



May 20, 2015

Ms. Helen Donaldson
Planning and Development Department
City of Portland, Maine
389 Congress Street
Portland, Maine 04101-3509

**Subject: Century Tire Plaza – 195 Kennebec Street
Project ID# 2015-027
Review Comments Received May 7, 2015**

Dear Ms. Donaldson:

We have received and reviewed the City's review comments and offer our responses to these comments. For ease of reference we have repeated each comment in *italics* with our response following.

Traffic Comments – Tom Errico, P.E.

Comment 1:

5/7/2015 Status: The applicant should include a pavement marking plan for Marginal Way for review and comment.

Response

A Sketch Plan showing the proposed pavement marking was provided on May 7, 2015. The response to this submission did not indicate any further revisions are required. The updated plans include the pavement markings as presented on the May 7, 2015 Sketch Plan.

Comment 2:

5/7/2015 Status: The pedestrian path located on site along the easterly property boundary shall have a curb or other physical element that prevents vehicle encroachment. A plan shall be provided for review and approval.

Response

A curb is shown along the eastern edge of the parking lot to provide separation between the parking area and the adjacent sidewalk.

Comment 3:

5/7/2015 Status: I find the revised configuration of the parking spaces to be acceptable with the removal of the parking space nearest Marginal Way.

Response

The parking space nearest Marginal Way has been removed on the revised plan sheet.

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Comment 4:

5/7/2015 Status: The applicant should provide feedback as it relates to the type of time-limit parking regulations. The curb extension is generally acceptable. The plans should be revised to include a short sidewalk connection to the sidewalk on Brattle Street and the sidewalk ramp configuration on the opposite of Kennebec Street needs to be ADA compliant so the flares of the ramp are not in the pedestrian travel path. For the cycle-track the following details should be incorporated. I would suggest that the applicant provide a sketch for review and comment before finalizing (the details below are subject to change as the design develops).

- 1. 10 foot surface path*
- 2. 2-foot buffer space. Two 4 inch lines – dashed at driveways 7 to 8 foot parking will be permitted on the outside of the cycle-track where driveways do not exist.*
- 3. The cycle-track shall have green paint across driveways. Bollards shall be installed in the buffer area. The bollards will be permanently attached to the pavement. I will provide a specification for installation. It should be assumed that they will be located every 20 feet.*
- 4. All paint shall be durable long-lasting paint. I will provide a specification for installation.*

Response:

A Sketch Plan was provided for review on May 7, 2015.

Additional Comments on the Sketch Plan Provided May 7, 2015

The City has reviewed the cycle track path concept provided by you and please note the following comments.

Comment 1:

The green paint shall extend all the way to Forest Avenue from the Enterprise driveway. Attached is the green paint product that should be installed.

Response

The location of the green paint has been modified as requested to extend to the Forest Avenue intersection.

Comment 2:

There should be a 4" yellow dashed line dividing the cycle track with bike lane markings (helmeted cyclist and arrows) indicating a two-way cycle track.

Response

A yellow dashed centerline has been added to the bike lane as requested.

Comment 3:

The attached sign shall be installed at Forest Avenue/Kennebec Street intersection on that corner illustrating two-way bicycle traffic to motor vehicles. The sign size shall be 24" x 24".

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Response

The sign has been added on the revised plan sheet.

Comment 4:

Bollards shall be spaced 15 feet apart. Green colored 42-inch high bollards should be installed per the attached standard.

Response

Delineation bollards have been added in the buffer zone between the bike lane and the travel lane, as requested.

Comment 5:

The 2-foot buffer between the parked vehicles and the raised path needs to be removed per Fire Department comments (they are fine with the bollard area).

Response

The buffer zone has been eliminated alongside the raised path at the request of the Fire Department. It should be noted that this creates a potential conflict between open car doors and bicyclists traveling along the raised path.

Comment 6:

The plan should note that the project shall be responsible for a bituminous patching within the cycle track before the suggested treatment is installed. The limits, extent, and methods of patching shall be coordinated with DPS.

Response

A note has been added to plan to this effect.

Comment 7:

The buffer space shall have chevron markings. A six-inch white gore line should be provided every 15 feet.

Response

Chevron markings at 15-foot intervals have been added along the buffer zone, as requested.

Comment 8:

The outside line of the cycle track shall be 6" wide and the inside line shall be 4" wide.

Response

The revised plan shows the striping designation, as requested.

Comment 9:

The dashed lines at the driveways shall be a 2-foot stripe with a 2-foot gap.

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Response

Dashed lines are shown and noted on the revised site plan. A note has been added indicating all snow will be removed from the site.

Planning Department – Nell Donaldson

Comment 1:

The lights on Kennebec Street should be the medium Bayside fixtures, and I believe your specs are correct, but you list the lights on both streets as 'large scale' on the site plan. Could you revise your note to clarify this?

Response

The note has been revised on the plan sheet to reflect the correct designations for the light poles.

Comment 2:

The snow storage note still appears on the landscape plan, as do references to Note #18 (which has been removed). Could you revise? Since you're planning to remove all snow from the site, could you add a note as such?

Response

The note and references to the note have been eliminated from the revised Landscape Plan.

Department of Public Services – David Margolis-Pineo

Comment 1:

- 1. On Marginal Way, where does the proposed electrical conduit connect to the existing lighting circuit? Show the location and details of the connection. Show the location of the conduit to be installed.*
- 2. On Kennebec Street, show connection from the proposed meter to the utility power grid. Show the electrical panel design details.*
- 3. Specify the conduit size, type, and provide installation detail showing the depth of cover (minimum of 36"), conduit encasement, and marker tape.*
- 4. Specify and show the conduit location with respect to right of way or some other reference.*
- 5. Are light fixtures, poles, and pole foundations details provided?*
- 6. The applicant will be responsible for all connections, wiring and any other ancillary work required for a complete street project. Please delete reference that the City is responsible for such work.*
- 7. It is understood that all street lighting will have LED fixtures, all circuits metered for City billing and please not that a 120v outlet per pole is not required*

Response

Please see revised electrical plan from Bennett Engineering that addresses these comments.

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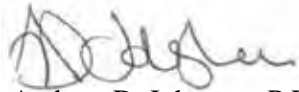
Additional Item

Please note that we have also added a single sheet (C-4.1 Marginal Way Entrance Detail) to the plan set. This clarifies the configuration of the entrances to Building B from Marginal Way.

If you have any questions with regards to the information provided, please contact our office.

Sincerely,

FAY, SPOFFORD & THORNDIKE



Andrew D. Johnston, P.E.
Principal Engineer

ADJ/cmd

Enclosure

c: Brad Fries