

29-L-1

#99700001

207-209 Fore Street

Hampton Inn Hotel (former Jordan's Site)

Old Port Hospitality

Att. AA.1



added to names
& memo, Rep'd
for PB Report #7-10

March 5, 2010

Mr. William Needelman, Senior Planner
Planning Division
389 Congress Street, 4th floor
Portland, ME 04101

Re: Response Letter for the March 9th Planning Board Workshop
Project: Hotel, Restaurant, & Residences – Old Port
Address: 207 & 209 Fore Street (formerly 38 India Street)
Applicant: Old Port Hospitality, LLC

Dear Bill,

Please find below Old Port Hospitality, LLC responses to the review comments received to date for the above mentioned project. The responses are brought to attention with bold italic text.

► Bill Needelman, Senior Planner, City of Portland:

1. Right Title Interest: From your deed, "Terms and provisions" of the MDEP Protection Orders, Book 19244, Page 271. We need copy.

The MDEP Protection Orders were submitted to Bill Needelman via email on March 3rd.

2. 33' driveway with a 10' curb radius.

The applicant intends to withdraw the waiver request for the driveway width. The driveway will be revised to provide a 24 foot width with 10' curb radii. Also, the line striping will be revised to delineate 20' turning radii.

3. Parking management plan.

A parking management plan has been submitted to planning and review comments are pending.

4. Building lighting.

The applicant intends to provide lighting designs for the building façade illumination.

5. Provide the Planning Department with materials sample board.

Material samples were provided to the Planning Department on March 4th.

► Woodward & Curran:

1. As currently shown, the lawn areas surrounding the parking lot are sloped towards the parking lot. We ask that the applicant consider sloping the parking lot toward the grassed area, resulting in some treatment of the stormwater runoff from the pavement.

We will revise the grading and utility plan to eliminate the sidewalk adjacent to the valet parking spaces. Vertical granite curbing with curb inlets to (2) tree filters will be installed.

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2. The stormwater report references the decision to use 3' sumps for stormwater quality treatment in the parking lots. The catch basin detail should be modified to say that 3' sumps are required for all catch basins, not just those in the City right-of-way as the detail currently states.

The catch basin details will be revised as requested.

3. The project will be separating and rerouting the combined sewer that currently crosses the site. The plans currently do not show how the existing combined sewer will be demolished or abandoned. The existing pipe is shown ending at the sidewalk on the utility plan. More detail should be provided as to how the existing combined sewer will be abandoned.

A Site and Utility Demolition plan will be added to the plan set. The plan notes will indicate that the contractor is required to secure Pipe Sealing permits from Public Services Department and shall plug, remove or abandon storm drains and sanitary sewers in accordance with City standards. All abandoned manholes will be removed. The 30" sewer will be removed within the site limits. We will meet with public services staff to confirm pipe sealing and abandonment requirements within the right of way. Staff has indicated that they may prefer that some pipes be abandoned in place to minimize excavation in Franklin Arterial.

► T.Y. Lin International:

1. The driveway is proposed to be 33 feet wide. Based upon anticipated traffic volumes I find this width to be excessive. Additionally, the radii seem large. One possibility would be to provide a 24-foot driveway in the public right-of-way and then widen to 33 feet on-site to accommodate valet parking needs. In any event the applicant should provide an auto-turn analysis that supports the design (I would note that we normally don't overdesign for infrequent large vehicle turning movements).

The applicant intends to withdraw the waiver request for the driveway width. The driveway will be revised to provide a 24 foot width with 10' curb radii. Also, the line striping will be revised to delineate 20' turning radii.

2. The proposed crosswalks at the Franklin Street/Fore Street and India Street/Fore Street intersections should be two parallel lines.

The crosswalk striping will be revised as requested.

3. The applicant should assess the design of the sidewalk ramp at the northwest corner of the India Street/Fore Street intersection such that separate ramps are provided for crossing India Street north of the intersection and Fore Street west of the intersection.

It was discussed at the Traffic permit Scoping Meeting on March 3rd that we will review and revise the ramp configuration to the extent possible. Due to the unusual geometry of this intersection, and the location of the existing ramps on the east side of India Street and south side of Fore Street creating completely separate ramps that cross perpendicular to the curb lines may not be feasible. In this case, a revised ramp, with separated detectable warning strips will be provided.

4. A waiver from the City's Technical Standard will be required for the size of the parking spaces (9'x18' spaces are proposed). I support this waiver.

The applicant still intends to seek the waiver request.

5. A waiver from the City's Technical Standard for provision of compact parking is required. I support this waiver.

The applicant still intends to seek the waiver request.

6. The applicant will be required to prepare a preliminary Maintenance of Traffic plan that addresses any anticipated closures of either public streets or sidewalks.

Traffic control plans will be submitted with next revised application submittal.

7. The provision of a crosswalk on Middle Street at Hampshire Street is to be reviewed by the City's Crosswalk Committee on March 4th. Comments will be provided soon after.

The applicant is awaiting review comments.

8. The applicant should provide a summary on how truck deliveries will take place, including types of vehicles, locations where loadings will occur, and whether the 12-foot clearance allows for large vehicle movements entering and exiting the site.

The applicant will include a narrative that describes the facility's intended delivery and loading operations with the revised application submittal.

9. I will provide an opinion of parking supply adequacy upon receipt and review of the parking analysis to be provided by the applicant.

A parking management plan has been submitted.

10. I would note that the City supports the plan for on-street parking on Fore Street. We currently are assessing appropriate parking regulations (e.g. time limit or meters) and will provide a recommendation in the future. I would ask the applicant provide an opinion of what is preferred for parking on Fore Street. Lastly, I would note that parking regulation changes will likely require City Council action and the applicant will be required to provide the necessary information in support of the Council review (with support from City staff).

It is the applicant's preference that the new on-street parking will be regulated by signage indicating a time limit that is accommodating to a restaurant use.

► David Margolis-Pineo, Deputy City Engineer, City of Portland:

1. None of the streets around the Jordan Meat site are under a moratorium.

No response necessary.

2. Any proposed sewer laterals to be abandoned from this site must follow City requirements for permitting and sealing. Any manholes associated located on laterals to be abandoned shall be removed and all associated pipes sealed.

A Site and Utility Demolition plan will be added to the plan set. The plan notes will indicate that the contractor is required to secure Pipe Sealing permits from Public Services Department and shall plug, remove or abandon storm drains and sanitary sewers in accordance with City standards. All abandoned manholes will be removed. The 30" sewer will be removed within the site limits. We will meet with public services staff to confirm pipe sealing and abandonment requirements within the right of way. Staff has indicated that they may prefer that some pipes be abandoned in place to minimize excavation in Franklin Arterial.

3. There is no mention made as to how the existing 30" sewer which passes through the lot to Franklin Street will be abandoned. This issue will need to be coordinated with John Emerson (874-8468) of this department.

A Site and Utility Demolition plan will be added to the plan set. The plan notes will indicate that the contractor is required to secure Pipe Sealing permits from Public Services Department and shall plug, remove or abandon storm drains and sanitary sewers in accordance with City standards. All abandoned manholes will be removed. The 30" sewer will be removed within the site limits. We will meet with public services staff to confirm pipe sealing and abandonment requirements within the right of way. Staff has indicated that they may prefer that some pipes be abandoned in place to minimize excavation in Franklin Arterial.

4. Since the City has no interest to enter the proposed 30' sanitary and stormwater easement area for any future system maintenance, and due to the proposed 12' over head canopy clearance, it is requested that the applicant use ductile iron pipe from the manholes in Middle St to the manholes in Fore St for both sewer lines. *We will revise the pipe material as requested by Public Services. Public Services suggested several alternative pipe materials that will be acceptable. We will confirm the pipe material with Public Services and revise the plans.*

5. All catch basins shall have three foot sumps, not just the catch basins within the city street right of ways. *The catch basin details will be revised as requested.*

6. All manholes with external drop connections shall meet TR-16 design and city detail standards. *We will revise the drop manhole detail as requested. Based on our discussion with Mr. Emerson, drops of more than 2' within a sewer manhole require a drop structure. There is a single existing 8" sewer connection at SMH-1 that will require a drop.*

7. Applicant may use Type "B" gravels in lieu of Type "A" gravels. *The materials will be revised as recommended.*

8. We will continue to review plan details for city standards. *No response necessary.*

9. The Following are comments relating to the site survey. Any questions should be directed to Bill Clark of this department, 874-8847

The incorrect vertical datum of NAVD88 was used, and the City of Portland Datum was not used. This is significant introducing and vertical difference of 0.7 feet, and can lead to serious problems.

The NAVD 88 datum for this site was established as part of an ALTA survey completed in 2005 and was maintained throughout the design and permitting of the previously approved projects on this site. As part of the original survey Sebago Technics performed a level loop to a U.S. Geodetic survey monument for which the City has an established elevation. Note 6 on the ALTA survey and existing conditions plans documents the datum elevations as follows:

THE BEARINGS, COORDINATES, AND ELEVATIONS SHOWN HEREON ARE BASED UPON THE MAINE STATE PLANE COORDINATE GRID, WEST ZONE 1802 ON NAD83 AND NAVD 88. THE CITY DATUM WAS VERIFIED UTILIZING U.S. COAST AND GEODETIC SURVEY BENCHMARK, BEING A DRILL HOLE LOCATED IN THE GRANITE STEPS OF THE CUSTOM HOUSE ON COMMERCIAL STREET. THE CITY ELEVATION IS 14.072', THE NGVD 29 ELEVATION IS 14.042', AND NAVD88 IS 13.30'

As we discussed with planning staff, as significant number of design and legal documents have been prepared based on the survey work completed to date. We proposed to address the concern regarding the datum by adding notes the each plan where elevation are referenced that will clearly indicate that design plans are based on the NAVD 88 datum will note the difference between NAVD88 and the City Datum elevations. In addition, any as-built plans for public infrastructure will note the NAVD88 and City datum elevations.

B. The GRID bearing of India Street between Middle St and Fore St is 00 degrees 11' 37" different from the bearing used by the City of Portland for Ocean Gateway, and as used by Owen Haskell Inc for the Ocean Gateway Subdivision and Ocean Gateway Garage projects. This amounts to a horizontal difference of approximately 0.62 feet in the 182.01 feet along the southwesterly line of India Street; this may not amount to much for a parking lot, but may be of consequence for a building in the future. Retracing the street lines on the Peninsula can be difficult, and it did take a good deal of effort to retrace the line of India Street from Commercial Street to Congress Street. I would be more than willing to provide a copy of our survey.

We have received a copy of the City survey completed in 2008. In it's determination of the easterly right of India Street Right of Way the City survey created an angle point on the easterly side of India Street that is not reflected in any of the street acceptance documents for India Street or the deed references for our project site. The City plan includes a note stating:

"DURING THE COURSE OF THIS SURVEY IS WAS DISCOVERED THAT AN ANGLE EXISTS IN THE LINE OF INDIA STREET. THE ACCEPTANCE OF INDIA STREET DOES NOT CALL FOR AN ANGLE POINT. HOWEVER, OVER THE YEARS BETWEEN APPROXIMATELY 1850 TO 2005, AN ANGLE POINT HAS DEVELOPED. THIS MAY BE DUE TO THE PAST PRACTICE OF SURVEYING "BLOCK TO BLOCK" AND HOLDING CITY OF PORTLAND GRANITE SURVEY MONUMENTS THAT WERE SET TO DEFINE THE STREET LINES"

It is the introduction of this angle point that is causing the difference in bearings between the two plans. The City's plan has not yet been recorded and was therefore not identified in our recent survey research. It appears that the introduced angle point and bearing will move the India Street Right of Way northeasterly, away from the project site, potentially increasing the size of the parcel by approximately 76 square feet.

We are reviewing the implications of this potential change on previously recorded documents and the subdivision plans. We will discuss this further with Mr. Clark and the city to determine if the locus property boundary along India Street property line should be adjusted.

In the interim, we added the City Survey as an unrecorded plan reference on our survey and subdivision plan. If appropriate we will note the differences in bearings between the two surveys.

C. The monument at the easterly corner of India Street and Middle Street is incorrectly shown. That monument is what we call an "M" monument where the back corner of the monument is the street line corner, and not the middle of the monument.

We are aware that this is an "M" monument. The back (Micucci's) corner of the monument was located as part of our survey. The discrepancy noted is simply due to the graphic symbol used on the plan and the scale of the drawing. We will add the note to our plan documenting the monument location as follows: "10"X10" GRANITE "M" MONUMENT, EASTERLY CORNER OF MONUMENT LOCATED"

D. The monument at the northerly corner of India Street and Middle Street should not be used, as it was disturbed by City crews in the 1990s.

The monument discussed was determined in our survey to be off the right of way corner by approximately 1.25 feet. Although several pieces of monumentation were located during the survey, the monument was not held. Monuments that were held to establish the locus property are noted as "held" on the plan.

E. Proposed 3 foot offset Monuments will be requested on the subdivision plan at the following locations.

- Corner of India St and Middle St
- Corner of Middle St and Franklin St Art
- Corner of Fore St and Franklin St Art
- Fore St PC 123.41' southwesterly of 3 foot offset monument near India St.

We have discussed the placement of monuments with Mr. Clark and have proposed the following:

- *A monument is not proposed at the corner of India Street and Middle Street due to a conflict with an existing underground electrical duct bank.*
- *A monument is not proposed at the corner of Middle Street and Franklin Arterial, This location is not part of the locus site's boundary and there is a conflict with an existing underground electrical manhole and duct bank.*
- *A monument is not proposed at the corner of Fore Street and Franklin Street Arterial due to a conflict with an existing underground electrical manhole and ductbank.*
- *The subdivision plan will be revised to include a 3-ft offset monument on Fore Street at the PC 123.41 southwesterly of the monument near India Street as requested.*

Rebar corner pins will be installed where the proposed 30' utility easement meets Middle Street. The intersections of the easement at Fore Street fall within the proposed brick sidewalk/driveway apron and immediately adjacent to a granite curb. As such, pins are not proposed at Fore Street.

10. Stormwater treatment is technically not required for projects such as this. However, the applicant is encouraged to consider treatment options such as tree or shrub box filters which can be relocated if future changes to the site are desired.

We will revise the grading and utility plan to eliminate the sidewalk adjacent to the valet parking spaces. Vertical granite curbing with curb inlets to (2) tree filters will be installed.

11. The City is reviewing and may ask the applicant to extend the sidewalk on Middle St to the property line. *The applicant is interesting in providing whatever the City deems appropriate on this matter.*



12. Instead of installing new granite curbing on Middle Street, for environmental reasons, no manufacturing or transporting of new curbing, we would like the applicant to consider re-installing the existing curbing. Either way it is understood that the applicant would like to keep the material of this walk bituminous to accommodate future expansion. Since the existing sidewalk is not in great condition and the curb line is being modified, we would ask that the applicant re-pave the sidewalk. And since the sidewalk material policy in this section of the city is for brick sidewalks, the applicant is requested to post a two year Performance Guarantee to cover the installation of a brick walk in the likelihood the next phase of the development does not materialize.
The plans will be revised as requested. The Site and Utility Demolition plan will indicate the limits of curbing to be removed and salvaged for re-installation. The site plan indicates that salvaged curbing is to be reinstalled.

13. The applicant is request to submit the anticipated quantity of wastewater which will be added to the city's sewer system.
A Wastewater Capacity Application has been submitted to Planning. This application documents the anticipated flows from the new facility. However, please note that in comparison to the previous Jordan's Meats factory, this proposal (including phase II) will be a gross decrease in wastewater discharge of approximately 100,000 gpd. Using round numbers, here is a brief comparison: In 2004, Jordan's Meats used an average daily flow of approximately 140,000 gpd. Proposed phase I will be approximately 26,000 gpd. Full build-out of the site is anticipated to be approximately 40,000 gpd.

14. This department is still reviewing lighting standards for this project, i.e. light pole spacing and power feed requirements with CMP.
No response necessary.

15. The sidewalk ramp tactile warning strips shall be as follows and colored Tan.
A tactile warning strip will be added to the details.

► Marge Schmuckal, Zoning Administrator, City of Portland:

1. I believe that this project is probably meeting the maximum building height of 65 feet for this area of the City. However, I would like to confirm that supposition with more information from the applicant using the same methodology that I use with all applicants. I would like to know the grades around the outside of the proposed building so that I can average the grades. I would also like to know where the top of the structural beams will be located on the structure. The actual height of the structure can be determined from that information.
We have provided the building height on the submitted elevations. However, based on the building architecture comments below, it is anticipated that the building footprint will be changing. Therefore, when we resubmit the revised application; we will provide the Zoning Administrator with all the requested information to calculate the average grade and building height building height.

2. I would also like to confirm where the property line is located at the rear of the proposed building where the stairs descend from the street. It was unclear to me.
The property line follows the southerly and easterly wall of the existing building to remain at the corner of Middle Street and Franklin Street Arterial. This building is the property of 80-90 Corps that hosts Hugo's Restaurant.



3. It is noted that no official loading bays are required under section 14-351. However, it may be useful to indicate how the rear dumpster area will be accessed and utilized by servicing vehicles. This area may also service deliveries, linen services and the like.

The applicant will include a narrative that describes the facility's intended delivery and loading operations with the revised application submittal.

4. Separate permits from Inspection Services will be required for the demolition of existing structures and for signage.

Agreed.

► Jeff Tarling, City Arborist, City of Portland:

a) Street-trees - The project proposes to include planting various shade trees within the sidewalk / public way along Franklin Street & Fore Street. Placement of these trees should be coordinated with the on-street parking to ensure that car doors do not open into the planted tree. Also, the placement of parking meters is typically recommended to alternate with tree planting, post with 2 parking meters between car spaces, then tree well, then parking meter... Placing the tree well near the end of the spaces as proposed may work; just want to have this considered. Tree types are good as shown, tree grates & guards should be reviewed to make sure the finish, painted black or core-10 steel is preferred...

We have discussed the tree spacing with the City Arborist. The proposed tree spacing for Fore Street is coordinated with the proposed parking stalls as shown on the site plan. The proposed parking stall striping will be added to the landscape plan to clarify. The trees are located near the end of the parking stalls to minimize potential conflicts with car doors. The tree grates are clear of the parking stall striping to avoid conflicts with meters should they be installed.

Also, we will revise the tree grate and tree guard details to clarify that tree guards are to be painted with one coat shop primer and two coats of low luster black enamel paint. Tree grates will be weathering (COR-TEN) steel.

b) Esplanade or sidewalk with trees on Franklin Street... "If" future on-street parking is considered for Franklin Street, then the proposed wide sidewalk with street trees with tree grates is good, if not, the use of a turf esplanade with trees should be considered. The proposed planter at the corner of Fore & Franklin should allow enough space for pedestrian travel & sidewalk snow plowing between the two sidewalk tip down ramps.

We will discuss the sidewalk/esplanade design with staff and revise the plans accordingly.

c) Parking lot trees & landscape - Will the six street-trees w/ tree grates in the sidewalk proposed for Fore Street near the temporary parking lot be in the way of future development? An option to plant on the lawn area could be considered.

It is not anticipate that the street trees along Fore Street will be in the way of the future development.

Tree / landscape density - Recommend adding one more Birch tree to the proposed two birch tree groups spaced along Middle Street. The lawn area along India Street should have some buffering / tree planting. One suggestion would be to slightly berm or contour the lawn area, perhaps 1-2' to help buffer the parking lot and help prevent cut-through from the parking lot to the sidewalk area.

The plan will be revised to include an additional birch tree to the Middle Street plantings and additional birch trees in the lawn area adjacent to India Street. The lawn area surrounding the surface parking is proposed with a berm to help buffer the parking lot.

d) Hampshire Street access to Fore Street - The Middle Street gateway to the proposed development is well landscaped and should help with pedestrian circulation. Recommend reuse of the existing granite slabs in the old section of Hampshire Street / crosswalk in this small plaza design or on site if possible.

The plan will be revised to include salvaged granite slabs from the site into the design.

► Planning Staff, City of Portland:

Site Plan Standards 14-526 (a)(16) a.1.

General Comment: The elevation drawings are at a concept level of development and greater detail will be needed for final review prior to public hearing.

More detailed building elevations have been submitted.

It is unclear from the site plan and elevations whether the Fore and Franklin Street door to the restaurant is oriented to the corner or to Fore Street - Clarification requested.

The applicant intends to revise the building architecture as per the discussions with William Needelman and Alex Jagerman.

Site Plan Standards 14-526 (a)(16) a.1.(a)

Comment: Provide details and specifications on the storefront design and first floor windows including the transparency index and sample of the proposed glass. Visual permeability between interior and exterior spaces at the pedestrian level is indicated by this standard. Highly tinted or reflective glass at the pedestrian level is not recommended.

The applicant intends to revise the building architecture as per the discussions with William Needelman and Alex Jagerman.

Site Plan Standards 14-526 (a)(16) a.1.(b)

Comment: The prominence and frequency of pedestrian entrances along Fore Street is undermined by the lack of canopy and/or signage at the mid-block shared entrance (for the restaurant and hotel) and the location of the hotel lobby entrance at the porte-cochere. Consider adding a prominent pedestrian door to the Fore Street façade adjacent to the vehicle entrance. Also consider adjusting signage location from the Franklin/Fore intersection to the pedestrian entrance to the lobby.

The applicant intends to revise the building architecture as per the discussions with William Needelman and Alex Jagerman.

Site Plan Standards 14-526 (a)(16) a.1.(c)

The building is generally well fenestrated and does not show excessive blank walls in the pedestrian areas.

No response necessary

Site Plan Standards 14-526 (a)(16) a.1.(d)

The pedestrian access stairs from Middle Street are well positioned for use and are integrated with the site and building design. Consideration may be given to replacing the existing "cobra head" light fixture with a district street light pole and fixture to better integrate the stairs with the street. Design Note: The City Arborist suggests utilizing the historic granite plank and cobble from the existing alley in this area into the design.

The plan will be revised to include salvaged granite slabs from the site into the design. The applicant will give consideration to removing the existing cobra head light and adding a district street light in the location of the pedestrian stairs from Middle Street.

We will discuss adding a removing the existing cobra head light and add a district street light with staff. There is some design encumbrances to investigate before the applicant can determine if this is feasible.

Site Plan Standards 14-526 (a)(16) a. 2.

Not Applicable

No response necessary

Site Plan Standards 14-526 (a)(16) a. 3.

Applicable to Middle Street only for any later phase of development.

No response necessary

Site Plan Standards 14-526 (a)(16) a.4.(a)

Cross walk design is being addressed by Public Services. There has been discussion of bumping the Middle Street curbing out to shorten the crossing distance from Hampshire Street to the proposed pedestrian stairs.

The applicant will take in consideration any recommendations from Public Services.

Site Plan Standards 14-526 (a)(16) a.4.(b)

See City Arborist comments.

Provide planter details for drainage and irrigation that promote the viability of plantings and minimize impacts to the public sidewalk.

A planter detail will be added to the plans. The proposed development will be implementing some green practices into the design of the facility and in the interest of conserving water; irrigation will not be installed.

Site Plan Standards 14-526 (a)(16) a.4.(c)

The plans show appropriate street lighting styles and spacing (see comment above regarding the pedestrian stairs.) Building mounted lighting details have not yet been provided. Private site lighting of the parking area should be provided with cut-off fixtures.

Building mounted lighting details will be added to the plans. Cut-off fixtures will be provided for the private site lights.

Applicant should anticipate the an electrical distribution system with a metering box suitable for serving all fixtures to be owned by the City

We will review the plans and identify a proposed meter location. We understand that the Planning Staff will provide guidance regarding the type of enclosure requested.

Site Plan Standards 14-526 (a)(16) a. 4. (d)

Granite benches are proposed for Fore Street and a granite seat wall is proposed for the head of the Middle Street stairs.

No response necessary

Site Plan Standards 14-526 (a)(16) a. 4. (e)

The restaurant use is proposed to extend onto the Fore Street façade of the building near Franklin Street.

No response necessary

Site Plan Standards 14-526 (a)(16) b. 1.

General Comment: Context information is not provided. The development should be shown within its context by use of perspective drawings, photomontage, or other graphic means. Suggested views are from Franklin Arterial at Federal Street (Court House and Lincoln Park), Fore Street in front of Bangor Savings, Commercial and Franklin Street (from the Casco Bay Garage, and Fore Street and India Street (from the sewer pump station.)

The applicant intends to submit all requested graphics to assist the Planning Staff and the Planning Board with their review of the project.

Site Plan Standards 14-526 (a)(16) b.1.(a)

The street walls of Fore and Franklin are enforced by this development.

No response necessary.

Site Plan Standards 14-526 (a)(16) b. 1. (b)

NA

No response necessary.

Site Plan Standards 14-526 (a)(16) b. 1. (c)

See below:

No response necessary.

Site Plan Standards 14-526 (a)(16) b. 1. (d)

The building massing and composition does not provide distinct massing elements at the key corners at Franklin Street. Given the visibility of the building from Franklin, Fore (as the terminating view from the Old Port) and Commercial Street, these corners are important.

The applicant intends to revise the building architecture as per the discussions with William Needelman and Alex Jagerman.

The Middle and Franklin corner has no vertical differentiation from the main mass of the building and is un-fenestrated. The Fore and Franklin corner is punctuated by only a minor parapet rise at the center of the bay.

The applicant intends to revise the building architecture as per the discussions with William Needelman and Alex Jagerman.

The design's relationship to its prominent location and to other buildings in the downtown and Old Port will be strengthened by vertical differentiation and fenestration of the building corner at Middle Street and simplifying and enlarging the punctuation at Fore Street. If fenestration of the top story of the Middle Street corner interferes with proposed signage, an alternative location for signage should be considered on the body of the building.

The applicant intends to revise the building architecture as per the discussions with William Needelman and Alex Jagerman.

Site Plan Standards 14-526 (a)(16) b. 1. (e)

Addressed above

No response necessary.

Site Plan Standards 14-526 (a)(16) b. 1. (f)

No comment was made.

Site Plan Standards 14-526 (a)(16) b. 2. (a, b, c, & d)

Standards for set back appear to be met.

No response necessary.

Site Plan Standards 14-526 (a)(16) c.

Need details – none provided. Readily visible mechanical equipment requires screening

Details will be provided in the revised application submittal.

Site Plan Standards 14-526 (a)(16) d.

NA

No response necessary.

Site Plan Standards 14-526 (a)(16) e.

Undue wind impacts are not anticipated.

Agreed.

Site Plan Standards 14-526 (a)(16) f.

Set backs from the adjacent building at Franklin and Middle will be increased.

Agreed.

Site Plan Standards 14-526 (a)(16) g.

NA

No response necessary.

The applicant and design team looks forward to working in close cooperation with the Planning Staff and the Planning Board to develop, permit, and ultimately construct this viable project.

Sincerely,

Opechee Construction Corporation



Barry Stowe

NEEDELMAN
mtg notes



Planning Board Report #7-10

PLANNING BOARD REPORT PORTLAND, MAINE

HOTEL, RESIDENCES, AND RESTAURANT
207-209 FORE STREET (FORMER JORDAN'S MEATS SITE)
MAJOR SITE PLAN, SUBDIVISION, AND MDOT TRAFFIC MOVEMENT PERMIT
PROJECT ID #99700001
OLD PORT HOSPITALITY, LLC., OPECHEE CONSTRUCTION, APPLICANT

Submitted to: Portland Planning Board Public Hearing Date: APRIL 13, 2010	Prepared by: Bill Needelman, Senior Planner Date: April 9, 2010
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I. INTRODUCTION

Opechee Construction Corporation, doing business as, Old Port Hospitality, LLC., requests a public with the Planning Board to review a proposed hotel, restaurant, and residential development at 207-209 Fore Street on the former Jordan's Meats production plant site. The project anticipates total clearance and redevelopment of the site and is being reviewed as a major site plan, subdivision, conditional use (surface parking in the B-3 zone), and for an MDOT Traffic Movement Permit under delegated review authority.

As the Board will remember, this site has been the subject of extensive development review process, including a conditional rezoning and approval of a Westin Hotel/condominium project, and the recent reversion of the conditional rezoning to the underlying B-3 zone.

The Planning Board held a March 9, 2010 workshop covering the relevant issues regarding this application and the applicant has provided updated material responding to review comments. Updated material is provided in the attachments and has been reviewed by City staff for compliance with the relevant standards of the land use code.

154 notices were sent to area residents and interested parties by mail and an additional 60 interested parties were notified by email. Notices also appeared in the April 5 and April 6, 2010 editions of the *Portland Press Herald*.

additionally, Board is asked to consider several waivers to technical standards described below.

The applicant held the require neighborhood meeting on March 18. The

II. PROJECT DATA

Total area of Lot: 72,290 sq. ft.
Zone: B-3 Downtown Business
Existing Uses: Food packaging plant (vacant) and commercial parking
Proposed Use: Hotel (122 rooms), restaurant (7000 sf) and 12 residential condominiums
Proposed bldg height: 65 feet (six stories)
Proposed floorspace: 96,953 sq. ft
Proposed parking: 93 parking spaces on site with 16-19 on-street spaces gained
Proposed bicycle pkg.: 22
Impervious surface:
Existing: 70,580
Proposed: 49,420

Traffic Trip generation: "Net" after subtracting out 50 "existing" trips from the Jordan's site activity –
70 am peak hour,
95 pm peak hour,
137 Saturday peak hour.

III. PROPOSED DEVELOPMENT

a. Proposal Site and Context

The subject development parcel encompasses a nearly full city block of 1.75 acres in the heart of Portland's urban core. The site does not include an abutting two-story brick building at the corner of Franklin and Middle Streets, which currently houses Hugo's restaurant, Rabelais Books, and the Pepper Club restaurant. The surrounding area is a transitional district that links the Old Port retail/office area with the India Street neighborhood. As one moves east from the subject site, the area's character transitions from the mixed use India Street area to the Eastern Waterfront district, which has recently been improved with the Ocean Gateway Garage and the Residence Inn hotel on Fore Street.

The site currently reflects its industrial past showing nearly complete impervious surface. The Franklin Street side of the lot is improved with a concrete sidewalk and esplanade. The Middle Street frontage of the subject parcel begins at the North West corner near the extension of the Hampshire Street right of way (across the street to the north at the end of the Hugos building.) The cobbled alley way from Middle Street (currently fenced) at the Pepper Club end of the abutting building is on the subject parcel. The Middle Street corridor is dominated by the concrete block façade of the Jordan's plant and has hourly parking and a bituminous sidewalk in poor condition. The Jordan's building steps back

from the India Street frontage of the site and is served by a brick sidewalk in good condition with on-street parking. Fore Street previously served as the truck loading and parking area for the site, and reflects this use with no sidewalks, broken and intermittent curbing, and limited on-street parking.

The Jordan's complex is separated into two buildings. The westerly structure is a garage and maintenance building located adjacent to Franklin Street and shares a party wall with the Hugo's building to the north. The site property line follows this party wall from Franklin Street to Middle Street. Starting at the cobbled alley along Middle Street, the main Jordan's plant building extends nearly 320 feet toward India Street.

With the abandonment of meat processing several years ago, commercial parking is the only current activity on the site and is somewhat randomly distributed within the previous loading and circulation areas for the plant. As of the writing of this report, the applicant has begun the environmental remediation and interior demolition of the Jordan's plant and exterior demolition has been approved by the City.

The site slopes from Middle to Fore Street with drainage currently utilizing a combination of sheet flow and on-site drainage structures to carry storm water into the municipal combined sewer system at Fore and Franklin Streets. A public sewer easement currently crosses the site diagonally from Middle Street to Franklin Street running south down the cobbled alley and turning west along the front of the garage building.

On March 15 of this year, the applicant received City Council approval to relocate the sewer easement from its current location to a new corridor from Middle to Fore Street. The revised sewer easement is shown on the draft subdivision plat (Attachment Y.)

The site is served by all public utilities.

4. Proposed Development

The current proposal shows a single six-story building oriented to the Fore and Franklin Street corner of the site. The site is proposed to be cleared of all buildings resulting in the Hugos building, which currently shares a party wall with the westerly Jordan's structure, being opened to light and air on its southerly side.

a. Program

The first floor of the building is split nearly evenly between restaurant and hotel use. The restaurant occupies the corner location and includes an outside seating area at the Fore Street sidewalk. The restaurant has a primary entrance at the corner facing Fore Street and a secondary shared pedestrian entrance toward the middle of the Fore Street façade. The hotel lobby is located toward the easterly side of the building accessed from a porte cochere/valet entry from Fore Street. The easterly end of the Fore Street façade has a dedicated pedestrian lobby and elevator core for residential condominiums.

Floors two to five are entirely occupied by hotel use and the top floor is entirely

Let the applicant describe the program and site details

architect

Plans have changed - Emphasis on pedestrian entrance, new entrance on Franklin, Window change

residential units.

The easterly portion of the site is occupied by surface parking which is buffered by a 35-foot strip of green space at Fore, India and Middle Streets (as required by B-3 conditional use standards.)

b. Circulation

There is a single vehicle entrance proposed through the building from Fore Street. The vehicle entrance serves a valet area at the hotel door, the parking area, and a loading/trash area facing Middle Street and the rear of the Hugos building. According to the applicant, a portion of the trash area is covered by a roof in updated plans provided.

Pedestrian circulation to the site is provided from the City sidewalks at Fore, Franklin and Middle Streets. The building has three pedestrian entrances from Fore Street and a pedestrian connection from Fore Street to the valet area and hotel lobby door. Previously, there were no doors proposed at Franklin Street, but in the revised drawings a secondary pedestrian entrance is shown on the Franklin façade providing access to an interior stair tower at the northwesterly corner of the building. Pedestrian access from Franklin Street is also proposed by a set of exterior stairs shown between the proposed structure and the Hugos building. Likewise, the applicants propose a set of stairs from Middle Street in the approximate location and orientation of the Hampshire Street right of way projected into the site.

Note: there are no public pedestrian rights over the exterior stair passages shown, but the applicant is willing to condition approval with these passages being kept open to the public. The applicant has provided a note on the draft subdivision plat and a condition of approval is suggested with the subdivision motions.

A mid block crosswalk at the Hampshire Street projection is proposed and has been reviewed by the City's Crosswalk Committee and has been conditionally recommended for approval by the Planning Board. The cross walk is improved with a "bump out" curb to shorten the crossing distance and to improve pedestrian visibility between cars parked on Middle Street.

