly P. Casto, Senior Planner, City of Portland
From: John Q. Adams, P.E., Senior Transportation Engineer, Sebago Technics Inc.
Date: November 5, 2008
Subject: **Trip Generation Calculations**
Proposed Rufus Deering Redevelopment
Commercial Street, Portland



The purpose of this memo is to establish the net new trip-ends that will be generated by the proposed Rufus Deering redevelopment project. It is important to note the primary reasons for the redevelopment, which are to modernize program space and increase the efficiency of how the limited available site space is utilized. The existing site is limited in usable space without any practical alternatives to increase the size by acquiring more property. The intent of this redevelopment is not to significantly increase site generated trips, rather, to remain a viable option in the highly competitive retail lumber industry. As such, there is no increase in current employment levels planned as part of this redevelopment. In fact, with the current economic climate there will likely be a slight reduction in current employment levels. A letter from Rufus Deering Lumber is enclosed which states their intention to not increase employment levels as part of the redevelopment.

With the primary reasons for the project as stated above, the difficulty is in accurately estimating the expected increase in trip generation associated with the redevelopment. The Institute of Transportation Engineer's (ITE) Trip Generation Manual, calculates trip generation for Building Materials and Lumber store, Land Use Code 812, by both building square footage and number of employees. A reasonable procedure to estimate net new trips would be to average the two ITE methods. Calculating by new building square footage would account for an increase in trips which could result from a modernized and more efficient site layout, while calculating trips by the number of employees would account for the fact that employees levels are not changing or may be reduced. As part of the redevelopment, existing warehouse space will be converted to 10,648 s.f. of retail store and showroom use. The new retail space was used to calculate new trips based on building square footage. There will be no new employees as part of this redevelopment.

Table 1, on Page 2, summarizes net new trip generation based on averaging the ITE two methods.

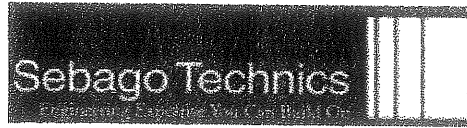


Table 1
 Net New Trip Generation
 Rufus Deering Lumber Redevelopment

Time Period	By Building SF			By Employees			Average
	Expansion SF	Rate	Subtotal	# New Employees.	Rate	Subtotal	
Weekday	10,648	45.16	481	0	32.12	0	240
AM Peak Site	10,648	4.16	44	0	3.94	0	22
PM Peak Site	10,648	5.56	59	0	3.83	0	30
AM Peak 7-9 am	10,648	2.60	28	0	2.42	0	14
PM Peak 4-6 pm	10,648	4.49	48	0	2.77	0	24
Saturday	10,648	51.60	549	0	36.69	0	275
Saturday Peak	10,648	9.58	102	0	5.23	0	51

As can be seen from the above Table 1, the redevelopment will not produce a significant amount of net new trips and will not need a MDOT Traffic Movement Permit (TMP). Projects that generate a minimum of 100 new trips in their peak hours are required to receive a MDOT TMP.

In summary the site will produce 14 and 24 new trips during the AM and PM peak hours of the adjacent roadways, respectively.

JQA:jqa/df

2008

November 7,

Molly P. Casto, Senior Planner, city of Portland

The primarily reasons for the redevelopment of the Rufus Deering Lumber Co. site is to improve efficiency and safety by the development of the proposed drive-thru lumber yard. It has become very difficult to compete and be profitable working out of this very antiquated yard. Our yearly losses due to cull lumber, boards that become weathered, split and unusable, is much higher than the national average. The only way to combat this is to become much more efficient and get all of our product undercover. With the newly designed yard this will reduce the number of employees and also improve the safety of our employees and customers.

Sincerely,

A handwritten signature in black ink, appearing to read "Dan LaBrie", with a horizontal line extending to the right.

Dan LaBrie VP General Manager

Att. 1-g

07383

**Public Notice
Neighborhood Meeting**

Please join us for a neighborhood meeting to discuss our development plans for:

Rufus Deering Company Proposed warehouse facility (29,000 square feet) remodeling of the existing storage barn for office, showroom and retail, located at 383 Commercial Street, Portland, Maine.

Date: October 6, 2008

Time: 5:30 PM

Location: Becky's Diner
390 Commercial Street, 2nd floor
Portland, Maine 04101

If you have questions, please call James Seymour, Sebago Technics, Inc., (856-0277), or Jay Breard, Rufus Deering Lumber Company (772-6505).

Sincerely,

Daniel LaBrie, Vice President
Rufus Deering Lumber Company
383 Commercial Street
Portland, ME 04101

NOTE: Under Section 14-32© of the City Code of Ordinances, an applicant for a major development, subdivision of over five lots/units, or zone change is required to hold a neighborhood meeting at least seven days prior to the Planning Board public hearing on the proposal.

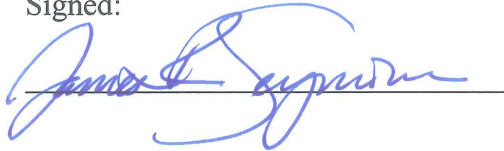
07383

Neighborhood Meeting Certificate

I, James Seymour, hereby certify that a neighborhood meeting was held on October 6, 2008 at the 2nd floor meeting room at Becky's Diner located at 390 Commercial Street, Portland, Maine, at 5:30 PM.

I also certify that on September 29, 2008 invitations were mailed to all addresses on the mailing list provided by the Planning Division, including property owners within 500 feet of the proposed development and all residents on the "interested parties" list.

Signed:



This the 7th day of October, 2008

Attached to this certification are:

- 1) Copy of the Invitation Sent
- 2) Sign-in Sheet
- 3) Meeting Minutes



07383

TO: Molly Castor, Planner
FROM: Jim Seymour P.E., Sebago Technics, Inc.
RE: Rufus Deering Lumber Co., 383 Commercial St., Neighborhood Meeting Minutes
DATE: October 7, 2008

Neighborhood Meeting Minutes:

On October 6, 2008 a Neighborhood Meeting discussing the site improvements proposed by Rufus Deering Lumber Co. at 383 Commercial Street. The meeting was held at Becky's Diner's meeting room at 390 Commercial Street

The following were in attendance representing the project for Rufus Deering Lumber Co.:

Dan LaBrie	--	Vice President	Rufus Deering Lumber Co.
Jay Breard	--	Controller	Rufus Deering Lumber Co.
James Seymour, PE	--	Civil Engineer	Sebago Technics Inc

The following were in attendance from the general public:

Mr. & Mrs. Anthony Mezoian	85 York Street
Paul Becker	75 York Street

Mr. Labrie described the basic function of the site and need to expand the facilities and how the warehouse is a necessary part of the Company's need to survive in a competitive marketplace. The abutters were told that the proposed warehouse would displace the current Quonset hut and older shelter. Mr. Seymour explained that the access will be gated and will be locked each night. No one objected regarding circulation or business hours.

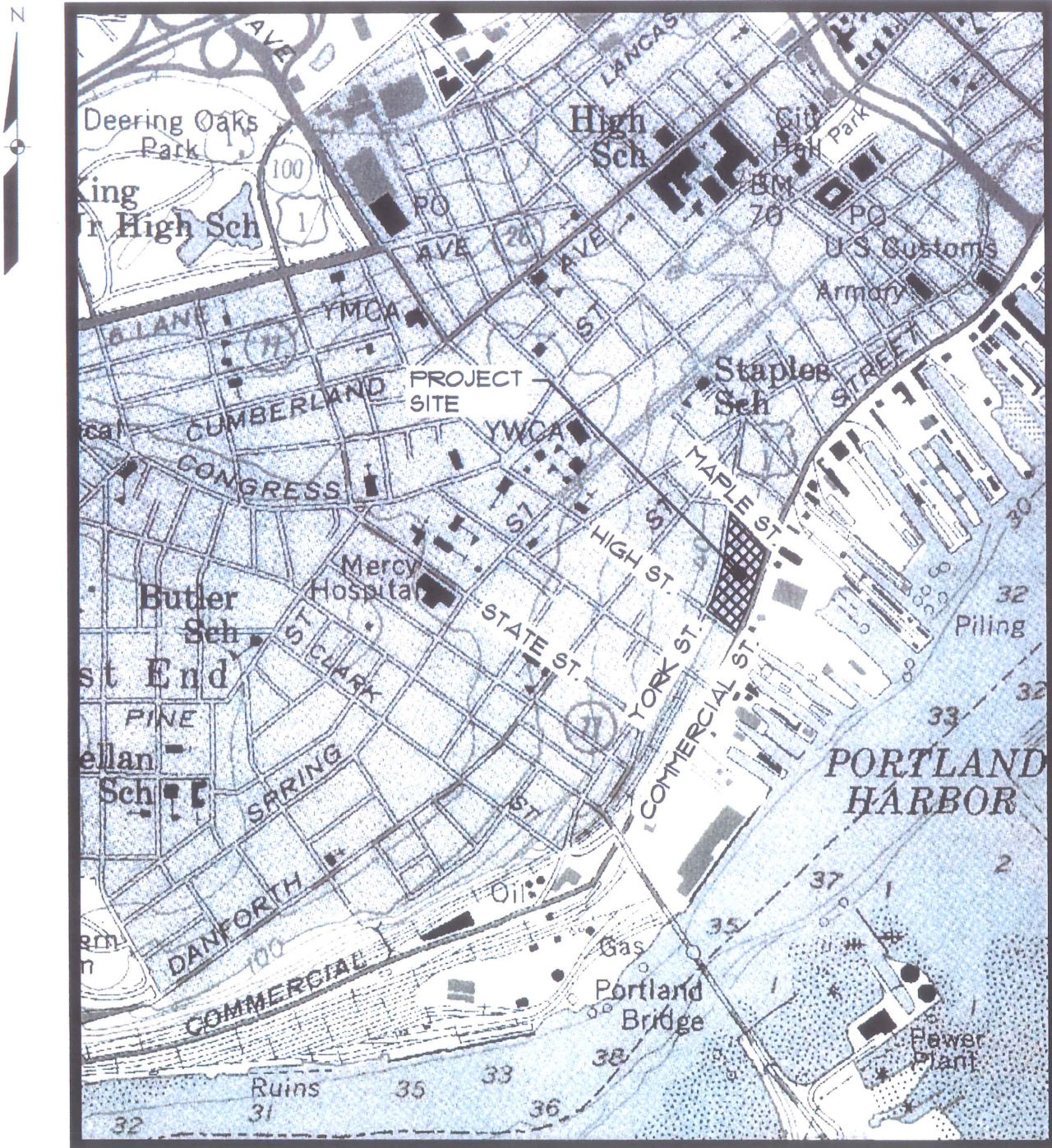
Mr. Seymour discussed the sites drainage, sidewalk and traffic improvements. All in attendance were aware of Rufus Deering's historical presence and had only concerns of proposed height. Most felt that the improvements on York Street were excessive, and agreed that the warehouse is an improved enhancement for the property.

Mr. & Mrs. Anthony Mezoian and Mr. Becker raised questions regarding the proposed building height. Mr. Labrie and Mr. Seymour both stated that the height could exceed the height of the existing shelters but that the height would not be much higher than a one story, structure on York Street. All felt that this would not be a burden, and with the preserving of trees along the street edge it would be a good buffer, and the height would not impact their view of the harbor from their upper floors of their properties' structures.

They offered support and wished Rufus Deering success.
The meeting adjourned at 6:30 PM.

JRS:jrs/kn

FIGURE 1



SITE LOCATION MAP

USGS TOPOGRAPHIC

7.5 MIN. QUADRANGLE

PORTLAND WEST

SCALE: 1"=1000'

Sebago Technics

Engineering Expertise You Can Build On

One Chabot Street
Westbrook, Me 04098-1339
Tel (207) 856-0277





225 DOUGLASS STREET
P.O. BOX 3553
PORTLAND, ME 04104-3553
P: 207.774.5961
F: 207.523.5440
WWW.PWD.ORG

Att. 1-e
0708

PORTLAND
WATER DISTRICT

RECEIVED

June 4, 2008

Sebago Technics
One Chabot Street
Westbrook, ME 04098-1339

Attn: Patrick M. Martin, Design Engineer
Re: Rufus Deering Lumber, 383 Commercial Street, Portland
Ability to serve with PWD water

Dear Mr. Martin:

This letter is to confirm that there should be an adequate supply of clean and healthful water to serve the needs of the commercial lumber yard at 383 Commercial Street in Portland. According to District records, there is a 12-inch ductile iron water main on the southwest side of High Street as well as 4 hydrants located adjacent to the site.

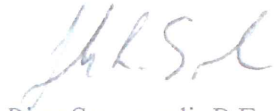
The current data from a nearby hydrant indicates there should be adequate capacity of water to serve the needs of your proposed project, as stated in your letter, dated May 29, 2008.

Hydrant Location: Commercial Street, 225' north of High Street
Hydrant Number: POD-HYD00062
Static Pressure: 109 psi
Flow: 1635 gpm
Last Tested: 4/27/2004

Please notify your mechanical engineer of these results so that they can design your system to best fit the noted conditions. Because the static pressure at the main is above the limit for which the Maine State Plumbing Code recommends installing pressure reducing valves, PWD recommends consulting with your mechanical engineer to determine if they should be used on your project.

Because there is an existing hydrant on the southwest corner of High Street and York Street, it is unnecessary to install another hydrant as shown on your Grading and Utilities Plan, unless requested by the Portland Fire Department. Also, the 3/4-inch PVC domestic service is not acceptable in the public right-of-way. PWD requires a minimum of 1-inch copper to be installed to the service valve, located 6 inches from the property line in the public right-of-way. PVC pipe is an acceptable material to use on private land, from the service valve to the building. Any existing services to this property that are not going to be reused must be shut and cut at the main. If the District can be of further assistance in this matter, please let us know.

