

33-E-1

#2016-078

117 Preble Street

Schlotterbeck Block Plan Amendment

Schlotterbeck Block, LLC. (John Anton)

City of Portland
 Development Review Application
 Planning Division Transmittal Form

Application Number: 2016-078 **Application Date:** 04/06/2016
CBL: 033 E001001 **Application Type:** Plan Amendment - Board Level III
Applicant: Schlotterbeck Block, LLC. /John Anton
Project Name: The Schlotterbeck Block
Address: 117 PREBLE ST
Project Description: Addition of a residential unit (revised total of units is now 56).
Zoning: B-7

Other Required Reviews:

<input type="checkbox"/> Traffic Movement	<input type="checkbox"/> 14-403 Streets	<input type="checkbox"/> Housing Replacement
<input type="checkbox"/> Storm Water	# Units _____	<input type="checkbox"/> Historic Preservation
<input checked="" type="checkbox"/> Subdivision	<input type="checkbox"/> Flood Plain	<input type="checkbox"/> Other:
# Lots <u> 1 </u>	<input type="checkbox"/> Shoreland	
<input type="checkbox"/> Site Location	<input type="checkbox"/> Design Review	
# Unit _____		

Distribution List:

Planner	Barbara Barhydt	Parking	John Peverada
Zoning	Ann Machado	Design Review	Caitlin Cameron
Traffic Engineer	Tom Errico	Corporation Counsel	Jennifer Thompson
Civil Engineer	David Sensus	Sanitary Sewer	John Emerson
Fire Department	Keith Gautreau	Inspections	Tammy Munson
City Arborist	Jeff Tarling	Historic Preservation	Deb Andrews
Engineering	David Margolis-Pineo	DRC Coordinator	Phil DiPierro
		Outside Agency	

Comments needed by 4/14/2016

April 5, 2016

Ms. Shukria Wiar, Planner
and Planning Board Members
Planning and Urban Development
City of Portland
389 Congress Street
Portland, ME 04101

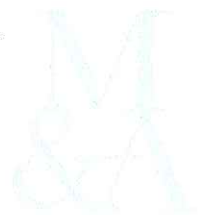
**RE: Amended Subdivision Plan & Level III Site Plan Application
Schlotterbeck Block – 117 Preble Street**

Dear Shukria and Planning Board Members:

On behalf of Schlotterbeck Block LLC, we are pleased to submit the following Amended Site Plan and Subdivision Application for the proposed redevelopment of the Schlotterbeck & Foss building at 117 Preble Street. Schlotterbeck Block LLC is requesting to construct one (1) additional studio apartment unit in the basement of the existing building. Also, since Planning Board approval on December 15, 2015, site excavation and exploration activities have taken place. Several utility connections need to be modified in response to the sub-surface findings. These modifications are further defined below.

Subdivision and Site Plan Modifications

1. **Additional Unit:** At its December 15, 2015 meeting, the Planning Board approved Schlotterbeck Block LLC's Subdivision Application, which included the development of 55 units at the former Schlotterbeck & Foss building at 117 Preble Street. The applicant now proposes an additional unit in the building's basement, resulting in a total of 56 units and 1,750 s.f. of office space. The attached Amended Subdivision Plan and Basement Floor Plan A1.1 have been revised accordingly.



The attached Amended Subdivision Plan and Basement Floor Plan A1.1 have been revised accordingly.

2. Vehicular Parking: Attached Exhibit A is a revised Parking Demands memo, prepared by Gorrill Palmer, dated March 24, 2016. The memo states the 45 vehicular parking spaces and 25 bicycle parking spaces are adequate for the proposed 56-unit project. The December 2015 vehicular parking layout has the capacity to serve the additional unit and is not revised or modified as part of this resubmission.
3. Additional Bike Racks: The additional unit eliminates the bike storage room in the basement. As a result, fourteen (14) exterior new bike parking space need to be located on the site. The attached revised L2.0 show an additional bike hitch under the canopy and six (6) additional bike racks on a concrete pad located near the building entrance. With these revisions, thirteen (13) bike loops are provided and 26 bike parking spaces will be provided.
4. Additional Sidewalk Ramp and Pedestrian Easement: The City's staff has requested an additional sidewalk ramp at the intersection of Preble and Kennebec Streets, as part of the December 2015 conditions of approval. The new ramp will require pedestrian easement on the applicant's lot. The attached Amended Subdivision Plan and sheet L2.0 show the new sidewalk ramp and the pedestrian easement. Also, attached Exhibit B also shows the detailed layout and grading the new sidewalk ramp.
5. 6-inch Fire Water Line: Attached revised L3.0 adds a new six-inch fire suppression water line from Preble Street to the building's fire suppression system. The applicant had planned to continue using the existing 6-inch fire suppression line, but existing 6-inch line is in poor condition.
6. Roof Drain Connection: Attached revised L3.0 shows a new 6" HDPE roof drain connecting to catch basin #5. The existing roof drain pipe is in poor condition and needs to be replaced.
7. Sanitary Sewer Line: Excavation exploration revealed the existing 6-inch cast iron sanitary sewer line is in poor condition. The applicant proposes to replace the existing sewer line with a new PVC sewer in approximately the same location and wye connect the 15-inch combined sewer on Elm Street.

Submission

This submission includes the following information:

1. Cover letter, dated April 5, 2016;
2. Amended Site Plan and Subdivision Application & Checklist;
3. Exhibit A -- Parking Demands Memo, dated March 24, 2016;
4. Exhibit B – Preble Street Sidewalk Ramp;
5. Application Fee: \$525.00;
6. One set of site plans (24" x 36");
7. One set of site plans (11"x 17"); and
8. One set of digital files.

We trust that the Planning Board will consider this a complete application for the board's meeting scheduled for April 26, 2016. If you desire any additional information, please do not hesitate to contact us. We look forward to our meeting with the Board at its earliest convenience.

Sincerely,
Mitchell & Associates



Michael W. King
Maine Licensed Landscape Architect

cc: John Anton, Schlotterbeck Block LLC
Rick Goduti, Goduti-Thomas Architects

PROJECT NAME: The Schlotterbeck Block

PROPOSED DEVELOPMENT ADDRESS:

117 Preble Street

PROJECT DESCRIPTION:

Applicant received Planning Board Subdivision and Level III Site Plan approval in December 2015 for 55 apartment units and 1,750 s.f. office space. Applicant seeks to add a residential unit (revised total of 56 units) and proposes site plan revisions based on requisite conditions of approval and upgrades to site utilities.

CHART/BLOCK/LOT: 033 E001001
033 E005001

PRELIMINARY PLAN _____ (date)
FINAL PLAN _____ (date)

CONTACT INFORMATION:

Applicant – must be owner, Lessee or Buyer Name: John Anton Business Name, if applicable: Schlotterbeck Block, LLC Address: 77 Spruce St City/State : Portland, ME Zip Code: 04102	Applicant Contact Information Work # Home# Cell # 207 650-8979 Fax# e-mail: antonatlarge@gmail.com
Owner – (if different from Applicant) Name: same Address: City/State : Zip Code:	Owner Contact Information Work # same Home# Cell # Fax# e-mail:
Agent/ Representative Name: Michael King Mitchell & Associates Address: 70 Center Street City/State : Portland, ME Zip Code: 04101	Agent/Representative Contact information Work # (207) 774-4427 Cell # e-mail: mking@mitchellassociates.biz
Billing Information Name: John Anton Schlotterbeck Block, LLC Address: 77 Spruce St Portland, ME 04102 City/State : Zip Code:	Billing Information Work # Cell # 207 650-8979 Fax# e-mail: antonatlarge@gmail.com

APPLICATION SUBMISSION:

1. All site plans and written application materials must be submitted electronically on a CD or thumb drive with each plan and each document submitted as separate files. Naming conventions for the individual files can be found on the **Electronic Plan and Document Submittal** page of the City's website at <http://me-portland.civicplus.com/764/Electronic-Plan-and-Documents-Submittal>
2. **In addition, one (1) paper set of the plans (full size), one (1) paper set of plans (11 x 17), paper copy of written materials, and the application fee must be submitted to the Building Inspections Office to start the review process.**

The application must be complete, including but not limited to the contact information, project data, application checklists, wastewater capacity, plan for fire department review, and applicant signature. The submissions shall include one (1) paper packet with folded plans containing the following materials:


1. **One (1) full size site plans that must be folded.**
2. One (1) copy of all written materials or as follows, unless otherwise noted:
 - a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
3. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet.
4. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
5. One (1) set of plans reduced to 11 x 17.

Please refer to the application checklist (attached) for a detailed list of submission requirements.

APPLICANT SIGNATURE:

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level III Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: 	Date: 4/4/16
-----------------------------------------------------------------------------------------------------------------------	------------------------

PROJECT DATA

The following information is required where applicable, in order to complete the application.

Total Area of Site	40,720 sq. ft.
Proposed Total Disturbed Area of the Site	26,500 sq. ft.
If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland.	
Impervious Surface Area	
Impervious Area (Total Existing)	30,070 sq. ft.
Impervious Area (Total Proposed)	27,300 sq. ft.
Building Ground Floor Area and Total Floor Area	
Building Footprint (Total Existing)	6,800 sq. ft.
Building Footprint (Total Proposed)	6,800 sq. ft.
Building Floor Area (Total Existing)	37,345 sq. ft.
Building Floor Area (Total Proposed)	37,345 sq. ft.
Zoning	
Existing	B-7
Proposed, if applicable	
Land Use	
Existing	Industrial
Proposed	Residential
Residential, If applicable	
# of Residential Units (Total Existing)	N/A
# of Residential Units (Total Proposed)	56
# of Lots (Total Proposed)	2
# of Affordable Housing Units (Total Proposed)	N/A
Proposed Bedroom Mix	
# of Efficiency Units (Total Proposed)	16
# of One-Bedroom Units (Total Proposed)	40
# of Two-Bedroom Units (Total Proposed)	0
# of Three-Bedroom Units (Total Proposed)	0
Parking Spaces	
# of Parking Spaces (Total Existing)	50 +/-
# of Parking Spaces (Total Proposed)	45
# of Handicapped Spaces (Total Proposed)	2
Bicycle Parking Spaces	
# of Bicycle Spaces (Total Existing)	0
# of Bicycle Spaces (Total Proposed)	25
Estimated Cost of Project	\$5,000,000

FINAL PLAN - Level III Site Plan			
Applicant Checklist	Planner Checklist	# of Copies	GENERAL WRITTEN SUBMISSIONS CHECKLIST (* If applicant chooses to submit a Preliminary Plan, then the * items were submitted for that phase and only updates are required)
X		1	* Completed Application form
X		1	* Application fees
X		1	* Written description of project
N/A		1	* Evidence of right, title and interest
N/A		1	* Evidence of state and/or federal permits
N/A		1	* Written assessment of proposed project's specific compliance with applicable Zoning requirements
N/A		1	* Summary of existing and/or proposed easements, covenants, public or private rights-of-way, or other burdens on the site
N/A		1	* Evidence of financial and technical capacity
N/A		1	Construction Management Plan
X		1	A traffic study and other applicable transportation plans in accordance with Section 1 of the technical Manual, where applicable.
N/A		1	Written summary of significant natural features located on the site (Section 14-526 (b) (a))
N/A		1	Stormwater management plan and stormwater calculations
N/A		1	Written summary of project's consistency with related city master plans
N/A		1	Evidence of utility capacity to serve
N/A		1	Written summary of solid waste generation and proposed management of solid waste
N/A		1	A code summary referencing NFPA 1 and all Fire Department technical standards
N/A		1	Where applicable, an assessment of the development's consistency with any applicable design standards contained in Section 14-526 and in City of Portland Design Manual
N/A		1	Manufacturer's verification that all proposed HVAC and manufacturing equipment meets applicable state and federal emissions requirements.

Applicant Checklist	Planner Checklist	# of Copies	SITE PLAN SUBMISSIONS CHECKLIST (* If applicant chooses to submit a Preliminary Plan, then the * items were submitted for that phase and only updates are required)
X		1	* Boundary Survey meeting the requirements of Section 13 of the City of Portland's Technical Manual
		1	Final Site Plans including the following:
X			Existing and proposed structures, as applicable, and distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone);
X			Existing and proposed structures on parcels abutting site;
X			All streets and intersections adjacent to the site and any proposed geometric modifications to those streets or intersections;
X			Location, dimensions and materials of all existing and proposed driveways, vehicle and pedestrian access ways, and bicycle access ways, with corresponding curb lines;
N/A			Engineered construction specifications and cross-sectional drawings for all proposed driveways, paved areas, sidewalks;
N/A			Location and dimensions of all proposed loading areas including turning templates for applicable design delivery vehicles;
N/A			Existing and proposed public transit infrastructure with applicable dimensions and engineering specifications;
X			Location of existing and proposed vehicle and bicycle parking spaces with applicable dimensional and engineering information;
N/A			Location of all snow storage areas and/or a snow removal plan;
N/A			A traffic control plan as detailed in Section 1 of the Technical Manual;
N/A			Proposed buffers and preservation measures for significant natural features, where applicable, as defined in Section 14-526(b)(1);
N/A			Location and proposed alteration to any watercourse;
N/A			A delineation of wetlands boundaries prepared by a qualified professional as detailed in Section 8 of the Technical Manual;
N/A			Proposed buffers and preservation measures for wetlands;
N/A			Existing soil conditions and location of test pits and test borings;
N/A			Existing vegetation to be preserved, proposed site landscaping, screening and proposed street trees, as applicable;
N/A			A stormwater management and drainage plan, in accordance with Section 5 of the Technical Manual;
N/A			Grading plan;
N/A			Ground water protection measures;
N/A			Existing and proposed sewer mains and connections;

- Continued on next page -

N/A		Location of all existing and proposed fire hydrants and a life safety plan in accordance with Section 3 of the Technical Manual;
N/A		Location, sizing, and directional flows of all existing and proposed utilities within the project site and on all abutting streets;
N/A		Location and dimensions of off-premises public or publicly accessible infrastructure immediately adjacent to the site;
N/A		Location and size of all on site solid waste receptacles, including on site storage containers for recyclable materials for any commercial or industrial property;
N/A		Plans showing the location, ground floor area, floor plans and grade elevations for all buildings;
N/A		A shadow analysis as described in Section 11 of the Technical Manual, if applicable;
N/A		A note on the plan identifying the Historic Preservation designation and a copy of the Application for Certificate of Appropriateness, if applicable, as specified in Section Article IX, the Historic Preservation Ordinance;
N/A		Location and dimensions of all existing and proposed HVAC and mechanical equipment and all proposed screening, where applicable;
N/A		An exterior lighting plan in accordance with Section 12 of the Technical Manual;
N/A		A signage plan showing the location, dimensions, height and setback of all existing and proposed signs;
X		Location, dimensions and ownership of easements, public or private rights of way, both existing and proposed.



Jeff Levine, AICP, Director
 Planning & Urban Development Department


Electronic Signature and Fee Payment Confirmation

Notice: Your electronic signature is considered a legal signature per state law.

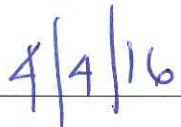
By digitally signing the attached document(s), you are signifying your understanding this is a legal document and your electronic signature is considered a **legal signature** per Maine state law. You are also signifying your intent on paying your fees by the opportunities below.

I, the undersigned, intend and acknowledge that no Site Plan or Historic Preservation Applications can be reviewed until payment of appropriate application fees are **paid in full** to the Inspections Office, City of Portland Maine by method noted below:

- Within 24-48 hours, once my complete application and corresponding paperwork has been electronically delivered, I intend to **call the Inspections Office** at 207-874-8703 and speak to an administrative representative and provide a credit/debit card over the phone.
- Within 24-48 hours, once my application and corresponding paperwork has been electronically delivered, I intend to **call the Inspections Office** at 207-874-8703 and speak to an administrative representative and provide a credit/debit card over the phone.
- I intend to deliver a payment method through the U.S. Postal Service mail once my application paperwork has been electronically delivered.



 Applicant Signature:



 Date:

 I have provided digital copies and sent them on:

 Date:

NOTE: All electronic paperwork must be delivered to buildinginspections@portlandmaine.gov or by physical means i.e. a thumb drive or CD to the Inspections Office, City Hall, 3rd Floor, Room 315.

**Parking Demands
Schlotterbeck & Foss Building
Portland, Maine
March 24, 2016
JN 3027**

Introduction:

Gorrill Palmer (GP) has evaluated the potential parking demand for the proposed redevelopment of the Schlotterbeck & Foss Building located in Portland, Maine. The site is located in the B-7 Mixed Development District Zone. The proposed building will have 1,750 square feet of office space and 56 market rate residential units with unbundled parking.

The proposed facility is located on an existing lot with City streets along each of the borders. The facility itself is the redevelopment of an existing building and not new construction. As such, accommodating the needs for off-street parking and the other requirements of the site is a balancing act.

After discussions with City Staff, the on-site parking has been maximized while balancing the other needs of the project. Once the maximum on-site parking was determined, GP could then make a determination as to if that parking is sufficient, or if other avenues for parking accommodations needed to be pursued. The following is a summary of the parking evaluation.

Vehicular Parking Evaluation:

Based on maximizing the use of the site and balancing green space, drainage needs, snow storage, pedestrian accommodations, and on-site parking, the number of on-site spaces is a total of 45, with 17 of those compact spaces.

Based on the City ordinance, the proposed 1,750 square foot office space would require 1 space per 400 square feet, which results in a required 5 parking spaces. This leaves 40 parking spaces to accommodate the 56 apartments.

Using the 40 parking spaces for 56 apartments yields a parking ratio of 0.71. The general parking requirement for the City for a market rate apartment is 1 space per unit. Based on discussions with the applicant for the West End project (also Market Rate), which has been constructed and is occupied, their actual parking rate is approximately 0.79. Based on our previous experience, other apartment buildings in downtown range from approximately 0.50 to 1.0, with previous usage of 0.70. It should be noted that the proposed apartments are referred to as micro-apartments because they generally do not exceed 500 square feet in size. We anticipate this type of tenant is more likely to use other forms of transportation such as bicycle, walking, taxi, UShare, or METRO.