New brick sidewalks are proposed along Fore and Franklin frontages of the site. The applicant proposes bituminous sidewalks along Middle Street, as a later phase of development is hoped for in the foreseeable future. The applicant is willing to post a performance guarantee equal to the value of a brick sidewalk for Middle Street in case a future development does not occur within a reasonable timeframe. A condition of approval is suggested in the site plan motions.

c. Parking

The 93 surface parking spaces are proposed and are designed to conform to the 35-foot street set back requirements of the B-3 zone. The applicant proposes stacking the hotel spaces in tandem as these spaces can be managed by the hotel valet staff. The residential spaces are proposed for the non-tandem spots in the surface lot and handicapped spaces

are proposed near the loading area. Eleven bike racks (22 bicycle spaces) are shown adjacent to the easterly end of the building along Fore Street at the residential lobby. No parking is proposed for the restaurant.

d. Building Architecture

The proposed building is a conservative brick and concrete block structure oriented to Fore Street. The applicants have provided revised elevations responding an initial staff review. See the B-3 Design Review comments below.

Generally, the design respects the direction and intent of the applicable design standards.

e. Utilities

The applicants are proposing upgrades for all utilities.

A public sewer easement currently crosses the site from Middle to Franklin, carrying combined wastewater from on-site and off-site sources. As noted above, the proposal will move the easement to the area in line with the Hampshire Street right of way projection from Middle Street, under the valet area, out to Fore Street. The applicants have designed separated storm and sanitary lines through the site to Fore Street continuing all the way to Franklin Street. The applicant's design will also pick up previously separated storm water lines from the Hampshire Street area. The Hampshire Street connection is significant as it will result in over six acres of watershed being removed from the combined system.

New waterlines are proposed from Franklin Street. Underground electrical service will route from Franklin down Fore through the valet area to an on-site transformer in the loading area. Gas will route from Middle Street.

Telecommunications, sewer, water, gas, and electrical "ability to serve" letters have been provided.

**IV. SITE PLAN SUBMISSION REQUIREMENTS (Section 14-525) and
SUBDIVISION PLAT AND RECORDING PLAT REQUIREMENTS
(Section 14-496)**

The application material provided is sufficiently complete to allow adequate review of the proposal under applicable standards. After the March 9 Planning Board Workshop, the applicant has provided an updated set of plans, and building drawings reflecting comments previously provided.

Planning Staff is requesting additional specifications on certain architectural details, as noted in the design discussion below. Additionally, a condition of approval is suggested that the applicant provide a revised site plan addressing pavement marking adjustments and the installation of parking meter posts. Site plan comments are more fully described below.

The draft subdivision plat contains the minimum required information for recording, however, the Public Services surveying staff have recommended that certain horizontal locations and vertical datum be adjusted or referenced to City survey control. (See Public Services Review Memo, April 7, 2010, Attachment 4.) Furthermore, it is recommended that mutual access easements between lots created through this subdivision should be noted with greater clarity on the subdivision plat with recording information. A condition of approval is recommended in the suggested motions.

V. WAIVER REQUESTS

The applicant is requesting waivers from certain technical standards as part of this project review. The waiver narrative below was supplied by the applicant (applicant comments in ***bold italics***) followed by Planning Staff comments. Waiver motions for the Board to consider, with recommended conditions, are provided in the Motions section at the conclusion of this report.

A. B-3 Build-To Line:

Section 14-220 (c) (Zoning Ord.): All buildings or structures shall be located within five (5) feet of the property line along street frontages, unless the Planning Board requires or approves an additional distance to comply with the requirements of section 14-526(a)(16)

This waiver is required in order to place the south side of the building a distance greater than 5' from the property line. The increased distance will provide adequate room for the outdoor eating area associated with the proposed restaurant. The waiver also allows for increased pedestrian access along the southern side of the building.

Planning Staff Comment: The B-3 site plan standards are addressed below. Planning Staff supports this waiver.

B. Driveway curb radius:

Standard: Under the City's 'Technical and Design Standards and Guidelines' **Section III, 2 (c) Curbing of Driveways**, requires that the minimum radius be 20 feet.

The waiver is required in order to provide 10 foot wide curb radii at the driveway. The smaller curb radii are proposed in order to decrease the length of pedestrian travel across the driveway mouth. However, with the proposed on-street parking along Fore Street, the entrance can accommodate 20-foot turning radii and will accommodate truck turning movements.

Planning Staff Comment: The City's consulting traffic engineer, Tom Errico, has reviewed this request and has no concerns with the waiver.

4 Standards
① open space support activity pedestrian
② ③ protect development and open space
④ lift quality of materials and design

C. Parking Layout:

Standard: Under the City's 'Technical and Design Standards and Guidelines' **Section III, 3 C Parking Layout:** Lot layout shall conform to figures III-1 and III-2.

A waiver is required in order to provide a parking lot layout that includes tandem valet spaces. These spaces will smaller than the required 9 foot by 19 foot standard parking space. These spaces will be utilized by the valet service attendants only.

Planning Staff Comment: The City's consulting traffic engineer, Tom Errico, has reviewed this request and has no concerns with the waiver.

D. Required Brick Sidewalk along Middle Street:

As shown on the Sidewalk Replacement Material Map.

A waiver is required in order to replace a portion of the sidewalk along Middle Street with a bituminous sidewalk where a brick sidewalk is required. This portion of sidewalk is intended to be temporary. A brick sidewalk will be installed in the future. A bond will be provided for the installation of a brick sidewalk if none is installed in the next three years.

Planning Staff Comment: The sidewalk material policy of the city is a separate ordinance subject to City Council review and is not subject to waiver by the Planning Board. The applicant is committed to the eventual installation of the required brick material, but asks for a temporary asphalt sidewalk to serve this site while development plans for the easterly portion of the site mature. The installation of temporary asphalt sidewalks in the downtown on lots that have planned or anticipated future development has been approved for other developments in the vicinity of the subject site. Public Services and Planning Staff support allowing a temporary asphalt sidewalk for a portion of Middle Street, subject to the performance guarantee condition. A condition of approval requiring a performance guarantee for the brick sidewalk is suggested in the Site Plan motions.

add by

E. Building facade illumination:

Standard: Under the City's 'Technical and Design Standards and Guidelines' **Section XV: Site Lighting Standards, 3. General Standards,** All fixtures, including wall packs, shall be a "cut-off" type where...no direct light shall be directed at or above the horizontal plane.

A waiver is required in order to provide the building facade with illumination. Illumination that directs light above a horizontal plane has

been integrated into the exterior design of the structure to help create a greater sense of security and interest for pedestrians, patrons, and residents. Also, facade illumination will help draw the awareness of new patrons to the building that are unfamiliar with the City.

Planning Staff Comment: Planning Staff was only provided a short period of time to evaluate the lighting submission. Regarding the architectural up-lighting, the fixtures proposed (LED panels) are new to Portland, and the illumination levels shown on the building façade appear greater than have been previously approved by the Planning Board for comparable projects. Planning Staff would like to take additional time to evaluate the architectural lighting for the building and there is a suggested condition of approval that the architectural lighting for the development be subject to staff review and approval prior to issuance of an electrical permit.

F. Illumination levels above the maximum:

Standard: Under the City's 'Technical and Design Standards and Guidelines' **Section XV: Site Lighting Standards, 4. Specific Standards,** Maximum illumination level of 5.0 foot candles shall be measured at the grade.

A waiver is required in order to integrate down lighting on the building facade to highlight the restaurant entrance. Maximum illumination level of 5.0 foot-candles will be exceeded as a result light accumulated from the restaurant's proposed down lighting and the public street lights. Since the ambient light that is spilled onto the property from the public street lights is not addressed in the standards, a wavier is being sought to highlight the restaurant entrance with building facade down lighting. This outside lighting often makes the first impression to the pedestrian customer and can attract customers passing by the establishment. Also, the facade down light illumination will relay a sense of security and comfort during the outdoor dining season.

Planning Staff Comment: The applicant's lighting plan shows illumination levels of over 8 foot candles near a rear door to the residential lobby, and over 10 foot candles near the restaurant main entrance. These levels would not be expected to cause significant glare or undue impacts to the surrounding area. Planning Staff supports this waiver.

VI. PUBLIC COMMENT AND WORKSHOP SUMMARY

- A. Public Comment: Other than at the Planning Board workshop and the required Neighborhood Meeting, no public comment has been received during this review.
- B. Neighborhood Meeting: The applicants held the required Neighborhood Meeting on March 18, 2010. The notice, attendance and minutes of this meeting are provided in Attachment AA of this report. As suggested by the Planning Board, the neighborhood meeting was noticed to the Franklin Arterial "revisioning"

contact list by email.

C. March 9, 2010 Planning Board Workshop

Issues raised by public were largely regarding the project's relationship to Franklin Street and mid-construction circulation issues.

The Planning Board explored the project in detail with both the applicants and staff. Some Board members questioned the details of design on the building and the landscaping around the parking lot. Several Board members expressed the opinion that additional landscaping was needed.

The pedestrian routes through the site received considerable discussion, mostly related to what public rights would be secured and by what means. The Board appeared comfortable with pedestrian access tied to a condition of approval as a compromise mechanism less than a pedestrian easement.

As the Planning Board is the authority that sets the parking requirement for the project, the Board discussed the parking proposal to some detail. Given that the applicant is not proposing parking to be dedicated to the restaurant, options for this parking were discussed with the applicant.

VII. STAFF REVIEW

The proposed development has been reviewed by staff for conformance with the relevant review standards of the subdivision ordinance, site plan ordinances, the requirements of the B-3 zone, and a Traffic Movement Permit under delegated authority from the Maine Department of Transportation. Staff comments are highlighted in this report.

A. ZONING ASSESSMENT

Initial Zoning Comments from Zoning Administrator, Marge Schmuckal, February 25, 2010

The entire property is located within a B-3 Downtown Business Zone. It is not within a Pedestrian Activities District (PAD). However the street frontage along Middle Street is considered a PAD Encouragement District. No structure is being proposed along Middle Street at this time. It is not in a Historic District.

The applicant is proposing to redevelop the site to include a new 180 seat restaurant, a 122 room hotel, and 12 residential condominiums. This is considered a change of use for the property. On an adjoining lot a parking lot for 93 parking spaces is being proposed. The proposed uses are meeting the allowable uses listed in the B-3 Zone. The surface parking lot use is a conditional use appeal before the Planning Board. The surface parking lot is required to be no closer than 35 feet to any street line. This submitted plan is showing all parking 35 feet from all the street lines. Because the proposed project is over 50,000 square feet in floor area, parking requirements are to be determined by the Planning Board under section 14-332(t). 22 bike spaces are proposed.

The B-3 Zone requires a street build-to setback of no more than 5 feet, unless the Planning Board reviews and approves an appropriate setback further back. The applicant has requested a further setback of approximately 10 feet for outdoor seating for the first floor restaurant use.

Because there will be 12 residential condominiums, this project must also be approved under Subdivision requirements.

I believe that this project is probably meeting the maximum building height of 65 feet for this area of the City. However, I would like to confirm that supposition with more information from the applicant using the same methodology that I see with all applicants. I would like to know the grades around the outside of the proposed building so that I can average the grades. The actual height of the structure can be determined from that information.

I would also like to confirm where the property line is located at the rear of the proposed building where the stairs descend from the street. It was unclear to me.

It is noted that no official loading bays are required under section 14-351. However, it may be useful to indicate how the rear dumpster area will be accessed and utilized by servicing vehicles. This area may also service deliveries, linen services and the like.

Separate permits from Inspection Services will be required for the demolition of existing structures and for signage.

The project is not in a Shoreland Zone. The project is not in a 100-year flood zone.

Final Zoning Comments for Public Hearing: March 22, 2010

On 3/17/10 Barry Stowe, Opechee, Construction, submitted (by e-mail) building height calculations for review. The calcs use the standard methodology for determining heights. It is understood that the building plans are still being created at this juncture. It is further understood that the development of the building plans will not differ significantly to violate the required building heights.

B. CONDITIONAL USE STANDARDS

As noted above, surface parking in the B-3 zone is a conditional use. Such parking is subject to a set back requirement where any surface parking must be located 35 feet from any public street. The proposed design meets this requirement.

24 month limit on lease for use

The proposed development has additionally been reviewed by staff for conformance with the relevant Conditional Use standards of Portland's Zoning Ordinance.

1. *There are unique or distinctive characteristics or effects associated with the proposed conditional use.*

2. *There will an adverse impact upon the health, safety, or welfare of the public or the surrounding area; and*
3. *Such impact differs substantially from the impact which would normally occur from such a use in that zone.*

The proposed parking is not anticipated to have unique characteristics or effects, will not adversely impact the surrounding area, and does not differ substantially from other surface parking lots in the B-3 zone.

B. SUBDIVISION STANDARDS

The proposed development has been reviewed by staff for conformance with the relevant review standards of the subdivision ordinance and applicable regulations. Staff comments are listed below.

1. Will Not Result in Undue Water and Air Pollution (Section 14-497 (a) 1), and Will Not Result in Undue Soil Erosion (Section 14-497 (a) 4)

The proposed development is not anticipated to cause undue water, air or soil pollution or erosion.

At the time of the previous review, the applicant was asked to make various amendments to the plans to improve the storm water quality. The revised plans are described in the applicant's Attachment BB and respond proactively to City comments. The revised plans have altered the proposed storm water system to route parking runoff into a vegetated swale and through "tree box" filters. The applicant has additionally increased catch basin sump depth as suggested. Given that storm water quality treatment is not technically required, Staff appreciates the applicant's willingness to improve the environmental infrastructure for the site.

Through extensive sewer separation, improved storm water quality enhancements, and reduced impervious surface, the proposed development should improve environmental impacts of the subject site.

Revised engineering comments are provided in Attachment 4 of this report.

- 2/3. Sufficient Water Available (Section 14-497 (a) 2 and 3)

The applicant has received a water capacity letter from the Portland Water District the Portland Fire Safety Officer, Captain Keith Gautreau had recommended that adequate hydrant access is available.

4. Will Not Cause Unreasonable Traffic Congestion (Section 14-497 (a) 5)

Traffic movement and parking are additionally addressed in the Site Plan section

Accordingly, the applicant concludes that the 93 parking spaces will adequately serve the project.

- According to the Parking Generation Manual, Institute of Transportation Engineers (ITE), a Business Type Hotel generates 0.64 parking spaces per room. This correlates to 78 parking spaces for the 122 room hotel. This closely matches that estimated by the applicant.
- I support the assumption that one parking space per residential unit is appropriate. Although some residential units may have more than one vehicle, the City has been permitting projects that provide one parking space per unit. Accordingly, 12 parking spaces will be required.
- According to ITE and other publications, the restaurant can be expected to generate a parking demand of between 43 and 67 spaces. Considering the project location, the City has recognized that many restaurant patrons will be captured from existing businesses and therefore would not generate the need defined in the professional publications. As a compromise, the City has required restaurant developments to provide for parking for their employees only, and customers can utilize on-street or public parking facilities. Based upon the applicants documents, the proposed restaurant can be expected to have 14 employees during peak time periods. To account for alternative modes of transport, I would estimate the need for 10 parking spaces. The City is in the process of implementing a fee-in-lieu parking program and such would suggest that the applicant make a contribution that would be used toward increasing the parking supply in the vicinity of the project. Based upon parking contribution rates being considered (the lower end is \$5,000 per space), the applicant should contribute \$50,000.00.

PARKING

The Planning Staff recommends that the off-street parking supply proposed for the hotel and residential components of the development are adequate for the anticipated demand of the development. The lack of parking proposed for the restaurant component of the development has the potential to increase pressure on the on-street parking supply for the area. The Planning Board is recommended to impose a \$50,000 contribution of funds for use by the City to increase the on-street parking supply in the vicinity of the subject property. The funds may be used for the design and construction necessary to adjust curbing, install signage, install meters, or implement other physical improvements needed to increase public auto and bicycle parking supply within 1500 feet of the subject property.

The applicant has indicated their agreement to this condition.

2. Bulk, Location, Health, Safety Air (Section 14-526 (a) 3) and Bulk, Location, Height of Proposed Buildings (Section 14-526 (a) 4)

The applicant has provided a context massing study of the proposed development

and surrounding area for the Board's evaluation. Please see Attachment ZZ. 9-ZZ.13. The bulk height and location of the development is not anticipated to negatively impact surrounding properties.

3. Sewers, Storm drains, Water (Section 14-526 (a) 5), Soils and Drainage (Section 14-526 (a) 8), and Consistent with City Infrastructure (Section 14-526 (a) 11

See Subdivision comments above and the City Engineer comments in Attachment 4.

The development is designed to be consistent with surrounding City infrastructure, including sewers, storm drains, and roadways.

The Franklin Street corridor has conceptual alternatives for future redevelopment and the proposed changes do not prohibit additional changes to the corridor.

4. Landscaping and Buffering (Section 14-526 (a) 6) and Minimizes Disturbance or Destruction of Existing Vegetation (Section 14-526 (a) 7)

At the time of the March 9 workshop, the City Arborist had several comments regarding additional trees and landscaping amendments to the site. The revised plans respond to these suggestions and the Arborist supports the new design.

5. Exterior Lighting (Section 14-526 (a) 9)

Exterior lighting is discussed in the "waiver" section above.

6. Fire and Emergency Access (Section 14-526 (a) 10)

Captain Keith Gautreau has conducted a review of the submitted plans and offers the following comments:

The initial Fire review is all set for the Jordan's project 207 & 209 Fore Street. Access to the structure will be adequate and the water supply (hydrants) are acceptable and in compliance with NFPA 1.

7. Industrial Development (Section 14-526 (a) 12)

Not applicable.

8. Existing Natural Resources (Section 14-526 (a) 20) and Significant Groundwater Aquifer (Section 14-526 (a) 21)

No significant natural or ground water resources will be impacted by this development. See subdivision standards, above.

9. Residence Professional Zone – Design Standard (Section 14-526 (a) 13)
Not applicable.
10. Planned Residential Unit Development Review (Section 14-526 (a) 14)
Not applicable.
11. Multi-family and Other Housing Types Design Standard (Section 14-526 (a) 15)
Not applicable.

12. B-3 Design Standards (Section 14-526 (a) 16)

Planning Staff has reviewed the project for conformance with the B-3 Design Standards and the applicable Downtown Urban Design Guidelines. The following review comments are offered by the Planning Staff after reviewing the revised design of a hotel, restaurant and residential development proposed by Opechee Construction Corp. for the former Jordan's Meats site at 207 to 209 Fore Street.

The review was based on site plan drawings supplied with the development application dated March 23, 2010. Review comments below are provided in *italics* in response to land use code text and the applicable site plan standards for development in the B-3 zone, as shown in underline. Review comments were further informed by language found within the Downtown Urban Design Guidelines.

Site Plan Standards 14-526 (a)

(16) Development located within the B-3 zone shall also meet the following standards. Adequacy in meeting these standards will be evaluated on the basis of descriptions and illustrations in the Downtown Urban Design Guidelines. Nothing in this section is intended to discourage creative and responsive design or to mandate similarity or mimicry of design in order to achieve the standards herein:

a. Relationship to the pedestrian environment:

1. General: The exterior design of portions of buildings within the first thirty-five (35) feet of height shall enhance the character, attractiveness, comfort, security, and usability of the street level pedestrian environment. Factors to be considered include the design, placement, character and quality of the following:

(a) Storefronts and building facades, including such factors as relationship to adjacent or nearby structures or open space, pedestrian character, materials and detailing, transparency and contemporary design;

Comment: Visual permeability between interior and exterior spaces at the pedestrian level is indicated by this standard. The sample glass provided appears overly opaque and more transparent glazing is recommended – particularly at the street level. A condition of approval is suggested.

- (b) Building entrances, including such factors as compatibility with the building's façade, prominence along the street, access to the street, and accessibility for physically handicapped or for those with special needs;

Comment: The prominence and frequency of pedestrian entrances along Fore Street has been improved with the revised design. The mid-block entry has a more pronounced treatment, which has been repeated at the valet area to accentuate the pedestrian entrance to the lobby from the street.

- (c) Blank facades; and

The building is generally well fenestrated and does not show excessive blank walls in the pedestrian areas.

- (d) Special features, such as selective use of such features as building arcades and skywalks or elevated walkways.

The pedestrian access stairs from Middle Street are well positioned for use and are integrated with the site and building design.

2. Pedestrian activities district (PAD):

Not Applicable

3. Pedestrian activities district (PAD) encouragement areas:

Applicable to Middle Street only for any later phase of development.

4. Sidewalk areas and open space:

- (a) Sidewalk, crosswalk, and street paving materials;

Cross walk design pavement materials have been addressed above

- (b) Landscaping, planters, irrigation, and tree guards and

grates;

See City Arborist comments.

Provide planter details for drainage and irrigation that promote the viability of plantings and minimize impacts to the public sidewalk.

(c) Lighting;

Addressed above in the waiver site plan sections.

(d) Pedestrian amenities such as benches and other seating, trash receptacles, kiosks, bus shelters, artwork, directional and informational signage, fountains, and other special features; and

Granite benches are proposed for Fore Street and a granite seat wall is proposed for the head of the Middle Street stairs.

(e) Sidewalk vendors and sidewalk cafes.

The restaurant use is proposed to extend onto the Fore Street façade of the building near Franklin Street.

b. Relationship to existing development:

1. General: Proposed development shall respect, enhance, and be integrated with the existing character of the general pattern of development in the downtown, surrounding building environment and streetscape, as described and illustrated in the Downtown Urban Design Guidelines. Factors to be considered include the relationship to the following existing patterns:

General Comment: Context information is provided with new materials in Attachments ZZ. The proposal is compatible with the general development pattern of the downtown area.

(a) Street walls and building setbacks;

The street walls of Fore and Franklin are enforced by this development.

(b) Open space;

NA

(c) Building form, scale and massing;

See below:

(d) Facade proportion and composition;

The revised elevation drawings adequately address proportion and composition issues previously addressed. Additional detail on the cornice feature shown at the Fore and Franklin Street corner should be provided prior to issuance of a building permit.

(e) Pedestrian circulation and building entrances;

Addressed above.

(f) Parking.

Surface parking is not typically associated with downtown development, but with the setbacks shown, there is room to develop around the parking or, more predictably, convert it to structured parking in a later development phase. The expanse of pavement is off-set by the augmented landscaping shown in the revised plans.

2. Standards for increasing setback beyond street build-to line: A proposed development may exceed maximum setbacks as required in section 14-220(c) only where the applicant demonstrates to the Planning Board that the introduction of increased building setbacks at the street level:

(a) Provides substantial and viable publicly accessible open space or other amenity at the street level that supports and reinforces pedestrian activity and interest. Such amenities may include without limitation plazas, outdoor eating spaces and cafes, or wider sidewalk circulation areas in locations of substantial pedestrian congestion;

The Fore Street façade sets back to accommodate outside seating and an expanded sidewalk and landscaped area.

(b) Does not substantially detract from the prevailing street wall character by introducing such additional setback at critical building locations such as prominent form-defining

corners, or create a sense of discontinuity in particularly consistent or continuous settings;

Prevailing street walls will not be interrupted by the setback.

- (c) Does not detract from existing publicly accessible open space by creating an excessive amount of open space in one (1) area or by diminishing the viability or liveliness of that existing open space; and

Not applicable

- (d) The area of setback is of high quality and character of design and of acceptable orientation to solar access and wind impacts as to be attractive to pedestrian activity.

The area of set back is on the southerly side of the building and should be attractive to pedestrian activity.

Standards for set back appear to be met. Staff recommends approval of the larger setback.

- c. Roof top appurtenances: All mechanical equipment, ventilating and air conditioning and other building systems, elevators, stairways, radio or television masts or equipment, or other rooftop elements not intended for human occupancy shall be fully enclosed in a manner consistent with the character, shape and materials of the principal building, as described and illustrated in the Downtown Urban Design Guidelines;

The applicant has provided section drawings (Attachment ZZ.3) showing that the roof top mechanicals will have no visibility from pedestrians in the area of the site. It is possible that rooftop mechanical equipment will be visible at a distance from Franklin Street at Congress Street and the area around Lincoln Park. The Board may want to explore with the applicant whether additional screening is needed for views at this distance.

- d. Shadow impact on open space: The location, massing and orientation of portions of buildings in excess of sixty-five (65) feet in height shall be such that substantial shadow impacts on public plazas, parks, and other publicly accessible open space are avoided. In determining the impact of shadows, the following factors shall be taken into account: the amount of area shadowed, the time and duration of the shadow, and the importance of sunlight to the utility of the type of open space being shadowed, as

described and illustrated in the Downtown Urban Design Guidelines;

NA

- e. Wind impacts: The location, massing, orientation and architectural design of a new building or a building addition shall be such that no significant adverse wind impacts are created. In determining the impact of winds, the following factors shall be taken into account: the pre-development and projected post-development wind speeds and their impact on pedestrian movement, comfort and safety; and the impact of projected wind speed on the use of and comfort within existing and proposed pedestrian seating areas and other adverse impacts upon the surrounding area;

Undue wind impacts are not anticipated.

- f. Setbacks from existing structures: The location and design of proposed structures shall not create a detrimental impact on the structural integrity or the safety of adjacent structures or the occupants thereof;

Set backs from the adjacent building at Franklin and Middle will be increased.

- g. Building tops: Buildings or structures which exceed one hundred fifty (150) feet in height shall be designed so as to provide a distinctive top to the building which visually conveys a sense of interest and vertical termination to the building, as described and illustrated in the Downtown Urban Design Guidelines;

NA

13. Adjacent or within 100 feet of a Historic Resource (Section 14-526 (a) 18)

The Historic Preservation Staff participated in the design review of the building and does not find the design incongruous with the near by waterfront historic district.

14. View Corridors (Section 14-526 (a) 19)

None impacted.

15. and 16 Signage (Section 14-526 (a) 22 and 23), Design Standards for Major and Minor Businesses (Section 14-526 (a) 24)

Signage will be reviewed at the time of building permit application. The

schematic signs shown appear compatible with comparable developments in the B-3 zone.

17. Design Standards for Development in Industrial Zones (Section 14-526 (a) 25)
Not applicable.
18. Design Standards for B-5 and B-5b (Section 14-526 (a) 26)
Not applicable.
19. Design Standards for B-1, B-1b, B-2 and B-2b (Section 14-526 (a) 27)
Not applicable.
20. Design Standards for R-6 Small Residential Lot Development (Section 14-526 (a) 28)
Not applicable.
21. University of Southern Maine Design Standards (Section 14-526 (a) 29)
Not applicable.
22. Bayside B-7 Design Standards (Section 14-526 (a) 30)
Not applicable.
23. Eastern Waterfront Design Standards (Section 14-526 (a) 31)

Geographically, the subject parcel is covered both by the Downtown Urban Design Guidelines (as reflected in the B-3 design standards above) and the Eastern Waterfront Design Guidelines. The two sets of design parameters are highly compatible, with the B-3 standards being the more specific and generally more rigorous of the two. Design *standards* for the Eastern Waterfront have not yet been adopted.

I. STAFF RECOMMENDATION

Subject to the condition suggested below, staff recommends approval of the proposed development. The waivers requested are reasonable accommodations to the specifics of the proposed uses and the site and will not unreasonably impact surrounding properties or the public.

The applicants have provided amenities and site design considerations that go beyond the minimums required by standards, including providing public access across the site and enhanced storm water management.

The off-street parking supplied by the proposal is a reasonable compromise that addressed the needs of the hotel and residential components of the program and mitigates the proposed restaurant parking impacts through direct increase in the on-street supply of parking on Fore Street and the funding of additional on-street spaces as will be identified and constructed in the area.

VI. MOTIONS FOR THE BOARD TO CONSIDER

A. Waivers:

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations, contained in the Planning Board Report for application # 99700001 relevant to the Portland's Technical and Design Standards and other regulations, and the testimony presented at the Planning Board hearing:

1. Planning Board (waives/does not waive) the Technical Standard for the Driveway curb radius, Section III, 2 (c) *ML 52 5-0 8, in about*
2. The Planning Board (waives/does not waive) the Technical Standard for Parking Layout, Section III, 3 C *ML 52 5-0*
3. The Planning Board (waives/does not waive) the Technical Standard requiring full cut off light fixtures, Section XV: Site Lighting Standards, 3.

Subject to the following condition: *ML 52, 4-1, silk opposed*

That the lighting plans and specifications provided for architectural up-lighting require additional evaluation of illumination levels and potential light pollution impacts. Architectural lighting for the development is subject to staff review and approval prior to issuance of an electrical permit.

4. The Planning Board (waives/does not waive) the Technical Standard for maximum illumination levels, Section XV: Site Lighting Standards, 4. *5-0 ML 52 @ Restaurant ? NE corner*
5. The Planning Board finds that the increased building setback beyond the requirements set forth in Section 14-220 (c), namely that all buildings or structures shall be located within five (5) feet of the property line along street frontages:

(a) (Does/Does not) Provide substantial and viable publicly accessible

open space or other amenity at the street level that supports and reinforces pedestrian activity and interest;

(b) (Does/Does not) substantially detract from the prevailing street wall character;

(c) (Does/ Does not) detract from existing publicly accessible open space; and,

(d) The area of setback (is/is not) of high quality and character of design and of acceptable orientation to solar access and wind impacts as to be attractive to pedestrian activity.

Therefore the ^{Board} (waives/does not waive) the 5-foot maximum building set back as per Site plan standard 14-526(a)16 b.2. *ML, 25. 5-0*

B. Conditional Use: *Parking*

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report for application # 99700001 relevant to Portland's Conditional Use Standards and other regulations, and the testimony presented at the Planning Board hearing:

The Planning Board finds the proposed conditional use for surface parking in the B-3 zone (does or does not) meet the standards of Section 14-474 and 14-218(b)5 subject to the following conditions of approval:

That the condominium documents for the site contain a provision that allow^s surface parking to transition to structured parking or be relocated to allow future development of the easterly portion of the site. *ML, 2L 5-0*

C. Subdivision:

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report for application # 99700001 relevant to the Subdivision Ordinance and other regulations, and the testimony presented at the Planning Board hearing, the Planning Board finds that the plan (is/is not) in conformance with the subdivision standards of the land use code, subject to the following conditions of approval: *w/all conditions ML, 25*

1. Public pedestrian access shall be maintained through the site from Middle Street to Fore Street over stairs and passageways shown on the site plans and noted on the subdivision plat note 38. Such access shall not be removed or interrupted, accept for routine safety or maintenance reasons without prior written request by *except*

5-0

the applicant or subsequent property owner subject to review and approval by the Planning Board.

2. The applicant shall submit a revised recording plat for Public Services review and approval prior to signature by the Planning Board. Plat revisions will include citations for recorded parking and access easements between the subdivision lots and shall address bearing and elevation datum revisions issues addressed in the Public Services review memo dated April 7, 2010, included in Attachment 4 of this report.
3. That the applicants provide copies of the condominium documents for City Legal staff review and approval prior to recording the subdivision plat.

D. Site Plan:

5-0, P, M absent

On the basis of the application, plans, reports and other information submitted by the applicant, findings and recommendations contained in the Planning Board Report for application # 99700001 relevant to the Site Plan Ordinance and a Maine Department of Transportation Traffic Movement Permit under delegated review authority and other regulations, and the testimony presented at the Planning Board hearing, the Planning Board finds that the plan (is/is not) in conformance with the site plan standards of the land use code, subject to the following conditions of approval:

ML, 29

site plan

w/all conditions

1. Prior to issuance of a building permit, the applicant shall provide a revised site plan for staff review and approval showing revised pavement striping and installation of parking meter posts on Fore Street. The pavement markings shall remove on-street parking stalls and turning demarcations and shall adjust the center line markings to reflect proposed curb and parking adjustments to Fore Street.

Performance Guarantee

2. Prior to issuance of a building permit, the applicant shall post a performance guarantee equal to the value of installing brick sidewalk for the entire length of the Middle Street frontage of the subject parcel. The term of the performance guarantee shall be no-longer than 2 years after the date of approval for the subject development, as may be extended at the City's sole discretion. If after 2 years following the date of site plan approval, the applicant has not installed a brick sidewalk along the entire Middle Street lot frontage, the City may draw on the Performance Guarantee funds to complete such and improvement.

move add condition 1 from subdivision motion, move L, 2 Lays 5-0

#8, subdivision condition #1

Traffic

3. Prior to the issuance of a building permit, the applicant should make a contribution of \$1,200.00 towards improvements at the India Street/Middle Street intersection.
4. Prior to the issuance of a certificate of occupancy, the applicant shall development and implement a signal timing plan for Franklin Street subject to Public Services review and approval that ensures optimal operations, including coordination with other intersections as may be necessary to mitigate impacts of the development.
5. Prior to issuance of a building Permit, the applicant shall contribute \$50,000 for use by the City to increase the on-street parking supply in the vicinity of the subject property. The funds may be used for the design and construction necessary to adjust curbing, install signage, install meters, or implement other physical improvements needed to increase public auto and bicycle parking supply within 1500 feet of the subject property. If not used within 10 years of the date of project approval, any remaining funds will be returned to the applicant.
6. Prior to issuance of a building permit the applicant shall provide an revised glazing sample for review and approval of the Planning Staff showing clear or virtually clear glass for the street level openings and a highly transparent glass for upper floors.
7. Prior to issuance of a building permit the applicant shall provide material details and design specifications for the roof cornice canopy and brackets at the Fore and Franklin Street corner of the building.

