

Jean Fraser - Planner
City of Portland
Planning & Urban Development Department
389 Congress Street
Portland, ME 04101

December 6, 2013

Subject: Munjoy Heights
 Final Level III Site Plan Application
 Comment Response Letter

On behalf of Redfern Munjoy, LLC we are pleased to address the comments that *pertain* to Acorn Engineering from the City of Portland, including:

- Email from Jean Fraser – Planner City of Portland to the project team dated December 4th, 2013
- Email from Tom Errico – City of Portland Traffic Engineer to City Staff dated November 20th, 2013.
- Email from Captain Chris Pirone, dated December 4th, 2013
- Zoning Memorandum from Ann Machado dated December 4th, 2013

To facilitate the review comments are provided below in italics followed by Acorn Engineering, Inc.’s response.

Planning Board request for additional information:

- 1) *Height of walls along the ramp to East Cove Street (to inform decision as re steps vs ramps)*

Response: Acorn Engineering’s Drawing C-33 Wall 4 depicts the proposed walkway profile. The height of Wall 4 (uphill wall) will be vary from 0 to 4 ft. in height while Wall 3 (lower wall) would vary from 4-7 ft. in height. If stairs were incorporated in place of the ramp the height of the uphill wall’s exposed surface may be reduced by 2-3 ft. The height of downhill wall would remain the same. The walkway or “ramp”, as presently proposed, will not exceed an 8.3% slope. As mentioned within the November 26th, 2013 Planning Board Workshop #2 this slope is less than the existing grades within East Cove Street.

Preliminary Review Comments:

- 1) *Stormwater*

Response: The City DPS, in an email to Acorn Engineering on November 28th, 2013, confirmed that they are agreeable with the additional stormwater generated by this development to Walnut Street until the existing combined sewer system is separated

within the next three years.

It is Acorn Engineering opinion that the proposed development as designed meets the requirements implemented by the MDEP under the Stormwater Management Statute (38 M.R.S.A. § 420-D) as well as the City of Portland Technical Manual – Section 5 – Portland Stormwater Management Standards. As a result the design of the proposed development and stormwater system does not anticipate to create erosion, drainage or runoff problems either in the development or with respect to adjoining properties.

2) *Traffic Comments Email from Tom Errico, P.E. –November 20th, 2013.*

Response:

- The following response is from the Soren Deniord Design Studio, “Bollards have been added to the Landscape Plan at the top of the East Cove Stairs and the base of the Jack Path steps to ensure an open / clear passage to the trail connections. In response to Jeff Tarlings concerns regarding tree protection, (2) additional bollards have also been added to the east and west sides of the Oak tree located in the center of the woonerf hardscape.”
- The following response is from the Soren Deniord Design Studio, “We have removed the designated 'guest' parking areas in front of the high pedestrian traffic areas. These areas will now be considered 'overflow' parking. We will ensure clear and open access to all pedestrian nodes / trail heads with bollards.”
- Acorn Engineering has updated the Fire Truck Turnaround Plan that depicts the ability for a Smeal Aerial MM 100 ft fire truck to access the woonerf driveway and turnaround and the end. Please refer to the attached Fire Truck Turnaround Plan. As a result, we believe the driveway entrance radii is acceptable.
- Crosswalk on Walnut Street – Pending input from the City’s Crosswalk Committee
- The proposed driveway entrance and rebuilt sidewalk will be in accordance with the City of Portland Technical Manual and thus be able to accommodate the City sidewalk plows at a level that meet or exceeds the current condition.
- The following response is from the Soren Deniord Design Studio, “It is our intention to have a bicycle ramp on the Jack Path Steps. It will be noted on the plans and detailed (in collaboration with Portland Trails) in the permit / construction drawings.”
- No further comment.

3) *Fire Department Comments Email from Captain Chris Pirone, dated December 4th, 2013.*

Response: It is my understanding that the requirement to maintain a 20 ft access will be included within the condominium documents developed by Tom Jewell. The geotechnical engineer has reviewed the Concrete Brick Paver Detail and recommend the minimum amount of aggregate material to support H-20 wheel loading. Please refer to the attached email from Bill Peterlein, P.E. This is also reflected within Acorn Engineering’s Concrete Brick Paver Detail.

4) *Zoning Memorandum from Ann Machado dated 12/4/13 with comments.*

Response:

- Comment 1 – The 128 North Street “bumpout” is an existing basement bulkhead that meets the criteria set forth within Section 14-425.
- Comment 2 – The utility closet associated with Building D (Units 15-18) was relocated outside the 20 ft. rear setback. This was achieved by shifting Building D to the East by 1.5 ft.

5) *Portland Water District*

Response:

- Acorn Engineering’s believes we have met the “conditions of service” as outlined within the PWD’s August 23rd, 2013 ability to serve letter. Acorn Engineering has requested a final review by the PWD.

6) *Retaining Wall Design*

Response:

- Acorn Engineering agrees with Woodard & Curran’s November 20th, 2013 Memorandum stating that, *“Summit Engineering Services in coordination with Structural Integrity Consulting Engineers, Inc., shall provide the retaining wall design, global stability analysis, and the design of the temporary soil restraint measures, as required. We recommend that it be made a condition of approval that this design be completed and submitted to the City as part of the Building Permit process prior to construction, and that it be stamped by a professional engineer.”*

Please let me know if you have any additional questions or comments.

Sincerely,



William H. Savage, P.E.
Project Manager
Acorn Engineering, Inc.