Sincerely,
Portland Water District

A handwritten signature in black ink, appearing to read 'R. Spugnardi', written in a cursive style.

Rico Spugnardi, P.E.
Business Development Engineer
rspugnardi@pwd.org

958

Att. 1-f
x NOTE
ETALS 8
START 96298

QUITCLAIM DEED WITH COVENANT

KNOW ALL BY THESE PRESENTS, That I, WILLIAM M. MOODY, JR., of Yarmouth, Cumberland County, Maine, for consideration paid, grant to MILLIKEN SMITH BLOCK, LLC, a Maine limited liability company, with a mailing address of 383 Commercial Street, Portland, Maine 04101, with Quitclaim Covenant, a one-tenth (1/10) interest in common and undivided, in and to a certain lot or parcel of land situated in Portland, Cumberland County, Maine, described more particularly as follows:

Beginning at the intersection of the northwesterly sideline of Commercial Street with the southwesterly sideline of Maple Street;

Thence northwesterly in said southwesterly sideline of Maple Street a distance of one hundred ten (110) feet, more or less, to the easterly corner of a strip of land fifteen (15) feet wide conveyed by the Portland Terminal Company to the Family Laundry, Inc. by deed dated May 26, 1938, recorded in the Cumberland County Registry of Deeds in Book 1555, Page 24, said strip of land to be used as a passageway in common with the Portland Terminal Company, its successors and assigns forever;

Thence southwesterly in the southeasterly sideline of said strip of land a distance of eighty-five (85) feet to the southerly corner of said strip of land, said southerly corner being also the southerly corner of land conveyed by the Portland Terminal Company to said Family Laundry, Inc. by said deed dated May 26, 1938;

Thence northwesterly in said southwesterly sideline of said Family Laundry land a distance of ninety-five (95) feet, more or less, to the southeasterly sideline of York Street;

Thence southwesterly in said southeasterly sideline of said York Street five hundred sixteen (516) feet, more or less, to the northeasterly sideline of High Street;

Thence southeasterly in said northeasterly sideline of High Street a distance of one hundred sixty-two (162) feet, more or less, to the westerly corner of a triangle of land conveyed by Boston and Maine Railroad, predecessor in title to the Portland Terminal Company, to the City of

MAP 42
LOT 1

Portland by deed dated May 21, 1874 and recorded in said Registry of Deeds in Book 410, Page 473;

Thence northeasterly in the northwesterly sideline of said triangle of land a distance of ninety-eight (98) feet, more or less, to its intersection with said northwesterly sideline of Commercial Street;

Thence northeasterly in said northwesterly sideline of said Commercial Street four hundred seventy (470) feet, more or less, to the point of beginning.

The Grantor hereby also conveys to the Grantee, its successors and assigns, any right, title or interests she may have to lands abutting the above described parcel of land between the northeasterly sideline of High Street and the centerline of High Street and between the northwesterly sideline of Commercial Street and the centerline of Commercial Street.

ALSO conveying to the Grantee herein, all rights of the Grantor herein to use the passageway located on the strip of land as described in a deed from the Portland Terminal Company to Family Laundry, Inc., dated May 26, 1938 and recorded in the Cumberland County Registry of Deeds in Book 1555, Page 24.

Being the same premises conveyed by Rufus Deering Company to William M. Moody, Jr., et als. by deed dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 82.

This conveyance is made SUBJECT TO:

1. Mortgage and Security Agreement from William M. Moody, Jr., et als. to Rufus Deering Company dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 85.
2. Memorandum of Lease by and between William M. Moody, Jr., et als. (Lessor) and Rufus Deering Company (Lessee) dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 91.
3. Agreement by and between William M. Moody, Jr., et al. and Rufus Deering Company, et al. dated April 2, 1990 and recorded in the Cumberland County Registry of Deeds in Book 9215, Page 93. Pursuant

to Paragraph 3 of said Agreement, unanimous written consent to this conveyance has been obtained from all parties to said Agreement.

Witness my hand and seal this 1st day of Decemb, 2004.

Witness:

Jean M Rolland

William M. Moody, Jr.
William M. Moody, Jr.

STATE OF MAINE
COUNTY OF CUMBERLAND, ss.

On Dec 12th, 2004, personally appeared the above-named William M. Moody, Jr. and acknowledged the foregoing instrument to be his free act and deed.

Before me,

Patricia A. Merritt
Notary Public

Printed Name:

PATRICIA A. MERRITT

Notary Public, Maine

My Commission Expires:

My Commission Expires April 1, 2010

SEAL

Received
Recorded Register of Deeds
Mar 30, 2005 03:30:49P
Cumberland County
John E O'Brien

BK 14115PG013

61271

WARRANTY DEED

YORK STREET, INC., a Maine corporation with a mailing address of P.O. Box 2518, South Portland, Maine 04116, for consideration paid, GRANTS to RUFUS DEERING COMPANY, a Maine corporation with a mailing address of 383 Commercial Street, Portland, Maine 04101, with WARRANTY COVENANTS, that certain lot or parcel of land situated in the City of PORTLAND, County of CUMBERLAND and State of MAINE, and more particularly described on EXHIBIT A attached hereto and made a part hereof.

Being the same premises described in a deed from York Street Associates to York Street, Inc., of even or recent date herewith to be recorded.

IN WITNESS WHEREOF, the said YORK STREET, INC. has caused this instrument to be signed on its behalf by WERNER SCHNETZER its duly authorized President on this 1st day of September, 1998.

YORK STREET, INC.

By:

Werner Schnetzer
Werner Schnetzer
Its: President

MAINE REAL ESTATE TAX PAID

STATE OF MAINE
COUNTY OF CUMBERLAND

September 1, 1998

Personally appeared the above named Werner Schnetzer in his capacity as President of YORK STREET, INC. and acknowledged the foregoing instrument to be his free act and deed in his said capacity and the free act and deed of said corporation.

Before me,

James S. Buckner
Notary Public/Attorney-at-Law
Print Name: *James S. Buckner*

122895/PSB/SKLF
91401.4926

MAP 42
LOT 7 & 8

BK 415PG014

EXHIBIT A

A certain lot or parcel of land, with the buildings thereon, situated on York Street in the City of Portland, County of Cumberland and State of Maine, bounded and described as follows:

Beginning at the point of intersection of the southeast sideline of York Street and the southwesterly sideline of Maple Street; thence southerly along said sideline of Maple Street one hundred twenty-five (125) feet; thence westerly on a line at right angles with the last mentioned line eighty-five (85) feet to a point; thence northwesterly on a line parallel with the line of Maple Street eighty (80) feet, more or less, to the aforesaid line of York Street; thence northeasterly along said line of York Street one hundred (100) feet, more or less, to the point of beginning; containing 8,712 square feet, more or less.

There is also conveyed a strip of land to be used as a passageway in common with Portland Terminal Company adjacent to the southeasterly sideline of the above-described premises commencing at said line of Maple Street and extending southwesterly with a uniform width of fifteen (15) a distance of eighty-five (85) feet.

RECEIVED
RECORDED REGISTRY OF DEEDS

1998 SEP -1 PH 4: 48

CUMBERLAND COUNTY

John B O'Brien

RUFUS DEERING LUMBER

AH.15

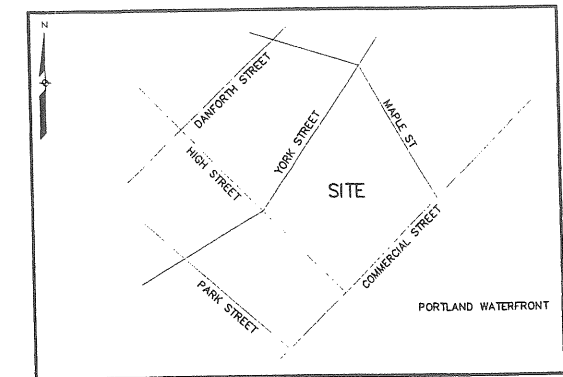
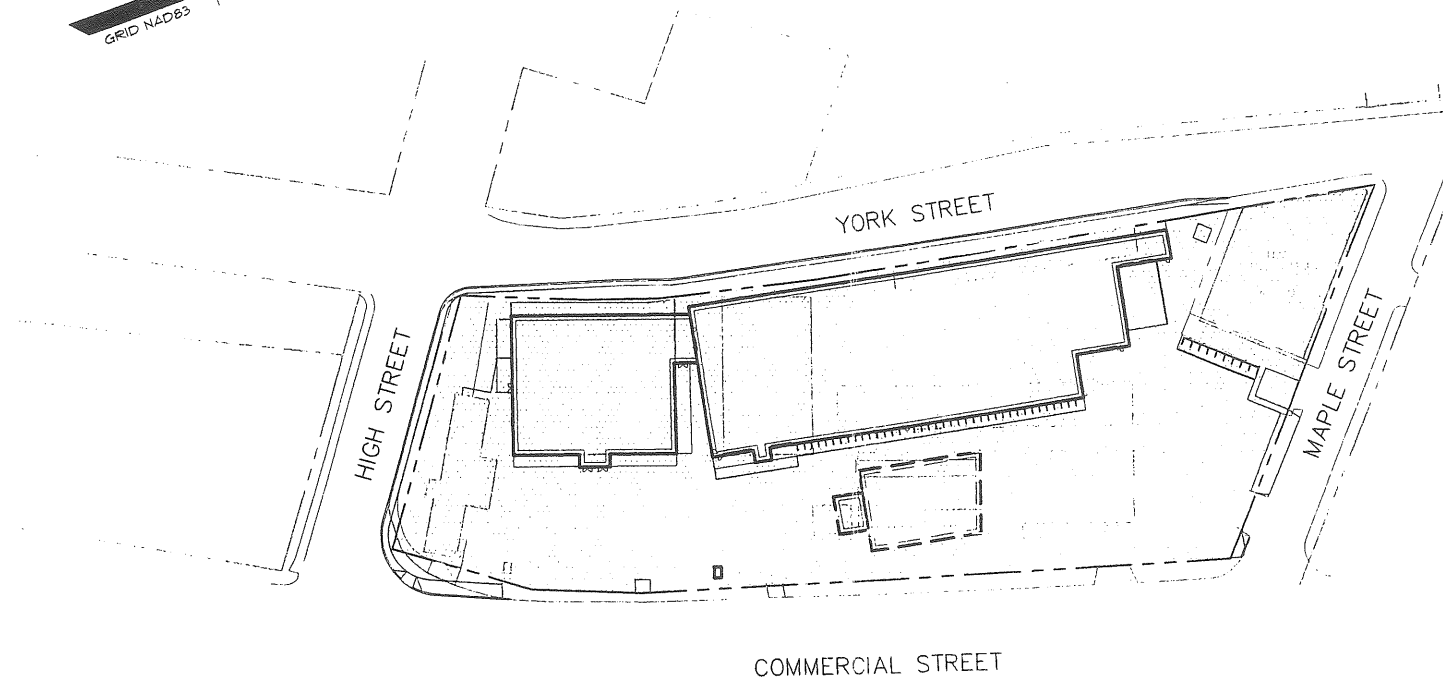
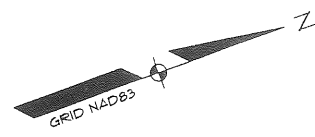
383 COMMERCIAL STREET
PORTLAND, MAINE

OWNER/APPLICANT:
RUFUS DEERING LUMBER
383 COMMERCIAL STREET
PORTLAND, MAINE

ENGINEER / SURVEYOR:

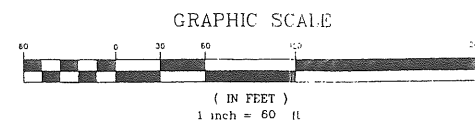

Sebago Technics
Engineering Expertise You Can Build On
ONE CHABOT STREET
WESTBROOK, ME 04098-1339
TEL (207) 856-0277

ARCHITECT:
JOHNSON DESIGN SERVICES
420 RAY STREET, UNIT 26
PORTLAND, MAINE



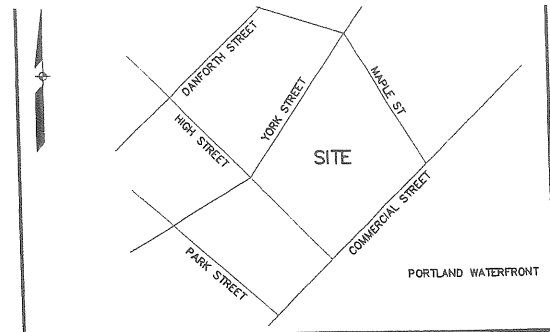
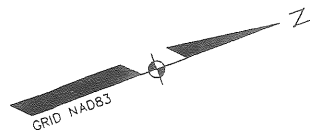
LOCATION MAP

N.T.S.