Based on this analysis, it is our opinion that the parking ratio of 0.71 is appropriate for the proposed apartments in a downtown location.



Bicycle Parking:

Per City Ordinance, non-residential structures are required to provide bicycle accommodations in proportion to the vehicular parking. For residential structures 2 bicycle parking spaces are required for every 5 dwelling units and for non-residential structures 2 bicycle parking spaces are required for every 10 vehicle parking spaces. Based on the ordinance this project would require 23 residential and 2 non-residential bicycle parking spaces for a total of 25 bicycle parking spaces.

Conclusions:

In our opinion, 45 vehicular parking spaces and 25 bicycle parking spaces are adequate to accommodate the proposed project. However, this methodology needs to be reviewed and approved by the City.

Prepared by:
Randy Dunton, PE, PTOE
Senior Engineer

Prepared For:
 Owner and Applicant:
SCHLOTTERBECK BLOCK L.L.C.
 104 Grant Street
 Portland, Maine 04101
 Tel.: 207-650-8979

Prepared By:
MITCHELL & ASSOCIATES
 Landscape Architects
 70 Center Street
 Portland, Maine 04101
 Tel.: 207-774-4427

RANSOM CONSULTING ENGINEERS
 400 Commercial Street #4
 Portland, Maine 04101
 Phone: 207-772-2891

GODUTI-THOMAS ARCHITECTS
 44 Oak Street #2
 Portland, Maine 04101
 Phone: 207-775-3184

GENERAL NOTES

1. SITE AREA: 40,720 S.F. OR 0.93 ACRE
2. APPLICANT: SCHLOTTERBECK BLOCK, LLC
77 SPRUCE STREET
PORTLAND, MAINE 04101
3. RECORD OWNER: J.B. BROWN & SONS
482 CONGRESS STREET
P.O. BOX 207
PORTLAND, MAINE 04112
4. LOT DATA: TAX MAP 33, BLOCK E, LOT 1
C.C.R.D BOOK 26354, PAGE 312
AREA: 30,492 S.F.

TAX MAP 33, BLOCK E, LOT 5
C.C.R.D BOOK 26354, PAGE 314
AREA: 10,314 S.F.
5. THE SURVEYED PARCELS ARE SHOWN AS LOTS 1 AND 5, BLOCK E, ON THE CITY OF PORTLAND'S ASSESSORS MAP 33.
6. A PORTION OF THE PARCEL IS LOCATED, BY SCALING METHODS, WITHIN ZONE C (AREAS OF MINIMAL FLOODING) AS SHOWN ON THE FLOOD INSURANCE RATE MAP FOR THE CITY OF PORTLAND, MAINE COMMUNITY PANEL NO. 23005 1-001 3-B WITH AN EFFECTIVE DATE OF JULY 17, 1986. THE FLOOD ELEVATION FOR THE A2 ZONE WHICH ENCOMPASSES DACK COVE HAS AN ELEVATION OF 10 N.G.V.D. 1929. HOWEVER, THE TOPOGRAPHICAL SURVEY OF THE PARCEL SHOWS THE LOT ABOVE THE ELEVATION OF 10 N.G.V.D. (1929).
7. UTILITIES SHOWN ARE BASED ON A PLAN ENTITLED 'BOUNDARY AND TOPOGRAPHIC SURVEY OF 117 PREBLE STREET', PERFORMED BY OWEN HASKELL, INC., DATED JULY 7, 2015.
9. ZONING DISTRICT: B-7 MIXED DEVELOPMENT DISTRICT ZONE
10. SPACE AND BULK STANDARDS:

	REQUIRED	PROPOSED
MINIMUM LOT SIZE:	NONE	40,720 S.F.
MINIMUM FRONTAGE:	NONE	242 FEET
FRONT SETBACK:	NONE REQUIRED	18 FEET
SIDE SETBACK:	NONE REQUIRED	5 FEET
REAR SETBACK:	NONE REQUIRED	31 FEET
MAX. STREET SETBACK:	10 FEET	18 FEET (EXISTING)
MAX. BUILDING LOT COVERAGE:	100 PERCENT	68 PERCENT
MAX. BUILDING HEIGHT:	105 FEET	NA
MIN. BUILDING HEIGHT:	45 FEET	NA
11. BICYCLE PARKING:

REQUIRED:	25 SPACES (23 RESIDENTIAL, 2 COMMERCIAL)
PROPOSED:	26 SPACES (EXTERIOR)
12. ALL WORK WITHIN THE ROAD RIGHT-OF-WAY AND ALL UTILITIES CONNECTING TO INFRASTRUCTURE WITHIN THE ROAD SHALL MEET CITY OF PORTLAND TECHNICAL MANUAL STANDARDS.
13. PARKING SUMMARY:

PARKING REQUIRED PER PARKING ANALYSIS:	55 - 26 SPACES
PROPOSED PARKING:	45 SPACES
PROPOSED COMPACT (8' X 15'):	17 SPACES (38%)
REQUIRED A.D.A. ACCESSIBLE SPACES:	2 SPACES
PROPOSED A.D.A. ACCESSIBLE SPACES:	2 SPACES

The Schlotterbeck Block
 Portland, Maine
 117 Preble Street

Date: OCTOBER 5, 2015

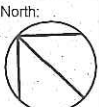
Issued For:

Revisions:
 DEC. 1, 2015: REVISED PER STAFF REVIEW COMMENTS
 DEC. 10, 2015: REVISED PER STAFF REVIEW COMMENTS
 FEB. 25, 2016: REVISED PER CONDITIONS OF APPROVAL
 APR. 5, 2016: AMENDED SUBDIVISION AND SITE PLAN SUBMISSION

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Title: LAYOUT AND LIGHTING PLAN

Scale: 1" = 20'

North:  Sheet No.: **L2.0**

LEGEND

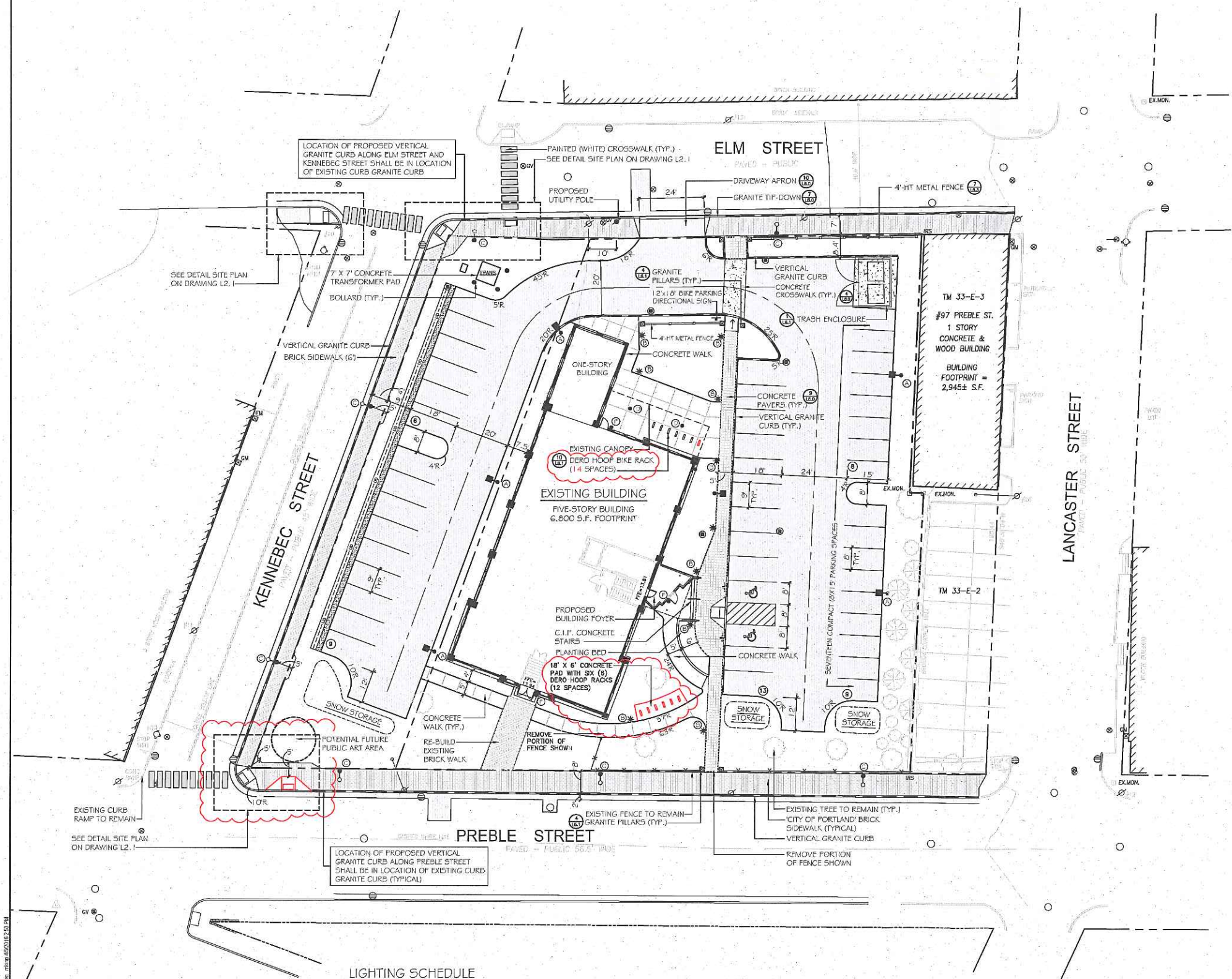
	EXISTING	PROPOSED
PROPERTY LINE		
IRON PIPE OR ROD FOUND		
MONUMENT FOUND		
UTILITY POLE		
MANHOLE		
ELECTRIC/GAS METER		
SIGN		
CATCH BASIN		
HYDRANT		
WATER VALVE		
LIGHT POLE		
FENCE		
GRANITE CURB		
OVERHEAD WIRES		
WATER LINE		
GAS LINE		
TELEPHONE		
STORM DRAIN		
SANITARY SEWER		
1' CONTOUR		
BRICK PAVERS		
CONCRETE PAVERS		
GRANITE PILLAR		

LIGHTING SCHEDULE

KEY	QTY	MANUFACTURER	CATALOG NUMBER	DESCRIPTION	LAMP
	6	KIM	RA17-3-E35-GOL	KIM 17" ERA LED - PARKING LOT LIGHT	LED, TYPE III, FULL CUT-OFF
	9	KIM	V5B1-15L-3K-JV-DB	KIM VANDAL RESISTANT BOLLARD	15L LED, FULL CUT-OFF
	7	HOLOPHANE	ESL 110 4K AS X	BAYSIDE DISTRICT FIXTURE MEDIUM LED TEARDROP	110 W 100 LED ARRAY
	2	KIM	FGL71-E35-GOL	KIM FGL7 LED - CANOPY LIGHT	GOL - LED
	3	KIM	9W3-10L3-KUV	KIM SITE WALLFORMS LED - BUILDING MOUNTED LIGHT (DOOR)	GOL - LED



PERMITTING DRAWINGS -- NOT FOR CONSTRUCTION



P:\Projects\2015\117 Preble Street\117 Preble Street.dwg (5/25/2016 10:25:29 AM)

DRAINAGE STRUCTURE SCHEDULE

ALL CATCH BASIN PRECAST CONCRETE STRUCTURES SHALL BE 4'-0" DIAMETER (INSIDE).

CB#1	RIM:	10.75
	INV. IN (8"):	8.60
CB#2	RIM:	11.10
	INV. IN (8"):	8.47
	INV. OUT (12"):	8.37
CB#3	RIM:	10.80
	INV. IN (12"):	8.07
	INV. OUT (12"):	7.97
CB#4	RIM:	10.40
	INV. IN (12"):	7.79
	INV. OUT (12"):	7.69
CB#5	RIM:	10.35
	INV. IN (6"RD):	7.47
	INV. IN (12"):	7.47
	INV. OUT:	7.37
CB#6	RIM:	9.40
	INV. IN (UD):	7.00
	INV. IN (12"):	7.00
	INV. OUT (12"):	6.90

EXIST. CB (ELM STREET)

RIM:	10.20
INV. IN (8"):	6.62 (EXISTING)
INV. IN (12"):	6.72 (PROPOSED: CORE DRILL)
INV. OUT (8"):	6.62 (EXISTING)

STORM DRAIN SCHEDULE

SD NO.	PIPE TYPE	LENGTH	SLOPE (FT/FT)
SD#1	8" HDPE	26 LF.	0.005
SD#2	12" HDPE	60 LF.	0.005
SD#3	12" HDPE	36 LF.	0.005
SD#4	12" HDPE	44 LF.	0.005
SD#5	12" HDPE	73 LF.	0.005
SD#6	12" SDR-35	16 LF.	0.005

GRADING AND DRAINAGE NOTES:

- DO NOT SCALE THESE DRAWINGS. ANY DISCREPANCIES BETWEEN DRAWINGS, DETAILS, NOTES AND SPECIFICATIONS SHALL BE IMMEDIATELY REPORTED TO THE LANDSCAPE ARCHITECT FOR FURTHER DIRECTION AND RESOLUTION BEFORE ADDITIONAL WORK PROCEEDS.
- CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND GRADES ON THE GROUND. ANY DISCREPANCIES SHALL BE REPORTED IMMEDIATELY TO THE LANDSCAPE ARCHITECT FOR DIRECTION AND RESOLUTION PRIOR TO ANY FURTHER WORK.
- CONTRACTOR SHALL FIELD VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES WITH THE APPROPRIATE UTILITY COMPANIES PRIOR TO EXCAVATION OF OR INSTALLATION OF ANY PART OF THIS WORK.
- CONTRACTOR SHALL AVOID ALL AREAS NOT REQUIRING GRADING. CONTRACTOR SHALL NOT DISTURB THESE AREAS.
- THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. CALL DIGSAFE (1-888-344-7233) AT LEAST THREE BUSINESS DAYS BEFORE PERFORMING ANY CONSTRUCTION.
- CONTRACTOR SHALL CONDUCT TEST PIT EXCAVATION TO CONFIRM LOCATION OF EXISTING UTILITIES WHERE NEW UNDERGROUND ELECTRICAL, TELEPHONE AND CATV SERVICES ARE PROPOSED.
- WHERE STORM DRAINS OR SEWER MAINS HAVE LESS THAN FOUR (4) FEET OF COVER BETWEEN TOP OF PIPE AND FINISH GRADE, INSULATE PIPE USING TWO (2) INCH THICK STYROFOAM INSULATION BOARD OVER AND ALONG SIDES OF THE PIPE.
- EXISTING DRAINAGE CATCHBASIN SHALL BE CORED TO ACCEPT NEW DRAINAGE PIPE AS NOTED.
- PIPE LENGTH EQUALS THE CENTER TO CENTER DISTANCES BETWEEN CATCH BASINS AND/OR MANHOLES MINUS ONE-HALF OF THE DIAMETER OF EACH CATCH BASIN OR MANHOLE.
- ALL UTILITIES SHALL BE INSTALLED IN CONFORMANCE WITH INDIVIDUAL UTILITY DISTRICT REQUIREMENTS; CONTRACTOR SHALL COORDINATE INSTALLATION WITH APPROPRIATE UTILITY DISTRICT BEFORE INSTALLATION.
- ALL WORK WITHIN THE ROAD RIGHT-OF-WAY AND ALL UTILITIES CONNECTING TO INFRASTRUCTURE WITHIN THE ROAD SHALL MEET CITY OF PORTLAND TECHNICAL MANUAL STANDARDS.
- DISCONTINUED UTILITY SERVICES INCLUDING WATER, GAS AND SANITARY SEWER SHALL BE PERMANENTLY CAPPED CONFORMING TO UTILITY COMPANY AND CITY OF PORTLAND STANDARDS. CONTRACTOR SHALL COORDINATE WITH PUBLIC SERVICES AND/OR UTILITY COMPANY.
- THE PROJECT BENCH MARK IS BONNET BOLT OF FIRE HYDRANT LOCATED AT INTERSECTION OF ELM STREET AND KENNEBEC STREET. BENCHMARK ELEVATION: 13.50.
- BMF INSPECTIONS: THE OWNER SHALL HIRE A QUALIFIED POST-CONSTRUCTION STORMWATER INSPECTOR TO ANNUALLY INSPECT THE BMFS, INCLUDING CATCH BASIN, DRYWELL, IN-LINE ROOF FILTERS, PIPES AND RELATED STRUCTURES, IN ACCORDANCE WITH ALL MUNICIPAL AND STATE INSPECTION, CLEANING AND MAINTENANCE REQUIREMENTS OF THE APPROVED POST-CONSTRUCTION STORMWATER MANAGEMENT PLAN (PER SECTION 32 OF THE CITY OF PORTLAND CODE OF ORDINANCES).
- SITE LIGHTING SHALL MEET THE CITY OF PORTLAND STANDARDS AS OUTLINED IN SECTION 1.2 OF THE CITY OF PORTLAND TECHNICAL DESIGN STANDARDS. SITE LIGHTING SHALL ADHERE TO SECTION 1.2.5 LIGHT TRESPASS-LIGHTING SHALL NOT BE GREATER THAN 0.1 FOOTCANDLES AT THE PROPERTY LINE.

