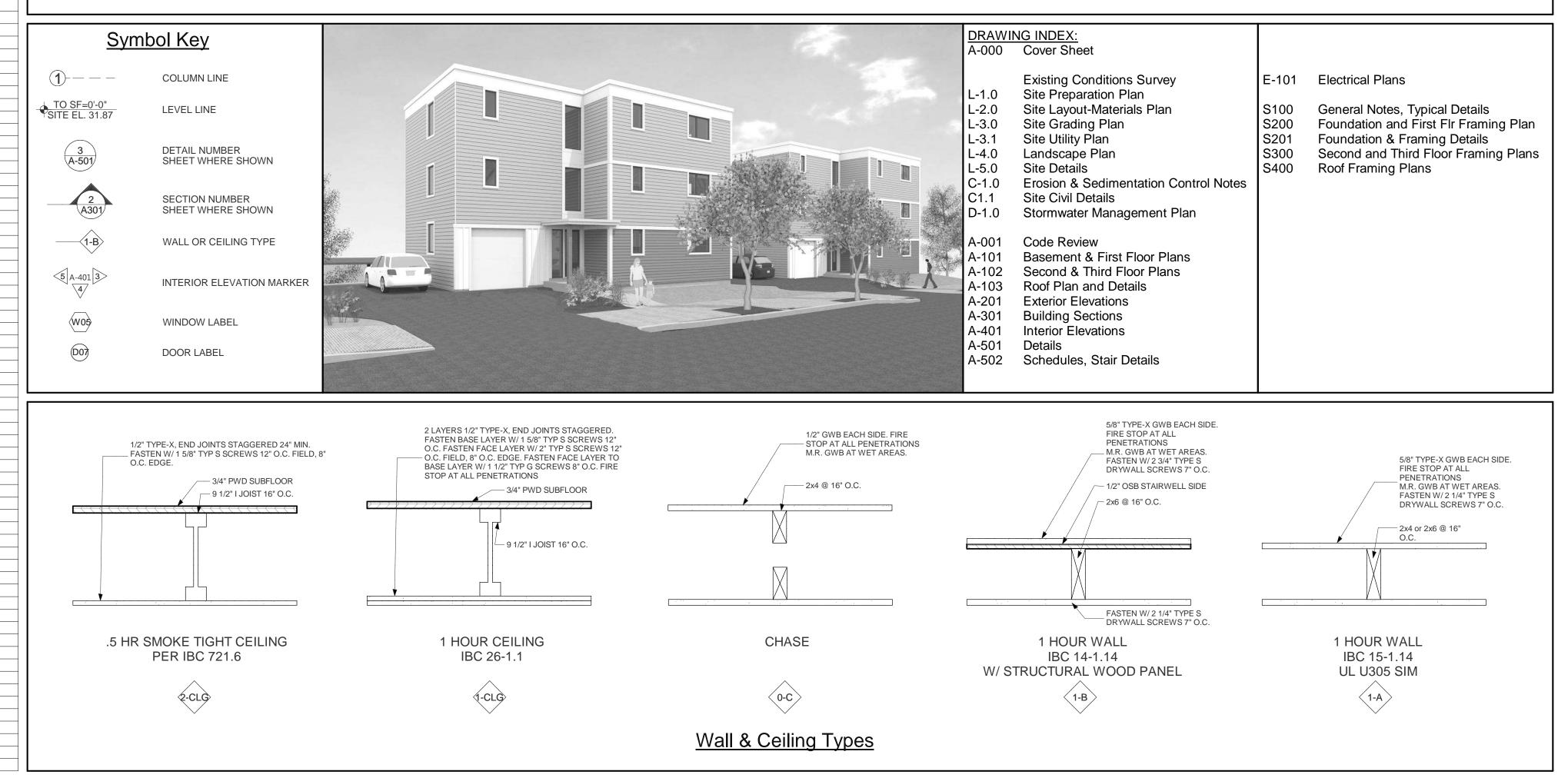
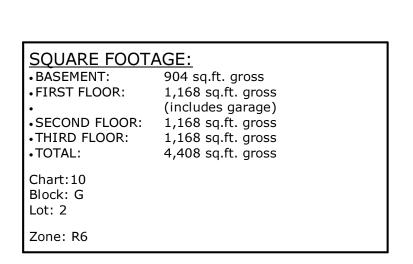
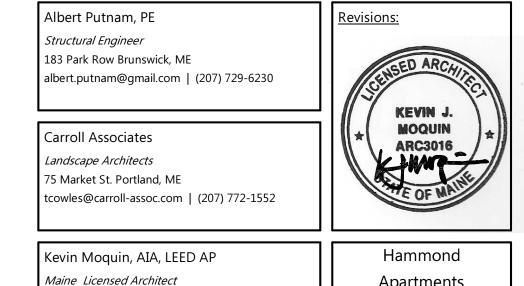
Α	AMPERES	DR	DOOR	INCL.	"INCLUDE, INCLUSIVE"	R	RADIUS	
A.B.	ANCHOR BOLT	E.A.	EXPANSION ANCHOR	INSUL.	INSULATION	R.D.L.	ROOF DRAIN LEADER	
A.F.F.	ABOVE FINISHED FLOOR	E.F.	EXHAUST FAN	INT.	INTERIOR	R.D.O.	ROOF DRAIN OVERFLOW	
A.F.G.	ABOVE FINISHED GRADE	E.J.	EXPANSION JOINT	J-BOX	JUNCTION BOX	R.O.	ROUGH OPENING	
A/C	AIR CONDITIONING	E.N.	END NAILING	JCT	JUNCTION		RIGHT OF WAY	
ABC	AGGREGATE BASE COURSE	E.W.	EACH WAY	JST.	JOIST	REF	REFRIGERATOR	-
ABS	ACRYLONITRILE-BUTADIENE-STYRENE	EA.	EACH	JT.	JOINT LANGUE BOWN	REF.	REFERENCE	
ABV.	ASBESTOS-CEMENT BOARD	EL	ELEVATION	K-D	KNOCK DOWN	REINF.	REINFORCED	-
ACOLL	ACOUSTIC ACOUSTIC	ELECT.	"ELECTRIC, ELECTRICAL" ELEVATOR	KD KO	KILN DRIED KNOCK OUT	REQ'D.	REQUIRED RETURN	
ACOU.		EMC				RET.		-
ACT ADD.	ACOUSTICAL CEILING TILE ADDITION or ADDENDUM	EMT	ELECTRICAL METALLIC CONDUIT ELECTRICAL METALLIC TUBING	L.E.D. L.FT.	LIGHT EMITTING DIODE LINEAR FEET	REV.	REVISION ROOM	-
		ENT					REMOVE	
AG	ABOVE GRADE		ELECTRICAL NON-METALLIC TUBING	LAM	LATERAL	RMV.	-	-
AHU	AIR HANDLER UNIT	EQ.	EQUAL	LAT.	LAYATORY	S.C. S.D.	SOLID CORE	
AL. or ALUM.	ALUMINUM	EQUIP.	EQUIPMENT	LAV	LAVATORY LEAD	S.O.V.	SMOKE DETECTOR	
ALT.	ALTERNATE	EST. EVAP.	ESTIMATE EVAPORATIVE COOLER	LD.	LINEAR	S.U.V.	SHUT OFF VALVE	-
ANL	ANNEALED ASPHALT	EWC	EVAPORATIVE COOLER ELECTRIC DRINKING COOLER	LIN.	LINOLEUM	S/S	SKYLIGHT STAINLESS STEEL	-
ASPH.			EXCAVATE	LINO.		SC SC		-
AVG	AVERAGE	EXC EXH.		LTC	LIGHT		SELF CLOSING	-
AWG	AMERICAN WIRE GAUGE		EXHAUST	LTG.		SCHED.	SCHEDULE	-
<u>A</u>	ANGLE	EXIST. or E	EXISTING	LVL	LAMINATED VENEER LUMBER	SECT.	SECTION SERVICE ENTRANCE SECTION	
B.M.	BENCH MARK BOUNDARY NAILING	F.A.	EXTERIOR FIRE ALARM	M.B.	MACHINE BOLT MANHOLE	SES SH	SERVICE ENTRANCE SECTION SHEET	-
B.N.				M.H.				-
B.O.	BOTTOM OF BOTTOM OF FOOTING	F.C. F.C.O.	FAN COIL FLOOR CLEAN OUT	M.I.	MALLEABLE IRON MASONRY OPENING	SHT'G. SIM.	SHEATHING	-
B.O.F.				M.O.			SIMILAR	-
B.U.	BUILT UP	F.D.	FLOOR DRAIN	MAR.	MARBLE	SPA.	SPACE	-
B/C	BACK OF CURB	F.E.	FIRE EXTINGUISHER	MAS.	MASONRY	SPECS	SPECIFICATIONS	
BD.	BOARD	F.N.	FIELD NAILING	MAT'L	MATERIAL	SPKR.	SPEAKER	
BLDG	BUILDING	F.O.	FACE OF	MAX.	MAXIMUM	SQ. FT.	SQUARE FEET	
BLK.	BLOCK	F.S.	FLOOR SINK	MECH.	MECHANICAL	SQ. IN.	SQUARE INCHES	
BLKG.	BLOCKING	F/G	FIBERGLASS	MED.	MEDIUM	STC	SOUND TRANSMISSION CLASS	-
BM.	BEAM	FAB.	FABRICATE	MFG.	MANUFACTURING	STD.	STANDARD	
BR	BRASS	FACP	FIRE ALARM CONTROL PANEL	MFR.	MANUFACTURER	STL.	STEEL	
BRG.	BEARING	FDC	FIRE DEPARTMENT CONNECTION	MIN.	MINIMUM	SUSP.	SUSPENDED	
BRZ	BRONZE	FDN.	FOUNDATION	MISC.	MISCELLANEOUS	SW	SWITCH	
C.A.P.	CONCRETE ASBESTOS PIPE	FHC	FIRE HOSE CABINET	MOD	MODULAR	SYM	SYMMETRICAL	
C.D.	CONSTRUCTION DOCUMENTS	FIN.	FINISH	MTL.	METAL	SYS.	SYSTEM	
C.I.P.	CAST IN PLACE	FL	FLOOR	MUL	MULLION	T & G	TONGUE AND GROOVE	
C.J.	CONTROL JOINT	FLG.	FLOORING	N.I.C.	NOT IN CONTRACT	T.B.	THROUGH BOLT	
C.O.	CLEAN OUT	FLUOR.	FLUORESCENT	N.T.S.	NOT TO SCALE	T.M.B.	TELEPHONE MOUNTING BOARD	
C.T.	CERAMIC TILE	FP	FIRE PROOF	NCM	NON-CORROSIVE METAL	T.O.	TOP OF	
CAB	CABINET	FTG.	FOOTING	NFC	NOT FOR CONSTRUCTION	T.O.B.	TOP OF BEAM	
CAM.	CAMBER	FURN.	FURNISH	NLR.	NAILER	T.O.C.	TOP OF CURB	
CCTV	CLOSED CIRCUIT TELEVISION	G.I.	GALVANIZED IRON	NO.	NUMBER	T.O.F.	TOP OF FOOTING	
CEM.	CEMENT	GA.	GAUGE	NOM.	NOMINAL	T.O.J.	TOP OF JOIST	
CER	CERAMIC	GALV.	GALVANIZED	O.C.	ON CENTER	T.O.M.	TOP OF MASONRY	
CFM	CUBIC FEET PER MINUTE	GAR.	GARAGE	O.D.	OUTSIDE DIAMETER	T.O.S.	TOP OF SLAB	
CH or \Box	CHANNEL	GFCI	GROUND FAULT CIRCUIT INTERRUPTER	O.H.	OVER HANG	T.O.W.	TOP OF WALL	
CKT. BKR.	CIRCUIT BREAKER	GFI	GROUND FAULT INTERRUPTER	O.I.	ORNAMENTAL IRON	T.S.	TUBE STEEL	
CL or G	CENTERLINE	GL	GLASS	O.R.	OUTSIDE RADIUS	T.V.	TELEVISION OUTLET	
CLG.	CEILING	GLB	GLUE LAMINATED BEAM	OAI	OUTSIDE AIR INTAKE	TEL.	TELEPHONE	
CLKG.	CAULKING	GM	GRADE MARK	ОН	OVER HEAD	TH.	THRESHOLD	
CLO.	CLOSET	GM	GATE VALVE	OPNG.	OPENING	THD.	THREADED	
CLR.	CLEAR	GRC	GALVANIZED RIGID TUBING	OPPO.	OPPOSITE	THK.	THICK	
CMU	CONCRETE MASONRY UNIT	GYP.	GYPSUM	P.C.	PRECAST CONCRETE	THRU	THROUGH	
CTR.	CENTERED	GWB	GYPSUM BOARD	P.L. or P	PROPERTY LINE	TLT.	TOILET	
COL.	COLUMN	H.B.	HOSE BIBB	P.LAM.	PLASTIC LAMINATE	TRANS.	TRANSFORMER	
COMB.	COMBINATION	H.C.	HOLLOW CORE	P.O.C.	POINT OF CONNECTION	TYP.	TYPICAL	
CONC.	CONCRETE	H.M.	HOLLOW METAL	PERF.	PERFORATED	UNF.	UNFINISHED	
CONST.	CONSTRUCTION	H/C	HANDICAPPED	PERP. or ⊥	PERPENDICULAR	UR	URINAL	
CONT.	CONTINUOUS	HDBD.	HARDBOARD	PH	PHASE	V.B.	VAPOR BARRIER	
CONTR.	CONTRACTOR	HDW	HARDWARE	PL.	PLASTER	V.I.F.	VERIFY IN FIELD	
CU	COPPER	HGT.	HEIGHT	PL. or PL	PLATE	VA	VOLT AMPERE	
d	PENNY	HOR.	HORIZONTAL	PLAS.	PLASTIC	VCT	VINYL COMPOSITION TILE	
D.F.	DRINKING FOUNTAIN	HTR	HEATER	PLUMB.	PLUMBING	VERT.	VERTICAL	
D.G.	DECOMPOSED GRANITE	HVAC	"HEATING, VENTILATING & AIR COND."	PWD	PLYWOOD	W/C	WATER CLOSET	
D.S.	DOWN SPOUT	HW	HOT WATER	PORC.	PORCELAIN	WDW	WINDOW	
D/W	DISHWASHER	HYD.	HYDRAULIC	PREFAB.	PREFABRICATED	WCT	WAINSCOT	
DBL.	DOUBLE	I.C.	INTERCOM OUTLET	PSF	POUNDS PER SQUARE FOOT	WP	WEATHER PROOF	
DEMO	DEMOLITION	I.D.	INSIDE DIAMETER	PSI	POUNDS PER SQUARE INCH	WT.	WEIGHT	
DIA.	DIAMETER	I.F.	INSIDE FACE	PTN.	PARTITION	W/	WITH	
DIAG.	DIAGONAL	ID	IDENTIFICATION	PVC	POLYVINYLCLORIDE	W/O	WITHOUT	
DIM.	DIMENSION	IG	ISOLATED GROUND	PWR.	POWER		WOOD	\neg
	DEAD LOAD	IMC	INTERMEDIATE METALLIC CONDUIT	Q.T.	QUARRYTILE	W.I.	WROUGHT IRON	
DL					,			1

Hammond Street Apartments



Parcel A





Kevin Moquin, AIA, LEED AP

Maine Licensed Architect

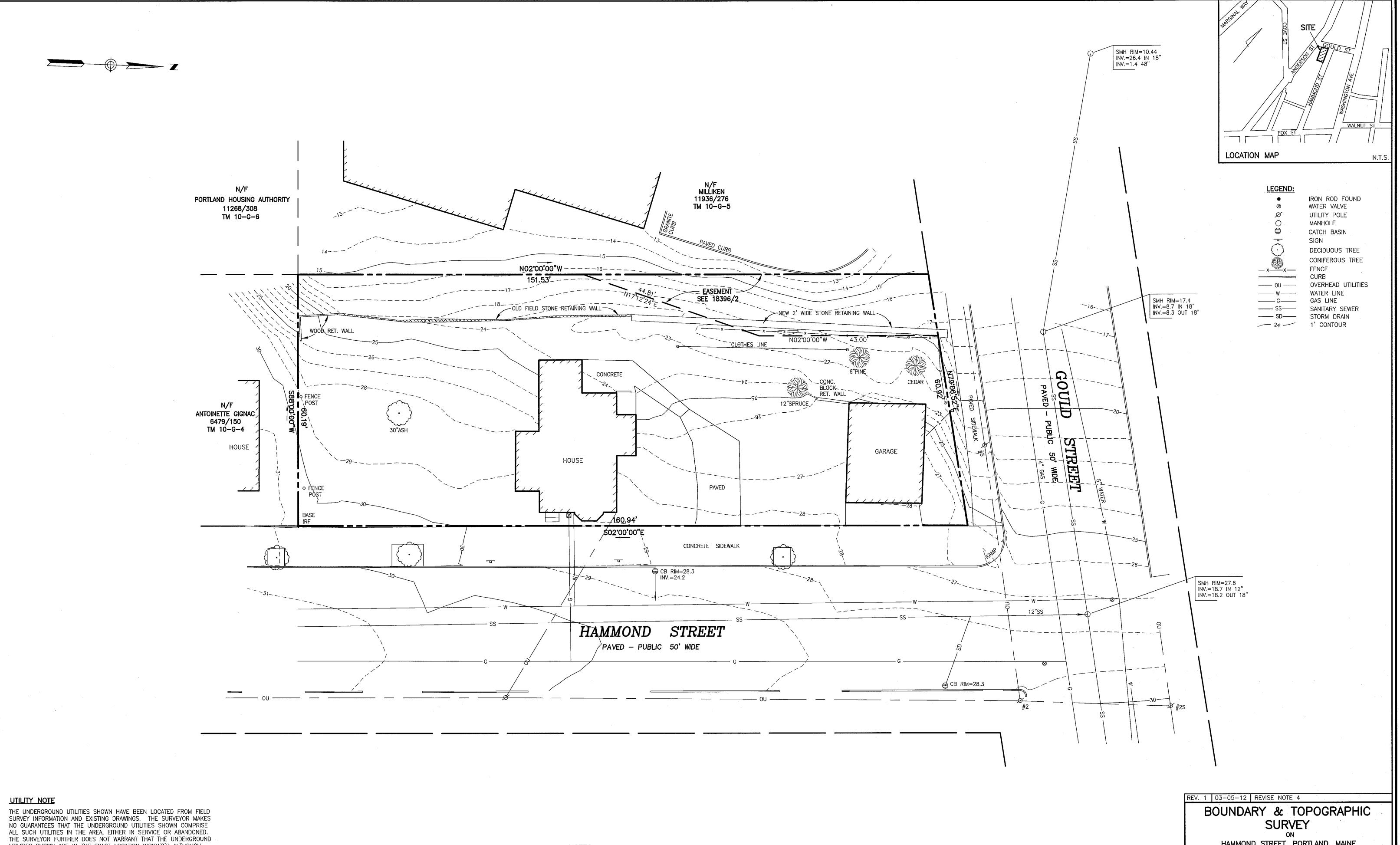
53B Hammond St. Portland, ME

kevin@km-a.me | (207)-615-6421

Apartments
Portland, ME

Cover Sheet

A000
PERMIT SET
5/18/2012



UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. CALL 1-888-DIGSAFE AT LEAST THREE BUSINESS DAYS BEFORE PERFORMING ANY CONSTRUCTION.

GRAPHIC SCALE

PLAN REFERENCES:

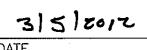
- 1. "PROPERTY REVISION, LAND OF B.H. MILLIKEN, PORTLAND, MAINE" DATED AUGUST 8, 2002 BY LAND SERVICES, INC.
- 2. WORKING DRAWING TOPOGRAPHIC SURVEY OF LAND IN PORTLAND, MAINE BY LAND SERVICES, INC. DATED JANUARY 9, 1994.

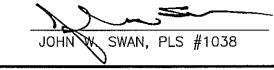
- 1. OWNER OF RECORD: STEVEN E. COPE, ROBERTA S. COPE C.C.R.D. BOOK 18396 PAGE 201
- 2. PARCEL IS SHOWN AS LOT 2, BLOCK G, ON THE CITY OF PORTLANDS ASSESSORS MAP 10.
- 3. BEARINGS ARE BASED ON PLAN REFERENCE 1.
- 4. ELEVATIONS ARE BASED ON CITY DATUM PER PLAN REFERENCE 2.
- 5. BOUNDARY INFORMATION SHOWN HEREON IS BASED EXCLUSIVELY ON PLAN REFERENCES 1 AND 2.

CERTIFICATION:

OWEN HASKELL, INC. HEREBY CERTIFIES THAT THIS PLAN IS BASED ON, AND THE RESULT OF, AN ON THE GROUND FIELD SURVEY AND THAT TO THE BEST OF OUR KNOWLEDGE, INFORMATION AND BELIEF, IT CONFORMS TO THE BOARD OF LICENSURE FOR PROFESSIONAL LAND SURVEYORS CURRENT STANDARDS OF PRACTICE.

ORIGINALLY SIGNED BY WILLIAM C. SHIPPEN, PLS #2118 APRIL 2006



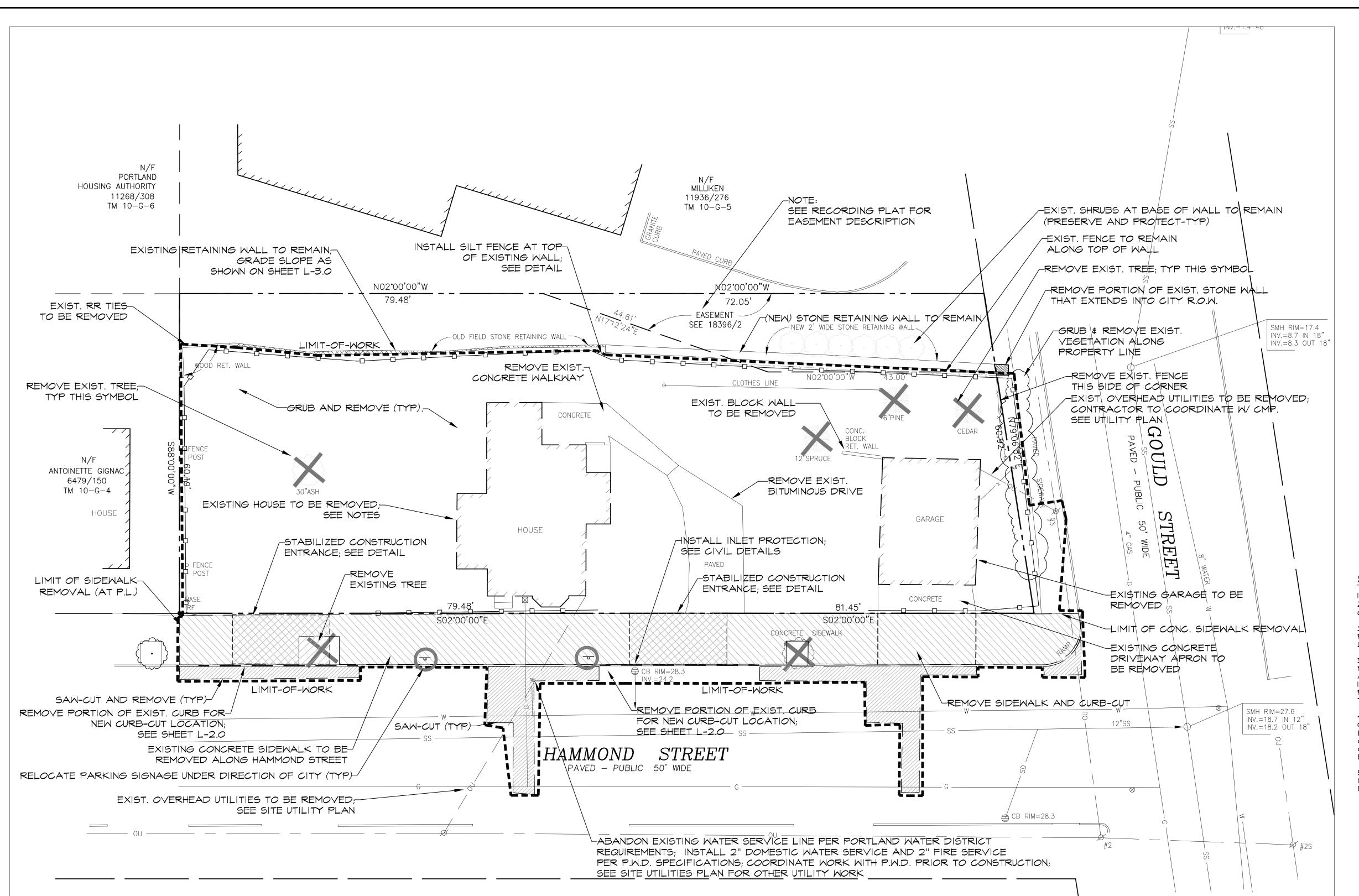


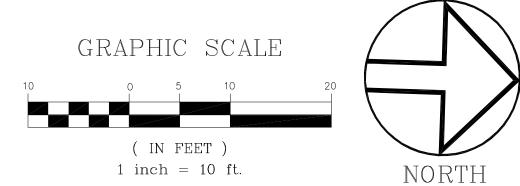
HAMMOND STREET, PORTLAND, MAINE MADE FOR

MITCHELL & ASSOCIATES



Drwn By	WCS	Date	Job No.		
Trace By	JLW	APRIL 10, 2006	2006-023P		
Check By	WCS	Scale	Drwg. No.		
Book No.	1025	1" = 10'	1		





LEGEND

EXISTING DESCRIPTION PROPOSED PROPERTY/ROW SETBACK EASEMENT CENTERLINE MONUMENT IRON PIPE/ROD BUILDING EDGE PAVEMENT GRAYEL ROAD CURBLINE CONTOURS SD STORM DRAIN SD UNDERGROUND ---UGE #T---ELEC. & TEL. OVERHEAD OHE &T ELEC. 4 TEL. GATE VALVE UTILITY POLE MANHOLE POTABLE WELL WETLANDS EDGE WETLAND SIGN BENCHMARK SPOT GRADE CHAIN LINK FENCE RIPRAP \prec STOCKADE FENCE ZONE LINE

SITE PREPARATION NOTES

I. INSTALL INLET PROTECTION FOR ALL CATCH BASINS WITHIN WORK ZONE CONSISTING OF SILT SACK OR OTHER APPROVED METHODS (SEE CIVIL PLANS)

2. THE SITE SHALL BE GRUBBED AS SHOWN ON THE PLANS. EXISTING PAVEMENT SHALL BE REMOVED FROM THE SITE AND DISPOSED OF IN A LAWFUL MANNER.

3. KEEP ALL STREETS FREE OF DUST, MUD, AND DEBRIS. SURFACES MUST BE SWEPT CLEAN AT END OF EACH WORK DAY AND OTHER TIMES AS REQUIRED TO MINIMIZE TRACKING OF MUD AND CREATION OF DUST. INSTALL STABILIZED CONSTRUCTION ENTRANCE AT PRIMARY ACCESS ROUTE ONTO LANCASTER STREET AND MAINTAIN AS REQUIRED THROUGHOUT CONSTRUCTION.

4. REMOVAL OF EXISTING BUILDING BY OTHERS UNDER SEPARATE CONTRACTS WITH THE OWNER PRIOR TO CONSTRUCTION.
BUILDING REMOVAL WORK SHALL INCLUDE COMPLETE DEMOLITION OF ALL ABOVE-GRADE STRUCTURES AND FOUNDATIONS, DISPOSAL OF DEMOLITION WASTE GENERATED, AND DISCONNECTION OF ELECTRICAL, WATER, SEWER, AND GAS SERVICE TO THE BUILDING.

5. CONTRACTOR SHALL INSTALL ALL EROSION CONTROL MEASURES IN ACCORDANCE WITH THE EROSION AND SEDIMENTATION CONTROL NOTES AND DETAILS.

