

# GENERAL NOTES

1. THE BUILDING SHALL BE CONSTRUCTED TO CONFORM WITH ALL CURRENT APPLICABLE CODES INCLUDING, BUT NOT LIMITED TO, THE LATEST EDITIONS OF IBC, IECC, NFPA 101, ANSI, UFAS, HUD/AG, ADA/AG, MFS.
2. ALL WOOD IN CONTACT WITH CONCRETE SHALL BE PRESURE PRESERVATIVE TREATED.
3. CONTRACTOR SHALL WORK FROM GIVEN DIMENSIONS AND LARGE SCALE DETAILS ONLY. DO NOT SCALE THE DRAWINGS.
4. INSTALL BLOCKING FOR SURFACE APPLIED FIXTURES, TRIM, CABINETS, COUNTER TOPS, AND GRAB BARS WHEN MOUNTED ON STUD WALLS, INCLUDING ALL FUTURE INSTALLATIONS.
5. ALL GRAB BARS SHALL BE ABLE TO SUPPORT A DEAD WEIGHT OF 250 LBS AT ANY POINT. (BY TENANT)
6. INSTALL MOISTURE RESISTANT GYPSUM BOARD IN HOUSEKEEPING, UTILITY, STORAGE, SPRINKLER AND ALL OTHER HIGH HUMIDITY AREAS, INCLUDING UNIT BATHROOMS. MOISTURE RESISTANT FIRECODE G.W.B. @ ALL FIRE RATED WALLS COMMON WITH HOUSE UTILITY AND UNIT BATH ROOMS. SHOWERS ALL SIDES TO HAVE DURAROCK BACKING.
7. ALL CAULKING AROUND WINDOWS SHALL BE NON-HARDENING TYPE SEALANT.
8. INSTALL U.L. SLEEVE FIRE-STOPPING SYSTEM & FIRE RATED SEALANTS EQUAL TO THE FIRE RATED FLOOR, CEILING AND WALL ASSEMBLY.
9. FIRESTOP VERTICAL MECHANICAL CHASES @ FLOOR & CEILING U.L. RATED PENETRATIONS. CAULK JOINTS. COORDINATE AND FLASH ALL ROOF/WALL PENETRATIONS WITH THE SUB-TRADES.
10. CONTINUE SEPARATION WALLS TO UNDERSIDE OF FLOOR/CEILING ABOVE. UNIT TO UNIT, UNIT TO CORRIDOR, CORRIDOR WALLS, STORAGE WALLS, ELEVATOR, STAIRWELL WALLS, ETC.
11. REPORT ANY AND ALL DISCREPANCIES TO THE ARCHITECT PRIOR TO PROCEEDING WITH WORK.
12. PROJECT ASSUMES ALL EXISTING HAZARDOUS MATERIALS HAVE BEEN REMOVED PRIOR TO THE COMMENCEMENT OF WORK. ALL HAZARDOUS MATERIAL REMOVAL IS THE RESPONSIBILITY OF THE OWNER.
13. DESIGNATED EGRESS WINDOWS IN SLEEPING ROOMS SHALL MEET THE FOLLOWING MINIMUM CLEAR OPENING REQUIREMENTS OF 22" WIDE, 24" HIGH & 5.7 SF CLEAR OPENING. ALL BEDROOMS SHALL CONTAIN A MINIMUM OF (1) EGRESS OPENING.
14. WINDOW SAFETY GLAZING SHALL BE LOCATED AS FOLLOWS, BUT NOT LIMITED TO:  
ALL WINDOWS WITHIN 60" HORIZ. & VERT. DIMENSION OF STAIRS MUST BE TEMPERED.  
ALL WINDOWS WITHIN 24" OF A DOOR SWING MUST BE TEMPERED.  
ALL WINDOWS WITHIN A SHOWER ENCLOSURE MUST BE TEMPERED.
15. BUILDING INSULATION SHALL BE PROVIDED AS INDICATED & NOTED IF NOT SHOWN IN ITS ENTIRETY THROUGHOUT THE DRAWING SET. INSULATE ALL BATHROOM, LAUNDRY, MECH. ROOM WALLS, AND ROOF DRAINS THRU BUILDING.
16. BEAM, JOIST OR OTHER STRUCTURAL MEMBER PENETRATIONS NOT SHOWN OR INDICATED ON DRAWINGS MUST BE CONSULTED WITH THE ARCHITECT & OR ENGINEER PRIOR TO WORK.
17. ANY CHANGE DURING CONSTRUCTION OF USE, OCCUPANCY OR CONSTRUCTION TYPE MUST BE DISCUSSED WITH ARCHITECT PRIOR TO ANY WORK PERFORMED AND SUBSTANTIAL TIME ALLOWED FOR REQUIRED CODE RESEARCH AND DRAWING MODIFICATIONS OR ADDITIONS.

