

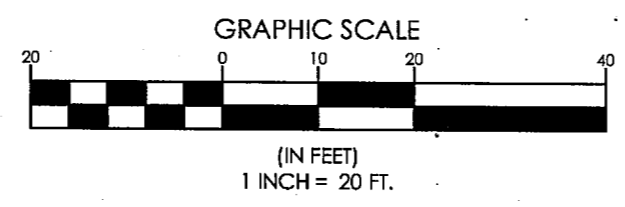
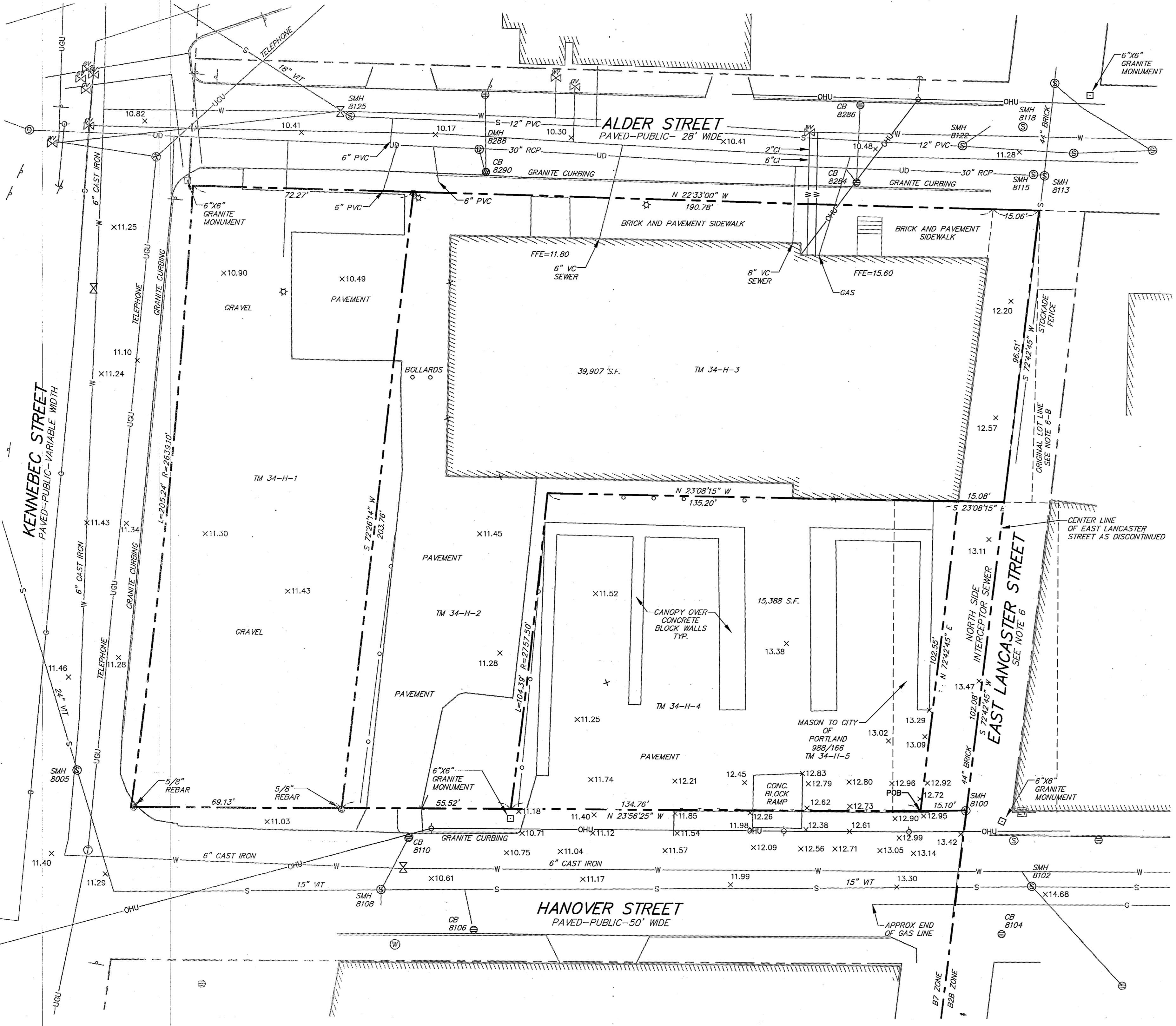
LOCATION MAP N.T.S.