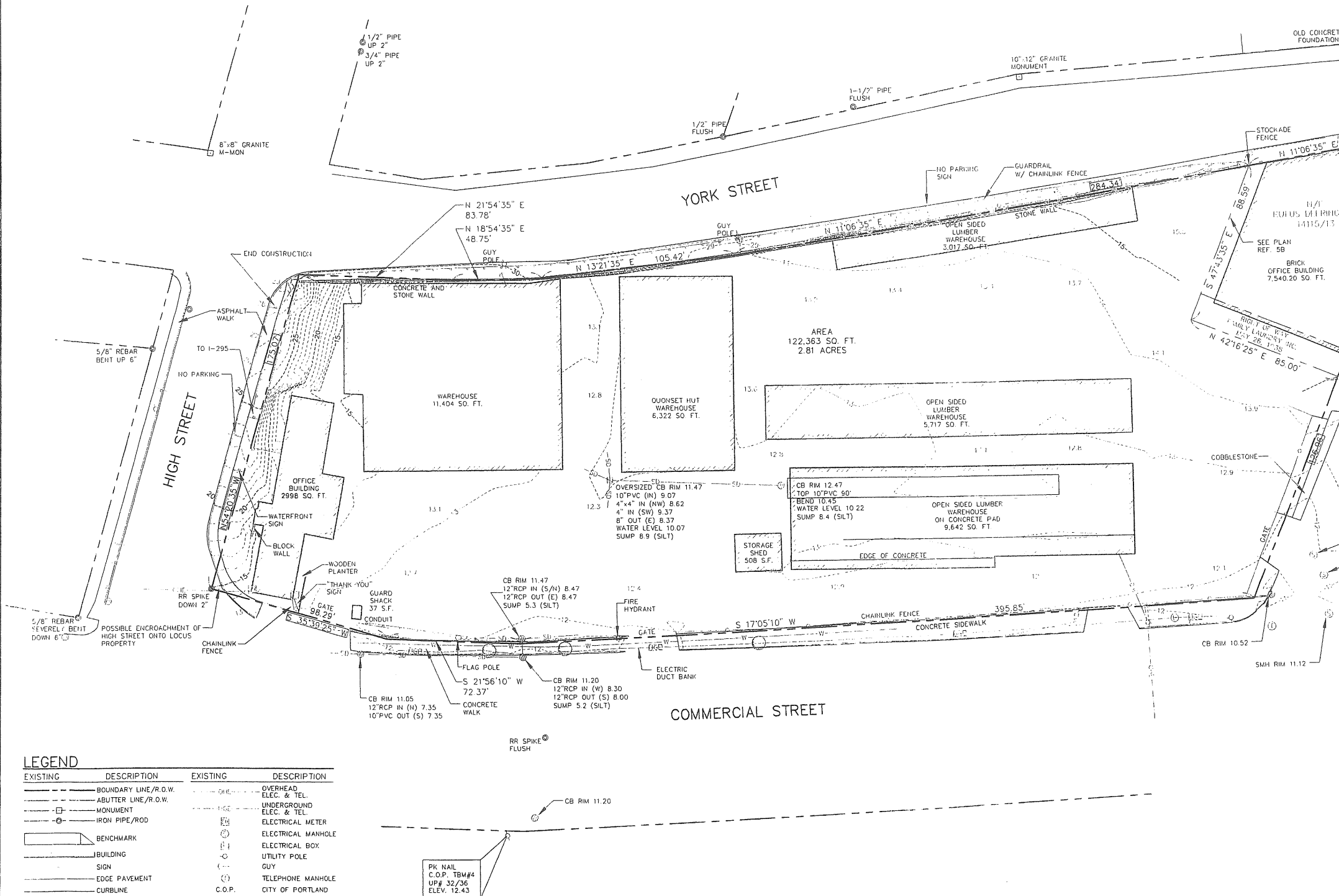
SHEET INDEX:

SHEET	DESCRIPTION
1	COVER SHEET
2	EXISTING CONDITIONS
3	DEMOLITION PLAN
4	SITE PLAN
5	GRADING AND UTILITIES PLAN
6	LANDSCAPE PLAN
7	DETAILS
8	DETAILS
9	DETAILS



LOCATION MAP

N.T.S.



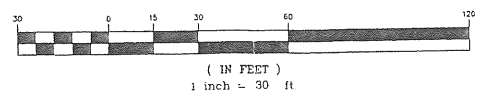
GENERAL NOTES

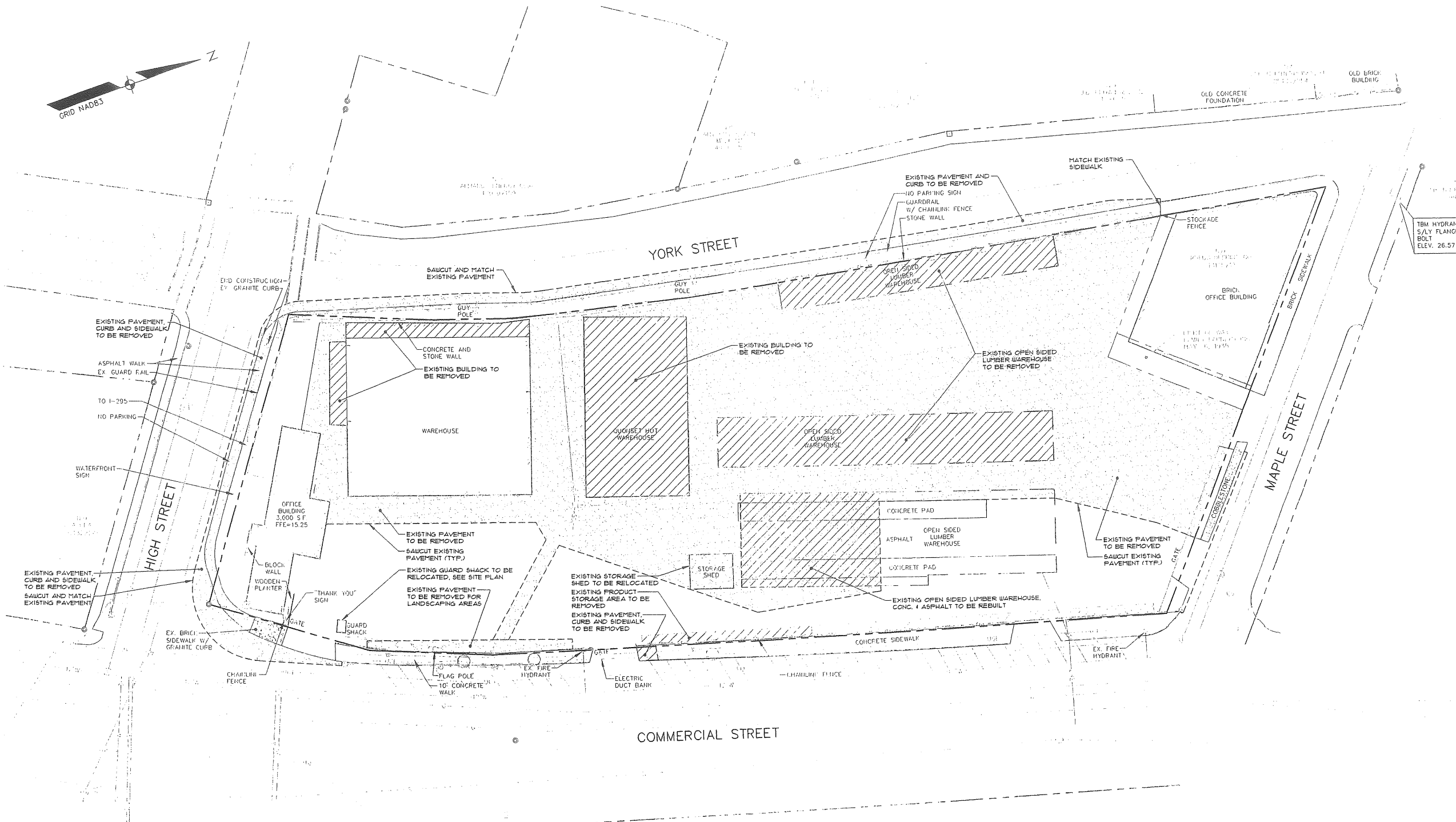
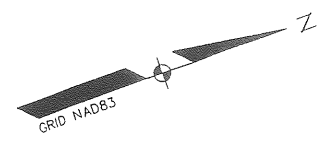
- 1) THE RECORD OWNER OF THE LOT OR PARCEL IS MILLIKEN SMITH BLOCK, LLC AS RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS BOOK 22473, PAGE 298 THRU 307.
- 2) THE PROPERTY IS DEPICTED ON THE CITY OF PORTLAND TAX MAP 42, AS BLOCK A, LOTS 1, 7, AND 8.
- 3) THE BASIS OF BEARINGS IS MAINE STATE PLANE GRID, MAINE WEST ZONE, NORTH AMERICAN DATUM 1983(NAD83), AND REFERENCED BY REAL TIME GLOBAL POSITIONING SYSTEM RECEIVERS. THE VERTICAL DATUM IS CITY OF PORTLAND DATUM REFERENCED TO A PK NAIL IN UTILITY POLE #CMP 32/NETT 36.
- 4) THE CONTOURS SHOWN ARE ONE FOOT CONTOUR INTERVAL, BASED UPON A FIELD SURVEY COMPLETED BY SEBAGO TECHNICS AND CITY OF PORTLAND AERIAL MAPPING DATED 2000.
- 5) PLAN REFERENCES:
 - A) STANDARD BOUNDARY AND EXISTING CONDITIONS SURVEY A.G.A. REALTY TRUST, PREPARED BY OWEN HASKELL, INC. DATED OCTOBER 2 1995, RECORDED IN SAID REGISTRY PLAN BOOK 195 PAGE 380.
 - B) PLAN OF LAND FOR RUFUS DEERING COMPANY, PREPARED BY OWEN HASKELL, INC. DATED JANUARY 9, 1989.
 - C) CITY OF PORTLAND PLAN OF CHANGE OF LINE CORNER OF HIGH STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 366/9.
 - D) CITY OF PORTLAND PLAN OF HIGH STREET BETWEEN YORK STREET AND COMMERCIAL STREET, CITY OF PORTLAND PUBLIC WORKS RECORDS 668/5 DATED 1969.
 - E) PLAN OF PROPERTY FOR CITIES SERVICE OIL CO., PREPARED BY H.I. AND E.C. JORDAN DATED FEBRUARY 9, 1960, CITY OF PORTLAND PUBLIC WORKS RECORDS 755/3.
- 6) SEBAGO TECHNICS, INC. AND THIS SURVEYOR MAKE NO CERTIFICATION TO THE LOCATION OF UTILITIES, UTILITY POLES, MANHOLES, AND OTHER STRUCTURES SHOWN HEREON ARE FOR INFORMATIONAL PURPOSES ONLY. IT SHOULD BE NOTED THAT ELECTRICAL, GAS, TELEPHONE, AND OTHER SERVICES SHOULD BE LOCATED BY THE APPROPRIATE UTILITIES PRIOR TO ANY CONSTRUCTION ACTIVITY.

LEGEND

EXISTING	DESCRIPTION	EXISTING	DESCRIPTION
---	BOUNDARY LINE/R.O.W.	---	OVERHEAD ELEC. & TEL.
---	ABUTTER LINE/R.O.W.	---	UNDERGROUND ELEC. & TEL.
---	MONUMENT	---	ELECTRICAL METER
---	IRON PIPE/ROD	---	ELECTRICAL MANHOLE
---	BENCHMARK	---	ELECTRICAL BOX
---	BUILDING	---	UTILITY POLE
---	SIGN	---	GUY
---	EDGE PAVEMENT	---	TELEPHONE MANHOLE
---	CURBLINE	---	CITY OF PORTLAND
---	CONTOURS		
---	SPOT GRADE		
---	CHAIN LINK FENCE		
---	STOCKADE FENCE		
---	RETAINING WALL		
---	GUARDRAIL		
---	WATER		
---	GATE VALVE		
---	HYDRANT		
---	SEWER MH		
---	STORM DRAIN		
---	CATCH BASIN		
---	DRAINAGE MH		

GRAPHIC SCALE





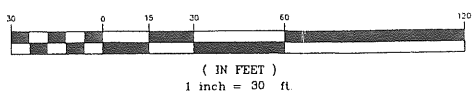
TBM HYDRANT
S/LY FLANGE
BOLT
ELEV. 26.57