Prepared For:
Owner and Applicant:
SCHLOTTERBECK BLOCK L.L.C.
104 Grant Street
Portland, Maine 04101
Tel: 207-650-8979

Prepared By:
MITCHELL & ASSOCIATES
Landscape Architects
70 Center Street
Portland, Maine 04101
Tel: 207-774-4127

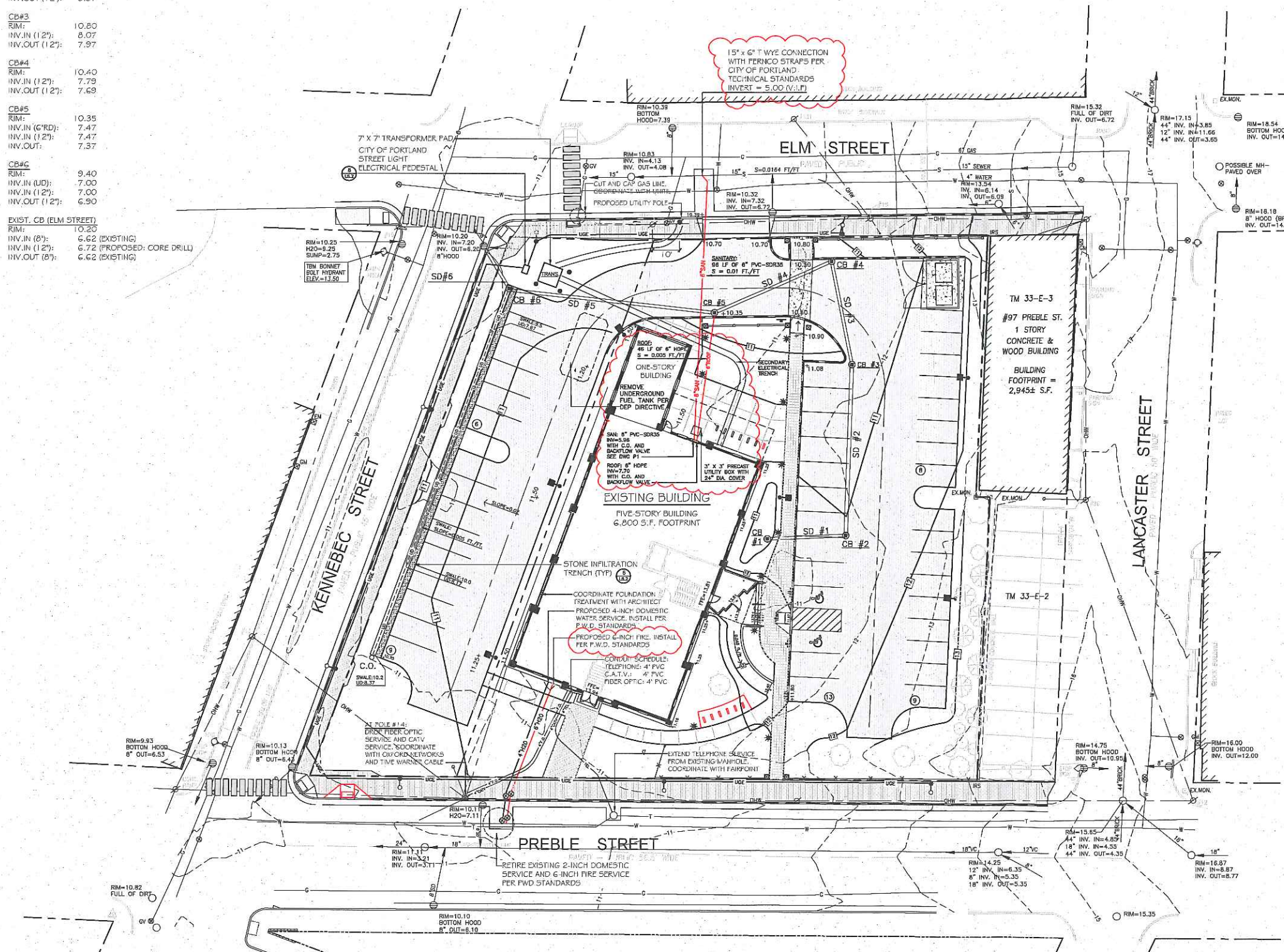
RANSOM CONSULTING ENGINEERS
410 Commercial Street # 1
Portland, Maine 04101
Phone: 207-772-2891

GODUTI-THOMAS ARCHITECTS
41 Oak Street # 2
Portland, Maine 04101
Phone: 207-773-3184

The Schlotterbeck Block

Portland, Maine

117 Preble Street



LEGEND

	EXISTING	PROPOSED
PROPERTY LINE	[Symbol]	[Symbol]
IRON PIPE OR ROD FOUND	[Symbol]	[Symbol]
MONUMENT FOUND	[Symbol]	[Symbol]
UTILITY POLE	[Symbol]	[Symbol]
MANHOLE	[Symbol]	[Symbol]
ELECTRICAL GAS METER	[Symbol]	[Symbol]
SIGN	[Symbol]	[Symbol]
CATCH BASIN	[Symbol]	[Symbol]
HYDRANT	[Symbol]	[Symbol]
WATER VALVE	[Symbol]	[Symbol]
LIGHT POLE	[Symbol]	[Symbol]
FOUNCE	[Symbol]	[Symbol]
GRANITE CURB	[Symbol]	[Symbol]
OVERHEAD WIRES	[Symbol]	[Symbol]
WATER LINE	[Symbol]	[Symbol]
GAS LINE	[Symbol]	[Symbol]
TELEPHONE	[Symbol]	[Symbol]
STORM DRAIN	[Symbol]	[Symbol]
SANITARY SEWER	[Symbol]	[Symbol]
1' CONTOUR	[Symbol]	[Symbol]
BRICK PAVERS	[Symbol]	[Symbol]
CONCRETE PAVERS	[Symbol]	[Symbol]
GRANITE PAVAR	[Symbol]	[Symbol]
CLEANOUT	[Symbol]	[Symbol]
STONE INFILTRATION TRENCH	[Symbol]	[Symbol]
SECONDARY ELECTRIC (FOR CITY LIGHTS)	[Symbol]	[Symbol]

Date: OCTOBER 5, 2015

Issued For:

Revisions:
DEC. 1, 2015: REVISED PER STAFF REVIEW COMMENTS
DEC. 10, 2015: REVISED PER STAFF REVIEW COMMENTS
FEB. 25, 2016: REVISED PER CONDITIONS OF APPROVAL
APR. 5, 2016: AMENDED SUBDIVISION AND SITE PLAN SUBMISSION

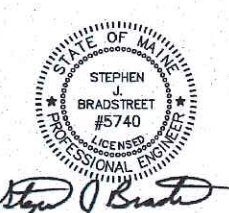
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Title:
GRADING, DRAINAGE AND UTILITIES PLAN

Scale: 1" = 20'

North: [Compass rose]

Sheet No.: L3.0



PERMITTING DRAWINGS -- NOT FOR CONSTRUCTION

project name

Schlotterbeck Block

117 Preble Street
Portland, Maine

Schlotterbeck Block LLC

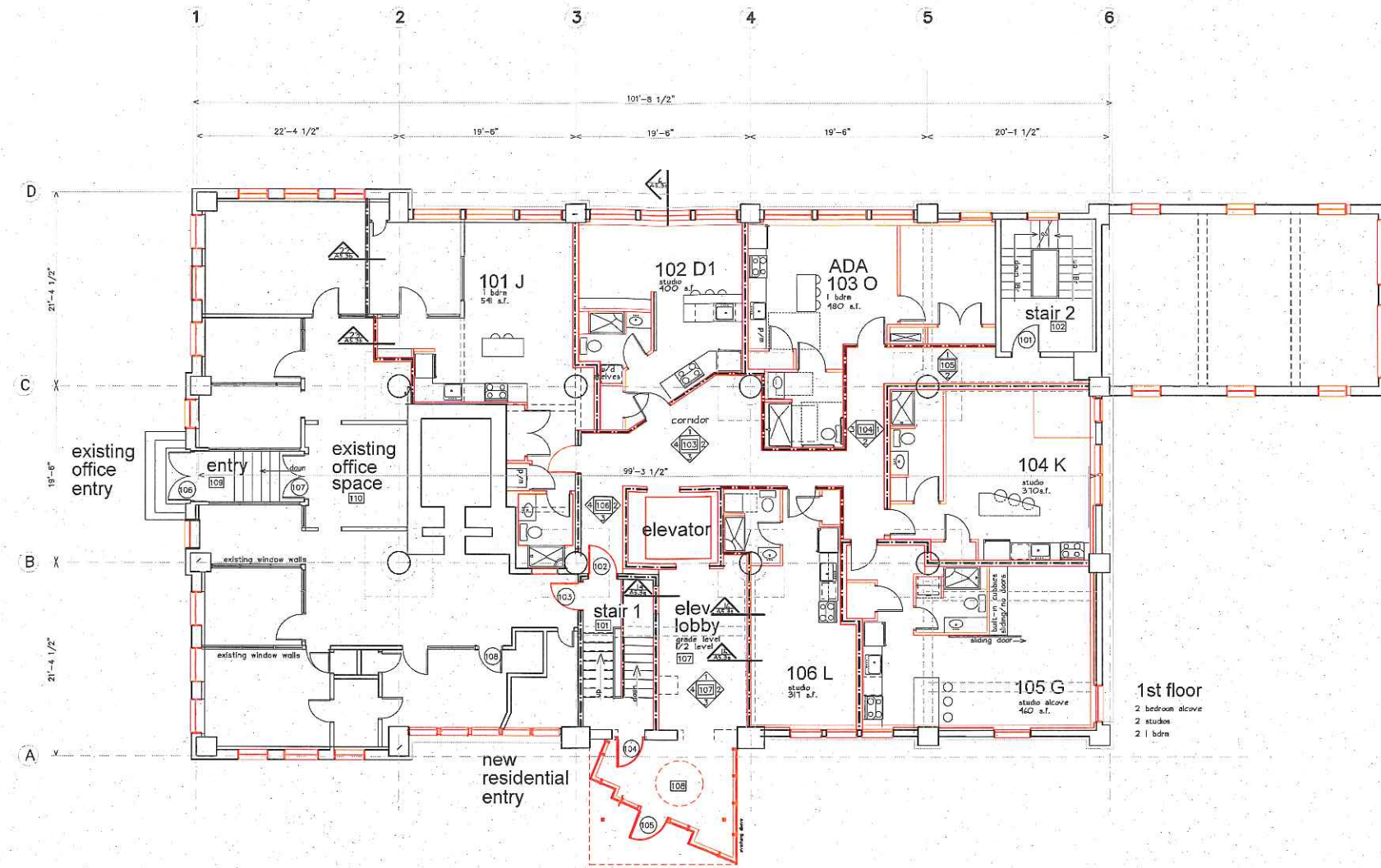
goduti/thomas architects

44 oak st.

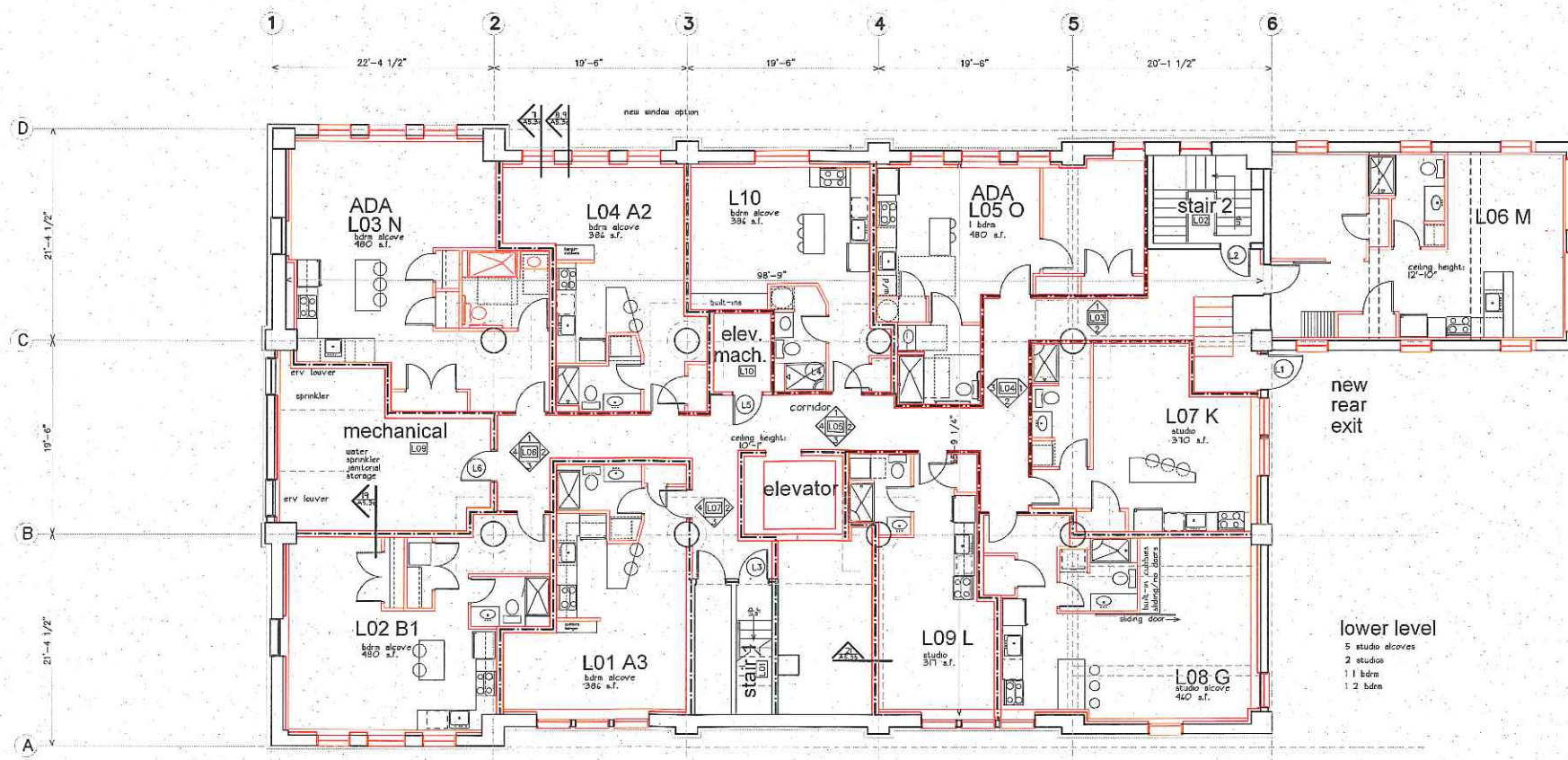
portland, maine 04101

ph. 207-775-3184

fax 207-774-0846



1st floor
2 bedroom alcove
2 studios
2 1 bdrm



lower level
5 studio alcoves
2 studios
1 1 bdrm
1 2 bdrm

revisions

date

march 15th, 2016

sheet title

**1st floor plan
lower level plan**

scale

1/8"=1'-0"

drawn by

MS

project number

foss2016

sheet number

A1.1



PLANNING BOARD REPORT PORTLAND, MAINE

Subdivision and Site Plan Amendments
117 Preble Street
Level III Amendment to Subdivision and Site Plan
Project ID #2016-078
The Schlotterbeck Building, Applicant

Submitted to: Portland Planning Board Public Hearing Date: April 26, 2016	Prepared by: Shukria Wiar, Planner Date: April 21, 2016
------------------------------------------------------------------------------	------------------------------------------------------------

I. INTRODUCTION

The Schlotterbeck Building has submitted a Level III Amendment application for site plan and subdivision review for their property at 117 Preble Street (refer to Applicant's submission). The property is bounded by Elm, Kennebec, and Preble Street. The purpose of this amendment is to create an additional residential unit, as well as modify the site plan to meet the conditions of approval for the original application.

The Planning Board held a public hearing on December 15, 2015 and approved the project with conditions of approval, please see approval letter ([Attachment 1](#)). The original proposal converted the existing Schlotterbeck and Foss building into a fifty-five (55) unit market-rate apartment building with approximately 1,750 square feet of office space. The building is five and a half stories above ground with six floors of living area. As part of the original application, the project proposed forty-five (45) parking spaces as part of this development. The applicant is seeking to amend the application by increasing the units from fifty-five (55) to fifty-six (56) residential units.

The original subdivision approval with all the associated waivers and conditions still apply. The Planning Board will review the proposed amended plan against the Subdivision and Site Plan Ordinance of Land Use Code.

Applicant Name	John Anton of Schlotterbeck Block, LLC
Consultants	
Agent	Michael King of Mitchell & Associates
Representative	
Engineer	Steve Bradstreet of Random Consulting, Inc.
Surveyor	Owen Haskell, Inc.
Architect	Rick Goduti of Goduti Thomas Architects

Project Review

<i>Review</i>	<i>Applicable Standards</i>
Site Plan	14-526
Subdivision	14-491

II. PROJECT DATA

Existing Zoning	B-7
Tax Map	CBL: 33 E001001 and 33 E005001
Existing Use	Industrial
Proposed Use	Residential (Rental Units)
Parcel Size	40,720 SF
Number of Units to be Demolished	None
Bedroom Mix	16 efficiencies 40 one-bedroom

Number of Affordable Units	The proposed units will be market rate.
Residential Data	56 units
Building Footprint	6,800 SF
Building Floor Area	37,345 SF
Impervious Surface Area	27,300 SF
Parking Spaces	45 spaces of which 17 are compact spaces
Bicycle Parking Spaces	24 spaces
Estimated Cost of Project	\$ 5,000,000

III. EXISTING CONDITIONS

The Schlotterbeck & Foss building was designed by John Calvin Stevens and built in 1925. It was the home of the Schlotterbeck & Foss Company until 2015 for the manufacturing of Schlotterbeck and Foss condiment products. The site includes two lots (Tax Map 33, Block E, Lots 1 and 5) totaling 40,702 square feet. The site contains the 6,800 sq. ft. existing building, a gravel parking lot of about twenty (20) parking spaces and a paved parking lot that accommodates about thirty (30) spaces. The site is surrounded by Preble, Elm and Kennebec Streets. Currently the main entrance to the building is from Preble Street.