Project Name:

HAMMOND STREET APARTMENTS

56 Hammond Street Portland, Maine

CARROLL ASSOCIATES

Landscape Architects

217 Commercial Street -Portland, Maine 04101
207.772.1552
772.0712 Fax

wner:
Steven & Roberta Cope
172 Concord Street
Portland, Maine 04103

Consultants:

Kevin Moquin, Architect Hammond Stret Portland, Maine 04104 207.615-6421

<u>Civil Engineer</u> Blais Civil Engineers

967 Broadway
South Portland, Maine 04106
207.767.7300

Drawing Set:

No. Date Revision

1 12–12–11 PRELIM. CITY REVIEW

2 2–3–12 RE–SUBMIT FOR CITY REVIEW

3 3–5–12 FINAL SITE PLAN

4 3-20-12 RESPONSE TO 3/14/12 CITY COMMENTS
5 5-24-12 PERMIT SET

PERMIT SET:
NOT FOR CONSTRUCTION

Drawing Set:

Drawn:

TC

Checked:

PC

Approved:

SB

Drawing Title:

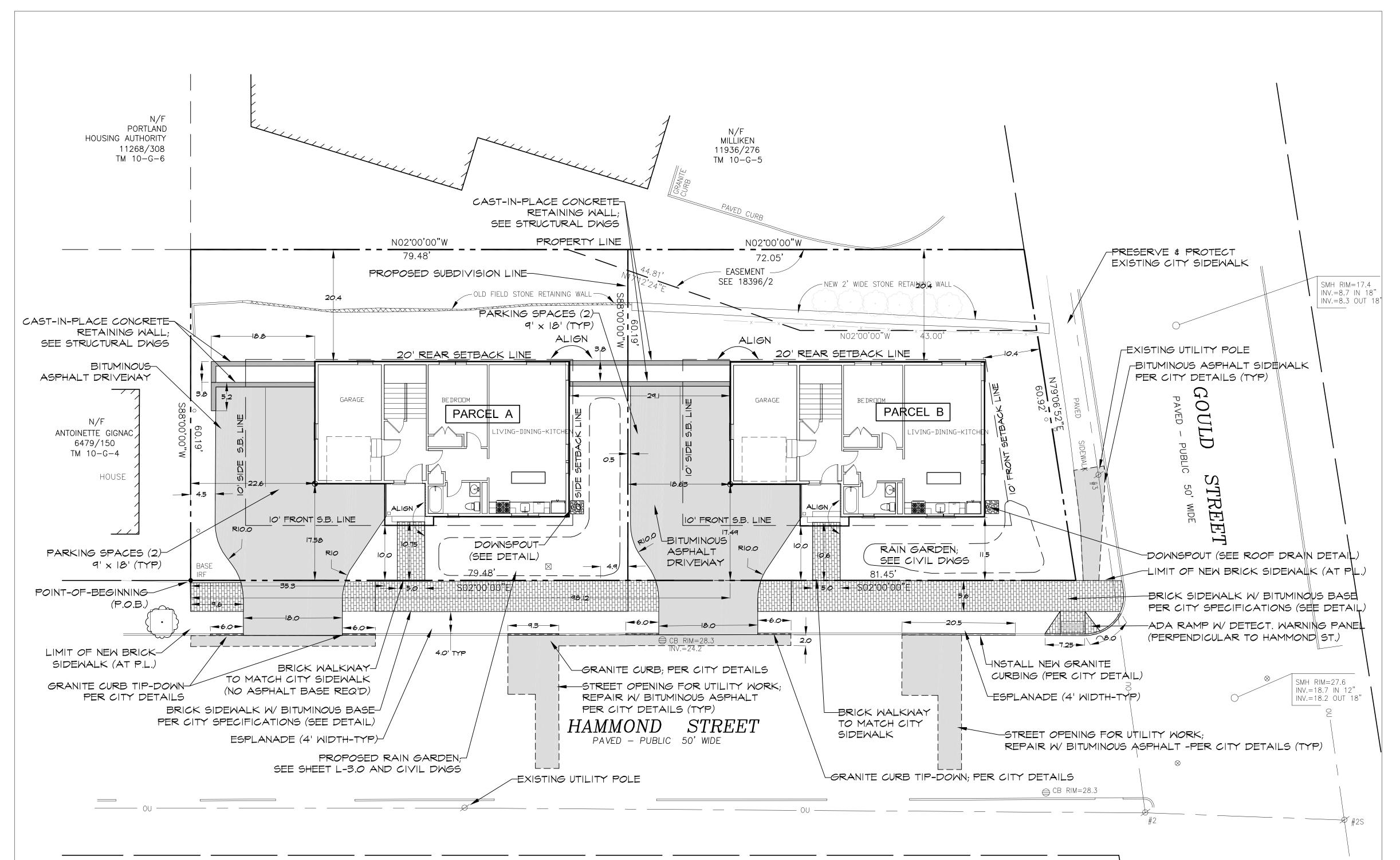
SITE PREPARATION PLAN

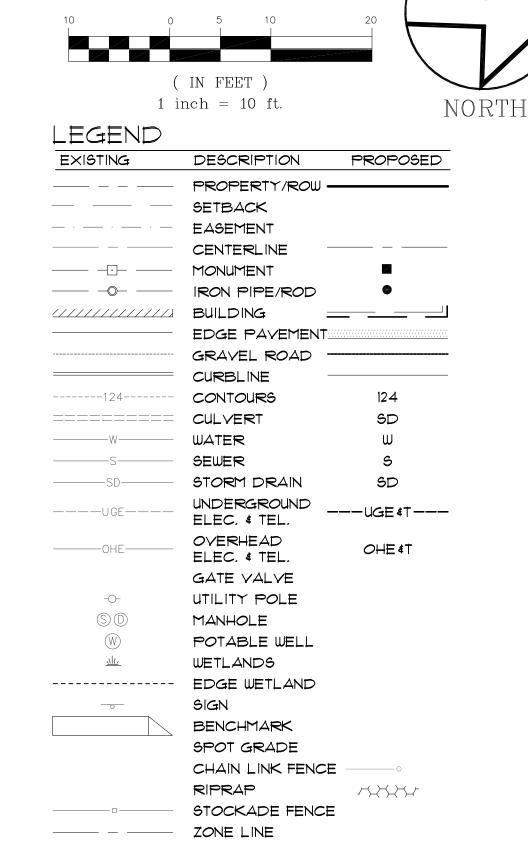
Job Number:

ile:

Date: **5–24–12** Scale: **1"– 10'–0"** Drawing Number:

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GRAPHIC SCALE

R-6 / RESIDENTIAL ZONE						
SPATIAL STANDARDS	REQUIRED	PARCEL A	PARCEL B			
MINIMUM LOT SIZE	4,500 SF	4,791 SF	4,619 SF			
MINIMUM FRONTAGE	40 FT.	79.48'	142.37'			
FRONT YARD SETBACK	10 FT.	10.0'	10.0'			
SIDE YARD SETBACK (3 STORIES)	10 FT.	22.6'	N/A			
REAR YARD SETBACK	20 FT.	20.4'	20.4'			
MAXIMUM LOT COVERAGE	50%	45.5 %	48.5 %			
MINIMUM LOT WIDTH	40 FT.	79.48'	72.05'			
MAXIMUM STRUCTURE HEIGHT	45'	34.0'	36.5'			
OPEN SPACE	20%	54.5 %	51.5 %			
PARKING	3	3	3			

Project Name:

HAMMOND STREET APARTMENTS

56 Hammond Street Portland, Maine

> SSOCIATES Architects Landscape ARROLL

Owner: Steven & Roberta Cope 172 Concord Street Portland, Maine 04103

Consultants: Kevin Moquin, Architect Hammond Stret Portland, Maine 04104 207.615-6421

Civil Engineer Blais Civil Engineers 967 Broadway South Portland, Maine 04106 207.767.7300

Drawing Set: No. Date | Revision 1 | 12-12-11 PRELIM. CITY REVIEW 2 2-3-12 RE-SUBMIT FOR CITY 3 3-5-12 FINAL SITE PLAN 4 3-20-12 RESPONSE TO 3/14/12 CITY COMMENTS 5 | 5-24-12 | PERMIT SET

Drawing Set: PERMIT SET: NOT FOR CONSTRUCTION

Drawn: STEVE G. Checked: BLAIS No. 10084 Approved:

Drawing Title:

SITE LAYOUT & MATERIALS PLAN

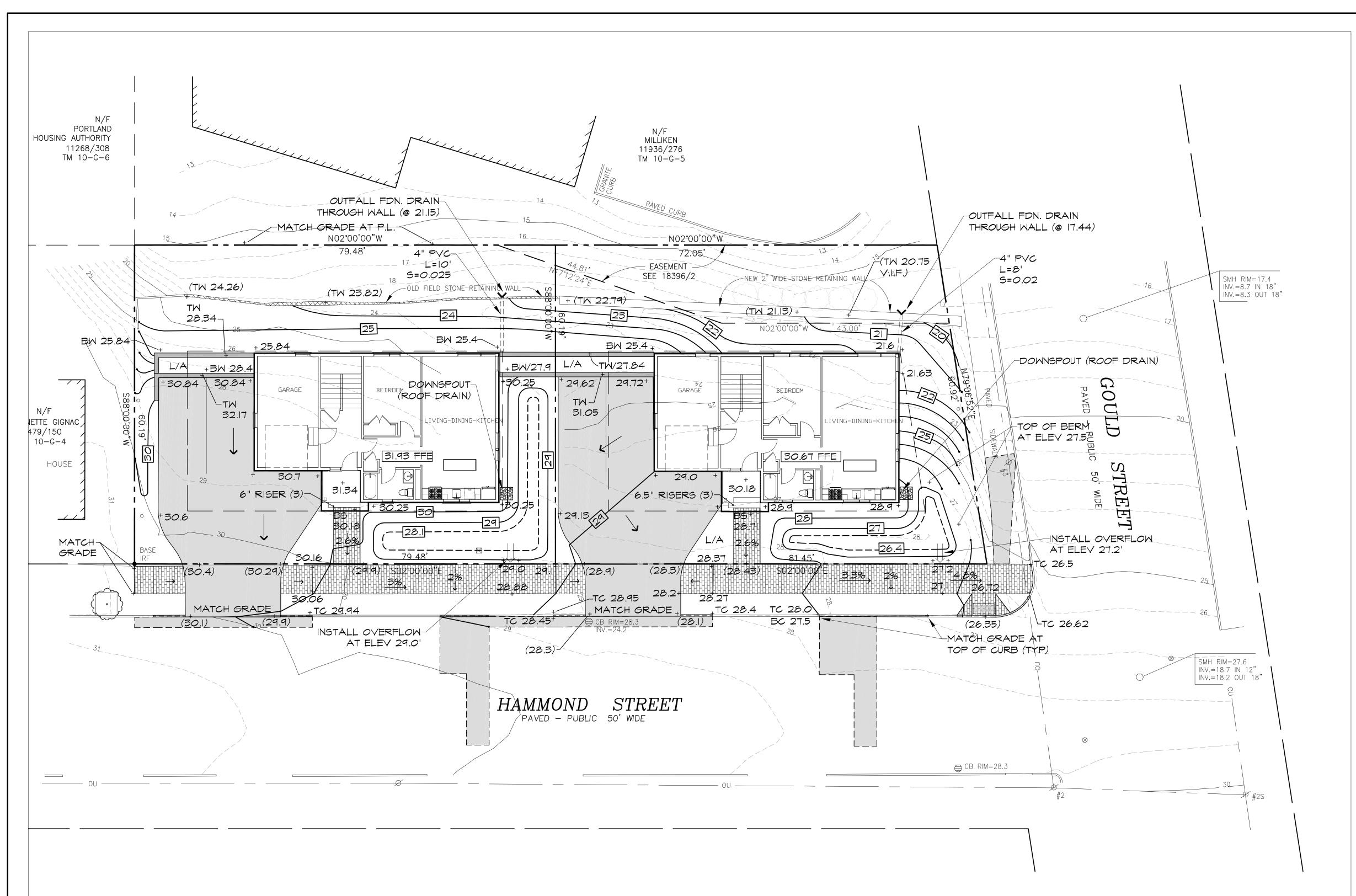
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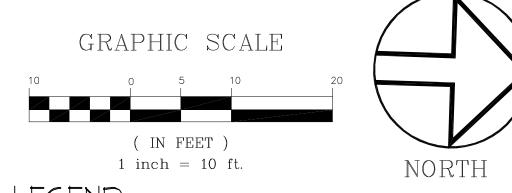
Date: **5–24–12** | Scale: **1"– 10'–0"** Drawing Number:

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NOTES

- ALL WORK SHALL CONFORM TO THE APPLICABLE CODES AND ORDINANCES.
- EXISITNG CONDITIONS BASED ON TOPOGRAPHIC SURVEY COMPLETED BY OWEN HASKELL SURVEYING IN APRIL 10, 2006. FIELD CONDITIONS MAY VARY SLIGHTLY FROM WHAT IS DEPICTED ON THE PLANS.
 - ELEVATIONS ARE BASED ON CITY DATUM PER "WORKING DRAWING-TOPOGRAPHIC SURVEY OF LAND IN PORTLAND, MAINE BY LAND SERVICES INC. DATED JANUARY 9, 1994": SPIKE IN UTILITY POLE AT THE SOUTHWESTERLY CORNER OF ANDERSON STREET AND
- GOULD STREET ELEVATION 12.29 NGVD 1929. CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION. ANY UNUSUAL
- CONDITIONS SHALL BE REPORTED TO THE ATTENTION OF THE OWNER.
- CONTRACTOR SHALL CLEAN AND REMOVE DEBRIS AND SEDIMENT DEPOSITED ON PUBLIC STREETS, SIDEWALKS, ADJACENT AREAS, OR OTHER PUBLIC WAYS DUE TO CONSTRUCTION.
- CONTRACTOR SHALL PROTECT EXISTING STRUCTURES, PHYSICAL FEATURES, AND MAINTAIN SITE STABILITY DURING CONSTRUCTION. CONTRACTOR SHALL RESTORE ALL AREAS TO ORIGINAL CONDITION AND AS DIRECTED.
- THE CONTRACTOR SHALL CONTACT DIG SAFE (1-888-DIGSAFE) AT LEAST THREE (3) BUT NOT MORE THAN THIRTY (30) DAYS PRIOR TO COMMENCEMENT OF EXCAVATION OR DEMOLITION TO VERIFY HORIZONTAL AND VERTICAL LOCATION OF ALL
- ALL PAVEMENT JOINTS SHALL BE SAWCUT PRIOR TO PAVING TO PROVIDE A DURABLE AND UNIFORM JOINT.
- NO HOLES, TRENCHES OR STRUCTURES SHALL BE LEFT OPEN OVERNIGHT IN ANY EXCAYATION ACCESSIBLE TO THE PUBLIC OR IN PUBLIC RIGHTS-OF-WAY.
- ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL REQUIRE A STREET-OPENING PERMIT FROM THE CITY OF PORTLAND AS
- APPLICABLE.
- POINT-OF-BEGINNING (P.O.B.) FOR PROPOSED BUILDING LAYOUT IS THE S.E. PROPERTY MARKER (I.R.F.). PROPOSED BUILDINGS ARE LOCATED PARALLEL W/ EASTERLY PROPERTY LINE (BEARING \$02°00'00"E) AND THE SOUTHERLY PROPERTY LINE (BEARING 588°00'00"W). THE S.E. CORNER OF BLDG. A IS 17.58' FROM EASTERLY PROPERTY LINE AND 22.6' FROM SOUTHERLY PROPERTY LINE. THE S.E. CORNER OF BLDG. B IS 17.49' FROM EASTERLY PROPERTY LINE AND 98.12' FROM SOUTHERLY PROPERTY LINE.





LEGEND

EXISTING DESCRIPTION PROPOSED PROPERTY/ROW . SETBACK EASEMENT MONUMENT EDGE PAVEMENT GRAVEL ROAD CURBLINE CULVERT STORM DRAIN UNDERGROUND ---UGE &T---ELEC. & TEL. OVERHEAD ELEC. & TEL. GATE VALVE UTILITY POLE MANHOLE POTABLE WELL WETLANDS EDGE WETLAND SIGN BENCHMARK SPOT GRADE CHAIN LINK FENCE RIPRAP STOCKADE FENCE ZONE LINE

GRADING AND DRAINAGE NOTES

1. EXISITNG CONDITIONS BASED ON TOPOGRAPHIC SURVEY COMPLETED BY OWEN HASKELL SURVEYING IN APRIL 10, 2006. FIELD CONDITIONS MAY YARY SLIGHTLY FROM WHAT IS DEPICTED ON THE PLANS.

2. ELEVATIONS ARE BASED ON CITY DATUM PER "WORKING DRAWING-TOPOGRAPHIC SPIKE IN UTILITY POLE AT THE SOUTHWESTERLY CORNER OF ANDERSON STREET AND GOULD STREET ELEVATION 12.29 NGVD 1929.

NOTIFY THE ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES.

4. AREAS NOT REQUIRING GRADING SHALL BE LEFT UNDISTURBED. CONTRACTOR SHALL AVOID THESE AREAS AND PRESERVE ALL EXISTING VEGETATION AS NOTED.

3. CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND GRADES IN THE FIELD AND

5. UTILITY LOCATIONS ARE APPROXIMATE AND SHALL BE VERIFIED PRIOR TO CONSTRUCTION. ANY DISCREPANCIES OR CONFLICTS SHALL BE IMMEDIATELY REPORTED TO THE ARCHITECT.

TO SEED, AND MAT.

6. ALL UTILITY INSTALLATIONS SHALL MEET THE REQUIREMENTS OF THE CITY OF PORTLAND, AS WELL AS ANY OTHER LOCAL, STATE, AND FEDERAL REQUIREMENTS. 7. SITE CONTRACTOR SHALL REPAIR ALL DISTURBED AREAS IN ACCORDANCE WITH

THE EROSION CONTROL PLAN AND DETAILS. 8. ALL PROPOSED SLOPES GREATER THAN 3:1 SHALL HAVE AN EROSION MAT INSTALLED OVER FINISH GRADES TO PROTECT SEEDED SLOPES FROM EROSION. AT CONTRACTOR'S OPTION, SLOPES MAY BE SODDED AS AN APPROPRIATE SUBSTITUTION

9. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH THE GUIDELINES ESTABLISHED IN THE BEST MANAGEMENT PRACTICES, AND MAINTAINED THROUGHOUT CONSTRUCTION OF THIS

IO. ALL AREAS DISTURBED BY CONSTRUCTION NOT TO BE PAVED OR OTHERWISE TREATED SHALL BE LOAMED AND SEEDED ACCORDING TO THE PROJECT SPECIFICATIONS. EXISTING LAWN AREAS WHICH ARE DISTURBED SHALL BE RESTORED TO THEIR ORIGINAL CONDITION.

II. THE POND OVERFLOW ELEVATIONS SHALL BE MAINTAINED AND KEPT FREE OF SEDIMENT, SNOW, DEBRIS, ETC. TO ALLOW UNIMPEDED OVERLAND FLOW IN ORDER TO KEEP STORMWATER RUNOFF FROM FLOWING OVER THE PROPOSED RETAINING WALLSUNPAVED STEEP SLOPES, AND ALONG GOULD STREET AND OTHER AREAS DURING FLOOD CONDITIONS.

12. INFILTRATION POND AREAS ARE NOT TO BE UTILIZED FOR SNOW STORAGE.

Project Name:

HAMMOND STREET **APARTMENTS**

56 Hammond Street Portland, Maine

Architects

Owner: Steven & Roberta Cope 172 Concord Street Portland, Maine 04103

Consultants:

Kevin Moquin, Architect Hammond Stret Portland, Maine 04104 207.615-6421

Civil Engineer Blais Civil Engineers 967 Broadway South Portland, Maine 04106 207.767.7300

Drawing Set: No. Date Revision 12-12-11 PRELIM. CITY REVIEW 2 2-3-12 RE-SUBMIT FOR CITY REVIEW 3 3-5-12 FINAL SITE PLAN

Drawing Set: PERMIT SET: NOT FOR CONSTRUCTION

5 | 5-24-12 | PERMIT SET

3-20-12 RESPONSE TO 3/14/12 CITY COMMENTS

Drawn: Checked:

STEVE G. No. 10084 Approved:

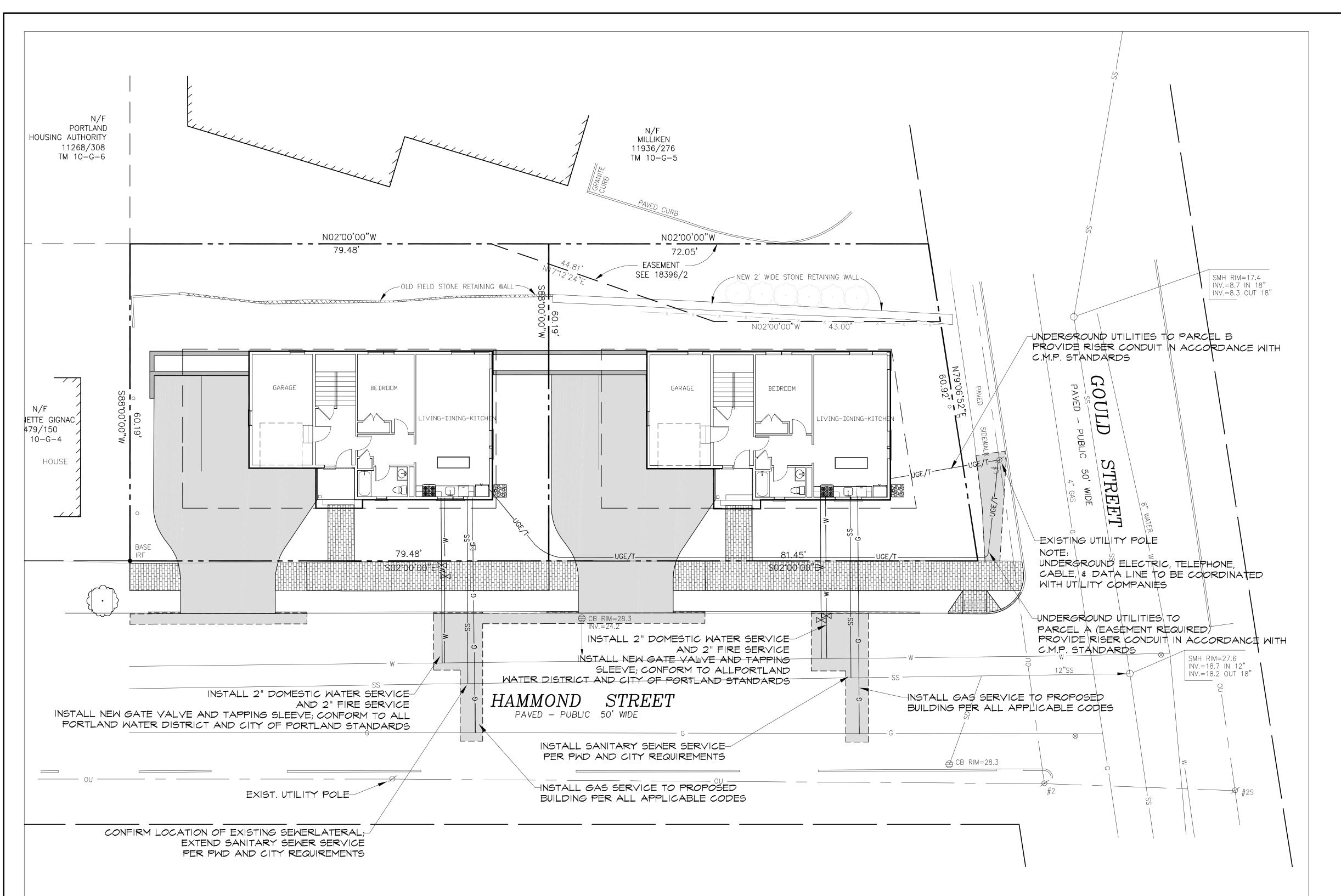
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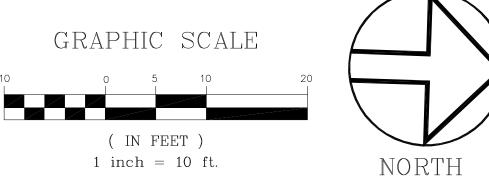
SITE GRADING PLAN

Job Number:

Date: 5-24-12 | Scale: 1"- 10'-0" Drawing Number:

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LEGEND

DESCRIPTION EXISTING PROPOSED PROPERTY/ROW ---SETBACK EASEMENT MONUMENT EDGE PAVEMENT GRAVEL ROAD CURBLINE CONTOURS ====== CULYERT WATER STORM DRAIN UNDERGROUND ---UGE &T---ELEC. & TEL. OVERHEAD ELEC. & TEL. GATE VALVE UTILITY POLE MANHOLE POTABLE WELL WETLANDS ----- EDGE WETLAND BENCHMARK SPOT GRADE CHAIN LINK FENCE RIPRAP \prec STOCKADE FENCE ZONE LINE

UTILITY NOTES

1. EXISITNG CONDITIONS BASED ON TOPOGRAPHIC SURVEY COMPLETED BY OWEN HASKELL SURVEYING IN APRIL 10, 2006. FIELD CONDITIONS MAY YARY SLIGHTLY FROM WHAT IS DEPICTED ON THE PLANS.

3. ALL UTILITY INSTALLATIONS SHALL MEET THE REQUIREMENTS OF THE CITY OF PORTLAND, AS WELL AS ANY OTHER LOCAL, STATE, AND FEDERAL REQUIREMENTS.

2. UTILITY LOCATIONS ARE APPROXIMATE AND SHALL BE VERIFIED PRIOR TO CONSTRUCTION. ANY DISCREPANCIES OR CONFLICTS SHALL BE IMMEDIATELY REPORTED TO THE ARCHITECT.

Project Name:

HAMMOND STREET APARTMENTS

56 Hammond Street Portland, Maine

Architects

Owner: Steven & Roberta Cope 172 Concord Street Portland, Maine 04103

Consultants:

Kevin Moquin, Architect Hammond Stret Portland, Maine 04104 207.615-6421

Civil Engineer Blais Civil Engineers 967 Broadway South Portland, Maine 04106

Drawing Set:

207.767.7300

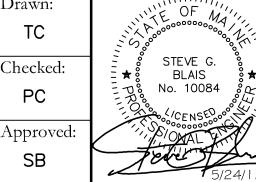
No.	Date	Revision
1	12-12-11	PRELIM. CITY REVIEW
2	2-3-12	RE-SUBMIT FOR CITY REVIEW
3	3-5-12	FINAL SITE PLAN
4	3-20-12	RESPONSE TO 3/14/1 CITY COMMENTS

Drawing Set:

PERMIT SET: NOT FOR CONSTRUCTION

5 | 5-24-12 | PERMIT SET

Drawn: Checked:



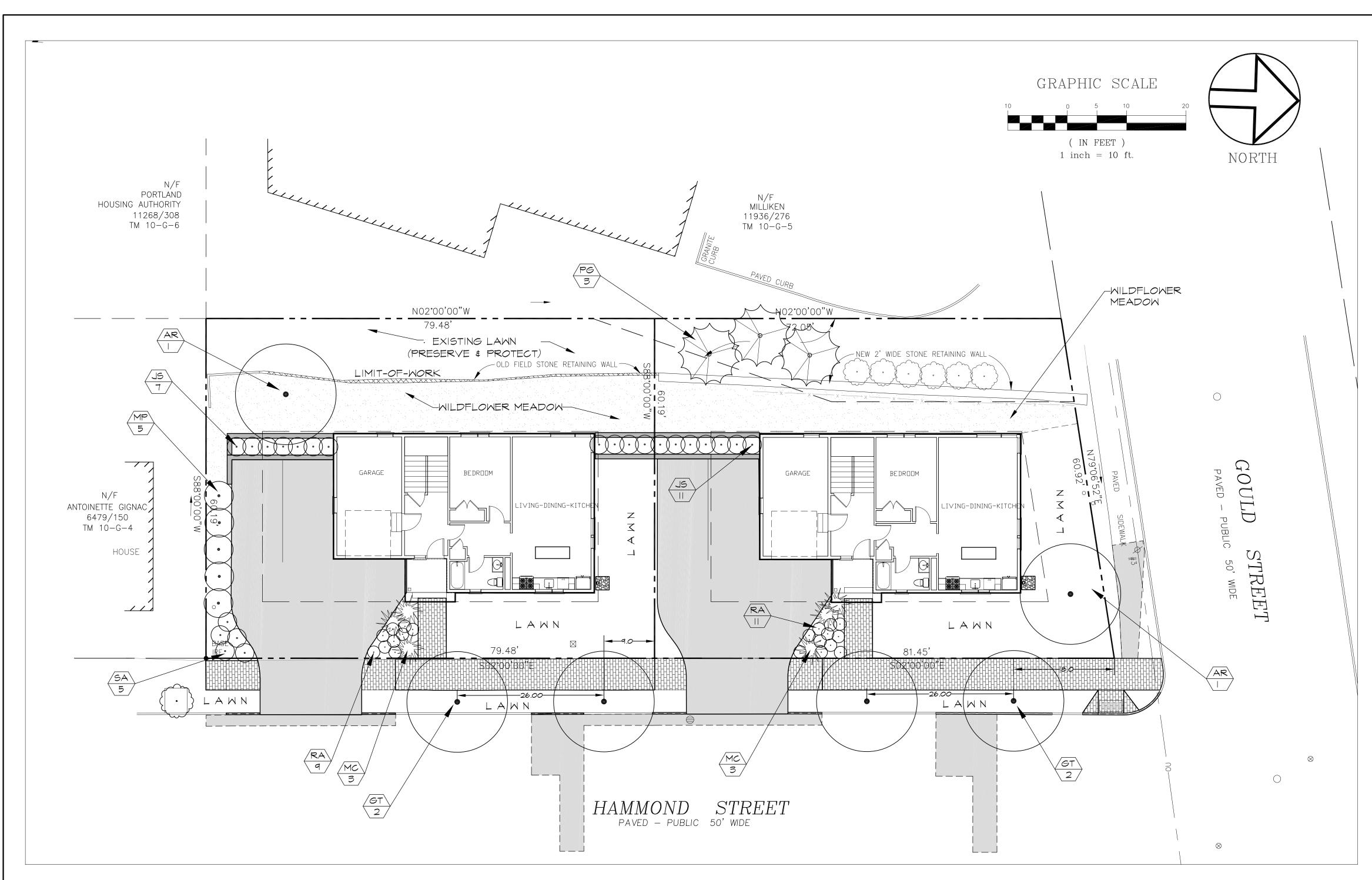
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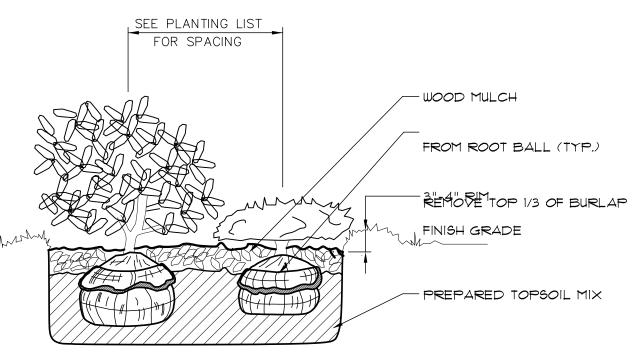
SITE UTILITIES PLAN

Job Number:

Date: **5–24–12** Scale: **1"– 10'–0"** Drawing Number:

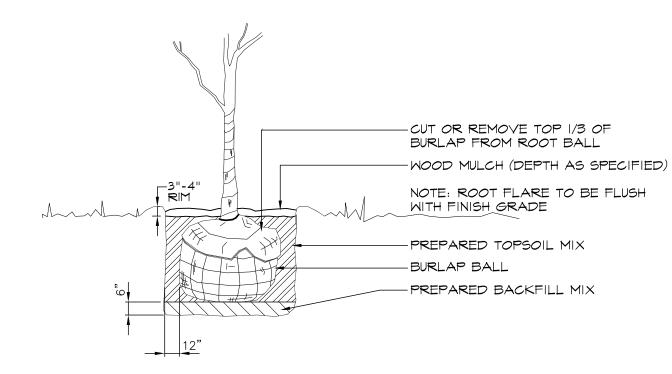
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NOT TO SCALE

SHRUB PLANTING DETAIL



TREE PLANTING DETAIL

NOT TO SCALE

Project Name:

HAMMOND STREET APARTMENTS

56 Hammond Street Portland, Maine

CARROLL ASSOCIATES

Landscape Architects

217 Commercial Street -Portland, Maine 04101
207.772.1552

Owner:
Steven & Roberta Cope
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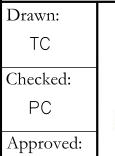
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3	3-5-12	FINAL SITE PLAN

4 3-20-12 RESPONSE TO 3/14/12 CITY COMMENTS

Drawing Set:
PERMIT SET

5 | 5-24-12 | PERMIT SET



PATRICK J. CARROLL NO. 127

Drawing Title:

LANDSCAPE PLAN

Job Number:

Date: 5-24-12 | Scale: 1"- 10'-0" | Drawing Number:

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PLANTING NOTES

- I. CONTRACTOR SHALL BEGIN MAINTENANCE IMMEDIATELY AFTER PLANTING. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MEANS AND METHODS OF WATERING AND MAINTENANCE UNTIL FINAL ACCEPTANCE.
- 2. CONTRACTOR SHALL SUPPLY PLANTS IN QUANTITIES SUFFICIENT TO COMPLETE WORK SHOWN ON THE PLAN. ANY DISCREPANCY
 BETWEEN THE QUANTITIES SHOWN IN THE PLANT SCHEDULE AND THOSE REQUIRED ON THE PLAN SHALL NOT ENTITLE THE CONTRACTOR TO
 ADDITIONAL RENUMERATION. ANY DESCREPANCIES SHALL BE CLARIFIED WITH THE LANDSCAPE ARCHITECT PRIOR TO ORDERING PL;ANT
- MATERIAL.