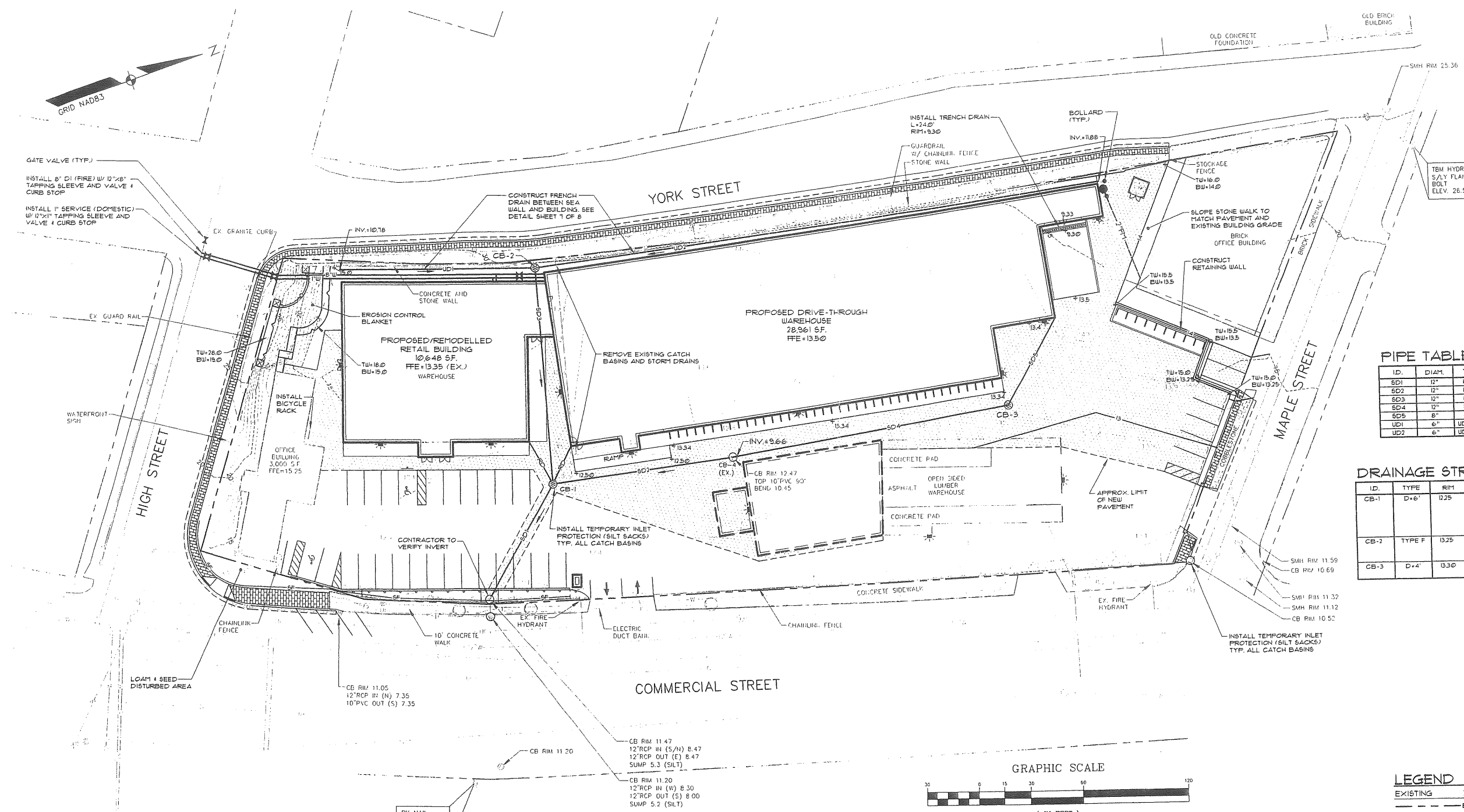
PK NAIL
C.O.P. TBM#4
UP# 32/36
ELEV. 12.43

LEGEND

EXISTING	DESCRIPTION	EXISTING	DESCRIPTION
---	BOUNDARY LINE/ROW	○	SEWER
---	ABUTTER LINE/ROW	○	SEWER MH
□	MONUMENT	○	STORM DRAIN
○	IRON PIPE/ROD	○	CATCH BASIN
○	BENCHMARK	○	DRAINAGE MH
▭	BUILDING	○	OVERHEAD ELEC. & TEL.
○	SIGN	○	UNDERGROUND ELEC. & TEL.
---	EDGE PAVEMENT	○	TRANSFORMER PAD
---	CURBLINE	○	ELECTRICAL MANHOLE
---	CHAIN LINK FENCE	○	TELEPHONE MANHOLE
---	RETAINING WALL	○	LIGHT POLE/WALL
○	DECIDUOUS TREE	○	UTILITY POLE
---	GUARDRAIL	○	GUY
○	GAS	○	ELEC. METER
○	WATER	○	ELEC. BOX
○	GATE VALVE		
○	HYDRANT		

GRAPHIC SCALE



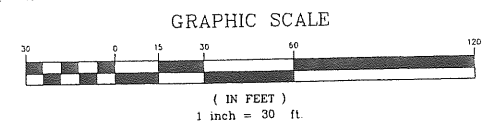


PIPE TABLE

ID.	DIAM.	TYPE	LENGTH	SLOPE
SD1	12"	HDPE	715'	0.0075
SD2	12"	HDPE	1010'	0.0046
SD3	12"	HDPE	1705'	0.0005
SD4	12"	HDPE	1600'	0.0005
SD5	8"	HDPE	550'	0.0005
UD1	6"	UD/HDPE	3350'	0.0005
UD2	6"	UD/HDPE	150'	0.0005

DRAINAGE STRUCTURE TABLE

ID.	TYPE	RM	PIPE	INVERT ELEV.
CB-1	Dx6"	12.25	12"	9.10 (FROM CB-2)
			12"	9.60 (TO EX. CB4)
			6" RD	9.50 (FROM RETAIL)
			6" RD	9.10 (FROM WAREHOUSE)
CB-2	TYPE F	13.25	6" UD	10.20 (NORTHERLY UD)
			6" UD	10.20 (SOUTHERLY UD)
			12"	9.70 (TO CB-1)
CB-3	Dx4"	13.30	8"	10.75 (ROOF DRAIN)
			12"	10.46 (TO EX. CB-4)



CONSTRUCTION NOTES

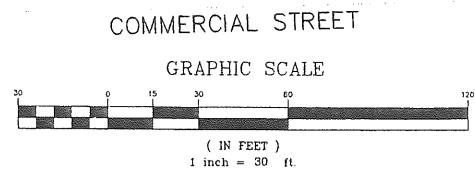
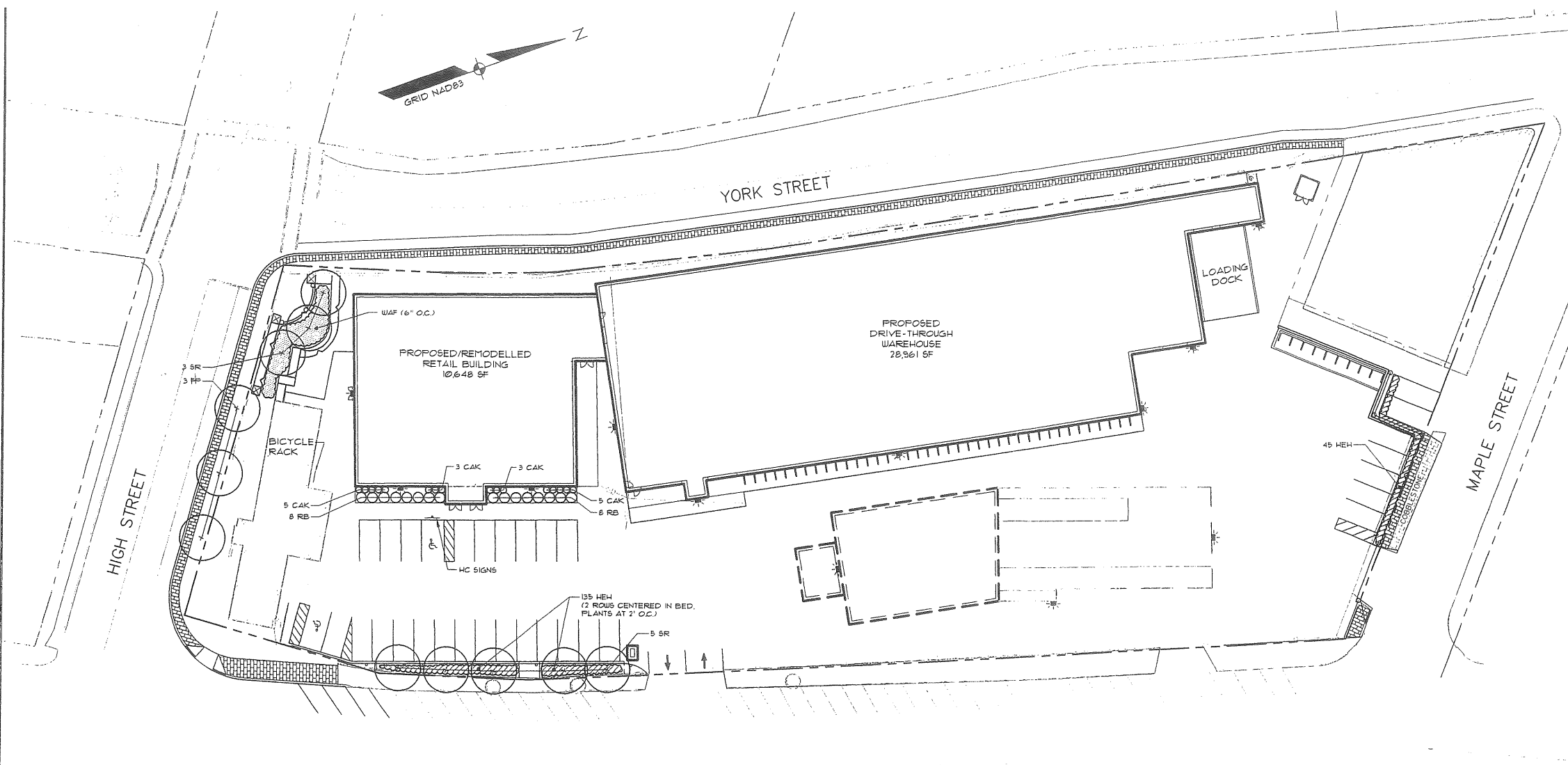
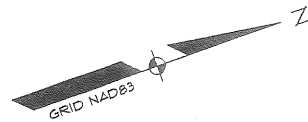
- ALL WORK SHALL CONFORM TO THE APPLICABLE CODES AND ORDINANCES.
- CONTRACTOR SHALL VISIT THE SITE AND FAMILIARIZE HIM OR HERSELF WITH ALL CONDITIONS AFFECTING THE PROPOSED WORK AND SHALL MAKE PROVISIONS AS TO THE COST THEREOF. CONTRACTOR SHALL BE RESPONSIBLE FOR FAMILIARIZING HIM OR HERSELF WITH ALL CONTRACT DOCUMENTS, FIELD CONDITIONS AND DIMENSIONS AND CONFIRMING THAT THE WORK MAY BE ACCOMPLISHED AS SHOWN PRIOR TO PROCEEDING WITH CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO THE COMMENCEMENT OF WORK.
- CONTRACTOR SHALL NOTIFY ENGINEER OF ALL PRODUCTS OR ITEMS NOTED AS "EXISTING" WHICH ARE NOT FOUND IN THE FIELD.
- INSTALL ALL EQUIPMENT AND MATERIALS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND OWNER'S REQUIREMENTS UNLESS SPECIFICALLY OTHERWISE INDICATED OR WHERE LOCAL CODES OR REGULATIONS TAKE PRECEDENCE.
- CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO FABRICATION AND ERECTION OF ANY MATERIAL. ANY UNUSUAL CONDITIONS SHALL BE REPORTED TO THE ATTENTION OF THE ENGINEER.
- CONTRACTOR SHALL CLEAN AND REMOVE DEBRIS AND SEDIMENT DEPOSITED ON PUBLIC STREETS, SIDEWALKS, ADJACENT AREAS, OR OTHER PUBLIC WAYS DUE TO CONSTRUCTION.
- CONTRACTOR SHALL INCORPORATE PROVISIONS AS NECESSARY IN CONSTRUCTION TO PROTECT EXISTING STRUCTURES, PHYSICAL FEATURES, AND MAINTAIN SITE STABILITY DURING CONSTRUCTION. CONTRACTOR SHALL RESTORE ALL AREAS TO ORIGINAL CONDITION AND AS DIRECTED BY DESIGN DRAWINGS.
- SITE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS PRIOR TO CONSTRUCTION.
- ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH "MAINE EROSION AND SEDIMENTATION CONTROL HANDBOOK FOR CONSTRUCTION: BEST MANAGEMENT PRACTICES" PUBLISHED BY THE CUMBERLAND COUNTY SOIL AND WATER CONSERVATION DISTRICT AND MAINE DEPARTMENT OF ENVIRONMENTAL PROTECTION, MARCH 1991 OR LATEST EDITION. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO POSSESS A COPY OF THE EROSION CONTROL PLAN AT ALL TIMES.
- THE CONTRACTOR IS HEREBY CAUTIONED THAT ALL SITE FEATURES SHOWN HEREON ARE BASED ON FIELD OBSERVATIONS BY THE SURVEYOR AND BY INFORMATION PROVIDED BY UTILITY COMPANIES. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL CONTACT DIG SAFE (1-888-DIGSAFE) AT LEAST THREE (3) BUT NOT MORE THAN THIRTY (30) DAYS PRIOR TO COMMENCEMENT OF EXCAVATION OR DEMOLITION TO VERIFY HORIZONTAL AND VERTICAL LOCATION OF ALL UTILITIES.
- CONTRACTOR SHALL BE AWARE THAT DIG SAFE ONLY NOTIFIES ITS "MEMBER" UTILITIES ABOUT THE DIG. DIG SAFE WILL ADVISE CONTRACTOR OF MEMBER UTILITIES IN THE AREA. CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING AND CONTACTING NON-MEMBER UTILITIES DIRECTLY. NON-MEMBER UTILITIES MAY INCLUDE TOWN OR CITY WATER AND SEWER DISTRICTS AND SHALL LOCAL UTILITIES, AS WELL AS USGS PUBLIC WORKS SYSTEMS.
- CONTRACTORS SHALL BE RESPONSIBLE FOR COMPLIANCE WITH THE REQUIREMENTS OF 23 MRS.A 3360-A. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITIES TO OBTAIN AUTHORIZATION PRIOR TO RELOCATION OF ANY EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS. IF A UTILITY CONFLICT ARISES, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER, THE MUNICIPALITY AND APPROPRIATE UTILITY COMPANY PRIOR TO PROCEEDING WITH ANY RELOCATION.
- ALL PAVEMENT MARKINGS AND DIRECTIONAL SIGNAGE SHOWN ON THE PLAN SHALL CONFORM TO THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) STANDARDS.
- ALL PAVEMENT JOINTS SHALL BE SAUCUT PRIOR TO PAVING TO PROVIDE A DURABLE AND UNIFORM JOINT.