Paving

Architectural Details

4-21
5-0

consistent with the Sustainable Guiding Process

to be submitted by site plan

to be submitted by site plan

Motion deleted #9
 evidence prior to C/O shall be submitted for 10 year distance of the development
 RTI by owner or tenant for reasonable use
 5-0

5-0

#9 Based on applicant's responsibility to the Board, short term decision

Indecision is substitution, but if applicant does not appear by 2 year w/ 2m extension, see applicant submit

Attachments

1. Traffic Permit Scoping Meeting Notes, Tom Errico, Consulting Traffic Engineer
2. Hotel Parking Statistics, John Peverada, City Parking Manager
3. Sewer Capacity Letter, March 9, 2010
4. Public Services comment, April 7, 2010
5. Traffic Engineering comment, April 5, 2010

Applicant's Submittal

- AA. Neighborhood meeting information
- BB. Updated Project narrative and response to City Development Review Comments, March 23, 2010
- CC. Lighting fixture specifications
 - A. Cover and Application Forms
 - B. Table of Contents
 - C. Narrative and Land Area Stats
 - D. Easements
 - E. Solid Waste
 - F. Off Site Facilities and Utility Capacity
 - G. Storm Water
- GG. Storm Water Management Plan Addendum, calculations omitted, March 23, 2010
- H. Construction Plan and Regulatory Approvals
- I. Technical and Financial Capacity
- J. Right Title and Interest
- K. Natural and Historic Areas
- L. Recyclable Material
- M. Traffic Management
- MM. Traffic Movement Permit Submission, calculations omitted, March 22, 2010
- N. Parking Analysis
- O. Schedule
- P. Conditional Use Permit Application
- Q. Waiver Requests, revised April 7, 2010
- R. Sewer Easement Draft
- S. Plan Set Cover, Existing Conditions, and Demolition Plans
- T. Site Plan
- V. Landscaping
- U. Grading and Utilities
- W. Details
- X. Traffic Turning Templates
- Y. Subdivision Plat – Draft, Updated March 23, 2010
- Z. Lighting photometric plans and elevations
- ZZ. Architectural Drawings – Floor Plans, Elevations, Rendering, Massing Models (Updated, March 23, 2010)

City of Portland

TRAFFIC MOVEMENT PERMIT SCOPING MEETING

Permit Category: [X] 100 - 200 PCEs [] 200 + PCEs
[] New [] Modification

Attendance: Date: 3/3/2010

John Adams, Mark Woglom, Tom Errico

Meeting Location: Planning Department, City Hall

Project Name: 207 & 209 Fore Street - Hotel, Restaurant and Residences

Address: 207 & 209 Fore Street, Portland, Maine

Applicant: Old Port Hospitality

Address: 11 Corporate Drive Belmont, NH 03220

Project Engineer: Sebago Technics (John Adams)

Address: One Chabot Street, Westbrook, ME 04098-1339

Portland Traffic Engineer (reviewer): Tom Errico

Filings: Has the Applicant notified abutters via certified mail at least 7 days prior to this meeting? Yes

Has the "Notice of Intent to File" been published in a local newspaper? Yes

Section 1 - Site and Traffic Information

1A. Site Plan

Size of development parcel (acres): 1.75

Size of development parcel to be left non-vegetated (acres):

Traffic Movement Permit Scoping Meeting
Project Name: 207 & 209 Fore Street

Basis for using above-listed percentages:

- Existing traffic patterns of adjacent street
- Zip code survey
- Gravity model
- Other. Explain. _____

Comments: _____

Section 2 - Traffic Crashes

- 2006-08 Year(s) of MDOT crash records for study area
- 1 Number of high crash locations
- No Collision diagrams provided

Mitigation provided for each high crash location: _____

Other traffic problems: _____

Comments: A collision diagram and crash analysis will be prepared by the applicant at the India Street and Middle Street intersection.

Section 3 - Development Entrances and Exits

3A. Entrance and Exit Locations

- _____ Distance to nearest intersecting road or town line (to the nearest hundredth of a mile)
- _____ Number, width and surface of each proposed entrance/exit

3B. Plan View of Each Intersection Created

Names of intersecting road Fore Street and Driveway

Posted speed limits 30 mph (assumed) on Fore Street (The City will investigate parking ordinance in proximity of to determine whether spaces needs to be removed)

Entrance/Exit Sight Distance:	Left	Right
Fore Street	350 feet	350 feet
_____	_____	_____
_____	_____	_____
_____	_____	_____

Traffic Movement Permit Scoping Meeting
Project Name: 207 & 209 Fore Street

Usage and location of all driveways and roads located adjacent to the development site:

3C. Entrance/Exit Design

- Lighting
- Driveway spacing and corner clearance (Access Management)
- Adequate sight distance for vehicles exiting development
- Entrance grade (see fact sheet)
- Entrance/exit widths less than 42 feet (12.8 meters)
- Left turn lane
- Right turn lane
- Signal warranted
- Lane encroachment
- Separation islands (see fact sheet)
- Drainage study using 50 year storm for culverts and to connect to MDOT system
- Capacity Analysis
- Queuing Analysis
- Study to be submitted to MDOT Division Engineer

Comments: _____

Section 4 - Title, Right or Interest

- Title, right or interest in project site
- Title, right or interest in entrance/exits
- Title, right or interest in drainage easement affecting MDOT

Section 5 - Public or Private Rights-of-Way

- Location and width or proposed streets, easements and other public or private rights-of-way
- No signs, structures or pavement connected to the entrance

Section 6 - Schedule

Project schedule: Complete in 2011

Further study items required (\$500.00 additional fee): _____

Section 7 - Full Traffic Study

_____ \$1,500.00 additional fee required

Build-out year (Phase 1): 2011
Build-out year (Full Occupancy): _____

FS 1. Time Period(s) for Traffic Engineering Analysis

- _____ AM weekday peak hour of adjacent street.
- _____ Noontime weekday peak hour of adjacent street
- x _____ PM weekday peak hour of adjacent street.
- _____ Noontime Saturday peak hour of adjacent street.
- _____ Other. Explain. The applicant confirmed that AM volumes are lower than PM and therefore AM study is not required.

FS 2. Background Annual Traffic Growth Rate

- Type of counts taken:
- X _____ Peak hour
 - _____ AADT
 - yes _____ Base counts less than 2 years old?

FS 3. Study Area to Include the Following Intersections

Franklin Street/Middle Street, Franklin Street/Fore Street, Fore Street/Site Drive

Additional intersection(s) required if one hour volumes from development are:
25 vehicles in left turn only lane
35 vehicles in through, right turn lane or combined through and right turn
35 vehicles (multiplying the left turn volume by 1.5) in a combined left turn and through lane, or a combined left turn, through and right turn lane

FS 4. Intersection Capacity Analysis

- _____ Isolated
- X _____ Interconnected. Intersection(s) Franklin Street/Fore Street and Franklin Street/Middle Street

Software package (isolated): _____
Software package (interconnected): Synchro/SimTraffic

FS 5. Analyze or Evaluate the Following

	<u>Location</u>
_____ Left turn lane warranted	_____
_____ Right turn lane warranted	_____
_____ Traffic signal warranted	_____
_____ Sight distance evaluation	_____
_____ Truck climbing lane warranted	_____
_____ Truck/RV turning radii evaluation	_____
<u>X</u> _____ Investigation of HCL	<u>India Street and Middle Street</u>

FS 6. Other Development Traffic To Be Included in Study

Noted previously

FS 7. List Location(s) and Date(s) of Completion of Other Projects and Their Traffic Engineer

Other Items: The applicant will review the implication of queuing at the study intersections, particularly as it relates to queuing on Fore Street and whether vehicles will block the proposed driveway.

Parking Provided for Existing Portland Hotels

Hotel Name	Address	# of Rooms	# of Available Parking Spaces	Sq. Ft. Exhibit Space	Sq. Ft. of Banquet Space	Sq. Ft. of Restaurant Space	Sq. Ft. of Retail Space
Westin Hotel	38 India Street	229 rooms 97 condos 17 hotelominiums	324 pkg spaces	n/a		5,871 sq. ft.	15,374 sq. ft.
Holiday Inn	Spring Street	238 rooms	320 total Parking garage Free St pkg lot Pleasant St pkg lot. Free St pkg lot open to public but only when there are no large events going on.	12,500	10,000 sq. ft.	110 seating ca	n/a
Hilton Gardens Hc	Commercial/Fr	120 rooms	86 total 46 on site but leased 40 off site at two locations. Max out all of the pkg when there is an event.	n/a	1,200 sq. ft.	46 seating	n/a
Double Tree Hotel	Congress Street	149 rooms	200 total	n/a	5,000 sq. ft.		n/a
PtInd Regency	Milk Street	95 rooms	28 total but lease spaces for overflow pkg when needed	n/a	5,800 sq. ft.	54 seating	n/a
PtInd Harbor Hotel	Fore Street	97 rooms	50 spaces leased at parking garage on site	n/a	1,800 sq. ft.	54 seating	n/a

HOTEL PARKING REQUIREMENTS

Portland, ME
Zoning Ord.

NPA

Parking

Prkng 101

ITE 3rd Edit
peak

1 sp per 4 guest Rm

1 per Rm

1 per Room +
1 per 4 Ees
+ Rstrnt/Bar/Bnqt.

1 per Room + .91 per Room
1 per 4 Ees Suites 1.1 per
1.2 & employees
Bus.Hotel .66 per rm



PORTLAND MAINE

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Public Services Department
Michael J. Bobinsky, Director

9 March 2010

Mr. Daniel Riley, P.E.,
Sebago Technics, Inc.,
P.O. Box 1339,
Westbrook, Maine 04098-1339

RE: The Capacity to Handle Anticipated Wastewater and Stormwater Flows, from a Proposed Hotel, Restaurant, and Residential Units Development, at 207-209 Fore Street.

Dear Mr. Riley:

Corrected Copy

This corrected letter supercedes the letter of March 8, 2010. The existing thirty-inch diameter, vitrified clay, sanitary sewer pipe, located in Franklin Street, has adequate capacity to **transport**, while The Portland Water District sewage treatment facility, located off Marginal Way, has adequate capacity to **treat**, the total anticipated wastewater flows of **(117,940) GPD**, from this proposed development project.

Anticipated Wastewater Flows from

The Proposed Restaurant:

158 Proposed Three-Meal-Restaurant Seats @ 45 GPD/Seat	=	7,110 GPD
22 Proposed Lounge-Bar Seats @ 20 GPD/Seat	=	440 GPD
20 Proposed Employees @ 15 GPD/Employee	=	300 GPD

The Proposed Residential Units:

10 Proposed Two-Bedroom Units @ 180 GPD/Unit	=	1,800 GPD
2 Proposed One-Bedroom Units @ 120 GPD/Unit	=	240 GPD

The Proposed Hotel:

34 Proposed Single Bed Hotel Rooms @ 100 GPD/Hotel Room	=	3,400 GPD
36 Proposed Single Bed Hotel Rooms w/ Pullout Sofa @ 200 GPD/Hotel Room	=	7,200 GPD
52 Proposed Double Bed Hotel Rooms @ 200 GPD/Hotel Room	=	10,400 GPD
15 Proposed Employees @ 15 GPD/Employee	=	225 GPD

Total Design Wastewater Flow, for Proposed Hotel, etc., Project:	=	31,115 GPD
Less Wastewater Flow, from Previous Owners (Jordan Meats):	=	149,055 GPD

Total Net Wastewater Flows for this Project	=	(117,940) GPD
--	---	----------------------

The City combined sewer overflow (C.S.O.) abatement consent agreement, with the U.S.E.P.A. and with the Maine D.E.P., requires C.S.O. abatement, as well as storm water mitigation, from all projects, in order to offset any increase in sanitary flows.

This project proposes to relocate and separate the existing combined sewer traversing the site, and

Att. 3.2

separate that portion of the Fore Street combined sewer, from the point where the site-separated sanitary and storm lines enter Fore Street, to their respective connections, in the Franklin Arterial. The proposed storm drain, in Fore Street, will connect to a forty-eight inch combined sewer overflow drain, in the Franklin Arterial, which eventually drains to Casco Bay. The proposed storm drain will separate all storm drainage, from the eastern half of the project site, and all of Fore Street, between India Street and the Franklin Arterial.

<u>Anticipated Existing & Developed Two Year Stormwater Flows from the:</u>		
	<u>Existing</u>	<u>Developed</u>
30 Inch Combined Franklin Arterial Sewer.	29.7	24.8
48 Inch Combined Franklin Arterial Sewer Overflow.	28.3	28.8

In a two-year design storm, of three inches, in twenty-four hours, the peak rate runoff, to the thirty-inch diameter combined sewer outlet, located in the northbound lanes, of the Franklin Arterial, at the intersection of Franklin, at Fore, is reduced from twenty-nine point seven cubic feet per second, in predevelopment conditions, to twenty-four point eight c.f.s., post development. This decrease in runoff is the result of the proposed storm drainage separation that removes over six acres from the combined sewer system.

In that same two-year design storm, the predevelopment to post development peak rate runoff, shows increases, from twenty-eight point three c.f.s., to twenty-eight point eight c.f.s., for the existing forty-eight inch combined sewer overflow drain outlet, designed to convey storm overflows out of the combined sewer system.

The existing forty-eight-inch diameter, reinforced concrete, combined sewer overflow pipe, located in Franklin Street, has adequate capacity to **transport** storm water flows of **twenty-eight point eight cubic feet per second**, from this project.

Moreover, redevelopment of the site will reduce impervious cover by approximately 20,000 square feet.

If The City can be of further assistance, please call 874-8832.

Sincerely,
CITY OF PORTLAND



Frank J Brancely, B.A., M.A.
Senior Engineering Technician

FJB

- CC: Penny Littell, Director, Department of Planning, and Urban Development, City of Portland
 Barbara Barhydt, Development Review Services Manager, Department of Planning, and Urban Development, City of Portland
 William Needelman, Senior Planner, Planning Division, Department of Planning, and Urban Development, City of Portland
 David Margolis-Pineo, Deputy City Engineer, City of Portland
 Michael Farmer, P.E., Project Engineer, City of Portland
 Bradley A. Roland, P.E., Environmental Projects Engineer, City of Portland
 Stephen K. Harris, Assistant Engineer, City of Portland
 John Emerson, Wastewater Coordinator, City of Portland
 Jane Ward, Administrative Assistant, City of Portland
 Desk file

February 24, 2010

April 7, 2010

From: Deputy City Engineer
 David Margolis-Pineo

To: Barbara Barhydt
 Bill Needleman

Re: Old Port Hospitality LLC
 Former Jordan Meat Site

The following are responses to the applicant's comments with the remainder being final comments from the Dept. of Public Services

1. None of the streets around the Jordan Meat site are under a moratorium.
No response necessary.
2. Any proposed sewer laterals to be abandoned from this site must follow City requirements for permitting and sealing. Any manholes associated located on laterals to be abandoned shall be removed and all associated pipes sealed.
The applicant has responded and all required "Seal Drain Permits" will be obtained. The applicant shall coordinate with John Emerson of this department (207)-318-0239 with any questions or inspections concerning the sewer system.
3. There is no mention made as to how the existing 30" sewer which passes through the lot to Franklin Street will be abandoned. This issue will need to be coordinated with John Emerson (874-8468) of this department.
Addressed by the Applicant.
4. Since the City has no interest to enter the proposed 30' sanitary and stormwater easement area for any future system maintenance, and due to the proposed 12' over head canopy clearance, it is requested that the applicant use ductile iron pipe from the manholes in Middle St to the manholes in Fore St for both sewer lines.
This issue has been discussed and addressed by the Applicant.
5. All catch basins shall have three foot sumps, not just the catch basins within the city street right of ways.
The catchbasin detail has been revised to show three foot sumps.
6. All manholes with external drop connections shall meet TR-16 design and city detail standards.
This issue has been discussed and addressed by the Applicant.
7. Applicant may use Type "B" gravels in lieu of Type "A" gravels.
This issue has been discussed and addressed by the Applicant.
8. We will continue to review plan details for city standards.
No response necessary.
9. The Following are comments relating to the site survey. Any questions should be directed to Bill Clark of this department, 874-8847

a. The incorrect vertical datum of NAVD88 was used, and the City of Portland Datum was not used. This is significant introducing and vertical difference of 0.7 feet, and can lead to serious problems.

B. The GRID bearing of India Street between Middle St and Fore St is 00 degrees 11' 37" different from the bearing used by the City of Portland for Ocean Gateway, and as used by Owen Haskell Inc for the Ocean Gateway Subdivision and Ocean Gateway Garage projects. This amounts to a horizontal difference of approximately 0.62 feet in the 182.01 feet along the southwesterly line of India Street; this may not amount to much for a parking lot, but may be of consequence for a building in the future. Retracing the street lines on the Peninsula can be difficult, and it did take a good deal of effort to retrace the line of India Street from Commercial Street to Congress Street. I would be more than willing to provide a copy of our survey.

C. The monument at the easterly corner of India Street and Middle Street is incorrectly shown. That monument is what we call an "M" monument where the back corner of the monument is the street line corner, and not the middle of the monument.

D. The monument at the northerly corner of India Street and Middle Street should not be used, as it was disturbed by City crews in the 1990s.

E. Proposed 3 foot offset Monuments will be requested on the subdivision plan at the following locations.

Corner of India St and Middle St

Corner of Middle St and Franklin St Art

Corner of Fore St and Franklin St Art

Fore St PC 123.41' southwesterly of 3 foot offset monument near India St.

The following are additional comments by Bill Clark:

A few things to note regarding the Opechee Construction Corporation response letter to William Needelman dated March 5, 2010.

Vertical Datum

Sebago Technics used the correct datum on the Village Cafe Project, but not on this one. I would expect that they know the correct datum to use by now as they have been an engineering consultant for the City of Portland.

Sebago Technics used the incorrect datum on the Autumn Glen Subdivision, and had to correct all the elevations on the As-Built Plans prior to us accepting the streets.

In the interest of making this project move ahead I would make it (as a condition for accepting the relocated sewer and occupancy permit) that the final As-Built Plans be submitted with all elevations on the City Datum, and not to just have a note stating the difference. We will require City Datum elevations on the As-Built Plans because in the future someone could make an expensive error if they used the information from the As-Built plan without seeing a note stating that the elevations are on the wrong datum.

Plan of Streets

It is true that the DPW Survey plan I sent on March 2, 2010 to Sebago Technics for India Street, Fore Street, Middle Street, and Hancock Street is not on file at the Cumberland County Registry of Deeds.

However, I sent the same plan (without the 2008 title block) to Sebago Technics on August 30, 2005, and Sebago Technics used the DPW Survey plan for the definition of Middle Street and Hancock Street on the Village Cafe Project.

The bearings from the DPW Survey plan were used on the Ocean Gateway were also used by Owen Haskell for the Shipyard Brewery Subdivision and Ocean Gateway Garage project.

At this point in time I do not see the line of India Street as a large stumbling block or anything to hold up the project as only parking is proposed along India Street.

Proposed Survey Monuments

I agree that we will not be able to set the monuments as proposed due to conflicts with electrical duct banks in the sidewalks.

Please contact me if you need more information.

Bill Clark

- 10. Stormwater treatment is (not) technically required for projects such as this. However, the applicant is encouraged to consider treatment options such as tree or shrub box filters which can be relocated if future changes to the site are desired. **The applicant has agreed to install measures to improve stormwater quality. The City is very appreciative of these efforts.**
- 11. The City is reviewing and may ask the applicant to extend the sidewalk on Middle St to the property line. **To be honest, I question the intent of this statement and I'm not sure where it came from. The applicant intends to reinstall the granite curbing and install new bituminous sidewalks along their frontage on Middle St. See following comment.**
- 12. Instead of installing new granite curbing on Middle Street, for environmental reasons, no manufacturing or transporting of new curbing, we would like the

applicant to consider re-installing the existing curbing. Either way it is understood that the applicant would like to keep the material of this walk bituminous to accommodate future expansion. Since the existing sidewalk is not in great condition and the curb line is being modified, we would ask that the applicant re-pave the sidewalk. And since the sidewalk material policy in this section of the city is for brick sidewalks, the applicant is requested to post a two year Performance Guarantee to cover the installation of a brick walk in the likelihood the next phase of the development does not materialize.

The application has agreed to reuse the granite curbing but has not agreed to post a Performance Guarantee if after two years the next phase of the development does not materialize. I would qualify that the two year period may be extended with mutual agreement.

13. The applicant is request to submit the anticipated quantity of wastewater which will be added to the city's sewer system.

This has been done.

14. This department is still reviewing lighting standards for this project, i.e. light pole spacing and power feed requirements with CMP.

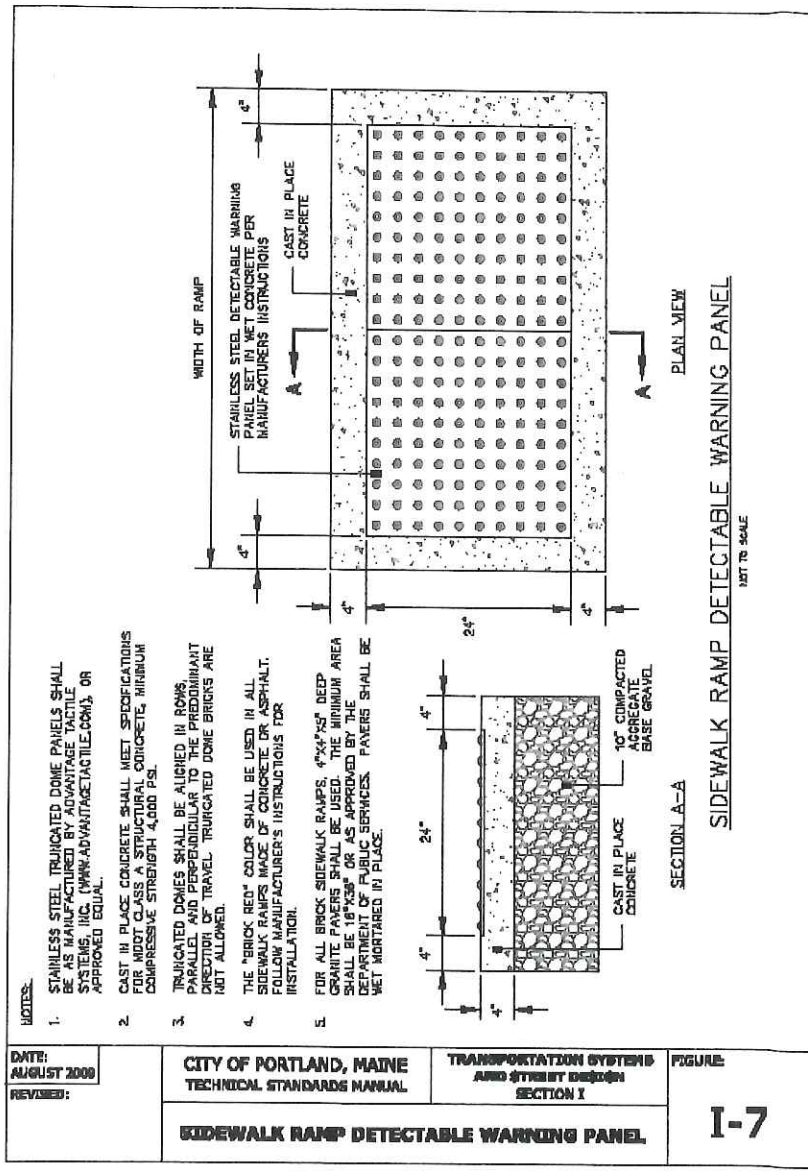
No response necessary.

15. The sidewalk ramp tactile warning strips shall be as follows and colored Tan.

Issue addressed.

16. I did not see any provisions for bicycle parking. Did I miss it?

Issue addressed.



Additional Comments:

17. The applicant initially proposed installing a brick driveway apron on their drive cut off Fore St. City guidelines require paved aprons in this area. After additional review and realizing that the applicant is installing a brick drive to site, if the applicant still wishes, the City is agreeable to waving the bituminous drive apron for this project. This may be done as a deminius change after project approval.
18. Per City requirements, the applicant is required to submit an Annual Inspection and Maintenance Plan for the stormwater treatment units.
19. The applicant is asked to install parking meter posts along the property line east of the drive cut on Fore St. and along the curb line to the west of the

drive cut. This also may be done as a deminius change ~~a~~fter project approval.

20. The applicant shall be responsible to assure that no ponding of street drainage results from building the cross walk bump out on Middle St. at Hampshire St.
21. The applicant is reminded that the occupancy of the sidewalk area has a fee of \$10/day and occupancy of each street parking spot has a fee of \$10/day.

This department has no further comments.

project. Based upon parking contribution rates being considered (the lower end is \$5,000 per space), the applicant should contribute \$50,000.00.

- I would note that the City supports the plan for on-street parking on Fore Street. We currently are assessing appropriate parking regulations (e.g. time limit or meters) and will provide a recommendation in the future. I would ask the applicant provide an opinion of what is preferred for parking on Fore Street. Lastly, I would note that parking regulation changes will likely require City Council action and the applicant will be required to provide the necessary information in support of the Council review (with support from City staff).

Status: Based upon feedback from the City Parking Manager, parking meters shall be installed on Fore Street. The applicant will be responsible for installing the meter posts.

New Traffic Comments

- *On-street parking stalls on Fore Street should not include pavement marking lines.*
- *The pavement markings delineating the radii at the entrance should be removed.*
- *I would suggest that the curb line on Franklin Street between Fore Street and Middle Street be adjusted to reflect a consistent roadway width.*
- *The applicant should make a contribution of \$1,200.00 towards improvements at the India Street/Middle Street intersection.*
- *Based upon the results of the traffic study and need to retime the Franklin Street signal at Fore Street, the applicant shall be responsible for the development of a signal timing plan for Franklin Street that ensures optimal operations. It should be noted that coordination with other intersections may be necessary.*
- *On-street parking stalls on Fore Street should have a width of 8 feet. Accordingly, two 13-foot travel lanes with a center line shall be provided.*
- *A STOP sign shall be installed at the driveway on Fore Street.*
- *Crosswalks should have a minimum width of 8 feet.*

If you have any questions on the above, please contact me.

Best regards,

Thomas A. Errico, P.E.

TYLIN INTERNATIONAL

12 Northbrook Drive
Building A, Suite One
Falmouth, ME 04105

207.347.4354 (Direct)
207.781.4721 (Main)
207.781.4753 (Fax)
207.400.0719 (Mobile)

Applicant's Submittal

271 101

Neighborhood Meeting Certification

I, Greg Kirsch, Member of Old Port Hospitality, LLC, hereby certify that a neighborhood meeting was held on Thursday March 18, 2010 at 6:00pm at the Public Market House, 28 Monument Square, Portland, Maine 04101.

I also certify that on Thursday March 11, 2010, invitations were mailed to all addresses on the mailing list provided by the Planning Division, which I understand included the property owners within 500 feet of the proposed development and the residents on the "interested parties" list.

Signed,



Greg Kirsch

3-22-2010

Date

Enclosed with this certification are:

1. Copy of the invitation sent
2. Sign-in sheet
3. Meeting Minutes

Old Port Hospitality, LLC
11 Corporate Drive
Belmont, NH 03220
603-527-9090

A.A.Z

COPY

March 11, 2010

RE: Neighborhood Meeting Invitation – Discussion of Development Plans for the former Jordan’s Meats Site

Dear Neighbor,

Please join us for a neighborhood meeting to discuss our plans for a hotel, restaurant and residential condominium development located at 207-209 Fore Street, formerly known as 38 India St.

Meeting Location: Public Market House - Upstairs, 28 Monument Square
Meeting Date: Thursday March 18, 2010
Meeting Time: 6:00PM

The City code requires that property owners within 500 feet of the proposed development and residents on an “interested parties list”, be invited to participate in a neighborhood meeting. A sign-in sheet will be circulated and minutes of the meeting will be taken. Both the sign-in sheet and minutes will be submitted to the Planning Board.

If you have any questions, please call Brent Berc at 603-527-9090.

Sincerely,



Brent Berc
Old Port Hospitality, LLC

Note:

Under Section 14-32(C) of the City Code of Ordinances, an applicant for a major development, subdivision of over five lots/units, or zone change is required to hold a neighborhood meeting at least seven days prior to the Planning Board public hearing on the proposal. Should you wish to offer additional comments on this proposed development, you may contact the Planning Division at 207-874-8721 or send written comments to the Department of Planning and Development, Planning Division 4th Floor, 389 Congress Street Portland, ME 04101 or by email to: bab@portlandmaine.gov.



AN. 3

COPY

Wednesday, March 10, 2010

RE: Former Jordan's Meats Site

Dear Neighbor,

The City Inspection Department requires that adjoining property owners be notified that demolition will be taking place. Please note that the demolition of the Jordan's Meats buildings is scheduled to begin on or about April 1st, 2010.

If you have any questions, please feel free to give me a call.

Sincerely,

A handwritten signature in dark ink, appearing to read "D. Adams", written over a horizontal line.

Dana Adams
2010.03.10 15:21:52 -05'00'

Dana M. Adams
Executive Vice President

Sign-in Sheet

	Name	Address
1	Mary Ann Thompson	
2	Marni Maynard	Newbury Street
3		
4	SAMANTHA LINDA	16 Middle St
5	Bob & Nancy	
6	DEAN	DEAN'S SWEETS
7		
8		
9	Eddie Fitzmaurice	
10	Nike Zimmerman	210 Park St
11	G. Fitzmaurice	
12		
13	John J Napolitano	14716dm@uconn.net.org
14		
15		
16		
17		
18	DEAN@DEANSSWEETS.COM	
19		
20		
21	Chris Tucker	LIUNA
22		
23		
24	John Evans	ironw496@myfairpoint.net
25		
26		
27		
28		
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Neighborhood Meeting Minutes
Site Plan and Subdivision Application for Hotel, Restaurant & Residences
207-209 Fore Street (former Jordan's Meats site)

Date: March 18, 2010

Time: 6:00pm

Location: Public Market House, 28 Monument Square, Portland, ME

Presenters: Greg Kirsch of Old Port Hospitality, LLC; Brent Berc of Opechee Construction Corporation; approximately 20 neighbors (see sign up sheet, only 15 signed in)

A general overview was given by Greg Kirsch to open up the meeting. A rendering from Franklin Street and Fore Street was displayed and a description of the site layout and subdivision was given. Lot 1 - hotel, restaurant and residences, lot 2 - surface parking for now but a good site for office use with structure parking

- Neighbors raised concerns regarding the orientation of the hotel, specifically that it was not facing Middle Street and pulled attention and traffic away from the existing businesses on Middle Street. This disappointment with the orientation of the hotel was expressed by a number of the neighbors, all of whom owned or operated business on Middle Street. Greg Kirsch informed the neighbors that the orientation of the hotel and restaurant "front door" was determined by a number of factors, primarily the operator's best business judgment as to which orientation would generate the most patronage.
 - Neighbors commented that Middle Street had a significant amount of businesses and foot traffic that the hotel could contribute to if it were facing Middle Street. Greg Kirsch informed the neighbors that the attractiveness of the Port, the ships, shops and restaurants on Fore Street and near the water was a large component of the feasibility of this project.
 - One neighbor suggested that everyone keep in mind the developments on the East End that are in various stages, which might bring more attention in the future. The neighbor suggested that the Owners not focus solely on Fore Street and Franklin Street as feasible frontages.
 - One neighbor asked why we don't simply put the hotel directly on the Jordan's building foundation rather than on the parking area where the hotel is proposed. Greg Kirsch explained that by the time an appropriate demolition and environmental remediation was performed, the entire site would be essentially one flat area and there would be no advantage from a construction standpoint or cost-saving to locating on the India Street side as opposed to the Franklin Street side.

Discussion shifted to the future development of proposed Lot 2. Questions were raised regarding what the development might look like and how Lot 2 might be laid out. Greg Kirsch explained that the design of that site has been discussed internally, but nothing has been finalized nor submitted to the City, and that the only component of the current application on Lot 2 is the proposed surface parking lot as shown on the plans.

- Neighbors expressed interest in seeing more attention given to Middle Street in future phases.
- Neighbors also pointed out that the sidewalk on Middle Street in front of the current Jordan's building is in disrepair and asked what was being planned for that sidewalk. Greg Kirsch informed the neighbors that phase 1 of the proposed project included a repaving of the sidewalk surface with asphalt, but that a performance guarantee for the installation of brick pavers on that sidewalk would be posted by the owners and eventually that side walk would be paved in brick.
 - The operator of Pepper Club inquired whether or not the brick would stop at Pepper Club. Kirsch responded that it would and reiterated that no brick would be laid on Middle Street until Lot 2 is developed.
- Neighbors inquired as to where the structured parking lot might be located on Lot 2. Greg Kirsch reiterated that the design of that site cannot be finalized until a use and user for Lot 2 is determined.

A neighbor asked when demolition of the site would begin. Kirsch responded that on or about April 1 the removal of special materials (asbestos, etc.) would begin and that might take a couple of weeks and then the full-scale demolition would start.

A representative from the Laborer's Union commented that the unions could not bid competitively because of the high cost of providing health care to their members and asked that the developer/contractor consider that. Extended discussion of union issues ensued. A steel worker also commented on his desire to work on the job and informed Kirsch that he had worked on recent projects near the site. He expressed his interest in seeing locals work on the site, instead of out of state sub-contractors. Kirsch informed the Union representative and the steel worker that all bids would be considered fairly.

Greg Kirsch provided a presentation of the architectural elevations of the site and discussion ensued regarding Sebago Brewery's space and parking arrangement.

- Neighbors inquired as to whether or not Sebago Brewery would have parking on the site. Greg Kirsch informed the neighbors that Sebago would not have parking on the site, just as they now do not have or rely on dedicated parking in their current location.
 - A neighbor commented (apparently in jest) that one parking space in the surface parking lot should be given to everyone property owner on Middle Street.

- A neighbor commented that the site would be grossly over-parked (apparently meaning that there was not enough new parking being created to accommodate the demand for parking that would be generated).
- A neighbor asked whether the surface parking lot would be flat. Kirsch responded that it would not be perfectly flat and that few are for drainage reasons.
 - Additional questions were raised regarding the change in grade of the site and parking lot from Fore Street to Middle Street. Kirsch replied that the change in grade from Fore to Middle Street was approximately 9-10 feet; the change in grade of the parking lot is approximately 5-8 feet.
- A neighbor asked whether there was a landscaping plan for the site. Brent Berc of Opechee Construction laid out a landscaping plan for all to view, and a number of interested persons were observed reviewing the plan at the back of the room.
- A neighbor asked if snow would be piled up on Middle Street during plowing. Kirsch replied that the 35 foot grassed/landscaped setbacks around the paved area provided ample room for snow storage.
- A neighbor commented that the neighborhood was happy that the site was being developed but did not want Middle Street to be treated like a back alley.
- Neighbors inquired whether or not underground parking had been contemplated for the site. Greg Kirsch informed the neighbors that the site plan did not contemplate digging down below grade to provide parking, but that the grade change from Middle to Fore effectively would make a lower parking deck appear to be below-grade as viewed from Middle St. Kirsch also explained that future development of the site would likely include measures to make the structured parking areas inconspicuous from the street level, such as with retail or residential “wrapper” spaces at street level as encouraged by the City of Portland.
- Neighbors raised the question as to how long the surface parking lot would exist on Lot 2. Greg Kirsch informed the neighbors that the parking lot is zoning compliant and there is no legal restriction on the time that it is allowed to exist. Kirsch went on to state the we have tremendous economic incentive to use the valuable Lot 2 land for a higher and better use than surface parking, and that he was hopeful that a proposal for development of Lot 2 might follow the Lot 1 proposal relatively soon.
- A neighbor asked if the project was still in the design phase. Kirsch responded that it was in the sense that final plans are being submitted on March 23 with the intention of holding the public hearing on April 13.
- One neighbor of the site raised questions regarding the type of foundation that the site would be built on. The concern was apparently about the noise and vibration involved in driving traditional piles into the soil. Kirsch stated that the geotechnical aspects were not his area of expertise, but that he believed the site was upgradient of the fill areas (e.g., Commercial St.) that require piles and that the hotel would be built on a “geo-pier” foundation. Kirsch stated that Sebago Technics had designed the foundation and that they would be better suited to explain the foundation.