 3. ALL MATERIALS SHALL CONFORM TO SPECIFICATIONS OF THE AMERICAN STANDARDS FOR NURSERY STOCK (LATEST EDITION) AS SET FORTH BY THE AMERICAN ASSOCIATION OF NURSERYMEN.
- 4. ALL PLANTS SHALL BEAR THE SAME RELATIONSHIP TO FINISH GRADE AS THE ORIGINAL GRADES BEFORE DIGGING.
- 5. THE LANDSCAPE CONTRACTOR SHALL GUARANTEE ALL PLANT MATERIALS FOR ONE (I) FULL YEAR FROM DATE OF SUBSTANTIAL COMPLETION.
- 6. ALL PLANT MATERIALS ARE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT AT THE NURSERY AND AT THE SITE.
- 7. ALL PLANT BEDS SHALL MEET MINIMUM TOPSOIL REQUIREMENTS (SEE SPECIFICATIONS).
- 8. NO PLANT MATERIAL SHALL BE INSTALLED UNTIL GRUBBING, BED PREPARATION, AND FINISH GRADING HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- 9. ALL PLANTS BEDS AND TREE WELLS SHALL HAVE A MINIMUM OF 3" OF UNIFORMLY DISTRIBUTED, DARK, SHREDDED BARK MULCH.
 10. CONTRACTOR SHALL SUBMIT SOIL SAMPLE AND TEST OF TOPSOIL TO LANDSCAPE ARCHITECT PRIOR TO INSTALLATION. SOIL
- ADMIXTURE SHALL BE ADDED TO EXISTING SOIL (BY CONTRACTOR) IF DEEMED NECESSARY BY SOIL TEST RESULTS.
- II. ROUGH GRADING AND BED PREPARATION SHALL BE APPROVED BY LANDSCAPE ARCHITECT PRIOR TO PLANT INSTALLATION.

 CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT 48 HOURS PRIOR TO REQUIRED SITE VISIT.
- 12. ALL PLANTS SHALL BE BALLED AND WRAPPED OR CONTAINER GROWN AS SPECIFIED. ALL ROOT WRAPPING AND CONTAINER MATERIAL MADE OF SYNTHETICS OR PLASTICS SHALL BE REMOVED AT THE TIME OF PLANTING.
- 13. ALL BROAD LEAF EVERGREEN PLANTS SHALL BE SPRAYED WITH AN ANTIDESSICANT AT THE BEGINNING OF THEIR FIRST WINTER.

 14. ALL PLANTS SHALL BE INSTALLED AS PER DETAILS AND THE CONTRACT SPECIFICATIONS. THE LANDSCAPE CONTRACTOR SHALL

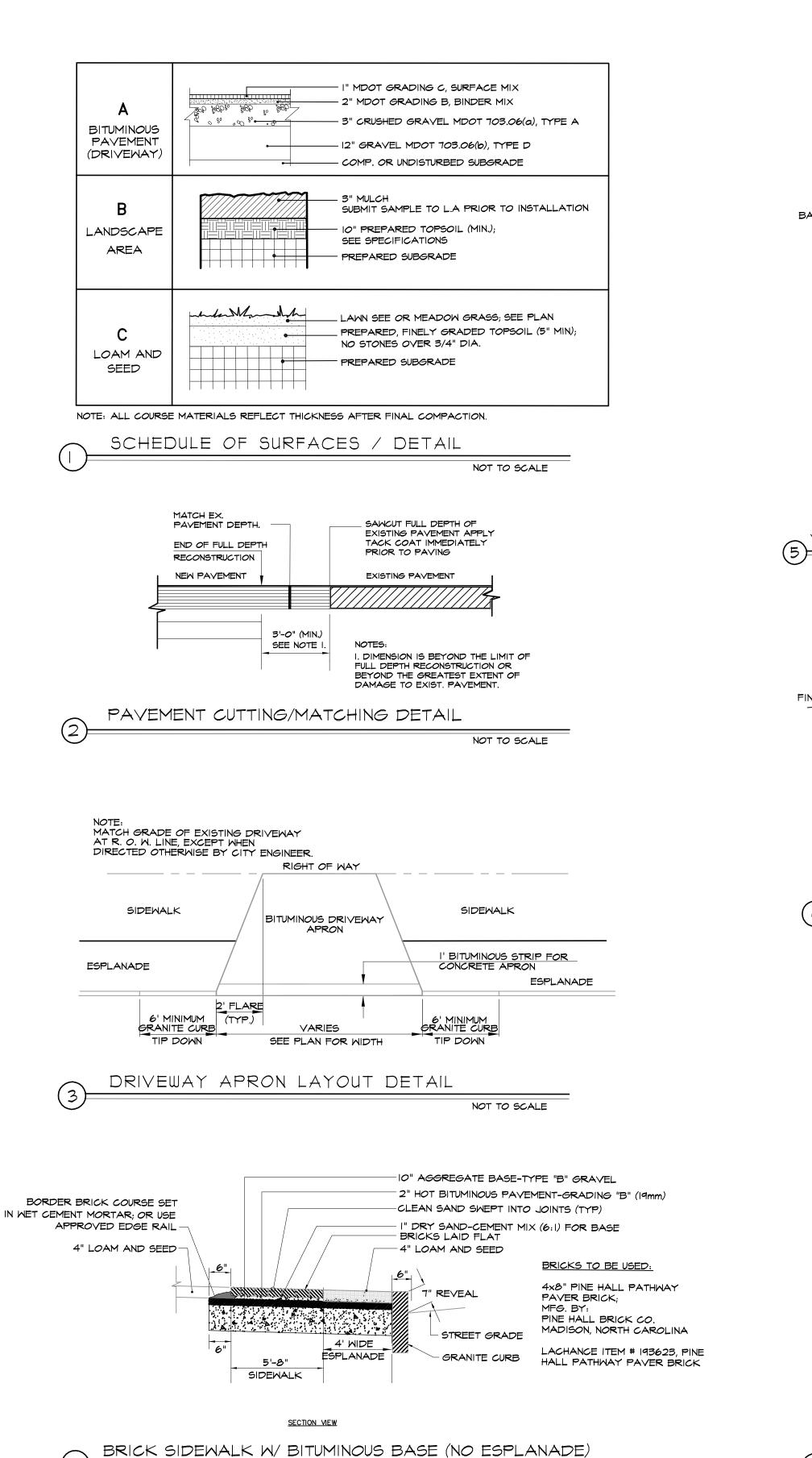
 REFER TO THE CONTRACT SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
- 15. CONTRACTOR SHALL STAKE TREES SHALL IF DEEMED NECESSARY.
- 16. THE LANDSCAPE CONTRACTOR SHALL PROVIDE CLEAN LOAM FILL AS PER THE CONTRACT SPECIFICATIONS.
- 17. THE LANDSCAPE CONTRACTOR SHALL REFER TO THE PLANT LIST AND PLANTING SPECIFICATIONS FOR SEASONAL REQUIREMENTS AND OTHER RESTRICTIONS RELATED TO THE TIME AND SEASON OF PLANTING.
- 16. THE LANDSCAPE CONTRACTOR SHALL PROVIDE CLEAN LOAM FILL AS PER THE CONTRACT SPECIFICATIONS.

 17. THE LANDSCAPE CONTRACTOR SHALL REFER TO THE PLANT LIST AND PLANTING SPECIFICATIONS FOR SEASONAL REQUIREMENTS AND

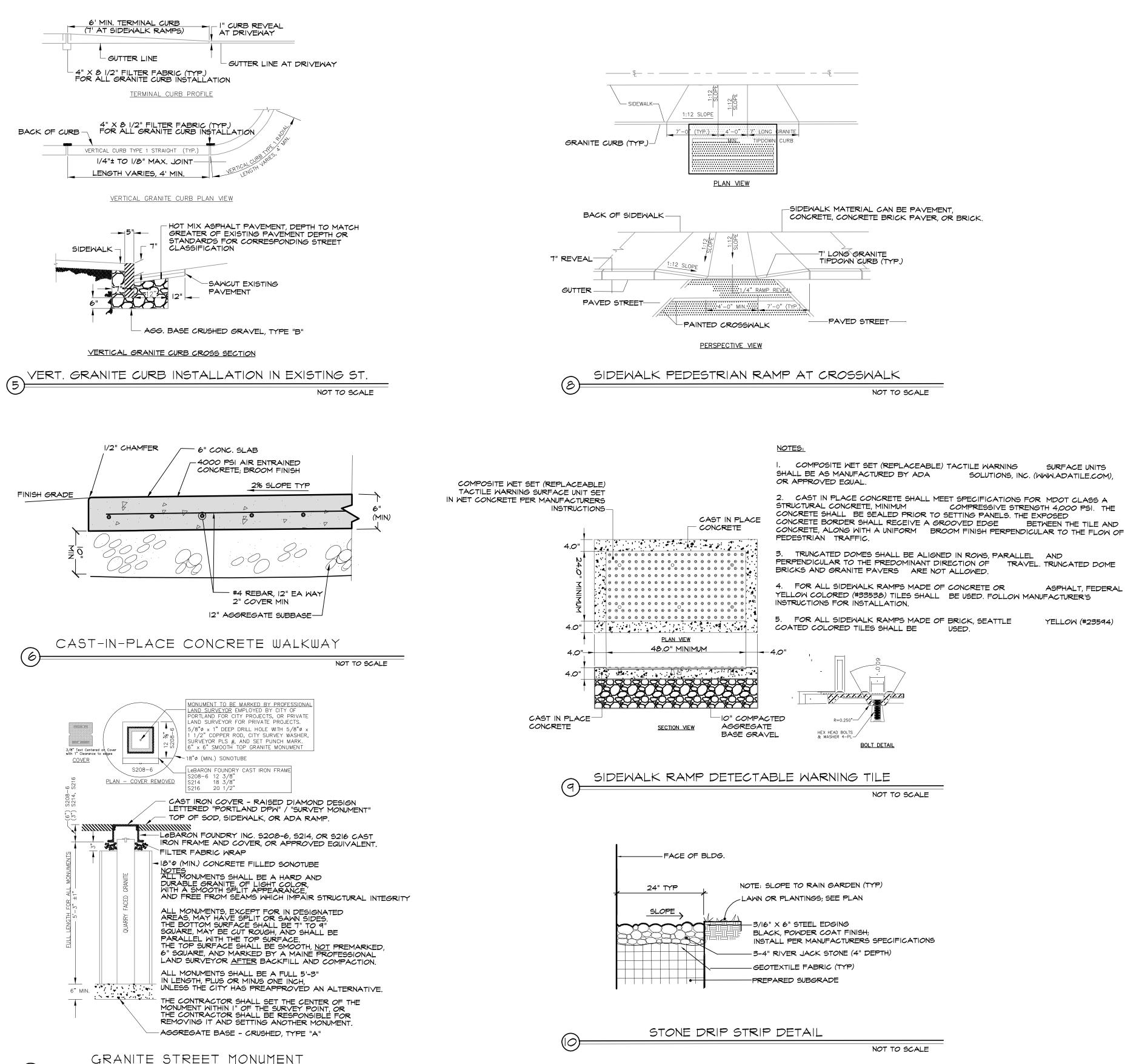
PLANT SCHEDULE

QTY.	SYM.	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS
			TREES		
2	AR	ACER RUBRUM	RED MAPLE	3" CAL.	B≰B
4	G T	GLEDITSIA TRIACANTHOS 'INERMIS'	THORNLESS HONEYLOCUST	3" CAL.	B¢B
3	P6	PICEA GLAUCA 'DENSATA'	BLACK HILS SPRUCE	8-10'	B¢B
		SHRUBS / ORNAMI	ENTAL GRASSES / GROUNDCOVERS/BULB	5	
18	Js	JUNIPERUS SQUAMATA	HOLGER JUNIPER	18-24"	B \$ B
6	MC	MISCANTHUS SINENSIS 'STRICTUS'	PORCUPINE GRASS	3 GAL.	CONT.
5	MP	MYRICA PENNSYLVANICA	NORTHERN BAYBERRY	30-36"	CONT.
20	RA	RHUS AROMATICA 'GRO-LOW'	DWARF FRAGRANT SUMAC	I GAL.	CONT.
5	SA	SALIX PURPUREA	DWARF ARCTIC WILLOW	3 GAL.	CONT.

NOTE: ALL SUBSTITUTIONS SHALL BE APPROVED BY LANDSCAPE ARCHITECT PRIOR TO ORDERING OF PLANTS



NOT TO SCALE



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Project Name:

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5 5-24-12 PERMIT SET

Drawing Set:
PERMIT SET:

NOT FOR CONSTRUCTION

STEVE G.

BLAIS

No. 10084

Drawn:
TC
Checked:
PC
Approved:

PC
Approved:

Drawing Title:

SITE DETAILS

Job Number:

Date: **5–24–12** Scale: **AS SHOWN**

Drawing Number:

1 Solution | 1

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WINTER CONSTRUCTION NOTES

THE WINTER CONSTRUCTION PERIOD IS FROM NOVEMBER 1 THROUGH APRIL 15. IF THE CONSTRUCTION SITE IS NOT STABILIZED WITH PAVEMENT, A ROAD GRAVEL BASE, 75% MATURE VEGETATION COVER, OR RIPRAP BY NOVEMBER 15 THEN THE SITE NEEDS TO BE PROTECTED WITH OVER-WINTER STABILIZATION. AN AREA CONSIDERED OPEN IS ANY AREA NOT STABILIZED WITH PAVEMENT, VEGETATION, MULCHING, EROSION CONTROL MATS, RIPRAP OR GRAVEL BASE ON A ROAD.

WINTER EXCAVATION AND EARTHWORK SHALL BE COMPLETED SUCH THAT NO MORE THAN I ACRE OF THE SITE IS WITHOUT STABILIZATION AT ANY ONE TIME. LIMIT THE EXPOSED AREA TO THOSE AREAS IN WHICH WORK IS EXPECTED TO BE UNDER TAKEN DURING THE PROCEEDING 15 DAYS AND THAT CAN BE MULCHED IN ONE DAY PRIOR TO ANY SNOW EVENT.

ALL AREA SHALL BE CONSIDERED TO BE DENUDED UNTIL THE SUBBASE GRAVEL IS INSTALLED IN ROADWAY AREAS OR THE AREAS OF FUTURE LOAM AND SEED HAVE BEEN LOAMED, SEEDED, AND MULCHED. HAY AND STRAW MULCH RATES SHALL BE A MINIMUM OF 150 LBS./1,000 S.F. (3

TONS/ACRE) AND SHALL BE PROPERLY ANCHORED. THE CONTRACTOR MUST INSTALL ANY ADDED MEASURES WHICH MAY BE NECESSARY TO CONTROL EROSION/SEDIMENTATION FROM THE SITE DEPENDENT UPON THE ACTUAL SITE AND WEATHER CONDITIONS.

CONTINUATION OF EARTHWORK OPERATIONS ON ADDITIONAL AREAS SHALL NOT BEGIN UNTIL THE EXPOSED SOIL SURFACE ON THE AREA BEING WORKED HAS BEEN STABILIZED, IN ORDER TO MINIMIZE AREAS WITHOUT EROSION CONTROL PROTECTION.

STOCKPILES OF SOIL OR SUBSOIL WILL BE MULCHED FOR OVER WINTER PROTECTION WITH HAY OR STRAW AT TWICE THE NORMAL RATE OR AT 150 LBS/1,000 S.F. (3 TONS PER ACRE) OR WITH A FOUR-INCH LAYER OF WOODWASTE EROSION CONTROL MIX. THIS WILL BE DONE WITHIN 24 HOURS OF STOCKING AND RE-ESTABLISHED PRIOR TO ANY RAINFALL OR SNOWFALL. ANY SOIL STOCKPILE WILL NOT BE PLACED (EVEN COVERED WITH HAY OR STRAW) WITHIN 100 FEET FROM ANY NATURAL RESOURCES.

NATURAL RESOURCES PROTECTION

ANY AREAS WITHIN 100 FEET FROM ANY NATURAL RESOURCES, IF NOT STABILIZED WITH A MINIMUM OF 75 % MATURE VEGETATION CATCH, SHALL BE MULCHED BY DECEMBER 1 AND ANCHORED WITH PLASTIC NETTING OR PROTECTED WITH EROSION CONTROL MATS. DURING WINTER CONSTRUCTION, A DOUBLE LINE OF SEDIMENT BARRIERS (I.E. SILT FENCE BACKED WITH HAY BALES OR EROSION CONTROL MIX) WILL BE PLACED BETWEEN ANY NATURAL RESOURCE AND THE DISTURBED AREA.

PROJECTS CROSSING THE NATURAL RESOURCE SHALL BE PROTECTED A MINIMUM DISTANCE OF 100 FEET ON EITHER SIDE FROM THE RESOURCE. EXISTING PROJECTS NOT STABILIZED BY DECEMBER 1 SHALL BE PROTECTED WITH THE SECOND LINE OF SEDIMENT BARRIER TO ENSURE FUNCTIONALITY DURING THE SPRING THAW AND RAINS.

SEDIMENT BARRIERS

DURING FROZEN CONDITIONS, SEDIMENT BARRIERS SHALL CONSIST OF WOODWASTE FILTER BERMS AS FROZEN SOIL PREVENTS THE PROPER INSTALLATION OF HAY BALES AND SEDIMENT SILT FENCES.

ALL AREA SHALL BE CONSIDERED TO BE DENUDED UNTIL AREAS OF FUTURE LOAM AND SEED HAVE BEEN LOAMED, SEEDED AND MULCHED. HAY AND STRAW MULCH SHALL BE APPLIED AT A RATE OF 150 LB. PER 1.000 SQUARE FEET OR 3 TONS/ACRE (TWICE THE NORMAL ACCEPTED RATE OF 75-LBS./1,000 S.F. OR 1.5 TONS/ACRE) AND SHALL BE PROPERLY ANCHORED.

MULCH SHALL NOT BE SPREAD ON TOP OF SNOW. THE SNOW WILL BE REMOVED DOWN TO A ONE-INCH DEPTH OR LESS PRIOR TO APPLICATION. AFTER EACH DAY OF FINAL GRADING, THE AREA WILL BE PROPERLY STABILIZED WITH ANCHORED HAY OR STRAW OR EROSION CONTROL

AN AREA SHALL BE CONSIDERED TO HAVE BEEN STABILIZED WHEN EXPOSED SURFACES HAVE BEEN EITHER MULCHED WITH STRAW OR HAY AT A RATE OF 150 LB. PER 1.000 SQUARE FEET (3TONS/ACRE) AND ADEQUATELY ANCHORED SO THAT GROUND SURFACE IS NOT VISIBLE THOUGH

BETWEEN THE DATES OF NOVEMBER I AND APRIL 15, ALL MULCH SHALL BE ANCHORED BY EITHER PEG LINE, MULCH NETTING, ASPHALT EMULSION CHEMICAL, TRACK OR WOOD CELLULOSE FIBER. WHEN GROUND SURFACE IS NOT VISIBLE THOUGH THE MULCH THEN COVER IS SUFFICIENT.

AFTER NOVEMBER 1ST, MULCH AND ANCHORING OF ALL BARE SOIL SHALL OCCUR AT THE END OF EACH FINAL GRADING WORK DAY.

MULCHING ON SLOPES AND DITCHES

SLOPES SHALL NOT BE LEFT EXPOSED FOR ANY EXTENDED TIME OF WORK SUSPENSION UNLESS FULLY MULCHED AND ANCHORED WITH PEG AND NETTING OR WITH EROSION CONTROL BLANKETS.

MULCHING SHALL BE APPLIED AT A RATE OF 230 LBS/1,000 S.F. ON ALL SLOPES GREATER THAN 8 %. MULCH NETTING SHALL BE USED TO ANCHOR MULCH IN ALL DRAINAGE WAYS WITH A SLOPE GREATER THAN 3 % FOR SLOPES EXPOSED TO DIRECT WINDS AND FOR ALL OTHER SLOPES GREATER THAN 8 %.

EROSION CONTROL BLANKETS SHALL BE USED IN LIEU OF MULCH IN ALL DRAINAGE WAYS WITH SLOPES OF 8 %.

EROSION CONTROL MIX CAN BE USED TO SUBSTITUTE EROSION CONTROL BLANKETS ON ALL SLOPES EXCEPT DITCHES.

BETWEEN THE DATES OF OCTOBER 15 AND APRIL 1ST, LOAM OR SEED WILL NOT BE REQUIRED. DURING PERIODS OF ABOVE FREEZING TEMPERATURES FINISHED AREAS SHALL BE FINE GRADED AND EITHER PROTECTED WITH MULCH OR TEMPORARILY SEEDED AND MULCHED UNTIL SUCH TIME AS THE FINAL TREATMENT CAN BE APPLIED. IF THE DATE IS AFTER NOVEMBER 1ST AND IF THE EXPOSED AREA HAS BEEN LOOMED, FINAL GRADED WITH A UNIFORM SURFACE, THEN THE AREA MAY BE DORMANT SEEDED AT A RATE OF 3 TIMES HIGHER THAN SPECIFIED FOR PERMANENT SEED AND THEN MULCHED.

DORMANT SEEDING MAY BE SELECTED TO BE PLACED PRIOR TO THE PLACEMENT OF MULCH AND FABRIC NETTING ANCHORED WITH STAPLES. IF DORMANT SEEDING IS USED FOR THE SITE, ALL DISTURBED AREAS SHALL RECEIVE 4" OF LOAM AND SEED AT AN APPLICATION RATE OF 5LBS/1000 S.F. ALL AREAS SEEDED DURING THE WINTER WILL BE INSPECTED IN THE SPRING FOR ADEQUATE CATCH. ALL AREAS SUFFICIENTLY VEGETATED (LESS THAN 75 % CATCH) SHALL BE REVEGETATED BY REPLACING LOAM, SEED AND MULCH. IF DORMANT SEEDING IS NOT USED FOR THE SITE, ALL DISTURBED AREAS SHALL BE REVEGETATED IN THE SPRING.

TRENCH DEWATERING AND TEMPORARY STREAM DIVERSION

WATER FROM CONSTRUCTION TRENCH DEWATERING OR TEMPORARY STREAM DIVERSIONS WILL PASS FIRST THROUGH A FILTER BAG OR SECONDARY CONTAINMENT STRUCTURE (E.G. HAY BALE LINED POOL) PRIOR TO DISCHARGE. THE DISCHARGE SITE SHALL BE SELECTED TO AVOID FLOODING, ICING, AND SEDIMENT DISCHARGES TO A PROTECTED RESOURCE. IN NO CASE SHALL THE FILTER BAG OR CONTAINMENT STRUCTURE BE LOCATED WITHIN 100 FEET OF A PROTECTED NATURAL RESOURCE.

INSPECTION AND MONITORING

MAINTENANCE MEASURES SHALL BE APPLIED AS NEEDED DURING THE ENTIRE CONSTRUCTION SEASON. AFTER EACH RAINFALL, SNOW STORM OR PERIOD OF THAWING AND RUNOFF, THE SITE CONTRACTOR SHALL PERFORM A VISUAL INSPECTION OF ALL INSTALLED EROSION CONTROL MEASURES AND PERFORM REPAIRS AS NEEDED TO INSURE THEIR CONTINUOUS FUNCTION.

FOLLOWING THE TEMPORARY AND OR FINAL SEEDING AND MULCHING. THE CONTRACTOR SHALL IN THE SPRING INSPECT AND REPAIR ANY DAMAGES AND/ OR UNESTABLISHED SPOTS. ESTABLISHED VEGETATIVE COVER MEANS A MINIMUM OF 85 TO 90 % OF AREAS VEGETATED WITH VIGOROUS GROWTH.

STANDARDS FOR TIMELY STABILIZATION OF CONSTRUCTION SITES DURING WINTER:

STANDARD FOR THE TIMELY STABILIZATION OF DITCHES AND CHANNELS

THE APPLICANT WILL CONSTRUCT AND STABILIZE ALL STONE-LINED DITCHES AND CHANNELS ON THE SITE BY NOVEMBER 15. THE APPLICANT WILL CONSTRUCT AND STABILIZE ALL GRASS-LINED DITCHES AND CHANNELS ON THE SITE BY SEPTEMBER 15. IF THE APPLICANT FAILS TO STABILIZE A DITCH OR CHANNEL TO BE GRASS-LINED BY SEPTEMBER 15, THEN THE APPLICANT WILL TAKE ONE OF THE FOLLOWING ACTIONS TO STABILIZE THE DITCH FOR LATE FALL AND WINTER.

INSTALL A SOD LINING IN THE DITCH -- THE APPLICANT WILL LINE THE DITCH WITH PROPERLY INSTALLED SOD BY OCTOBER 1. PROPER INSTALLATION INCLUDES THE APPLICANT PINNING THE SOD ONTO THE SOIL WITH WIRE PINS, ROLLING THE SOD TO GUARANTEE CONTACT

BETWEEN THE SOD AND UNDERLYING SOIL, WATERING THE SOD TO PROMOTE ROOT GROWTH INTO THE DISTURBED SOIL, AND ANCHORING THE SOD WITH JUTE OR PLASTIC MESH TO PREVENT THE SOD STRIPS FROM SLOUGHING DURING FLOW CONDITIONS INSTALL A STONE LINING IN THE DITCH --THE APPLICANT WILL LINE THE DITCH WITH STONE RIPRAP BY NOVEMBER 15. THE APPLICANT WILL HIRE A REGISTERED PROFESSIONAL ENGINEER TO DETERMINE THE STONE SIZE AND LINING THICKNESS NEEDED TO WITHSTAND THE ANTICIPATED FLOW VELOCITIES AND FLOW DEPTHS WITHIN THE DITCH. IF NECESSARY, THE APPLICANT WILL REGRADE THE DITCH PRIOR TO PLACING THE STONE LINING TO PREVENT THE STONE LINING FROM REDUCING THE DITCH'S CROSS-SECTIONAL AREA.

STANDARD FOR THE TIMELY STABILIZATION OF DISTURBED SLOPES

THE APPLICANT WILL CONSTRUCT AND STABILIZE STONE-COVERED SLOPES BY NOVEMBER 15. THE APPLICANT WILL SEED AND MULCH ALL SLOPES TO BE VEGETATED BY SEPTEMBER 15. THE DEPARTMENT WILL CONSIDER ANY AREA HAVING A GRADE GREATER THAN 15% (10H:1V) TO BE A SLOPE. IF THE APPLICANT FAILS TO STABILIZE ANY SLOPE TO BE VEGETATED BY SEPTEMBER 15, THEN THE APPLICANT WILL TAKE ONE OF THE FOLLOWING ACTIONS TO STABILIZE THE SLOPE FOR LATE FALL AND WINTER.

STABILIZE THE SOIL WITH TEMPORARY VEGETATION AND EROSION CONTROL MATS -- BY OCTOBER | THE APPLICANT WILL SEED THE DISTURBED SLOPE WITH WINTER RYE AT A SEEDING RATE OF 3 POUNDS PER 1000 SQUARE FEET AND APPLY EROSION CONTROL MATS OVER THE MULCHED SLOPE. THE APPLICANT WILL MONITOR GROWTH OF THE RYE OVER THE NEXT 30 DAYS. IF THE RYE FAILS TO GROW AT LEAST THREE INCHES OR COVER AT LEAST 75% OF THE DISTURBED SLOPE BY NOVEMBER 1, THEN THE APPLICANT WILL COVER THE SLOPE WITH A LAYER OF WOODWASTE COMPOST AS DESCRIBED IN ITEM III OF THIS CONDITION OR WITH STONE RIPRAP AS DESCRIBED IN ITEM IV OF THIS CONDITION. STABILIZE THE SLOPE WITH SOD -- THE APPLICANT WILL STABILIZE THE DISTURBED SLOPE WITH PROPERLY INSTALLED SOD BY OCTOBER 1. PROPER INSTALLATION INCLUDES THE APPLICANT PINNING THE SOD ONTO THE SLOPE WITH WIRE PINS, ROLLING THE SOD TO GUARANTEE CONTACT BETWEEN THE SOD AND UNDERLYING SOIL, AND WATERING THE SOD TO PROMOTE ROOT GROWTH INTO THE DISTURBED SOIL. THE APPLICANT WILL NOT USE LATE-SEASON SOD INSTALLATION TO STABILIZE SLOPES HAVING A GRADE GREATER THAN 33% (3H: I V). STABILIZE THE SLOPE WITH WOODWASTE COMPOST -- THE APPLICANT WILL PLACE A SIX-INCH LAYER OF WOODWASTE COMPOST ON THE SLOPE BY NOVEMBER 15. PRIOR TO PLACING THE WOODWASTE COMPOST, THE APPLICANT WILL REMOVE ANY SNOW ACCUMULATION ON THE

OR HAVING GROUNDWATER SEEPS ON THE SLOPE FACE. STABILIZE THE SLOPE WITH STONE RIPRAP -- THE APPLICANT WILL PLACE A LAYER OF STONE RIPRAP ON THE SLOPE BY NOVEMBER 15. THE APPLICANT WILL HIRE A REGISTERED PROFESSIONAL ENGINEER TO DETERMINE THE STONE SIZE NEEDED FOR STABILITY AND TO DESIGN A FILTER LAYER FOR UNDERNEATH THE RIPRAP.

DISTURBED SLOPE. THE APPLICANT WILL NOT USE WOODWASTE COMPOST TO STABILIZE SLOPES HAVING GRADES GREATER THAN 50% (2H: I V)

WINTER CONSTRUCTION NOTES (CONTINUED)

3. STANDARD FOR THE TIMELY STABILIZATION OF DISTURBED SOILS

BY SEPTEMBER 15 THE APPLICANT WILL SEED AND MULCH ALL DISTURBED SOILS ON AREAS HAVING A SLOPE LESS THAN 15%. IF THE APPLICANT FAILS TO STABILIZE THESE SOILS BY THIS DATE, THEN THE APPLICANT WILL TAKE ONE OF THE FOLLOWING ACTIONS TO STABILIZE THE SOIL FOR LATE FALL AND WINTER.