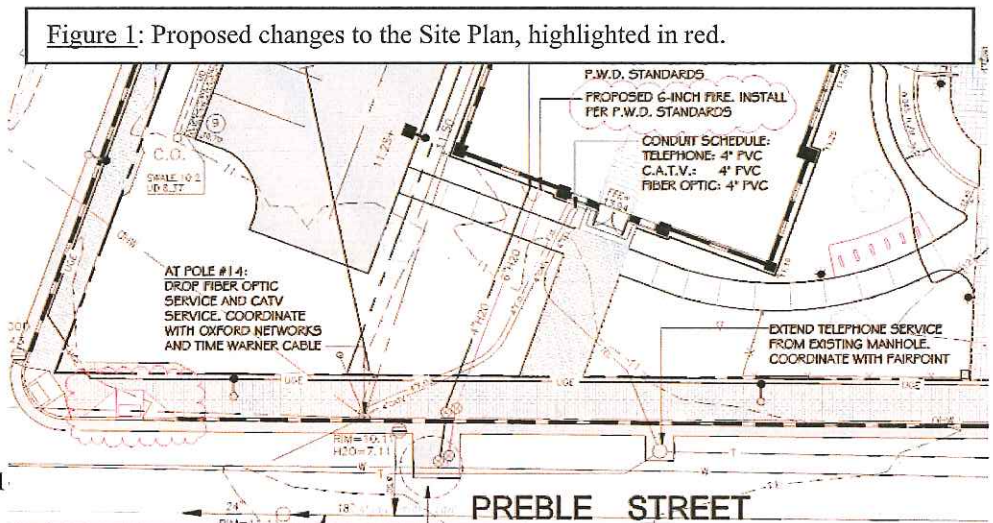
IV. PROPOSED DEVELOPMENT

As part of the amendment, the applicant now wants to create an additional studio apartment unit and will increase the units from fifty-five (55) units to fifty-six (56).

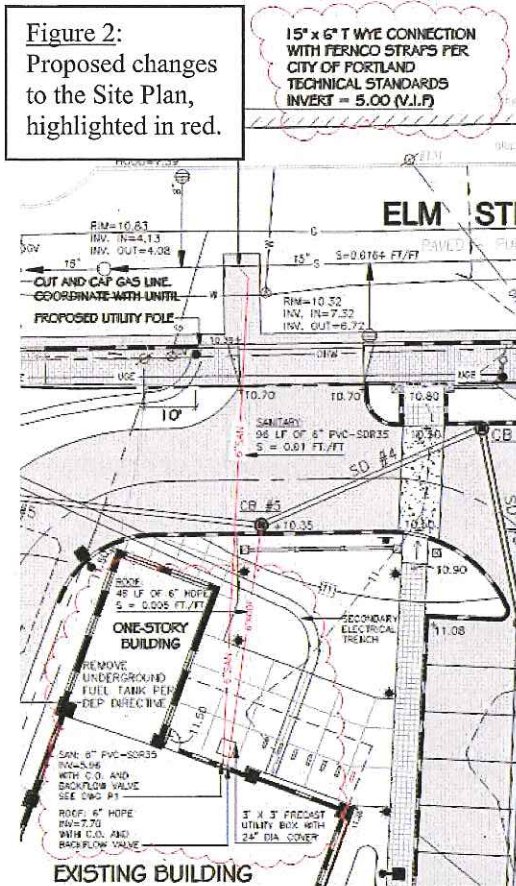
The applicant is also proposing to make changes to the site plan as well. The changes consist of the following:

1. **Additional Bicycle Racks:** The additional unit eliminates the bicycle storage room in the basement. As a result, fourteen (14) bicycle parking spaces need to be located on the site. The attached revised L2.0 show an additional bicycle hitch under the canopy and six (6) exterior bicycle racks on a concrete pad located near the building entrance. With these revisions, thirteen (13) bicycle loops are provided and 26 bicycle parking spaces will be provided.
2. **6-inch Fire Water Line:** Attached revised L3.0 adds a new six-inch fire suppression water line from Preble Street to the building's fire suppression system. The applicant had planned to continue using the existing 6-inch fire suppression line, but the existing 6-inch line is in poor condition.
3. **Roof Drain Connection:** Attached revised L3.0 shows a new 6" HDPE roof drain connecting to catch basin #5. The existing roof drain pipe is in poor condition and needs to be replaced.
4. **Sanitary Sewer Line:** Excavation exploration revealed the existing 6-inch cast iron sanitary sewer line is in poor condition. The applicant proposes to replace the existing sewer line with a new PVC sewer in approximately the same location and wye connect the 15-inch combined sewer on Elm Street.

5. **Additional Sidewalk Ramp and Pedestrian Easement:** The City's staff has requested an additional sidewalk ramp at the intersection of Preble and Kennebec Streets, as part of the December 2015 conditions of approval. The new ramp will require pedestrian easement on the



applicant's lot. The attached Amended Subdivision Plan and sheet L2.0 show the new sidewalk ramp and the pedestrian easement.



V. PUBLIC COMMENT

Upon receipt of the subject site plan application, staff noticed property owners within 500 feet of the site and placed a legal ad in the *Portland Press Herald* newspaper in accordance with requirements. Seventy-seven (77) notices were sent to area residents. A notice also appeared in the April 18th, 2015 and April 19th, 2016 editions of the *Portland Press Herald*.

The applicant is not required to hold a neighborhood meeting since the amendment is for an amendment to an approved subdivision. As of the date of writing this report, staff has not received any written public communication pertaining to this application.

VI. RIGHT, TITLE AND INTEREST AND FINANCIAL/TECHNICAL CAPACITY

The owner of the property is JB Brown. The applicant has provided a copy of a quitclaim deed, recorded at the Cumberland County Registry of Deeds (Book 26354 Page 314), and purchase and sale agreement, which demonstrates their right, title and interest in the property.

The estimated cost of the development is \$5,000,000.

VII. ZONING ASSESSMENT

The original plans had reviewed for the minimum requirements in the Urban Commercial Business (B7) zone and the project met the

zoning requirements. The purposed amendments also meet the zoning requirements. The additional unit is housed on the lower ground level of the building.

VIII. DEVELOPMENT REVIEW

A. SUBDIVISION PLAN AND RECORDING PLAT REQUIREMENTS (Section 14-496)

The applicant has submitted a recording plat. The final plat has been revised to reflect the new unit, the revised sidewalk easement, and any waivers and conditions of approval that relate to the subdivision plan.

B. SUBDIVISION (Section 14-497)

As an amendment to an approved subdivision, the subject application is subject to conformance with the applicable standards of the subdivision ordinance.

Section 14-496 (c) states:

Alterations to an approved plot. The planning authority may approve alterations to an approved recording plat when all of the following conditions are met; otherwise, a new subdivision plat must be submitted to the Planning Board:

- 1. The rearrangement of lot lines does not increase the number of lots within a block or other subdivision unit or area;*
- 2. The alteration will not affect any street, alley, utility easement or drainage easement;*
- 3. The alteration meets all of the minimum requirements of this article, article III of this chapter on zoning and other applicable state and local codes;*
- 4. The alteration is approved by the public works authority and the fire department.*

Such approved alterations shall be properly recorded in the registry within thirty (30) days thereof or they shall be null and void. Recording of approved alterations also shall be in accordance with the requirements of 30-A M.R.S.A. Section 4406.

Based on the application submitted, the project does not meet all of the above standards and therefore this application is before the Planning Board for review. The thirty (30) day recording applies to both Planning Authority and Planning Board review of amended subdivision plans.

As state before, the December 15, 2016 Planning Board approval of this project with all relevant waivers and conditions of approval remain in effect. The proposed amended plan has been reviewed by staff for conformance with the relevant review standards of Portland's Subdivision Ordinance and applicable regulations. Staff comments are listed below.

1. Will Not Result in Undue Water and Air Pollution (Section 14-497 (a) I), and Will Not Result in Undue Soil Erosion (Section 14-497 (a) 4)

As part of the amendment application, there is no proposed increase in site improvements or disturbance, so the Staff finds the proposed project in conformance with this standard.

2. Sufficient Water Available (Section 14-497 (a) 2 and 3)

The applicant has demonstrated in the original proposal that the project had sufficient water available, which can handle the additional unit. The project is currently served by the Portland Water District.

3. Will Not Cause Unreasonable Traffic Congestion (Section 14-497 (a) 5)

Tom Errico, P.E. Ty Lin has reviewed the original proposal. The proposed project is not anticipated to cause unreasonable traffic congestion. The staff finds the proposed project is in conformance with this standard.

4. Will Provide for Adequate Sanitary Sewer and Stormwater Disposal (Section 14-497 (a) 6), and Will Not Cause an Unreasonable Burden on Municipal Solid Waste and Sewage (Section 14-497 (a) 7)

As part of the amendment application, a new roof drain connection and replacement of an existing sewer line with a new PVC sewer is being proposed. Michael Farmer, Assistant Engineer, has reviewed the upgrades for the Department of Public Services and has the following recommendation:

The proposed new 6" sanitary sewer connection to the sewer main in Elm Street is OK. However, the connection should be made with an INSERTA TEE, (not a saddle Y attached with stainless steel bands), provided that the 15-inch sewer main is structurally sound. If the City inspector finds that the existing sewer main is not structurally sound, he or she may request that a new, replacement section of the existing main be installed with a factory manufactured T or Y fitting.

In the original application, the City of Portland has confirmed that the sewer system has sufficient capacity.

5. Scenic Beauty, Natural, Historic, Habitat and other Resources (Section 14-497 (a) 8)

The site is not within an historic district but the building is a historic building constructed in 1927, designed by John Calvin Stevens. The applicant is in the process of obtaining state and federal historic rehabilitation tax credits. According to the Applicant, Schlotterbeck Block LLC has received the first part of the approval for the building from the National Park Service (NPS). This approval means that the NPS has determined that the property appears to meet the National Register Criteria for Evaluation and will likely be listed in the National Register of Historic Places if nominated by the State Historic Preservation Officer. The second step is approval of the proposed renovations and the eligibility for state and federal historic rehabilitation tax credits. Ultimately, this building could be a designated historic local landmark.

6. Comprehensive Plan (Section 14-497 (a) 9)

The staff has identified the following goals and policies which are relevant to the proposed housing development and finds the proposal in conformance with the Comprehensive Plan:

Portland Housing Goal: Ensure that an adequate supply of housing is available to meet the needs, preferences, and financial capabilities of all Portland households, now and in the future.

Policies

- Ensure the construction of a diverse mix of housing types that offers a continuum of options across all income levels, which are both renter and owner-occupied, including but not limited to the following:
 - ii. Housing units for decreasing household size, such as young professionals, empty nesters, single-parent households, and senior citizens.
 - viii. Housing with a range of services and medical support for the elderly and special needs population, including assisted living, congregate care, group homes and nursing homes;
- Encourage higher density housing for both rental and home ownership opportunities, particularly located near services, such as schools, businesses, institutions, employers, and public transportation.
- Increase Portland's rental housing stock to maintain a reasonable balance between supply and demand yielding consumer choice, affordable rents, and reasonable return to landlords.
- Identify vacant land and redevelopment opportunities throughout the City to facilitate the construction of new housing.

C. SITE PLAN STANDARDS (Section 14-526)

The proposed amended plan has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and applicable regulations. Staff review and comments are listed below.

1. Transportation Standards

Impact on surrounding street systems, access and circulation, public transit access, and parking were reviewed under the original application by Tom Errico, P.E. TY Lin. The amendments to the site plan for the additional unit do not affect the transportation standards. Tom Errico, Consultant Traffic Engineer, reviewed the parking demand study and states:

The applicant has provided an updated parking analysis with the one additional residential unit. I find the conclusions to be reasonable given the likelihood that the tenant may not have a vehicle based on the small size of the unit.

I find the changes to bicycle parking to be acceptable.

Sidewalks and Ramps

As part of the original approval, there was a condition in regard to the design of the handicapped ramp at the Kennebec Street intersection with Preble Street are needed. The ramp needs to be fully ADA complaint and upgrades of the ramp. The condition of approval is stated below:

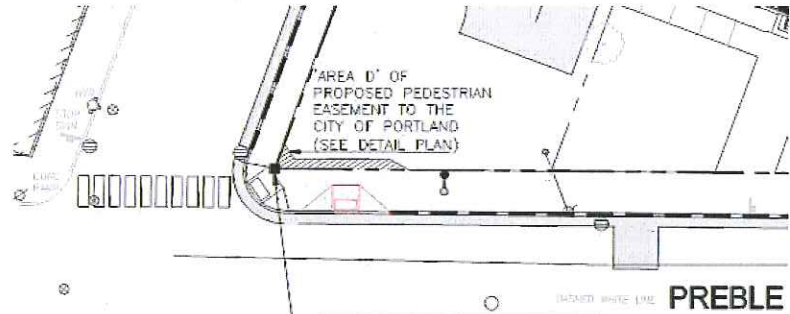
The details for the design engineering of the ramps shall be further reviewed by City staff and any possible adjustment shall be coordinated with Department of Public Works and the Planning Division. The applicant shall also provide a detail for Sidewalk Ramp Detectable Warning Panels in accordance with Figure I-7 of the City of Portland Technical Manual.

The applicant and staff has worked together to obtain a design that meets standards. As part of this amendment application, a revised design has been submitted for this ramp. Mr. Errico has reviewed proposal and states:

The applicant has provided a detail for the sidewalk ramps at the corner of Preble Street and Kennebec Street. The general configuration is acceptable. Some adjustments are suggested for consideration including 7-foot tip-down curbs at the Preble Street ramps. The City generally prefers a minimum 4-foot separation between ramps. I need to coordinate with DPW on this detail. The plan seems to indicate a low curb reveal between ramps that doesn't meet City standards. I would suggest that a condition of approval be required that notes final ramp details to be provided for review and approval prior to construction.

Therefore the original ramp design condition of approval will be retained as part of this amendment application.

Currently there is no sidewalk on the Kennebec Street side of the site. There are sidewalks on Elm and Preble Streets but the sidewalks are not in good condition and range in materials (from concrete to brick). Portions of the public sidewalk cross onto private property. The City Council has accepted public access easements where the sidewalks cross onto private property. The easement at the corner of Preble and Kennebec Streets will need to be revised to include a wider pedestrian way.



The amendments to the site plan for the additional unit do not affect the transportation standards.

2. Environmental Quality Standards

- a. *Preservation of Significant Natural Features and Landscaping and Landscape Preservation*
Jeff Tarling, City Arborist, had reviewed the landscaping plan as part of the original application. The amendments to the site plan do not affect the approved landscaping plan.
- b. *Water Quality, Storm Water Management and Erosion Control*
David Senus of Woodard and Curran, Consulting Engineer, had reviewed the stormwater management plan as part of the original application. The amendments to the site plan do not affect the stormwater standards.

3. Public Infrastructure and Community Safety Standards

- a. *Public Safety and Fire Prevention*
Assistant Chief Keith Gautreau has reviewed the amendment to the plans and does not have any comments. The upgraded six-inch water line for the fire suppression is acceptable.
- b. *Availability and Adequate Capacity of Public Utilities*
Please see Paragraph VIII (B) Subdivision, above.

4. Site Design Standards

- a. *Zoning Related Design Standards*
The amendment application is not proposing any changes to the building façade; therefore the design was not reviewed.

IX. STAFF RECOMMENDATION

Subject to the proposed motions and conditions of approval listed below, Planning Division staff recommends that the Planning Board approve the proposed amendments to the approved Subdivision and Site Plan.

X. PROPOSED MOTIONS

Subdivision Plat

On the basis of the subdivision plat approved on December 15, 2016 with conditions, the application for an amendment, plans, reports, and other information submitted by the applicant, findings and recommendations contained in Planning Board Report for application 2015-078 relevant to the Subdivision Ordinance and other regulations, as well as the Planning Board deliberations and the testimony presented at the Planning Board hearings, the Planning Board finds that the amended plan, when taken together with the original plan and the conditions contained therein (is or is not) in conformance with the subdivision standards and all other applicable provisions of the land use code, with the following condition:

1. The existing pedestrian easement for the ramp at the intersection of Kennebec and Preble Streets shall be reviewed by Corporation Counsel and approved by the City Council, prior to the certificate of occupancy.

6-0
Stanley
revised

Level III Site Plan

On the basis of the site plan approved December 15, 2016 with conditions, the application for an amendment, plans, reports, and other information submitted by the applicant, findings and recommendations contained in Planning Board Report for application 2016-078 relevant to the Site Plan Ordinance and other regulations, as well as the Planning Board deliberations and the testimony presented at the Planning Board hearings, the Planning Board finds that the amended plan, when taken together with the original plan and the conditions contained therein (is or is not) in conformance with the site plan standards and all other applicable provisions of the land use code, with the following condition:

Delete

6-0

1. The final details for the design of the ramps at Kennebec and Preble Streets shall be reviewed by City staff and any possible adjustment shall be coordinated with Department of Public Works and the Planning Division, prior to the construction of the ramps.
2. The proposed new six-inch sanitary sewer connection to the sewer main in Elm Street shall be made with an INSERTA TEE connection, (not a saddle Y attached with stainless steel bands). This detail shall be shown on the final plan.

ATTACHMENTS:

Staff Review Comments and Background Information

1. February 10, 2015 Approval Letter
2. Traffic Review (Tom Errico) comments dated 04.21.2016
3. DPW Engineer (Michael Farmer) comments dated 04.14.2016

Public comments (none at the time PB Report completed)

Applicant's Submittal

- A. Cover Letter
- B. Application
- C. Exhibit A Parking Memo 3-23-16
- D. Exhibit B 4-5-2016

Plans

- Plan1 1 OF 1 Subdivision REV.4-5-2016
- Plan2 L2.0 Layout REV. 4-5-2016
- Plan3 L3.0 Grading REV. 4-5-2016
- Plan4 A1.1 Floor Plan-Revised

issuance of a building permit with a copy to the Department of Public Services.

6. That all HVAC systems and external mechanical equipment shall meet the maximum allowable noise requirements of the zone; each unit shall submit documentation of dBA output to confirm compliance of both the unit and the building in respect of rated noise levels and cumulative noise levels, to the satisfaction of the Zoning Administrator prior to the issuance of a Building Permit for that unit.

The approval is based on the submitted plans and the findings related to site plan and subdivision review standards as contained in Planning Report for application 2015-174 which is attached.