## WALL LEGEND

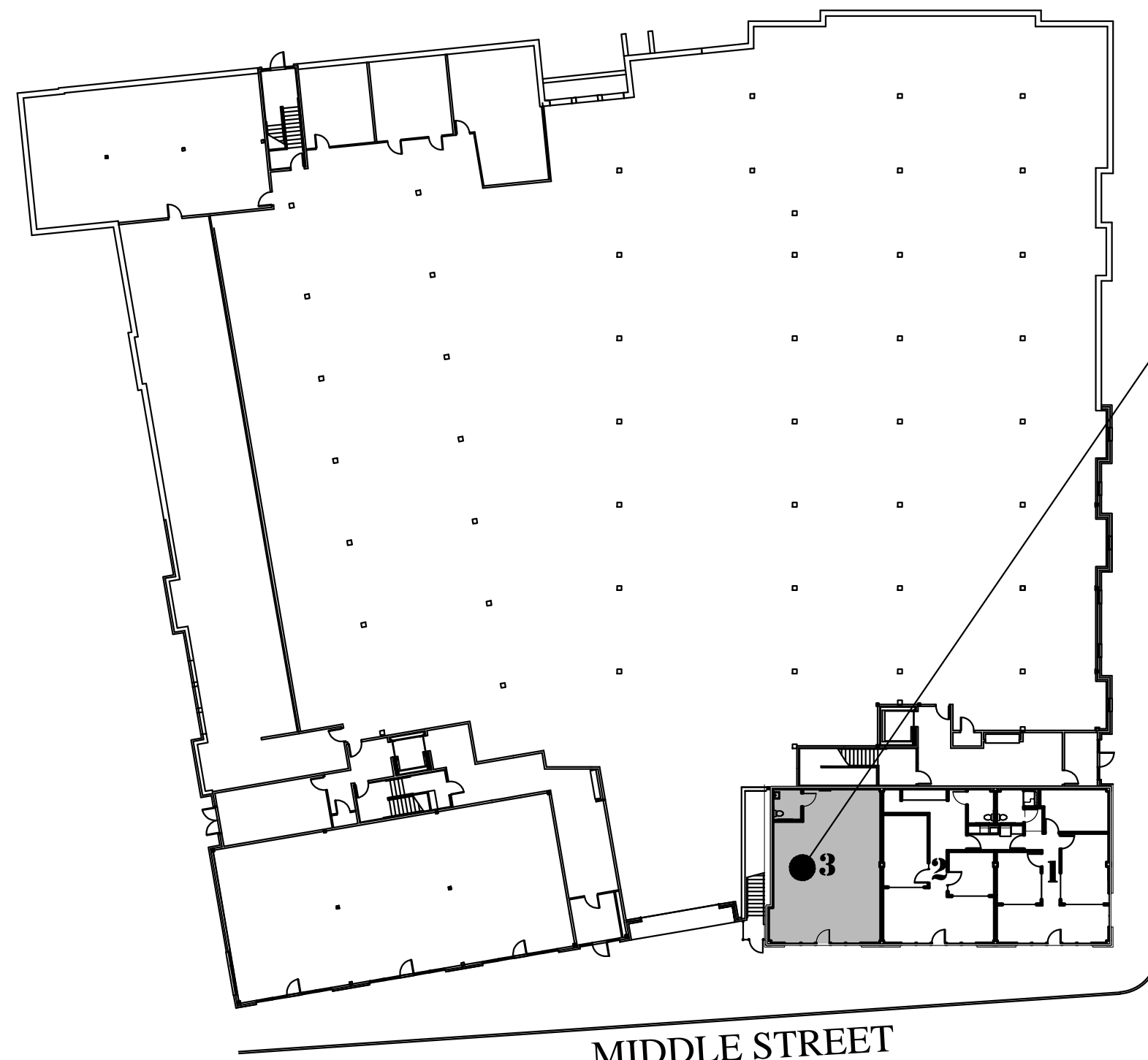
EXISTING WALL	=====
NEW WALL	=====
FIRE RATED WALL	=====
BRICK WALL	=====
1 HOUR	=====
EXISTING WALL TO BE REMOVED	-----