STABILIZE THE SOIL WITH TEMPORARY VEGETATION -- BY OCTOBER 1 THE APPLICANT WILL SEED THE DISTURBED SOIL WITH WINTER RYE AT A

SEEDING RATE OF 3 POUNDS PER 1000 SQUARE FEET, LIGHTLY MULCH THE SEEDED SOIL WITH HAY OR STRAW AT 75 POUNDS PER 1000 SQUARE FEET, AND ANCHOR THE MULCH WITH PLASTIC NETTING. THE APPLICANT WILL MONITOR GROWTH OF THE RYE OVER THE NEXT 30 DAYS. IF THE RYE FAILS TO GROW AT LEAST THREE INCHES OR COVER AT LEAST 75% OF THE DISTURBED SOIL BEFORE NOVEMBER 15, THEN THE APPLICANT WILL MULCH THE AREA FOR OVER-WINTER PROTECTION AS DESCRIBED IN ITEM III OF THIS STANDARD. STABILIZE THE SOIL WITH SOD -- THE APPLICANT WILL STABILIZE THE DISTURBED SOIL WITH PROPERLY INSTALLED SOD BY OCTOBER 1

PROPER INSTALLATION INCLUDES THE APPLICANT PINNING THE SOD ONTO THE SOIL WITH WIRE PINS, ROLLING THE SOD TO GUARANTEE CONTACT BETWEEN THE SOD AND UNDERLYING SOIL, AND WATERING THE SOD TO PROMOTE ROOT GROWTH INTO THE DISTURBED SOIL. STABILIZE THE SOIL WITH MULCH -- BY NOVEMBER 15 THE APPLICANT WILL MULCH THE DISTURBED SOIL BY SPREADING HAY OR STRAW AT A RATE OF AT LEAST 150 POUNDS PER 1000 SQUARE FEET ON THE AREA SO THAT NO SOIL IS VISIBLE THROUGH THE MULCH. PRIOR TO APPLYING THE MULCH, THE APPLICANT WILL REMOVE ANY SNOW ACCUMULATION ON THE DISTURBED AREA. IMMEDIATELY AFTER APPLYING THE MULCH. THE APPLICANT WILL ANCHOR THE MULCH WITH PLASTIC NETTING TO PREVENT WIND FROM MOVING THE MULCH OFF THE DISTURBED SOIL.

EROSION CONTROL NOTES

- EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE PROVIDED IN ACCORDANCE WITH THE LATEST "MAINE EROSION AND SEDIMENT CONTROL BMPS" BY THE BUREAU OF LAND AND WATER QUALITY, MAINE DEPARTMENT OF ENVIRONMENTAL PROTECTION.
- 2. THE CONTRACTOR SHALL ONLY DISTURB THE AREAS OF THE PROPOSED CONSTRUCTION AND GRADING. ANY DISTURBANCE OUTSIDE THESE LIMITS MUST BE APPROVED BY THE ENGINEER.
- 3. THE CONTRACTOR SHALL INSTALL ALL EROSION AND SEDIMENTATION CONTROL MEASURES IN ACCORDANCE WITH THE EROSION CONTROL PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EMPLOYING EROSION CONTROL METHODS BEYOND THE CONTROLS SHOWN ON THE PLANS IN ORDER TO MEET THE ABOVE-REFERENCED DEP EROSION CONTROL STANDARDS.
- 4. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY SITE EXCAVATION OR RE-GRADING. ALL DISTURBED AREAS ON SITE NOT COVERED BY BUILDINGS OR DESIGNATED PARKING AREAS, DRIVEWAYS, OR SIDEWALKS SHALL BE STABILIZED WITH LOAM AND SEED OR OTHER METHODS AS REQUIRED DESCRIBED IN THE MAINE DEP BMP STANDARDS.
- PERMANENT SEEDING OR STABILIZATION SHALL BE PERFORMED IMMEDIATELY AFTER FINAL GRADING IS COMPLETED OR TEMPORARY MEASURES SHALL BE APPLIED SUCH AS MULCHING OR SEEDING UNTIL PERMANENT MEASURES ARE IN PLACE.
- 6. WITHIN 7 CALENDAR DAYS FOLLOWING THE COMPLETION OF ANY SOIL DISTURBANCE, AND PRIOR TO ANY STORM EVENT, MULCH MUST BE SPREAD ON ANY EXPOSED SOILS.
- 7. THE CONTRACTOR SHALL STABILIZE ANY SOIL STOCKPILES WHICH WILL REMAIN UNUSED FOR MORE THAN 7 DAYS, OR PRIOR TO A STORM
- 8. ALL EROSION CONTROL DEVICES MUST BE CHECKED WEEKLY AND AFTER EACH SIGNIFICANT RAINFALL TO MINIMIZE PONDING, DAMAGE, DETERIORATION OR UNDERMINING. ANY PROBLEMS SHALL BE REPAIRED IMMEDIATELY. TRAPPED SEDIMENT SHALL BE REMOVED WHEN IT HAS ACCUMULATED TO NO MORE THAN HALF THE ORIGINAL HEIGHT OF ANY BARRIER OR AS OTHERWISE SHOWN ON THE PLANS
- 9. ALL TRAFFIC INTO AND OUT OF THE SITE SHALL BE OVER THE STABILIZED CONSTRUCTION EXIT.
- IO. LINEAR UTILITY CONSTRUCTION SHALL BE SEEDED AND MULCHED WITHIN 7 DAYS AFTER BACKFILL AND NO MORE THAN 500 FEET SHALL BE OPEN AT ANY ONE TIME. WHERE CONSISTENT WITH JOB SAFETY REQUIREMENTS, ALL EXCAVATED MATERIALS SHALL BE PLACED ON THE UPHILL SIDES OF ALL TRENCHES. ALL TEMPORARY EARTH BERMS SHALL BE SEEDED AND MULCHED WITH TEMPORARY VEGETATION WITHIN 7 DAYS AFTER GRADING.
- 11. SEDIMENT BARRIERS MUST BE MAINTAINED UNTIL DISTURBED AREAS ARE PERMANENTLY STABILIZED.
- 12. EROSION CONTROL DEVICES SUCH AS SILT FENCE AND INLET PROTECTION SHALL BE REMOVED WITHIN 30 DAYS OF FINAL STABILIZATION.
- 13. SEEDED AREAS SHALL BE FERTILIZED AND RESEEDED AS NECESSARY TO ENSURE VEGETATION IS ESTABLISHED.
- 14. SILT BARRIER TYPE TO BE FIELD DETERMINED BY DESIGN ENGINEER.

EROSION CONTROL SEEDING NOTES

- 1. ALL FINAL SEEDING SHALL BE COMPLETED WITHIN SEVEN (7) DAYS FOLLOWING FINAL GRADING.
- 2. A CONSERVATION SEED MIX SHALL BE USED FOR ALL SEEDING.
- 3. ALL AREAS SHALL BE MULCHED IMMEDIATELY AFTER SEEDING. THE CONTRACTOR SHALL MONITOR THE MULCH PERFORMANCE AND, IF MULCHING PROVES TO BE INEFFECTIVE, THEN NETTING AND MATTING SHALL BE USED IN IT'S PLACE.
- 4. SEEDING SHALL BE PERFORMED BETWEEN APRIL 15TH AND OCTOBER 1ST (WITHOUT DORMANT SEEDING).
- 5. IF SEEDING IS APPROVED BY THE ENGINEER BEYOND THOSE DATES, DORMANT SEEDING SHALL BE APPLIED AT DOUBLE THE APPLICATION RATE. IN THIS CASE, ALL FERTILIZING, SEEDING AND MULCHING SHALL BE COMPLETED ON THE SAME DAY IMMEDIATELY AFTER THE LOAM IS SPREAD. FINAL GRADING SHALL BE LIMITED TO AREAS WHICH CAN BE COMPLETED AND SEEDED THE SAME DAY.

SEEDING NOTES

1. USE PERMANENT SEED MIXES AND RATES BETWEEN 5/15 AND 9/30.

IMING AND FERTILIZER RATES WILL BE BASED ON FIELD SOIL TESTING OF ON-SITE TOPSOILS BY A CERTIFIED LABORATORY SUBMIT TEST RESULTS TO THE ENGINEER.

AS REQUIRED MODERATE TO HIGH

VELOCITY AREAS \$ STEEP SLOPES

STRAW OR HAY (ANCHORED) 70-90 LBS PROTECTED AREAS STRAW OR HAY (ANCHORED) 185-275 LBS WINDY AREAS SHREDDED OR CHOPPED 185-275 LBS

AS REQUIRED

EXCELSIOR MAT

JUTE MESH

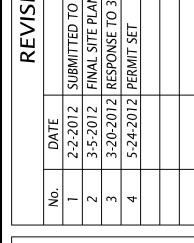
MULCH ANCHORING: PEG AND TWINE LIQUID ASPHALT MULCH NETTING WOOD CELLULOSE FIBER ASPHALT EMULSION CHEMICAL TACK

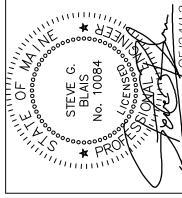
LAWN SEED MIXTURE				
NAME	PROPORTION BY WEIGHT			
CREEPING RED	40%			
FESCUE				
TRIFECTA PERENNIAL RYEGRASS	30%			
KENBLUE KENTUCKY BLUEGRASS	20%			
CHEWINGS FESCUE	10%			

WILDFLOWER MIX FOR SHADE

PERENNIALS: 5% DWARF CORNFLOWER; 10% SIBERIAN WALLFLOWER; 10% OX-EYE DAISY; 10% ROCKET LARKSPUR; 10% LANCE-LEAVED COREOPSIS; 10% SWEET WILLIAM; 2% FOX-GLOVE; 10% PURPLE CONEFLOWER; 10% DAME'S ROCKET, 5% CORN POPPY; 10% BLACK-EYED SUSAN, 1% JOHNNY-JUMP-UP

ANNUALS: 5% ANNUAL BABYS BREATH' 2% BABY SNAPDRAGON





RT AND REET

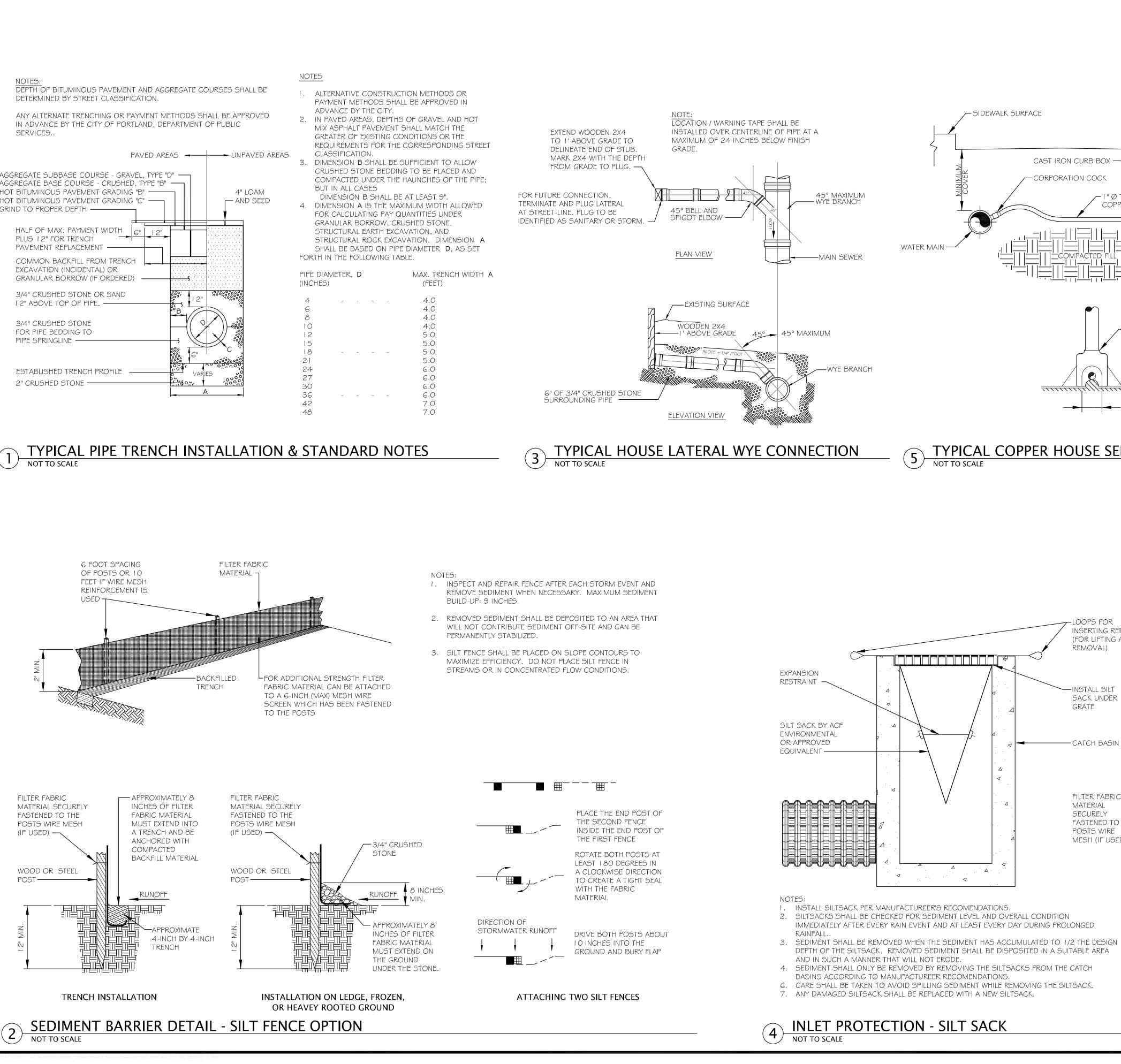
STREET

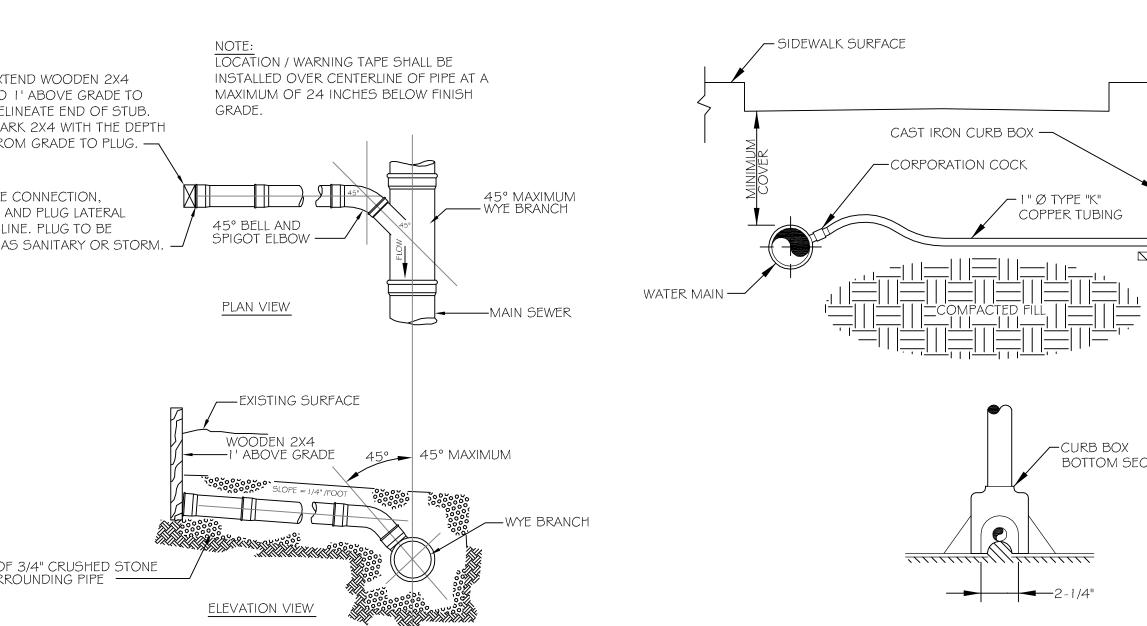
MAINE

AM

C1.0

SHEET 2 OF 3





FINISHED GRADE -CURB STOP **BOTTOM SECTION**

TYPICAL COPPER HOUSE SERVICE DETAIL

INSERTING REBAR

(FOR LIFTING AND

REMOVAL)

-INSTALL SILT

GRATE

SACK UNDER

— CATCH BASIN

FILTER FABRIC

FASTENED TO THE

MATERIAL

SECURELY

POSTS WIRE

MESH (IF USED)

IMMEDIATELY AFTER EVERY RAIN EVENT AND AT LEAST EVERY DAY DURING PROLONGED

DEPTH OF THE SILTSACK. REMOVED SEDIMENT SHALL BE DISPOSITED IN A SUITABLE AREA

RAINFALL..

AND IN SUCH A MANNER THAT WILL NOT ERODE.

BASINS ACCORDING TO MANUFACTUREER RECOMENDATIONS.

- SAND BEDDING OR AS SPECIFIED BY ENGINEER — COMPACTED SUBGRADE 1. ALL WORK TO COMPLY WITH UTILTY COMPANY STANDARDS. CONTRACTOR TO PROVIDE $\frac{1}{4}$ " POLYPROPYLENE PULL ROPES IN ALL CONDUITS.

4" MIN. LOAM & SEED. MOUND AS

- BACKFILL WITH EXCAVATED MATERIAL

- PLASTIC WARNING TAPE, 12" MIN

ABOVE UTILITY OR AS DIRECTED BY

OR SELECT BACKFILL AS DIRECTED BY

AFTER SETTLING

ENGINEER

ENGINEER

NECESSARY TO ACHEIVE FINAL GRADES

PAVEMENT-

SEE DETAIL | LAWN AREA

ELECTRIC UTILITY TRENCH SECTION

1. THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE LOCATED WHERE TRAFFIC WILL ENTER

OR LEAVE THE CONSTRUCTION SITE ONTO A PUBLIC STREET. 2. FILTER FABRIC OR COMPACTED CRUSHER RUN STONE SHALL BE USED AS A BASE FOR THE CONSTRUCTION ENTRANCE.

3. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC STREETS OR EXISTING PAVEMENT. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS WARRANT AND REPAIR OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.

4. ANY SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO PUBLIC STREETS MUST BE REMOVED IMMEDIATELY.

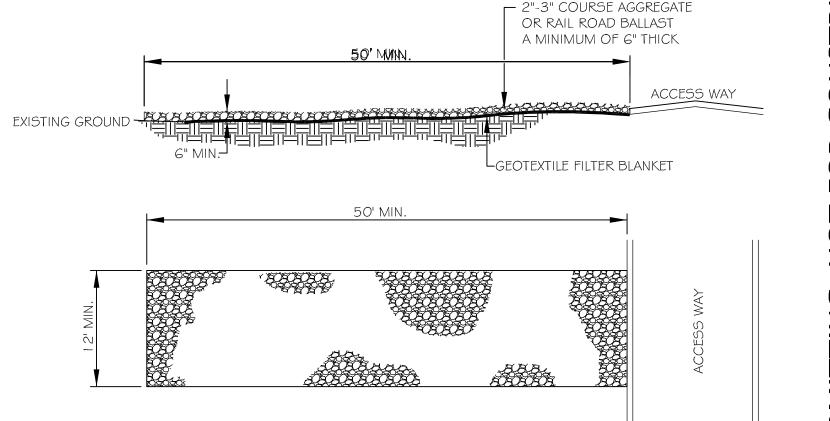
5. ADDITIONAL EROSION CONTROL MEASURES SHALL BE INSTALLED IN ORDER TO TRAP AND STABILIZE ANY SEDIMENT THAT RUNS OFF OF THE STABILIZED CONSTRUCTION ENTRANCE.

6. WHEN APPROPRIATE, WHEELS MUST BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTERING

7. WHEN WASHING IS REQUIRED, IT SHALL BE DONE IN AN AREA STABILIZED WITH CRUSHED

A PUBLIC STREET.

STONE WHICH DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.



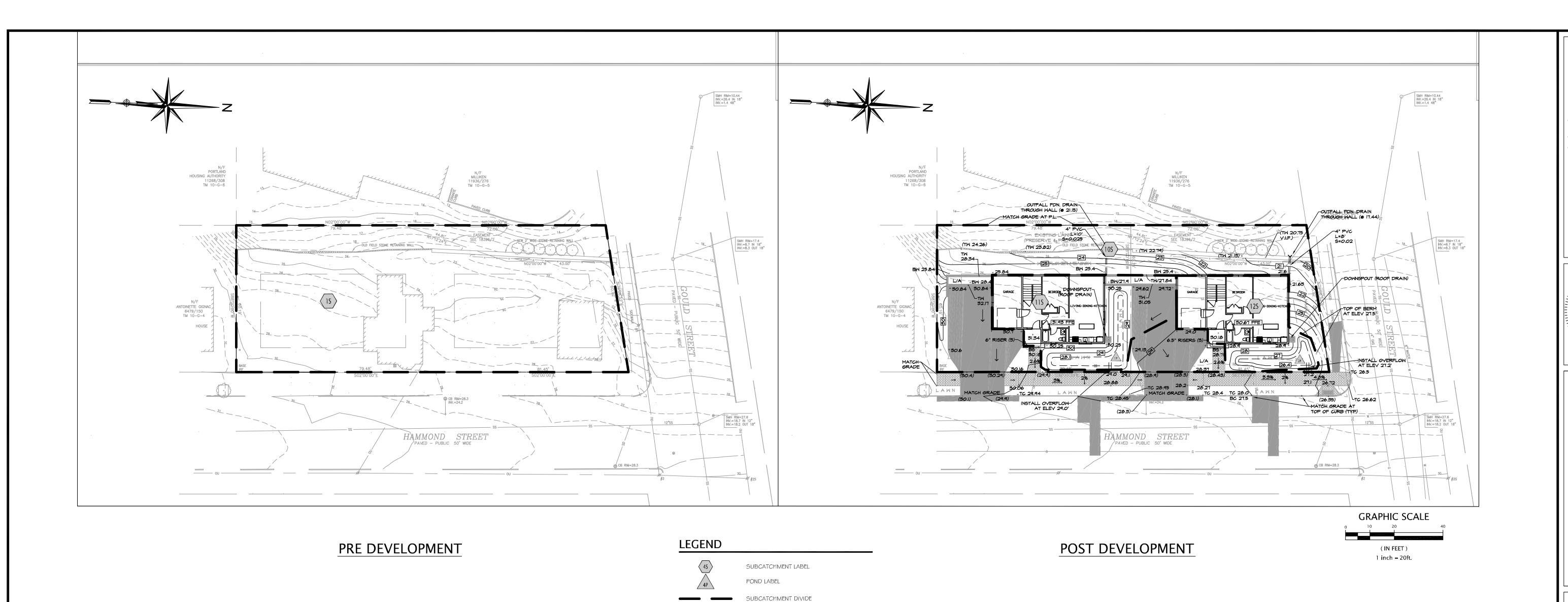
STABILIZED CONSTRUCTION ENTRANCE DETAIL

REET

STREET

MAINE

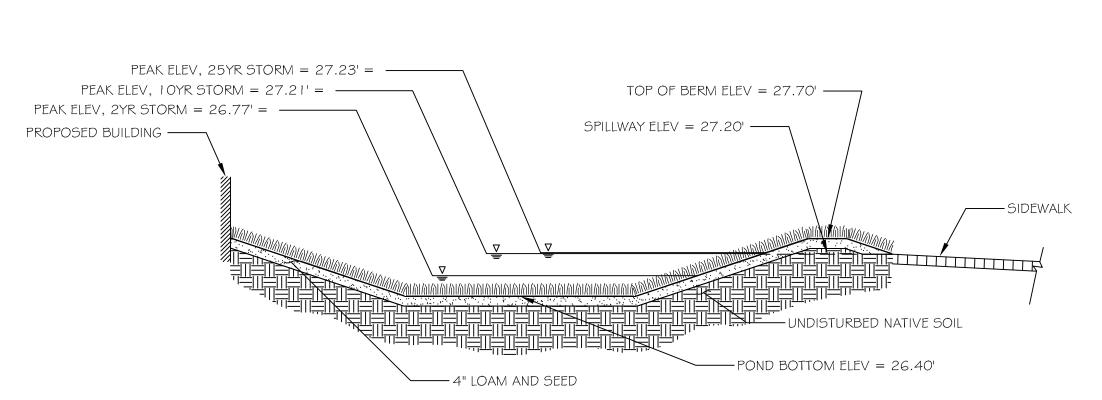
SHEET 2 OF 3



PEAK ELEV, 25YR STORM = 29.02' = ----TOP OF BERM ELEV = 29.50' ----PEAK ELEV, 10YR STORM = 28.89' = ----PEAK ELEV, 2YR STORM = 28.47' = ----SPILLWAY ELEV = 29.00' ----PROPOSED BUILDING -— UNDISTURBED NATIVE SOIL - POND BOTTOM ELEV = 28.10'

- 1. POND SIZES VARIES. SEE GRADING PLAN FOR LOCATION AND GEOMETRY. 2. LOAM MATERIAL SHALL CONSIST OF A SILTY SAND SOIL WITH NO LESS THAN 8% PASSING THE 200 SIEVE AND SHALL HAVE A CLAY CONTENT OF LESS THAN 2% WITH AN INFILTRATION CAPACITY SUFFICIENT TO DRAIN POND WITHIN 24 TO 48 HOURS.
- THE CONTRACTOR IS RESPONSIBLE FOR THE TESTING OF THE LOAM MATERIAL. THE POND AREAS SHALL BE TESTED TO ENSURE AN INFILTRATION RATE BETWEEN 3 INCHES TO 5 INCHES PER HOUR PRIOR TO SEEDING.

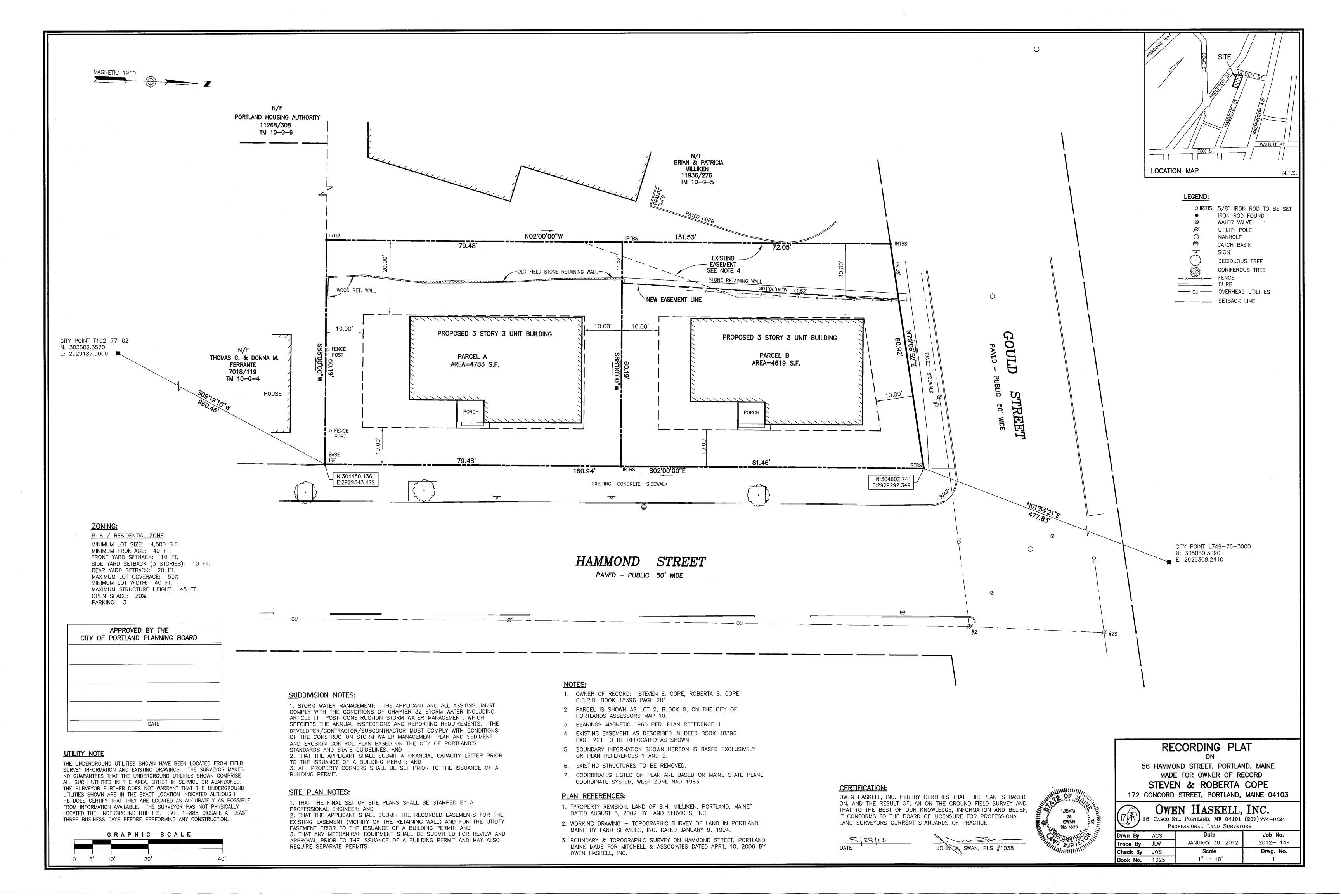
INFILTRATION POND DETAIL POND 1P NOT TO SCALE

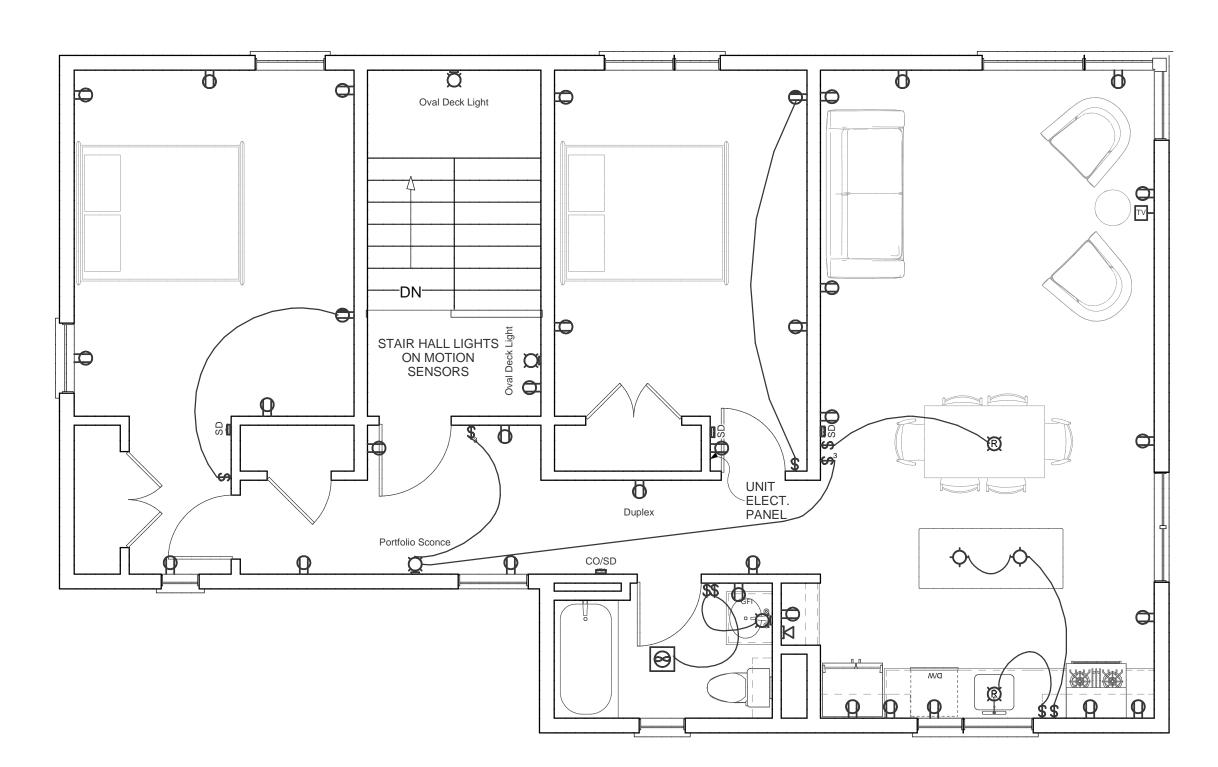


- 1. POND SIZES VARIES. SEE GRADING PLAN FOR LOCATION AND GEOMETRY. 2. LOAM MATERIAL SHALL CONSIST OF A SILTY SAND SOIL WITH NO LESS THAN 8% PASSING THE 200 SIEVE AND SHALL HAVE A CLAY CONTENT OF LESS THAN 2% WITH AN INFILTRATION CAPACITY SUFFICIENT TO DRAIN POND WITHIN 24 TO 48 HOURS.
- THE CONTRACTOR IS RESPONSIBLE FOR THE TESTING OF THE LOAM MATERIAL. THE POND AREAS SHALL BE TESTED TO ENSURE AN INFILTRATION RATE BETWEEN 3 INCHES TO 5 INCHES PER HOUR PRIOR TO SEEDING.