- Neighbors raised further concerns that the site would be over-parked. Kirsch informed the neighbors that the Ocean Gateway garage had a surplus of parking available a few hundred feet from the site and that the development would add metered parking spaces on Fore Street.
 - Neighbors responded that Ocean Gateway garage was not an adequate parking option in the summer and there is a need for more parking.
- A neighbor raised a question regarding future structured parking and whether or not there would be times during the day when the spaces might be made available to the public, perhaps during the evening when office users did not need it. Kirsch replied that the idea had potential, but would depend on the needs and requirements of the tenants for any building that might be developed on the site. A neighbor commented that such an arrangement would be a useful way to get cars off of the street.
- A neighbor asked if the hotel was totally set back from Middle Street or if it extended over the Hugo's/Pepperclub building. Further discussions of the site plan and lay out ensued. Kirsch showed an elevation of the hotel as seen from Middle St. with a scaled photo of the Hugo's/Pepperclub building superimposed. That image showed that the service areas of the hotel would be shielded from view and that much of the hotel façade was not visible from Middle Street.
- Neighbors raised questions as to where the construction workers would park and that the neighbors did not want construction workers' vehicles parking in spaces that customers and businesses would otherwise use. Kirsch committed to set up site rules that all construction workers would park on the site to the maximum extent feasibly possible, but that it might be difficult to enforce violations of the rules.
 - A neighbor suggested that construction workers be required to place stickers on their vehicles so neighbors could identify any construction workers' vehicles parking in public space.
- Neighbors asked whether Sebago Brewery would be willing to validate parking in the Ocean Gateway garage. Kirsch replied that the owners of Sebago Brewery would have to make that decision.

Greg Kirsch continued with a presentation of each of the building elevations and with a marker indicated the building foot print and the new pedestrian ways to be created from Middle to Fore Street and between the proposed hotel and the 80-90 Corps building.

- Neighbors inquired as to whether or not pedestrians could walk through the site from Middle to Fore Street. Kirsch replied that they could.
- Neighbors also inquired as to what the pedestrian way between the hotel and the 80-90 Corps building would look like. Kirsch provided an explanation of the walk way.
- A neighbor inquired as to how the pest problem (i.e., rats) in the Jordan's building would be addressed. Kirsch replied that he was not familiar with the demolition of a building that had a pest problem. Kirsch indicated that he would advise the demolition project team to be prepared for the potential of rat issues.

Business owners asked that lines of communication remain open between them and the owner, Kirsch agreed and asked that the business owners select a point person for all communication to go through. Email addresses were exchanged. Kirsch stated that he would work to set up some system for advising the Middle Street business neighbors in regard to upcoming construction phases, street closures, etc.

Brent Berc of Opechee Construction gave a presentation on the early stages of demolition of the project and the effects the demolition would have on pedestrian and vehicular traffic. The key dates / components of demolition are as follows:

- April 1, demolition begins. A temporary chain link fence will be put up around the perimeter of the site including the sidewalk adjacent to the site on Middle Street. Vehicular traffic access will not change in the earliest stage of demolition. Brent illustrated how pedestrian foot traffic on Middle Street and Fore Street would be altered.
 - Neighbors commented that Memorial Day weekend was not a good weekend for traffic to be disrupted.
 - Neighbors commented that a sidewalk on the North side of Fore street did not exist and they were pleased to see that a pedestrian way would be provided on Fore Street would have jersey barriers to separate it from vehicular traffic.
- May 1, separation of the sewer and drainage lines on Middle Street at the Hampshire Street intersection will begin and last approximately 2 weeks. At this time no thru traffic will be allowed on Middle Street between Franklin and Fore and on Hampshire Street near the intersection with Middle.
 - Neighbors questioned how customers and employees would get to their businesses during the temporary road closures. Brent responded by saying that vehicles and pedestrians would be allowed on Middle Street during this phase and that clear access to driveways and parking areas on Middle would be provided, but thru traffic would not be allowed.
 - Neighbors asked if the parking spaces on Middle Street would remain available during this phase. Brent responded that most of them would, however, any spaces near or over the area where excavation will occur to access the sewer and drainage lines would be temporarily unavailable. Brent illustrated where the excavation would occur on a set of demolition plans.
 - Neighbors inquired whether the temporary fence would extend up Franklin Street to the corner of Middle Street. Brent responded that the plan indicated that it would, but revisions could be made to the plan if necessary. The neighbors preferred that the temporary fence on Franklin Street stop at the start of the 80-90 corps building.
- June 1, sewer and drainage line separation continues on Fore Street, working west down Fore Street. Brent illustrated that during this phase of demolition Middle Street would have normal vehicular access, but vehicular traffic between India and Franklin Street would be temporarily re-routed in order to allow the installation of the sewer and drainage lines. Brent illustrated on demolition plans

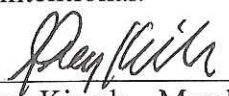
that the temporary detour from Fore Street would re-route cars thru Middle Street. Brent illustrated how pedestrian traffic on Fore Street would be re-routed minimally.

- June 15, sewer and drainage connection on Franklin Street. Brent illustrated that during this phase of demolition, excavation on Franklin Street in the intersection with Fore Street would require that east bound traffic on Fore would not be allowed to cross Franklin Street and continue on Fore, that traffic would be re-routed to Middle Street. Upon the completion of the work in the Franklin and Fore intersection, it is anticipated that all vehicular ways will return to normal, Brent estimated this to be before July 1.
- December 2010, completion of the brick sidewalk bordering Franklin street. All pedestrian ways surrounding the site are open to the public, with the exception the Fore Street pedestrian way which will remain a temporary way until the completion of the project, anticipated for Memorial Day weekend 2011.

There were no additional questions or comments from the neighbors.

The neighborhood meeting was adjourned at approximately 8:30 p.m.

The above are the best efforts of Greg Kirsch and Brent Berc to create a true and complete record of the meeting. If any comments or suggestions are missing it is unintentional.



Greg Kirsch March 20, 2010

March 23, 2010

Mr. William Needelman, Senior Planner
Planning Division
389 Congress Street, 4th floor
Portland, ME 04101

Re: Response Letter for the March 9th Planning Board Workshop
Project: Hotel, Restaurant, & Portside Residences
Address: 207 & 209 Fore Street (formerly 38 India Street)
Applicant: Old Port Hospitality, LLC

Dear Bill,

Please find below Old Port Hospitality, LLC responses to the review comments received to date for the above mentioned project. The responses are brought to attention with bold italic text.

► Bill Needelman, Senior Planner, City of Portland:

1. Right Title Interest: From your deed, "Terms and provisions" of the MDEP Protection Orders, Book 19244, Page 271. We need copy.

The MDEP Protection Orders were submitted to Bill Needelman via email on March 3rd.

2. 33' driveway with a 10' curb radius.

The applicant will withdraw the waiver request for the driveway width. The driveway has been revised to provide a 24 foot width with 10' curb radii. Also, the line striping will be revised to delineate 20' turning radii.

3. Parking management plan.

A parking management plan has been submitted to planning and review comments are pending.

4. Building lighting.

The lighting designs for the building façade illumination are forth coming.

5. Provide the Planning Department with materials sample board.

Material samples were provided to the Planning Department on March 4th.

► Woodward & Curran:

1. As currently shown, the lawn areas surrounding the parking lot are sloped towards the parking lot. We ask that the applicant consider sloping the parking lot toward the grassed area, resulting in some treatment of the stormwater runoff from the pavement.

The revised grading and utility plan eliminates the sidewalk adjacent to the valet parking spaces and runoff will now drain into a swale with tree box filters.

2. The stormwater report references the decision to use 3' sumps for stormwater quality treatment in the parking lots. The catch basin detail should be modified to say that 3' sumps are required for all catch basins, not just those in the City right-of-way as the detail currently states.

The catch basin details have been revised as requested.

3. The project will be separating and rerouting the combined sewer that currently crosses the site. The plans currently do not show how the existing combined sewer will be demolished or abandoned. The existing pipe is shown ending at the sidewalk on the utility plan. More detail should be provided as to how the existing combined sewer will be abandoned.

A Site and Utility Demolition plan has been added to the plan set. The plan notes now indicate that the contractor is required to secure Pipe Sealing permits from Public Services Department and shall plug, remove or abandon storm drains and sanitary sewers in accordance with City standards. All abandoned manholes will be removed. The 30" sewer will be removed within the site limits. We have met with public services staff to confirm pipe sealing and abandonment requirements within the right of way. Staff has indicated that they may prefer that some pipes be abandoned in place to minimize excavation in Franklin Arterial.

► T.Y. Lin International:

1. The driveway is proposed to be 33 feet wide. Based upon anticipated traffic volumes I find this width to be excessive. Additionally, the radii seem large. One possibility would be to provide a 24-foot driveway in the public right-of-way and then widen to 33 feet on-site to accommodate valet parking needs. In any event the applicant should provide an auto-turn analysis that supports the design (I would note that we normally don't overdesign for infrequent large vehicle turning movements).

The applicant is withdrawing the waiver request for the driveway width. The driveway has been revised to provide a 24 foot width with 10' curb radii. Also, the line striping has been revised to delineate 20' turning radii.

2. The proposed crosswalks at the Franklin Street/Fore Street and India Street/Fore Street intersections should be two parallel lines.

The crosswalk striping has been revised as requested.

3. The applicant should assess the design of the sidewalk ramp at the northwest corner of the India Street/Fore Street intersection such that separate ramps are provided for crossing India Street north of the intersection and Fore Street west of the intersection.

It was discussed at the Traffic Permit Scoping Meeting on March 3rd that we will review and revise the ramp configuration to the extent possible. Due to the unusual geometry of this intersection, and the location of the existing ramps on the east side of India Street and south side of Fore Street creating completely separate ramps that cross perpendicular to the curb lines are not feasible. A revised ramp, with separated detectable warning strips has been provided.

4. A waiver from the City's Technical Standard will be required for the size of the parking spaces (9'x18' spaces are proposed). I support this waiver.

The applicant is still seeking relief from the City's Technical Standards with this matter.

5. A waiver from the City's Technical Standard for provision of compact parking is required. I support this waiver.

The applicant is still seeking relief from the City's Technical Standards with this matter.

6. The applicant will be required to prepare a preliminary Maintenance of Traffic plan that addresses any anticipated closures of either public streets or sidewalks.

Traffic control plans are included in this revised submittal.

7. The provision of a crosswalk on Middle Street at Hampshire Street is to be reviewed by the City's Crosswalk Committee on March 4th. Comments will be provided soon after.

Comments were received and revisions were made as requested.

8. The applicant should provide a summary on how truck deliveries will take place, including types of vehicles, locations where loadings will occur, and whether the 12-foot clearance allows for large vehicle movements entering and exiting the site.

The hotel valet area has been revised to provide a 14'-foot clearance in the driveway aisle. An Autoturn Analysis Plan has been submitted to illustrate intended delivery movements and where the loading will occur. It is anticipated that the delivery schedule will be as follows:

<i>Hotel:</i>	<i>Continental Breakfast –</i>	<i>1 to 2 deliveries per week (SU Box Truck)</i>
	<i>Hotel Supplies –</i>	<i>1 to 2 deliveries per month (WB-40 Truck)</i>
	<i>Vending –</i>	<i>1 delivery per week (van)</i>
<i>Restaurant:</i>	<i>Monday –</i>	<i>4 deliveries (1 van & 3 rest SU or WB-40 trucks)</i>
	<i>Tues thru Sat –</i>	<i>1 to 3 deliveries (1 van & rest SU or WB-40 trucks)</i>
	<i>Sunday -</i>	<i>No deliveries</i>

9. I will provide an opinion of parking supply adequacy upon receipt and review of the parking analysis to be provided by the applicant.

A parking management plan has been submitted.

10. I would note that the City supports the plan for on-street parking on Fore Street. We currently are assessing appropriate parking regulations (e.g. time limit or meters) and will provide a recommendation in the future. I would ask the applicant provide an opinion of what is preferred for parking on Fore Street. Lastly, I would note that parking regulation changes will likely require City Council action and the applicant will be required to provide the necessary information in support of the Council review (with support from City staff).

The applicant's will install the City's recommended choice.

► David Margolis-Pineo, Deputy City Engineer, City of Portland:

1. None of the streets around the Jordan Meat site are under a moratorium.

No response necessary.



2. Any proposed sewer laterals to be abandoned from this site must follow City requirements for permitting and sealing. Any manholes associated located on laterals to be abandoned shall be removed and all associated pipes sealed.

A Site and Utility Demolition plan will be added to the plan set. The plan notes will indicate that the contractor is required to secure Pipe Sealing permits from Public Services Department and shall plug, remove or abandon storm drains and sanitary sewers in accordance with City standards. The notes on the demolition plan are based on the City's latest draft Technical standard. All abandoned manholes will be removed. The 30" sewer will be removed within the site limits. We will meet with public services staff to confirm pipe sealing and abandonment requirements within the right of way. Staff has indicated that they may prefer that some pipes be abandoned in place to minimize excavation in Franklin Arterial.

3. There is no mention made as to how the existing 30" sewer which passes through the lot to Franklin Street will be abandoned. This issue will need to be coordinated with John Emerson (874-8468) of this department.

A Site and Utility Demolition plan will be added to the plan set. The plan notes indicate that the contractor is required to secure Pipe Sealing permits from Public Services Department. The 30' combined sewer will be removed within the site limits. The drain will be sealed at the manhole in Franklin arterial, backfilled with concrete and abandoned in place to be in accordance with City standards.

4. Since the City has no interest to enter the proposed 30' sanitary and stormwater easement area for any future system maintenance, and due to the proposed 12' over head canopy clearance, it is requested that the applicant use ductile iron pipe from the manholes in Middle St to the manholes in Fore St for both sewer lines. ***We have discussed this Public Services And have agreed that all new sanitary and storm drains will be SDR-35 PVC sewer pipe.***

As we discussed with the Planning and Public Services Staff, the plans have been revised to alter one segment of the proposed 30" storm drain in Fore Street. Due to utility conflicts, storm drain SD-6 has been changed from a single 30" pipe to (2) parallel 24" pipes. Supporting calculations for this revision are included in the Stormwater Management Plan Addendum attached to this letter.

5. All catch basins shall have three foot sumps, not just the catch basins within the city street right of ways. ***The catch basin details have been revised as requested.***

6. All manholes with external drop connections shall meet TR-16 design and city detail standards. ***We have revised the drop manhole detail as requested. Based on our discussion with Mr. Emerson, drops of more than 2' within a sewer manhole require a drop structure. There is a single existing 8" sewer connection at SMH-1 that will require a drop.***

7. Applicant may use Type "B" gravels in lieu of Type "A" gravels. ***The materials have been revised as recommended.***

8. We will continue to review plan details for city standards. ***No response necessary.***

9. The Following are comments relating to the site survey. Any questions should be directed to Bill Clark of this department, 874-8847

The incorrect vertical datum of NAVD88 was used, and the City of Portland Datum was not used. This is significant introducing and vertical difference of 0.7 feet, and can lead to serious problems.

The NAVD 88 datum for this site was established as part of an ALTA survey completed in 2005 and was maintained throughout the design and permitting of the previously approved projects on this site. As part of the original survey Sebago Technics performed a level loop to a U.S. Geodetic survey monument for which the City has an established elevation. Note 6 on the ALTA survey and existing conditions plans documents the datum elevations as follows:

THE BEARINGS, COORDINATES, AND ELEVATIONS SHOWN HEREON ARE BASED UPON THE MAINE STATE PLANE COORDINATE GRID, WEST ZONE 1802 ON NAD83 AND NAVD 88. THE CITY DATUM WAS VERIFIED UTILIZING U.S. COAST AND GEODETIC SURVEY BENCHMARK, BEING A DRILL HOLE LOCATED IN THE GRANITE STEPS OF THE CUSTOM HOUSE ON COMMERCIAL STREET. THE CITY ELEVATION IS 14.072', THE NGVD 29 ELEVATION IS 14.042', AND NAVD88 IS 13.30'

As we discussed with planning staff, as significant number of design and legal documents have been prepared based on the survey work completed to date. We have addressed the concern regarding the datum by adding notes the each plan where elevation are referenced that will clearly indicates that design plans are based on the NAVD 88 datum and provides a formula to convert from NAVD88 to the City Datum.. In addition, any as-built plans for public infrastructure will note the NAVD88 and City datum elevations.

B. The GRID bearing of India Street between Middle St and Fore St is 00 degrees 11' 37" different from the bearing used by the City of Portland for Ocean Gateway, and as used by Owen Haskell Inc for the Ocean Gateway Subdivision and Ocean Gateway Garage projects. This amounts to a horizontal difference of approximately 0.62 feet in the 182.01 feet along the southwesterly line of India Street; this may not amount to much for a parking lot, but may be of consequence for a building in the future. Retracing the street lines on the Peninsula can be difficult, and it did take a good deal of effort to retrace the line of India Street from Commercial Street to Congress Street. I would be more than willing to provide a copy of our survey.

We have received a copy of the City survey completed in 2008. In it's determination of the easterly right of India Street Right of Way the City survey created an angle point on the easterly side of India Street that is not reflected in any of the street acceptance documents for India Street or the deed references for our project site. The City plan includes a note stating:

"DURING THE COURSE OF THIS SURVEY IS WAS DISCOVERED THAT AN ANGLE EXISTS IN THE LINE OF INDIA STREET. THE ACCEPTANCE OF INDIA STREET DOES NOT CALL FOR AN ANGLE POINT. HOWEVER, OVER THE YEARS BETWEEN APPROXIMATELY 1850 TO 2005, AN ANGLE POINT HAS DEVELOPED. THIS MAY BE DUE TO THE PAST PRACTICE OF SURVEYING "BLOCK TO BLOCK" AND HOLDING CITY OF PORTLAND GRANITE SURVEY MONUMENTS THAT WERE SET TO DEFINE THE STREET LINES"

It is the introduction of this angle point that is causing the difference in bearings between the two plans. The City's plan has not yet been recorded and was therefore not identified in our recent survey research. It appears that the introduced angle point and bearing will move the India Street Right of Way northeasterly, away from the project site, potentially increasing the size of the parcel by approximately 76 square feet.

We are reviewing the implications of this potential change on previously recorded documents and the subdivision plans. We will discuss this further with Mr. Clark and the city to determine if the locus property boundary along India Street property line should be adjusted.

In the interim, we added the City Survey as an unrecorded plan reference on our survey and subdivision plan. If appropriate we will note the differences in bearings between the two surveys.

We have added the City Survey as a plan reference on our survey and subdivision plans. It is our opinion that until the City Council takes action to re-define the India Street right of way, there are no additional revisions required.

C. The monument at the easterly corner of India Street and Middle Street is incorrectly shown. That monument is what we call an "M" monument where the back corner of the monument is the street line corner, and not the middle of the monument.
We are aware that this is an "M" monument. The back (Micucci's) corner of the monument was located as part of our survey. The discrepancy noted is simply due to the graphic symbol used on the plan and the scale of the drawing. We have added a note to our plan documenting the monument location as follows: "10"X10" GRANITE "M" MONUMENT, EASTERLY CORNER OF MONUMENT LOCATED"

D. The monument at the northerly corner of India Street and Middle Street should not be used, as it was disturbed by City crews in the 1990s.
The monument discussed was determined in our survey to be off the right of way corner by approximately 1.25 feet. Although several pieces of monumentation were located during the survey, the monument was not held. Monuments that were held to establish the locus property are noted as "held" on the plan.

E. Proposed 3 foot offset Monuments will be requested on the subdivision plan at the following locations.

- Corner of India St and Middle St
- Corner of Middle St and Franklin St Art
- Corner of Fore St and Franklin St Art
- Fore St PC 123.41' southwesterly of 3 foot offset monument near India St.

We have discussed the placement of monuments with Mr. Clark and have proposed the following:

- *A monument is not proposed at the corner of India Street and Middle Street due to a conflict with an existing underground electrical duct bank.*
 - *A monument is not proposed at the corner of Middle Street and Franklin Arterial, This location is not part of the locus site's boundary and there is a conflict with an existing underground electrical manhole and duct bank.*
 - *A monument is not proposed at the corner of Fore Street and Franklin Street Arterial due to a conflict with an existing underground electrical manhole and ductbank.*
 - *The subdivision plan will be revised to include a 3-ft offset monument on Fore Street at the PC 123.41 southwesterly of the monument near India Street as requested.*
- Rebar corner pins will be installed where the proposed 30' utility easement meets Middle Street. The intersections of the easement at Fore Street fall within the proposed brick sidewalk/driveway apron and immediately adjacent to a granite curb. As such, pins are not proposed at Fore Street.*

BB,)

10. Stormwater treatment is technically not required for projects such as this. However, the applicant is encouraged to consider treatment options such as tree or shrub box filters which can be relocated if future changes to the site are desired.

The revised site, grading and utility plans eliminate the sidewalk adjacent to the valet parking spaces. Runoff will now drain into a swale with tree box filters.

11. The City is reviewing and may ask the applicant to extend the sidewalk on Middle St to the property line. *This request has not been required of the applicant to date.*

12. Instead of installing new granite curbing on Middle Street, for environmental reasons, no manufacturing or transporting of new curbing, we would like the applicant to consider re-installing the existing curbing. Either way it is understood that the applicant would like to keep the material of this walk bituminous to accommodate future expansion. Since the existing sidewalk is not in great condition and the curb line is being modified, we would ask that the applicant re-pave the sidewalk. And since the sidewalk material policy in this section of the city is for brick sidewalks, the applicant is requested to post a two year Performance Guarantee to cover the installation of a brick walk in the likelihood the next phase of the development does not materialize.

The plans have been revised as requested. The Site and Utility Demolition plan will indicate the limits of curbing to be removed and salvaged for re-installation. The site plan indicates that salvaged curbing is to be reinstalled.

13. The applicant is request to submit the anticipated quantity of wastewater which will be added to the city's sewer system.

A Wastewater Capacity Application has been submitted and approved.

14. This department is still reviewing lighting standards for this project, i.e. light pole spacing and power feed requirements with CMP.

No response necessary.

15. The sidewalk ramp tactile warning strips shall be as follows and colored Tan.

A tactile warning strip detail has been added.

► Marge Schmuckal, Zoning Administrator, City of Portland:

1. I believe that this project is probably meeting the maximum building height of 65 feet for this area of the City. However, I would like to confirm that supposition with more information from the applicant using the same methodology that I use with all applicants. I would like to know the grades around the outside of the proposed building so that I can average the grades. I would also like to know where the top of the structural beams will be located on the structure. The actual height of the structure can be determined from that information.

Building height calculations have been reviewed by Ms. Schmuckal.



2. I would also like to confirm where the property line is located at the rear of the proposed building where the stairs descend from the street. It was unclear to me.

The property line follows the southerly and easterly wall of the existing building to remain at the corner of Middle Street and Franklin Street Arterial. This building is the property of 80-90 Corps that hosts Hugo's Restaurant.

3. It is noted that no official loading bays are required under section 14-351. However, it may be useful to indicate how the rear dumpster area will be accessed and utilized by servicing vehicles. This area may also service deliveries, linen services and the like.

The hotel valet area has been revised to provide a 14'-foot clearance in the driveway aisle. An Autoturn Analysis Plan has been submitted to illustrate intended delivery movements and where the loading will occur. It is anticipated that the delivery schedule will be as follows:

<i>Hotel:</i>	<i>Continental Breakfast –</i>	<i>1 to 2 deliveries per week (SU Box Truck)</i>
	<i>Hotel Supplies –</i>	<i>1 to 2 deliveries per month (WB-40 Truck)</i>
	<i>Vending –</i>	<i>1 delivery per week (van)</i>
<i>Restaurant:</i>	<i>Monday –</i>	<i>4 deliveries (1 van & 3 rest SU or WB-40 trucks)</i>
	<i>Tues thru Sat –</i>	<i>1 to 3 deliveries (1 van & rest SU or WB-40 trucks)</i>
	<i>Sunday -</i>	<i>No deliveries.</i>

4. Separate permits from Inspection Services will be required for the demolition of existing structures and for signage.

Agreed.

► Jeff Tarling, City Arborist, City of Portland:

a) Street-trees - The project proposes to include planting various shade trees within the sidewalk / public way along Franklin Street & Fore Street. Placement of these trees should be coordinated with the on-street parking to ensure that car doors do not open into the planted tree. Also, the placement of parking meters is typically recommended to alternate with tree planting, post with 2 parking meters between car spaces, then tree well, then parking meter... Placing the tree well near the end of the spaces as proposed may work; just want to have this considered. Tree types are good as shown, tree grates & guards should be reviewed to make sure the finish, painted black or core-10 steel is preferred...

We have discussed the tree spacing with the City Arborist. The proposed tree spacing along Fore Street is coordinated with the revised parking stalls shown on the site plan. The proposed parking stall striping has been added to the landscape plan to clarify. The trees are located near the end of the parking stalls to minimize potential conflicts with car doors. The tree grates are clear of the parking stall striping to avoid conflicts with meters should they be installed.

We have revised the tree grate and tree guard details to clarify that tree guards are to be painted with one coat shop primer and two coats of low luster black enamel paint. Tree grates will be weathering (COR-TEN) steel.

b) Esplanade or sidewalk with trees on Franklin Street... "If" future on-street parking is considered for Franklin Street, then the proposed wide sidewalk with street trees with tree grates is good, if not, the use of a turf esplanade with trees should be considered. The proposed planter at the corner of Fore & Franklin should allow enough space for pedestrian travel & sidewalk snow plowing between the two sidewalk tip down ramps.
We have discussed the sidewalk/esplanade design with Staff and the Planning Board. To date, no revisions have been made.

c) Parking lot trees & landscape - Will the six street-trees w/ tree grates in the sidewalk proposed for Fore Street near the temporary parking lot be in the way of future development? An option to plant on the lawn area could be considered.
It is not anticipate that the street trees along Fore Street will be in the way of the future development.

Tree / landscape density - Recommend adding one more Birch tree to the proposed two birch tree groups spaced along Middle Street. The lawn area along India Street should have some buffering / tree planting. One suggestion would be to slightly berm or contour the lawn area, perhaps 1-2' to help buffer the parking lot and help prevent cut-through from the parking lot to the sidewalk area.
The plan has been revised to include an additional birch tree to the Middle Street plantings and additional birch trees in the lawn area adjacent to India Street. The lawn area surrounding the surface parking is proposed with a berm to help buffer the parking lot.

d) Hampshire Street access to Fore Street - The Middle Street gateway to the proposed development is well landscaped and should help with pedestrian circulation. Recommend reuse of the existing granite slabs in the old section of Hampshire Street / crosswalk in this small plaza design or on site if possible.
The plan has been revised to include salvaged granite slabs from the site into the design.

► Planning Staff, City of Portland:

Site Plan Standards 14-526 (a)(16) a.1.

General Comment: The elevation drawings are at a concept level of development and greater detail will be needed for final review prior to public hearing.

More detailed building elevations have been submitted.

It is unclear from the site plan and elevations whether the Fore and Franklin Street door to the restaurant is oriented to the corner or to Fore Street - Clarification requested.

The applicant has revised the building architecture as per the discussions with William Needelman and Alex Jagerman.

Site Plan Standards 14-526 (a)(16) a.1.(a)

Comment: Provide details and specifications on the storefront design and first floor windows including the transparency index and sample of the proposed glass. Visual permeability between interior and exterior spaces at the pedestrian level is indicated by this standard. Highly tinted or reflective glass at the pedestrian level is not recommended.

The applicant has revised the building architecture as per the discussions with William Needelman and Alex Jagerman.

Site Plan Standards 14-526 (a)(16) a.1.(b)

Comment: The prominence and frequency of pedestrian entrances along Fore Street is undermined by the lack of canopy and/or signage at the mid-block shared entrance (for the restaurant and hotel) and the location of the hotel lobby entrance at the porte-cochere. Consider adding a prominent pedestrian door to the Fore Street façade adjacent to the vehicle entrance. Also consider adjusting signage location from the Franklin/Fore intersection to the pedestrian entrance to the lobby.

The applicant has revised the building architecture as per the discussions with William Needelman and Alex Jagerman.

Site Plan Standards 14-526 (a)(16) a.1.(c)

The building is generally well fenestrated and does not show excessive blank walls in the pedestrian areas.

No response necessary

Site Plan Standards 14-526 (a)(16) a.1.(d)

The pedestrian access stairs from Middle Street are well positioned for use and are integrated with the site and building design. Consideration may be given to replacing the existing "cobra head" light fixture with a district street light pole and fixture to better integrate the stairs with the street. Design Note: The City Arborist suggests utilizing the historic granite plank and cobble from the existing alley in this area into the design.

The plans have been revised to include salvaged granite slabs from the site into the design. The applicant is still reviewing the possibility of removing the existing cobra head light to add a district street light. More information on this matter will be forthcoming.

Site Plan Standards 14-526 (a)(16) a. 2.

Not Applicable

No response necessary

Site Plan Standards 14-526 (a)(16) a. 3.

Applicable to Middle Street only for any later phase of development.

No response necessary

Site Plan Standards 14-526 (a)(16) a.4.(a)

Cross walk design is being addressed by Public Services. There has been discussion of bumping the Middle Street curbing out to shorten the crossing distance from Hampshire Street to the proposed pedestrian stairs.

The plans have been revised to incorporate a bump out into Middle Street.

Site Plan Standards 14-526 (a)(16) a.4.(b)

See City Arborist comments.

Provide planter details for drainage and irrigation that promote the viability of plantings and minimize impacts to the public sidewalk.

A planter detail has been added to the plans. The proposed development will be implementing some green practices into the design of the facility and in the interest of conserving water; irrigation will not be installed.

Site Plan Standards 14-526 (a)(16) a.4.(c)

The plans show appropriate street lighting styles and spacing (see comment above regarding the pedestrian stairs.) Building mounted lighting details have not yet been provided. Private site lighting of the parking area should be provided with cut-off fixtures.

Building mounted lighting details will be forthcoming. Cut-off fixtures will be provided for the private site lights.

Applicant should anticipate the an electrical distribution system with a metering box suitable for serving all fixtures to be owned by the City

We have identified a proposed electric panel & meter location on the plans.

Site Plan Standards 14-526 (a)(16) a. 4. (d)

Granite benches are proposed for Fore Street and a granite seat wall is proposed for the head of the Middle Street stairs.

No response necessary

Site Plan Standards 14-526 (a)(16) a. 4. (e)

The restaurant use is proposed to extend onto the Fore Street façade of the building near Franklin Street.

No response necessary

Site Plan Standards 14-526 (a)(16) b. 1.

General Comment: Context information is not provided. The development should be shown within its context by use of perspective drawings, photomontage, or other graphic means. Suggested views are from Franklin Arterial at Federal Street (Court House and Lincoln Park), Fore Street in front of Bangor Savings, Commercial and Franklin Street (from the Casco Bay Garage, and Fore Street and India Street (from the sewer pump station.)

The requested graphics to assist the Planning Staff and the Planning Board with their review of the project has been submitted.

Site Plan Standards 14-526 (a)(16) b.1.(a)

The street walls of Fore and Franklin are enforced by this development.

No response necessary.

Site Plan Standards 14-526 (a)(16) b. 1. (b)

NA

No response necessary.

Site Plan Standards 14-526 (a)(16) b. 1. (c)

See below:

No response necessary.

Site Plan Standards 14-526 (a)(16) b. 1. (d)

The building massing and composition does not provide distinct massing elements at the key corners at Franklin Street. Given the visibility of the building from Franklin, Fore (as the terminating view from the Old Port) and Commercial Street, these corners are important.

The applicant has revised the building architecture as per the discussions with William Needelman and Alex Jagerman.

The Middle and Franklin corner has no vertical differentiation from the main mass of the building and is un-fenestrated. The Fore and Franklin corner is punctuated by only a minor parapet rise at the center of the bay.

The applicant has revised the building architecture as per the discussions with William Needelman and Alex Jagerman.

The design's relationship to its prominent location and to other buildings in the downtown and Old Port will be strengthened by vertical differentiation and fenestration of the building corner at Middle Street and simplifying and enlarging the punctuation at Fore Street. If fenestration of the top story of the Middle Street corner interferes with proposed signage, an alternative location for signage should be considered on the body of the building.

The applicant has revised the building architecture as per the discussions with William Needelman and Alex Jagerman.

Site Plan Standards 14-526 (a)(16) b. 1. (e)

Addressed above

No response necessary.

Site Plan Standards 14-526 (a)(16) b. 1. (f)

No comment was made.

Site Plan Standards 14-526 (a)(16) b. 2. (a, b, c, & d)

Standards for set back appear to be met.

No response necessary.

Site Plan Standards 14-526 (a)(16) c.

Need details – none provided. Readily visible mechanical equipment requires screening

Details have been added to the revised application submittal.

Site Plan Standards 14-526 (a)(16) d.

NA

No response necessary.

Site Plan Standards 14-526 (a)(16) e.

Undue wind impacts are not anticipated.

Agreed.

Site Plan Standards 14-526 (a)(16) f.

Set backs from the adjacent building at Franklin and Middle will be increased.

Agreed.

Site Plan Standards 14-526 (a)(16) g.

NA

No response necessary.

The applicant and design team looks forward to working in close cooperation with the Planning Staff and the Planning Board to develop, permit, and ultimately construct this viable project.