STRUCTURES

- SMH 8108 RIM=10.6
8" VIT INV IN=5.7
12" VIT INV IN=4.5
12" VIT INV OUT=4.4
SHELF=4.8
- CB 8110 RIM=9.7
8" VIT INV IN=5.7
SUMP=4.0
- SMH 8102 RIM=14.3
15" CMP INV IN=8.9
18" VIT INV IN=8.9
15" VIT INV IN=6.2
15" VIT INV OUT=6.1
SHELF=6.9
- SMH 8115 RIM=11.1
44" BRICK INV IN=4.0
44" BRICK INV IN=3.9
TOP WEIR=5.9
30" RCP INV OUT=3.2
- CB 8284 RIM=9.9
12" PVC INV IN=6.2
12" PVC INV OUT=5.0
SUMP=1.6
- CB 8290 RIM=9.5
8" PVC INV IN=5.4
12" PVC INV OUT=4.3
SUMP=1.7
- DMH 8288 RIM=9.8
30" RCP INV IN=1.7
12" PVC INV IN=3.8
30" RCP INV OUT=1.6
SHELF=3.9
- SMH 8125 RIM=10.1
CB 8106 RIM=10.1
CB 8104 RIM=13.4
- SMH 8005 RIM= 11.4
SMH 8100 RIM=11.9
SMH 8113 RIM=11.1
SMH 8118 RIM=11.1
SMH 8122 RIM=10.8
CB 8286 RIM=9.8

LEGEND

- EXISTING**
- PROPERTY LINE/R.O.W.
 - ABUTTER LINE/R.O.W.
 - DEED LINE/R.O.W.
 - MONUMENT
 - IRON PIPE/ROD
 - BUILDING
 - EDGE PAVEMENT
 - EDGE CONCRETE
 - PAVEMENT PAINT
 - EDGE GRAVEL
 - CURB LINE
 - CHAIN LINK FENCE
 - STOCKADE FENCE
 - GUARD RAIL
 - SPOT GRADE
 - DECIDUOUS TREE
 - BOLLARD
 - SIGN
 - GAS
 - GAS GATE VALVE
 - WATER
 - WATER GATE VALVE
 - SANITARY SEWER
 - SANITARY MANHOLE
 - UNDER DRAIN
 - DRAINAGE MANHOLE
 - CATCH BASIN
 - UNDERGROUND UTILITY
 - TRANSFORMER PAD
 - TELEPHONE MANHOLE
 - LIGHT POLE
 - UTILITY POLE
 - GUY WIRE
 - POB POINT OF BEGINNING



GENERAL NOTES:

1. THE RECORD OWNERS OF THE PARCELS ARE:
BOPO LLC BY DEED DATED JUNE 30, 2011 RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS IN BOOK 28797 PAGE 222 TAX MAP 34, BLOCK H, LOTS 2 AND 3
BOPO LLC BY DEED DATED JUNE 30, 2011 RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS IN BOOK 28797 PAGE 224 TAX MAP 34, BLOCK H, LOT 1
CITY OF PORTLAND BY DEED DATED APRIL 23, 1849 RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS IN BOOK 213 PAGE 449 TAX MAP 34, BLOCK H, LOT 4
CITY OF PORTLAND BY DEED DATED MARCH 14, 1917 RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS IN BOOK 988 PAGE 166 TAX MAP 34, BLOCK H, LOT 5
2. THE SURVEYED PARCELS ARE SHOWN AS LOTS 1, 2, 3, 4 AND 5, BLOCK H, ON THE CITY OF PORTLAND'S ASSESSORS MAP 34.
3. BEARINGS ARE GRID NORTH PER PLAN REFERENCE 2. ELEVATIONS ARE CITY OF PORTLAND DATUM BASED ON TOP OF GRANITE MONUMENT ELEVATION 11.55 AS SHOWN ON PLAN REFERENCE 5.
4. THE PARCEL IS LOCATED, BY SCALING METHODS, WITHIN ZONE C (AREAS OF MINIMAL FLOODING) AS SHOWN ON THE FLOOD INSURANCE RATE MAP FOR THE CITY OF PORTLAND, MAINE COMMUNITY PANEL NO. 230051-0013-B WITH AN EFFECTIVE DATE OF JULY 17, 1986. THE FLOOD ELEVATION FOR THE A2 ZONE WHICH ENCOMPASSES BACK COVE HAS AN ELEVATION OF 10 NGVD 1929.
5. UTILITIES SHOWN ARE BASED ON THE REFERENCED PLANS AND A FIELD SURVEY OF VISIBLE STRUCTURES PERFORMED IN FEBRUARY 2015. DURING THE FIELD SURVEY THERE WAS SNOW IN EXCESS OF THREE FEET DEEP AND ASSOCIATED ICE THAT MADE VERIFICATION DIFFICULT. A SITE VISIT TO VERIFY UTILITIES IS RECOMMENDED ONCE SNOW COVER IS GONE.
6. THE FOLLOWING NOTES APPEAR IN THE "SURVEY REPORT FOR HANOVER STREET SALT SHED SITE" DATED JANUARY 7, 2002 BY WILLIAM G. SCOTT, P.L.S., CITY OF PORTLAND D.P.W.:
A LANCASTER STREET EAST HAS BEEN DISCONTINUED AS A CITY STREET ON MAY 5, 1995. THE DISCONTINUANCE RESERVED THE CONTINUED USE OF THIS PROPERTY AS A SEWER EASEMENT FOR THE STILL-ACTIVE NORTH SIDE INTERCEPTING SEWER. HOWEVER, THE DISCONTINUANCE NOTICE MADE NO MENTION OF ALSO DISCONTINUING THE PUBLIC EASEMENT WITHIN THE STREET BOUNDARIES.
B. "THE CITY HAS A STOCKADE FENCE BUILT OUT TO THE APPROXIMATE CENTERLINE OF THE STREET WHICH FENCES IN A DUMPSTER AND MATERIALS STORAGE AREA. I BELIEVE THAT THE REVERSIONARY RIGHTS INVOLVED IN THE STREET DISCONTINUANCE IN MAY 1995 WOULD ESTABLISH THE PROPERTY LINE BACK IN ITS LOCATION PRIOR TO THE CREATION OF LANCASTER STREET. AS IS CLEARLY SHOWN ON THE 1913 PLAN OF LANCASTER STREET EXTENSION, THE FORMER PROPERTY LINE BETWEEN WHAT IS NOW FURMAN AND THE CITY, FRONTING ALONG ALDER STREET, DID NOT RUN ALONG WITH THE CENTERLINE OF THE PROPOSED STREET. IN FACT, MORE LAND WAS TAKEN FROM WHAT IS NOW FURMAN'S PARCEL THAN WAS TAKEN FROM WHAT IS NOW THE CITY'S."
7. WATER SERVICES SHOWN ARE BASED ON THE CARDS PROVIDED BY THE PORTLAND WATER DISTRICT AND ARE TO BE CONSIDERED APPROXIMATE.

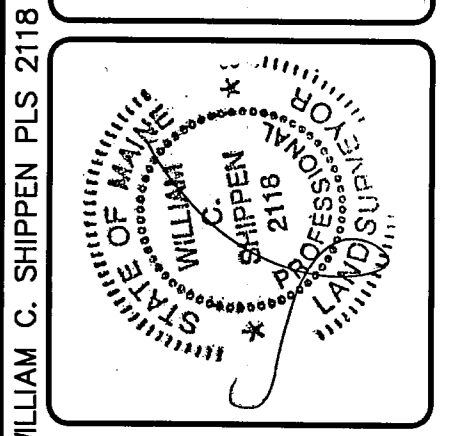
ZONING:

- DIVISION 10. B-2B COMMUNITY BUSINESS ZONE**
- DIMENSIONAL REQUIREMENTS:**
- MINIMUM LOT SIZE NONE
 - MINIMUM STREET FRONTAGE 20 FEET
 - FRONT YARD SETBACK MINIMUM NONE
 - REAR YARD SETBACK MINIMUM 10 FEET
 - SIDE YARD SETBACK MINIMUM NONE REQUIRED
 - SIDE YARD ON SIDE STREET SETBACK MINIMUM NONE
 - HEIGHT MAXIMUM 45 FEET
 - MAXIMUM IMPERVIOUS SURFACE RATIO 80%
- DIVISION 17. B-7 MIXED DEVELOPMENT DISTRICT ZONE**
- MINIMUM LOT SIZE NONE
 - MINIMUM FRONTAGE NONE
 - FRONT SETBACK NONE REQUIRED
 - SIDE SETBACK NONE REQUIRED
 - REAR SETBACK NONE REQUIRED
 - MAXIMUM BUILDING LOT COVERAGE ONE-HUNDRED PERCENT(100%)
 - MAXIMUM RESIDENTIAL DENSITY NONE
 - MAXIMUM BUILDING HEIGHT AS DETAILED ON THE BAYSIDE HEIGHT OVERLAY MAP ON FILE IN THE PLANNING AND DEVELOPMENT DEPARTMENT OFFICE.
 - MINIMUM BUILDING HEIGHT NEWLY CONSTRUCTED BUILDINGS SHALL HAVE THE REQUIRED MINIMUM OF FLOORS AS PROVIDED BY THE BAYSIDE HEIGHT OVERLAY MAP WITHIN FIFTY (50) FEET OF ANY STREET FRONTAGE. SUCH FLOORS SHALL BE OCCUPIABLE OR HABITABLE AND ABOVE THE AVERAGE GRADE OF THE ADJUTING STREET.

REFERENCE IS MADE TO CHAPTER 14 IN THE LAND USE ORDINANCE FOR THE CITY OF PORTLAND, MAINE FOR ADDITIONAL INFORMATION.

PLAN REFERENCES:

1. ALDER STREET RECONSTRUCTION PLAN AND PROFILE CITY OF PORTLAND PUBLIC WORKS DEPARTMENT ENGINEERING SECTION DATED JUNE 1996.
2. STANDARD BOUNDARY SURVEY PROPOSED SALT SHED LOT HANOVER STREET CITY OF PORTLAND PUBLIC WORKS DEPARTMENT ENGINEERING SECTION DATED SEPTEMBER 2001.
3. LAND TITLE SURVEY ON KENNEBEC STREET, PORTLAND, MAINE PORTLAND TERMINAL COMPANY TO ROSS Y. FURMAN 58 ALDER ST., PORTLAND MAINE DATED JULY 27, 1894 BY OWEN HASKELL, INC. AND RECORDED IN THE CUMBERLAND COUNTY REGISTRY OF DEEDS IN PLAN BOOK 194 PAGE 292.
4. EXISTING CONDITIONS AND STANDARD BOUNDARY SURVEY ON KENNEBEC STREET, PORTLAND, MAINE FOR ROSS Y. FURMAN 58 ALDER STREET, PORTLAND, MAINE DATED APRIL 18, 1996 BY OWEN HASKELL, INC.
5. BAYSIDE TRAIL SOMERSET STREET EXTENSION ELM STREET TO KENNEBEC STREET DATED JULY 3, 2012 CITY OF PORTLAND, MAINE PUBLIC SERVICES DEPARTMENT ENGINEERING DIVISION
6. LANCASTER STREET EXTENSION FROM PREBLE ST. TO FOREST AVE. DATED FEB. 1913 ON FILE WITH THE CITY OF PORTLAND ENGINEERING DIVISION.
7. RIGHT OF WAY AND TRACK MAP PORTLAND TERMINAL OPERATED BY THE PORTLAND TERMINAL COMPANY STATION 52+80.0 TO STATION 105+60.0 JUNE 30, 1916 SHEET VI-C/2
8. CITY OF PORTLAND CITY PROPERTY REVALUATION SHEET 6.
9. BOWL PORTLAND SITE GRADING AND UTILITY PLAN DATED 8-27-09 REVISED THROUGH 12-29-09 BY CASCO BAY ENGINEERING.



DESIGNED	CHECKED
DJP-WCS	WCS

SEBAGO TECHNICALS

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EXISTING CONDITIONS PLAN

OF: **BAYSIDE BOWL**
58 ALDER ST
PORTLAND, ME 04101

FOR: **PROJECT RESOURCES, INC.**
P.O. BOX 661
YARMOUTH, ME 04096-0661