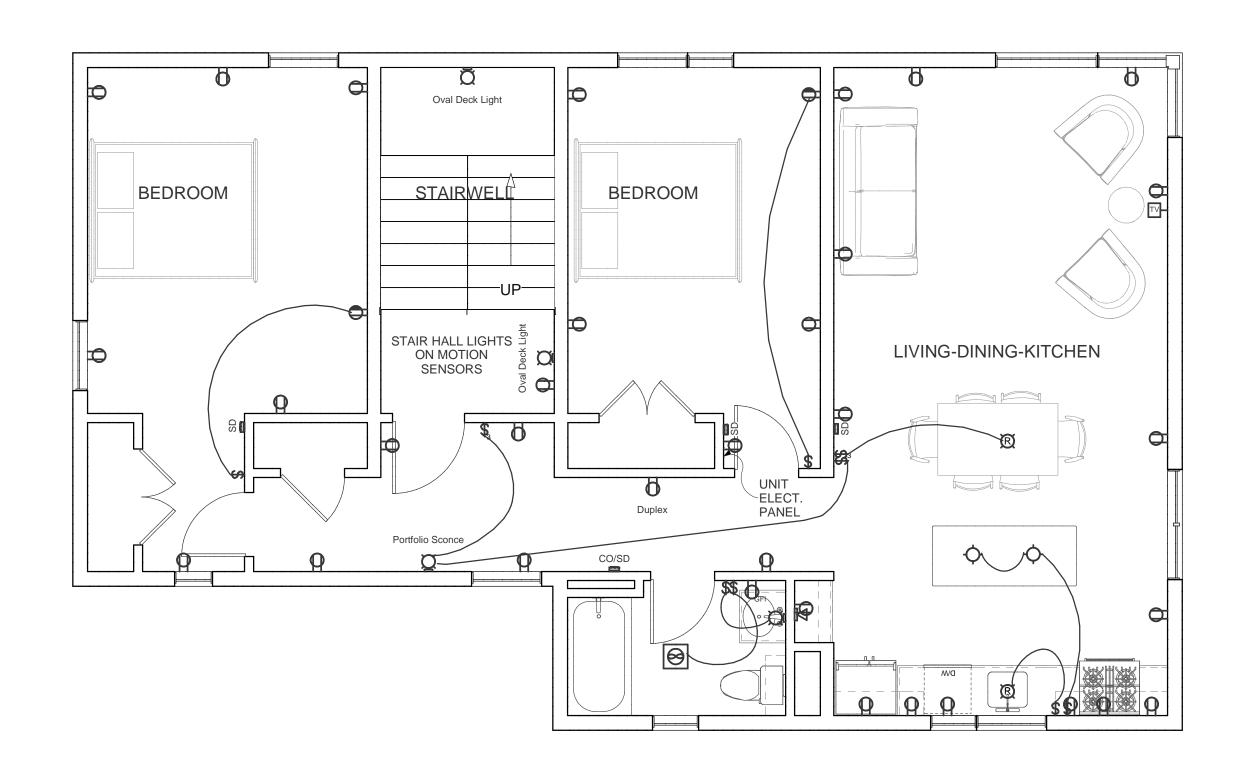
2 INFILTRATION POND DETAIL POND 2P NOT TO SCALE

SHEET 3 OF 3

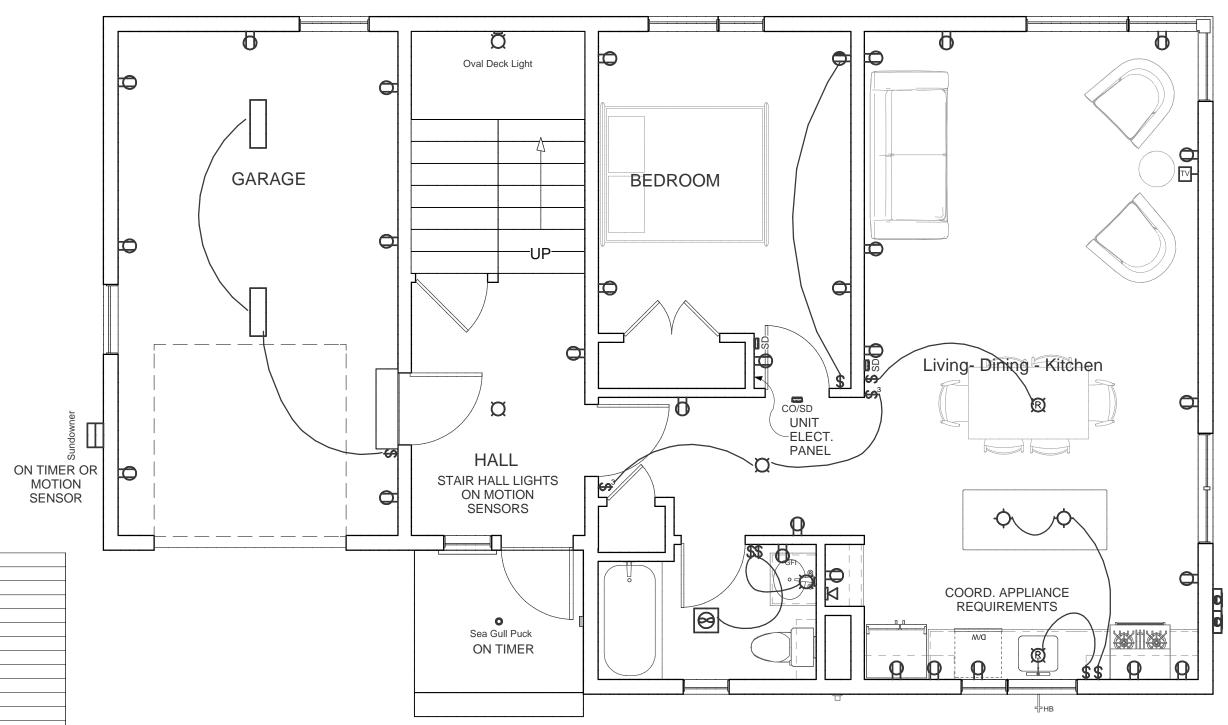




4~Third Floor Electrical Plan - 1/4 in = 1 ft



3~Second Floor Electrical Plan - 1/4 in = 1 ft



2~First Floor Electrical Plan - 1/4 in = 1 ft



Portfolio Oval Deck Light

Lowe's item #336745

Lowe's item #336745

Lowe's item #0048583

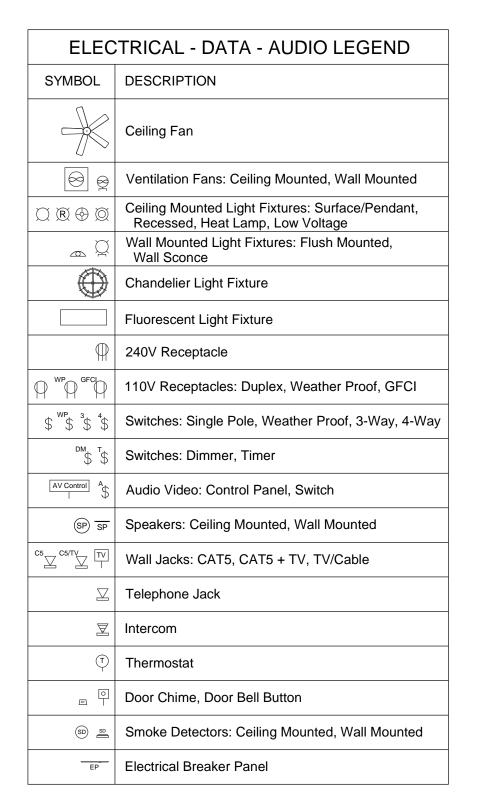
Lighting Schedule

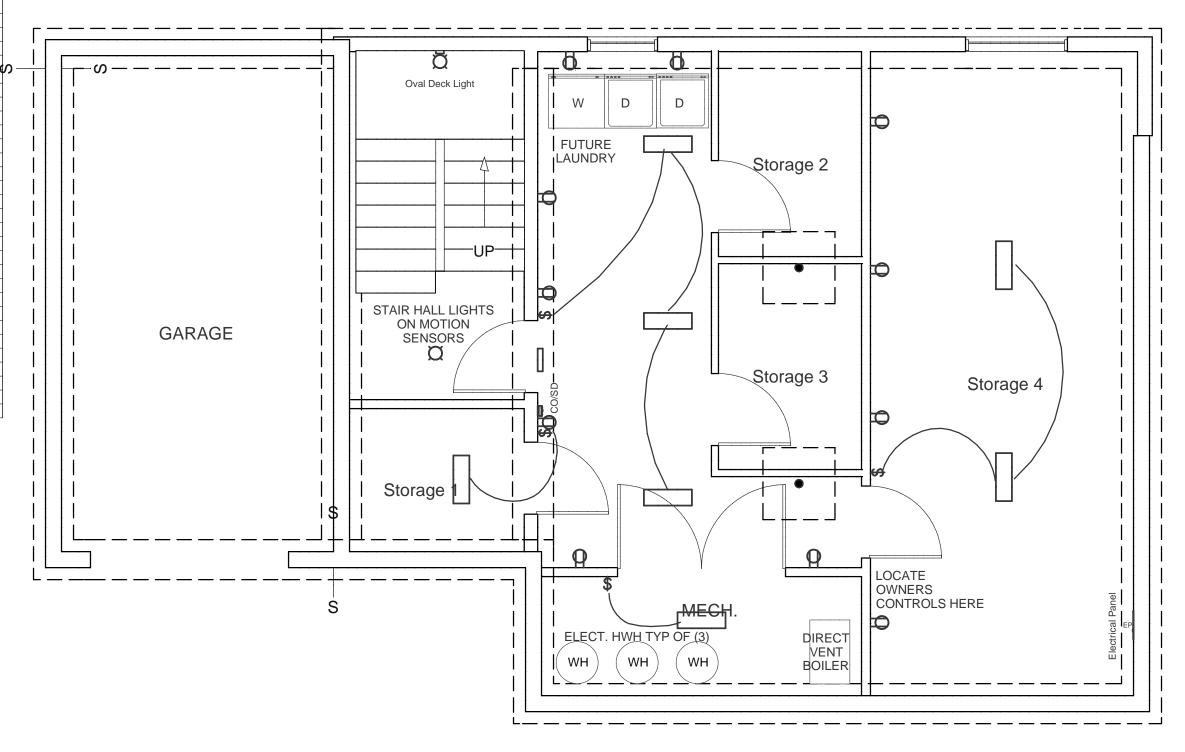
Basement 6 surface ceiling - fluorescent

Garage
2 surface ceiling - fluorescent

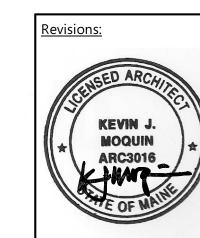
Common Halls/Stairwells

4 wall fixtures





1~Basement Electrical Plan - 1/4 in = 1 ft



Hammond

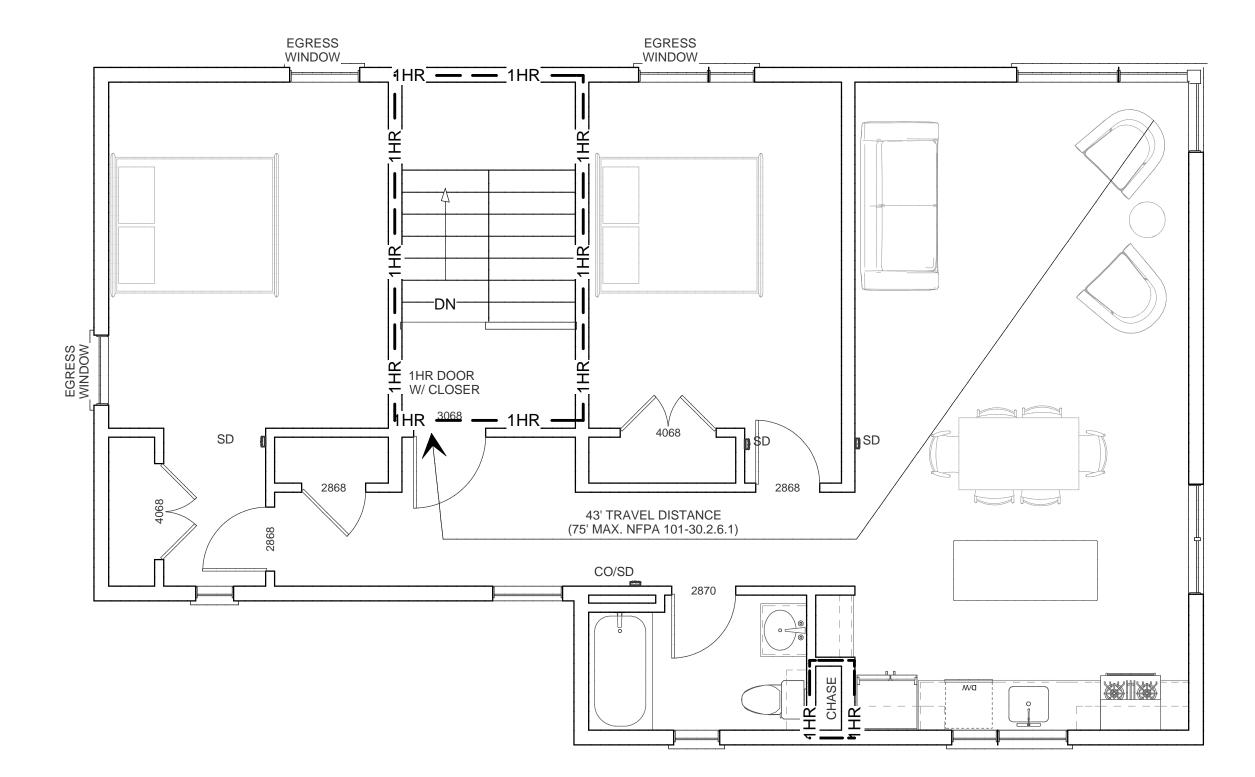
Apartments

Portland, ME

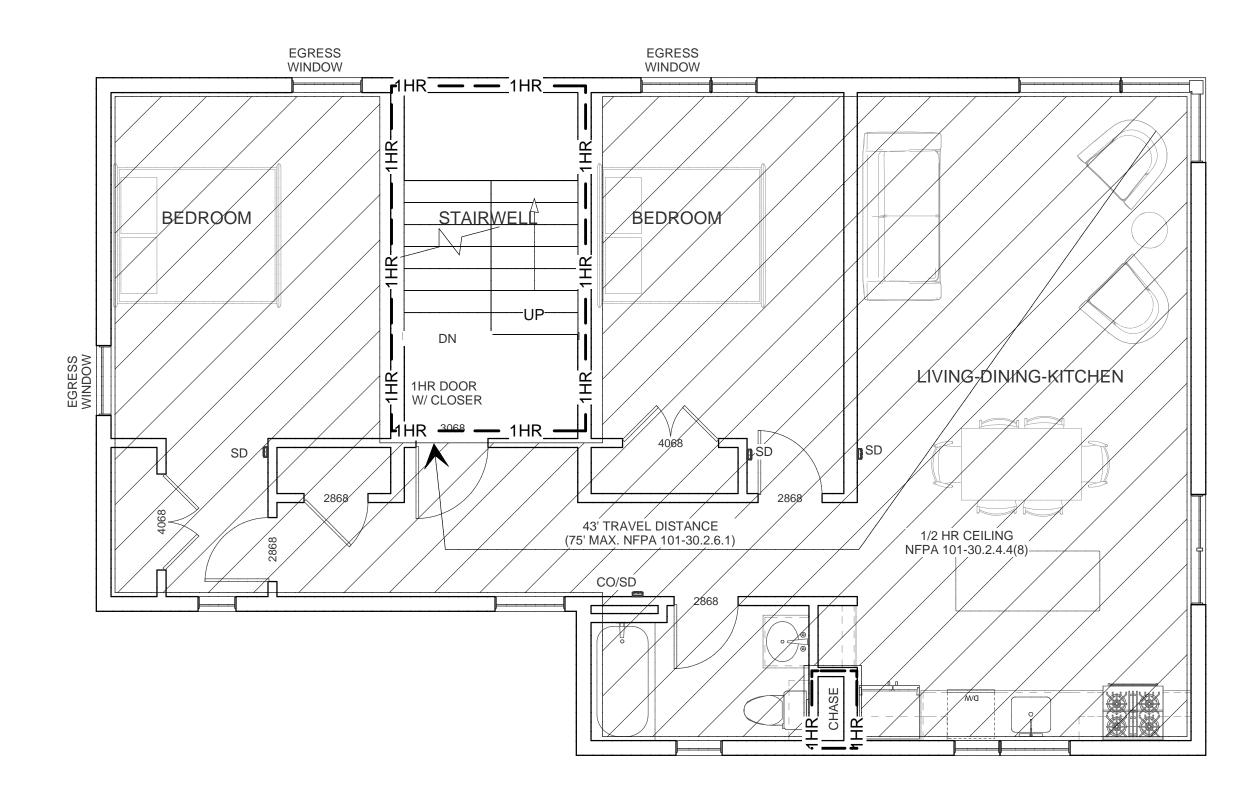
Kevin Moquin, AIA, LEED AP Maine Licensed Architect 53B Hammond St. Portland, ME kevin@km-a.me | (207)-615-6421

Electrical Plans E101

Parcel A PERMIT SET 1/4" = 1'-0" 5/18/2012



Third Floor Plan - 1/4 in = 1 ft



Second Floor Plan - 1/4 in = 1 ft

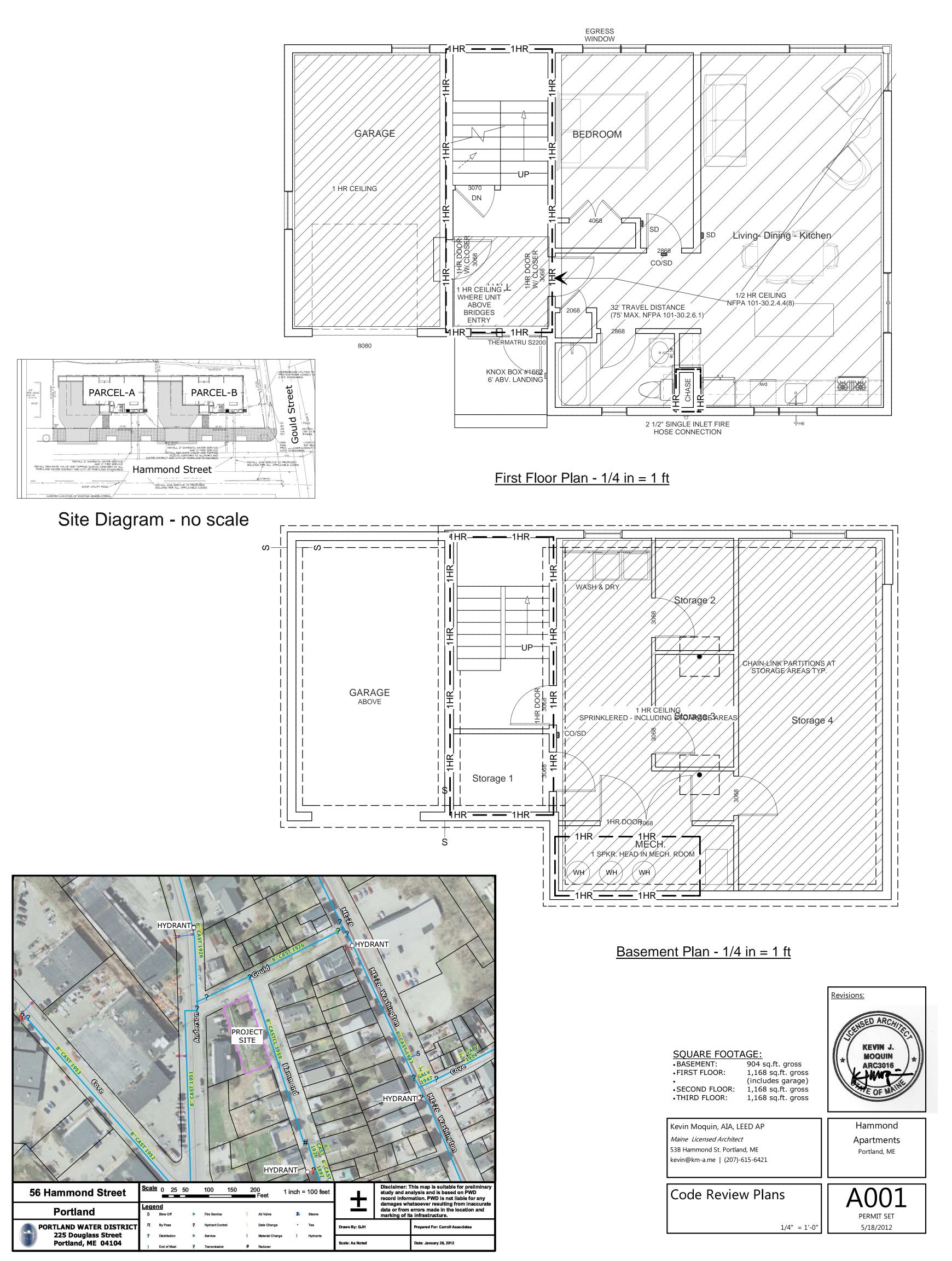
GENERAL NOTES FROM FINAL APPLICATION FIRE DEPARTMENT CHECKLIST:

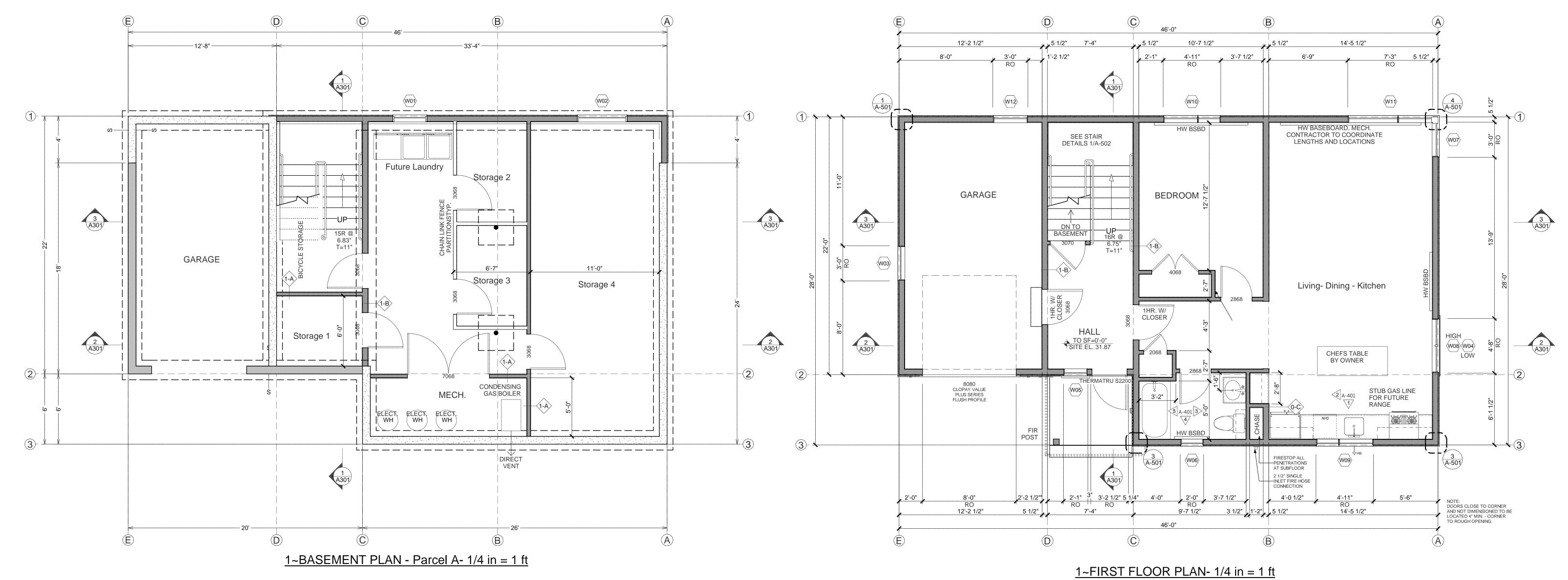
NOTE THAN PLANS OF BOTH BUILDINGS ARE IDENTICAL AND SAFETY FEATURED DESCRIBED HERE ARE TO BE IMPLEMENTS ON BOTH BUILDINGS.

- 1) APPLICANT: Steven & Roberta Cope, 172 Concord St., Portland, ME 04103, 207-415-5833
- 2) ARCHITECT: Kevin Moquin Architect, 207-615-6421, kevin@km-a.me
- 3) USE: IBC=R-2 RESIDENTIAL; NFPA=APARTMENT BUILDING (6.1.8.1.5)
- 4) CONSTRUCTION TYPE V-B, SPRINKLERED
- 5) SEE SHEET A201FOR BUILDING ELEVATIONS.
- 6) OCCUPANT LOAD (TABLE 14.8.1.2): 200 SQ.FT. PER FOR APARTMENTS = 18, 500 SQ.FT. PER FOR BASEMENT = 2, TOTAL = 20
- 7) SPRINKLER SYSTEM TO BE TYPE 13 R SYSTEM.
- 8) SMOKE ALARMS SHALL BE INTERCONNECTED SUCH THAT THE ACTIVATION OF ONE ALARM WILL ACTIVATE ALL ALARMS IN THE INDIVIDUAL UNIT.
- 9) SMOKE ALARMS SHALL RECIEVE THEIR PRIMARY POWER FROM THE BUILDING WIRING AND SHALL BE EQUIPPED WITH A BATTERY BACKUP.
- 10) HYDRANTS ARE LOCATED ON LOCUS PLAN PREPARED BY PORTLAND WATER DISTRICT AND INCLUDED ON THIS SHEET.

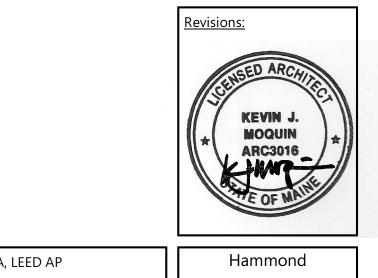
- 11) WATER MAIN SIZE AND LOCATION IS SHOWN ON SITE PLAN.
- 1) THE BUILDING ON PARCEL A IS ACCESSIBLE FROM THE FRONT ON HAMMOND STREET AND FROM THE PARKING AREA ON EACH SIDE. T HE BUILDING ON PARCEL B IS ACCESSIBLE FROM THE FRONT ON HAMMOND STREET AND FROM THE PARKING AREA ON THE SOUTH SIDE AND FROM GOULD
- STREET ON THE NORTH SIDE.

 2) DOOR AND WALL RATINGS FROM NFPA 101 TABLE 8.3.4.2 & NFPA 1 SECTION 14.3.1
- 3) ONE EXIT FROM EACH DWELLING UNIT IBC 1021.1(EX.4) & TABLE 1021.2, NFPA 101-30.2.4.4
- 4) EMERGENCY LIGHTING NOT REQUIRED NFPA 101-30.2.9
- 5) STAIR WIDTH 36" MIN. NFPA 101-7.2.2.2.1.2(A) 42" PROVIDED.