Sincerely,

Opechee Construction Corporation



Barry Stowe

Project: Old Port Hospitality
Location: Portland, Maine
Date: April 5, 2010

Bartlett Design
942 Washington Street, Bath, ME
(T) 207-443-5447 (F) 207-443-5560

LIGHTING FIXTURE SCHEDULE

Lamp numbers refer to Osram Sylvania designations. Also approved are equal lamps manufactured by General Electric, and Philips. Voltage for exterior luminaires shall be verified with the project electrical engineer.



Type: S1

Description: Street light luminaire and pole as identified by the City of Portland Waterfront Commercial District large series. Luminaire shall include dropped refractor to provide Type IV optical distribution. Pole shall be tapered steel, 20'-0" in height with a 4'-4" top bracket arm. Luminaire finish shall be black. Provide integral photocell with luminaire.

Lamps: (1) MS175/PS/BU-ONLY

Manufacturers: *Holophane Lighting* # ESU 175PM MA CMA 45-64319 (luminaire)
Holophane Lighting # OUC 6063-T6 (bracket Arm)
Holophane Lighting # Cambridge series (pole base)

Type: S2

Description: Similar to Type S1 except City of Portland Waterfront Commercial District medium series. Pole shall be 15'-0" in height.

Lamps: (1) MS175/PS/BU-ONLY

Manufacturers: *Holophane Lighting* # ESU 175PM MA CMA 45-64319 (luminaire)
Holophane Lighting # OUC 6063-T6 (bracket Arm)
Holophane Lighting # Cambridge series (pole base)



Luminaire Dimensions: Height = 13" Diameter = 22"

Type: S3

Description: Street light luminaire with full cut-off optical distribution. Luminaire shall include a flush, flat tempered glass lens. Pole shall be round tapered aluminum, 20'-0" in height with a top bracket arm. Luminaire and pole finish shall be black. Provide integral photocell with luminaire.

Lamps: (1) MS175/HOR

Manufacturers: *U.S. Architectural* # DSCP/HR-III/175MH/PC (luminaire)
U.S. Architectural # XPK (bracket Arm)
U.S. Architectural # 2064-188/PT27/RBC (pole)

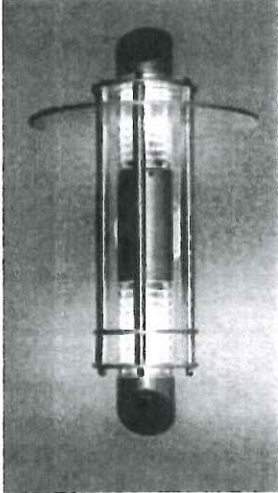
C.C.2

Type: S4

Description: Similar to Type S3 except pole shall be 14'-0" in height and luminaire shall utilize a 100 watt lamp.

Lamps: (1) MP100/U/MED

Manufacturers: U.S. Architectural # DSCP/HR-III/100MH/PC (luminaire)
U.S. Architectural # XPK (bracket Arm)
U.S. Architectural # 1443-125/PT27/RBC (pole)



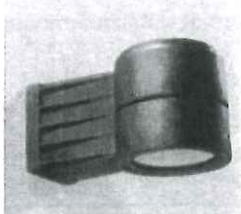
Dimensions: Height = 26 3/8" Diameter = 15" Projection = 14 1/8"

Type: S5

Description: Decorative wall sconce UL listed for wet locations. Luminaire shall include an aluminum housing, acrylic diffuser, and an aluminum top shield. Luminaire finish shall be black.

Lamps: (2) CF26DD/E/835

Manufacturers: Se'Lux # SA90-MR-Q26-BK-120



Dimensions: Height = 10" Diameter = 9 1/2" Projection = 17"

Type: S6

Description: Wall mounted luminaire with cast aluminum housing and flush, flat bottom diffuser lens. Luminaire finish shall be white. Luminaire shall be UL listed for wet locations. Install luminaire at decorative stone band as directed by the architect.

Lamps: (1) CF42DT/E/IN/830/ECO

Manufacturers: Luminis Lighting # WF1220-F142-WHT

Type: S7

Description: Identical to Type S1 except with 70 watt metal halide lamp.

Lamps: (1) MCP70/U/MED/830PB

Manufacturers: Luminis Lighting # WF1220-F142-WHT



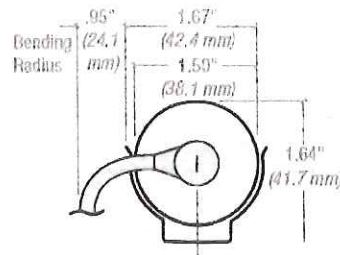
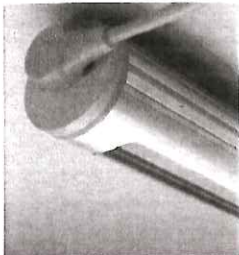
Dimensions: Height = 6 1/2" Width = 12 1/2"

Type: S8

Description: Adjustable wall mounted LED accent light, UL Listed for Wet locations installed as an uplight unit. Luminaire shall be capable of being rotated 350 degrees and tilted 110 degrees and shall have a locking base to secure the fixture after aiming. Luminaire optics shall produce a 10 degree beam spread. Luminaires shall be equipped with a top glare shield. Luminaire and shield finish shall be white. Provide a single scene controller for all Type S8 lights, as well as a data enabler (to be installed within the building at a location directed by the architect). Also provide all necessary inter-connecting wiring. Controller and enabler shall facilitate color output of luminaires. Install luminaires just above the decorative stone band as directed by the architect.

Lamps: LED (by manufacturer)

Manufacturers: *Color Kinetics* # 523-000033-00/120-000070-00/120-000009-03 (luminaire)
Color Kinetics # 503-000001-00 (scene controller)
Color Kinetics # 523-000006-00 (data enabler)



Dimensions: Overall Length = 18 1/4"

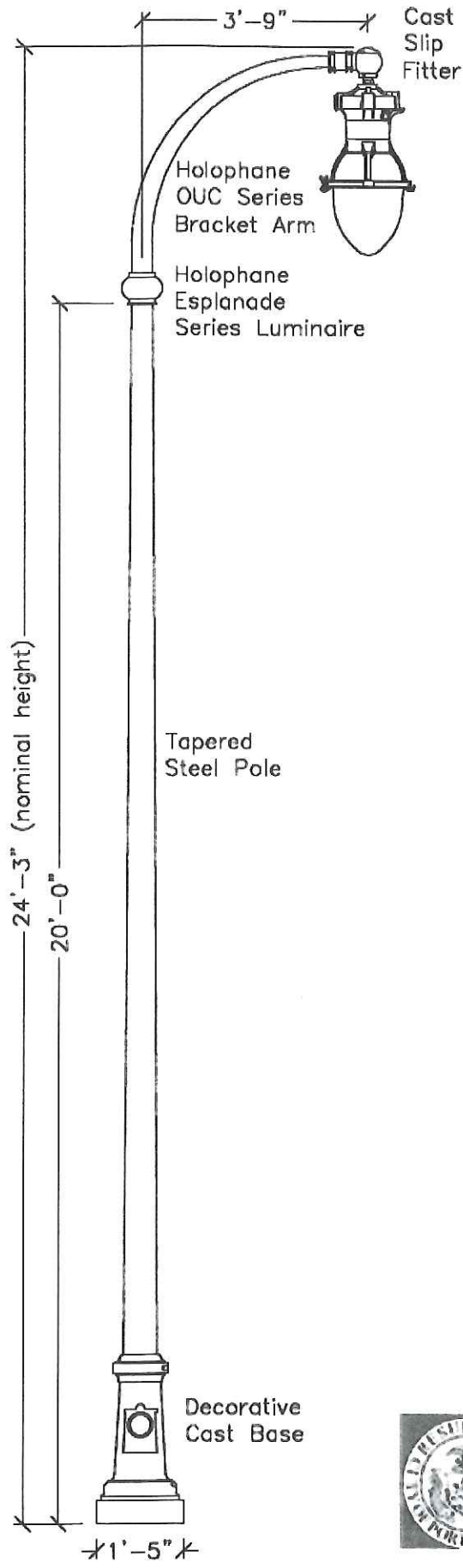
Type: S9

Description: Linear LED strip light with aluminum housing, UL listed for wet locations. Strip light shall be mounted in a down-light configuration and shall be installed to storefront window frame, directed downward through glass canopy. Provide all necessary fittings to make a complete assembly. Strip light shall be mounted with adjustable wall brackets that allow fixture aiming. Provide remote LED driver to be installed immediately inside above the closest accessible ceiling. Provide a single driver for each canopy LED system.

Lamps: LED (by manufacturer)

Manufacturers: *IO Lighting* # 0.10.E.3KHO.10.100.1.18 (strip light)
IO Lighting # DR200AM (driver)

Type S1



LUMINAIRE

Holophane Lighting model ESU 175PM MA CMC 45-64319
 Cast aluminum housing with stainless steel hardware.
 Dropped refractor shall be thermal resistant borosilicate glass. Internal reflector and prismatic diffuser shall provide an IES Type IV distribution pattern. Luminaire shall include an integral ballast with modular wiring connectors and multi-voltage taps. Provide an internal receptacle type photocell control. Luminaire shall be manufacturer's standard color black.

LAMP

Sylvania model MS175/PS/BU-ONLY
 Vertical mounted, 175 watt mogul base clear metal halide lamp.

BRACKET ARM

Holophane Lighting model OJC 6063-T6
 Aluminum crossarm with a post-top fitting for a 3-1/2" by 8" tenon. Bracket arm shall be manufacturer's standard black color.

SLIP FITTER

Holophane Lighting model BHLF200-SCA/AS (Boston Harbor Series)
 2-3/8" O.D. with swivel cast fitter. Slip fitter shall be manufacturer's standard black color.

LIGHTING POLE

Tapered steel pole shaft rated for a 90mph wind load with a 1.3 gust factor. Provide four hot-dipped galvanized steel L-type anchor bolts. Lighting pole shall be manufacturer's standard black color.

DECORATIVE POLE BASE

Holophane Lighting model Cambridge Series
 Decorative clamshell cast aluminum base. Hardware shall be stainless steel. Decorative pole base shall be manufacturer's standard black color.

SERVICE CONNECTION

New street/sidewalk lighting shall be connected to a metered electrical service from The Central Maine Power Company unless the City directs otherwise.

APPLICATION

Street/Sidewalk lighting for two-way streets with parking on both sides.

Suggested layout:

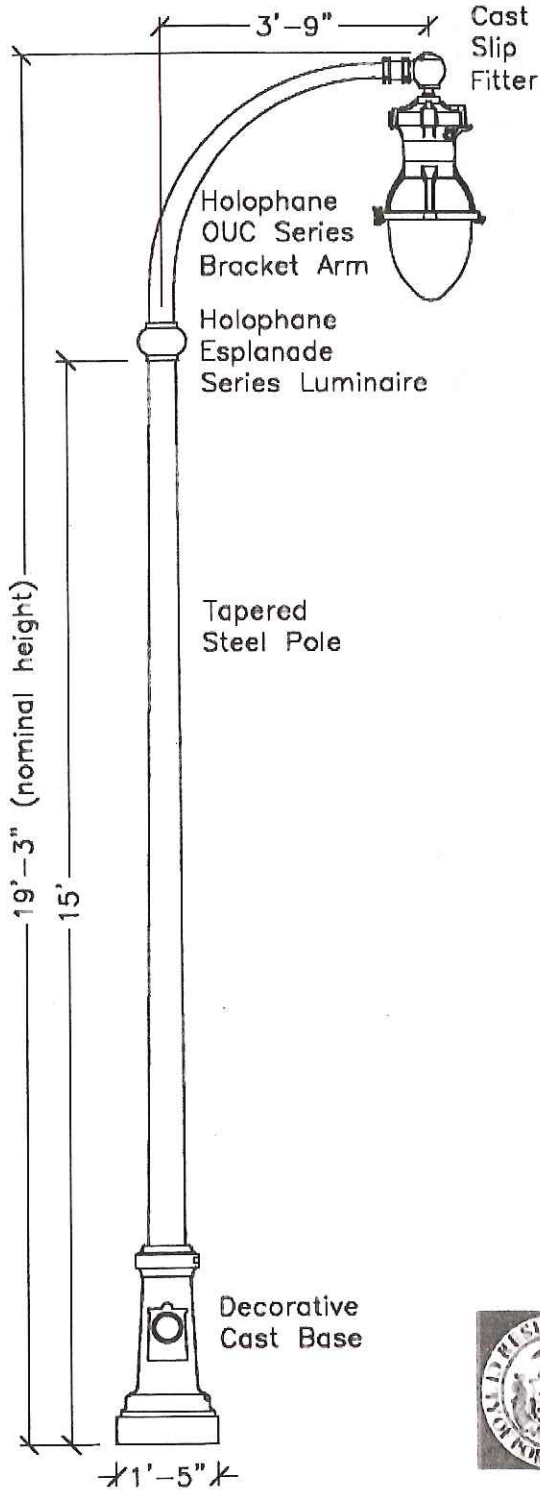
90-100 ft on center (one side only)

175-225 ft on center (staggered pattern both sides)



City of Portland, Maine
 Street & Sidewalk Lighting
 WATERFRONT COMMERCIAL DISTRICT
 Large Scale Lighting Pole

Type S2



LUMINAIRE

Holophane Lighting model ESU 175PM MA CMC 45-64319
Cast aluminum housing with stainless steel hardware.
Dropped refractor shall be thermal resistant borosilicate glass. Internal reflector and prismatic diffuser shall provide an IES Type IV distribution pattern. Luminaire shall include an integral ballast with modular wiring connectors and multi-voltage taps. Provide an internal receptacle type photocell control. Luminaire shall be manufacturer's standard color black.

LAMP

Sylvania model MS175/PS/BU-ONLY
Vertical mounted, 175 watt mogul base clear metal halide lamp.

BRACKET ARM

Holophane Lighting model OUC 6063-T6
Aluminum crossarm with a post-top fitting for a 3-1/2" by 8" tenon. Bracket arm shall be manufacturer's standard black color.

SLIP FITTER

Holophane Lighting model BHLF200-SCA/AS (Boston Harbor Series)
2-3/8" O.D. with swivel cast fitter. Slip fitter shall be manufacturer's standard black color.

LIGHTING POLE

Tapered steel pole shaft rated for a 90mph wind load with a 1.3 gust factor. Provide four hot-dipped galvanized steel L-type anchor bolts. Lighting pole shall be manufacturer's standard black color.

DECORATIVE POLE BASE

Holophane Lighting model Cambridge Series
Decorative clamshell cast aluminum base. Hardware shall be stainless steel. Decorative pole base shall be manufacturer's standard black color.

SERVICE CONNECTION

New street/sidewalk lighting shall be connected to a metered electrical service from The Central Maine Power Company unless the City directs otherwise.

APPLICATION

Street/Sidewalk lighting for two-way streets with parking on one sides, or one-way streets.

Suggested layout:

80-100 ft on center (one side only)

150-200 ft on center (staggered pattern both sides)



City of Portland, Maine

**Street & Sidewalk Lighting
WATERFRONT COMMERCIAL DISTRICT**

Medium Scale Lighting Pole

CC-6

DSCP

Type S3



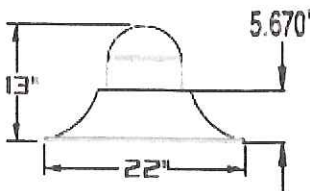
SPECIFICATIONS

- HOUSING:** ONE PIECE HEAVY WALL ALUMINUM CONSTRUCTION WITH REMOVABLE BALLAST TRAY FOR EASY BALLAST ACCESS.
- LENS ASS'Y:** ONE PIECE HINGED HEAVY GAUGE DIE FORMED ALUMINUM DOOR FRAME SURROUNDS 3/16" CLEAR TEMPERED GLASS LENS. GLASS IS SEALED TO DOOR WITH HIGH TEMPERATURE SILICONE SEAL. CAPTIVE THUMB SCREW DISENGAGES LENS ASSEMBLY AND ACTIVATES ACCUHINGE™ TO ALLOW LENS ASSEMBLY TO HINGE AWAY FROM HOUSING FOR LUMINAIRE ACCESS WITHOUT THE USE OF TOOLS.
- OPTICS:** COMPUTER DESIGNED ONE PIECE SPECULAR SEGMENTED SIDE REFLECTORS COMBINE WITH AN AUXILIARY SPECULAR TOP REFLECTOR TO PRODUCE A HIGHLY EFFICIENT, SHARP CUTOFF. OPTICAL ASSEMBLY IS ROTATABLE IN 90° INCREMENTS.
- GASKETING:** CLOSED CELL EPDM GASKETING COMPRESSED BETWEEN DOOR AND HOUSING WORKS IN COMBINATION WITH ACCUHINGE™ TO PRESSURE SEAL OPTICAL CHAMBER.
- LAMP HOLDER:** MEDIUM OR MOGUL BASE PORCELAIN. LAMPHOLDER EQUIPPED WITH FACTORY INSTALLED QUICK DISCONNECT PLUG.
- LAMP:** (BY OTHERS)
- BALLAST:** H.P.F./C.W.A. AUTOTRANSFORMER. -20° STARTING TEMPERATURE. ELECTRICAL COMPONENTS ARE MOUNTED TO A REMOVABLE BALLAST TRAY. BALLAST IS EQUIPPED WITH FACTORY INSTALLED QUICK DISCONNECT PLUG.
- FINISH:** POLYESTER POWDER COAT-STATE OF THE ART 20 PSI PRESSURE POWER WASH AT 140° TEMPERATURE INCORPORATES FOUR STEP IRON PHOSPHATE PROCESS TO CLEANSE AND PRETREAT THE METAL SURFACE FOR MAXIMUM PAINT ADHESION. ELECTROSTATICALLY APPLIED TEXTURED POLYESTER POWDER TOPCOAT IS BAKED AT 400° TEMPERATURE FOR MAXIMUM HARDNESS AND EXTERIOR DURABILITY.

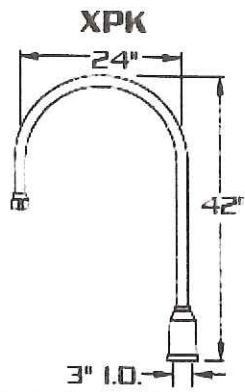


CC.7

Type S3



DSCP1



MODEL NO.:	OPTICS	LAMP	MOUNTING	FINISH	OPTIONS
D S C P	HORIZONTAL LAMP REFLECTORS	WATTAGE TYPE VOLTAGE	<input type="checkbox"/> XPK <input type="checkbox"/> XPS	STANDARD TEXTURED FINISH	<input type="checkbox"/> CONVEX GLASS.....DG
	<input type="checkbox"/> TYPE II HR-II (segmented)	<input type="checkbox"/> 250 <input type="checkbox"/> HPS <input type="checkbox"/> 120	<input type="checkbox"/> XPD <input type="checkbox"/> WM..... <input type="checkbox"/> 1.....	<input type="checkbox"/> BLACK RAL-9005-T	<input type="checkbox"/> CLEAR CONVEX POLYCARBONATE DIFFUSER.....LEX
	<input type="checkbox"/> TYPE III HR-III (segmented)	<input type="checkbox"/> 175 <input type="checkbox"/> MH <input type="checkbox"/> 208		<input type="checkbox"/> WHITE RAL-9003-T	<input type="checkbox"/> 3/4" IPS STEM MOUNT AND SWIVEL CANOPY (4'-0" MAX.).....SM
	<input type="checkbox"/> TYPE IV (FORWARD THROW) HR-IV (segmented)	<input type="checkbox"/> 150 <input type="checkbox"/> MV <input type="checkbox"/> 240		<input type="checkbox"/> GREY RAL-7004-T	<input type="checkbox"/> CHAIN MOUNTED (4'-0" MAX.).....CM
	<input type="checkbox"/> TYPE V HR-V-SQ (segmented)	<input type="checkbox"/> 100 <input type="checkbox"/> 277		<input type="checkbox"/> DRK BRONZE RAL-6019-T	<input type="checkbox"/> HOUSE SIDE SHIELD.....HS
	<input type="checkbox"/> TYPE V HR-V-SQ (segmented)	<input type="checkbox"/> 75 <input type="checkbox"/> 480		<input type="checkbox"/> GREEN RAL-6005-T	<input type="checkbox"/> QUARTZ RESTRIKE.....QTZ
	VERTICAL LAMP REFLECTORS	<input type="checkbox"/> 70 <input type="checkbox"/> MT			<input type="checkbox"/> PHOTO CELL + VOLTAGE (EXAMPLE: PC120V).....PC+V
	<input type="checkbox"/> ASYMMETRIC VR-ASY (segmented)	DSCP25 USE MOGUL BASE LAMPS		FOR SMOOTH FINISH REMOVE SUFFIX T (EXAMPLE: RAL-9500)	<input type="checkbox"/> SINGLE FUSE (120V., 277V).....SF
	<input type="checkbox"/> TYPE V VR-V-SQ (segmented)	DSCP1 USE MEDIUM BASE LAMPS		SEE WEBSITE FOR ADDITIONAL COLORS	<input type="checkbox"/> DOUBLE FUSE (208V., 240V).....DF
	VERTICAL LAMP REFLECTOR REQUIRES CONVEX GLASS			SEE ARM SECTION FOR ADDITIONAL ARMS	

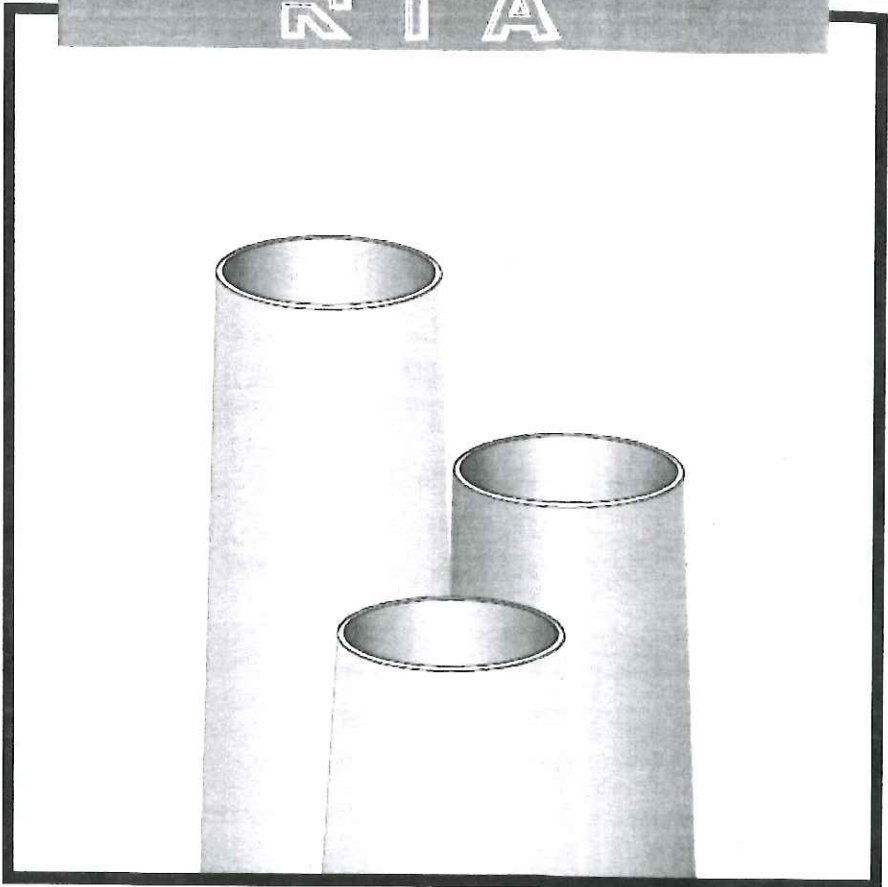
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Type S3

RTA



ROUND TAPERED ALUMINUM



SPECIFICATIONS

- SHAFT:** SPUN TAPERED FROM 6063 ALLOY ALUMINUM TUBING. HEAT TREATED TO PRODUCE A T6 TEMPER. SHAFT INCLUDES A PERIPHERALLY REINFORCED HAND HOLE FURNISHED WITH COVER. SHAFT IS FURNISHED WITH GROUND LUG LOCATED ON CAST ALUMINUM BASE PLATE.
- BASE PLATE:** CAST ALUMINUM CONSTRUCTED OF A-356 ALUMINUM ALLOY HEAT TREATED TO PRODUCE A T6 TEMPER. STRUCTURALLY ENGINEERED BASE INCLUDES EIGHT HEAVY WALL REINFORCING VERTICAL GUSSETS. BASE TELESCOPES AND IS CIRCUMFERENTIALLY WELDED TO SHAFT AT BOTH THE OUTSIDE TOP AND INSIDE BOTTOM OF THE BASE.
- ANCHORAGE:** (4) ANCHOR BOLTS FABRICATED FROM HOT ROLLED STEEL BAR. MINIMUM YIELD STRENGTH OF 50,000 P.S.I. BOLTS HAVE "L" BEND ON ONE END AND ARE THREADED ON THE OTHER END. BOLTS ARE FULLY GALVANIZED AND ARE FURNISHED WITH TWO NUTS AND TWO WASHERS.
- BASE COVER:** FABRICATED FROM HEAVY WALL ALUMINUM CONSTRUCTION. TWO PIECE COVER ENTIRELY CONCEALS BASE
- FINISH:** POLYESTER POWDER COAT-STATE OF THE ART 20 PSI PRESSURE POWER WASH AT 140° TEMPERATURE INCORPORATES FOUR STEP IRON PHOSPHATE PROCESS TO CLEANSE AND PRETREAT THE METAL SURFACE FOR MAXIMUM PAINT ADHESION. ELECTROSTATICALLY APPLIED TEXTURED POLYESTER POWDER TOPCOAT IS BAKED AT 400° TEMPERATURE FOR MAXIMUM HARDNESS AND EXTERIOR DURABILITY.

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F1-41

CC.9

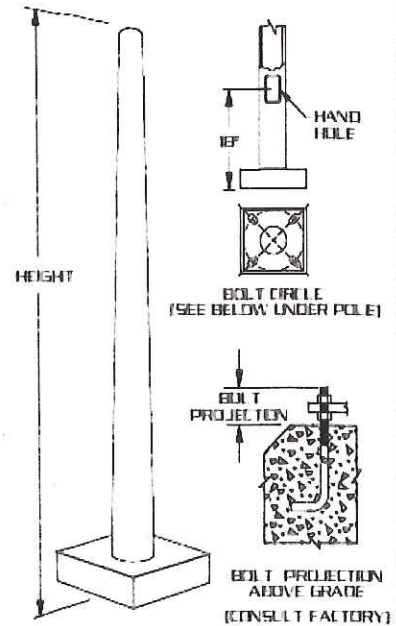
Type S3

RTA SERIES

ENGINEERING DATA Maximum EPA - Square Feet

Catalog Number	Maximum Fixt. wgt.	100 MPH	90 MPH	80 MPH	70 MPH
RTA 1443-125	50	2.2	3.0	4.5	6.2
RTA 1643-125	50	1.1	1.8	2.9	4.4
RTA 1853-188	100	4.1	5.6	7.6	10.8
RTA 2064-188	200	5.6	7.5	10.1	14.2
RTA 2574-188	225	5.7	7.3	9.5	13.6
RTA 3084-188	200	5.6	7.4	9.9	13.5
RTA 3584-188	100	3.8	5.1	7.0	9.3
RTA 4084-250	200	4.2	5.4	7.6	10.7

All above design calculations are based on sustained wind forces plus additional 1.3 wind gust
(Example: Pole rated at 80 MPH withstands 104 MPH gusts)



ORDERING INFORMATION

MODEL NO.:	POLES	MOUNTING	FINISH	OPTIONS																																													
RTA																																																	
MODEL NO.:	POLES	MOUNTING	FINISH	OPTIONS																																													
RTA	<table border="0"> <tr> <th>POLE HEIGHT</th> <th>SHAFT SIZE</th> <th>WALL THICKNESS</th> <th>BOLT CIRCLE</th> <th>ANCHORAGE</th> </tr> <tr> <td><input type="checkbox"/> 1443-125 14'</td> <td>4.0X3.0</td> <td>.125</td> <td>10 1/4"</td> <td>3/4"X18"X3"</td> </tr> <tr> <td><input type="checkbox"/> 1643-125 16'</td> <td>4.0X3.0</td> <td>.125</td> <td>11 1/4"</td> <td>3/4"X18"X3"</td> </tr> <tr> <td><input type="checkbox"/> 1853-188 18'</td> <td>5.0X3.0</td> <td>.188</td> <td>11 1/4"</td> <td>1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 2064-188 20'</td> <td>6.0X4.0</td> <td>.188</td> <td>13"</td> <td>1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 2574-188 25'</td> <td>7.0X4.0</td> <td>.188</td> <td>13"</td> <td>1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 3084-188 30'</td> <td>8.0X4.0</td> <td>.188</td> <td>13"</td> <td>1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 3584-188 35'</td> <td>8.0X4.0</td> <td>.188</td> <td>13"</td> <td>1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 4084-250 40'</td> <td>8.0X4.0</td> <td>.188</td> <td>13"</td> <td>1"X36"X4"</td> </tr> </table> <p><input type="checkbox"/> OTHER HEIGHTS SPECIFY _____</p>	POLE HEIGHT	SHAFT SIZE	WALL THICKNESS	BOLT CIRCLE	ANCHORAGE	<input type="checkbox"/> 1443-125 14'	4.0X3.0	.125	10 1/4"	3/4"X18"X3"	<input type="checkbox"/> 1643-125 16'	4.0X3.0	.125	11 1/4"	3/4"X18"X3"	<input type="checkbox"/> 1853-188 18'	5.0X3.0	.188	11 1/4"	1"X36"X4"	<input type="checkbox"/> 2064-188 20'	6.0X4.0	.188	13"	1"X36"X4"	<input type="checkbox"/> 2574-188 25'	7.0X4.0	.188	13"	1"X36"X4"	<input type="checkbox"/> 3084-188 30'	8.0X4.0	.188	13"	1"X36"X4"	<input type="checkbox"/> 3584-188 35'	8.0X4.0	.188	13"	1"X36"X4"	<input type="checkbox"/> 4084-250 40'	8.0X4.0	.188	13"	1"X36"X4"	<input type="checkbox"/> 2 3/8"X4" TENON PT23 <input type="checkbox"/> 2 7/8"X4" TENON PT27 <input type="checkbox"/> OTHER TENON MT _____ DRILL MOUNT <input type="checkbox"/> 1..... <input type="checkbox"/> 2-180..... <input type="checkbox"/> 2-50..... <input type="checkbox"/> 3-90..... <input type="checkbox"/> 4-90..... <input type="checkbox"/> 3-120..... 2-90, 3-90, 4-90 REQUIRES PT27 AND T490 ADAPTER 3-120 REQUIRES PT27 AND T3120 ADAPTER	STANDARD TEXTURED FINISH <input type="checkbox"/> BLACK RAL-9005-T <input type="checkbox"/> WHITE RAL-9003-T <input type="checkbox"/> GREY RAL-7004-T <input type="checkbox"/> DRK BRONZE RAL-8019-T <input type="checkbox"/> GREEN RAL-6005-T OPTION: <input type="checkbox"/> BRONZE ANODIZED AZ313 <input type="checkbox"/> NATURAL ANODIZED AZ <input type="checkbox"/> BLACK ANODIZED AZBK FOR SMOOTH FINISH REMOVE SUFFIX "T" (EXAMPLE: RAL-9000) SEE WEBSITE FOR ADDITIONAL COLORS	<input type="checkbox"/> DUPLEX RECEPTACLE DUP <input type="checkbox"/> GFI RECEPTACLE GFI <input type="checkbox"/> 3 WAY ADAPTER T3120 <input type="checkbox"/> 4 WAY ADAPTER T490 <input type="checkbox"/> ROUND BASE COVER RBC <input type="checkbox"/> 1/2" COUPLING CPLN1/2 <input type="checkbox"/> 3/4" COUPLING CPLN3/4 <input type="checkbox"/> 2" COUPLING CPLN2 (SPECIFY COUPLING LOCATION) SEE ACCESSORIES SECTION FOR OTHER OPTIONS.
POLE HEIGHT	SHAFT SIZE	WALL THICKNESS	BOLT CIRCLE	ANCHORAGE																																													
<input type="checkbox"/> 1443-125 14'	4.0X3.0	.125	10 1/4"	3/4"X18"X3"																																													
<input type="checkbox"/> 1643-125 16'	4.0X3.0	.125	11 1/4"	3/4"X18"X3"																																													
<input type="checkbox"/> 1853-188 18'	5.0X3.0	.188	11 1/4"	1"X36"X4"																																													
<input type="checkbox"/> 2064-188 20'	6.0X4.0	.188	13"	1"X36"X4"																																													
<input type="checkbox"/> 2574-188 25'	7.0X4.0	.188	13"	1"X36"X4"																																													
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DSCP

Type S4

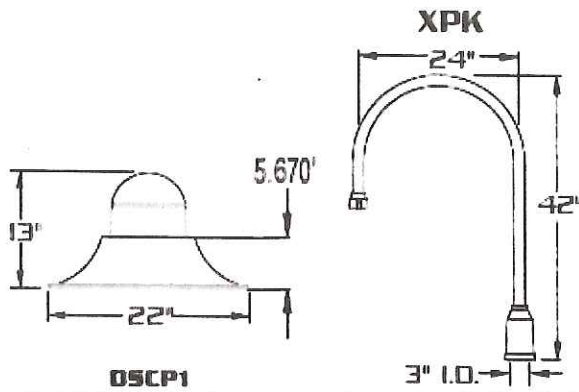


SPECIFICATIONS

- HOUSING:** ONE PIECE HEAVY WALL ALUMINUM CONSTRUCTION WITH REMOVABLE BALLAST TRAY FOR EASY BALLAST ACCESS.
- LENS ASSY:** ONE PIECE HINGED HEAVY GAUGE DIE FORMED ALUMINUM DOOR FRAME SURROUNDS 3/16" CLEAR TEMPERED GLASS LENS. GLASS IS SEALED TO DOOR WITH HIGH TEMPERATURE SILICONE SEAL. CAPTIVE THUMB SCREW DISENGAGES LENS ASSEMBLY AND ACTIVATES ACCUHINGE™ TO ALLOW LENS ASSEMBLY TO HINGE AWAY FROM HOUSING FOR LUMINAIRE ACCESS WITHOUT THE USE OF TOOLS.
- OPTICS:** COMPUTER DESIGNED ONE PIECE SPECULAR SEGMENTED SIDE REFLECTORS COMBINE WITH AN AUXILIARY SPECULAR TOP REFLECTOR TO PRODUCE A HIGHLY EFFICIENT, SHARP CUTOFF. OPTICAL ASSEMBLY IS ROTATABLE IN 90° INCREMENTS.
- GASKETING:** CLOSED CELL EPDM GASKETING COMPRESSED BETWEEN DOOR AND HOUSING WORKS IN COMBINATION WITH ACCUHINGE™ TO PRESSURE SEAL OPTICAL CHAMBER.
- LAMP HOLDER:** MEDIUM OR MEDIUM BASE PORCELAIN. LAMP HOLDER EQUIPPED WITH FACTORY INSTALLED QUICK DISCONNECT PLUG.
- LAMP:** (BY OTHERS)
- BALLAST:** H.P.F./C.W.A. AUTOTRANSFORMER, -20° STARTING TEMPERATURE. ELECTRICAL COMPONENTS ARE MOUNTED TO A REMOVABLE BALLAST TRAY. BALLAST IS EQUIPPED WITH FACTORY INSTALLED QUICK DISCONNECT PLUG.
- FINISH:** POLYESTER POWDER COAT-STATE OF THE ART 20 PSI PRESSURE POWER WASH AT 140° TEMPERATURE INCORPORATES FOUR STEP IRON PHOSPHATE PROCESS TO CLEANSE AND PRETREAT THE METAL SURFACE FOR MAXIMUM PAINT ADHESION. ELECTROSTATICALLY APPLIED TEXTURED POLYESTER POWDER TOPCOAT IS BAKED AT 400° TEMPERATURE FOR MAXIMUM HARDNESS AND EXTERIOR DURABILITY.