## LIFE SAFETY LEGEND:

	EMERGENCY BACKUP LIGHT
	EXIT LIGHT
	PHOTOELEC SMOKE DETECTOR
	HEAT & CO DETECTOR
	HORN/STROBE
	STROBE
	FIRE EXTINGUISHER IN RECESSED CABINET
	PULL STATION - SPRINKLER SYSTEM

## LIFE SAFETY NOTES:

- LIFE SAFETY AND FIRE ALARM SYSTEMS SHALL BE DESIGNED AND INSTALLED PER NFPA 72, BY A CITY/STATE APPROVED CONTRACTOR WITH A SEPARATE PERMIT.
- ALL STROBES TO BE 30 CANDELA UNLESS OTHERWISE NOTED.
- SYSTEM SHALL BE SUPERVISED BY AN OFF-SITE CERTIFIED BY THE CITY OF PORTLAND THIRD PARTY
- EXISTING LIFE SAFETY SYSTEM EQUIPMENT SHALL REMAIN, TYP EACH SIDE



## KEY PLAN

1/16"=1'-0"

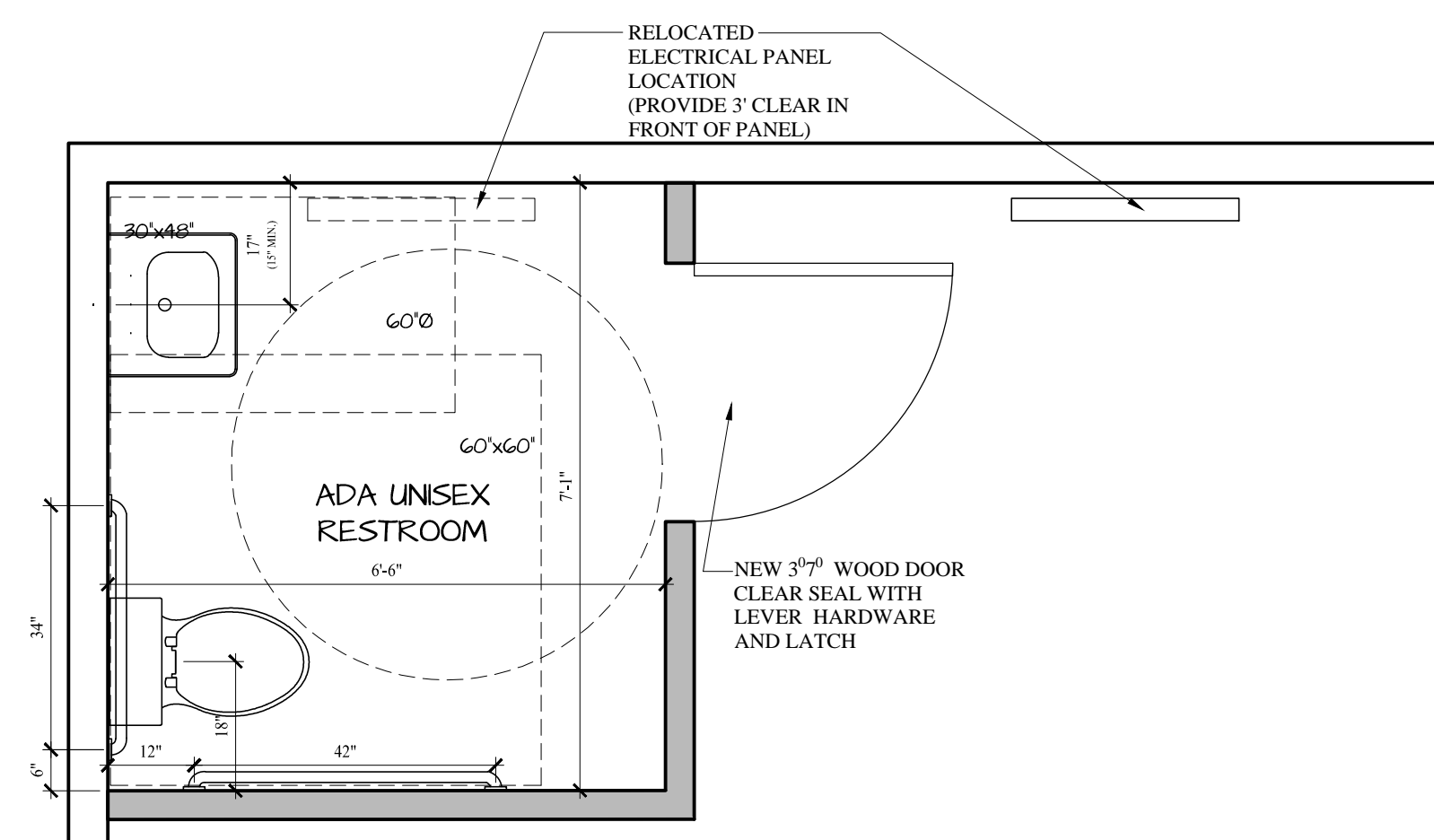
## PROJECT NOTES

1. SCOPE OF WORK: SHALL INCLUDE THE INTERIOR CONSTRUCTION OF AN ADA RESTROOM WITHIN SUITE 3 AT 22 HANCOCK STREET. HISTORIC PRESERVATION APPROVAL IS NOT REQUIRED, NO EXTERIOR WORK.
3. ALL NEW WORK SHALL CONFORM TO THE CITY OF PORTLAND INSPECTIONS, FIRE, ZONING & HISTORIC PRESERVATION OFFICES.
4. THE GENERAL CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FOR THE CONSTRUCTION, INCLUDING BUT NOT LIMITED TO: STREET CLOSINGS, ELECTRICAL, MECHANICAL, SPRINKLER, ETC. AS REQUIRED AND NECESSARY.
5. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR ALL INTERIOR WORK AS NOTED ON PLANS. WORK SHALL ALSO INCLUDE:
  - a. NFPA 13 SPRINKLER SYSTEMS PER THE FLOOR PLANS.
  - b. MECHANICAL SYSTEMS (HVAC)
  - c. ELECTRICAL
  - d. PLUMBING
6. MECHANICAL/ ELECTRICAL INSTALLATIONS ARE TO BE PERFORMED ON A DESIGN-BUILD BASIS UNLESS NOTED OTHERWISE WITHIN PLANS. ALL SYSTEMS SHALL BE ENGINEERED AND CERTIFIED BY A STATE OF MAINE PROFESSIONAL ENGINEER.