PARCEL A BUILDING ELEVATION 0'-0" = SITE ELEVATION 31.87 = STRUCTURAL ELEVATION 31'-10 1/2" SEE STRUCTURAL DRAWINGS FOR FOUNDATION DIMENSIONS AND REINFORCING

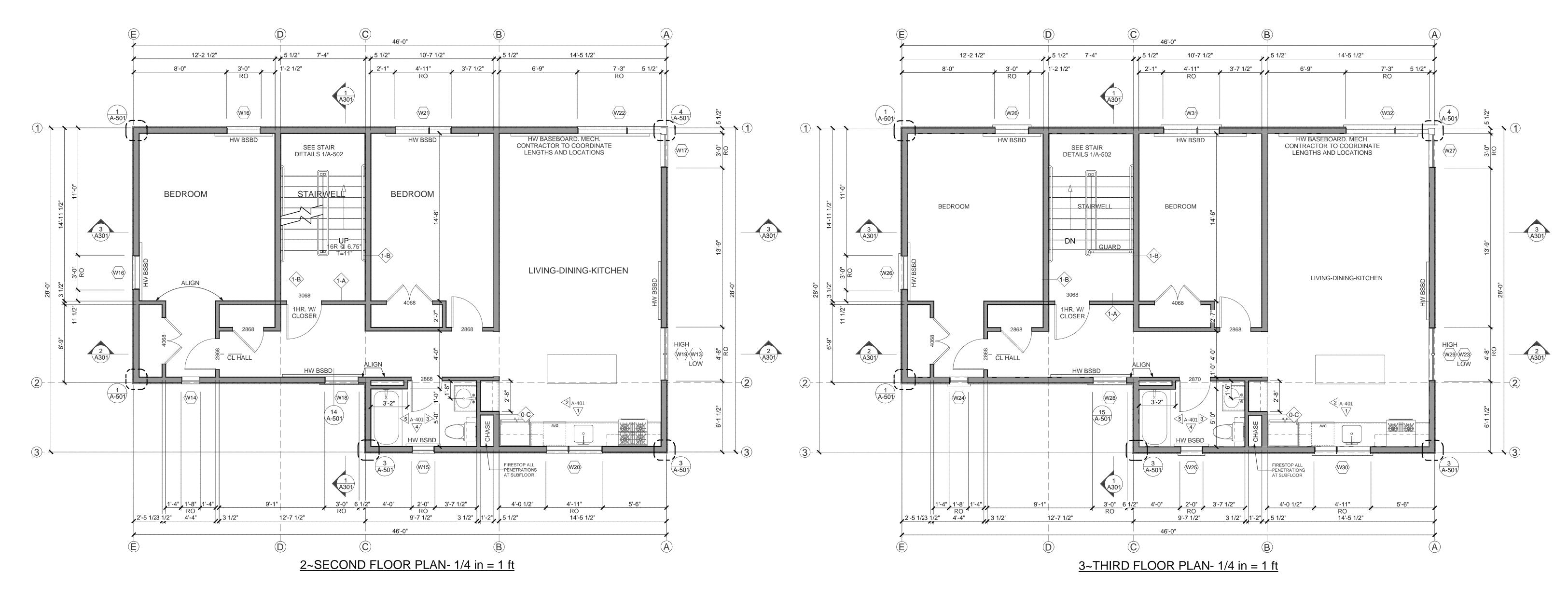


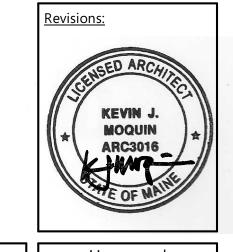
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Apartments Portland, ME

Basement & First Floor Plans - Parcel A

A101 PERMIT SET 5/18/2012





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Maine Licensed Architect

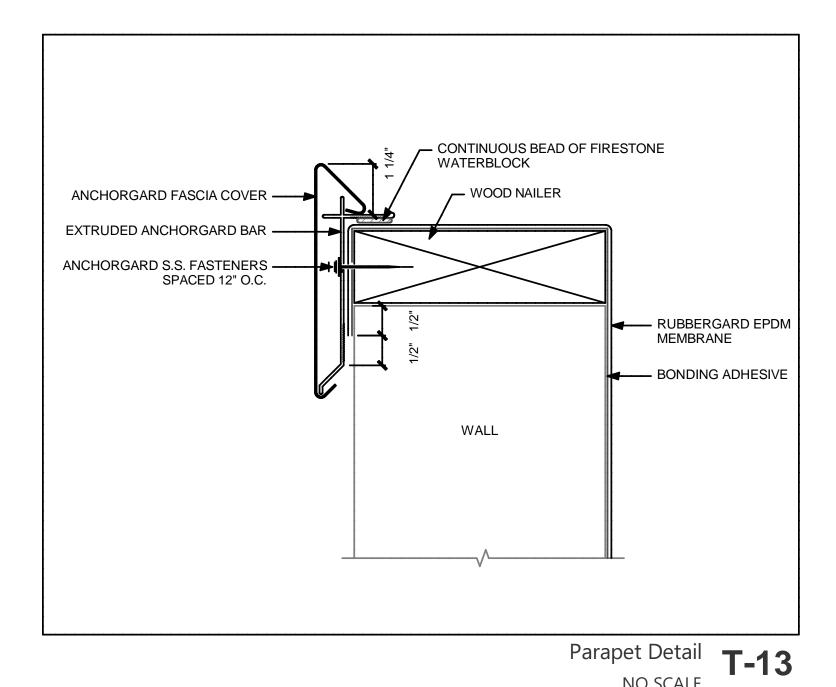
53B Hammond St. Portland, ME

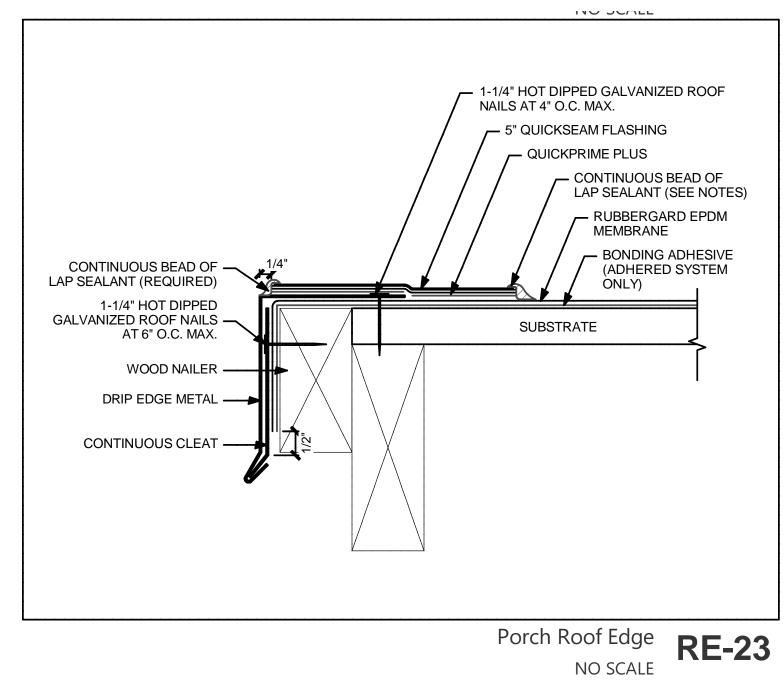
kevin@km-a.me | (207)-615-6421

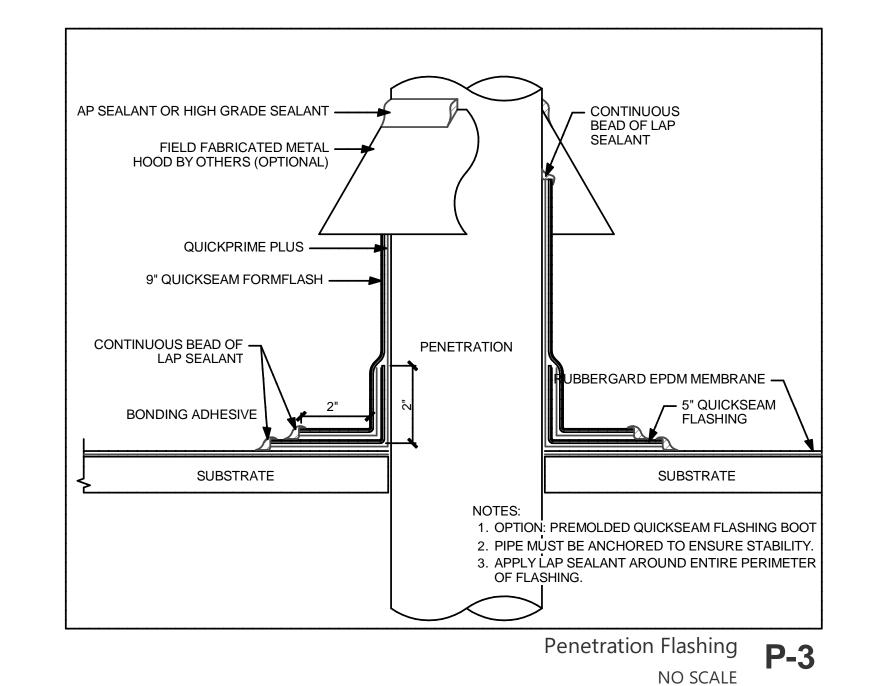
Hammond
Apartments
Portland, ME

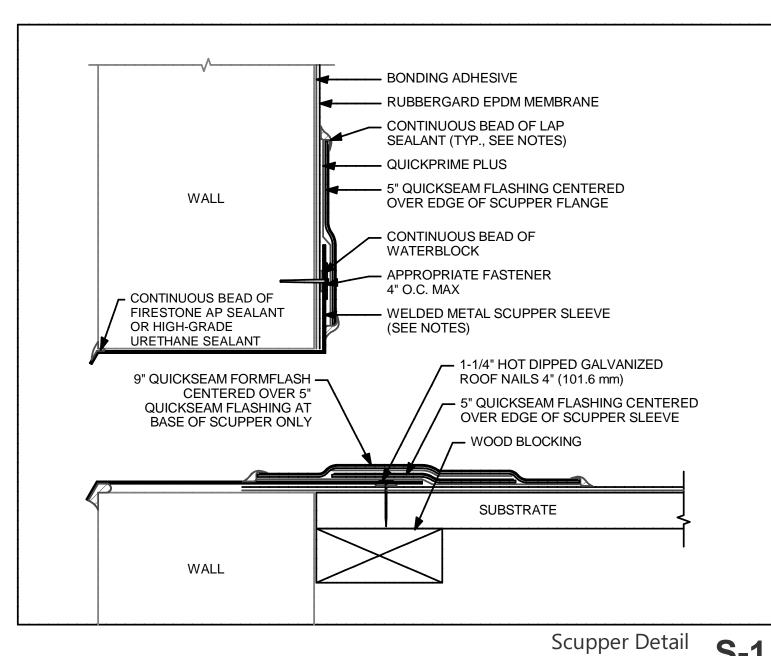
Second & Third Floor Plans A102
PERMIT SET
5/18/2012

1/4" = 1'-0"









NO SCALE

Revisions:

Maine Licensed Architect

53B Hammond St. Portland, ME

kevin@km-a.me | (207)-615-6421

Roof Plan & Details

1/4" = 1'-0

MOQUIN ARC3016

Hammond

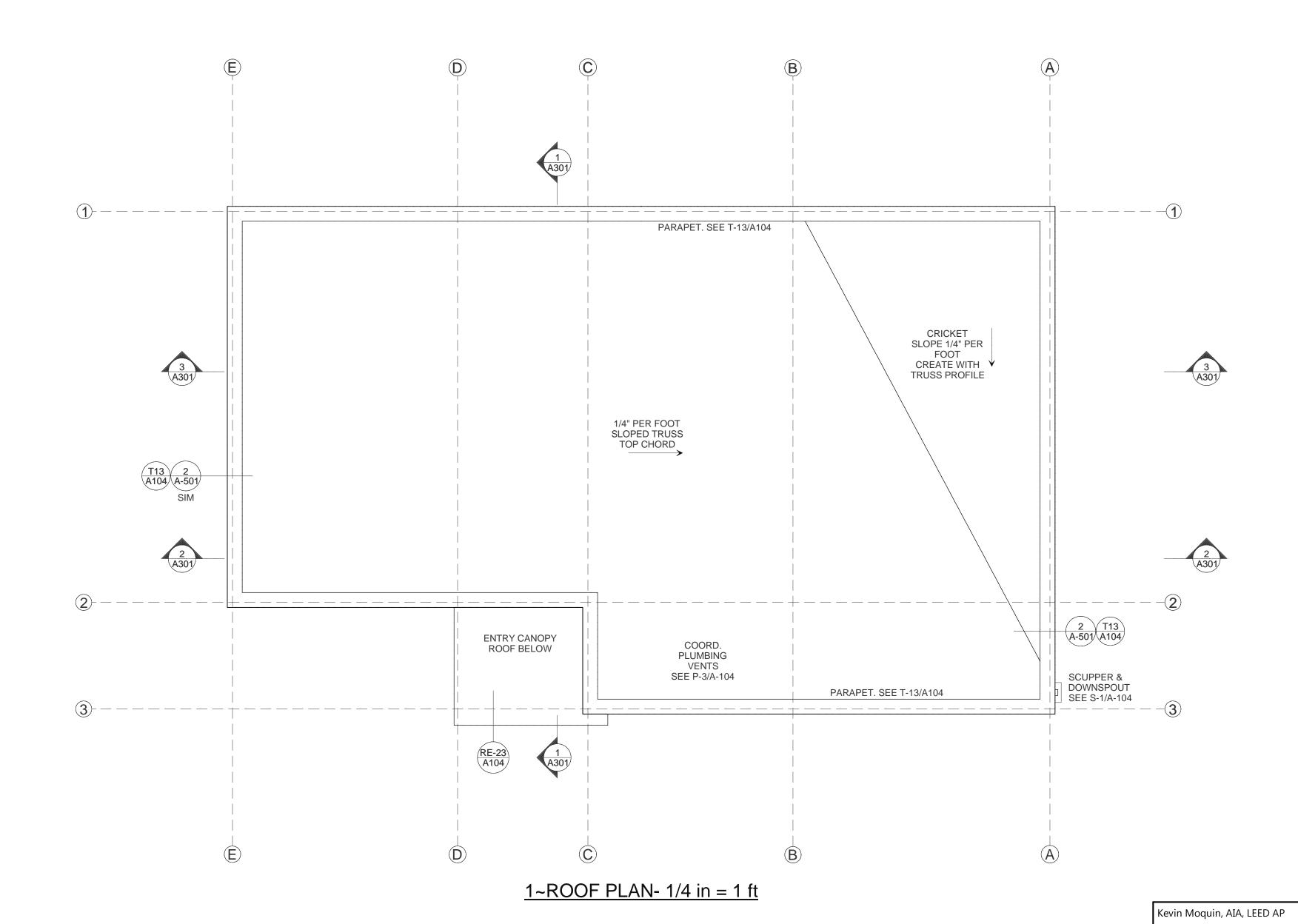
Apartments

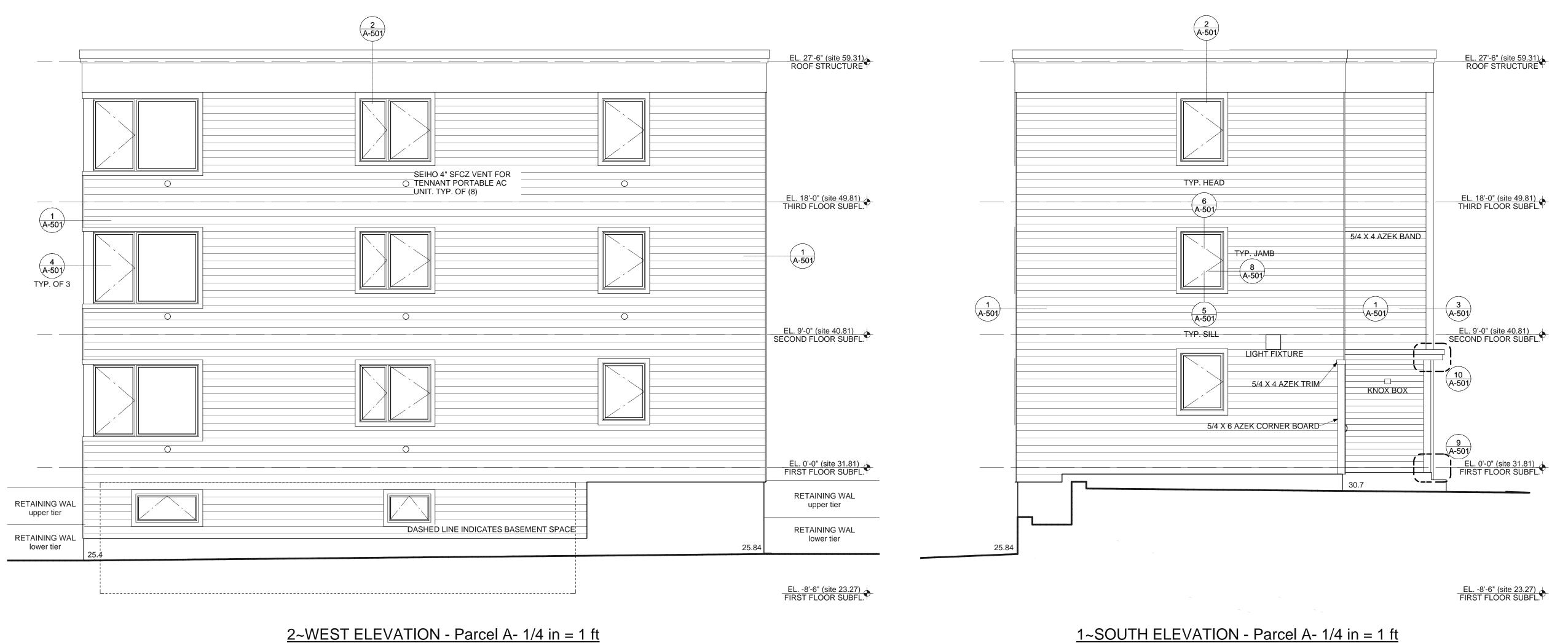
Portland, ME

A103

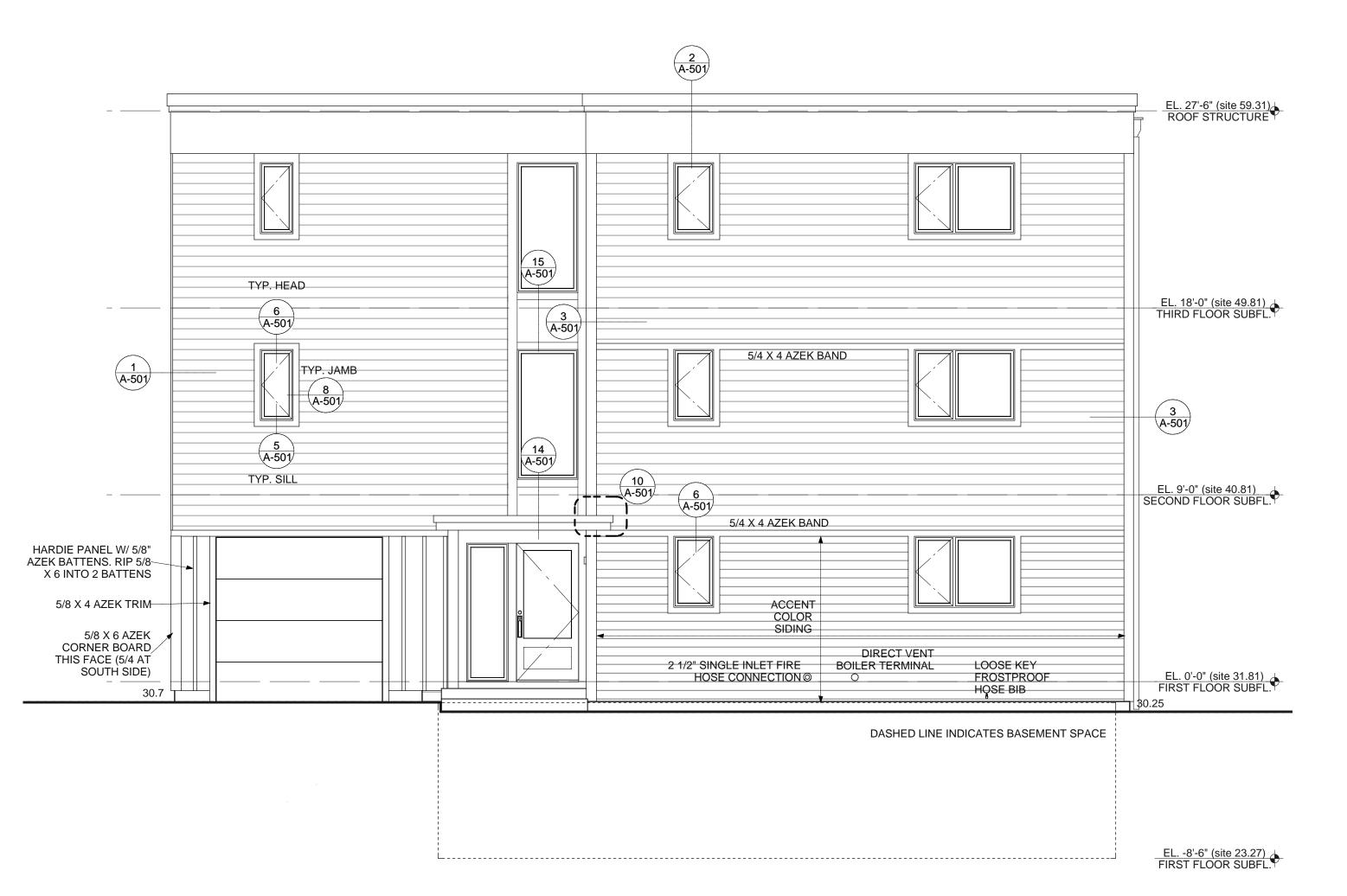
PERMIT SET

5/18/2012

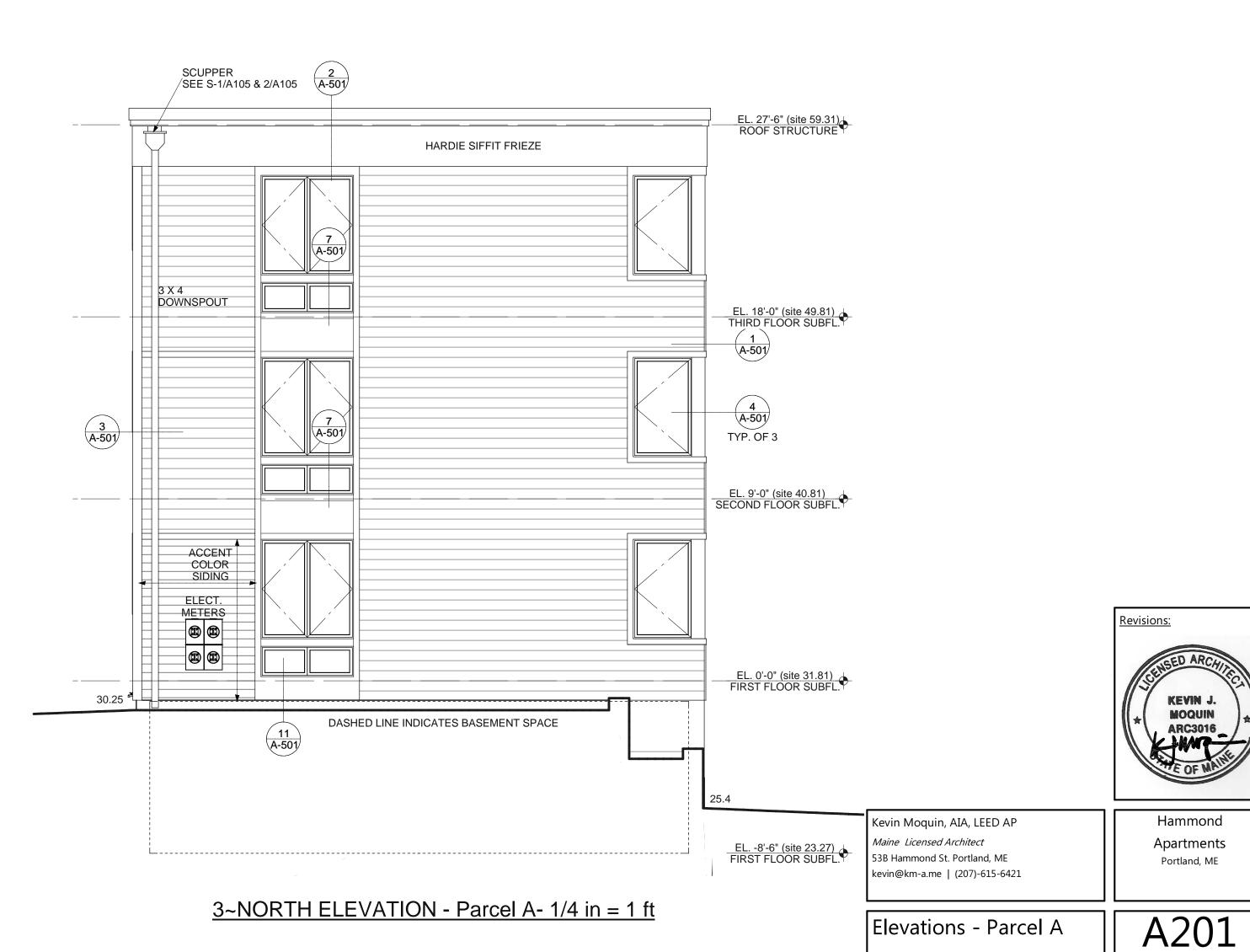








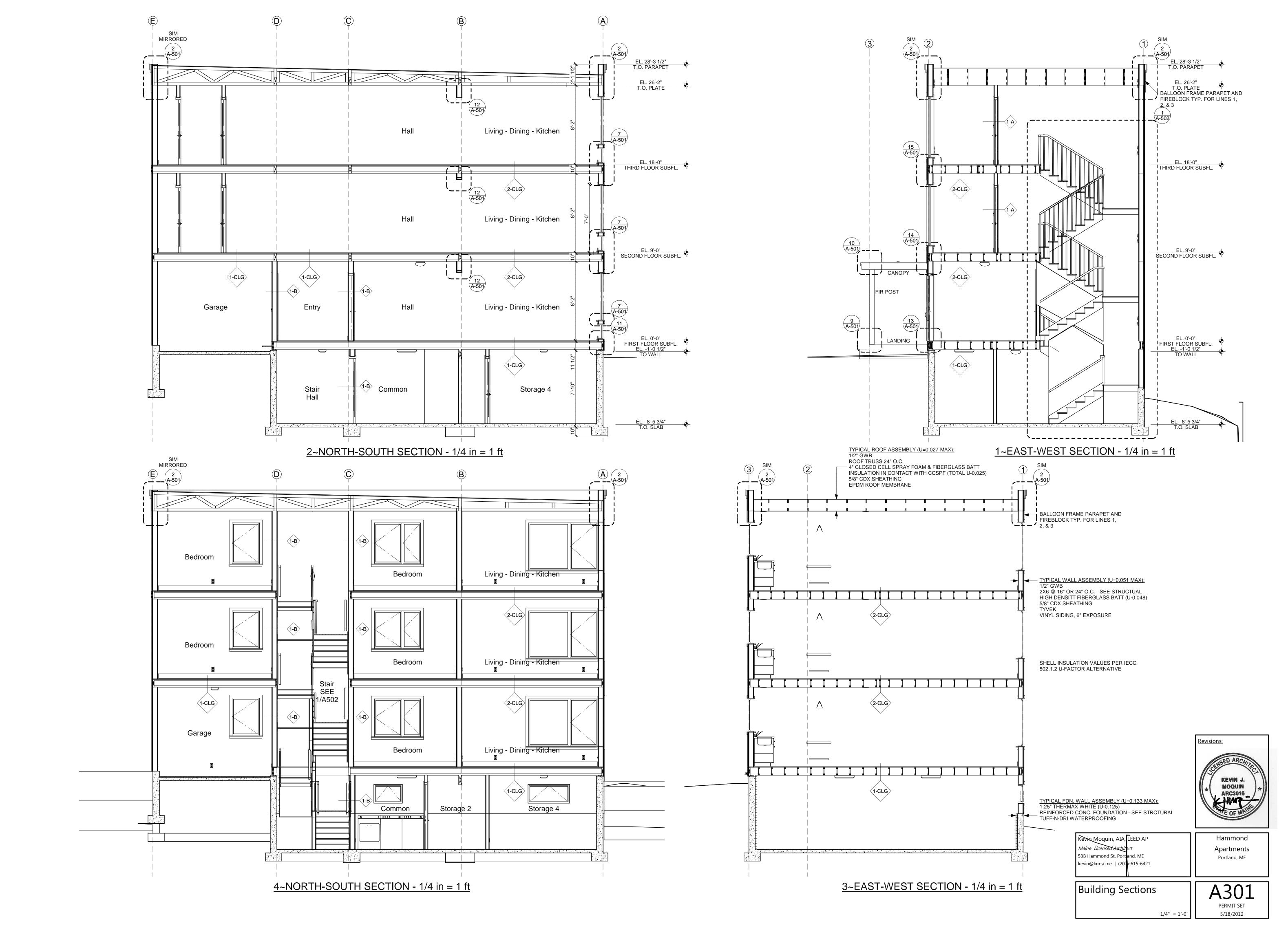
4~EAST ELEVATION - Parcel A- 1/4 in = 1 ft

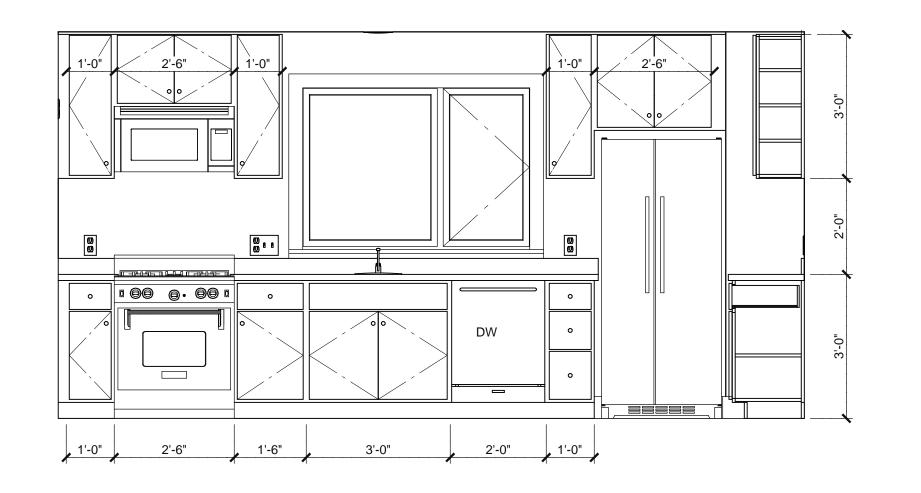


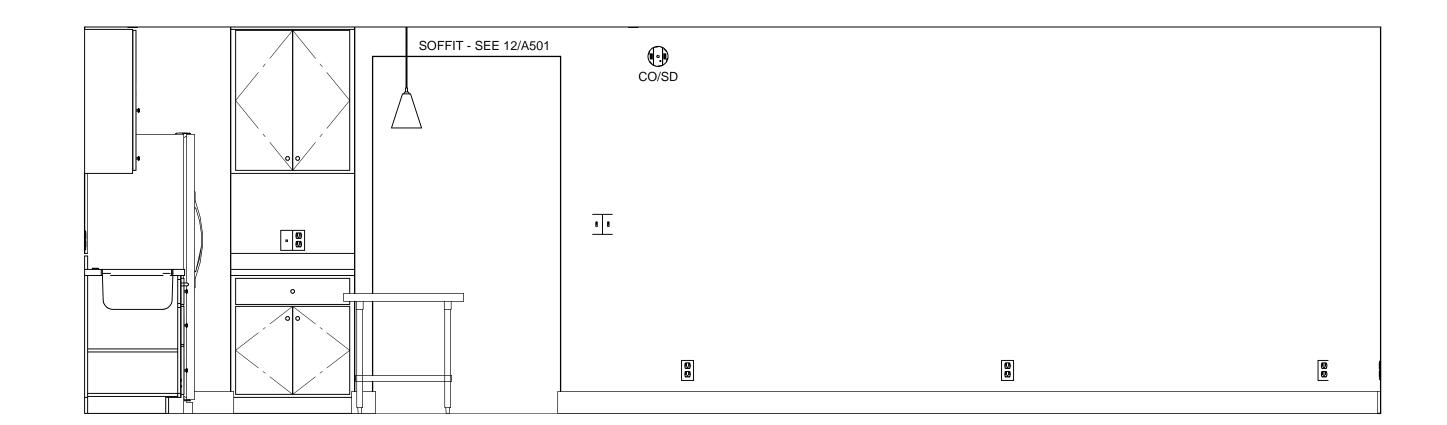
PERMIT SET

5/18/2012

1/4" = 1'-0"