CC. 11

Type S4



MODEL NO.:	OPTICS	LAMP	MOUNTING	FINISH	OPTIONS
D S C P	HORIZONTAL LAMP REFLECTORS	WATTAGE TYPE VOLTAGE	<input type="checkbox"/> XPK <input type="checkbox"/> XPS	STANDARD TEXTURED FINISH	<input type="checkbox"/> CONVEX GLASS.....DG
	<input type="checkbox"/> TYPE II HR-II... (segmented)	<input type="checkbox"/> 250 <input type="checkbox"/> HPS <input type="checkbox"/> 120	<input type="checkbox"/> XPD <input type="checkbox"/> WM..... <input type="checkbox"/> 1.....	<input type="checkbox"/> BLACK RAL-9005-T	<input type="checkbox"/> CLEAR CONVEX POLYCARBONATE DIFFUSER.....LEX
	<input type="checkbox"/> TYPE III HR-III... (segmented)	<input type="checkbox"/> 175 <input type="checkbox"/> MH <input type="checkbox"/> 208		<input type="checkbox"/> WHITE RAL-9003-T	<input type="checkbox"/> 3/4" IPS STEM MOUNT AND SWIVEL CANOPY (4-0" MAX.).....SM
	<input type="checkbox"/> TYPE IV (FORWARD THROW) HR-IV... (segmented)	<input type="checkbox"/> 150 <input type="checkbox"/> MV <input type="checkbox"/> 240		<input type="checkbox"/> GREY RAL-7004-T	<input type="checkbox"/> CHAIN MOUNTED (4-0" MAX.).....CM
	<input type="checkbox"/> TYPE V HR-V-SQ (segmented)	<input type="checkbox"/> 100 <input type="checkbox"/> 277		<input type="checkbox"/> DRK BRONZE RAL-8019-T	<input type="checkbox"/> HOUSE SIDE SHIELD.....HS
	<input type="checkbox"/> TYPE V ASYMMETRIC VR-ASY... (segmented)	<input type="checkbox"/> 75 <input type="checkbox"/> 480		<input type="checkbox"/> GREEN RAL-6005-T	<input type="checkbox"/> QUARTZ RESTRIKE.....QTZ
	<input type="checkbox"/> TYPE V VR-V-SQ (segmented)	<input type="checkbox"/> 70 <input type="checkbox"/> MT			<input type="checkbox"/> PHOTO CELL + VOLTAGE (EXAMPLE: PC120V).....PC+V
	VERTICAL LAMP REFLECTORS				<input type="checkbox"/> SINGLE FUSE (120V., 277V).....SF
	<input type="checkbox"/> ASYMMETRIC VR-ASY... (segmented)				<input type="checkbox"/> DOUBLE FUSE (208V., 240V).....DF
	<input type="checkbox"/> TYPE V VR-V-SQ (segmented)				
VERTICAL LAMP REFLECTOR REQUIRES CONVEX GLASS					
	DSCP25 USE MGSU. BASE LAMPS		SEE ARM SECTION FOR ADDITIONAL ARMS	FOR SMOOTH FINISH REMOVE SUFFIX "T" (EXAMPLE: RAL-9500)	
	DSCP1 USE MEDIUM BASE LAMPS			SEE WEBSITE FOR ADDITIONAL COLORS	

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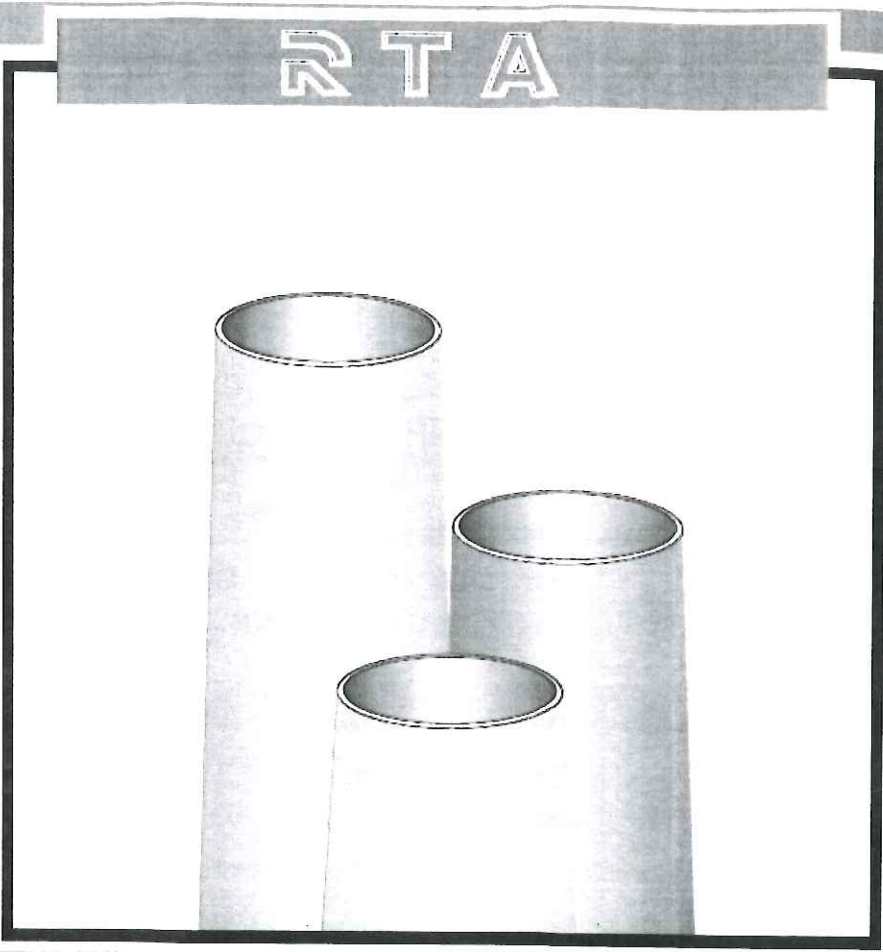
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 (861) 233-2000
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B2-12

CC.12

Type S4

RTA



ROUND TAPERED ALUMINUM

SPECIFICATIONS

- SHAFT:** SPUN TAPERED FROM 6063 ALLOY ALUMINUM TUBING. HEAT TREATED TO PRODUCE A T6 TEMPER. SHAFT INCLUDES A PERIPHERALLY REINFORCED HAND HOLE FURNISHED WITH COVER. SHAFT IS FURNISHED WITH GROUND LUG LOCATED ON CAST ALUMINUM BASE PLATE.
- BASE PLATE:** CAST ALUMINUM CONSTRUCTED OF A-356 ALUMINUM ALLOY HEAT TREATED TO PRODUCE A T6 TEMPER. STRUCTURALLY ENGINEERED BASE INCLUDES EIGHT HEAVY WALL REINFORCING VERTICAL GUSSETS. BASE TELESCOPES AND IS CIRCUMFERENTIALLY WELDED TO SHAFT AT BOTH THE OUTSIDE TOP AND INSIDE BOTTOM OF THE BASE.
- ANCHORAGE:** (4) ANCHOR BOLTS FABRICATED FROM HOT ROLLED STEEL BAR. MINIMUM YIELD STRENGTH OF 50,000 P.S.I. BOLTS HAVE "L" BEND ON ONE END AND ARE THREADED ON THE OTHER END. BOLTS ARE FULLY GALVANIZED AND ARE FURNISHED WITH TWO NUTS AND TWO WASHERS.
- BASE COVER:** FABRICATED FROM HEAVY WALL ALUMINUM CONSTRUCTION. TWO PIECE COVER ENTIRELY CONCEALS BASE
- FINISH:** POLYESTER POWDER COAT-STATE OF THE ART 20 PSI PRESSURE POWER WASH AT 140° TEMPERATURE INCORPORATES FOUR STEP IRON PHOSPHATE PROCESS TO CLEANSE AND PRETREAT THE METAL SURFACE FOR MAXIMUM PAINT ADHESION. ELECTROSTATICALLY APPLIED TEXTURED POLYESTER POWDER TOPCOAT IS BAKED AT 400° TEMPERATURE FOR MAXIMUM HARDNESS AND EXTERIOR DURABILITY.

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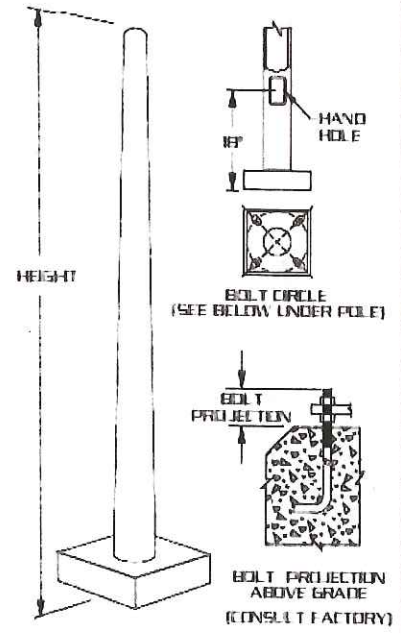
F1-41

Type S4

RTA SERIES

ENGINEERING DATA Maximum EPA - Square Feet

Catalog Number	Maximum Fixt. wgt.	100 MPH	90 MPH	80 MPH	70 MPH
RTA 1443-125	50	2.2	3.0	4.5	6.2
RTA 1643-125	50	1.1	1.8	2.9	4.4
RTA 1853-188	100	4.1	5.6	7.6	10.8
RTA 2064-188	200	5.6	7.5	10.1	14.2
RTA 2574-188	225	5.7	7.3	9.5	13.6
RTA 3084-188	200	5.6	7.4	9.9	13.5
RTA 3584-188	100	3.8	5.1	7.0	9.3
RTA 4084-250	200	4.2	5.4	7.6	10.7



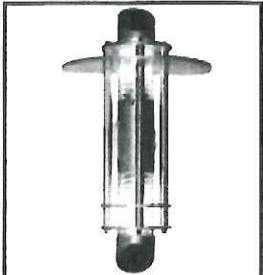
All above design calculations are based on sustained wind forces plus additional 1.3 wind gust
(Example: Pole rated at 80 MPH withstands 104 MPH gusts)

ORDERING INFORMATION

MODEL NO.	POLES	MOUNTING	FINISH	OPTIONS																																													
RTA																																																	
MODEL NO.:	POLES	MOUNTING	FINISH	OPTIONS																																													
RTA	<table border="1"> <thead> <tr> <th>POLE HEIGHT</th> <th>SHAFT SIZE</th> <th>WALL THICKNESS</th> <th>BOLT CIRCLE</th> <th>ANCHORAGE</th> </tr> </thead> <tbody> <tr> <td><input type="checkbox"/> 1443-125</td> <td>14'</td> <td>4.0X3.0</td> <td>.125</td> <td>10 1/4" 3/4"X18"X3"</td> </tr> <tr> <td><input type="checkbox"/> 1643-125</td> <td>16'</td> <td>4.0X3.0</td> <td>.125</td> <td>11 1/4" 3/4"X18"X3"</td> </tr> <tr> <td><input type="checkbox"/> 1853-188</td> <td>18'</td> <td>5.0X3.0</td> <td>.188</td> <td>11 1/4" 1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 2064-188</td> <td>20'</td> <td>6.0X4.0</td> <td>.188</td> <td>13" 1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 2574-188</td> <td>25'</td> <td>7.0X4.0</td> <td>.188</td> <td>13" 1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 3084-188</td> <td>30'</td> <td>8.0X4.0</td> <td>.188</td> <td>13" 1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 3584-188</td> <td>35'</td> <td>8.0X4.0</td> <td>.188</td> <td>13" 1"X36"X4"</td> </tr> <tr> <td><input type="checkbox"/> 4084-250</td> <td>40'</td> <td>8.0X4.0</td> <td>.188</td> <td>13" 1"X36"X4"</td> </tr> </tbody> </table> <p><input type="checkbox"/> OTHER HEIGHTS SPECIFY _____</p>	POLE HEIGHT	SHAFT SIZE	WALL THICKNESS	BOLT CIRCLE	ANCHORAGE	<input type="checkbox"/> 1443-125	14'	4.0X3.0	.125	10 1/4" 3/4"X18"X3"	<input type="checkbox"/> 1643-125	16'	4.0X3.0	.125	11 1/4" 3/4"X18"X3"	<input type="checkbox"/> 1853-188	18'	5.0X3.0	.188	11 1/4" 1"X36"X4"	<input type="checkbox"/> 2064-188	20'	6.0X4.0	.188	13" 1"X36"X4"	<input type="checkbox"/> 2574-188	25'	7.0X4.0	.188	13" 1"X36"X4"	<input type="checkbox"/> 3084-188	30'	8.0X4.0	.188	13" 1"X36"X4"	<input type="checkbox"/> 3584-188	35'	8.0X4.0	.188	13" 1"X36"X4"	<input type="checkbox"/> 4084-250	40'	8.0X4.0	.188	13" 1"X36"X4"	<p><input type="checkbox"/> 2 3/8"X4" TENON PT23</p> <p><input type="checkbox"/> 2 7/8"X4" TENON PT27</p> <p><input type="checkbox"/> OTHER TENON MT _____</p> <p>DRILL MOUNT</p> <p><input type="checkbox"/> 1..... </p> <p><input type="checkbox"/> 2-180..... </p> <p><input type="checkbox"/> 2-90..... </p> <p><input type="checkbox"/> 3-90..... </p> <p><input type="checkbox"/> 4-90..... </p> <p><input type="checkbox"/> 3-120..... </p> <p>2-90, 3-90, 4-90 REQUIRES PT27 AND T490 ADAPTER</p> <p>3-120 REQUIRES PT27 AND T3120 ADAPTER</p>	<p>STANDARD TEXTURED FINISH</p> <p><input type="checkbox"/> BLACK RAL-9005-T</p> <p><input type="checkbox"/> WHITE RAL-9003-T</p> <p><input type="checkbox"/> GREY RAL-7004-T</p> <p><input type="checkbox"/> DRK BRONZE RAL-8019-T</p> <p><input type="checkbox"/> GREEN RAL-6005-T</p> <p>OPTION:</p> <p><input type="checkbox"/> BRONZE ANODIZED AZ313</p> <p><input type="checkbox"/> NATURAL ANODIZED AZ</p> <p><input type="checkbox"/> BLACK ANODIZED AZBK</p> <p>FOR SMOOTH FINISH REMOVE SURFIX™ (EXAMPLE: RAL-9500)</p> <p>SEE WEBSITE FOR ADDITIONAL COLORS</p>	<p><input type="checkbox"/> DUPLEX RECEPTACLE DUP</p> <p><input type="checkbox"/> GFI RECEPTACLE GFI</p> <p><input type="checkbox"/> 3 WAY ADAPTER T3120</p> <p><input type="checkbox"/> 4 WAY ADAPTER T490</p> <p><input type="checkbox"/> ROUND BASE COVER RBC</p> <p><input type="checkbox"/> 1/2" COUPLING CPLN1/2</p> <p><input type="checkbox"/> 3/4" COUPLING CPLN3/4</p> <p><input type="checkbox"/> 2" COUPLING CPLN2 (SPECIFY COUPLING LOCATION)</p> <p>SEE ACCESSORIES SECTION FOR OTHER OPTIONS.</p>
POLE HEIGHT	SHAFT SIZE	WALL THICKNESS	BOLT CIRCLE	ANCHORAGE																																													
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Type S5

Saturn 90 Wall

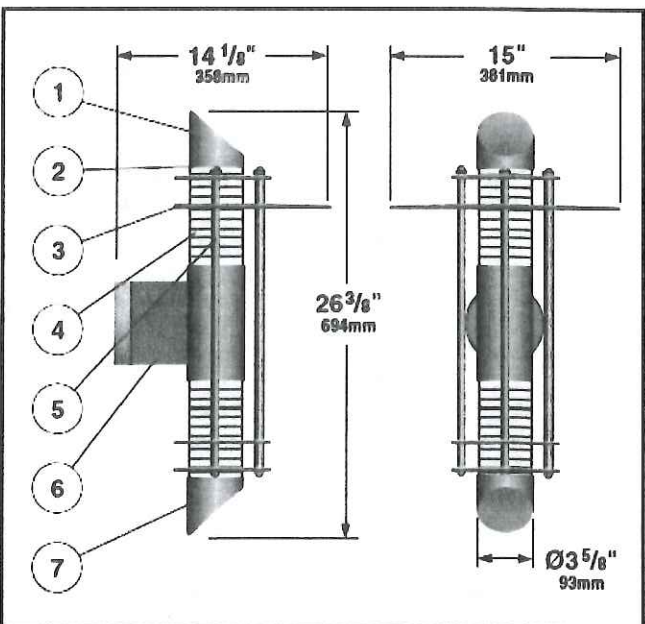


Project: _____
 Type: _____ Qty: _____

SA90 - MR - _____ - _____ - _____
 Fixture Series - Optics - Lamp Type / Wattage - Finish - Voltage

Series	Optics	Lamp Type / Wattage	Finish	Voltage
SA90 Saturn 90 Wall	MR MTR* 90 Refractor	T 13 (2) 13w Twin tube fluorescent	WH White	120
	Refractor is an integral part of the fixture and is not an option.	Q 18 (2) 18w Quad fluorescent	BK Black	277
		Q 26 (2) 26w Quad fluorescent	BZ Bronze	347
			SV Silver	
			SP Specify RAL#	

* US Patent No. 4,669,034



1. **Fixture Cover** - Die-cast aluminum cover, with round angled form. Thick-walled aluminum cover removes by loosening two, vandal-resistant, stainless steel set screws for easy access to lamp chamber.

2. **Gasketing** - Continuous molded neoprene gasket provides weatherproofing, dust, and insect control at shielding base, fixture cover, and between MTR rings.

3. **Fixture Shades** - Formed aluminum shades are painted white on undersides for maximum luminaire efficiency and provide continuity within the Saturn series.

4. **Shielding** - Injection-molded acrylic multi-prisms for total reflection (MTR). MTR rings are patterned after the light bending characteristics of a prism. Rings are secured to die-cast aluminum fitter. Additional small reflector is available for asymmetrical light distribution. Consult factory for information.

5. **Shade Support** - Machined aluminum support rods maintain spacing of fixture shades.

6. **Wall Mount Arm** - Cast aluminum with die-cast canopy.

7. **Column** - Extruded, thick-walled aluminum, minimum wall thickness 0.118" (3mm). Column houses cold weather ballast.

8. **Column Fitter** - (Not shown) Die-cast aluminum fitter holds ballast assembly and lamp socket. Fastened to column with three, vandal-resistant, stainless steel, countersunk screws. Column fitter removal allows access to ballast assembly.

9. **Ballasting** - (Not shown) Electronic ESB, high power factor, class P, type A sound rating. Specify 120v, 277v, or 347v. Consult factory for more detailed ballast information. Lamp provided by others.

10. **Lamp** - (Not shown) For use with two compact fluorescent lamps. T13W single end 2 pin base GX23; and Q18w and Q26w single end 4 pin base G24q. Lamps by others.

Exterior Luminaire Finish - SELUX utilizes a high quality Polyester Powder Coating. All SELUX luminaires and poles undergo a five stage intensive pretreatment process where product is thoroughly cleaned, phosphated and sealed. SELUX powder coated products provide excellent salt and humidity resistance as well as ultra violet resistance for color retention. All products are tested in accordance with test specifications for coatings from ASTM and PCI. Standard exterior colors are White (WH), Black (BK), Bronze (BZ), and Silver (SV). RAL colors (SP) are available, please specify RAL #.

CC.15

Type S5

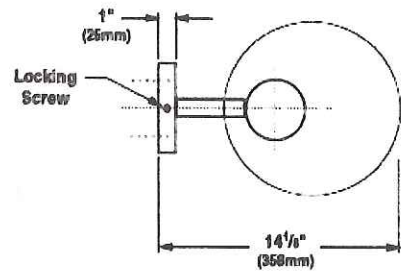
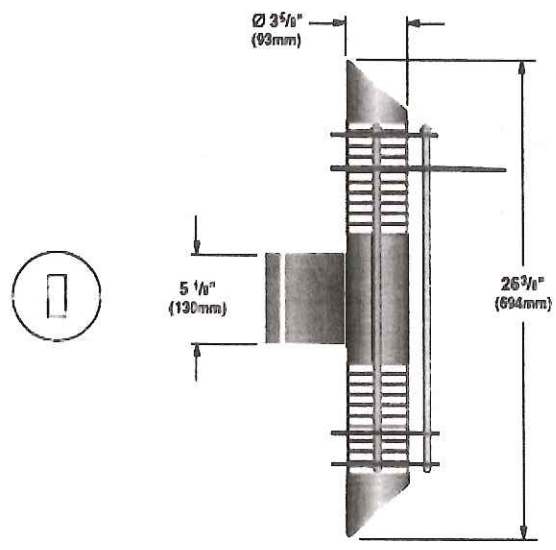
Saturn 90 Wall

se'lux

Mounting

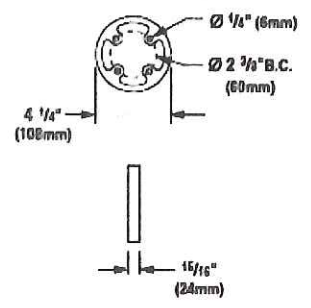
Die cast aluminum Wall Mount Arm with die-cast aluminum Canopy. Secured to Wall Arm Mounting Plate wall with Locking Screws located on top and bottom of Canopy.

Fixture Weight: 18 lbs.



Wall Arm Mounting Detail

Die cast aluminum Wall Mount. Secured to wall with 1/4" (6mm) threaded fasteners by others. Conduit by others.



LUMINIS

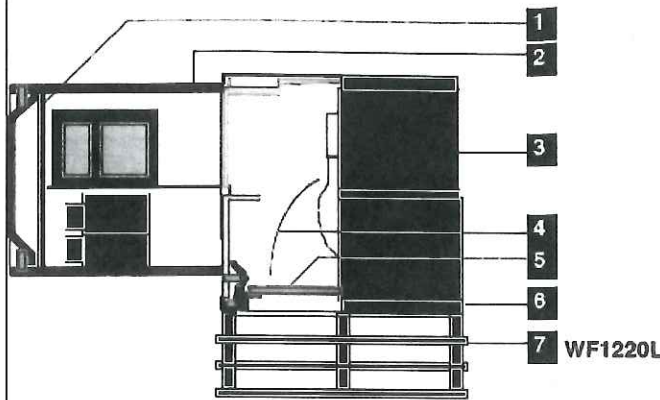
Type S6/S7

Wall mount

SYRIOS

Specification

WF1220



Alternate model with internal lamp adjustability allowing a 358° rotation X 30° tilt from vertical position with P39/P70/P100 H.I.D. sources.

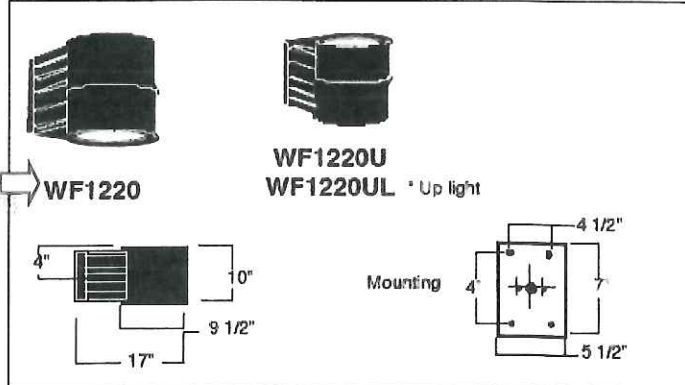
All exposed cast aluminum components are made of non corrosive pure aluminum copper free (Aluminum is less than .1% copper content.#356 alloy.)

Finish: All polyester powder baked coatings preceded with a 5 baths preparation process. Meets military grade MIL-C781706 with a 2000 hours salt spray test.

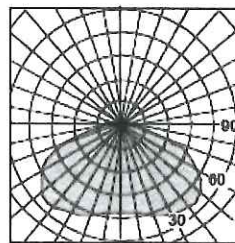
- Pulse rated porcelain socket or 75W/600V thermoplastic compact fluorescent socket.
- One-piece molded silicone gasket with memory retention.
- High power factor CWA ballast. Or 98% P.F. electronic smart ballast with end of life detection for compact fluorescent.
- All stainless steel hardware.
- All Moving and removable parts are sealed with a memory retention silicone gasket.

- 1- #12 Ga, Galvanized wall pressure mounting plate.
- 2- Cast aluminum ballast housing.
- 3- Cast aluminum modular optical light chamber.
- 4- Semi-specular aluminum reflector.
- 5- Clear tempered lens.
- 6- Cast aluminum sealed lens frame.
- 7- Set of 3 cast aluminum louvers for WF1220L.

WF1220UL: Up-light with clear tempered glass.
Flush mount installation to avoid accumulation of debris.



Photometry



Separate updated technical specification sheets with related models and light sources with I.E.S formatted photometry are available on www.luminis.com.

Certification

UL. Wet Location.

Mounting

Max. weight: 25 lbs.
Installation: Mounts over a standard 4" octagonal electrical box with 3 1/2" c/c mounting holes. (by others). Additional separate mounting holes are provided with wall plate.

Please refer to page 128 for additional details.

Model#	Light source selection	Voltage	Finish	Options
Type S7	<input type="checkbox"/> MH (ED17) <input type="checkbox"/> M70 <input type="checkbox"/> M100 <input type="checkbox"/> M150 <input type="checkbox"/> M175	120 V Standard factory prewired (if no voltage is specified). For other voltages, please specify with catalog number.	(Refer to color chart) Standard selection <input type="checkbox"/> WHT: Snow white <input type="checkbox"/> BKT: Jet black <input type="checkbox"/> BZT: Bronze <input type="checkbox"/> GRT: Titanium gray <input type="checkbox"/> DGT: Gun metal <input type="checkbox"/> CHT: Champagne <input type="checkbox"/> MST: Matte silver <input type="checkbox"/> GNT: Forest green Optional colors <input type="checkbox"/> CS: Custom color <input type="checkbox"/> RAL: RAL# color	<input type="checkbox"/> EM42S: 1 lamp (26/32/42W) EM battery. <input type="checkbox"/> FS: Fuse. <input type="checkbox"/> PH1: 120/277V photo cell. <input type="checkbox"/> PH2: 347V photo cell. <input type="checkbox"/> QS: Quartz lamp socket for external emergency power. <input type="checkbox"/> QRS: Quartz restrike standby relay. EM optional battery test switch and pilot lamp are installed remotely. (See page 128)
	<input type="checkbox"/> WF1220 <input type="checkbox"/> WF1220U <input type="checkbox"/> WF1220L <input type="checkbox"/> WF1220UL			
Type S6	<input type="checkbox"/> CF (Gx24q) Single lamp <input type="checkbox"/> F142 <input type="checkbox"/> F157	Compact fluorescent smart ballast 120/277 <input type="checkbox"/> 208 V <input type="checkbox"/> 240 V <input type="checkbox"/> 277 V <input type="checkbox"/> 347 V <input type="checkbox"/> 347 V (To specify)		
	<input type="checkbox"/> HPS (ED17) <input type="checkbox"/> H70 <input type="checkbox"/> H100 <input type="checkbox"/> H150 <input type="checkbox"/> PAR lamps <input type="checkbox"/> PH39 (PAR20) <input type="checkbox"/> PH70 (PAR30L) <input type="checkbox"/> PH100 (PAR38)			
	(Lamps by others)			

Luminaries may be altered for improvements or discontinued without prior notice.

CC, 17

Type S8

Date: _____ Type: _____

Firm Name: _____

Project: _____

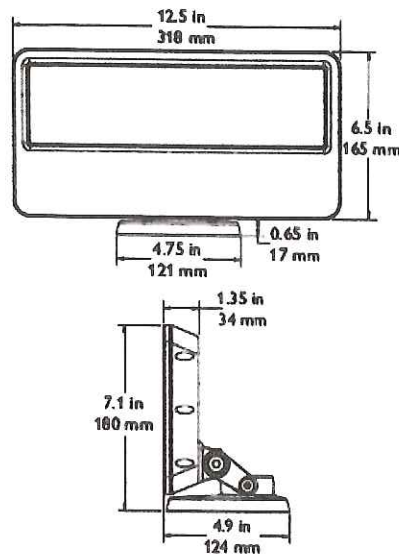


iW Blast Powercore 10° Clear Lens

Intelligent, high-performance white floodlight with variable color temperature

iW™ Blast Powercore is an intelligent, high-intensity white-light LED fixture that goes where white lights have never gone before. With superior fixture-to-fixture color consistency, iW Blast Powercore is ideal for situations in which bulb maintenance may be difficult or impossible. Rated for both indoor and outdoor use, iW Blast Powercore offers color temperatures ranging from a warm 2,700 K to a cool 6,500 K, iW Blast Powercore offers the improved efficiency, simplified installation, and cost-effectiveness of Powercore® technology in a rugged die-cast aluminum housing, available in black or white.

- Integrates patented Powercore technology— Powercore technology rapidly, efficiently, and accurately controls power output to iW Blast Powercore fixtures directly from line voltage, eliminating the need for an external power supply. Contractor-friendly installation using standard mounting and wiring dramatically simplifies installation and helps lower total system cost.
- Supports new applications for white light— Long-life LEDs (90,000 hours at 50% lumen maintenance) significantly reduce or eliminate maintenance problems. For example, iW Blast Powercore can effectively illuminate building features from positions accessible only by crane.
- Wide range of color temperature and brightness — Channels of warm white and cool white LEDs produce color temperatures ranging from 2,700 K to 6,500 K. Fixture brightness is adjustable separately from color temperature.
- High-intensity, energy-efficient white light — With an output of over 1,600 lumens, iW Blast Powercore offers high-intensity illumination at a significantly lower power draw than comparable ceramic metal halide light sources.
- Versatile lighting options — A 23° frosted glass lens for a soft-edge beam and a 10° clear lens for extended light projection support a wide range of white lighting applications.



- Flexible light positioning — Locking base swivel, offering friction-free rotation of up to 350°, and 110° fixture tilting let you quickly aim the fixture without special tools.
- Easy installation — Fixtures can be mounted to a junction box on a wall, ceiling, or floor for maximum flexibility. The friction-free mounting base allows for after-installation rotation without precise junction box positioning.
- Universal power input range — iW Blast Powercore accepts a universal power input range of 100 to 240 VAC, allowing the installation of many units in a continuous run.
- Indoor and outdoor rated — Fully sealed for maximum fixture life and UL rated for both indoor and outdoor applications, iW Blast Powercore fixtures meet or exceed specifications for use in wet locations.

For detailed product information, please refer to the iW Blast Powercore Product Guide at www.colorkinetics.com/ls/intelliwhite/iwblastpc/

PHILIPS

CC.18

Specifications

Due to continuous improvements and innovations, specifications may change without notice.

Item	Specification	Details
	Beam Angle	10° clear lens
	Lumens†	1777
	Color Temperature	2700 K (+/- 145) – 6500 K (+/- 275)
Output	Efficacy (lm/W)	35.5
	Mixing Distance	6 in (152 mm) to uniform light
	CRI	73
	Lumen Maintenance‡	90,000 hours L50 @ 50° C
	Input Voltage	100 – 240 VAC
Electrical	Power Consumption	50 W maximum at full output, steady state
	Power Factor	>=0.7 residential, >=0.9 commercial
Control	Interface	iW Data Enabler (DMX or Ethernet)
	Control System	iW Scene Controller
Physical	Dimensions (Height x Width x Depth)	7.1 x 12.5 x 4.9 in (172 x 317 x 125 mm)
	Weight	6.4 lbs (2.9 kg)
	Housing	Die-cast aluminium White or black powder-coated finish
	Lens	Soft-focus, frosted polycarbonate
	Fixture Connections	6 ft (1.8 m) unified power / data cable
	Temperature	-40° F – 122° F (-40° C – 50° C) Operating -4° F – 122° F (-20° C – 50° C) Startup
	Humidity	0 – 95%, non-condensing
	Fixture Run Lengths Per iW Data Enabler*	33 – 110 VAC 36 – 120 VAC 52 – 220 VAC 52 – 240 VAC
	Certification	UL / cUL FCC
	LED Class	Class 2 LED product
Environment	Dry / Damp / Wet Environment, IP66	

*Configuration:
20 A circuit, 20 ft (6.1 m) leader
cable from iW Data Enabler to
first junction box, 1 ft (305 mm)
fixture cables, 2 ft (610 mm)
jumper cables*

† Lumen measurement complies with IES LM-79-08

‡ L50 = 50% maintenance of lumen output (when light output drops below 50% of initial output) Ambient temperature specified.