CONSTRUCTION NOTES

- NO HOLES, TRENCHES OR STRUCTURES SHALL BE LEFT OPEN OVERNIGHT IN ANY EXCAVATION ACCESSIBLE TO THE PUBLIC OR IN PUBLIC RIGHTS-OF-WAY.
- ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL REQUIRE A M.D.O.T. PERMIT AS WELL AS PERMITS FROM THE TOWN AS APPLICABLE.
- IMMEDIATELY UPON COMPLETION OF CUTS/FILLS, THE CONTRACTOR SHALL STABILIZE DISTURBED AREAS IN ACCORDANCE WITH EROSION CONTROL NOTES AND AS SPECIFIED ON PLANS.
- THE CONTRACTOR SHALL BE FULLY AND SOLELY RESPONSIBLE FOR THE REMOVAL, REPLACEMENT AND RECTIFICATION OF ALL DAMAGED AND DEFECTIVE MATERIAL AND WORKMANSHIP IN CONNECTION WITH THE CONTRACT WORK. THE CONTRACTOR SHALL REPLACE OR REPAIR AS DIRECTED BY THE OWNER ALL SUCH DAMAGED OR DEFECTIVE MATERIALS WHICH APPEAR WITHIN A PERIOD OF ONE YEAR FROM THE DATE OF SUBSTANTIAL COMPLETION.
- ALL WORK PERFORMED BY THE GENERAL CONTRACTOR AND/OR TRADE SUBCONTRACTOR SHALL CONFORM TO THE REQUIREMENTS OF LOCAL, STATE OR FEDERAL LAWS, AS WELL AS ANY OTHER GOVERNING REQUIREMENTS, WHETHER OR NOT SPECIFIED ON THE DRAWINGS.
- WHERE THE TERMS "APPROVED EQUAL", "OTHER APPROVED", "EQUAL TO", "ACCEPTABLE" OR OTHER GENERAL QUALIFYING TERMS ARE USED IN THESE NOTES, IT SHALL BE UNDERSTOOD THAT REFERENCE IS MADE TO THE RULING AND JUDGMENT OF SEBAGO TECHNICS, INC.
- THE GENERAL CONTRACTOR SHALL PROVIDE ALL NECESSARY PROTECTION FOR THE WORK UNTIL TURNED OVER TO THE OWNER.
- THE GENERAL CONTRACTOR SHALL MAINTAIN A CURRENT AND COMPLETE SET OF CONSTRUCTION DRAWINGS ON SITE DURING ALL PHASES OF CONSTRUCTION FOR USE OF ALL TRADES.
- THE CONTRACTOR SHALL TAKE FULL RESPONSIBILITY FOR ANY CHANGES AND DEVIATION OF APPROVED PLANS NOT AUTHORIZED BY THE ARCHITECT/ENGINEER AND/OR CLIENT/OWNER.
- DETAILS ARE INTENDED TO SHOW END RESULT OF DESIGN. ANY MODIFICATION TO SUIT FIELD DIMENSION AND CONDITION SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO ANY WORK.
- BEFORE THE FINAL ACCEPTANCE OF THE PROJECT THE CONTRACTOR SHALL REMOVE ALL EQUIPMENT AND MATERIALS, REPAIR OR REPLACE PRIVATE OR PUBLIC PROPERTY WHICH MAY HAVE BEEN DAMAGED OR DESTROYED DURING CONSTRUCTION, CLEAN THE AREAS WITHIN AND ADJACENT TO THE PROJECT WHICH HAVE BEEN OBSTRUCTED BY HIS/HER OPERATIONS, AND LEAVE THE PROJECT AREA NEAT AND PRESENTABLE.
- ALL SUBSURFACE UTILITY LINES SHOWN HEREON ARE BASED SOLELY ON THE FIELD LOCATION OF VISIBLE STRUCTURES, SMHS, CB'S, HYDRANTS, ETC. IN CONJUNCTION WITH DESIGN AND/OR AS-BUILT PLANS SUPPLIED TO SEBAGO TECHNICS INC. BY OTHERS. PRIOR TO ANY CONSTRUCTION, EXCAVATION, TEST BORINGS, DRILLING, ETC. DIG SAFE MUST BE NOTIFIED AND A SITE IDENTIFICATION NUMBER ALONG WITH A SAFE TO DIG DATE OBTAINED. THE SITE CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD VERIFYING THE LOCATION, DEPTH AND MATERIAL OF ALL SUBSURFACE UTILITY LINES SHOWN HEREON AND ANY AND ALL OTHERS LOCATED ON SITE WITHIN THE CONSTRUCTION AREA.
- ALL NEW CATCH BASINS SHALL BE FITTED WITH A CASCO TRAP OR APPROVED EQUAL.

LEGEND

EXISTING	DESCRIPTION	PROPOSED
---	BOUNDARY LINE/ROW	---
---	ABUTTER LINE/ROW	---
---	BENCHMARK	---
---	BUILDING	---
---	SIGN	---
---	EDGE PAVEMENT	---
---	CURBLINE	---
---	CONTOURS	---
---	SPOT GRADE	---
---	CHAIN LINK FENCE	---
---	RETAINING WALL	---
---	DECIDUOUS TREE	---
---	GUARDRAIL	---
---	GAS	---
---	WATER	---
---	GATE VALVE	---
---	HYDRANT	---
---	SEWER MH	---
---	STORM DRAIN	---
---	UNDERDRAIN	---
---	CATCH BASIN	---
---	DRAINAGE MH	---
---	OVERHEAD ELEC. TEL.	---
---	UNDERGROUND ELEC. TEL.	---
---	TRANSFORMER PAD	---
---	ELECTRICAL MANHOLE	---
---	TELEPHONE MANHOLE	---
---	LIGHT POLE/WALL	---
---	UTILITY POLE	---
---	GUT	---
---	ELEC. METER	---
---	ELEC. BOX	---
---	EC. BLANKET	---
---	SILT FENCE	---
---	INLET PROTECTION	---



LANDSCAPE NOTES

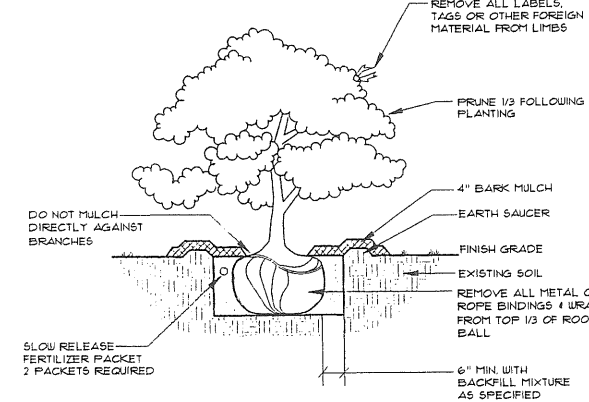
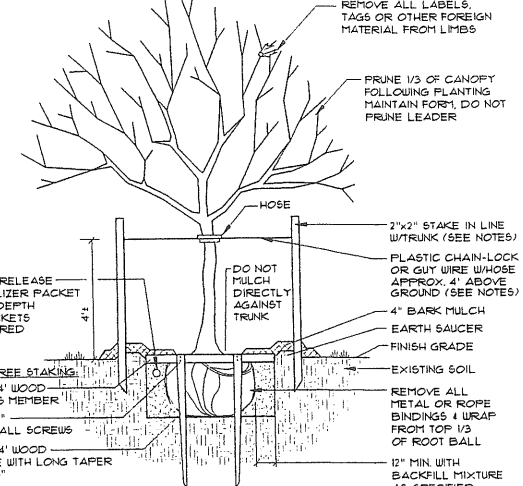
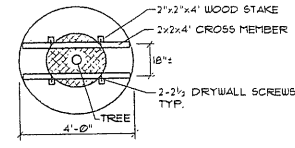
1. PLANT QUANTITIES SHOWN ON PLANT LISTS ARE FOR CONVENIENCE TO THE CONTRACTOR ONLY. THE CONTRACTOR IS RESPONSIBLE FOR ALL PLANT MATERIAL INSTALLATION AS SHOWN ON PLANS.
2. SIZE AND GRADING STANDARDS OF PLANT MATERIALS SHALL CONFORM TO THE LATEST EDITION OF "U.S.A. STANDARD FOR NURSERY STOCK," BY THE AMERICAN ASSOCIATION OF NURSERYMEN, INC.
3. ALL PLANT MATERIAL SHALL BE FREE FROM INSECTS AND DISEASE.
4. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH ACCEPTABLE HORTICULTURAL PRACTICES. THIS IS TO INCLUDE PROPER PLANTING MIX, PLANT BED AND TREE PIT PREPARATION, PRUNING, STAKING OR GUTTING, WRAPPING, SPRAYING, FERTILIZATION, PLANTING AND ADEQUATE MAINTENANCE UNTIL ACCEPTANCE BY THE OWNER.
5. PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR BY THE CONTRACTOR AND A PERIOD OF TWO YEARS THEREAFTER BY THE OWNER FROM DATE OF INSTALLATION. DURING THE ONE YEAR GUARANTEE PERIOD, DEAD PLANT MATERIAL SHALL BE REPLACED AT NO COST TO THE OWNER. AT THE END OF THE ONE YEAR PERIOD, THE CONTRACTOR SHALL OBTAIN FINAL ACCEPTANCE FROM THE OWNER.
6. ALL GRASS, OTHER VEGETATION AND DEBRIS SHALL BE REMOVED FROM ALL PLANTING AREAS PRIOR TO PLANTING.
7. EXISTING TREES TO BE PRESERVED WILL BE PROTECTED DURING CONSTRUCTION AND SHALL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR.
8. THE LANDSCAPE CONTRACTOR IS ADVISED OF THE PRESENCE OF THE UNDERGROUND UTILITIES AND SHALL VERIFY THE EXISTENCE AND LOCATION OF SAME BEFORE COMMENCING AND DIGGING OPERATIONS. THE LANDSCAPE CONTRACTOR SHALL REPLACE OR REPAIR UTILITIES, PAVING, WALKS, CURBING, ETC. DAMAGED IN PERFORMANCE OF THIS JOB AT NO ADDITIONAL COST TO THE OWNER.
9. ALL SHRUB BEDS SHALL BE MULCHED WITH 3" CLEAN SHREDDED DARK BROWN BARK MULCH.
10. THE CONTRACTOR SHALL PROVIDE 4" LOAM FOR ALL AREAS TO BE SODDED OR SEED. PLANTING AREAS SHALL RECEIVE 1" ROLLED THICKNESS OF LOAM. THE LANDSCAPE CONTRACTOR SHALL COORDINATE SUBGRADE PREPARATION WITH THE GENERAL CONTRACTOR PRIOR TO PLACING LOAM.
11. ANY DEVIATION FROM THE LANDSCAPE PLAN INCLUDING PLANT LOCATION, SELECTION, SIZE, QUANTITY OR CONDITION SHALL BE REVIEWED AND APPROVED BY THE OWNER AND LANDSCAPE ARCHITECT (AND MUNICIPAL AUTHORITY, IF APPLICABLE) PRIOR TO INSTALLATION ON SITE.
12. WHERE INDICATED ON PLAN, PLANTING SOIL MIXTURE FOR PERENNIAL AND ANNUAL FLOWER BED AREAS SHALL CONSIST OF FOUR PARTS TOPSOIL, TWO PARTS SPHAGNUM PEAT MOSS, AND ONE PART HORTICULTURAL PERLITE BY VOLUME. PEAT MOSS MAY BE SUBSTITUTED WITH WELL-ROTTED OR DEHYDRATED MANURE OR COMPOST. ROTOTILL BEDS TO A DEPTH OF 8 INCHES.

PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	SIZE
CAK	CALAMAGROSTIS x ACUTIFOLIA 'KARL FOERSTER'	KARL FOERSTER'S FEATHER REED GRASS	NO. 1 CONT.
FP	FRAXINUS PENNSYLVANICA 'CIMMAM'	CIMMAMON ASH	2 1/2" CAL.
HEH	HEMEROCALLIS 'HYPERION'	DAYLILY	NO. 1 CONT.
RB	ROSA 'BONICA'	BONICA SHRUBROSE	18-24" HIGH
SR	SYRINGA RETICULATA	JAPANESE TREE LILAC	2" CAL. SINGLE STEM
WAF	WALDSTENIA FRAGARIOIDES	BARREN STRAWBERRY	CLUMP

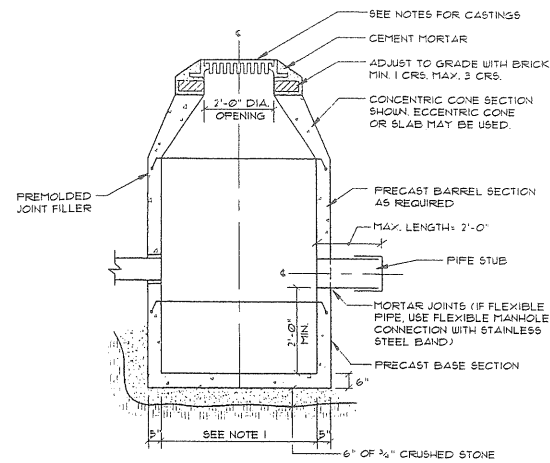
LEGEND

EXISTING	DESCRIPTION	PROPOSED
---	BOUNDARY LINE/ROW	---
---	ABUTTER LINE/ROW	---
---	BUILDING SIGN	---
---	EDGE PAVEMENT	---
---	PAVEMENT PAINT	---
---	CURBLINE	---
---	CHAIN LINK FENCE	---
---	RETAINING WALL	---
○	DECIDUOUS TREE	○
○	CONIFEROUS TREE	○
---	GUARDRAIL	---