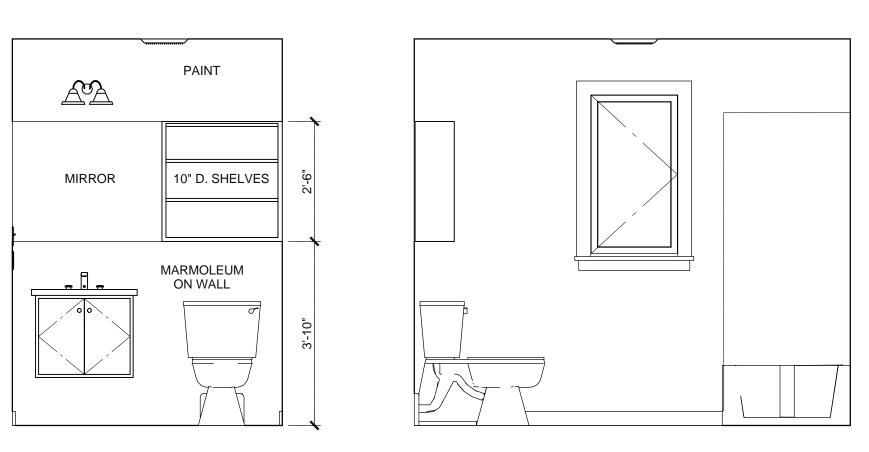


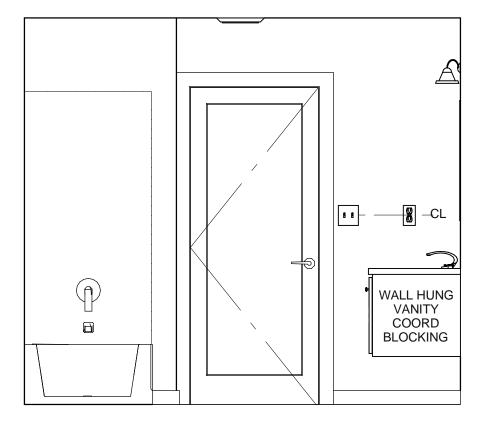




1~Kitchen, East- 1/2 in = 1 ft

2~Kitchen-Living, South - 1/2 in = 1 ft

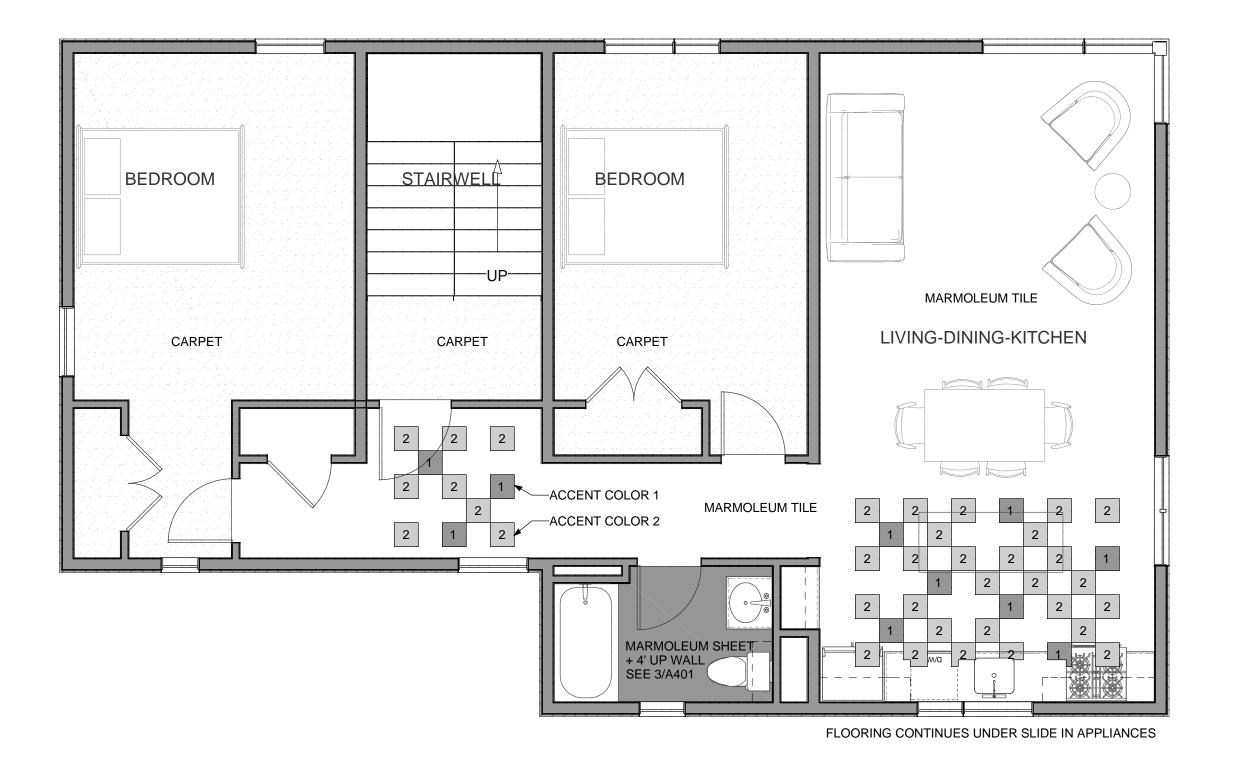


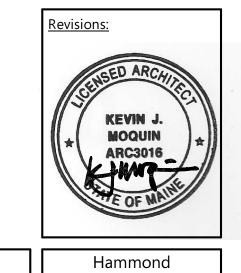


3~Bath, North- 1/2 in = 1 ft

4~Bath, East - 1/2 in = 1 ft

5~Bath, West - 1/2 in = 1 ft





Apartments

Portland, ME

PERMIT SET

5/18/2012

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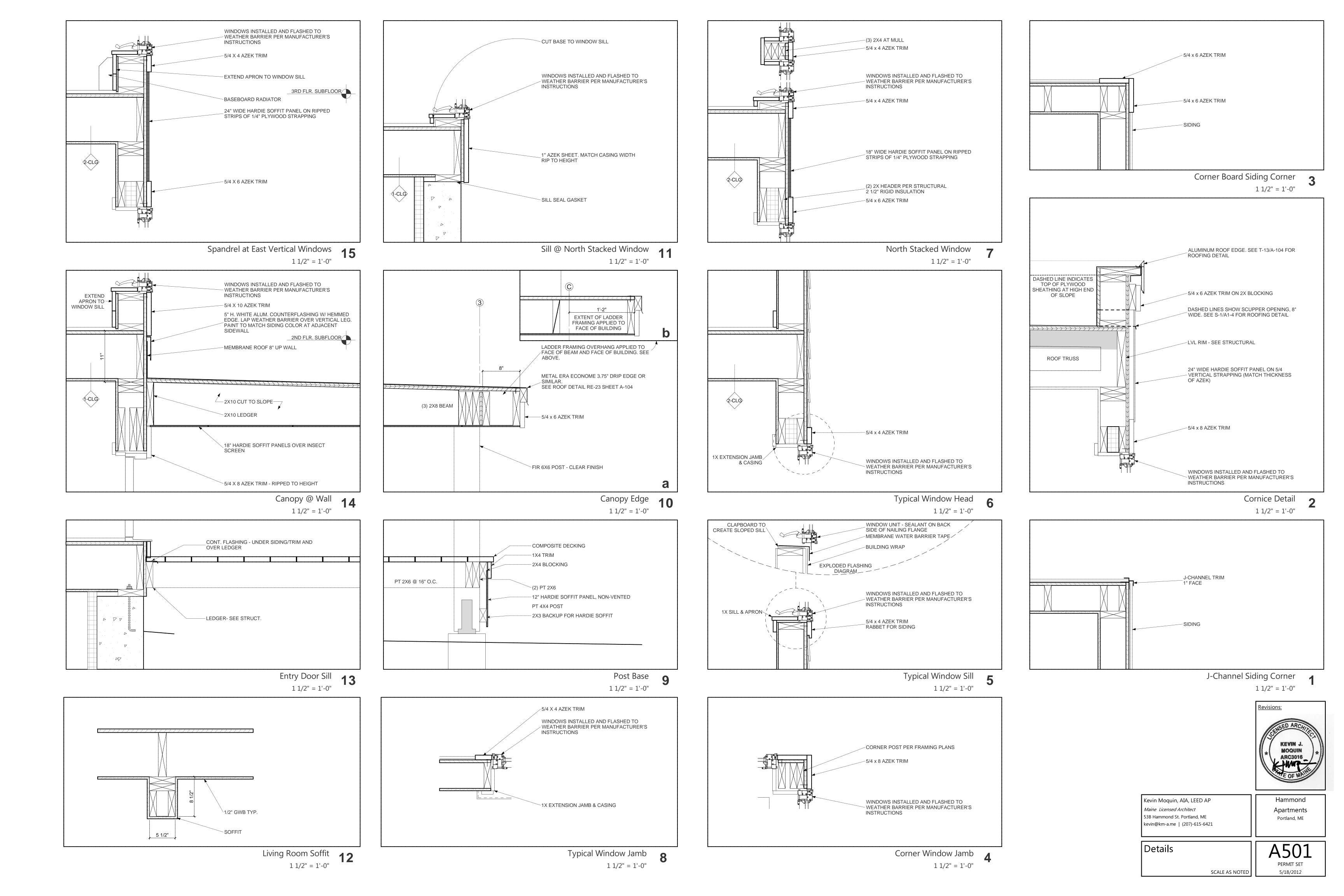
kevin@km-a.me | (207)-615-6421

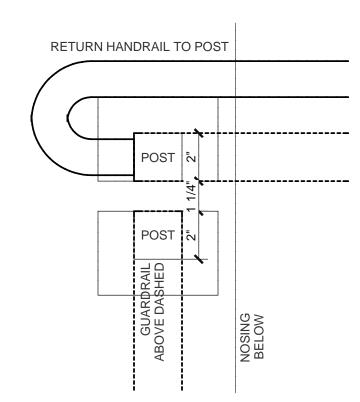
evations A401

Interior Elevations

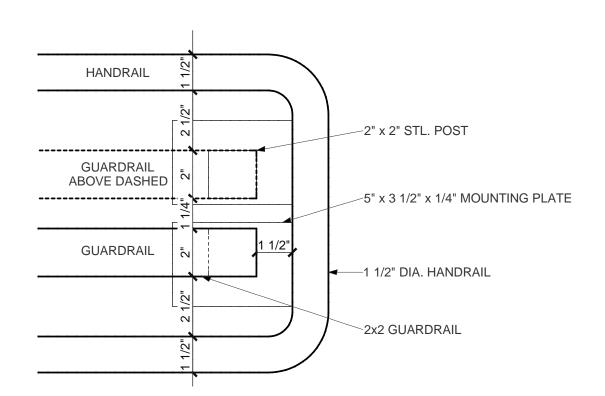
1/4" = 1'-0"

6~ Floor Pattern Plan - 1/4 in = 1 ft

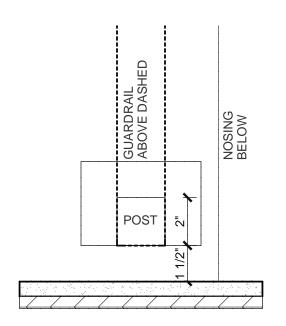




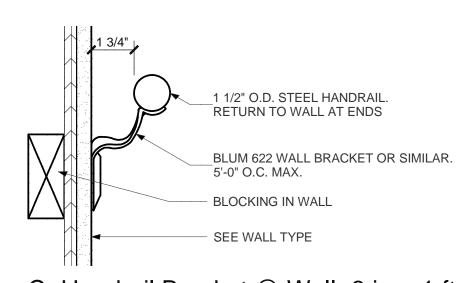
B~Guardrail @ 3rd Flr. Landing -3 in = 1 ft



A~Guardrail @ Intermediate Landing -3 in = 1 ft



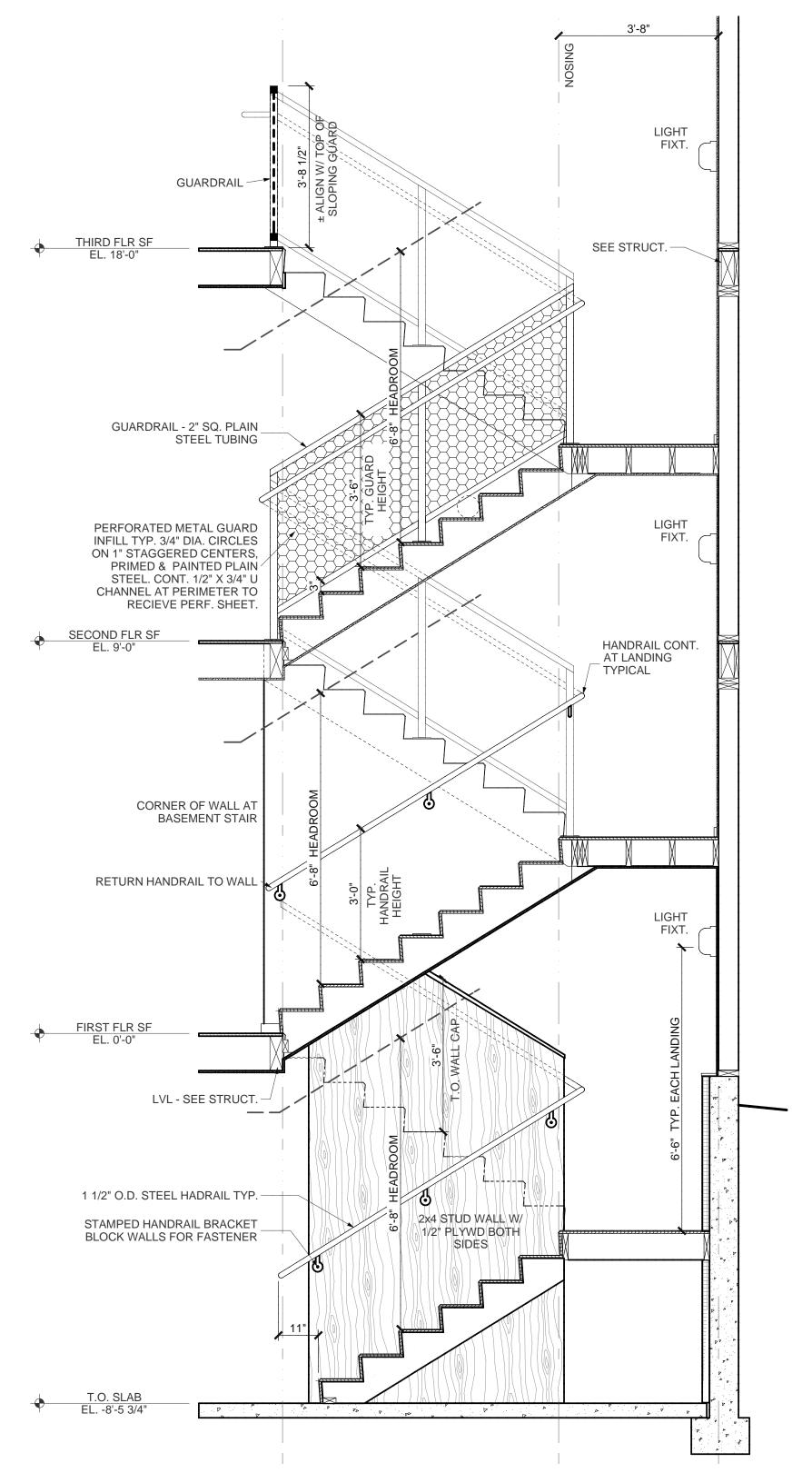
D~Guardrail @ Wall, 3Rd Flr. Landing -3 in = 1 ft



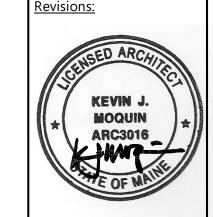
C~Handrail Bracket @ Wall- 3 in = 1 ft

NUMBER	QTYIF	LOOR TOP	R/O	EGRESS	TEMPERED	DESCRIPTION	CODE	MANUFACTURER	COMMENTS
V01	1 0	80"	36"X24"	† · · · · · · · · · · · · · · · · · · ·		AWNING	A3624	PARADIGM	
V02	1 0		52"X24"			AWNING	A5224	PARADIGM	
V03	1 1	106"	36"X48"			SNGL CASEMENT-HR	C3648	PARADIGM	HEAD ALIGNS W/ TYP. WINDOW
V04	1 1	20 1/	2" 56"X19"		YES	DBL CASEMENT	CF2819-2	PARADIGM	
V05	1 1	80"	25"X81"		YES	FIXED GLASS	CF2480	PARADIGM	
V06	1 1	84"	24"X42"			SNGL CASEMENT-HL	C2448	PARADIGM	
V07	1 1	84"	36"X60"			SNGL CASEMENT-HL	C3660	PARADIGM	
V08	1 1	84"	56"X60"			DBL CASEMENT-LHL/RHR	C2860-2	PARADIGM	
V09	1 1	84"	59"X42"	Ţ		MULLED UNIT	C2442+CF3642	PARADIGM	OP. UNIT HINGE L
/10	1 1	84"	59"X48"	YES		MULLED UNIT	C2448+C3648	PARADIGM	HINGE R
V11	1 1	84"	87"X60"			MULLED UNIT	C3660+CF5260	PARADIGM	OP. UNIT HINGE R
V12	1 1	98"	36"X48"			SNGL CASEMENT-HR	C3648	PARADIGM	HEAD ALIGNS W/ TYP. WINDOW
V13	1 2	20 1/	2" 56"X19"		YES	DBL CASEMENT	CF2819-2	PARADIGM	
V14	1 2		20"X42"		YES	SNGL CASEMENT-HL	C2042	PARADIGM	1
V15	1 2	84"	24"X42"			SNGL CASEMENT-HL	C2442	PARADIGM	
V16	2 2		36"X48"	YES		SNGL CASEMENT-HR	C3648	PARADIGM	
V17	1 2	84"	36"X60"			SNGL CASEMENT-HL	C3660	PARADIGM	
V18	1 2	84"	36"X76"		YES	FIXED GLASS	CF3676	PARADIGM	T
V19	1 2	84"	56"X60"			DBL CASEMENT-LHL/RHR	C2860-2	PARADIGM	
V20	1 2	84"	59"X42"			MULLED UNIT	C2442+CF3642	PARADIGM	OP. UNIT HINGE L
V21	1 2		59"X48"	YES		MULLED UNIT	C2448+C3648	PARADIGM	HINGE R
V22	1 2	84"	87"X60"			MULLED UNIT	C3660+CF5260	PARADIGM	OP. UNIT HINGE R
V23	1 3				YES	DBL CASEMENT	CF2819-2	PARADIGM	
V24	1 3		20"X42"		YES	SNGL CASEMENT-HL	C2042	PARADIGM	
V25	1 3		24"X42"			SNGL CASEMENT-HL	C2442	PARADIGM	
V26	2 3		36"X52"	YES		SNGL CASEMENT-HR	C3652	PARADIGM	
V27	1 3	84"	36"X60"			SNGL CASEMENT-HL	C3660	PARADIGM	
V28	1 3	84"	36"X76"		YES	FIXED GLASS	CF3676	PARADIGM	
V29	1 3		56"X60"			DBL CASEMENT-LHL/RHR	C2860-2	PARADIGM	
V30	1 3		59"X42"			MULLED UNIT	C2442+CF3642	PARADIGM	OP. UNIT HINGE L
V31	1 3		59"X52"	YES		MULLED UNIT	C2448+C3648	PARADIGM	HINGE R
V32	1 3	84"	87"X60"			MULLED UNIT	C3660+CF5260	PARADIGM	OP. UNIT HINGE R

<u>WINDOW NOTES:</u>
PARADIGM FIXED, CASEMENT & AWNING, LOW E INSULATING GLAZING, COLOR WHITE U= 0.30, SHGC= 0.22



1~Stair Section - 1/2 in = 1 ft



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Hammond **Apartments** Portland, ME

Stair Details Schedules SCALE AS NOTED A502 PERMIT SET 5/18/2012

GENERAL NOTES

I. CONSULT ARCHITECTURAL, MECHANICAL, ELECTRICAL, PLUMBING, AND SITE DRAWINGS FOR LOCATIONS AND DIMENSIONS OF OPENINGS, CHASES, INSERTS, REGLETS, SLEEVES, DEPRESSIONS, AND OTHER DETAILS NOT SHOWN ON STRUCTURAL DRAWINGS.

2. ALL DIMENSIONS AND CONDITIONS MUST BE VERIFIED IN THE FIELD. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER BEFORE PROCEEDING WITH THE AFFECTED PART OF THE WORK.

3. THE CONTRACTOR IS RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING ALL PHASES OF ERECTION AND CONSTRUCTION.

4. DETAILS SHOWN AS "TYPICAL" APPLY TO ALL SIMILAR CONDITIONS.

5. COORDINATION FOR THE PROPER INSTALLATION OF FINISHES, ELECTRICAL, MECHANICAL AND ALL OTHER 'NON-STRUCTURAL' ELEMENTS IN THE BUILDING IS THE RESPONSIBILITY OF OTHERS. WATERPROOFING AND INSULATION DETAILS SHALL BE PROVIDED

6. DESIGN AND DETAILING OF ALL MASONRY IS THE RESPONSIBILITY OF OTHERS UNLESS SPECIFICALLY NOTED ON THE DRAWINGS.

7. IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO ADEQUATELY PROTECT ALL MATERIALS AND ASSEMBLIES FROM WEATHER DURING CONSTRUCTION. G.C. VERIFY MOISTURE CONTENT OF ALL SUB BASE MATERIALS BEFORE INSTALLATION OF FINISHES; INSTALL ALL FINISHES PER MANUFACTURERS' INSTRUCTIONS.

8. STAIR DESIGN BY OTHERS, UNO

FOUNDATION NOTES

I. PREPARE ALL SOILS IN ACCORDANCE WITH THE SPECIFICATION OF THE GEOTECHNICAL REPORT #11212 BY SUMMIT GEOENGINEERING.

2. THE BOTTOM OF ALL EXTERIOR FOOTINGS SHALL BE ATLEAST 48" BELOW FINISH GRADE. GC COORDINATE FOOTING STEP LOCATIONS WITH FINISH GRADES AND SLAB ELEVATIONS TO ENSURE MINIMUM FROST COVERAGE, REINFORCE WALL AND FOOTING STEPS PER TYPICAL DETAILS. THE MAXIMUM SLOPE OF COMPACTED SUBGRADE BELOW INSOLATED FOOTINGS IS 1/2" IN 12"; TOP OF FOOTINGS SHALL BE MAX 1/4" IN 12".

3. ALL PAVEMENT, EXISTING FOUNDATIONS AND UNCONTROLLED GRANULAR FILL SHOULD BE REMOVED FROM THE AREA OF THE PLANNED CONSTRUCTION TO AT LEAST 4 FEET BEYOND THE FOOTING LIMIT.

4. COMPACTED STRUCTURAL FILL SHALL BE USED TO BACKFILL TO THE DESIGN FOOTING SUBGRADE AND BENEATH ALL SLABS ON GRADE AND SHALL CONFORM TO THE SPECIFICATION OF THE GEOTECHNICAL REPORT.

5. STRUCTURAL FILL SHALL BE PLACED IN UNIFORM LIFTS NOT EXCEEDING & INCHES IN LOOSE THICKNESS AND BE COMPACTED BENEATH SLABS TO 95 PERCENT OF MAXIMUM DRY DENSITY PER ASTM D-1551, MODIFIED PROCTOR TEST. COMPACT ADJACENT TO FOUNDATION WALLS SUPPORTING UNBALANCED FILL (RETAINING WALLS) TO 94-96 PERCENT OF MAXIMUM DRY DENSITY PER ASTM O-1557. HAND OPERATED EQUIPMENT SHALL BE USED FOR COMPACTION WITHIN 8 FEET OF NEW FOUNDATION WALL. DO NOT BACKFILL UNTIL THE FLOOR FRAMING IS INSTALLED OR THE WALLS ARE OTHERWISE ADEQUATELY BRACED AND THE CONCRETE HAS CURED FOR AT LEAST SEVEN DAYS.

6. PROVIDE DRAIN TO DAYLIGHT AROUND THE PERIMETER OF THE STRUCTURE (SPEC BY OTHERS). REFER TO ARCH/SITE DRAWINGS. REFER TO THE ARCHITECTURAL, STRUCTURAL, ELECTRICAL AND MECHANICAL DRAWINGS FOR ANY BONDOUT REQUIREMENTS. SEE TYPICAL CONCRETE DETAILS FOR BONDOUT REINFORCEMENT REQUIREMENTS.

7. SLOPE FOOTING EXCAVATIONS AS REQUIRED FOR STABILITY AND SAFETY PER OSHA REQUIREMENTS. BRACED EXCAVATIONS SHALL BE DESIGNED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF MAINE.

<u>REINFORCED CONCRETE</u>

I. CONCRETE WORK SHALL CONFORM TO THE LATEST EDITIONS OF THE FOLLOWING:

ACI 301 "SPECIFICATIONS FOR STRUCTURAL CONCRETE"

ACI 304 "GUIDE FOR MEASURING, MIXING, TRANSPORTING, AND PLACING CONCRETE" ACI 305 "HOT WEATHER CONCRETING"

ACI 306 "COLD WEATHER CONCRETING" ACI 318 "BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE"

2. CEMENT: ASTM CI5O, TYPE I. MAXIMUM AGGREGATE SIZE 3/4". CALCIUM CHLORIDE NOT PERMITTED.

3. CONCRETE FOR FOOTINGS, FOUNDATION WALLS, PIERS AND INTERIOR SLABS SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3000 PSI, UNO. WATER-CEMENT RATIO SHALL NOT EXCEED 0.50, UNO. CONCRETE FOR EXTERIOR SLABS (INCLUDES ATTACHED AND DETACHED GARAGE SLABS), RAMPS, RETAINING WALLS AND STEPS SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI; WATER-CEMENT RATIO SHALL NOT EXCEED 0.45. CONCRETE EXPOSED TO IN SERVICE FREEZE/THAW CYCLES (INCLUDING, BUT NOT LIMITED TO: FOUNDATION WALLS, FOOTINGS, EXTERIOR SLABS) SHALL BE AIR ENTRAINED WITH AN AIR CONTENT OF 5% TO 6%. EXTERIOR SLABS REQUIRE APPLICATION OF A CURING COMPOUND OR USE CURING TARPS. GENERAL CONTRACTOR COORDINATE USE OF EITHER WITH ARCHITECTURAL REQUIREMENTS FOR SLAB - APPEARANCE, SUITABILITY FOR INSTALLATION OF FUTURES FINISHES, ETC.

4. SLUMP SHALL NOT EXCEED 5" +/- I" PER ASTM CI43 UNLESS AN APPROVED WATER REDUCING ADMIXTURE IS USED. MAXIMUM SLUMP AFTER ADDITION OF ADMIXTURE IS 8".

5. SEE ARCHITECTURAL DRAWINGS FOR FOUNDATION DRAINAGE, UNDERSLAB UTILITIES, UNDERSLAB VAPOR BARRIER AND INSULATION ASSOCIATED WITH CONCRETE WORK. ALSO SEE ARCHITECTURAL DRAWINGS FOR CONCRETE FINSHES AND DEPRESSIONS.

6. CONCRETE SHALL NOT BE PLACED IN WATER OR ON FROZEN GROUND. GENERAL CONTRACTOR SHALL HEAT CONCRETE MATERIALS AND PROTECT CONCRETE AFTER PLACEMENT WHEN PLACING AT OR BELOW 50F TEMPERATURES IN ACCORDANCE WITH AC1306.

7. DEPOSIT CONCRETE IN FORMS IN HORIZONTAL LAYERS NOT DEEPER THAN 18" AND IN A MANNER TO AVOID INCLINED CONSTRUCTION JOINTS. WHERE PLACEMENT CONSISTS OF SEVERAL LAYERS, PLACE EACH LAYER WHILE THE PRECEDING LAYER IS STILL PLASTIC TO AVOID CONSTRUCTION JOINTS.

8. REINFORCING BARS SHALL CONFORM TO ASTM A615 WITH 60,000 PSI YIELD STRENGTH WITH MINIMUM ANCHORAGE AND SPLICE REQUIREMENTS FOR REINFORCING IN ACCORDANCE WITH THE LATEST EDITION OF ACI 318. WELDED WIRE FABRIC SHALL BE 6x6 WI.4xI.4 AND SHALL CONFORM TO ASTM A-185. PROVIDE IN FLAT SHEETS.

9. MINIMUM CONCRETE PROTECTIVE COVERING FOR REINFORCEMENT, UNLESS NOTED OTHERWISE, SHALL BE AS FOLLOWS:

FOOTINGS: 3" FOUNDATION WALLS: 2" EXTERIOR SLABS: 2" INTERIOR SLABS: I"

PIERS: 1.5" TO TIES

IO. REINFORCEMENT SHALL BE CONTINUOUS AROUND CORNERS AND AT INTERSECTIONS. PROVIDE LAPPED BARS AT NECESSARY SPLICES OR HOOKED BARS AT DISCONTINUOUS ENDS. PROVIDE CLASS - B TENSION LAP SPLICES FOR ALL REINFORCING UNLESS OTHERWISE SHOWN ON PLAN.

II. WELDING OF REINFORCEMENT IS NOT PERMITTED.

12. PROVIDE ADDITIONAL REINFORCEMENT AROUND CONCRETE OPENING AS SHOWN ON THE TYPICAL DETAILS.

I3. PROVIDE MIN 6 MIL VAPOR BARRIER UNDER INTERIOR SLABS CAST ON GRADE, OR AS SHOWN ON THE ARCHITECTURAL PLANS.

14. CONCRETE WALLS SHALL BE CAST IN ALTERNATE PANELS NOT EXCEEDING 50 FEET LONG. CONSTRUCTION JOINTS MAY BE USED -

15. ANCHOR BOLTS SHALL BE HOT DIPPED GALVANIZED J BOLT (SIZED PER DETAILS) SPACED 3'-O" OC AND WITHIN 12" OF STEP OR CORNER (UNLESS OTHERWISE NOTED). MINIMUM TWO ANCHOR BOLTS PER ANY WALL SECTION. ANCHOR BOLTS AT SHEARWALL HOLDOWNS MUST BE GALVANIZED A301 THREADED ROD WITH DOUBLE NUT AT EMBEDDED END (UNO).

16. USE NON-SHRINK GROUT BENEATH BASE PLATES & BEARING PLATES.

SEE TYPICAL DETAILS. SLAB CONTROL JOINTS ARE REQUIRED AS SHOWN.