STANDARD CONDITIONS OF APPROVAL

Please note the following standard conditions of approval and requirements for all approved site plans:

1. **Subdivision Recording Plat** A revised recording plat listing all conditions of subdivision approval must be submitted for review and signature prior to the issuance of a performance guarantee. The performance guarantee must be issued prior to the release of the recording plat for recording at the Cumberland County Registry of Deeds.
2. **Subdivision Waivers** Pursuant to 30-A MRSA section 4406(B)(1), any waiver must be specified on the subdivision plan or outlined in a notice and the plan or notice must be recorded in the Cumberland County Registry of Deeds within 90 days of the final subdivision approval).
3. **Develop Site According to Plan** The site shall be developed and maintained as depicted on the site plan and in the written submission of the applicant. Modification of any approved site plan or alteration of a parcel which was the subject of site plan approval after May 20, 1974, shall require the prior approval of a revised site plan by the Planning Board or the Planning Authority pursuant to the terms of Chapter 14, Land Use, of the Portland City Code.
4. **Separate Building Permits Are Required** This approval does not constitute approval of building plans, which must be reviewed and approved by the City of Portland's Inspection Division.
5. **Site Plan Expiration** The site plan approval will be deemed to have expired unless work has commenced within one (1) year of the approval or within a time period up to three (3) years from the approval date as agreed upon in writing by the City and the applicant. Requests to extend approvals must be received before the one (1) year expiration date.
6. **Subdivision Plan Expiration** The subdivision approval is valid for up to three years from the date of Planning Board approval.
7. **Performance Guarantee and Inspection Fees** A performance guarantee covering the site improvements as well as an inspection fee payment of 2.0% of the guarantee amount and seven (7) final sets of plans must be submitted to and approved by the Planning Division and Public Services Department prior to the release of a subdivision plat for recording at the Cumberland County of Deeds, and prior to the release of a building permit, street opening permit or certificate of occupancy for site plans. If you need to make any modifications to the approved plans, you must submit a revised site plan application for staff review and approval.
8. **Defect Guarantee** A defect guarantee, consisting of 10% of the performance guarantee, must be posted


before the performance guarantee will be released.

9. **Preconstruction Meeting** Prior to the release of a building permit or site construction, a pre-construction meeting shall be held at the project site. This meeting will be held with the contractor, Development Review Coordinator, Public Service's representative and owner to review the construction schedule and critical aspects of the site work. At that time, the Development Review Coordinator will confirm that the contractor is working from the approved site plan. The site/building contractor shall provide three (3) copies of a detailed construction schedule to the attending City representatives. It shall be the contractor's responsibility to arrange a mutually agreeable time for the pre-construction meeting.
10. **Department of Public Services Permits** If work will occur within the public right-of-way such as utilities, curb, sidewalk and driveway construction, a street opening permit(s) is required for your site. Please contact Carol Merritt at 874-8300, ext. 8828. (Only excavators licensed by the City of Portland are eligible.)
11. **As-Built Final Plans** Final sets of as-built plans shall be submitted digitally to the Planning Division, on a CD or DVD, in AutoCAD format (*.dwg), release AutoCAD 2005 or greater.
12. **Mylar Copies** Mylar copies of the as-built drawings for the public streets and other public infrastructure in the subdivision must be submitted to the Public Services Dept. prior to the issuance of a certificate of occupancy.

The Development Review Coordinator must be notified five (5) working days prior to date required for final site inspection. The Development Review Coordinator can be reached at the Planning Division at 874-8632. All site plan requirements must be completed and approved by the Development Review Coordinator prior to issuance of a Certificate of Occupancy. Please schedule any property closing with these requirements in mind.

If there are any questions, please contact Shukria Wiar at 756-8083 or via shukriaw@portlandmaine.gov

Sincerely,



Elizabeth Boepple, Chair
Portland Planning Board

Attachments:

1. Planning Board Report
2. City Code, Chapter 32
3. Sample Stormwater Maintenance Agreement
4. Performance Guarantee Packet

Electronic Distribution:

Jeff Levine, AICP, Director of Planning and Urban Development
Stuart G. O'Brien, City Planning Director
Barbara Barhydt, Development Review Services Manager
Shukria Wiar, Planner
Philip DiPierro, Development Review Coordinator, Planning
Ann Machado, Zoning Administrator, Inspections Division
Tammy Munson, Inspections Division Director

Jonathan Rioux, Inspections Division Deputy Director
Jeanie Bourke, Plan Reviewer/CEO, Inspections Division
Brad Saucier, Administration, Inspections Division
Katherine Earley, Engineering Services Manager, Public Services
Bill Clark, Project Engineer, Public Services
David Margolis-Pineo, Deputy City Engineer, Public Services
Doug Roncarati, Stormwater Coordinator, Public Services

Greg Vining, Associate Engineer, Public Services
Michelle Sweeney, Associate Engineer
John Low, Associate Engineer, Public Services
Rhonda Zazzara, Field Inspection Coordinator, Public Services
Mike Farmer, Project Engineer, Public Services
Jane Ward, Administration, Public Services
Jeff Tarling, City Arborist, Public Services

Jeremiah Bartlett, Public Services
Keith Gautreau, Fire Department
Jennifer Thompson, Corporation Counsel
Thomas Errico, P.E., TY Lin Associates
David Senus, P.E., Woodard and Curran
Rick Blackburn, Assessor's Department
Approval Letter File



PLANNING BOARD REPORT PORTLAND, MAINE

Schlotterbeck and Foss Building
117 Preble Street
Level III Subdivision and Site Plan Review
2015-174
Schlotterbeck Building, LLC, Applicant

Submitted to: Portland Planning Board:
Public Hearing Date: December 15, 2015

Prepared by: Shukria Wiar
Date: December 10, 2015

I. INTRODUCTION

John Anton of Schlotterbeck Block, LLC has submitted a Level III Site Plan and Subdivision application for the proposed redevelopment of the Schlotterbeck and Foss Building at 117 Preble Street. The structure will be converted into a fifty-five (55) unit market-rate apartment building with approximately 1,750 square feet of office space. The building is five and a half stories above ground with six floors of living area. The project is proposing forty-five (45) parking spaces as part of this development. The site is located in the B-7 zone and subject to the B-7 design standards.

The Planning Board held a workshop on this project on November 24, 2015. Overall the Board supported the project. The Board did have a few concerns with the layout of the site. They thought the project was too “suburban” in an urban location. They encouraged changes to make the site pedestrian friendly with more tenants’ amenities, specifically at the corner of Kennebec and Preble Streets. The Board also felt the amount of pavement on site could be decreased. The last issue was the landscaping and the Board felt that the planting could be improved.



The applicant has revised their site plan to meet these concerns. They have submitted a cover letter, dated December 10, 2015 that address the proposed changes. A summary of the changes are:

1. Pedestrian Accessibility:

- Connecting Preble Street and Elm Street with a 5’-wide walkway through the site. A section of the existing fence along Preble Street will be removed to accommodate the walkway.
- Concrete pavers will be used as the connecting walkway’s pavement material.
- Introduction of Granite pillars will function as pedestrian gateways to the walkway from Elm Street and Preble Street.
- Additional landscaping has been added along the walkway.
- Vertical granite curbing has been added at the Elm Street gateway.
- Curbing will provide separation between vehicular and pedestrian circulation.
- A concrete crosswalk has been added where the walkway crosses the drive aisle.

2. Scale of Parking:

- The revised site plan includes the addition of planting islands at both the center parking area and the parking lot along Kennebec Street.
 - Parking has been deleted from the front yard along Preble Street.
 - The revised site plan has 45 parking spaces of which 17 compact spaces.
3. Uplighting: This has been eliminated from the project.

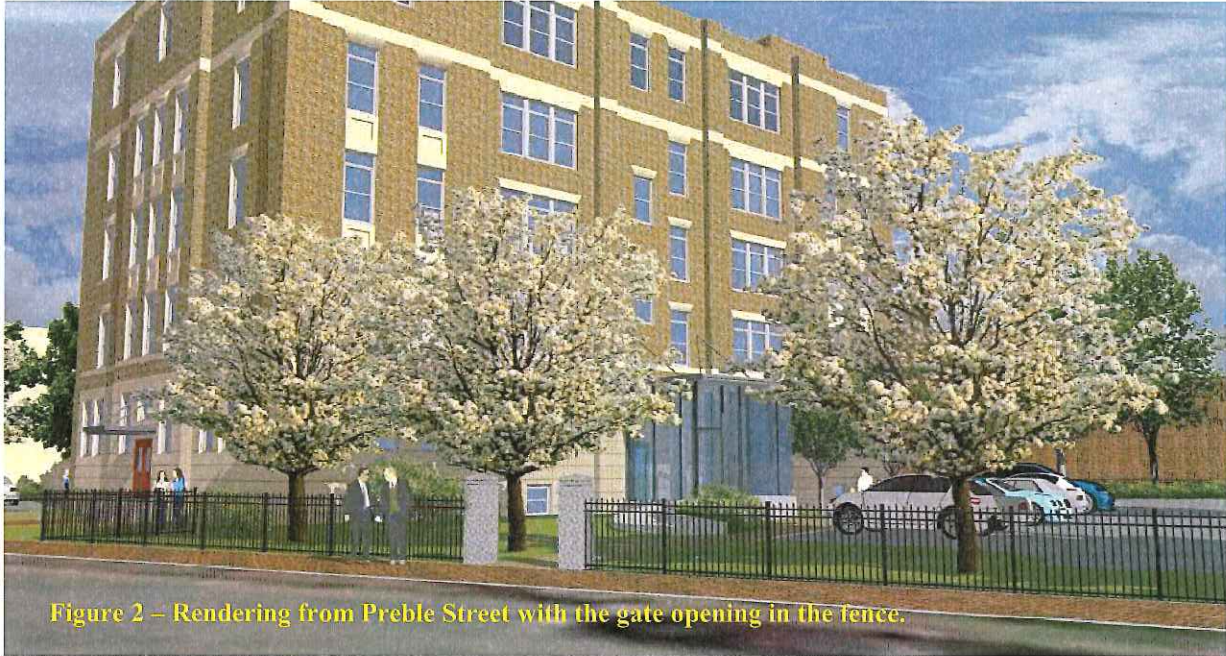


Figure 2 – Rendering from Preble Street with the gate opening in the fence.

This proposal is being reviewed as a final plan and subject to the Site Plan and Subdivision Ordinance of Land Use Code. The project will also be reviewed against the *B-7 Bayside Use Urban District* zone and the *Two-Family, Special Needs Independent Living Units, Multiple-Family, Lodging Houses, Bed and Breakfasts, and Emergency Shelters Standards* (Sections (d) and (I), and Appendix 2 of the Design Manual).

Seventy-four (74) notices were sent to area residents within 500 feet of the site and the interested party list. A notice also appeared in the December 7th and 8th editions of the *Portland Press Herald*.

Applicant Name	John Anton of Schlotterbeck Block, LLC
Consultants	
Agent Representative	Michael King of Mitchell & Associates
Engineer	Steve Bradstreet of Random Consulting, Inc.
Surveyor	Owen Haskell, Inc.
Architect	Rick Goduti of Goduti Thomas Architects

Project Review

<i>Review</i>	<i>Applicable Standards</i>
Site Plan	14-526
Subdivision	14-491

Design Manual

B-7 Bayside Use Urban District zone, and
 (i) Two-Family, Special Needs Independent Living Units, Multiple-Family, Lodging Houses, Bed And Breakfasts, And Emergency Shelters

Requests for Waivers

Waiver Request

Street Tree Requirement – Sec. 14-526 2.b.iii. a

The street tree requirement is one tree per unit for a total of 55 trees. There are existing streets trees that the applicant will get credit for (5 trees on Preble Street). The applicant is proposing to plant 15 street trees. The remaining street trees that applicant is requesting a waiver for 35 trees of financial contribution of \$7,000 to the City’s Tree Fund. Jeff Tarling, City Arborist supports the waiver.

Parking Lot and Parking Space Design (Technical Design Manual 1.14) - The applicant is requesting a waiver in order to increase the amount of compact parking spaces. The applicant proposes 17 compact (8' x 15') parking spaces, which would comprise 38% of the total parking spaces. Tom Errico, Consulting Engineer, supports the waiver.

Travel Aisle Width- The applicant is requesting to reduce the width of the parking lot travel aisles from 24 feet to 20 feet. Mr. Errico supports the waiver.

Applicable Standards

Sec. 14-526 2.b.iii (b) Waiver. Where the applicant can demonstrate that site constraints prevent the planting of required street trees in the City right of way, the Reviewing Authority may permit the planting of street trees in the front yard, within ten feet of the property line. Existing preserved healthy trees that are six (6) inches or more in caliper and are on the site within ten (10) feet of the property line may be counted towards this requirement.

Technical Design Standard Section 1.14. Parking Lot and Parking Space Design - Any parking lot with 10 or fewer spaces shall contain standard sized parking spaces. Parking lots with greater than 10 spaces may be comprised of up to 20% compact parking spaces.

Technical Design Standard Section 1.14. Parking Lot and Parking Space Design- Vehicular access shall be provided by one or more aisles. Minimum widths of aisles are illustrated in Figures I-29 (for 90° parking, 24’ of aisle width is required).

II. PROJECT DATA

Existing Zoning	B-7
Tax Map	CBL: 33 E001001 and 33 E005001
Existing Use	Industrial
Proposed Use	Residential (Rental Units)
Parcel Size	40,720 SF
Number of Units to be Demolished	None
Bedroom Mix	15 efficiencies 40 one-bedroom
Number of Affordable Units	The proposed units will be market rate.
Residential Data	55 units
Building Footprint	6,800 SF
Building Floor Area	37,345 SF
Impervious Surface Area	27,300 SF
Parking Spaces	45 spaces of which 17 are compact spaces
Bicycle Parking Spaces	24 spaces
Estimated Cost of Project	\$ 5,000,000

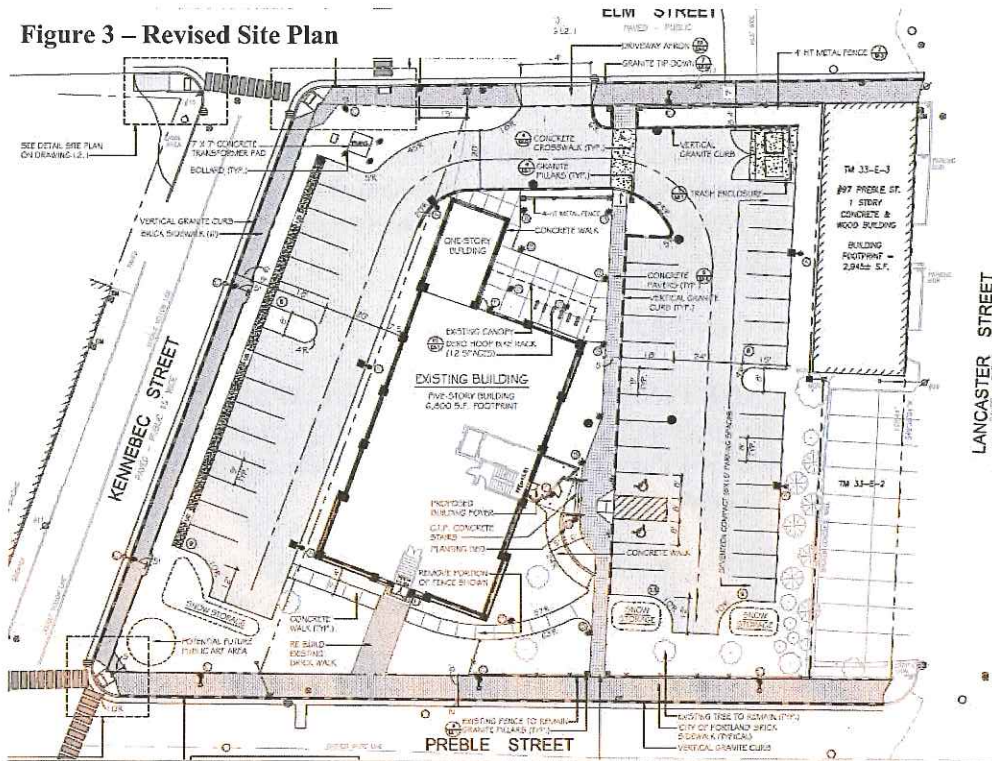
III. EXISTING CONDITIONS

The Schlotterbeck & Foss building was designed by John Calvin Stevens and built in 1925. It was the home of the Schlotterbeck & Foss Company until 2015 for the manufacturing of Schlotterbeck and Foss condiment products. The site includes two lots (Tax Map 33, Block E, Lots 1 and 5) totaling 40,702 square feet. The site contains the 6,800 sq. ft. existing building, a gravel parking lot of about twenty (20) parking spaces and a paved parking lot that accommodates about thirty (30) spaces. The site is surrounded by Preble, Elm and Kennebec Streets. Currently the main entrance to the building is from Preble Street.

IV. PROPOSED DEVELOPMENT

The proposal, including floor plans and elevations, are included in the plan set, Attachment Plans. The development project is proposed as the renovation of an industrial building with office space at the first level. The building is five and a half stories above ground with six floors of living area. The building will be fully renovated including masonry restoration, full window replacement and the installation of new systems.

Figure 3 – Revised Site Plan



The main vehicle access is proposed from Elm Street via a new 24 foot wide drive. Surface parking lots surround the existing building and are proposed along Kennebec Street and Elm Street. Originally the applicant was proposing fifty-five (55) parking spaces. Based upon a revised parking demand analysis, the number is of parking spaces is forty-five (45). Twenty-four bicycle parking spaces proposed on the site. The parking areas are visible from the public

streets. The revised parking plan has eliminated the proposed care share for tenants. Please note the U-haul car share vehicles are available on Elm Street at the Public Library. Designated areas for snow storage are located adjacent to parking spaces.