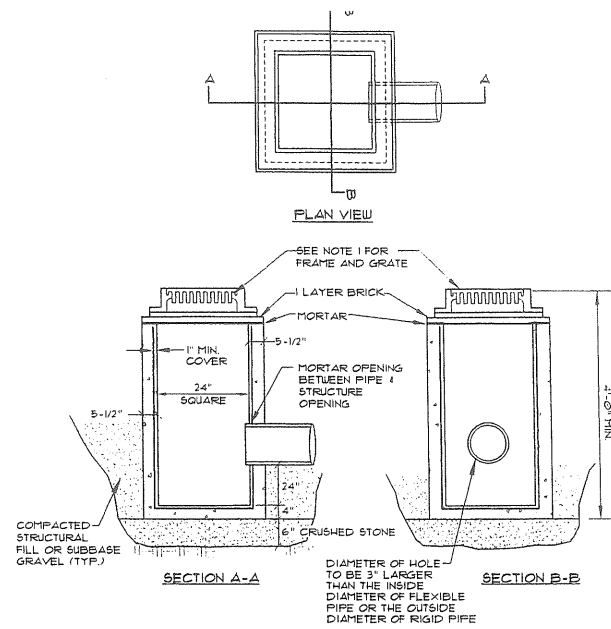
- NOTES:**
1. INSTALL STAKES AND GUYS TO TREES IF THE FOLLOWING APPLY:
 1. THE TREE IS OF SUBSTANTIAL SIZE.
 2. THE PLANTING LOCATION IS EXTREMELY WINDY, AS ON OPEN UNDEVELOPED SITES.
 3. THE PLANTING LOCATION IS COMPRISED OF SAND OR OTHER LOOSE TEXTURED SOILS.
 4. IF STAKES AND GUYS ARE REQUIRED, REMOVE AFTER ONE YEAR TIME.
- DECIDUOUS TREES 2" TO 4" CALIPER**
NOT TO SCALE

DECIDUOUS & EVERGREEN SHRUB
NOT TO SCALE



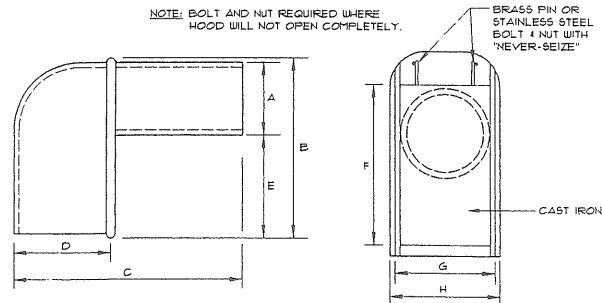
- NOTES:**
- 4'-0" I.D. TYPICAL. SOME STRUCTURES MAY REQUIRE LARGER I.D. PROVIDE SHOP DRAWINGS.
 - DRAINAGE STRUCTURES TO BE DESIGNED FOR H-20 LOADING.
 - PIPE SIZES AND INVERTS AS NOTED ON PLANS.
 - CATCH BASIN FRAME AND GRATE TO BE ETHERIDGE FOUNDRY S2424, TYPE H OR C OR APPROVED EQUAL.
 - DRAINAGE MANHOLE FRAME AND COVER TO BE ETHERIDGE FOUNDRY M2485 OR APPROVED EQUAL. COVER SHALL BE MARKED "DRAIN".

TYPICAL CATCH BASIN
NOT TO SCALE



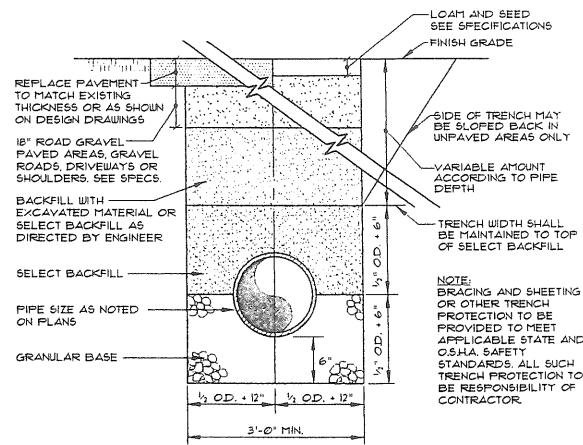
- NOTES:**
- FRAME SHALL BE FOR 24" SQUARE GRATE - LEBARON TYPE "F" SQUARE FRAME (LP 745) 4" FLANGE OR ETHERIDGE SQUARE FRAME S2424. GRATE SHALL BE 24"x24" CAST IRON.
 - ENTIRE CATCH BASIN WITH EXCEPTION OF LEVELING BRICK FRAME AND GRATE TO BE PRECAST AS SINGLE PORTLAND CEMENT CONCRETE UNIT.

TYP. TYPE "F" CATCH BASIN
NOT TO SCALE

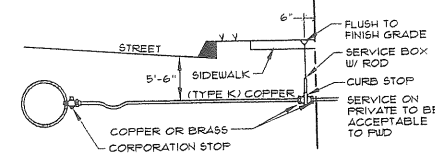
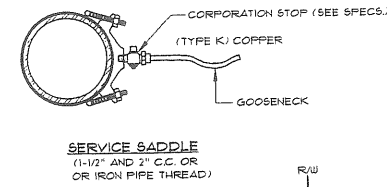
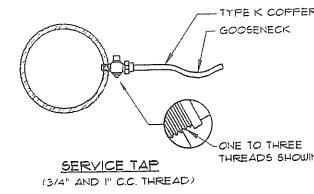


SIZE	A	B	C	D	E	F	G	H
6 IN.	5 1/2"	13 3/8"	13 3/4"	5 3/8"	5 7/8"	11 5/8"	6 1/2"	7 1/4"
8 IN.	7 1/2"	15"	15 3/8"	5 1/2"	5 3/8"	13 3/4"	8 3/4"	9 3/8"
10 IN.	9 1/2"	16"	16 1/4"	6"	4 1/2"	14 1/8"	11 1/2"	12 3/8"
12 IN.	11 1/2"	17"	17"	8"	3 1/2"	15"	12 1/2"	13 3/8"
15 IN.	14 1/2"	22 1/2"	23"	10 1/4"	5 3/4"	21"	14 1/4"	15 3/4"

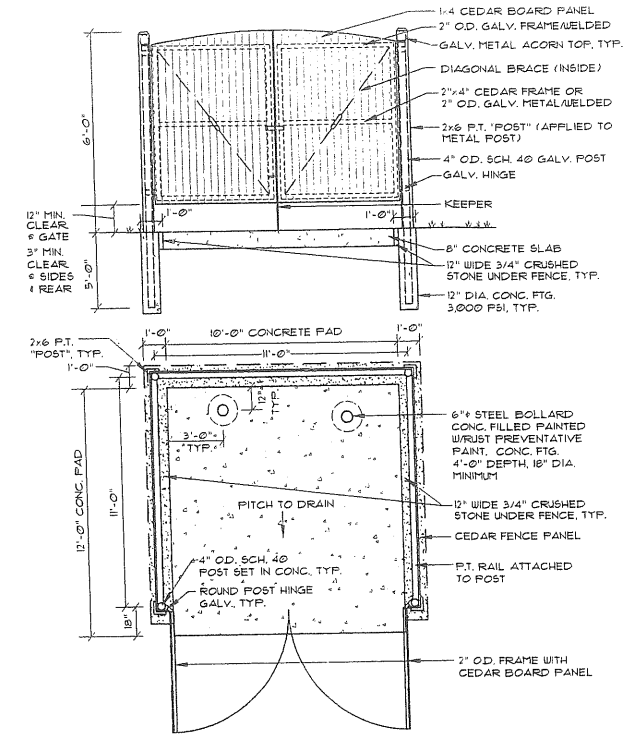
CASCO TRAP
NOT TO SCALE



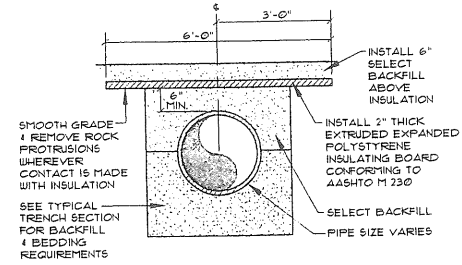
TRENCH SECTION
NOT TO SCALE



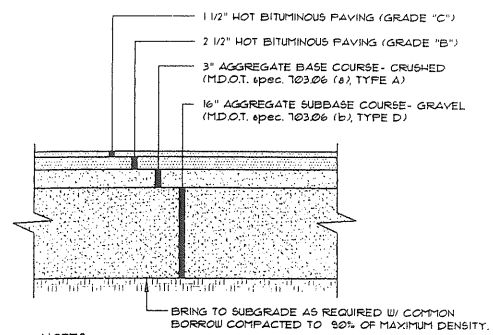
TYPICAL SERVICE CONNECTION
NOT TO SCALE



TYPICAL DUMPSTER ENCLOSURE
NOT TO SCALE

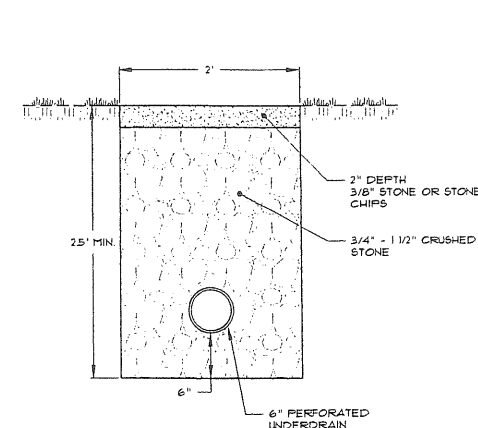


PIPE INSULATION DETAIL
NOT TO SCALE

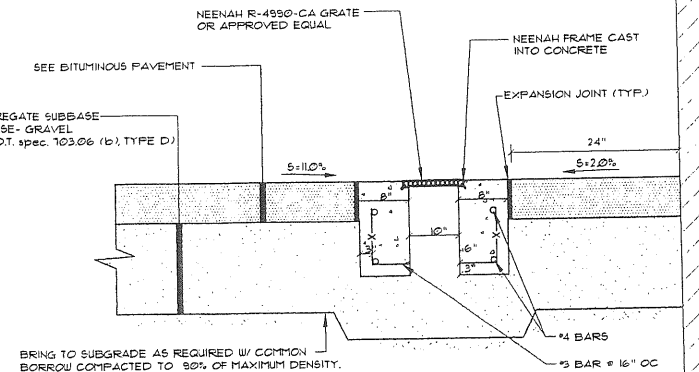


- NOTES:**
- COMPACT GRAVEL SUB-BASE, BASE COURSE TO 92% OF MAXIMUM DENSITY USING HEAVY ROLLER COMPACTION.
 - CONTRACTOR SHALL SET GRADE STAKES MARKING SUBBASE AND FINISH GRADE ELEVATIONS FOR CONSTRUCTION REFERENCE.
 - THE 16" DEPTH OF THE PROPOSED SUBBASE COURSE (TYPE D GRAVEL) REPRESENTS A TOTAL DEPTH WHERE EXISTING SUBBASE GRAVEL EXISTS. ADJUST DEPTH OF SUBBASE COURSE APPROPRIATELY TO ACHIEVE FINISH GRADE.

TYP. PAVED PARKING LOT SECTION
NOT TO SCALE

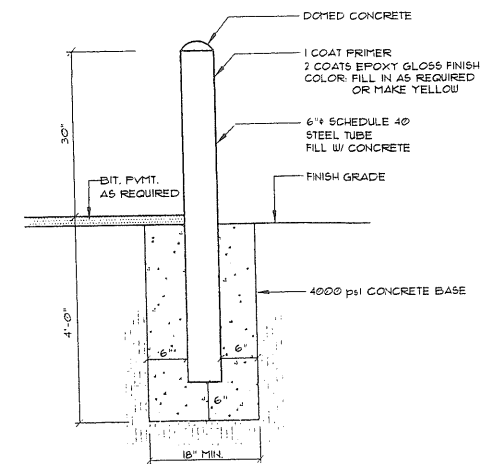


FRENCH DRAIN DETAIL
NOT TO SCALE



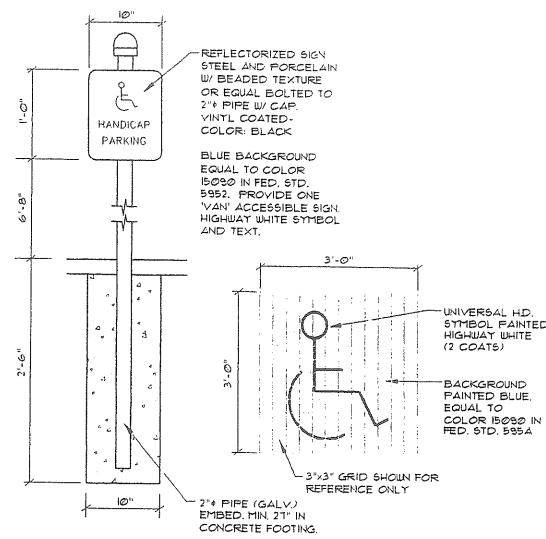
- NOTES:**
- COMPACT GRAVEL SUB-BASE, BASE COURSE TO 92% OF MAXIMUM DENSITY USING HEAVY ROLLER COMPACTION.
 - CONTRACTOR SHALL SET GRADE STAKES MARKING SUBBASE AND FINISH GRADE ELEVATIONS FOR CONSTRUCTION REFERENCE.