Attachment:

1. Summit Geoenvironmental Services, Inc. – Email to Will Savage dated 12/5/13
2. Fire Truck Turnaround Plan – Dated 12/6/13
3. Complete Civil Plan Set – Revision Date 12/6/13

William Savage

From: Bill Peterlein <bpeterlein@summitgeoeng.com>
Sent: Thursday, December 05, 2013 8:27 PM
To: William Savage
Subject: Concrete Brick Paver Detail

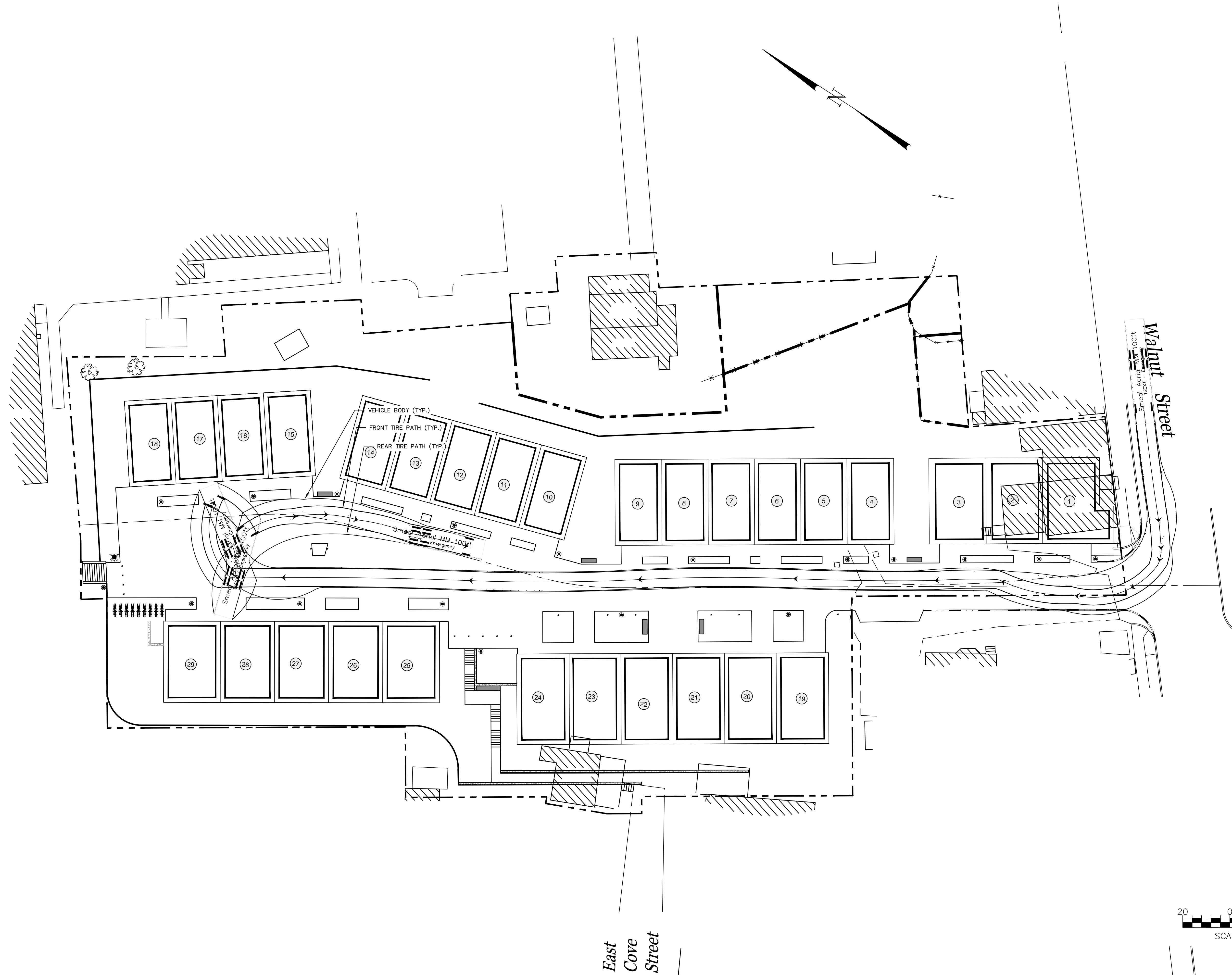
Will;

I have reviewed the Concrete Brick Paver Detail on Drawing C40. I would recommend 4" of sand and 12" of MDOT Type D beneath the pavers. I am not sure that stone dust is the best material beneath the pavers – it tends to be high in fines and non-free draining – susceptible to frost heave. On the other hand it does compact well which minimizes differential movement of the pavers. Compacted sand could accomplish the same thing...

The above materials will be adequate to support a H-20 wheel loading.

Bill


Bill Peterlein, P.E.
President & Principal Engineer
Summit Geoengineering Services, Inc.
145 Lisbon Street, Suite 601
Lewiston, Maine 04240
(207) 576-3313



ISSUED FOR	BY
FIRE CAPTAIN	WHS
DATE	11/4/13
TRAFFIC ENGINEER	WHS
	12/6/13

REVISION	REV. DATE

DRAWING NAME: FIRE TURNAROUND PLAN
 PROJECT NAME: MUNJOY HEIGHTS
 CLIENT: REDFERN MUNJOY, LLC
 P.O. BOX 8816, PORTLAND, MAINE 04104

A C O R N ENGINEERING, INC.

 ACORN ENGINEERING, INC.
 P.O. BOX 3372 PORTLAND, MAINE 04104
 (207) 775-2655

FILE: 1047_CIVIL
 DATE: 11/4/13
 JN: 1047
 SCALE: 1"=20'
 DESIGN BY: WHS
 DRAWN BY: ZRJ
 CHECKED BY: WHS

DRAWING NO.
F-10