Figure 4 – Rendering of Corner of Kennebec and Preble Streets



The corner of Kennebec and Preble Streets was discussed at the Planning Board workshop. The applicant has identified the corner of this corner as potential location for public art (see Street View rendering on the left). The applicant states

that the public art at the proposed location has the potential to function serve as a gateway to Bayside.

V. PUBLIC COMMENT

The applicant held two neighborhood meetings; on October 29th and November 2nd. The first meeting was not well attended because the Bayside Transportation Master Plan meeting was held at the same time; the applicant held a second meeting and invited the Bayside Neighborhood Association on November 2nd. The certification of the meeting and minutes has submitted, see [Attachment Y](#).

No public comments have been submitted as of writing this memo.

VI. RIGHT, TITLE AND INTEREST AND FINANCIAL/TECHNICAL CAPACITY

a. The owner of the property is JB Brown. The applicant has provided a copy of a quitclaim deed, recorded at the Cumberland County Registry of Deeds (Book 26354 Page 314), and purchase and sale agreement, which demonstrates their right, title and interest in the property.

b. The estimated cost of the development is \$5,000,000. The applicant has submitted a letter from NBT Bank, dated September 22, 2015, as demonstration of their financial and technical capacity to complete the proposed development.

VII. ZONING ASSESSMENT

A. Surface Parking Lot

The applicant is proposing parking along the Kennebec and Elm Streets that is within the 35 foot parking setback requirement in the B7 zone. The City Council on November 16, 2015 approved a text amendment to the B-7 zone which stipulates that a gravel surface parking lot that does not meet the 35-foot parking setback may be permitted subject to meeting the items in the Section 14-295.a.26. The applicant and staff are working together to have the surface parking lots comply with this standard. Below is the summary of how the application is meeting the standards:

(26) Notwithstanding Sec. 14-295(a) (24) and (25) above, surface parking

that does not meet the 35- foot parking setback, provided that:

a. All or a portion of the 35 foot setback area had a gravel surface on September 29, 2015.

Staff Comments: According to the application, the gravel parking lot has existed as of March 9, 2005 and has been in continuous existence since then.

b. The total gravel surface area on the lot and any contiguous lots did not exceed 15,000 square feet on September 29, 2015.

Staff Comments: The surface parking lot proposed for this project does not exceed 15,000 sq. ft.

c. The parking spaces provide parking to a principle building on the same lot and or a principle building on a contiguous lot.

Staff Comments: The parking lot is to provide parking to a principle building existing on the same site.

d. One of the buildings described in paragraph c above meets the minimum height requirements of the Bayside Height Overlay Map and/or a building on the site has a floor area of 25,000 SF or greater.

Staff Comments: The building is an existing building that will be renovated with proposed floor area of 37,345 sq. ft.

e. The total number of spaces within 35 foot setback in combination with other spaces on the lot does not exceed the minimum parking spaces required in sec. 14-526 (a) (4) (a) (1).

Staff Comments: The applicant is proposing to have forty-five (45) parking spaces onsite. Sec. 14-526 (a)(4)(a)(1) gives the Planning Board the right to establish the parking requirement for the project.

f. The proposed parking spaces meets the landscape and buffer requirements of 14-299 (F).

Staff Comments: The planting/landscaping plan shows that the parking lots are well landscaped with a landscaped island and is buffered from the street right-of-way.

g. Parking spaces within the 35 foot setback shall provide stormwater quality treatment if required by the City of Portland Stormwater Management Standards and the Maine DEP Chapter 500 Stormwater Management Standards. If not required, an alternative low impact development treatment system approved by the Planning Board shall be provided.

Staff Comments: David Senus, Consultant Engineer, has reviewed the stormwater and plans against this standards and states the following:

The spaces proposed within the 35' setback are located in an area that is currently gravel. Because the overall project will result in a net reduction of impervious area, treatment under the Maine DEP Chapter 500 Stormwater Management Standards is not required. We would consider the "Stone Infiltration Trench" as depicted in the detail on L6.2 to suffice as an acceptable low impact development treatment system, as this detail includes a loam and sand filter layer. As such we find the project to be in compliance with the General Standards and the City's B-7 zone requirements for water quality treatment.

B. Performance Standards

The B7 zone includes a specific requirement (14-332.2.c) for the submission of a parking analysis and for the Planning Board to establish the parking requirement for the project. The applicant has submitted a parking demand study (Attachment M) which states that the proposed project parking demand is at a ratio of .80 parking spaces per unit. The applicant is proposing forty-five (45) parking spaces for the residential units and the office spaces.

Under external effects, Section 14-299 Performance Standards (b), (f), (n) and (o) apply to this project. Below is how the project is meeting the standards:

Sec. 14-299. Performance standards: All uses in the B-7 mixed development district zone shall comply with the following standards. Standards 14-299(a), (f), (j), (m), (n) and (o) below shall be reviewed by the Planning Authority.

(b) **Noise:** (A List of standards is itemized)

Staff Comments: The maximum noise requirement of 60 decibels at lot boundaries shall be maintained during the "day time" hours. This information will have to be submitted for review and approval for all mechanical equipment being proposed.

(f) **Off-street parking and loading:** Off-street parking is required as provided in division 20 (off-street parking) of this article. Division 21 (off-street loading) of this article shall not apply. Surface parking shall meet the following standards:

1. None of the spaces shall occupy all or a portion of a sidewalk within a street right-of-way.

Staff Comments: The surface parking lots do not occupy any portions of the sidewalks. The lots are setback from the property lines.

2. Driveway entrances shall meet the City of Portland Technical Manual standards.

Staff Comments: The proposed entrance to the site meets City standards.

3. The parking area shall meet the landscape and buffer standards of the City of Portland Technical and Design Manuals.

Staff Comments: The planting/landscaping plan shows that the parking lots are well landscaped with a landscaped island and is buffered from the street right-of-way.

(n) **Lighting:** All lighting shall be designed and installed with cut-off fixtures to direct illumination onto the site and to prevent illumination from such fixtures on neighboring properties and as otherwise governed by the site lighting standards of the City of Portland Technical Manual.

Staff Comments: The photometric plans show no light pollution onto neighboring properties. The majority of the proposed lighting fixtures meet the cut-off standards except for the bollards. The applicant stated in the cover letter that these will be full cutoffs and has submitted a catalogue cut of the fixture. The revised detail to the bollard shows a cover that limits the light to a 90° angle. The staff is recommending approval of the fixtures as proposed.

(o) **Building entrances:** All buildings shall have a minimum of one (1) public pedestrian entrance facing the street frontage of the lot. Such building entrances shall also be reviewed under the B-7 bayside design standards.

Staff Comments: The building has two main entrances for the public. One of the entrances is on Preble Street and one is to the side of the building off of Preble Street. The entrances are highly visible from the street right-of-way.

VIII. DEVELOPMENT REVIEW

A. SITE PLAN SUBMISSION REQUIREMENTS (Section 14-527) and SUBDIVISION PLAT AND RECORDING PLAT REQUIREMENTS (Section 14-496)

The applicant has submitted a recording plat. The plat will need to note the number of proposed units and associated parking on site. The final plat will need to be revised to reflect any waivers and conditions of approval that relate to the subdivision plan. Any waivers granted must be recorded at the Registry of Deeds within 90 days of a Planning Board decision. Accordingly, review and approval of the final plat has been included as a condition of approval.

It should be noted that the plat depicts sidewalk areas on private property that will be contiguous with the right-of-way. It is assumed that these will be subject to public pedestrian easements and this agreement has been included as a condition of approval.

B. SUBDIVISION (Section 14-497)

The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's Subdivision Ordinance and applicable regulations. Staff comments are listed below.

1. Will Not Result in Undue Water and Air Pollution (Section 14-497 (a) 1), and Will Not Result in Undue Soil Erosion (Section 14-497 (a) 4)

David Senus, P.E. with Woodard and Curran Engineering, has reviewed the erosion and sediment control plan and finds it acceptable; please see Attachment 3 for the complete analysis.

2. Sufficient Water Available (Section 14-497 (a) 2 and 3)

The project will be served by the existing utility services located in Preble Street. The applicant will be upgrading the 2-inch domestic water service to a 4-inch domestic water service as part of this project. The Applicant has received an ability to serve letter from the Portland Water District.

3. Will Not Cause Unreasonable Traffic Congestion (Section 14-497 (a) 5)

The site is surrounded by Preble, Elm and Kennebec Streets. The vehicle entrance to the site will be off of the existing curb cut on Elm Street. The applicant has submitted a Traffic Assessment for the project with the following three conclusions:

1. The proposed development is forecast to generate 4 trip ends and 9 trip ends during the weekday AM and PM peak hours respectively. This level of trip generation does not require a MaineDOT traffic movement permit. Since the proposed net trip generation is forecast to be low, and the Somerset Street Expansion project will change traffic patterns on the adjacent roadway network, capacity and queuing analysis were not necessary.
2. The sight distance exiting the site exceeds both MaineDOT and City requirements.
3. The MaineDOT crash data indicates that there are two high crash locations in the vicinity of the site; the intersection of Elm Street with Lancaster Street and the intersection of Kennebec Street with Preble Street. The Somerset Street Extension project will modify the intersection of Kennebec Street and Preble Street.

Tom Errico, Consultant Traffic Engineer, has reviewed the report and states that he concurs with the contents of the report.

4. Will Provide for Adequate Sanitary Sewer and Stormwater Disposal (Section 14-497 (a) 6), and Will Not Cause an Unreasonable Burden on Municipal Solid Waste and Sewage (Section 14-497 (a) 7)

An existing 6-inch sanitary sewer line connects the existing building to the 15-inch combined sewer in Elm Street. The applicant has received a wastewater capacity letter from David Margolis-Pineo, Deputy Engineer.

5. Scenic Beauty, Natural, Historic, Habitat and other Resources (Section 14-497 (a) 8)

The site is not within an historic district but the building is a historic building constructed in 1927, designed by John Calvin Stevens. The applicant is in the process of obtaining state and federal historic rehabilitation tax credits. According to the Applicant, Schlotterbeck Block LLC has received the first part of the approval for the building from the National Park Service (NPS). This approval means that the NPS has determined that the property appears to meet the National Register Criteria for Evaluation and will likely be listed in the National Register of Historic Places if nominated by the State Historic Preservation Officer. The second step is approval of the proposed renovations and the eligibility for state and federal historic rehabilitation tax credits. Ultimately, this building could be a designated historic local landmark.

It is in an urban neighborhood surrounded by a variety of residential building types and business. The proposed building does not impact the natural beauty of the area or adversely affect any significant wildlife habitat, rare or irreplaceable natural areas, or any public access to the shoreline. The staff finds the proposal in conformance with this standard.

6. *Comprehensive Plan (Section 14-497 (a) 9)*

The applicant has submitted their analysis of the projects conformance with the Comprehensive Plan (refer to Attachment K). The staff has identified the following goals and policies which are relevant to the proposed housing development and finds the proposal in conformance with the Comprehensive Plan:

Portland Housing Goal: Ensure that an adequate supply of housing is available to meet the needs, preferences, and financial capabilities of all Portland households, now and in the future.

Policies

- Ensure the construction of a diverse mix of housing types that offers a continuum of options across all income levels, which are both renter and owner-occupied, including but not limited to the following:
 - ii. Housing units for decreasing household size, such as young professionals, empty nesters, single-parent households, and senior citizens.
- Encourage higher density housing for both rental and home ownership opportunities, particularly located near services, such as schools, businesses, institutions, employers, and public transportation.
- Increase Portland's rental housing stock to maintain a reasonable balance between supply and demand yielding consumer choice, affordable rents, and reasonable return to landlords.
- Identify vacant land and redevelopment opportunities throughout the City to facilitate the construction of new housing.

Portland Housing Goal: Maintain and enhance the livability of Portland's neighborhoods as the City grows and evolves through careful land use regulations, design and public participation that respect neighborhood integrity.

Policies

- Encourage innovative new housing development, which is designed to be compatible with the scale, character, and traditional development patterns of each individual residential neighborhood.
- Encourage new housing development in proximity to neighborhood assets such as open space, schools, community services and public transportation.
- Ensure the integrity and economic value of Portland's neighborhoods.

7. *Financial Capability (Section 14-497 (a) 10)*

The estimated cost of the development is \$5,000,000. The applicant has submitted a letter from NBT Bank, dated September 22, 2015, as demonstration of their financial and technical capacity to complete the proposed development.

C. SITE PLAN STANDARDS (Section 14-526)

The proposed development has been reviewed by staff for conformance with the relevant review standards of Portland's site plan ordinance and applicable regulations. Staff comments are listed below.

1. Transportation Standards

a. Impact on Surrounding Street Systems and Access and Circulation- see Subdivision, Paragraph VIII (B) (3), above.

b. Construction Management Plan

A construction management plan has been submitted by the applicant for review. Department of Public Works has reviewed it and finds it acceptable.

c. Sidewalks and Ramps

Currently there is no sidewalk on the Kennebec Street side of the site. There are sidewalks on Elm and Preble Streets but the sidewalks are not in good condition and range in materials (from concrete to brick). The applicant is proposing to install new brick sidewalk along the frontages on all three streets. This site is in the brick district. Portions of the public sidewalk cross onto private property. The City will need to work with the applicant to obtain public access easement where the sidewalks cross onto private property, which will need to be accepted by the City Council.

The handicapped ramps need to be fully ADA complaint and upgrades of the ramps at the Kennebec Street intersections with Elm Street and Preble Street are needed. Mr. Errico has reviewed proposal and states:

I have coordinated with Bruce Hyman on the ramp design and we generally find the layout of the ramps to be acceptable. I would note that the details for design of the ramps needs further review and possible adjustment and the applicant shall coordinate with DPW and Planning on revising the engineering design of the ramps. The ramp at the Preble Street/Alder Street corner is a unique geometric condition and additional review of the ramp location is required (a separate ramp on Preble Street may be required).

d. Public Transit Access

The site is situated on the METRO Route 8, the Peninsula Loop, as well as being a short distance from Oxford Street where Routes 2, 4, and 5 operate.

e. Parking

Off-Street/ Onsite Parking

The project will have forty-five (45) parking spaces of which seventeen will be compact spaces. The parking is laid out surrounding the building along Kennebec and Elm Street. The main entrance to the site is from Elm Street. The B-7 zone includes a particular requirement (14-332.1.i) that all projects regardless of size shall abide to Section 14-332.2.c for the submission of a parking analysis and for the Planning Board to establish the parking requirement for each project.

Sec. 14-332.1. Zone specific off-street parking exceptions and modifications. The off-street parking requirements established for uses, established above in section 14-332 of this

division, are hereby modified for the following zones according to the provisions described below.

(i) B-7, Mixed Development District Zone: Off-street parking for all projects regardless of size, shall be governed by 14-332.2(c) of this article.

Sec. 14-332.2. Categorical exceptions and modifications to offstreet parking requirements. Regardless of which zone a project of use is located, the following exceptions to the off-street parking requirements listed above in section 14-332 are additionally hereby established.

(c) Site plans over 50,000 square feet and projects in the B-6, B-7, and USM Overlay Zones: Where construction is proposed of new structures having a total floor area in excess of fifty thousand (50,000) square feet, the planning board shall establish the parking requirement for such structures. The parking requirement shall be determined based upon a parking analysis submitted by the applicant and upon the recommendation of the city transportation engineer.

Section 14-332.1 (k) – For residential development on the peninsula the parking requirement is one space per dwelling unit. This same section also states that:

The planning board may establish a parking requirement that is less than the normally required number of spaces upon a finding of unique conditions that result in a lesser parking demand, such as housing for persons who cannot drive, housing that participates in a travel demand management program, availability of transit, or housing which includes permanent restrictions on automobile usage, and which is permanently restricted from utilizing resident on-street parking stickers.

The applicant has submitted a parking demand study and states the project will require approximately 45 parking spaces, 40 for the residential units and 5 for the offices, or a ratio of 0.73. The applicant is requesting that the Board make a finding that the reduced number of spaces for the site is acceptable. With the proposed reduction of the parking requirement, the site plan has been revised to have parking stalls that meet dimensional standards and the number of the compact spaces has been reduced. The site plan now also shows two landscaped islands in the parking areas.

Following is the breakdown of the parking that is required by zoning versus the reduced ratio that the applicant is requesting:

	1:1 Ratio	.73 :1 Ratio (As requested by Applicant)
Parking spaces as required per zoning	Office Space - 5 Residential Units - 55	Office Space - 5 Residential Units - 40
Total of Spaces Required	60	45

Mr. Errico has reviewed the parking demand analysis and concludes the following:

The applicant has provided a parking demand analysis that concludes that the project will be providing 5 parking spaces for the office use and 40 parking spaces for 55 apartment units or a 0.73 parking ratio per unit. Given project characteristics, site location, and use of the office parking spaces during non-business hours (shared parking), I find the parking supply to be acceptable.

On-Street Parking

As part of the development, the on-street parking schedule will change. On-street parking regulations will need to be reviewed and changes will require City Council approval. The applicant will need to provide supporting information and/or plans for inclusion in the City Council packet for the changes to the on-street parking regulations.

Compact Parking Spaces

Originally the application was calling for thirty-eight (38) compact spaces on the site, or at 76% of the total number of spaces. Mr. Errico expressed concerns about this high ratio of compact spaces. With the revised parking demand analysis, the applicant proposes seventeen (17) of the parking spaces to be compact spaces (8' x 15'), which is 38% of the total. The site plan has been revised so the row of parking along Kennebec Street has been converted to standard parking spaces (9' x 18") with an aisle width of 20 feet (standard requirement is 24 feet). The Technical Manual Section 1.14 state "Any parking lot with 10 or fewer spaces shall contain standard sized parking spaces. Parking lots with greater than 10 spaces may be comprised of up to 20% compact parking spaces". The applicant states in their cover letter, "All the proposed residential rental units are either efficiency units or one-bedroom units. Renters of such units generally utilize smaller cars. We believe the proposed high percentage of compact parking spaces will be compatible with the user's of the building".