* These figures, provided as a guideline, are accurate for this configuration only. Changing the configuration can affect the fixture run lengths.



Fixtures, Data Enablers, and Controllers

Item	Housing Color	Item Number	Philips 12NC
iW Blast Powercore (UL)	White	523-000033-02	910503700381
10° Clear Lens	Black	523-000033-03	910503700382
iW Data Enabler		506-000001-00	910503700190
iW Scene Controller		503-000001-00	910503700189

Use Item Number when ordering in North America

iW Blast Powercore is part of a complete system which includes fixtures and:

- One or more iW Data Enablers
- iW Scene Controller (up to four per single run of iW Data Enablers)



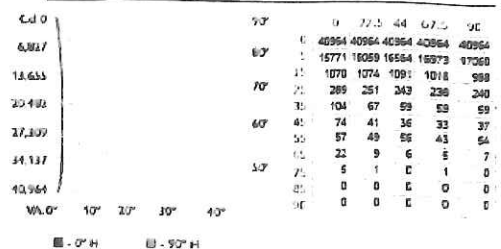
Philips Color Kinetics
3 Burlington Woods Drive
Burlington, Massachusetts 01803 USA
Tel 888.385.5742
Tel 617.423.9999
Fax 617.423.9998
www.colorkinetics.com

Photometrics

10° clear lens

Type S8

Polar Candle's Distribution



Luminance at Distance

Color Beam Angle	Beam Width
1.8	2.56 ft
6.6	40 ft
12.6	284 ft
16.6	160 ft
20.6	102 ft
24.6	71 ft

202 ft (61.6 m)
1 fc maximum distance

Power Consumption 50 W
Lumens 1777
Efficacy 35.5 lm/W

For lux multiply fc by 10.7

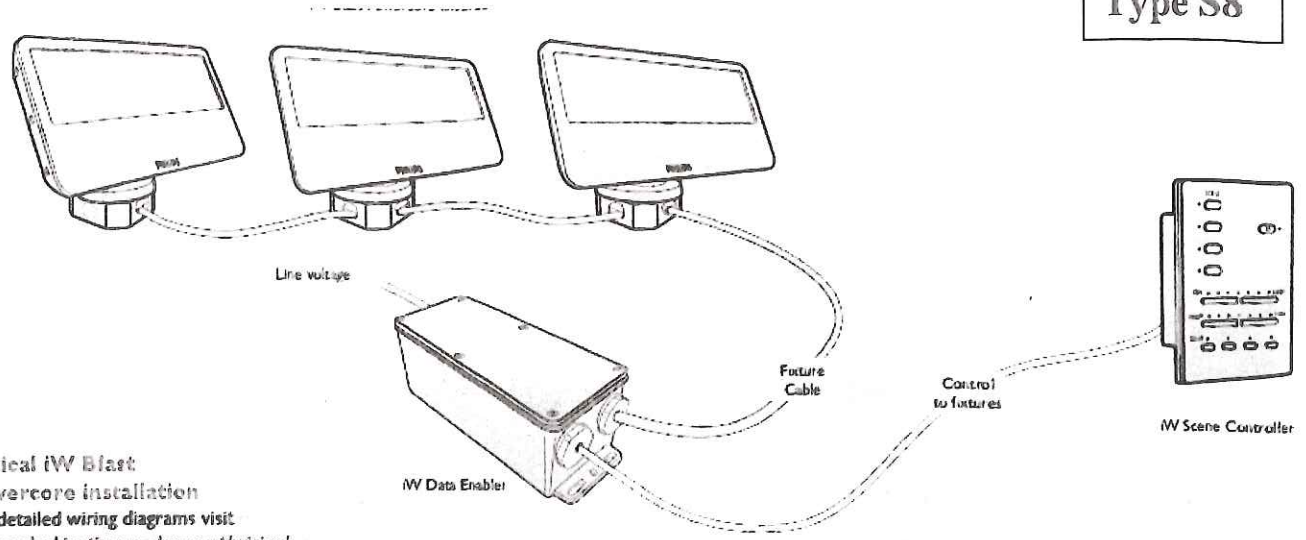
Accessories

Item	Housing Color / Type	Item Number	Philips 12NC
Accessory Holders	White	120-000070-00	—
	Black	120-000070-01	—
Half Top Hats	White	120-000009-03	—
	Black	120-000009-04	—
Top Hats	White	120-000005-03	—
	Black	120-000005-04	—
Egg Crate Louvers	White	120-000015-03	—
	Black	120-000015-04	—
Barndoors	White	120-000019-03	—
	Black	120-000019-04	—
Horizontal Spread Lens	36 / 50°	120-000025-00	—
Vertical Spread Lens	40°	120-000025-01	—

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DAS-000023-01 R02 12-09

Type S8

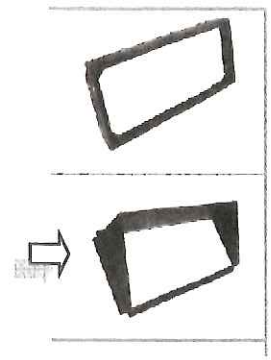


Typical IW Blast
Powercore installation
For detailed wiring diagrams visit
www.colorkinetics.com/support/wiring/

Accessories

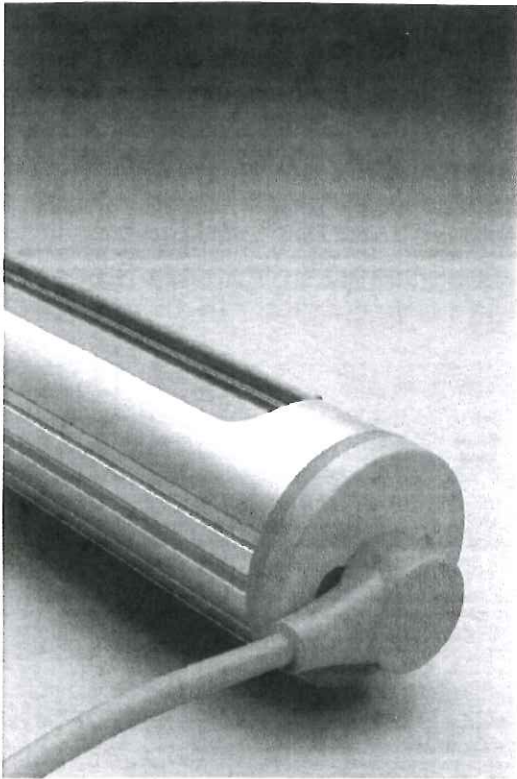
Designed specifically for the family of Blast fixtures, accessories provide additional options for controlling and dispersing light. Accessory holders snap to the front of the fixture and are required for mounting accessories. Accessory holders prevent accessories from falling out if the fixture is tipped or hung upside down.

Item	Housing Color	Item Number	Philips 12NC
Accessory Holders	White	120-000070-00	—
	Black	120-000070-01	—
Half Top Hats	White	120-000009-03	—
	Black	120-000009-04	—



Type S9

line 1.5



Application

ie Lighting's line series 1.5 is approximately 1.6" in diameter. UL listed for wet locations, this LED-based linear hoodlight produces luminous intensities for building facade and bridge accent illumination. Ideal for grading, electrical lead "plug" connectors eliminate hardware connections between fixtures, enabling ultimate flexibility for installation and re-orientation. Individual units may be placed end-to-end to create continuous rows without obvious shadows between fixtures. LEDs are similar to halogen light sources in that they are point sources that can reveal superior detail on textured surfaces and sparkle on reflective surfaces.

series 1.5 is a low voltage linear luminaire available in 18" lengths and may be daisy chained for continuous row mounted applications. Optional beam spreads along the perpendicular axis of the fixture include 10°, 45° and 65°. To ensure that each LED is provided thermal and electrical management properties in accordance with the LED manufacturers recommendations. Projected average rated life is 50,000 hours at 70% of maximum output. Contact factory for IES LM-80 compliance. To ensure proper performance, interior architectural details should allow for ventilation and air flow around the fixture. Ambient temperature surrounding the fixture shall not exceed 120°F (48.9°C).

Light Output

line series 1.5 is available with three times outputs for white light only. Red, green, blue and amber are available in high output only. All values below are initial lumens per foot. IES LM-79 format files may be obtained from the factory or downloaded from www.1e1ighting.com. Contact factory for High CRI options and availability.

	Standard Output	Mid Output	High Output
2700K White:	68 lms/ft	126 lms/ft	180 lms/ft
3000K White:	65 lms/ft	126 lms/ft	180 lms/ft
5000K White:	91 lms/ft	168 lms/ft	240 lms/ft

Construction

The light weight, yet durable extruded aluminum housing provides recommended heat sink requirements for LEDs. Patented precision optic assembly is composed of a customized acrylic material that offers very high transmissivity, UV stability and excellent longevity. series 1.5 is UL listed for wet locations.

Electrical

8'-0" 16 AWG, 300 volt rated power cords are supplied with plug connector. Injection molded end cap is designed to receive both the plug electrical connector and an interconnect for daisy chain. 24 volt 96 watt power supply will be provided as a standard if not specified otherwise. For detailed information regarding daisy chain limitations, remote distance limitations, power supply options, and dimming options consult the website, the catalog (pages 98-103) or an ie representative.

Power Consumption

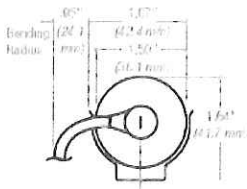
Standard Output:	2.92 w/ft	Mid Output:	5.34 w/ft	High Output:	7.62 w/ft
------------------	-----------	-------------	-----------	--------------	-----------

Power consumption does not include power supply losses.

Finish

Anodized aluminum finish is standard. Custom finishes may be available upon request.

Dimensions



ie line 1.5 (C10)

lighting facts^{CM}

A PART OF THE S 901

Light Output (Lumens)	126
Watts	12.3
Lumens per Watt (Efficacy)	10

Color Accuracy	76
Color Rendering Index (CRI)	

Light Color	3161 (Bright White)
Correlated Color Temperature (CCT)	

Warm White

2700K

Bright White

3000K

Daylight

4500K

5500K

All results are according to IESNA LM-79 2008. Approved Method for the Evaluation of Photometric Binning of Solid State Luminaires. For U.S. Department of Energy (DOE) verified product visit www.1e1ighting.com.

Visit www.lightingfacts.com for the Label Reference Guide.

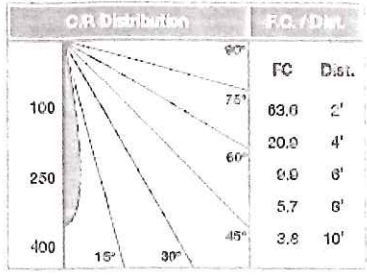
Registration Number: **6PRA-TMDFKX**
 Model Number: **610E 3000 45118**
 Type: **OD**

Label reference 60" line 1.5 fixture with a 45° beam spread in High Output 3000K. Lighting Facts for additional beam spreads and light output levels may be obtained from ie Lighting.

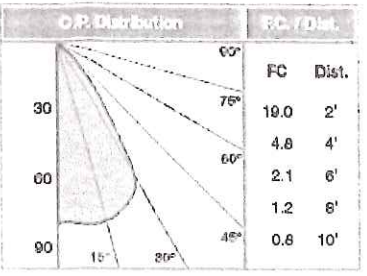
Type S9

Light Output / Distribution / Electrical Feed Options

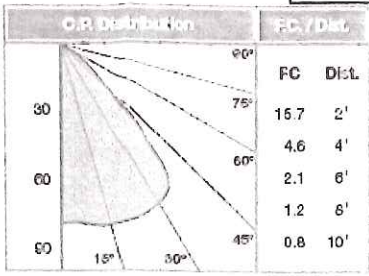
3KH0 10 DEGREE



3KH0 45 DEGREE



3KH0 65 DEGREE



LIGHT OUTPUT CONVERSION TABLE

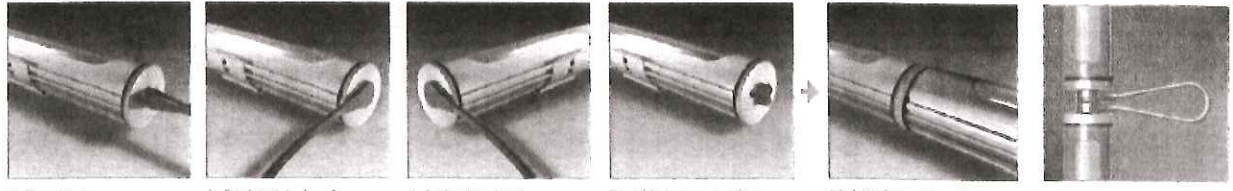
White Light Output	3000K S.O.	3000K M.O.	3000K H.O.	5000K S.O.	5000K M.O.	5000K H.O.
Light Output Multiplier	0.38 ⁽¹⁾	0.70 ⁽²⁾	1.0 ⁽³⁾	0.61 ⁽⁴⁾	0.93 ⁽⁵⁾	1.33 ⁽⁶⁾

Color Light Output:	RED	GREEN	BLUE	AMBER
Light Output Multiplier	0.83 ⁽⁷⁾	0.81 ⁽⁸⁾	0.30 ⁽⁹⁾	0.81 ⁽⁸⁾

IES format photometrics may be downloaded from www.ialighting.com.

Note: 2700K and 3000K have same light output.

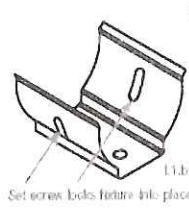
ELECTRICAL FEED "PLUG" CONNECTORS



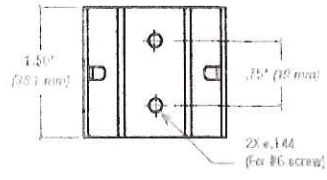
2 End feed 3 Right side feed 4 Left side feed Rigid Interconnector (for continuous row mounting) Rigid Interconnect plug (for daisy chaining - requires a LEVEL mounting surface) 6" Interconnect

Note: All plug connectors are field interchangeable.

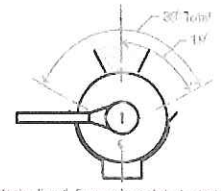
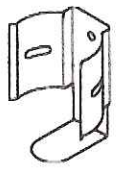
Mounting Options



Surface Mount



Vertical Mount



Line series 1.5 is not rated for submersible applications. Line 1.5 should not be mounted in conditions where there is any standing water whatsoever.

Note: Architectural detail MUST allow for access to set screws to ensure stable mounting condition.

Note: line 1.5 may be rotated with the bracket for field adjustable slanting.

Order Code

- | | | | | |
|--|---|---|---|--|
| <p>0 10 E</p> <p>10 1 2 3 4 5 6 7 8 9 10</p> | <p>1. LIGHT OUTPUT</p> <p>10 1.5 S.O. M.O. or H.O.</p> <p>2. LOCATION</p> <p>E Exterior</p> <p>3. COLOR</p> <p>27K White 2700K (Standard)⁽¹⁾</p> <p>27KM White 2700K (Mid Output)⁽²⁾</p> <p>27KH White 2700K (High Output)⁽³⁾</p> <p>5K White 3000K (Standard)⁽⁴⁾</p> <p>30M White 3000K (Mid Output)⁽⁵⁾</p> <p>3KH White 3000K (High Output)⁽⁶⁾</p> <p>5K White 5000K (Standard)⁽⁷⁾</p> <p>5M White 5000K (Mid Output)⁽⁸⁾</p> <p>5KH White 5000K (High Output)⁽⁹⁾</p> <p>R Red⁽⁸⁾</p> <p>G Green⁽⁸⁾</p> <p>B Blue⁽⁸⁾</p> <p>A Amber⁽⁸⁾</p> | <p>4. DISTRIBUTION</p> <p>10 10 Degree</p> <p>45 45 Degree</p> <p>65 65 Degree</p> <p>5. MOUNTING</p> <p>100 Surface mount</p> <p>105 Vertical mount</p> <p>Note: Universal mounting bracket may be used for all mounting orientations. Set screw locks in place.</p> <p>6. FINISH</p> <p>1 Anodized aluminum</p> <p>2 Anodized custom color</p> | <p>7. LENGTH</p> <p>UNITS (ACTUAL)</p> <p>10 18" (18.25")</p> <p>FOR CONTINUOUS ROW</p> <p>Specify length (e.g., 86"-0")</p> <p>Note: Overall length must be divisible by 18.25"</p> <p>8. ELECTRICAL FEED</p> <p>2 End feed</p> <p>3 Right side feed</p> <p>4 Left side feed</p> <p>5 6" whip, Left side and right side feed.</p> <p>Note: For straight continuous rows led by one driver, interconnects will be supplied as required.</p> | <p>9. VOLTAGE / DIMMING</p> <p>1 120v</p> <p>2 277v</p> <p>5 120v w/dim</p> <p>4 277v w/dim</p> <p>5 Other</p> <p>10. SPECIFY DRIVER / DIMMING</p> <p>Note: If not specified otherwise, we will supply 96 watt drivers. Refer to pages 95-100 for Power Supply options or download Power Supply specification sheet from www.ialighting.com.</p> |
|--|---|---|---|--|

1. White light transmittance between LEDs within a single fixture will not exceed ANSI Binning Standards.
2. Refer to conversion table for output. Only available in 7.5' width.

Footnotes

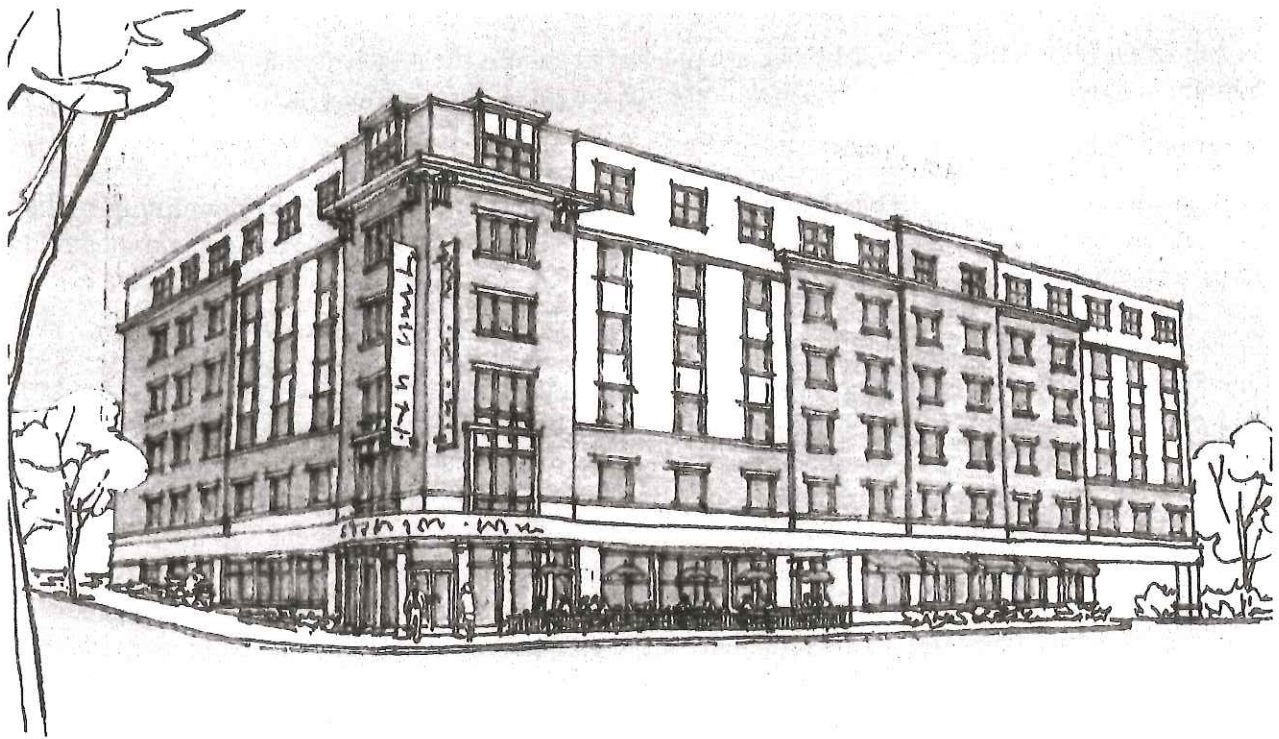
For Metric Conversion			S9L Chromaticity Standard: ANSI C78.397		
1"	1"	1"	Color	Nominal CCT	Typical CCT & tolerance (K)
25.4mm	2.54cm	0.3m	White	2700K	2725 ± 145
			White	3000K	3045 ± 175
			White	5000K	6028 ± 283

A.1

PROPOSED DEVELOPMENT

HOTEL, RESTAURANT & RESIDENCES - OLD PORT

SUBDIVISION/SITE PLAN
APPLICATION



OLD PORT HOSPITALITY, LLC
FEBRUARY 16, 2010



February 16, 2010

Mr. William Needelman, Senior Planner
Planning Division
389 Congress Street, 4th floor
Portland, ME 04101

Re: Development Review Application for
Hotel, Restaurant, & Residences – Old Port
207 & 209 Fore Street (formerly 38 India Street)

Dear Bill,

On behalf of Old Port Hospitality, LLC we are pleased to submit the attached materials in support of the Development Review Application for a new hotel, restaurant, and residences facility to be constructed at 207 & 209 Fore Street.

The Hotel, Restaurant & Residences – Old Port project is a three-unit mixed use condominium. The Hotel Unit will be a 122-room Hampton Inn with associated public circulation space, a continental breakfast area (for guests only), a meeting room, pool, fitness room, and customary “back office” support spaces. The Restaurant Unit will be an approximately 180-seat, 7,011 square foot restaurant located on the first floor located in the southwest corner of the building or the intersection of Fore Street and the Franklin Street Arterial. The Residences Unit will itself be a “sub-condominium” comprised of up to 12 condominium units to be located on the top (sixth) floor with its own separate lobby and elevator. The 1.75-acre site will be subdivided into two parcels: the 0.66-acre lot on which the condominium will be created and a 1.09-acre remainder lot that will be retained by the applicant for future development. The hotel and residences will have deeded parking easement rights to a dedicated surface parking lot on the remainder parcel, which will have 90 parking spaces comprised of regular single-car spaces and “tandem” two-car spaces for hotel valet parking. The applicant anticipates constructing structured parking on the remainder lot in conjunction with future development, and the parking easements for the hotel and residences will allow the applicant to temporarily provide off-site parking during construction and to ultimately provide structured parking spaces in lieu of the surface parking lot.

The proposal will complement scale and land uses of the surrounding neighborhood. Pedestrian access around the subject block will be enhanced with the addition of sidewalk along the entire perimeter. The proposed uses are allowed within the B-3 Downtown Business Zone. The surface parking will require a Conditional Use Permit.

The following is an itemized list of today’s submittal for review:

- This cover letter
- (7) Development Review Application

- (7) Wastewater Capacity Application
- (7) Project Narrative containing required written submittals
- (7) Traffic Analysis
- (3) Stormwater Management Plan
- (7) Alta Survey Plans – 2 full size sheets
- (7) Exterior Building Elevations – 2 full size sheets
- (7) Subdivision Plan – 1 full size sheet
- (7) Photometrics Plan – 1 full size sheet
- (7) Site Plan set by Sebago Technics – 10 full size sheets
- (1) Fire Department Submittal – 3 full size sheets

The applicant and design team looks forward working in close cooperation with the planning division to develop, permit, and ultimately construct this viable project.

Sincerely,

Opechee Construction Corporation



Barry Stowe



A. 4

Development Review Application PORTLAND, MAINE

Department of Planning and Urban Development,
Planning Division and Planning Board

PROJECT NAME: Hotel, Restaurant, & Residences - Old Port

PROPOSED DEVELOPMENT ADDRESS:
207 & 209 Fore Street (Formerly 38 India Street)

PROJECT DESCRIPTION:
Demolition of the Jordan Meats Packing Factory. Construction of a new hotel, restaurant, and residences complex.

CHART/BLOCK/LOT: 029 1003001 & 029 1001001

CONTACT INFORMATION:

APPLICANT

Name: Old Port Hospitality, LLC
Address: 11 Corporate Drive
Belmont, NH
Zip Code: 03303
Work #: 603-527-9090
Cell #: none
Fax #: 603-527-9191
Home: none
E-mail: gregk@opechee.com

PROPERTY OWNER

Name: Same as Applicant
Address: _____
Zip Code: _____
Work #: _____
Cell #: _____
Fax #: _____
Home: _____
E-mail: _____

BILLING ADDRESS

Name: Same as Applicant
Address: _____
Zip: _____
Work #: _____
Cell #: _____
Fax #: _____
Home: _____
E-mail: _____

RECEIVED

FEB 16 2010

City of Portland
Planning Division

~As applicable, please include additional contact information on the next page~

AGENT/REPRESENTATIVE

Name: Opechee Construction Corporation
Attn: Mark Woglom
Address: 11 Corporate Drive
Belmont, NH
Zip Code: 03220
Work #: 603-527-9090
Cell #: none
Fax #: 603-527-9191
Home: none
E-mail: stevel@opechee.com

ARCHITECT

Name: Opechee Construction Corporation
Attn: Don Blajda, R.A.
Address: 11 Corporate Drive
Belmont, NH
Zip Code: 03220
Work #: 603-527-9090
Cell #: none
Fax #: 603-527-9191
Home: none
E-mail: donb@opechee.com

SURVEYOR

Name: Sebago Technics, Inc.
Attn: Matthew Ek, PLS
Address: One Chabot Street, PO Box 1339
Westbrook, ME
Zip Code: 04098-1339
Work #: 207-856-0277
Cell #: none
Fax #: 207-856-2206
Home: none
E-mail: driley@sebagotechnics.com

ENGINEER

Sebago Technics, Inc.
Name: Attn: Dan Riley, P.E.
Address: One Chabot Street, PO Box 1339
Westbrook, ME
Zip Code: 04098-1339
Work #: 207-856-0277
Cell #: none
Fax #: 207-856-2206
Home: none
E-mail: driley@sebagotechnics.com

CONSULTANT

Name: Opechee Construction Corporation
Attn: Steve Long, P.E.
Address: 11 Corporate Drive
Belmont, NH
Zip Code: 03220
Work #: 603-527-9090
Cell #: none
Fax #: 603-527-9191
Home: none
E-mail: stevel@opechee.com

ATTORNEY

Name: Greg R. Kirsch, esq.
Address: 11 Corporate Drive
Belmont, NH
Zip Code: 03220
Work #: 603-527-9090
Cell #: none
Fax #: 603-527-9191
Home: none
E-mail: gregk@opechee.com

PROJECT DATA

A.5

The following information is required where applicable, in order complete the application

Total Site Area 76,290 sq. ft.
 Proposed Total Disturbed Area of the Site (includes right-of-way) approx. 100,000 sq. ft.
 (If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with DEP and a Stormwater Management Permit, Chapter 500, with the City of Portland)

IMPERVIOUS SURFACE AREA

Proposed Total Paved Area (total impervious area minus the building roof area) 32,060 sq. ft.
 Existing Total Impervious Area 70,580 sq. ft.
 Proposed Total Impervious Area 49,420 sq. ft.
 Proposed Impervious Net Change (-) 21,160 sq. ft.

BUILDING AREA

Existing Building Footprint 42,990 sq. ft.
 Proposed Building Footprint 14,780 sq. ft.
 Proposed Building Footprint Net change (-) 28,210 sq. ft.
 Existing Total Building Floor Area (according to tax card) 67,654 sq. ft.
 Proposed Total Building Floor Area 96,953 sq. ft.
 Proposed Building Floor Area Net Change (+) 6,855 sq. ft.
 New Building yes (yes or no)

ZONING

Existing B3
 Proposed, if applicable n/a

LAND USE

Existing industrial / vacant
 Proposed hotel, restaurant, and residences

RESIDENTIAL, IF APPLICABLE

Proposed Number of Affordable Housing Units none
 Proposed Number of Residential Units to be Demolished none
 Existing Number of Residential Units none
 Proposed Number of Residential Units 12
 Subdivision, Proposed Number of Lots no residential lots

PARKING SPACES

Existing Number of Parking Spaces incalculable (random)
 Proposed Number of Parking Spaces 93 Private & 19 Public
 Number of Handicapped Parking Spaces 2
 Proposed Total Parking Spaces 112

BICYCLE PARKING SPACES

Existing Number of Bicycle Parking Spaces none
 Proposed Number of Bicycle Parking Spaces 11 racks = 22 spaces
 Total Bicycle Parking Spaces 22 spaces

ESTIMATED COST OF PROJECT

15 Million

Please answer the following with a Yes/No response on all that apply to the proposed development

Institutional	<u>no</u>	Change of Use	<u>yes</u>
Parking Lot	<u>yes</u>	Design Review	<u>yes</u>
Manufacturing	<u>no</u>	Flood Plain Review	<u>no</u>
Office	<u>no</u>	Historic Preservation	<u>no</u>
Residential	<u>yes</u>	Housing Replacement	<u>no</u>
Retail/Business	<u>no</u>	14-403 Street Review	<u>no</u>
Warehouse	<u>no</u>	Shoreland	<u>no</u>
Single Family Dwelling	<u>no</u>	Site Location	<u>no</u>
2 Family Dwelling	<u>no</u>	Stormwater Quality	<u>yes</u>
Multi-Family Dwelling	<u>no</u>	Traffic Movement	<u>yes</u>
B-3 Ped Activity Review	<u>no</u>	Zoning Variance	<u>no</u> (or date)
Change of Use	<u>yes</u>	Historic Dist./Landmark	<u>no</u>
		Off Site Parking	<u>no</u>

A.6

APPLICATION FEE:

Check all reviews that apply. Payment may be made in cash or check to the City of Portland.

<p>Major Development (more than 10,000 sq. ft.)</p> <p><input type="checkbox"/> Under 50,000 sq. ft. (\$500.00)</p> <p><input checked="" type="checkbox"/> 50,000 - 100,000 sq. ft. (\$1,000.00)</p> <p><input type="checkbox"/> Parking Lots over 100 spaces (\$1,000.00)</p> <p><input type="checkbox"/> 100,000 - 200,000 sq. ft. (\$2,000.00)</p> <p><input type="checkbox"/> 200,000 - 300,000 sq. ft. (\$3,000.00)</p> <p><input type="checkbox"/> Over 300,000 sq. ft. (\$5,000.00)</p> <p><input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)</p>	<p>Plan Amendments</p> <p><input type="checkbox"/> Planning Staff Review (\$250.00)</p> <p><input type="checkbox"/> Planning Board Review (\$500.00)</p> <p>Subdivision</p> <p><input checked="" type="checkbox"/> Subdivision (\$500.00) + amount of lots <u>14</u> (\$25.00 per lot) \$ <u>850.00</u> + (applicable Major site plan fee)</p>
<p>Minor Site Plan Review</p> <p><input type="checkbox"/> Less than 10,000 sq. ft. (\$400.00)</p> <p><input type="checkbox"/> After-the-fact Review (\$1,000.00 plus applicable application fee)</p>	<p>Other Reviews</p> <p><input type="checkbox"/> Site Location of Development (\$3,000.00 (except for residential projects which shall be \$200.00 per lot _____))</p> <p><input checked="" type="checkbox"/> Traffic Movement (\$1,000.00)</p> <p><input checked="" type="checkbox"/> Storm water Quality (\$250.00)</p> <p><input type="checkbox"/> Section 14-403 Review (\$400.00 + \$25.00 per lot)</p> <p><input checked="" type="checkbox"/> Other <u>Conditional Use Permit</u></p>

DEVELOPMENT REVIEW APPLICATION SUBMISSION

Submissions shall include seven (7) packets with folded plans containing the following materials:

1. Seven (7) full size site plans that must be folded.
2. Application form that is completed and signed.
3. Cover letter stating the nature of the project.
4. All Written Submittals (Sec. 14-525 2. (c), including evidence of right, title and interest.
5. A stamped standard boundary survey prepared by a registered land surveyor at a scale not less than one inch to 100 feet.
6. Plans and maps based upon the boundary survey and containing the information found in the attached sample plan checklist.
7. Copy of the checklist completed for the proposal listing the material contained in the submitted application.
8. One (1) set of plans reduced to 11 x 17.

Refer to the application checklist (page 9) for a detailed list of submittal requirements.

Portland's development review process and requirements are outlined in the Land Use Code (Chapter 14), which includes the Subdivision Ordinance (Section 14-491) and the Site Plan Ordinance (Section 14-521). Portland's Land Use Code is on the City's web site: www.portlandmaine.gov Copies of the ordinances may be purchased through the Planning Division.

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, if a permit for work described in this application is issued, I certify that the Planning Authority and Code Enforcement's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for site review only; a Performance Guarantee, Inspection Fee, Building Permit Application and associated fees will be required prior to construction.