IT. TIE HOLES, BUG HOLES, VOIDS AND SURFACE IRREGULARITIES LARGER THAN 1/2" IN DIAMETER OR DEEPER THAN 1/8", OR BOTH, SHOULD BE EITHER PRETREATED WITH ENGINEER-APPROVED LIQUID FOUNDATION WATERPROOFING OR REPAIRED WITH A LEAN CONCRETE MIX OF GROUT. SEE ASTM D5295, PREPARATION OF CONCRETE SURFACES FOR ADHERED MEMBRANE WATERPROOFING SYSTEMS, FOR FURTHER DETAILS CONCERNING SUBSTRATE PREPARATION.

18. ALL SLABS REQUIRE CONTROL JOINTS AS SHOWN ON THE DRAWINGS. DEPTH OF JOINT SHALL BE 25% OF CONCRETE THICKNESS. CUT ALTERNATING WWF WIRES AT JOINT LOCATION. CUT TOP BARS ONLY AT HAUNCH SLAB CONDITION (OR 50% OF BARS IN SINGLE LAYER INSTALLATION). SAW CUT JOINTS WITH EARLY ENTRY SAW WITHIN ONE TO FOUR HOURS OR WITH A WET SAW AFTER FOUR TO TWELVE HOURS (AS SOON AS CONCRETE CAN BE SAWED WITHOUT AGGREGATE LOOSENING). GC COORDINATE CONTROL JOINT FINISHING REQUIREMENTS WITH ARCHITECT.

STRUCTURAL STEEL NOTES

I. STRUCTURAL STEEL FABRICATION, ERECTION, AND CONNECTION DESIGN SHALL CONFORM TO AISC "SPECIFICATION FOR THE DESIGN FABRICATION, AND ERECTION OF STRUCTURAL STEEL" 9TH EDITION, AND THE "CODE OF STANDARD" PRACTICE, LATEST EDITION. DESIGN AND DETAIL ALL CONNECTIONS ACCORDING TO AISC.

2. STRUCTURAL STEEL: ASTM A36 U.N.O.

STRUCTURAL TUBING: ASTM A500 GRADE B46 KSI.

3. WHERE WELDING IS INDICATED, ALL WELDING SHALL CONFORM TO AWS DI.I-LATEST EDITION. ELECTRODES SHALL BE CONFORM TO AWS A5.1 ETOXX

4. COAT ALL COLUMN AND BASE PLATES BELOW SLAB WITH BITUMINOUS MASTIC.

I. ALL EXTERIOR WALL CONSTRUCTION IS 2x6 @I6", U.N.O.

2. ALL INTERIOR BEARING WALL CONSTRUCTION IS 2x6 @16" OC, U.N.O.

3. ALL STRUCTURAL MEMBERS MUST BE CONTINUOUS. LOCATE SPLICES OVER BEARING, U.N.O.

4. G.C. REFER TO ROOF AND FLOOR FRAMING PLANS FOR LOCATIONS OF POSTS AND JACK STUDS. POSTS (2-2x4 AND LARGER) AND JACK STUDS SHALL STACK CONTINUOUSLY TO THE FOUNDATION WALL UNLESS INTERRUPTED BY A BEAM OR JACK STUDS. ALL JACK STUDS AND POST LOCATIONS REQUIRE MATCHING BLOCKING STUDS BELOW FLOOR SHEATHING DOWN TO FOUNDATION WALL OR

5. IF POST IS NOT SPECIFIED, PROVIDE JACK STUDS BELOW ALL BEAMS EQUAL TO OR GREATER THAN THE WIDTH OF THE BEAM ABOVE (EXAMPLE: 3.5" WIDE LVL REQUIRES (3)-2x JACK STUDS BELOW = 4.5" WIDE)

6. STUDS MUST BE CONTINUOUS UNLESS INTERRUPTED BY A BEAM OR HEADER.

7. PROVDE I-JOIST BLOCKING IN EACH FRAMING BAY OVER INTERIOR BEARING WALLS OR WHEN I-JOIST IS CONTINUOUS OVER AN EXTERIOR WALL. PROVIDE 1.25" WIDE ENGINEERED RIMBOARD AT PERIMETER OF BUILDING, U.N.O.

8. INSTALL ENGINEERED FRAMING PRODUCTS PER MANUFACTURER'S INSTRUCTIONS.

9. ALL FLOOR, ROOF AND DECK/BALCONY FRAMING MEMBERS REQUIRE SIMPSON HANGERS, UNO. CONNECTION HARDWARE IN CONTACT WITH PRESERVATIVE-TREATED LUMBER AND USED IN AN INTERIOR ENVIRONMENT SHALL BE HOT DIPPED GALVANIZED (SIMPSON Z-MAX), OR STAINLESS STEEL WITH MATCHING FASTENERS.

IO. ALL BEAM /COLUMN CONNECTIONS REQUIRE HARDWARE (UNO), SEE DETAILS FOR SPECIFIC CONNECTIONS. MINIMUM CONNECTIONS: WOOD BEAM TO LALLY POST REQUIRES SIMPSON LCC TYPE CONNECTION, 5.5" WIDE BEAM TO 5.5" WIDE COLUMN REQUIRES (2) SIMPSON AC6 CLIPS (AC4 AT 3.5/3.5").

II. ALL TIMBER FRAMING SHALL BE IN ACCORDANCE WITH THE NATIONAL DESIGN SPECIFICATION FOR WOOD CONSTRUCTION (NDS) 2005 EDITION. UNLESS NOTED OTHERWISE, ALL WOOD FRAMING SHALL BE FASTENED IN ACCORDANCE WITH 2003 IBC, SECTION

12. FRAMING GRADES

- LUMBER (2x STUDS, JOISTS, RAFTERS) SHALL BE #2 AND BETTER S.P.F. LESS THAN 19% MOISTURE CONTENT - WOOD I-JOISTS: SPECIFIC GRADES SHOWN ON THE DRAWINGS.

- LAMINATED VENEER LUMBER (LVL) - 2.0 E, 3100 Fb, 285 Fv

- PRESERVATIVE TREATED 2x FRAMING LUMBER (PT) SHALL BE #2 AND BETTER SOUTHERN YELLOW PINE TREATED IN ACCORDANCE WITH AWPA UC3B "EXTERIOR, ABOVE GROUND." WOOD CLOSER THAN 8", OR IN CONTACT WITH GRADE, SHALL BE TREATED IN ACCORDANCE WITH AWPA VCA4 "GROUND CONTACT, GENERAL VSE." ARCHITECTURALLY EXPOSED TIMBER MAY REQUIRE ALTERNATE SPECIES AND/OR TREATMENT - REFERENCE DRAWINGS.

13. MIN SHEATHING (ALSO SEE ARCHS):

- EXTERIOR WALL: 1/2" EXPOSURE I. 8d AT 6" O.C. AT PANEL EDGES, 8" O.C. WITHIN PANEL U.N.O. (ALSO SEE SHEARWALL NOTES FOR ADDITIONAL NAILING). NOTE: NAILS DRIVEN INTO PT FRAMING MEMBERS SHALL BE HAND NAILED AND HOT DIPPED GALV. GALV GUN NAILS ARE NOT AN ACCEPTABLE SUBSTITUTE WITHOUT PRIOR ENGINEER APPROVAL. - FLOOR: 23/32" ADVANTECH INSTALLED WITH STRENGTH AXIS PERPENDICULAR TO SUPPORTS. ADHESIVE: 0.25" BEAD OF POLYURETHANE OR SOLVENT-BASED ADHESIVE APPLIED TO CLEAN, DRY FLOOR JOIST. FASTENER: 8d RING SHANK NAIL (IOd FOR THICKNESS > 0.75") OR APPROVED SCREW. FASTENER SPACING: 6" O.C. AT EDGES, I2" O.C. WITHIN PANEL - ROOF: 5/8" ADVANTECH INSTALLED WITH STRENGTH AXIS PERPENDICULAR TO SUPPORTS. 8d AT 6" O.C. AT PANEL

EDGES, 12" OC WITHIN PANEL. - NAIL HEAD MUST BE DRIVEN FLUSH. SHTG WITH OVERDRIVEN NAILS MUST BE REFASTENED.

14. ALL BUILT-UP BEAMS AND COLUMNS SHALL BE NAILED AS FOLLOWS (U.N.O.):

- COLUMNS, KING/JACK STUDS: 2-10d NAILS AT 8" O.C.

- BEAMS LESS THAN 12" DEEP: 2-16d NAILS AT 12" O.C. IN EACH PLY

- BEAMS DEEPER THAN 12" (INCLUDING 12"): 3-16d NAILS AT 12" O.C. IN EACH PLY (SIDE-LOADED BEAMS MAY REQUIRE ADDITIONAL CONNECTIONS - SEE FRAMING DETAILS)

15. DRILLED EPOXY ANCHORS SHALL BE HOT DIP GALV A301 THREADED ROD WITH SIMPSON 'AT' EPOXY. BLOW HOLES FREE OF DUST AND INSTALL PER MANUFACTURER'S INSTRUCTIONS, TYP U.N.O; DRILLED EXPANSION ANCHORS SHALL BE MECHANICALLY GALVANIZED SIMPSON 5/8" Ø WEDGE-ALL' ANCHOR; RSS SCREWS SHALL BE CLIMATEK COATED AND MANUFACTURED BY GRK FASTENER. RSS SCREWS IN CONTACT WITH PRESERVATIVE-TREATED LUMBER SHALL BE STAINLESS STEEL.

I6. CONSULT ENGINEER PRIOR TO SHIMMING ANY STRUCTURAL MEMBER, CEDAR SHIMS ARE NOT ACCEPTABLE.

17. SHEARWALL TOP AND BOTTOM PLATES MUST BE CONTINUOUS - DO NOT NOTCH OR CUT.

SUBMITTALS

CONTRACTOR SHALL SUBMIT COMPLETE SHOP DRAWINGS, INCLUDING: - ROOF TRUSS CALCULATIONS STAMPED BY AN ENGINEER LICENSED IN MAINE - ROOF TRUSS ERECTION DRAWINGS

<u>TESTING</u>

OWNER SHALL ENGAGE A QUALIFIED TESTING AGENCY TO CONDUCT PERIODIC TESTS TO CONFIRM CONSTRUCTION IS IN ACCORDANCE WITH THE DRAWINGS AND SPECIFICATIONS. TEST REPORTS SHALL BE SUBMITTED TO THE ARCHITECT AND ENGINEER, INCLUDING:

- CONCRETE SLUMP, TEMPERATURE AND AIR CONTENT AT POINT OF PLACEMENT - CONCRETE COMPRESSION TESTS

<u>DESIGN LOADS</u>

I. BUILDING CODE:

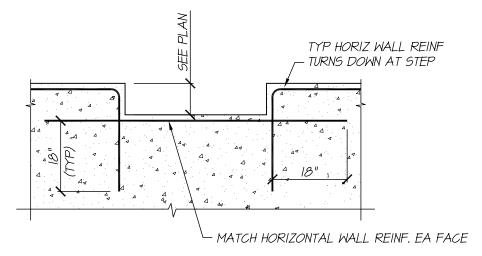
INTERNATIONAL BUILDING CODE (2003) ASCE 7-05 MINIMUM DESIGN LOADS FOR BUILDINGS

2. FLOOR LIVE LOADS STAIRS AND CORRIDORS = 100 PSF LIVING SPACE = 40 PSF

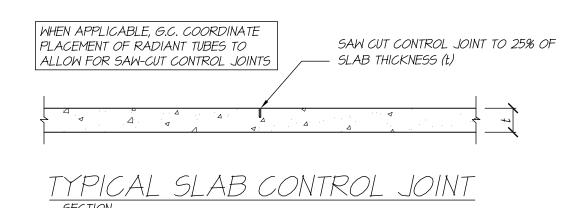
3. ROOF SNOW LOADS: GROUND SNOW LOAD (PG): 50 PSF EXPOSURE FACTOR (Ce): 1.0 THERMAL FACTOR (Ct) = 1.1 IMPORTANCE FACTOR (I): 1.0

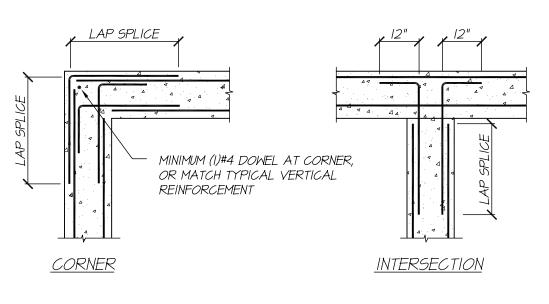
4. DESIGN WIND LOADS: BASIC WIND SPEED: 100 MPH EXPOSURE: C

IMPORTANCE FACTOR: 1.0

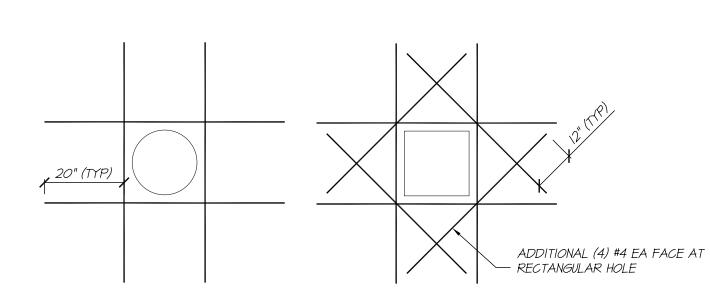


ELEVATION VIEW



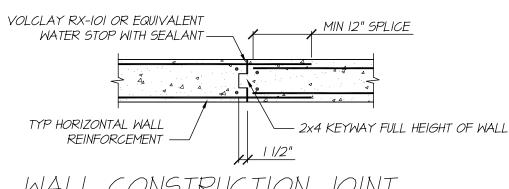


TYP WALL & FTG REINFORCEMENT

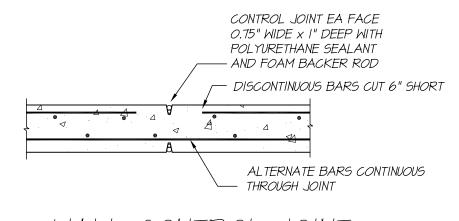


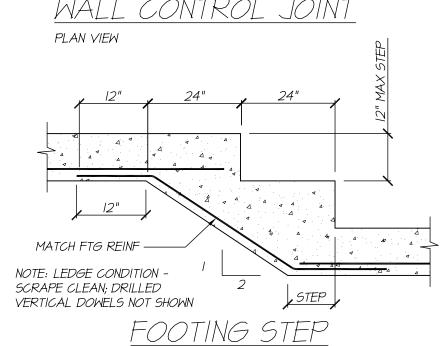
<u>ROUND</u> RECTANGULAR TYP OPENING IN WALL OR SLAB

NOTE: APPLIES TO ANY PENETRATION LARGER THAN 2"



WALL CONSTRUCTION JOINT





H

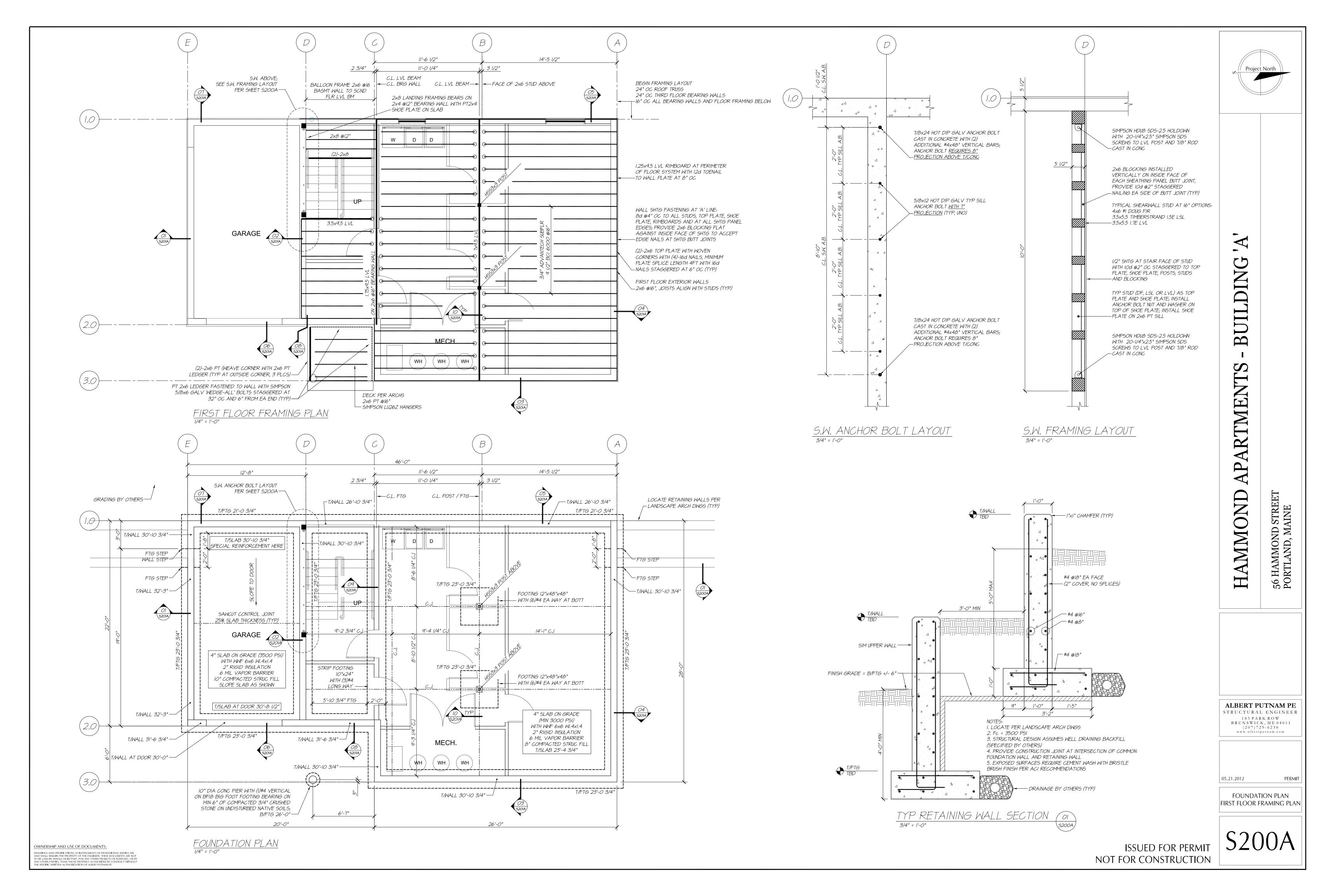
STRE! AINE

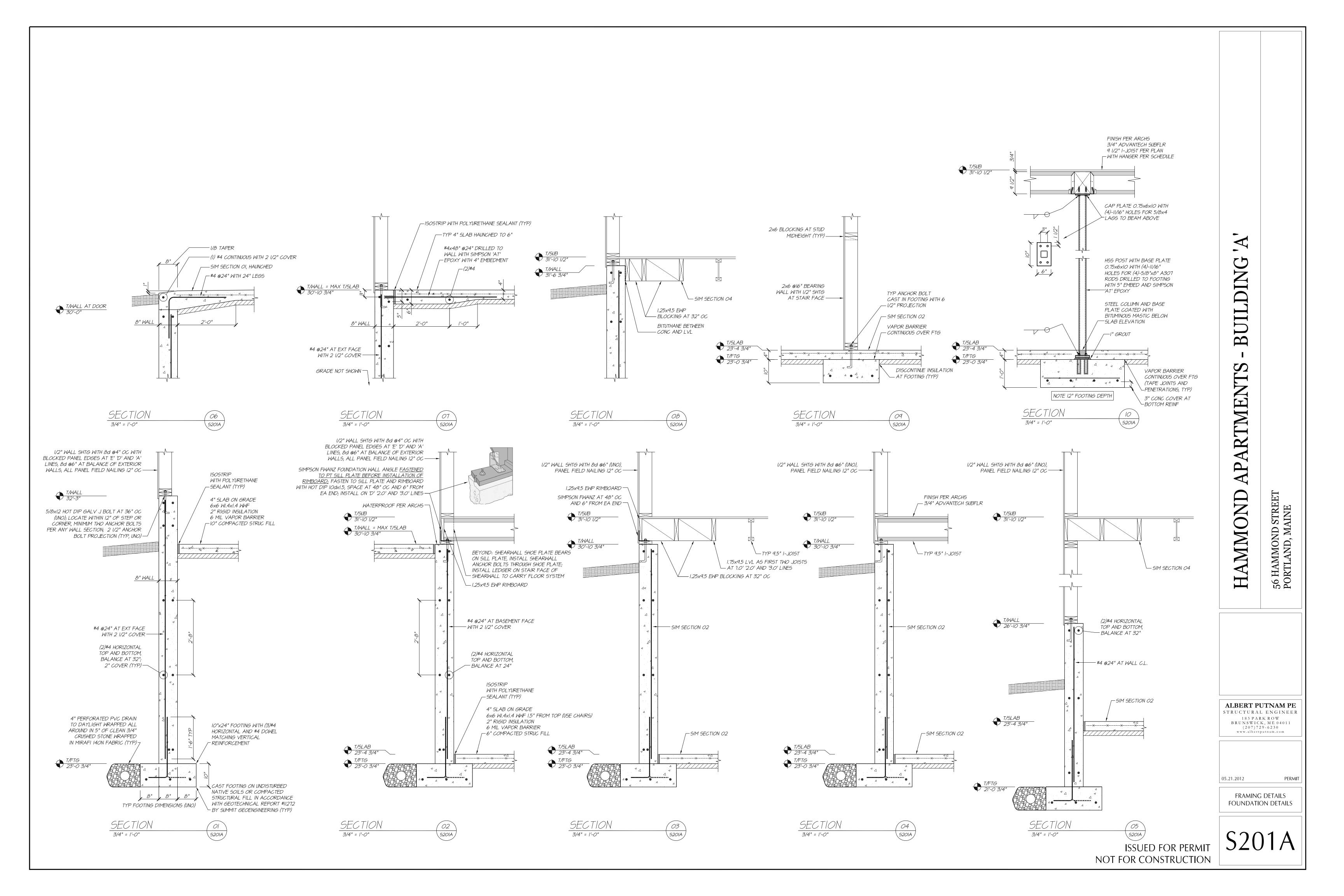
ALBERT PUTNAM PE STRUCTURAL ENGINEER 183 PARK ROW BRUNSWICK, ME 04011 (207)729-6230 www.albertputnam.com

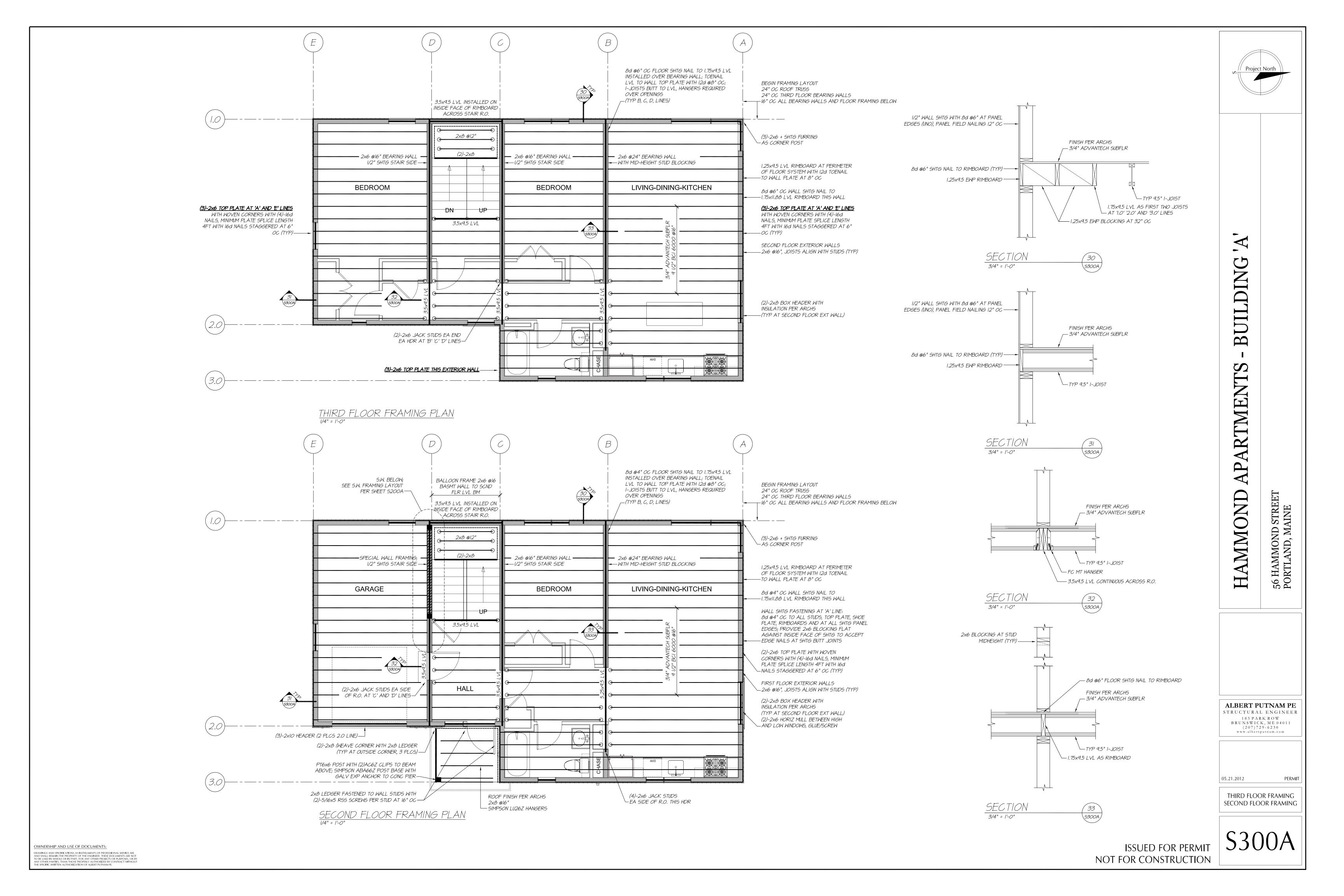
05.21.2012 GENERAL NOTES TYPICAL DETAILS

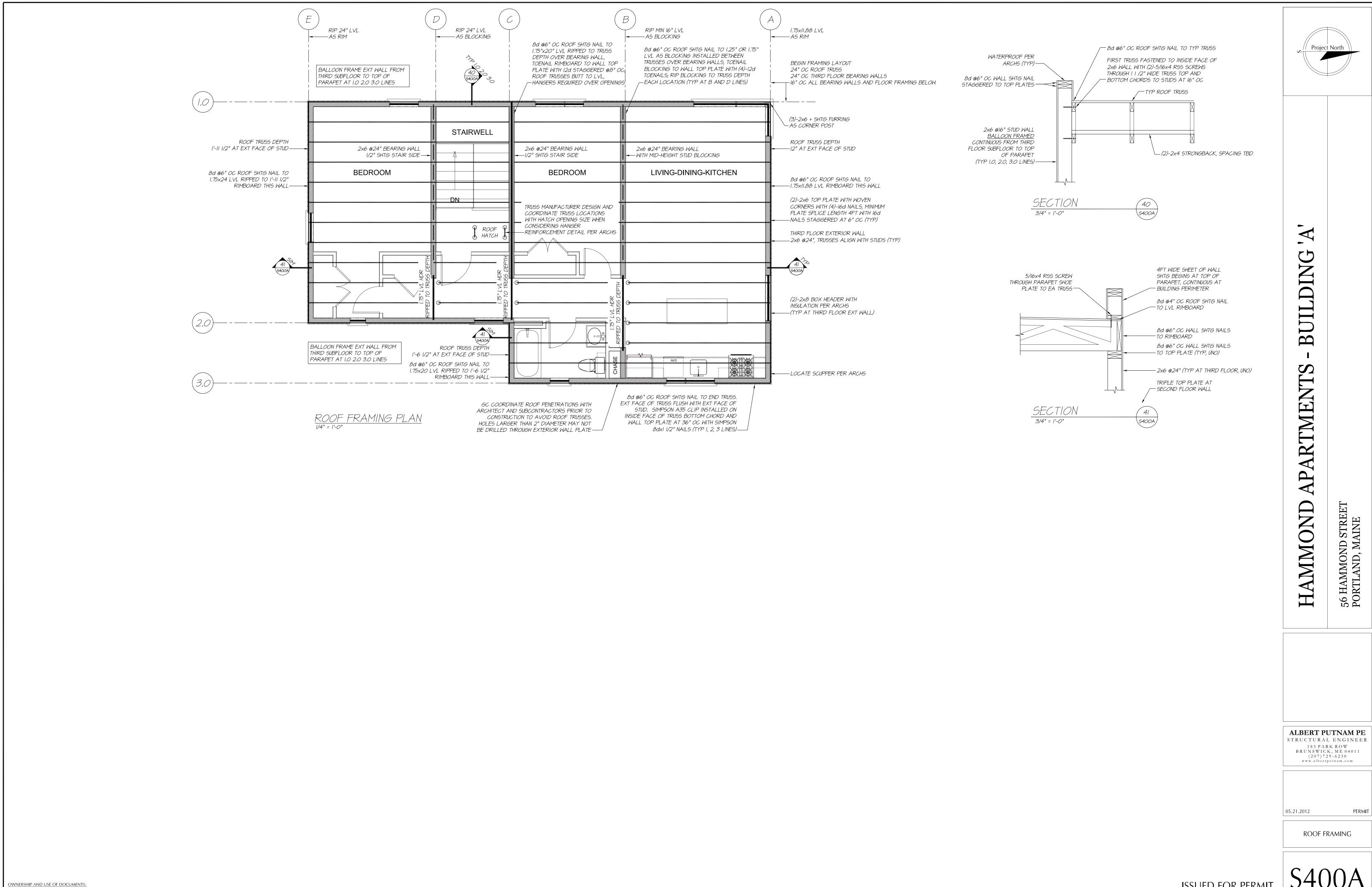
PERMIT

ISSUED FOR PERMIT NOT FOR CONSTRUCTION









DRAWINGS AND SPECIFICATIONS AS INSTRUMENTS OF PROFESSIONAL SERVICE ARE AND SHALL REMAIN THE PROPERTY OF THE ENGINEER. THESE DOCUMENTS ARE NOT TO BE USED IN WHOLE OR IN PART, FOR ANY OTHER PROJECTS OR PURPOSES, OR BY ANY OTHER PARTIES, THAN THOSE PROPERLY AUTHORIZED BY CONTRACT WITHOUT THE SPECIFIC WRITTEN AUTHORIZED BY CONTRACT WITHOUT THE SPECIFIC WRITTEN AUTHORIZATION OF ALBERT PUTNAM PE.

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