The applicant has requested waivers for aisle width and compact spaces. Mr. Errico has reviewed the revised site plan and he supports the waivers, Attachment 1.

f. Bicycle Parking

The proposals include 24 bicycle parking spaces on the site, which meets the ordinance standard of 2 bicycle spaces per 5 vehicle spaces. The TDM coordinator will monitor the bicycle rack use to identify if additional bike racks are warranted.

2. *Environmental Quality Standards*

a. Preservation of Significant Natural Features

There are no known significant natural features on the site.

b. Landscaping and Landscape Preservation

There are existing street trees that the applicant will be saving along Preble and Kennebec Streets (six on Preble Street and six on Kennebec Street). As part of the landscaping plan, the applicant is proposing to add more street trees on the site. Landscaping is being proposed along the edges of the parking lots. The zoning standards and B-7 zone design standards specifically states that surface parking lots will need to be screened from public streets, which the project is meeting.

Street Trees Requirement

As part of the project, there will be 55 units being proposed. Based on this number, the applicant is required to have 55 street trees. Since there are five existing streets on site that will be kept as part of the landscaping plan, the applicant can get a credit for the amount. The applicant will be installing fifteen (15) street trees along all three streets. Therefore the applicant is seeking the waiver and will contribute to the City's Tree Fund for the thirty-five (35) trees.

c. Water Quality, Storm Water Management and Erosion Control

The site is about 75% impervious. Currently the existing gravel parking lot sheet flows to both Kennebec Street and Elm Street. The stormwater is captured using existing catch basins and channeled into the existing 15-inch combined sewer on Elm Street. The stormwater from the existing roof is channeled to the Elm Street combined sewer as well. The proposed site improvements will remove 2,770 sq. ft. of impervious area.

The eastern parking lot will utilize catch basins and storm pipes to collect stormwater, whereas the western parking lot is intended to sheet flow toward a filter strip that has an underdrain connected to a proposed catchbasin. This catchbasin will tie into an existing catch basin on Kennebec Street. The roof runoff from the existing building will collect internally and be directed toward the east parking lot system.

Under Section 5 of the City of Portland Technical Manual, a Level III development project is required to submit a stormwater management plan pursuant to the regulations of MaineDEP Chapter 500 Stormwater Management Rules, including conformance with the Basic, General, and Flooding Standards. David Senus, P.E. with Woodard and Curran Engineering, has reviewed the drainage plan and stormwater report. Since there will be reduction in impervious area on site, the project is not required to include any specific stormwater management features for stormwater quality control. However, per the recently passed amendment to *Chapter 14. Land Use, Article III. Zoning, Sections 14-182, 14-295 and 14-296 Re: Commercial Kitchens and Small Gravel Lots*, "Parking spaces within the 35 foot setback shall provide stormwater quality treatment if required by the City of Portland Stormwater Management Standards and the Maine DEP Chapter 500 Stormwater Management Standards. If not required, an alternative low impact development treatment system approved by the Planning Board shall be provided", the applicant is proposing "Stone Infiltration Trench" and the City accepts this as a low impact development treatment system. Mr. Senus recommends that "a barrier or visual indicator (i.e. – curb stop or post) at the edge of pavement or reinforcing layer on the filter should be included to avoid vehicle tires from sinking into the swale/filter if cars are driven beyond the edge of pavement".

3. Public Infrastructure and Community Safety Standards***a. Consistency with Master Plans***

See Subdivision, Paragraph VIII (B)6 above.

b. Public Safety and Fire Prevention

The Crime Prevention through Environmental Design (CPTED) standards in the site plan ordinance address the principles of natural surveillance, access control and territorial

reinforcement so that the design of developments enhance the security of public and private spaces and reduce the potential for crime.

The applicant has introduced bollard lights along the walkways of each unit. Overhead lighting is being provided for the area by the entrance and parking spaces. The applicant is also proposing street lighting on all three streets. The revised detail to the bollard shows a cover that limits the light to a 90° angle. The staff is recommending approval of the fixtures as proposed.

c. Availability and Adequate Capacity of Public Utilities

The project will be served by the existing utility services located in Preble Street. The Applicant has received capacity letters from both the Portland Water District and the City Department of Public Services.

4. *Site Design Standards*

a. Snow and Ice Loading

The applicant is proposing on-site snow storage. There are three areas for storage on either side of the parking areas, see Site Plan (Attachment Plan 6).

b. View Corridors

This site is not within a Protected View Corridor as per the “View Corridor Protection Plan” approved by the Portland City Council in 2001.

c. Historic Resources

The site is not in any historic district; therefore the project does not have to be reviewed by Historic Preservation. Please refer to Paragraph VIII (B) (5), above.

d. Exterior Lighting

A lighting and photometric plan has been submitted. The proposal has noted bollard lighting at the walkways to the building (Plan 14 and 15). Refer to Paragraph VIII (C)3(b), above.

e. Noise and Vibration

All heating, ventilation and air conditioning equipment (HVAC), air handling units (AHU), emergency generators, and similar equipment will have to be shown on the plans and meet state and federal emissions requirements. These mechanical equipment should be located to the interior of the site, away from abutting properties and be screened from view from any public street and from adjacent sites by structure walls, evergreen landscaping, fencing, masonry wall or a combination thereof. The B-7 Design Standards have very specific requirements in regard to the mechanical equipment. This information will have to be submitted for review.

f. Signage and Wayfinding

This standard does not apply to the proposal.

g. Zoning Related Design Standards

A design review according to the *City of Portland Design Manual* Standards was performed for the revised proposal for redevelopment at 117 Preble Street. The plans were reviewed by Caitlin

Cameron, Urban Designer, Shukria Wiar, Planner, and Deb Andrews, Historic Preservation Program Manager of the Department of Planning & Urban Development. The project was reviewed against the B-7 *Mixed Use Urban District Zone Design Principles & Standards* (Appendix 4 of the Design Manual). Many of the standards do not apply because the project is adaptive reuse of an existing building. In seeking historic tax credits, the project must also meet the Secretary of Interior Standards for Rehabilitation which will be the guiding standards for alterations to the existing building. Overall, the project is meeting design standards but some of the outstanding issues are:

- If the bike racks are not visible from the street the standard regarding directional signage must be met. Bike racks are not located within proximity of the office entry.
- Landscape screening should be of sufficient type and height to effectively screen surface parking year round. Trees are kept on Preble Street to screen parking and new trees are proposed on Kennebec.
- The trash storage location as shown does not meet the standard because of its proximity to the street – the site has three street frontages and so is challenged to find a location that can meet the letter of the standard. Some landscape screening and a fence are provided to mitigate this element.

Ms. Cameron's full comments are included as Attachment 4.

IX. STAFF RECOMMENDATION

Subject to the proposed motions and conditions of approval listed below, Planning Division staff recommends that the Planning Board approve the proposed Schlotterbeck Building redevelopment project at 117 Preble Street.

X. PROPOSED MOTIONS

A. WAIVERS

On the basis of the application, plans, reports and other information submitted by the applicant; findings and recommendations contained in the Planning Board report for the public hearing on December 15, 2015 for application 2015-174 relevant to Portland's technical and design standards and other regulations; and the testimony presented at the planning board hearing:

The Planning Board **finds/does not find**, based upon the consulting transportation engineer's review (*Attachment 1*), that extraordinary conditions exist or undue hardship may result from strict compliance with the *Technical Manual* standard (*Section 1.14*) which requires that aisle width for right-angle parking be 24 feet per *Figure I-27* that the variation is consistent with the intent of the ordinance. The planning board **waives/does not waive** the *Technical Manual* standard (*Section 1.14*) to allow a 20 foot wide aisle in the parking area;

The Planning Board **finds/does not find**, based upon the consulting transportation engineer's review (*Attachment 1*), that extraordinary conditions exist or undue hardship may result from strict compliance with the *Technical Manual* standard (*Section 1.14 and Figures I-27 to I-29*) limiting compact spaces to 20% of total parking supply that the variation is consistent with the intent of the ordinance. The Planning Board **waives/does not waive** the *Technical Manual* standard (*Section 1.14 and Figures I-27 to I-29*) to allow 38% compact spaces; and

The Planning Board **finds/does not find** that the applicant has demonstrated that site constraints prevent the planting of all required street trees in the right-of-way. The Planning Board **waives/does not waive** the site plan standard (*Section 14-526 (b) (iii)*) requiring one street tree per unit for multi-family development and concludes that the applicant shall contribute \$7,000 for 35 street trees to Portland's tree fund.

B. SUBDIVISION

On the basis of the application, plans, reports and other information submitted by the applicant; findings and recommendations contained in the planning board report for the public hearing on December 15, 2015 for application 2015-174 relevant to the subdivision regulations; and the testimony presented at the Planning Board hearing, the planning board finds that the plan **is/is not** in conformance with the subdivision standards of the land use code, subject to the following conditions of approval, which must be met prior to the signing of the plat:

1. The applicant shall finalize the subdivision plat for review and approval by Corporation Counsel, the Department of Public Services, and the Planning Authority; and
2. The applicant shall provide a pedestrian access easement for the portions of sidewalk that do not lie within the right-of-way for adoption by the City Council.

C. DEVELOPMENT REVIEW

On the basis of the application, plans, reports and other information submitted by the applicant; findings and recommendations contained in the Planning Board Report for the public hearing on December 10, 2015 for application 2015-174 relevant to the site plan regulations; and the testimony presented at the Planning Board hearing, the Planning Board finds that the plan **is/is not** in conformance with the site plan standards of the land use code, subject to the following conditions of approval that must be met prior to the issuance of a building permit, unless otherwise stated:

1. The applicant shall address the Urban Designer memorandum, dated 12.10.2015, specifically the following recommendations:
 - a. If the bike racks are not visible from the street the standard regarding directional signage must be met. Bike racks are not located within proximity of the office entry.
 - b. Landscape screening should be of sufficient type and height to effectively screen surface parking year round. Trees are kept on Preble Street to screen parking and new trees are proposed on Kennebec.
 - c. The trash storage location as shown does not meet the standard because of its proximity to the street – the site has three street frontages and so is challenged to find a location that can meet the letter of the standard. Some landscape screening and a fence are provided to mitigate this element.
2. A barrier or visual indicator (i.e. – curb stop or post) at the edge of pavement or reinforcing layer on the filter shall be included to avoid vehicle tires from sinking into the swale/filter if cars are driven beyond the edge of pavement.
3. The details for the design engineering of the ramps shall be further reviewed by City staff and any possible adjustment shall be coordinated with Department of Public Works and the

Planning Division. The applicant shall also provide a detail for Sidewalk Ramp Detectable Warning Panels in accordance with Figure I-7 of the City of Portland Technical Manual.

4. The applicant shall provide supporting information and/or plans for inclusion in the City Council packet for the changes to the on-street parking regulations, prior to the issuance of a certificate of occupancy.
5. The developer/contractor/subcontractor must comply with conditions of the construction stormwater management plan and sediment and erosion control plan based on City standards and state guidelines. The owner/operator of the approved stormwater management system and all assigns shall comply with the conditions of Chapter 32 Stormwater including Article III, Post Construction Stormwater Management, which specifies the annual inspections and reporting requirements. A maintenance agreement for the stormwater drainage system, as attached, or in substantially the same form with any changes to be approved by Corporation Counsel, shall be submitted, signed, and recorded prior to the issuance of a building permit with a copy to the Department of Public Services.
6. That all HVAC systems and external mechanical equipment shall meet the maximum allowable noise requirements of the zone; each unit shall submit documentation of dBA output to confirm compliance of both the unit and the building in respect of rated noise levels and cumulative noise levels, to the satisfaction of the Zoning Administrator prior to the issuance of a Building Permit for that unit.

ATTACHMENTS:

Staff Review Comments and Background Information

1. Traffic Engineering Review (Tom Errico, T Y Lin), dated 12.11.2015
2. Department of Public Works Review, David Margolis-Pineo, dated 12.09.2015
3. Civic Engineer Review, David Senus, dated 12.10.2015
4. Urban Designer Review, Caitlin Cameron, dated 12.10.2015
5. City Arborist Review, Jeff Tarling, dated 11.04.2015

Applicant's Submittal

- A. Cover Letter dated 10.05.2015
- B. Application
- C. Right, Title or Interest
- D. Project Description
- E. Tax Map
- F. Existing Soils Condition
- G. Public Utilities
- H. Technical Capability, Financial Capability and Letter of Authorization
- I. Compliance with Applicable Zoning
- J. Waiver Request
- K. Consistency with City's Master Plan and Conformity with Design Standards
- L. Fire Department Checklist and HVAC Emissions Requirements
- M. Traffic and Parking Study

- N. Stormwater Management Plan
- O. Solid Waste Disposal and Snow Removal
- P. Light Fixtures
- Q. Construction Management Plan
- R. Easements
- S. Cover Letter dated 10.26.2015
- T. PWD Capacity Letter dated 10.15.2015
- U. DPS Capacity Letter dated 10.23.2015
- V. Response Letter dated 11.16.2015

Submittal after Workshop

- W. Applicant Cover Letter 2015-10-12
- X. Cover Letter Response to Comments dated 12.01.2015
- Y. Meeting Certification 11-20-2015
- Z. B7 Review 2015-20-11 Ltrhd
- AA. Transportation Demand Management- Revised
- BB. Parking Memo 12-10-15

Plans

- Plan1 Cover Sheet
- Plan2 1 SBS survey 10-5-2015
- Plan3 1 of 1 Sub.
- Plan4 L1.0 Ex.Cond.
- Plan5 L1.1 Con.Man.
- Plan6 L2.0 Layout.re.12-10-2015
- Plan7 L2.1 Ramps
- Plan8 L3.0 Grading
- Plan9 L4.0 ESC
- Plan10 L5.0 Planting
- Plan11 L6.0 Details
- Plan12 L6.1 Details
- Plan13 L6.2 Details
- Plan14 L6.3 Details
- Plan15 L7.0.Photometrics 10-23-15
- Plan16 L7.1 Photometrics 10-23-15 STREET LTG OFF
- Plan17 A1.1
- Plan18 A1.2
- Plan19 A1.3
- Plan20 A1.4
- Plan21 A2.1
- Plan22 A2.2
- Plan23 A2.3
- Plan24 A2.4
- Plan25 Renderings



Shukria Wiar <shukriaw@portlandmaine.gov>

Schlotterbeck 117 Preble Street - Amended Plan for One additional Residential Unit

1 message

Tom Errico <thomas.errico@tylin.com>

Thu, Apr 21, 2016 at 10:55 AM

To: Shukria Wiar <shukriaw@portlandmaine.gov>

Cc: David Margolis-Pineo <dmp@portlandmaine.gov>, Katherine Earley <KAS@portlandmaine.gov>, Jeremiah Bartlett <JBartlett@portlandmaine.gov>, Jeff Tarling <jst@portlandmaine.gov>

Hi Shukria – I have reviewed the application materials and I find the project to be acceptable from a traffic perspective with the following comments.

- The applicant has provided an updated parking analysis with the one additional residential unit. I find the conclusions to be reasonable given the likelihood that the tenant may not have a vehicle based on the small size of the unit.
- I find the changes to bicycle parking to be acceptable.
- The applicant has provided a detail for the sidewalk ramps at the corner of Preble Street and Kennebec Street. The general configuration is acceptable. Some adjustments are suggested for consideration including 7-foot tip-down curbs at the Preble Street ramps. The City generally prefers a minimum 4-foot separation between ramps. I need to coordinate with DPW on this detail. The plan seems to indicate a low curb reveal between ramps that doesn't meet City standards. I would suggest that a condition of approval be required that notes final ramp details to be provided for review and approval prior to construction.

If you have any questions, please contact me.

Best regards,

Thomas A. Errico, PE
Senior Associate
Traffic Engineering Director
TYLIN INTERNATIONAL T.Y. Lin International

12 Northbrook Drive

Falmouth, ME 04105

207.781.4721 (main)

207.347.4354 (direct)

207.400.0719 (mobile)

207.781.4753 (fax)

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"One Vision, One Company"

Please consider the environment before printing.



Shukria Wiar <shukriaw@portlandmaine.gov>

Schlotterbeck Building Amendment (2016-078)

Michael Farmer <mfarmer@portlandmaine.gov>
To: Shukria Wiar <shukriaw@portlandmaine.gov>
Cc: "Zazzara, Rhonda" <rjz@portlandmaine.gov>

Thu, Apr 14, 2016 at 2:33 PM

Shukria:

The proposed new 6" sanitary sewer connection to the sewer main in Elm Street is OK. However, the connection should be made with an *INSERTA TEE*, (not a saddle Y attached with stainless steel bands), provided that the 15-inch sewer main is structurally sound. If the City inspector finds that the existing sewer main is not structurally sound, he or she may request that a new, replacement section of the existing main be installed with a factory manufactured T or Y fitting.

[Quoted text hidden]

—
Michael Farmer, Project Engineer
Dept. of Public Services
55 Portland Street
Portland, ME 04101
phone: [207-874-8845](tel:207-874-8845)

April 5, 2016

Ms. Shukria Wiar, Planner
and Planning Board Members
Planning and Urban Development
City of Portland
389 Congress Street
Portland, ME 04101

**RE: Amended Subdivision Plan & Level III Site Plan Application
Schlotterbeck Block – 117 Preble Street**

Dear Shukria and Planning Board Members:

On behalf of Schlotterbeck Block LLC, we are pleased to submit the following Amended Site Plan and Subdivision Application for the proposed redevelopment of the Schlotterbeck & Foss building at 117 Preble Street. Schlotterbeck Block LLC is requesting to construct one (1) additional studio apartment unit in the basement of the existing building. Also, since Planning Board approval on December 15, 2015, site excavation and exploration activities have taken place

. Several utility connections need to be modified in response to the sub-surface findings. These modifications are further defined below.

Subdivision and Site Plan Modifications

1. **Additional Unit:** At its December 15, 2015 meeting, the Planning Board approved Schlotterbeck Block LLC's Subdivision Application, which included the development of 55 units at the former Schlotterbeck & Foss building at 117 Preble Street. The applicant now proposes an additional unit in the building's basement, resulting in a total of 56 units and 1,750 s.f. of office space.