<p>Signature of Applicant:</p> <p>OLD PORT HOSPITALITY, LLC <i>[Signature]</i>, MEMBER</p>	<p>Date:</p> <p>FEB. 15, 2010</p>
--	-----------------------------------

Site Plan Checklist

Portland, Maine

A.7

Department of Planning and Urban Development, Planning Division and Planning Board

Hotel, Restaurant, & Residences - Old Port

Project Name, Address of Project

Application Number

(The form is to be completed by the Applicant or Designated Representative)

Check Submitted		Required Information	Section 14-525 (b,c)
Applicant	Staff		
<input checked="" type="checkbox"/>	_____	Standard boundary survey (stamped by a registered surveyor, at a scale of not less than 1 inch to 100 feet and including:	1
<input checked="" type="checkbox"/>	_____	Name and address of applicant and name of proposed development	a
<input checked="" type="checkbox"/>	_____	* Scale and north points	b
<input checked="" type="checkbox"/>	_____	* Boundaries of the site	c
<input checked="" type="checkbox"/>	_____	* Total land area of site	d
<input checked="" type="checkbox"/>	_____	* Topography - existing and proposed (2 feet intervals or less)	e
<input checked="" type="checkbox"/>	_____	Plans based on the boundary survey including:	2
<input checked="" type="checkbox"/>	_____	* Existing soil conditions	a
<input type="checkbox"/> n/a	_____	* Location of water courses, wetlands, marshes, rock outcroppings and wooded areas	b
<input checked="" type="checkbox"/>	_____	* Location, ground floor area and grade elevations of building and other structures existing and proposed, elevation drawings of exterior facades, and materials to be used	c
<input checked="" type="checkbox"/>	_____	* Approx location of buildings or other structures on parcels abutting the site and a zoning summary of applicable dimensional standards (<u>example page 11 of packet</u>)	d
<input checked="" type="checkbox"/>	_____	* Location of on-site waste receptacles	e
<input checked="" type="checkbox"/>	_____	* Public utilities	
<input checked="" type="checkbox"/>	_____	* Water and sewer mains	e
<input checked="" type="checkbox"/>	_____	* Culverts, drains, existing and proposed, showing size and directions of flows	e
<input checked="" type="checkbox"/>	_____	* Location and dimensions, and ownership of easements, public or private rights-of-way, both existing and proposed	f
<input checked="" type="checkbox"/>	_____	* Location and dimensions of on-site pedestrian and vehicular access ways	g
<input checked="" type="checkbox"/>	_____	* Parking areas	
<input checked="" type="checkbox"/>	_____	* Loading facilities	g
<input checked="" type="checkbox"/>	_____	* Design of ingress and egress of vehicles to and from the site onto public streets	g
<input checked="" type="checkbox"/>	_____	* Curb and sidewalks	g
<input checked="" type="checkbox"/>	_____	Landscape plan showing:	h
<input checked="" type="checkbox"/>	_____	* Location of existing vegetation and proposed vegetation	h
<input checked="" type="checkbox"/>	_____	* Type of vegetation	h
<input checked="" type="checkbox"/>	_____	* Quantity of plantings	h
<input checked="" type="checkbox"/>	_____	* Size of proposed landscaping	h
<input type="checkbox"/> n/a	_____	* Existing areas to be preserved	h
<input type="checkbox"/> n/a	_____	* Preservation measures to be employed	h
<input checked="" type="checkbox"/>	_____	* Details of planting and preservation specifications	h
<input checked="" type="checkbox"/>	_____	* Location and dimensions of all fencing and screening	i
<input checked="" type="checkbox"/>	_____	Location and intensity of outdoor lighting system	j
<input checked="" type="checkbox"/>	_____	Location of fire hydrants, existing and proposed (<u>refer to Fire Department checklist – page 11</u>)	k
<input type="checkbox"/>	_____	Written statements to include:	c
<input checked="" type="checkbox"/>	_____	* Description of proposed uses to be located on site	cl
<input checked="" type="checkbox"/>	_____	* Quantity and type of residential, if any	cl
<input checked="" type="checkbox"/>	_____	* Total land area of the site	c2
<input checked="" type="checkbox"/>	_____	* Total floor area, total disturbed area and ground coverage of each proposed Building and structure	c2
<input checked="" type="checkbox"/>	_____	* General summary of existing and proposed easements or other burdens	c3
<input checked="" type="checkbox"/>	_____	* Type, quantity and method of handling solid waste disposal	c4
<input checked="" type="checkbox"/>	_____	* Applicant's evaluation or evidence of availability of off-site public facilities, including sewer, water and streets (<u>refer to the wastewater capacity application – page 12</u>)	c5
<input checked="" type="checkbox"/>	_____	* Description of existing surface drainage and a proposed stormwater management plan or description of measures to control surface runoff.	c6

B

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PROJECT NARRATIVE..... PN-1

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10. Right, Title and Interest 12
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12. Recyclable Material 14
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C, 1

The following narrative has been prepared to address the written statement requirements of the City of Portland Site Plan Review Ordinance identified in Section 14-525(c) of the Land Use Ordinance and to provide additional information required by the City of Portland Development Review application requirements.

Applicant/Owner

Old Port Hospitality, LLC
11 Corporate Drive
Belmont, NH 03220
Phone: (603) 527-9484
Fax: (603) 527-9191
Email: gkirsch@opechee.com

Evidence of the applicant's right, title, and interest in the property is included in Section 10 of this narrative.

1. Proposed Uses and Site Design

The Hotel, Restaurant & Residences – Old Port is a three-unit mixed use condominium. The Hotel Unit will be a 122-room Hampton Inn with associated public circulation space, a continental breakfast area (for guests only), a meeting room, pool, fitness room, and customary “back office” support spaces. The Restaurant Unit will be an approximately 180-seat, 7,011 square foot restaurant located on the first floor located in the southwest corner of the building or the intersection of Fore Street and the Franklin Street Arterial. The Residences Unit will itself be a “sub-condominium” comprised of up to 12 condominium units to be located on the top (sixth) floor with its own separate lobby and elevator. The 1.75-acre site will be subdivided into two parcels: the 0.66-acre lot on which the condominium will be created and a 1.09-acre remainder lot that will be retained by the applicant for future development. The hotel and residences will have deeded parking easement rights to a dedicated surface parking lot on the remainder parcel, which will have 90 parking spaces comprised of regular single-car spaces and “tandem” two-car spaces for hotel valet parking. The applicant anticipated constructing structured parking on the remainder lot in conjunction with future development, and the parking easements for the hotel and residences will allow the applicant to temporarily provide off-site parking during construction and to ultimately provide structured parking spaces in lieu of the surface parking lot. Two handicap parking spaces will be located near the hotel’s main entrance/valet area.

The proposal will complement scale and land uses of the surrounding neighborhood. Pedestrian access around the subject block will be enhanced with the addition of sidewalk along the entire perimeter. The proposed uses are allowed within the B-3 Downtown Business Zone. The surface parking will require a Conditional Use Permit.

The plan’s emphasis on providing street level restaurant and hotel uses with residences on the upper stories maintains the historic and existing commercial character of Middle Street and is expected to add pedestrian traffic and vitality to the surrounding neighborhood. The restaurant is accessed from the intersection of Fore Street and the Franklin Street Arterial. The hotel and residences will each have separate entrances both located on Fore Street.

Pedestrian details and amenities are provided as a principal feature of the building and site architecture. A new pedestrian alley will be created as an alternative pathway from Franklin Arterial to Middle Street and the hotel entrance. The alley will lead pedestrians to the hotel entrance from Middle Street. The pedestrian will ascend a terraced stairway to emerge at the entrance of the hotel.

The automobile entrance to the hotel is from Fore Street, with guests arriving under a porte-cochere. Valet parking will be provided for hotel guests. Condominium residents will have a separate elevator/lobby area accessed from the parking area and Fore Street.

The sidewalk, pedestrian alley and streetscape design on the abutting roads are designed to enhance the pedestrian-oriented access to and around the building.

- The design enhances the pedestrian environment by providing a “through-block” connection between Middle and Fore Streets.
- The design provides multiple entry points to the building.

C.3

- The design provides a street-level restaurant with an outdoor eating area.
- The proposal includes significant streetscape improvements on all of the site's frontages.

C. 4

2. Land Area

The project site is approximately 1.75 acres in size and is identified as Block L, Lots 001, 002 and 003 on the City of Portland Tax Map 29. The site address was 38 India Street and was recently changed to 207 and 209 Fore Street. As discussed above, the subdivision plan divides the site into a 0.66-acre lot to accommodate the mixed-use project and a 1.09-acre lot for future development (and the surface parking lot associated with the project).

The site is bounded by Franklin Arterial to the west, Middle Street to the north, Fore Street to the south, and India Street to the east. The project site occupies the entire city block with the exception of the existing building at the southeast corner of Franklin Arterial and Middle Street, currently occupied by Hugo's Restaurant and the Pepper Club.

The following summarizes the proposed building floor space:

- The Restaurant will occupy:
7,011 sq.ft. on the first floor.

- The 122 room Hotel will occupy
9,893 sq.ft. on the first floor
15,990 sq.ft. on the second floor
15,880 sq.ft. on the third floor
15,880 sq.ft. on the fourth floor
15,880 sq.ft. on the fifth

- The Residential Condominiums (up to 12 units) will occupy
539 sq.ft. on the first floor
15,433 sq.ft. on the sixth floor

- Total gross building area = 96,953 sq.ft.

D,

3. Easements

An ALTA/ACSM survey is attached. There are two recorded easements on the site.

There is an easement from the City of Portland to Jordan Meats granting rights to the existing HVAC and other utilities encroaching into the Middle Street right-of-way. The easement is recorded in the Cumberland County Registry of Deeds on Page 40 of Deed Book 13982 and is dated July 1, 1998.

The second recorded easement is granted to the City of Portland for the sewer that traverses the project site. The easement is recorded in the Cumberland County Registry of Deeds on Page 258 of Deed Book 2438. The 24" sewer and easement run in a southerly direction to the center of the project site. The sewer changes size to 30" and turns to the south to connect to an existing sewer in Franklin Arterial. The applicant proposes to dedicate a new sewer easement to accommodate the separation of sewer and storm drainage lines, as shown on the plans and as addressed in detail in the Stormwater Management Plan prepared for the project.

The railroad tracks shown on the existing conditions plan are located in a former railroad easement crossing the southerly corner of the project site. This easement no longer exists, all rights having been released to Jordan's Meats by the Marion Corporation in Deed Book 4713 on Page 207 on December 15, 1980, and by the City of Portland in Deed Book 4714 on Page 309 on December 18, 1980.

A number of apparent encroachments and/or unrecorded easements have also been identified as part of the title research. No easements were found for the overhead electrical lines crossing through and over the project site or the gas line crossing the property. Both of these utilities are proposed to be relocated or abandoned as part of the project construction.

Proposed easements are identified on the subdivision plan included with this application.

E.

4. Solid Waste

Solid waste for the hotel, restaurant and residential condominiums will be placed in a trash area located on the north side of the building. This area will be screened. Hotel and restaurant waste will be collected by their staff on a daily basis. Residents will bring their trash to the trash area individually. The dumpster and recycling bins will be purchased or leased from a commercial waste removal vendor.

F.1

5. Availability of Off-Site Facilities

The project is located on the site of an existing industrial facility, in a fully developed area of the City. The existing meat processing facility on the site required significant utility infrastructure, including water and sewer flow, natural gas, electrical power, and telecommunications.

An ALTA/ACSM survey is attached to this application which indicates the location of existing utility infrastructure abutting the site.

Water

Existing utilities abutting this project site include water mains in Middle Street, Fore Street, India Street and Franklin Arterial. Water service is currently provided to the Jordan's Meats facility at the northeast corner of the site from an existing 8" main in India Street. The proposed building will tie into the main located on the Franklin Street Arterial. We have contacted the Portland Water District to request a letter verifying capacity to serve the project. The District's response, in which capacity is verified, will be forwarded promptly.

Natural Gas

Natural gas service is provided from existing mains in India Street and Middle Street. An existing gas main traverses the southwest corner of the site. There is no recorded easement associated with this main. We are coordinating the relocation of this main with Unitil as part of the proposed project improvements. A new gas service is proposed connecting to the existing main in Middle Street. We have requested an "Ability to Serve" letter. The letter will be forwarded promptly.

Storm Drainage

The site fronts on public right-of-ways in a fully developed urban area. The site and surrounding rights-of-way are served by a combined sewer system which drains south towards Fore Street and the Franklin Arterial. Runoff from the project site enters the municipal combined sewer system and is conveyed to combined sewer overflow structures in Franklin Arterial. Normal low flow discharges within the system are conveyed to an interceptor in Commercial Street and on to the City of Portland wastewater treatment plant. Combined overflows during large storm events are diverted to a 48" diameter combined sewer overflow drain that runs down the center of Franklin Arterial, eventually discharging to Casco Bay south of Commercial Street.

An existing 30" combined sewer traverses the project site in an easement running from Middle Street at its intersection with Hampshire Street to Franklin Arterial. The combined sewer traversing the site conveys to a diversion structure (structure 2780) in the northbound lanes of Franklin Arterial, north of Fore Street. The proposed project improvements include the relocation and separation of the combined sewer traversing the site, the abandonment of its easement, and the separation of storm drainage and sanitary sewers in Fore Street. The City of Portland has completed a combined sewer separation project for Hampshire Street. The proposed design maintains this separation and extends it to Franklin Arterial.

The stormwater management report included in this application addresses the design and analysis of the proposed storm drainage system in detail.

A new easement is being created as part of the Subdivision Plan in order to accommodate the relocation and separation of the drain and sewer lines.

Sanitary Sewer

Sanitary sewer service for the existing Jordan Meats facility is provided by two connections to an existing 12" combined sewer in Fore Street. Sanitary sewer service for the Hugo's building, as well as the Jordan Meats maintenance building is provided by two connections to the existing 30" combined sewer that traverses the site. Three new sanitary sewer connections are proposed as part of this project and as shown on the Grading and Utility Plans:

- One 6" sanitary sewer connection is proposed to serve the hotel and residences.
- Two 6" sewer connections are proposed to replace the existing Hugo's building sewer connections. The existing Hugo's building sewer service currently runs below the Jordan's Meats maintenance building to be demolished.

The new sewer services will connect to the new sewer main that will cross the site. A new easement is being created as part of the Subdivision Plan in order to accommodate the relocation and separation of the drain and sewer lines. A Wastewater Capacity Application has been completed and is included as part of the submission material.

Electrical Power

Existing overhead power is provided on pole lines within the public right-of-way abutting all sides of the project site. The existing electrical service to the site is provided from Middle Street and feeds transformers owned by Central Maine Power which are located within an alcove of the existing building. The transformers are accessed by a gated entrance along Middle Street. The proposed electric service will be brought to the proposed development from an existing pole on Fore Street underground to a transformer located on the northern side of the building. Central Maine Power has provided an "Ability to Serve" letter.

Telecommunications

Fairpoint's telecommunications facilities are located within India Street. The existing building is served by underground service from an existing manhole at the intersection of India Street and Middle Street. The proposed service will be brought from an existing pole on Middle Street to the building via underground conduit. Fairpoint has provided an "Ability to Serve" letter.

Cable television is provided by Time Warner Cable from existing overhead services within the abutting right-of-way. The proposed service will be brought from an existing pole on Middle Street to the building via underground conduit. Time Warner Cable has provided an "Ability to Serve" email.

F.3

FairPoint
communications

Fairpoint Communications
Engineering Dept.
5 Davis Farm Rd
Portland, Me. 04103
February 11th, 2008

Steve Long
Opechee Construction Corporation
11 Corporate Dr
Belmont, NH 03220

To whom it may concern:

Fairpoint Communications does have the ability to service the proposed buildings located at 207 and 209 Fore St. Portland, Me. per the Public Utilities Commission Tariff.

Sincerely,
John Caprio
Engineer
Fairpoint Communications

F.4



Central Maine Power

February 10, 2010

Mr. Steve Long
C/O Opechee Construction
11 Corporate Drive
Belmont, NH 03220

RE: Hotel, Restaurant & Residences, Old Port, Portland

Dear Mr. Long,

This letter is to advise you Central Maine Power has sufficient three phase electrical capacity in the area to serve the subject project.

Once the project is accepted by the City of Portland, the owner will need to call our Customer Service Center at 1-800-565-3181 to sign up for a New Account and a Work Request Order so we may start a cost estimate.

I will need to know what size service and voltage the Customer will be requiring in order to get you the needed information for costs and easement possibilities.

If you have any questions please feel free to call me at 828-2882.

Sincerely,

Paul DuPerre
Energy Service Specialist

An equal opportunity employer

162 Canco Rd. | Portland, ME 04103

www.cmpco.com

An Energy East Company

F.5

Steve Long

From: Pelletier, Mark [mark.pelletier@twcable.com]
Sent: Thursday, February 11, 2010 1:52 PM
To: Steve Long
Subject: RE: Hotel, Restaurant & Residences - Old Port
Steve,

Time Warner Cable does have services on the poles on Middle Street and can provide services to your project.

All conduits will need to be in place by the Contractor and included in your project costs. All cable outlets will also need to be prewired during construction.

Time Warner Cable will provide main line cable service from Middle St, all labor to pull cable thru conduits from street and into electric rooms and or hub locations.

Mark

Mark Pelletier
Constuction Project Coordinator
Time Warner Cable
Portland, Maine
207-253-2324
mark.pelletier@twcable.com

From: Steve Long [mailto:stevel@opechee.com]
Sent: Wednesday, February 10, 2010 9:23 AM
To: Pelletier, Mark
Subject: Hotel, Restaurant & Residences - Old Port

Mark

You recently met with Dana Adams from our office regarding providing service to the project shown below. We are submitting plans February 16 to the City of Portland for site plan review. As part of our submission I would like to have "Ability to Serve" letters from utilities providing service to the site. I just need a simple letter or email stating that you can and will provide service when we are ready to move forward. We are currently working on the plans. I will forward a set of plans when they are complete so that we can finalize the connections. Please call me if you have any questions.

Thanks

Steve Long

2/11/2010

F. 6

CITY OF PORTLAND WASTEWATER CAPACITY APPLICATION

Department of Public Services,
55 Portland Street,
Portland, Maine 04101-2991



Mr. Frank J. Brancely,
Senior Engineering Technician,
Phone #: (207) 874-8832,
Fax #: (207) 874-8852,
E-mail: fjb@portlandmaine.gov

Date: February 8th, 2010

1. Please, Submit Utility, Site, and Locus Plans.

Site Address: 207 & 209 Fore Street (Changed from 38 India Street)
(Regarding addressing, please contact Leslie Kaynor, either at 756-8346, or at LMK@portlandmaine.gov) Chart Block Lot Number: 029 1003001 & 029 1001001

Proposed Use: Hotel, Restaurant, & Residences
Previous Use: Meat Packing Factory
Existing Sanitary Flows: Year 2004 148,640 GPD
Existing Process Flows: Incalculable GPD

Site Category	Commercial	<u>X</u>
	Industrial <i>(complete part 4 below)</i>	_____
	Governmental	_____
	Residential	<u>X</u>
	Other <i>(specify)</i>	_____

Description and location of City sewer, at proposed building sewer lateral connection:
The City's sewer main will pass through the site from Middle Street to down gradient to Fore Street. Sewer lateral connections to the sewer main will be on-site. Please see the Utility plan.
Clearly, indicate the proposed connection, on the submitted plans.

2. Please, Submit Domestic Wastewater Design Flow Calculations.

Estimated Domestic Wastewater Flow Generated: 26,945 GPD
Peaking Factor/ Peak Times: The Hotel contributes 79% of total flow and will peak between 6am to 8am.
Specify the source of design guidelines: *(i.e. "Handbook of Subsurface Wastewater Disposal in Maine," "Plumbers and Pipe Fitters Calculation Manual," Portland Water District Records, Other (specify))*

Proposed flows are from "Subsurface Wastewater Disposal Rules" & Existing flows are from the Portland Water District records
Note: Please submit calculations showing the derivation of your design flows, either on the following page, in the space provided, or attached, as a separate sheet.

3. Please, Submit Contact Information.

Owner/Developer Name: Old Port Hospitality, LLC
Owner/Developer Address: 11 Corporate Drive, Belmont, NH
Phone: 603-527-9090 Fax: 603-527-9191 E-mail: gregk@opechee.com
Engineering Consultant Name: Opechee Construction Corporation Attn: Steve Long, P.E.
Engineering Consultant Address: 11 Corporate Drive, Belmont, NH
Phone: 603-527-9090 Fax: 603-527-9191 E-mail: stewel@opechee.com
City Planner's Name: _____ Phone: _____

Note: Consultants and Developers should allow +/- 15 days, for capacity status, prior to Planning Board Review.

4. Please, Submit Industrial Process Wastewater Flow Calculations

Estimated Industrial Process Wastewater Flows Generated: Not Applicable GPD
Do you currently hold Federal or State discharge permits? Yes _____ No _____
Is the process wastewater termed categorical under CFR 40? Yes _____ No _____
OSHA Standard Industrial Code (SIC): _____
(http://www.osha.gov/oshstats/sicser.html)
Peaking Factor/Peak Process Times: _____

Note: On the submitted plans, please show the locations, where the building's sanitary, and process water sewer laterals, exit the facility, where they enter the city's sewer, the location of any control manholes, wet wells, or other access points, and the locations of any filters, strainers, or grease traps.

Notes, Comments, or Calculations:

Restaurant

158 seats (x) 20 gpd = 3,160 gpd
22 bar stools (x) 10 gpd = 220 gpd
20 employees (x) 15 gpd = 300 gpd
Total = 3,680 gpd

Hotel

34 single beds (x) 100 gpd = 3,400 gpd
36 single beds w/ pullout sofa (x) 200 gpd = 7,200 gpd
52 double beds (x) 200 gpd = 10,400 gpd
15 Employees (x) 15 gpd = 225 gpd
Total = 21,225 gpd

Residences

10 two bedrooms (x) 180 gpd = 1,800 gpd
2 one bedroom (x) 120 gpd = 240 gpd
Total = 2,040 gpd

Restaurant = 3,680 gpd (13%)
Hotel = 21,225 gpd (79%)
Residences = 2,040 gpd (8%)

Proposed Total Design Flow = 26,945 gpd

Jordan's Meat Packing Factory Existing Flows

Portland Water District claims that the last year the factory was operating at full capacity was 2004
Water usage from Dec 30th, 2003 to Dec. 23, 2004 was 7,194,000 cubic feet. Two sub-meters read
61,000 cubic feet was not discharged to the sewer.

$7,194,000 - 61,000 = 7,133,000$ cubic feet / 359 days = 19,869 cubic feet per day (x) 7.481 gallons per cubic foot

Existing Average daily demand = 148,640 gpd

5.1

6. Stormwater Drainage

A Stormwater Management Report, which has been prepared to compare pre-development and post-development stormwater flows and to verify off-site system capacity, is attached.

STORMWATER MANAGEMENT PLAN

Hotel Restaurant and Residences- Old Port
Former Jordan's Meats Site
207-209 Fore Street
Portland, Maine

Prepared for

Old Port Hospitality, LLC
11 Corporate Drive
Belmont, NH 03220

Prepared By

Sebago Technics, Inc.

February 16, 2010

05090

STORMWATER MANAGEMENT PLAN**Hotel Restaurant and Residences- Old Port
Former Jordan's Meats Site
Portland Maine****EXECUTIVE SUMMARY**

Sebago Technics, Inc. has prepared this Stormwater Management Plan to evaluate stormwater drainage for the proposed Hotel, Restaurant and Residences-Old Port project at 207 and 209 Fore Street on the site of the former Jordan's Meats facility in Portland, Maine. The project site occupies nearly the entire city block bounded by Fore Street, Middle Street, India Street and Franklin Arterial, with the exception of an existing two-story building at the southeast corner of Franklin Arterial and Middle Street, currently occupied by Hugo's Restaurant and the Pepper Club.

The property is approximately 1.75 acres in size and is currently fully developed with two buildings, paved parking and loading dock aprons that comprise the former Jordan's Meats facility. The proposed development consists of a mixed use development including a 122 room hotel, restaurant, 12 residential condominiums, surface parking and site improvements.

The project is required to meet the Stormwater Management Standards found in Section V of the City of Portland Technical Design Standards and Guidelines which adopts the Maine Department of Environmental Protection (MDEP) Chapter 500 Stormwater Management Rules (Chapter 500). The project includes the re-development of existing impervious area that was in existence as of November 16, 2005. As such, the project qualifies for an exemption from the Chapter 500 General Standards for stormwater quality treatment in accordance with Chapter 500.4.B.(3)(f) Stormwater Management Law Project Including Redevelopment.

An existing 30" combined sewer traverses the project site in an easement running from Middle Street at its intersection with Hampshire Street to Franklin Arterial. The proposed project improvements include the relocation and separation of the combined sewer traversing the site, the abandonment/relocation of its easement, and the separation of storm drainage and sanitary sewers in Fore Street. The City of Portland has completed a combined sewer separation project for Hampshire Street. The proposed design maintains and extends this separation to Franklin Arterial.

The proposed redevelopment will reduce impervious cover and peak rates of runoff from the site when compared to November 2005 conditions and stormwater peak flow control is not required. Stormwater quality treatment to reduce oil, grit and sediment from the proposed parking lot is provided through the use of catch basins with 3' deep sumps and outlet hoods. Discussions with City of Portland Public Services Staff indicates that this approach will provide appropriate treatment for this project in accordance with the City's Standards.

STORMWATER MANAGEMENT PLAN

Hotel Restaurant and Residences- Old Port Former Jordan's Meats Site Portland, Maine

1. Introduction

This Stormwater Management Plan has been prepared to address the potential impacts associated with this project due to the proposed modification of stormwater runoff characteristics. The analysis has been developed to represent the existing combined sewer system abutting and traversing the project site, to size the proposed separated storm drainage system, and evaluate the pre-development and post-development peak rates of runoff at the project study points.

2. Existing Conditions

The project plans include an ALTA/ACSM land title survey that includes a detailed survey of the existing drainage infrastructure on and abutting the site. The hydrologic modeling of existing storm drainage system and diversion structures is based on this survey and research in the City of Portland Public Services Department's archives. The structure numbers, referenced below, are indicated on this survey plan and in the HydroCAD model.

The site fronts on public right-of-ways in a fully developed urban area. The site and surrounding rights-of-way are served by a combined sewer system which drains south towards Fore Street and the Franklin Arterial. Runoff from the project site enters the municipal combined sewer system and is conveyed to combined sewer overflow structures in Franklin Arterial. Normal low flow discharges within the system are conveyed to an interceptor in Commercial Street and on to the City of Portland wastewater treatment plant. Combined overflows during large storm events are diverted to a 48" diameter combined sewer overflow drain that runs down the center of Franklin Arterial, eventually discharging to Casco Bay south of Commercial Street.

An existing 30" combined sewer traverses the project site in an easement running from Middle Street at its intersection with Hampshire Street to Franklin Arterial. The combined sewer traversing the site conveys to a diversion structure (structure 2780) in the northbound lanes of Franklin Arterial, north of Fore Street. The proposed project improvements include the relocation and separation of the combined sewer traversing the site, the abandonment of its easement, and the separation of storm drainage and sanitary sewers in Fore Street. The City of Portland has recently completed a combined sewer separation project for Hampshire Street. The proposed design maintains this separation and extends it to Franklin Arterial.

3. **Proposed Development and Study Points**

The redevelopment for the site will include the removal of the existing buildings and parking areas on the site and replace them with a new building, parking utilities and landscaped areas. The redevelopment of the site will reduce impervious cover by approximately 20,000 square feet compared to November 2005 conditions.

Three study points are identified in the hydrologic model and on the watershed maps.

Study Point SP-1

Runoff from approximately 32 acres to the north of the project site drains to the combined sewer in India Street, eventually reaching existing sanitary manhole (ESMH 1520) south of the intersection of India and Fore Streets.

This structure is represented as Study Point SP-1 and the tributary watershed is shown as Watershed 1 in the analysis.

Runoff from the project site is directed away from this study point. This sub-catchment was analyzed to study the possibility of directing drainage from the project to the existing drainage system in India Street. No such connection is currently proposed. Therefore, runoff at the study point does not change from the pre-development condition to the post development condition.

Study Point SP-2

Study Point SP-2 represents the 30” diameter outlet of existing sanitary manhole ESMH 1149 located in the northbound lanes of Franklin Arterial at the intersection of Fore and Franklin Streets.

Runoff from the eastern half of the project site, shown as Subcatchment 4, includes runoff from the main Jordan’s Meats building, existing loading docks, and the Fore Street right-of-way. This watershed drains southeasterly to existing catch basins and a 12” combined sewer in Fore Street. The Fore Street combined sewer drains southwesterly to a sanitary manhole (SMH-1149) and enters an existing 30” combined sewer at the intersection of Franklin and Fore Streets. This sewer drains from structure 1149 towards Commercial Street and is represented as Study Point SP-2 in the analysis.

New separated storm drain and sewer system is proposed in Fore Street as part of the project improvements. The proposed storm drain will separate all storm drainage from the eastern half of the project site and all of Fore Street between India Street and Franklin Arterial from the existing combined sewer system. Storm drainage will be directed to the existing 48” combined sewer overflow drain in Franklin Arterial identified as Study Point SP-3.

Study Point SP-3

Study Point SP-3 represents the existing 48" combined sewer overflow pipe installed in the center of Franklin Arterial. This drain is the outlet of a large existing drain manhole structure (structure 2784) in the Franklin Arterial median at Fore Street. Combined sewer overflows from the diversion structures at the intersection of Franklin and Middle Streets (structure 2622) and north of the intersection of Franklin and Fore Streets (structure 2780) combine with surface runoff at structure 2784. The outlet of structure 2784 runs east to Commercial Street and eventually discharges to Casco Bay.

Runoff from approximately 24 acres of land northwest of the project site drains to combined sewers located at the intersection of Hampshire and Middle Streets. A 30" combined sewer traverses the project site and enters diversion structure 2780 immediately up-gradient of Study Points SP-2 and SP-3.

Watershed 3 represents areas northwest of the site, extending as far as Congress Street and Market Street. Combined sewers in this watershed drain south and east to the existing combined sewer diversion structure (structure 2622) in the intersection of Franklin and Middle Streets. Normal (low) flow from structure 2622 continues east along Middle Street to Hampshire Street, then across the project site, eventually reaching Study Point SP-2. Overflows from structure 2622 are diverted south to structure 2784 and Study Point SP-3.

Watershed 2 represents runoff in the separated storm drainage system in Hampshire Street.

The Hampshire Street storm drains and sanitary sewers recombine and connect with existing combined sewers in Middle Street (watersheds 3A, 3B and 3C), low flow runoff from Watershed 3 (diversion structure 2622) at existing sanitary manhole ESMH-2317 immediately north of the project site. A combined sewer runs southeast from ESMH-2317 across the Jordan's Meats site, turns southwest at manhole ESMH-1339, and connects into diversion structure 2780 located in the northbound lanes of Franklin Street, north of Fore Street. Low flow from structure 2780 continues to Study Point SP-2, and overflows are diverted to structure 2784 and Study Point SP-3.

Watershed 5 (pre-development) represents approximately 20,000 square feet of existing building and pavement at the southwest corner of the site that drains to the combined sewer traversing the site.

The project improvements include the construction of new storm drains and sanitary sewers in Middle Street that will divert the separated storm drain and sanitary sewers at the Hampshire/India intersection south, across the site, to Fore Street, and then west to Franklin Street.

The proposed separated storm drain will cross the site to Fore Street and then west to Franklin Street, connecting into the 48" combined sewer overflow down gradient of structure 2784 (Study Point SP-3). This is a dedicated overflow line that drains to Commercial Street and Casco Bay.

The proposed sanitary sewer will cross the site to Fore Street and then west to connect into existing manhole ESMH-1149 (Study Point SP-2).

Watershed 6 represents approximately 4 acres of off-site land to the southwest of the site, tributary to structure 2784 and Study Point SP-3 at the intersection of Franklin Street and Fore Street. This area is unaffected by the proposed development and is included for capacity analysis.

The enclosed pre-development watershed map depicts the general drainage patterns at the project site.

4. **Stormwater Management**

Soils

Soil classifications within the project area were referenced from the Cumberland County Medium Intensity Soil Survey.

The site is comprised entirely of Hinckley gravelly sandy loam. The Hinckley soil series consists of gravelly sandy loam with low runoff potential and high infiltration rates. The soil is classified by the Soil Conservation Service as a Group A hydrologic soil.

Stormwater Analysis

In order to evaluate drainage characteristics in pre-development and post-development conditions, a quantitative analysis was performed to determine peak rates of runoff for the 2, 10 and 25-year storm events. Runoff calculations were performed following the methodology outlined in the USDA Soil Conservation Service's "Urban Hydrology for Small Watersheds, Technical Release #55" and HydroCAD Stormwater Modeling System software.

The 24-hour rainfall values used in the hydrologic model are as follows.

Storm Frequency Precipitation (in./24 hr)	
2-year	3.0
10-year	4.7
25-year	5.5

Drainage structures were modeled as catch basins with culvert outlets. Overflow structures were modeled with multiple outlets, according to survey information and the City of Portland Inflow-Infiltration Analysis maps.

Nine sub-watersheds were analyzed in pre-development and twelve watersheds were modeled in the post-development conditions. Three study points, corresponding to existing storm drains and combined sewers, were selected to evaluate the effects of the development on stormwater runoff. The sub-watershed boundaries, time of concentration

flow paths, and routing element locations are shown on the attached pre-development and post-development watershed maps.

The following table summarizes the results of the analysis. Computer generated data sheets and hydrographs are provided in the subsequent sections of this report.

Table 1 - Stormwater Runoff Summary Table Pre-Development vs. Post-Development						
Study Point	Peak Rates of Runoff (cfs)					
	2-Year		10-Year		25-Year	
	Pre	Post	Pre	Post	Pre	Post
SP-1	49.1	49.1	88.6	88.6	107.1	107.1
SP-2	29.7	24.8	37.5	32.1	42.6	33.4
SP-3	28.3	28.8	64.6	64.5	79.4	82.9

Study Point 1

The peak rate of runoff at Study Point SP-1 is unaffected by the proposed development.

Study Point SP-2

Study Point SP-2 represents the discharge in the 30" diameter combined sewer outlet of existing sanitary manhole ESMH-1149 located in the northbound lanes of Franklin Street at the intersection of Franklin Street and Fore Street

The analysis summarized in Table 1 indicates a decrease in peak runoff rates in all design storm events at this point. The decrease in runoff is the result of the proposed storm drainage construction which effectively separates 6.7 acres of existing urban development (post-development Watersheds 2, 4, 4A, 4B, 4C, and 5) from the combined sewer system.

Study Point SP-3

The results of the analysis indicate an increases in the peak rate of runoff at Study Point 3. This increase is due to the storm drainage separation proposed as part of the project. Study Point 3 represents the existing 48" combined sewer overflow drain outlet from structure 2784 that is designed to convey storm overflows from upstream diversion structures out of the combined sewer system.

5. Erosion and Sedimentation Control

An Erosion and Sedimentation Control Plan has been developed for the project site placing emphasis on the installation of sedimentation barriers to minimize erosion potential from development activities during and after construction. The project construction does not involve the construction of roadways to serve the site; therefore, the erosion control plan focuses on measures to protect the existing catch basins during construction. The Erosion Control Plan has been placed directly on the design plans to include locations of erosion control provisions (i.e., silt fence, inlet protection and construction entrances), along with a narrative and construction details for reference by the contractor during construction.

The incorporation of these measures and drainage provisions meets the standard for stormwater runoff for the proposed site development such that downstream properties will not be adversely impacted by the development.

Prepared by,

SEBAGO TECHNICS, INC.

Jayson Haskell
Design Engineer

Daniel L. Riley, P.E.
Senior Project Manager

DLR
February 16, 2010