7. FIRE ALARM WORK SHALL BE INSTALLED ACCORDINGLY TO MEET NFPA 72, CITY OF PORTLAND & STATE FIRE MARSHAL REQUIREMENTS.
8. ALL FIRE RATED WALL PENETRATIONS SHALL BE UL APPROVED.
9. VERIFY EXISTING SPRINKLER HEAD COVERAGE PER NEW PLAN. ADD NEW SPRINKLERS HEAD AS REQUIRED PER NFPA 72.
10. CONTACT THE ARCHITECT PRIOR TO WORK SHOULD DISCREPANCIES BE DISCOVERED DURING THE BUILDING PROCESS.

IBC 2009: CODE DATA	
OCCUPANT LOAD	T.B.D.
USE GROUP CLASSIFICATION	VACANT
TYPE OF CONSTRUCTION	SEPARATED MIXED USE - EXISTING TO REMAIN
TOTAL AREA OF WORK	912 SF
ACTUAL BUILDING AREA	E.T.R.
BUILDING HEIGHT	E.T.R.
BUILDING AREA LIMITATION - SQ. FT. (TABLE 503)	E.T.R.
BUILDING HEIGHT LIMITATION - STORIES (TABLE 503)	5
STREET FRONTAGE INCREASE (150%) - SQ. FT.	E.T.R.
AUTOMATIC SPRINKLER SYSTEM AREA INCREASE (200%) - SQ. FT.	E.T.R.
AUTOMATIC SPRINKLER SYSTEM HEIGHT INCREASE (1 STORY)	--
EXIT ACCESS TRAVEL DISTANCE (ALLOWABLE/ ACTUAL)	36
COMMON PATH OF TRAVEL (ALLOWABLE/ ACTUAL)	E.T.R.
FIRE SUPPRESSION:	YES - E.T.R.
EXIT ACCESS FIRE RATINGS:	
FIRE WALLS:	
PARTY WALLS	E.T.R.
STAIR ENCLOSURES	E.T.R.
SHAFTS	E.T.R.
EXIT ACCESS CORRIDORS	E.T.R.
FIRE PROTECTION OF STRUCTURE:	
PRIMARY STRUCTURAL FRAME	E.T.R.
LOAD BEARING WALLS - EXTERIOR	E.T.R.
NON-LOAD BEARING WALLS - EXTERIOR	E.T.R.
LOAD BEARING WALLS - INTERIOR	E.T.R.
NON-LOAD BEARING WALLS - INTERIOR	E.T.R.
FLOOR CONSTRUCTION & SECONDARY MEMBERS	E.T.R.
ROOF CONSTRUCTION & SECONDARY MEMBERS	E.T.R.
GENERAL NOTES	
OCCUPANT LOAD TENANT SPACE: T.B.D.	