TRENCH DRAIN CROSS SECTION
NOT TO SCALE

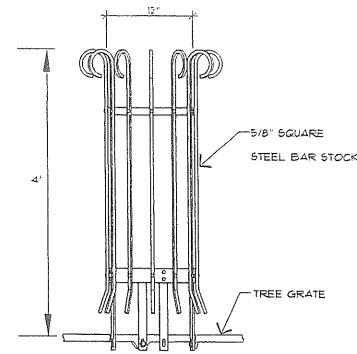


METAL BOLLARD
NOT TO SCALE

SIDEWALK AND DRIVEWAY DETAILS - OPTION FOR HISTORIC DISTRICT

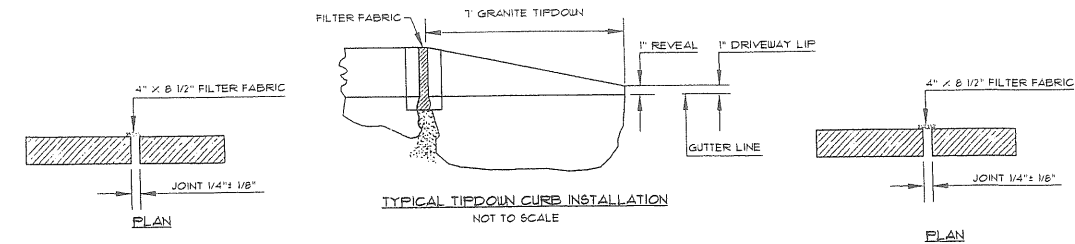


HANDICAP SIGNS
NOT TO SCALE



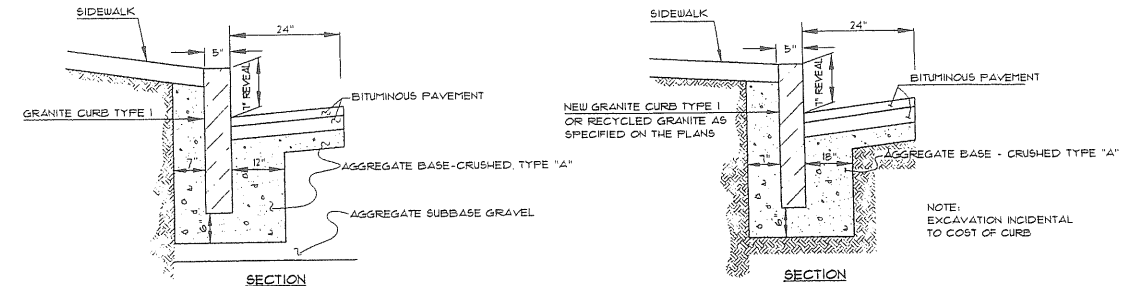
MATERIAL: ALL HOT ROLLED MILD STEEL
FINISH: PAINTED WITH ONE COAT LO-LUSTRE BLACK ENAMEL
Nashua Foundry Co. (nco.com), Item: Type A, L4, 12" opening

TYPICAL TREE GUARD
NOT TO SCALE



TYPICAL TIPDOWN CURB INSTALLATION
NOT TO SCALE

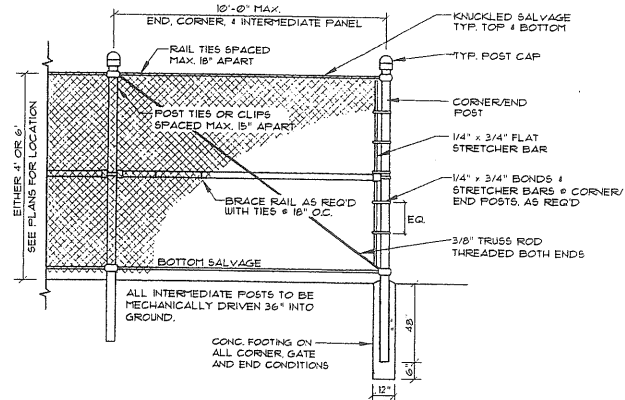
CURB REPLACEMENT DETAIL
NOT TO SCALE



INSTALLATION OF CURB
NOT TO SCALE

CURB REPLACEMENT DETAIL
NOT TO SCALE

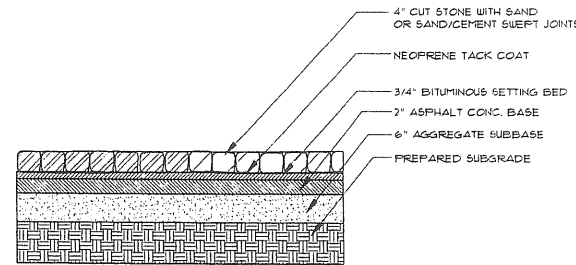
TYPICAL INSTALLATION OF CURB TYPE I
NOT TO SCALE



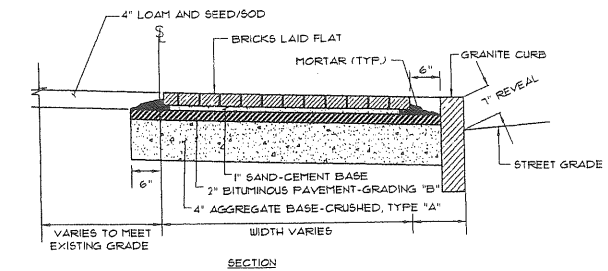
NOTES:

- CORNER OR END POST, NOMINAL 3" O.D. GALVANIZED STEEL PIPE, MIN. 5.78 lbs./L.F.
- INTERMEDIATE POST, NOMINAL 2-1/2" O.D. GALVANIZED STEEL PIPE, MIN. 3.65 lbs./L.F.
- BRACES (TOP & BOTTOM), NOMINAL 1-5/8" O.D. GALVANIZED STEEL PIPE, MIN. 2.71 lbs./L.F.
- THE OUTSIDE OF THE FENCE FABRIC SHALL BE 3" INSIDE THE EDGE OF PAVEMENT.
- BRACE RAIL AND DIAGONAL BRACE ROD SHALL BE INSTALLED AT EACH 10' CORNER SECTION OF ENCLOSURE.
- CONCRETE SHALL HAVE MINIMUM COMPRESSIVE STRENGTH (f_c) OF 3000 PSI WITH 6% AIR ENTRAINMENT.
- SUBMIT SHOP DRAWINGS FOR OWNER'S/ENGINEER'S APPROVAL.

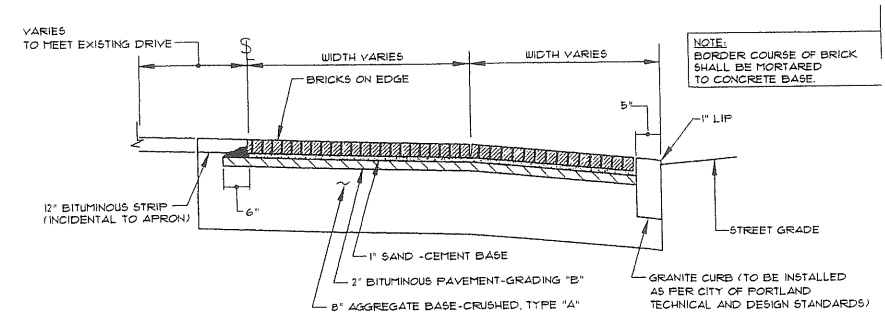
TYPICAL CHAIN LINK FENCE
NOT TO SCALE



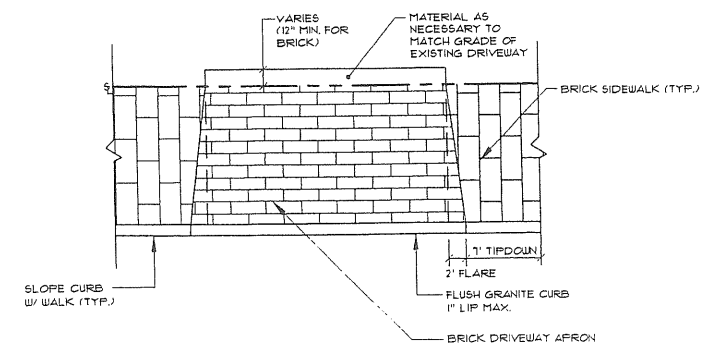
CUT STONE PAVERS WITH BITUMINOUS SETTING BED ON ASPHALT BASE-LIGHT DUTY
NOT TO SCALE



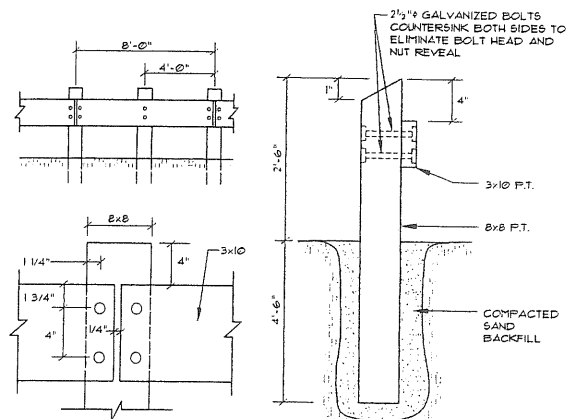
BRICK SIDEWALK DETAIL
NOT TO SCALE



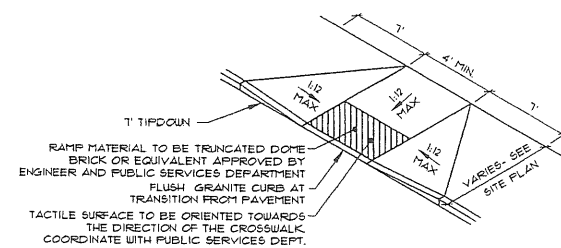
SECTION OF BRICK DRIVEWAY
NOT TO SCALE
(TO BE USED IN HISTORIC DISTRICT)



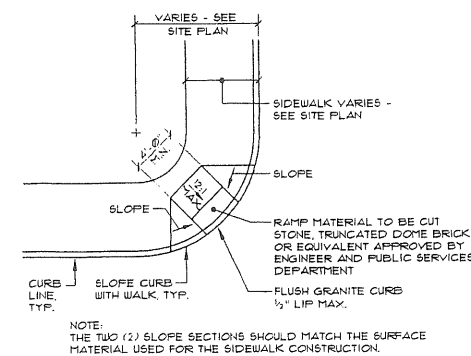
BRICK DRIVEWAY APRON W/ GRANITE CURB
NOT TO SCALE
(TO BE USED IN HISTORIC DISTRICT)



PRESSURE TREATED WOOD GUARDRAIL
NOT TO SCALE

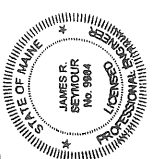
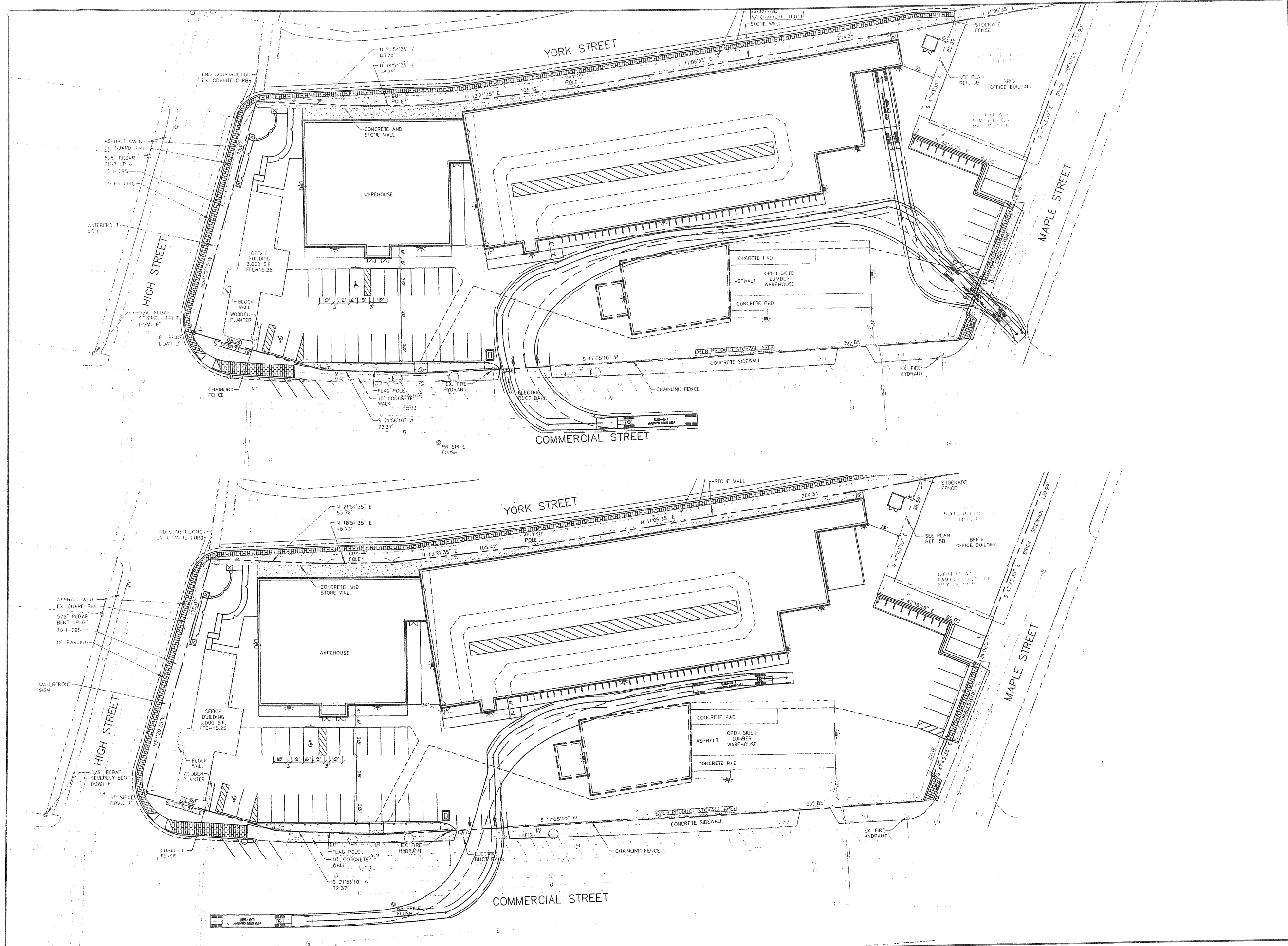