The attached Amended Subdivision Plan and Basement Floor Plan A1.1 have been revised accordingly.

2. Vehicular Parking: Attached Exhibit A is a revised Parking Demands memo, prepared by Gorrill Palmer, dated March 24, 2016. The memo states the 45 vehicular parking spaces and 25 bicycle parking spaces are adequate for the proposed 56-unit project. The December 2015 vehicular parking layout has the capacity to serve the additional unit and is not revised or modified as part of this resubmission.
3. Additional Bike Racks: The additional unit eliminates the bike storage room in the basement. As a result, fourteen (14) exterior new bike parking space need to be located on the site. The attached revised L2.0 show an additional bike hitch under the canopy and six (6) additional bike racks on a concrete pad located near the building entrance. With these revisions, thirteen (13) bike loops are provided and 26 bike parking spaces will be provided.
4. Additional Sidewalk Ramp and Pedestrian Easement: The City's staff has requested an additional sidewalk ramp at the intersection of Preble and Kennebec Streets, as part of the December 2015 conditions of approval. The new ramp will require pedestrian easement on the applicant's lot. The attached Amended Subdivision Plan and sheet L2.0 show the new sidewalk ramp and the pedestrian easement. Also, attached Exhibit B also shows the detailed layout and grading the new sidewalk ramp.
5. 6-inch Fire Water Line: Attached revised L3.0 adds a new six-inch fire suppression water line from Preble Street to the building's fire suppression system. The applicant had planned to continue using the existing 6-inch fire suppression line, but existing 6-inch line is in poor condition.
6. Roof Drain Connection: Attached revised L3.0 shows a new 6" HDPE roof drain connecting to catch basin #5. The existing roof drain pipe is in poor condition and needs to be replaced.
7. Sanitary Sewer Line: Excavation exploration revealed the existing 6-inch cast iron sanitary sewer line is in poor condition. The applicant proposes to replace the existing sewer line with a new PVC sewer in approximately the same location and wye connect the 15-inch combined sewer on Elm Street.

Ms. Shukria Wiar and Planning Board Members

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Submission

This submission includes the following information:

1. Cover letter, dated April 5, 2016;
2. Amended Site Plan and Subdivision Application & Checklist;
3. Exhibit A -- Parking Demands Memo, dated March 24, 2016;
4. Exhibit B -- Preble Street Sidewalk Ramp;
5. Application Fee: \$525.00;
6. One set of site plans (24" x 36");
7. One set of site plans (11" x 17"); and
8. One set of digital files.

We trust that the Planning Board will consider this a complete application for the board's meeting scheduled for April 26, 2016. If you desire any additional information, please do not hesitate to contact us. We look forward to our meeting with the Board at its earliest convenience.

Sincerely,
Mitchell & Associates



Michael W. King
Maine Licensed Landscape Architect

cc: John Anton, Schlotterbeck Block LLC
Rick Goduti, Goduti-Thomas Architects

PROJECT NAME: The Schlotterbeck Block

PROPOSED DEVELOPMENT ADDRESS:

117 Preble Street

PROJECT DESCRIPTION:

Applicant received Planning Board Subdivision and Level III Site Plan approval in December 2015 for 55 apartment units and 1,750 s.f. office space. Applicant seeks to add a residential unit (revised total of 56 units) and proposes site plan revisions based on requisite conditions of approval and upgrades to site utilities.

CHART/BLOCK/LOT: 033 E001001
033 E005001

PRELIMINARY PLAN _____ (date)
FINAL PLAN _____ (date)

CONTACT INFORMATION:

<p>Applicant – must be owner, lessee or Buyer</p> <p>Name: John Anton Business Name, if applicable: Schlotterbeck Block, LLC Address: 77 Spruce St City/State : Zip Code: Portland, ME 04102</p>	<p>Applicant Contact Information</p> <p>Work # Home# Cell # Fax# 207 650-8979 e-mail: antonatlarge@gmail.com</p>
<p>Owner – (if different from Applicant)</p> <p>Name: same Address: City/State : Zip Code:</p>	<p>Owner Contact Information</p> <p>Work # same Home# Cell # Fax# e-mail:</p>
<p>Agent/ Representative</p> <p>Name: Michael King Mitchell & Associates Address: 70 Center Street City/State : Zip Code: Portland, ME 04101</p>	<p>Agent/Representative Contact information</p> <p>Work # (207) 774-4427 Cell # e-mail: mking@mitchellassociates.biz</p>
<p>Billing Information</p> <p>Name: John Anton Schlotterbeck Block, LLC Address: 77 Spruce St Portland, ME 04102 City/State : Zip Code:</p>	<p>Billing Information</p> <p>Work # Cell # Fax# 207 650-8979 e-mail: antonatlarge@gmail.com</p>

Engineer Name: Ransom Consulting Steve Bradstreet, P.E. Address: 400 Commercial Street City/State: Portland, ME Zip Code: 04101	Engineer Contact Information Work # (207) 772-2891 Cell # Fax# e-mail: steven.bradstreet@ransomenv.com
Surveyor Name: Owen Haskell, Inc. Address: 390 US Route 1, Unit 10 City/State: Falmouth, ME Zip Code: 04105	Surveyor Contact Information Work # (207) 774-0424 Cell # Fax# e-mail:
Architect Rick Goduti Name: Goduti Thomas Architects Address: 44 Oak Street City/State: Portland, ME Zip Code: 04101	Architect Contact Information Work # (207) 775-3184 Cell # Fax# e-mail: gtarick@gmail.com
Attorney Cito Selinger Name: Curtis Thaxter Address: One Canal Plaza, #1000 City/State: Portland, ME Zip Code: 04101	Attorney Contact Information Work # (207) 774-9000 Cell # Fax# e-mail: mselinger@curtisthaxter.com

APPLICATION FEES:

Check all reviews that apply. (Payment may be made by Credit Card, Cash or Check payable to the City of Portland.)

Level III Development (check applicable reviews) ___ Less than 50,000 sq. ft. (\$500.00) ___ 50,000 - 100,000 sq. ft. (\$1,000) ___ 100,000 - 200,000 sq. ft. (\$2,000) ___ 200,000 - 300,000 sq. ft. (\$3,000) ___ over \$300,00 sq. ft. (\$5,000) ___ Parking lots over 11 spaces (\$1,000) ___ After-the-fact Review (\$1,000.00 plus applicable application fee) Plan Amendments (check applicable reviews) ___ Planning Staff Review (\$250) X Planning Board Review (\$500) The City invoices separately for the following: <ul style="list-style-type: none"> • Notices (\$.75 each) • Legal Ad (% of total Ad) • Planning Review (\$40.00 hour) • Legal Review (\$75.00 hour) Third party review fees are assessed separately. Any outside reviews or analysis requested from the Applicant as part of the development review, are the responsibility of the Applicant and are separate from any application or invoice fees.	Other Reviews (check applicable reviews) ___ Traffic Movement (\$1,000) ___ Stormwater Quality (\$250) X Subdivisions (\$500 + \$25/lot) # of Lots <u>1</u> x \$25/lot = <u>\$25</u> ___ Site Location (\$3,000, except for residential projects which shall be \$200/lot) # of Lots ___ x \$200/lot = ___ ___ Other _____ ___ Change of Use ___ Flood Plain ___ Shoreland ___ Design Review ___ Housing Replacement ___ Historic Preservation
----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

APPLICATION SUBMISSION:

1. All site plans and written application materials must be submitted electronically on a CD or thumb drive with each plan and each document submitted as separate files. Naming conventions for the individual files can be found on the **Electronic Plan and Document Submittal** page of the City's website at <http://me-portland.civicplus.com/764/Electronic-Plan-and-Documents-Submittal>
2. In addition, one (1) paper set of the plans (full size), one (1) paper set of plans (11 x 17), paper copy of written materials, and the application fee must be submitted to the Building Inspections Office to start the review process.

The application must be complete, including but not limited to the contact information, project data, application checklists, wastewater capacity, plan for fire department review, and applicant signature. The submissions shall include one (1) paper packet with folded plans containing the following materials:

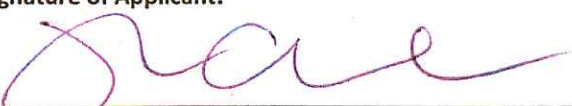
1. One (1) full size site plans that must be folded.
2. One (1) copy of all written materials or as follows, unless otherwise noted:
 - a. Application form that is completed and signed.
 - b. Cover letter stating the nature of the project.
 - c. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
3. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 50 feet.
4. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
5. One (1) set of plans reduced to 11 x 17.

Please refer to the application checklist (attached) for a detailed list of submission requirements.

APPLICANT SIGNATURE:

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for a Level III Site Plan review. It is not a permit to begin construction. An approved site plan, a Performance Guarantee, Inspection Fee, Building Permit, and associated fees will be required prior to construction. Other Federal, State or local permits may be required prior to construction, which are the responsibility of the applicant to obtain.

Signature of Applicant: 	Date: 4/4/16
----------------------------------------------------------------------------------------------------------------	-----------------

PROJECT DATA

The following information is required where applicable, in order to complete the application.

Total Area of Site	40,720 sq. ft.
Proposed Total Disturbed Area of the Site	26,500 sq. ft.
If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland.	
Impervious Surface Area	
Impervious Area (Total Existing)	30,070 sq. ft.
Impervious Area (Total Proposed)	27,300 sq. ft.
Building Ground Floor Area and Total Floor Area	
Building Footprint (Total Existing)	6,800 sq. ft.
Building Footprint (Total Proposed)	6,800 sq. ft.
Building Floor Area (Total Existing)	37,345 sq. ft.
Building Floor Area (Total Proposed)	37,345 sq. ft.
Zoning	
Existing	B-7
Proposed, if applicable	
Land Use	
Existing	Industrial
Proposed	Residential
Residential, if applicable	
# of Residential Units (Total Existing)	N/A
# of Residential Units (Total Proposed)	56
# of Lots (Total Proposed)	2
# of Affordable Housing Units (Total Proposed)	N/A
Proposed Bedroom Mix	
# of Efficiency Units (Total Proposed)	16
# of One-Bedroom Units (Total Proposed)	40
# of Two-Bedroom Units (Total Proposed)	0
# of Three-Bedroom Units (Total Proposed)	0
Parking Spaces	
# of Parking Spaces (Total Existing)	50 +/-
# of Parking Spaces (Total Proposed)	45
# of Handicapped Spaces (Total Proposed)	2
Bicycle Parking Spaces	
# of Bicycle Spaces (Total Existing)	0
# of Bicycle Spaces (Total Proposed)	25
Estimated Cost of Project	\$5,000,000

FINAL PLAN - Level III Site Plan			
Applicant Checklist	Planner Checklist	# of Copies	GENERAL WRITTEN SUBMISSIONS CHECKLIST (* If applicant chooses to submit a Preliminary Plan, then the * items were submitted for that phase and only updates are required)
X		1	* Completed Application form
X		1	* Application fees
X		1	* Written description of project
N/A		1	* Evidence of right, title and interest
N/A		1	* Evidence of state and/or federal permits
N/A		1	* Written assessment of proposed project's specific compliance with applicable Zoning requirements
N/A		1	* Summary of existing and/or proposed easements, covenants, public or private rights-of-way, or other burdens on the site
N/A		1	* Evidence of financial and technical capacity
N/A		1	Construction Management Plan
X		1	A traffic study and other applicable transportation plans in accordance with Section 1 of the technical Manual, where applicable.
N/A		1	Written summary of significant natural features located on the site (Section 14-526 (b) (a))
N/A		1	Stormwater management plan and stormwater calculations
N/A		1	Written summary of project's consistency with related city master plans
N/A		1	Evidence of utility capacity to serve
N/A		1	Written summary of solid waste generation and proposed management of solid waste
N/A		1	A code summary referencing NFPA 1 and all Fire Department technical standards
N/A		1	Where applicable, an assessment of the development's consistency with any applicable design standards contained in Section 14-526 and in City of Portland Design Manual
N/A		1	Manufacturer's verification that all proposed HVAC and manufacturing equipment meets applicable state and federal emissions requirements.

Applicant Checklist	Planner Checklist	# of Copies	SITE PLAN SUBMISSIONS CHECKLIST (* If applicant chooses to submit a Preliminary Plan, then the * items were submitted for that phase and only updates are required)
X		1	* Boundary Survey meeting the requirements of Section 13 of the City of Portland's Technical Manual
		1	Final Site Plans including the following:
X			Existing and proposed structures, as applicable, and distance from property line (including location of proposed piers, docks or wharves if in Shoreland Zone);
X			Existing and proposed structures on parcels abutting site;
X			All streets and intersections adjacent to the site and any proposed geometric modifications to those streets or intersections;
X			Location, dimensions and materials of all existing and proposed driveways, vehicle and pedestrian access ways, and bicycle access ways, with corresponding curb lines;
N/A			Engineered construction specifications and cross-sectional drawings for all proposed driveways, paved areas, sidewalks;
N/A			Location and dimensions of all proposed loading areas including turning templates for applicable design delivery vehicles;
N/A			Existing and proposed public transit infrastructure with applicable dimensions and engineering specifications;
X			Location of existing and proposed vehicle and bicycle parking spaces with applicable dimensional and engineering information;
N/A			Location of all snow storage areas and/or a snow removal plan;
N/A			A traffic control plan as detailed in Section 1 of the Technical Manual;
N/A			Proposed buffers and preservation measures for significant natural features, where applicable, as defined in Section 14-526(b)(1);
N/A			Location and proposed alteration to any watercourse;
N/A			A delineation of wetlands boundaries prepared by a qualified professional as detailed in Section 8 of the Technical Manual;
N/A			Proposed buffers and preservation measures for wetlands;
N/A			Existing soil conditions and location of test pits and test borings;
N/A			Existing vegetation to be preserved, proposed site landscaping, screening and proposed street trees, as applicable;
N/A			A stormwater management and drainage plan, in accordance with Section 5 of the Technical Manual;
N/A			Grading plan;
N/A			Ground water protection measures;
N/A			Existing and proposed sewer mains and connections;

- Continued on next page -

N/A		Location of all existing and proposed fire hydrants and a life safety plan in accordance with Section 3 of the Technical Manual;
N/A		Location, sizing, and directional flows of all existing and proposed utilities within the project site and on all abutting streets;
N/A		Location and dimensions of off-premises public or publicly accessible infrastructure immediately adjacent to the site;
N/A		Location and size of all on site solid waste receptacles, including on site storage containers for recyclable materials for any commercial or industrial property;
N/A		Plans showing the location, ground floor area, floor plans and grade elevations for all buildings;
N/A		A shadow analysis as described in Section 11 of the Technical Manual, if applicable;
N/A		A note on the plan identifying the Historic Preservation designation and a copy of the Application for Certificate of Appropriateness, if applicable, as specified in Section Article IX, the Historic Preservation Ordinance;
N/A		Location and dimensions of all existing and proposed HVAC and mechanical equipment and all proposed screening, where applicable;
N/A		An exterior lighting plan in accordance with Section 12 of the Technical Manual;
N/A		A signage plan showing the location, dimensions, height and setback of all existing and proposed signs;
X		Location, dimensions and ownership of easements, public or private rights of way, both existing and proposed.



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South Portland, Maine 04106 Att. C
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**Parking Demands
Schlotterbeck & Foss Building
Portland, Maine
March 24, 2016
JN 3027**

Introduction:

Gorrill Palmer (GP) has evaluated the potential parking demand for the proposed redevelopment of the Schlotterbeck & Foss Building located in Portland, Maine. The site is located in the B-7 Mixed Development District Zone. The proposed building will have 1,750 square feet of office space and 56 market rate residential units with unbundled parking.

The proposed facility is located on an existing lot with City streets along each of the borders. The facility itself is the redevelopment of an existing building and not new construction. As such, accommodating the needs for off-street parking and the other requirements of the site is a balancing act.

After discussions with City Staff, the on-site parking has been maximized while balancing the other needs of the project. Once the maximum on-site parking was determined, GP could then make a determination as to if that parking is sufficient, or if other avenues for parking accommodations needed to be pursued. The following is a summary of the parking evaluation.

Vehicular Parking Evaluation:

Based on maximizing the use of the site and balancing green space, drainage needs, snow storage, pedestrian accommodations, and on-site parking, the number of on-site spaces is a total of 45, with 17 of those compact spaces.

Based on the City ordinance, the proposed 1,750 square foot office space would require 1 space per 400 square feet, which results in a required 5 parking spaces. This leaves 40 parking spaces to accommodate the 56 apartments.

Using the 40 parking spaces for 56 apartments yields a parking ratio of 0.71. The general parking requirement for the City for a market rate apartment is 1 space per unit. Based on discussions with the applicant for the West End project (also Market Rate), which has been constructed and is occupied, their actual parking rate is approximately 0.79. Based on our previous experience, other apartment buildings in downtown range from approximately 0.50 to 1.0, with previous usage of 0.70. It should be noted that the proposed apartments are referred to as micro-apartments because they generally do not exceed 500 square feet in size. We anticipate this type of tenant is more likely to use other forms of transportation such as bicycle, walking, taxi, UShare, or METRO.

Based on this analysis, it is our opinion that the parking ratio of 0.71 is appropriate for the proposed apartments in a downtown location.



Bicycle Parking:

Per City Ordinance, non-residential structures are required to provide bicycle accommodations in proportion to the vehicular parking. For residential structures 2 bicycle parking spaces are required for every 5 dwelling units and for non-residential structures 2 bicycle parking spaces are required for every 10 vehicle parking spaces. Based on the ordinance this project would require 23 residential and 2 non-residential bicycle parking spaces for a total of 25 bicycle parking spaces.

Conclusions:

In our opinion, 45 vehicular parking spaces and 25 bicycle parking spaces are adequate to accommodate the proposed project. However, this methodology needs to be reviewed and approved by the City.

Prepared by:
Randy Dunton, PE, PTOE
Senior Engineer