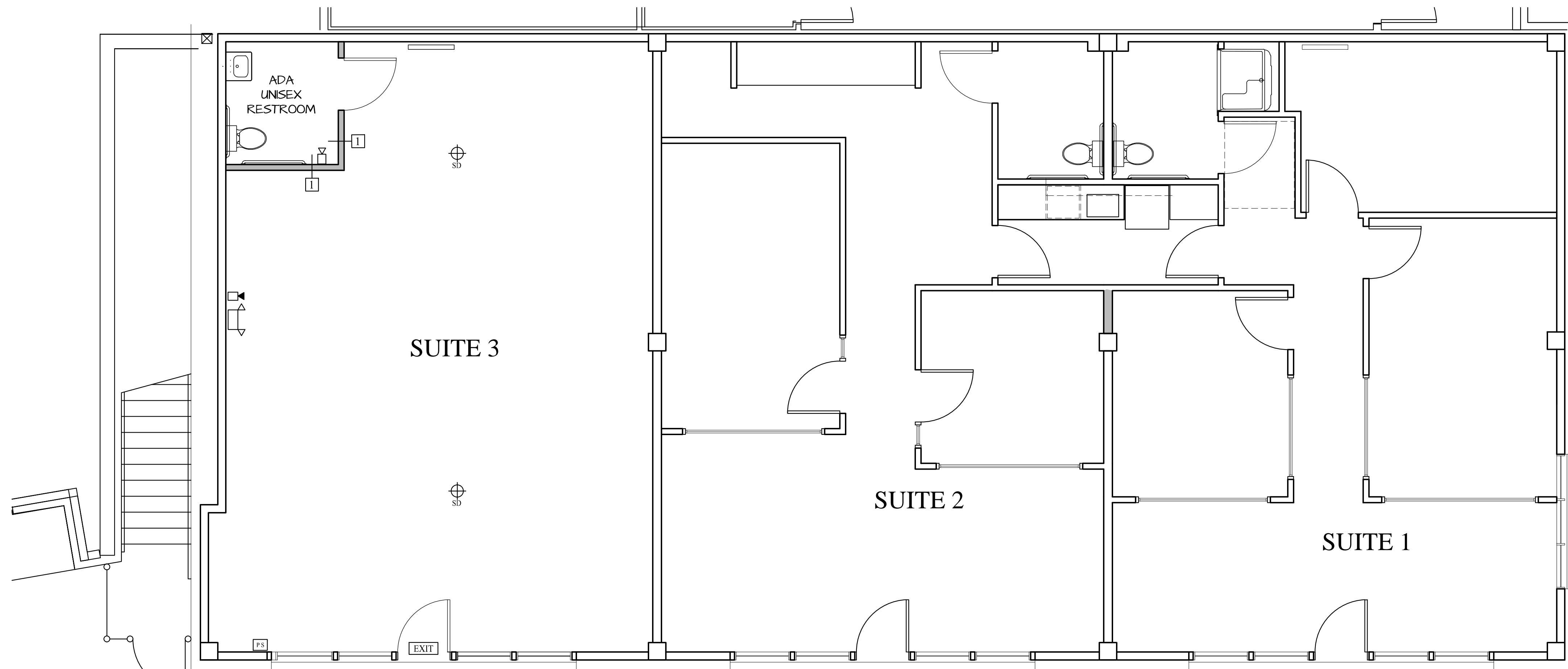
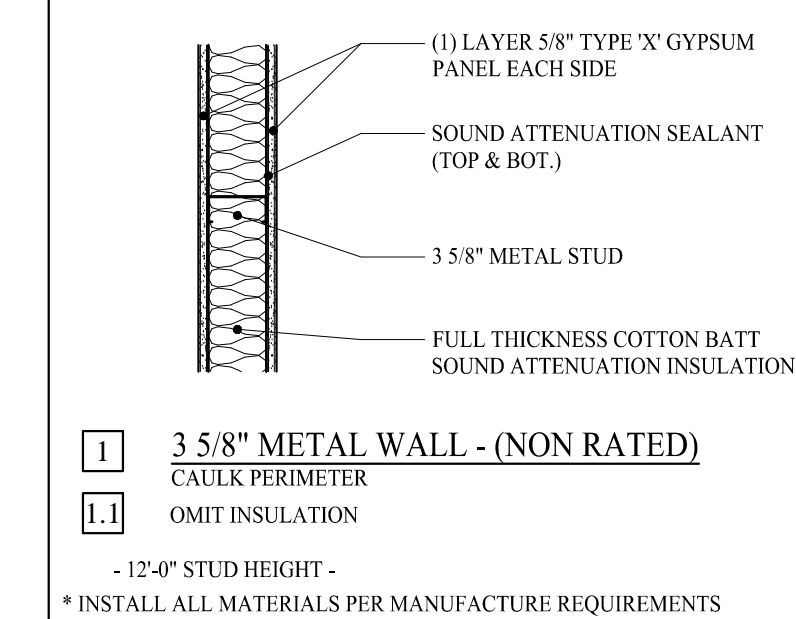
NFPA 101 2009 & NFPA 220 2006: CODE DATA	
OCCUPANT LOAD	T.B.D.
USE GROUP CLASSIFICATION	VACANT
TYPE OF CONSTRUCTION	SEPARATED MIXED USE - EXISTING TO REMAIN
TOTAL AREA OF WORK	912 SF
ACTUAL BUILDING AREA	E.T.R.
BUILDING HEIGHT	E.T.R.
BUILDING HEIGHT LIMITATION - STORIES	NONE
EXIT ACCESS TRAVEL DISTANCE (ALLOWABLE/ ACTUAL)	36
COMMON PATH OF TRAVEL (ALLOWABLE/ ACTUAL)	NONE
FIRE SUPPRESSION:	YES - E.T.R.
EXIT ACCESS FIRE RATINGS:	
FIRE WALLS:	
PARTY WALLS	E.T.R.
STAIR ENCLOSURES	E.T.R.
SHAFTS	E.T.R.
EXIT ACCESS CORRIDORS	E.T.R.
FIRE PROTECTION OF STRUCTURE:	
COLUMNS	E.T.R.
BEAMS, GIRDERS, TRUSSES & ARCHES	E.T.R.
LOAD BEARING WALLS - EXTERIOR	E.T.R.
NON-LOAD BEARING WALLS - EXTERIOR	E.T.R.
LOAD BEARING WALLS - INTERIOR	E.T.R.
NON-LOAD BEARING WALLS - INTERIOR	E.T.R.
FLOOR CONSTRUCTION	E.T.R.
ROOF CONSTRUCTION	E.T.R.
GENERAL NOTES	
OCCUPANT LOAD TENANT SPACE: T.B.D.	
INTERIOR FINISHES:	WALLS & CEILINGS: CLASS A, B, OR C
FLOORS:	CLASS 1 OR 2
F.F.E. SHALL MEET NFPA 265 & 701.	



## ENLARGED ADA TOILET PLAN

1/2"=1'-0"

## PROPOSED WALL - INTERIOR



## 1ST FLOOR PLAN - SUITE 3

1/4"=1'-0"

**REVISED: FEBRUARY 24, 2015**  
**PERMIT SET: DECEMBER 29, 2014**

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INTERIOR TENANT FIT-UP FOR:  
**OFFICE TENANT**  
SUITE 3  
22 HANCOCK STREET  
PORTLAND, MAINE 04101

**PROPOSED PLAN-SUITE 3**

REVISIONS  
DATE  
NOV 2014  
PROJECT  
BAYSIDE  
DRAWN BY  
JME  
CHECK BY  
MAM

**A-1.1**