STRAIGHT RAMP



CORNER RAMP

HANDICAP RAMP
NOT TO SCALE



REV	BY	DATE	STATUS
B	JRS	11/5/08	REVISED PER CITY REVIEW COMMENTS
A	JRS	6/11/08	ISSUED FOR PRELIMINARY PLAN REVIEW

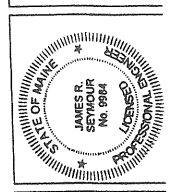
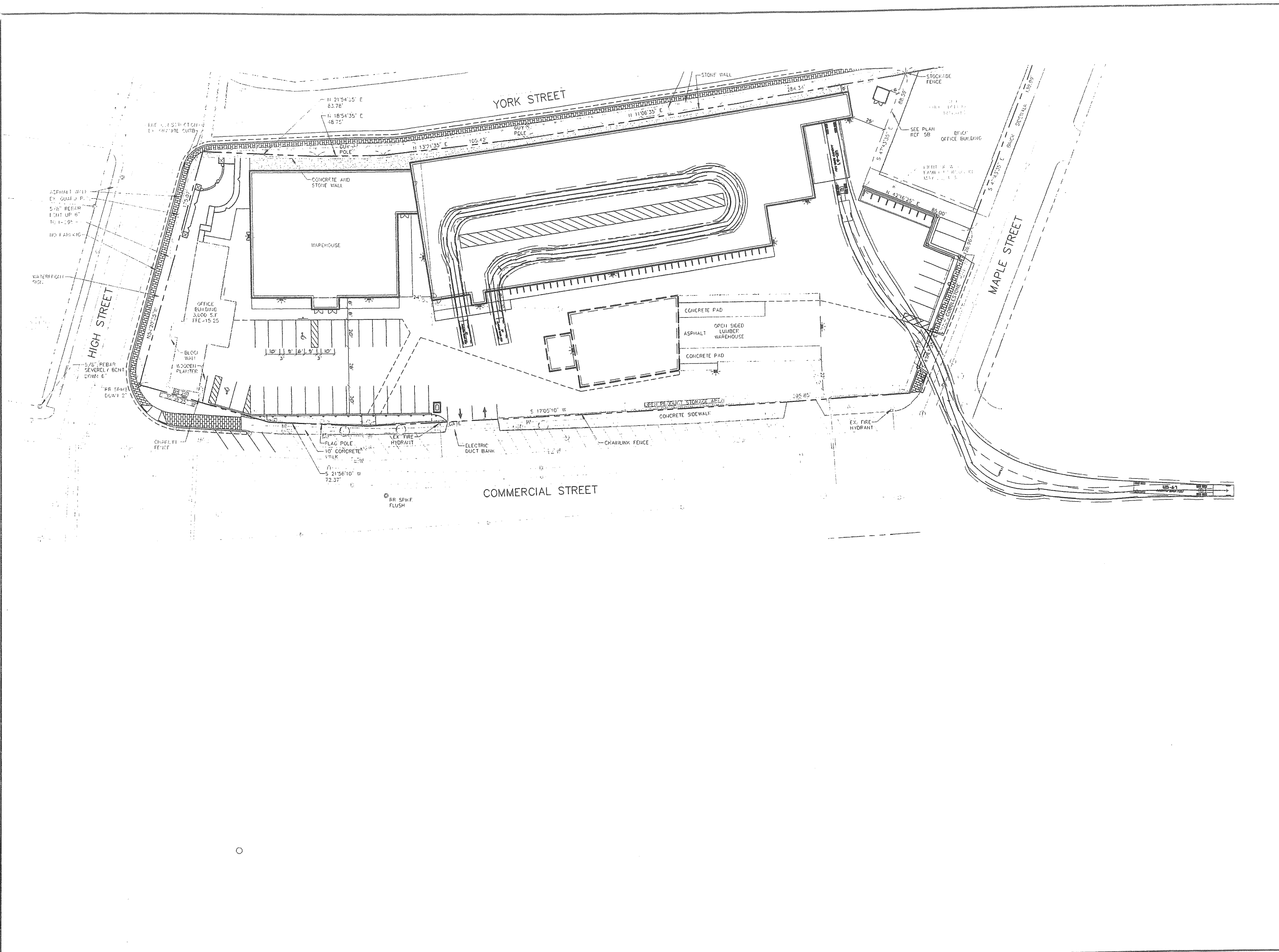
Sebago Technics
 Engineering Experience You Can Build On
 363 COMMERCIAL STREET
 PORTLAND, MAINE
 Tel: (207) 856-0277

PROJECT NO. FIELD BOOK | DESIGN | CHKD | DRAWN

TURNING TEMPLATES
 OF
 RUFUS DEERING LUMBER IMPROVEMENTS
 363 COMMERCIAL STREET
 PORTLAND, MAINE
 FOR
 RUFUS DEERING LUMBER CO.

DATE	SCALE
3/17/08	1" =

SHEET 1 OF



REV.	BY:	DATE:	STATUS:
B	JRS	11/5/08	REVISED PER CITY REVIEW COMMENTS
A	JRS	6/11/08	ISSUED FOR PRELIMINARY PLAN REVIEW

THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM SEBAGO TECHNICS, INC. ANY ALTERATIONS.

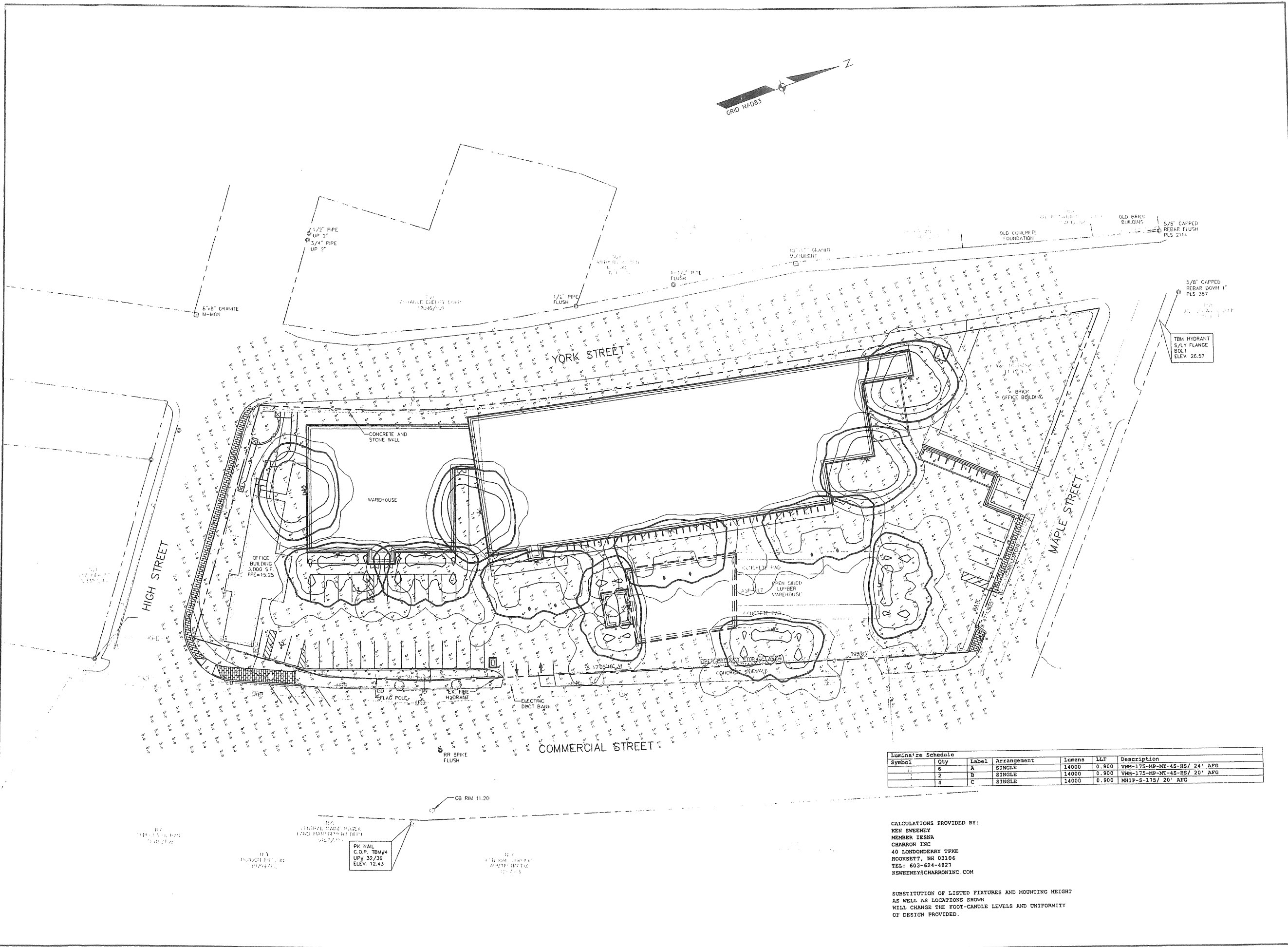
Sebago Technics
 Engineering Excellence You Can Build On
 100 Commercial Street
 Westbrook, Me 04094-1339
 Tel (207) 856-0277

PROJECT NO. FIELD BOOK | DESIGN | CHRD | DRAWN

TURNING TEMPLATES
 OF
 RUFUS DEERING LUMBER IMPROVEMENTS
 100 COMMERCIAL STREET
 PORTLAND, MAINE
 FOR RUFUS DEERING LUMBER CO.
 100 MANUFACTURERS STREET

DATE	SCALE
3/17/08	1" =

SHEET 2 OF



Luminaire Schedule

Symbol	Qty	Label	Arrangement	Lumens	LLF	Description
A	6	A	SINGLE	14000	0.900	VHM-175-MP-MT-48-RS/ 24' AFG
B	2	B	SINGLE	14000	0.900	VHM-175-MP-MT-48-RS/ 20' AFG
C	4	C	SINGLE	14000	0.900	WHIP-S-175/ 20' AFG

CALCULATIONS PROVIDED BY:
 KEN SHEENEY
 MEMBER IESNA
 CHARRON INC
 40 LONDONDERRY TPKE
 HOOKSETT, NH 03106
 TEL: 603-624-4827
 KSHEENEY@CHARRONINC.COM

SUBSTITUTION OF LISTED FIXTURES AND MOUNTING HEIGHT AS WELL AS LOCATIONS SHOWN WILL CHANGE THE FOOT-CANDLE LEVELS AND UNIFORMITY OF DESIGN PROVIDED.

PRELIMINARY
 Subject to Revision

Att. 16

REV.	BY	DATE	STATUS

THIS PLAN SHALL NOT BE MODIFIED WITHOUT WRITTEN PERMISSION FROM SEBAGO TECHNICS, INC. ANY ALTERATIONS AUTHORIZED OR OTHERWISE SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO SEBAGO TECHNICS, INC.

Sebago Technics
 Engineering Expertise You Can Build On
 One Chisbol Street
 Westbrook, ME 04092
 Tel: (207) 866-0272

PROJECT NO. FIELD BOOK DESIGN CHKD DRAWN
 07363 PNM JRS JKS

PHOTOMETRICS PLAN
 OF:
RUFUS DEERING LUMBER IMPROVEMENTS
 383 COMMERCIAL STREET
 PORTLAND, MAINE
 FOR:
RUFUS DEERING LUMBER CO.
 383 COMMERCIAL STREET
 PORTLAND, MAINE 04101

DATE	SCALE
3/17/08	1" = 30'



LANE ALIGNMENT W/BIKE LANE, NO SIDEWALK

GRAPHIC SCALE



(IN FEET)
1 inch = 30 ft.

Alt. 12

N/F ANTHONY & ANN MEZOIAN 40-C-25

N/F ALLIANCE ENERGY CORP. 17040/159

GRID NAD83

5' BIKE LANE
PROPOSED 4' BRICK SIDEWALK W/ GRANITE CURB

YORK STREET

PROPOSED 5' BRICK SIDEWALK W/ GRANITE CURB

HIGH STREET

REMODELLED BUILDING 3 SF

N/F AGA REALTY 13132/248

LANE ALIGNMENT W/ BIKE LANE & SIDEWALK

GRAPHIC SCALE



(IN FEET)
1 inch = 30 ft.

Att. 12

N/F ANTHONY & ANN MEZOIAN 40-C-25

N/F ALLIANCE ENERGY CORP. 17040/159

GRID NAD83

5' BIKE LANE
PROPOSED 4' BRICK SIDEWALK W/ GRANITE CURB

YORK STREET

PROPOSED 5' BRICK SIDEWALK W/ GRANITE CURB

PROPOSED REMODELLED RETAIL BUILDING 10648 SF

N/F AGA REALTY 13132/248

HIGH STREET

LANE ALIGNMENT W/ BIKE LANE & SIDEWALK

GRAPHIC SCALE



(IN FEET)
1 inch = 30 ft.